

DRAFT

Table C-9
Greenbriar Public Facilities Financing Plan
Water Common Use Factor Calculation

Land Use	Use Factor	Density	Common Use Factor (Use Factor x Density)
Low-Density Residential	608 Gallons per Unit	7.81 du/acre	4,748 Gallons/Acre
Medium-Density Residential	608 Gallons per Unit	12.93 du/acre	7,867 Gallons/Acre
High-Density Residential	371 Gallons per Unit	19.59 du/acre	7,297 Gallons/Acre
MOR - Comm. Commercial	371 Gallons per Unit	22.00 du/acre	8,162 Gallons/Acre
Age-Restricted Apartments	371 Gallons per Unit	28.67 du/acre	9,833 Gallons/Acre
Village Commercial	2,759 Gallons per Acre		2,759 Gallons/Acre
Community Commercial	2,759 Gallons per Acre		2,759 Gallons/Acre

water_EDU

Source: Placer Vineyards Public Facilities Financing Plan & EPS.

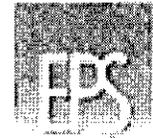
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Table C-10
Greenbriar Public Facilities Financing Plan
Wastewater Common Use Factor Calculation

Land Use	Use Factor	Density	Common Use Factor (Use Factor x Density)
Low-Density Residential	190 Gallons per Unit	7.91 du/acre	1,483 Gallons/Acre
Medium-Density Residential	190 Gallons per Unit	13.93 du/acre	2,646 Gallons/Acre
High-Density Residential	130 Gallons per Unit	19.58 du/acre	2,546 Gallons/Acre
HQR - Comm. Commercial	130 Gallons per Unit	22.03 du/acre	2,860 Gallons/Acre
Age-Restricted Apartments	130 Gallons per Unit	26.67 du/acre	3,467 Gallons/Acre
Village Commercial	850 Gallons per Acre		850 Gallons/Acre
Community Commercial	850 Gallons per Acre		850 Gallons/Acre

Wastewater_EOU

Source: Placer Vineyards Public Facilities Financing Plan & EPS



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APPENDIX D

GREENBRIAR CAPITAL IMPROVEMENT PROGRAM

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Greenbriar
 Table 1. Summary of Improvements (CIP)
 Overall Summary

<u>ON-SITE COSTS</u>	<u>TOTAL ON-SITE PROJECT COST</u>	<u>PHASE 1 FACILITIES</u>	<u>PHASE 2 FACILITIES</u>
Backbone Roadway	\$10,644,570	\$10,238,570	\$405,000
Backbone Sewer	\$3,866,928	\$3,866,928	\$0
Backbone Water	\$5,572,395	\$5,572,395	\$0
Backbone Drain	\$13,551,968	\$11,909,513	\$1,642,454
Backbone Landscaping	\$8,882,441	\$3,937,714	\$4,944,727
TOTAL ON-SITE COST:	\$42,348,301	\$33,516,120	\$8,832,181
<u>OFF-SITE COSTS</u>			
Backbone Roadway	\$20,764,116	\$9,098,702	\$11,665,414
Backbone Sewer	\$2,681,875	\$2,681,875	\$0
Backbone Water	\$4,235,500	\$3,556,980	\$678,520
Backbone Drain	\$1,707,150	\$1,707,150	\$0
Backbone Landscaping	\$0	\$0	\$0
TOTAL OFF-SITE COST:	\$29,379,241	\$16,945,307	\$12,333,934
<u>ON & OFF-SITE COST TOTALS</u>			
Backbone Roadway	\$21,408,686	\$19,338,272	\$12,070,414
Backbone Sewer	\$6,448,803	\$6,448,803	\$0
Backbone Water	\$9,707,895	\$9,129,375	\$608,520
Backbone Drain	\$15,259,118	\$13,607,263	\$1,652,454
Backbone Landscaping, Trails and Soundwalls	\$8,882,441	\$3,937,714	\$4,944,727
TOTAL ON & OFF-SITE COST:	\$71,827,542	\$52,461,427	\$19,366,115

OH-SITE

Greenbriar
Table 2. Summary of Improvements (CIP)
Roadway Infrastructure

Project	Segment	Description	Total Project Cost	Phase 1 Project Cost	Phase 2 Project Cost
OH-SITE					
Minor Way					
R2.1	All Grade Section from Lone Tree Rd to S-36	10' Street Section - Paving at One Side	\$4,672,000	\$4,672,000	
R3.1	On-Off - Weston Way 80' and Tree Road	Collector Side Crossing Bridge	\$1,017,000	\$1,017,000	
R3.2	On-Off - Weston Way 2-Crossings	Collector Side Crossing Bridge	\$2,005,000	\$2,005,000	
R3.3	On-Off - Collector Road 2-Crossings	Collector Side Crossing Bridge	\$1,360,000	\$1,360,000	
Minor Way Sub-Total			\$9,059,000	\$9,059,000	\$0
Collector Road					
R3.1	Over	Bridge Improvement	\$476,000	\$476,000	
Collector Rd Sub-Total			\$476,000	\$476,000	\$0
Signalization					
S2	Intersection of Sones 1 and Sprink 2	2-Phase Traffic Signal	\$525,750	\$525,750	
S3	Intersection of Merion Way and Street 57	4-Phase Traffic Signal	\$425,000		\$425,000
Signalization Improvement Sub-Total			\$950,750	\$950,750	\$425,000
OH-SITE SUB-TOTAL			\$10,485,750	\$10,485,750	\$425,000
OH-SITE					
Stadium Roadway					
R1.1	Lone Tree Road to E. Main Highway 26 Intersection	100' Street Section (Two-Side)	\$5,100,000	\$5,100,000	
R2.1	Lone Tree Road to E. Main Highway 26 Intersection	100' Street Section (Two-Side) (E. Main)	\$1,000,000		\$1,000,000
R2.2	Stadium Interceptor - Westside Station at Lone Tree	400' WB Curb 40' (Two Lane - 800 R)	\$20,400,000	\$20,400,000	
Stadium Rd Sub-Total			\$26,500,000	\$26,500,000	\$1,000,000

Approved By: _____
 (Print Name)

Sheet 046/069

Greenbriar
Table 2. Summary of Improvements (CIP)
Roadway Infrastructure

Project	Segment	Description	Total Project Costs	Phase 1 Project Cost	Phase 2 Project Cost
Major Way					
R2 2	Street 20 to East side of Hwy 31	Safe Right Turnoff at Intersection	\$1,273,900		\$1,273,900
R2 3	East side of Hwy 31 to East Commerce Way	20' Street Section	\$165,210		\$165,210
R2 4	Merle Way @ Metro At Parkway	Right of Way	\$27,500		\$27,500
R2 5	Merle Way @ One Way Road	Right of Way	\$50,750		\$50,750
Major Way Sub-Total:			\$1,417,360	\$0	\$1,417,360
Freeway Interchange / Intersection					
R4 1a	State Route 90 Northbound On Ramp @ Eastern Boulevard	When Signal and Merge of Ramp	\$17,000	\$17,000	
R4 1b	State Route 90 Southbound Off Ramp @ Eastern Boulevard	Right of Way Installation	\$42,000		\$42,000
R2 1	State Route 90 Northbound Merge	Right of Way and Signal	\$23,500	\$23,500	
R2 1	State Route 90 Southbound Off Ramp	Right of Way and Signal	\$14,750		\$14,750
R2 1	US 8 Merge @ Park Drive Southbound Off Ramp	Right of Way and Signal	\$147,750		\$147,750
R2 1	US 8 Merge @ Park Drive Southbound On Ramp	When and Merge of Ramp	\$69,000		\$69,000
Freeway Interchange / Intersection Sub-Total:			\$213,250	\$117,500	\$95,750
Interchanges					
R4 1	East Commerce & Merle Way Intersection Improvements	Installation 3-Way Traffic Signs	\$53,250		\$53,250
Interchange Sub-Total:			\$53,250	\$0	\$53,250
Project Segments					
R2 1	Interstate 1 Widening (Assume 10% for Sheet)	When Merge @ State Route 90 Merge @ Park Drive Southbound On Ramp	\$183,250		\$183,250
Project Segment Sub-Total:			\$183,250	\$0	\$183,250

Change 04/05/07

Greenbriar
Table 2. Summary of Improvements (CIP)
Roadway Infrastructure

Project	Segment	Description	Total Project Cost	Phase 1 Project Cost	Phase 2 Project Cost
Signalization					
S1	Easton Boulevard & Street 61 Signalizer	3-Way Traffic Signal	\$306,250	\$306,250	
S4	Master Way & Street 36 Signalization	4-Way Traffic Signal	\$403,000		\$403,000
S5	Easton Boulevard & Easton Commerce Way Signalizer Improvements	3-Way Traffic Signal	\$378,000	\$378,000	
S6	Easton Boulevard & Lion Tree Signalizer	4-Way Traffic Signal	\$405,000	\$405,000	
S7	Easton Boulevard & Pined Street #2 Signalizer	3-Way Traffic Signal	\$377,000	\$377,000	
S8	Easton Boulevard & Pined Street #3 Signalizer	3-Way Traffic Signal	\$377,000	\$377,000	
Signalization Improvement Sub-Total:			\$2,436,750	\$2,331,750	\$405,000
CIP-SPE SUB-TOTAL:			\$35,724,110	\$9,988,703	\$15,665,414
TOTAL ROADWAY IMPROVEMENTS			\$31,409,666	\$10,336,773	\$11,071,414

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Greenbriar
Table 3. Summary of Improvements (CIP)
Trunk Sewer

Project	Segment	Description	Total Project Costs	Phase 1 Project Costs	Phase 2 Project Costs
ON-SITE					
Lift Station & Force Mains					
S 1.1	Lift Station	On-Site - 2 MG	\$3,987,500	\$1,967,500	
S 2.1	Force Main	On-Site 10-inch Force Main	\$74,527	\$74,527	
Lift Station & Force Mains Sub-Total			\$4,062,027	\$2,042,027	
Gravity Sewer					
S 2.2	Master Man - Street 37 to Street 36	18" Trunk Pipe	\$226,000	\$226,000	
S 2.3	From Mosaic Way at Street 36 to HWY 98	24" Trunk Pipe	\$398,450	\$298,450	
Gravity Sewer Sub-Total			\$624,450	\$524,450	
ON-SITE SUB-TOTAL			\$4,686,477	\$2,566,477	
OFF-SITE					
Gravity Sewer					
S 3.1	Conduit 36" Gravity Sewer from West side of Highway 98, East to East 36" Gravity Trunk Circle	Directional Drilling across HWY 98	\$2,587,875	\$2,587,875	
OFF-SITE SUB-TOTAL			\$2,587,875	\$2,587,875	
TOTAL TRUNK SEWER			\$7,273,952	\$5,154,352	\$2,119,600

Phase 04/2007

Greenbriar
Table 4. Summary of Improvements (CIP)
Water Transmission Main

Project	Segment	Description	Total Project Costs	Phase 1 Project Cost	Phase 2 Project Cost
ON-SITE					
W2.1	Lone Tree Rd	30" Dia. T-Main	\$1,755,000	\$1,755,000	
W3.1	Middle Way	18" Dia. T-Main	\$560,250	\$560,250	
W3.2	Work at Progress from Elkton Blvd to Messer Way	18" Dia. T-Main	\$700,400	\$700,400	
W4.1	Messier Way	Directional Drilling	\$657,700	\$657,700	
W5.1	On-Site Make-Up Water Wells	On-Site Make-Up Water Wells	\$1,880,000	\$1,880,000	\$0
ON-SITE SUB-TOTAL:			\$6,572,395	\$6,572,395	
OFF-SITE					
W1.1	Elkton Blvd from Lone Tree Rd to HWY 99	24" Dia. T-Main	\$844,560	\$844,560	
W1.2	Elkton Northbound off Interchange	24" Dia. T-Main	\$1,578,400	\$1,578,400	
W1.3	Elkton Blvd from HWY 95 to National Blvd	24" Dia. T-Main	\$686,500		\$686,500
W2.2	Circumventing off Interstate 5 by Directional Drilling	Water T-Main Directional Drill	\$1,134,000	\$1,134,000	
OFF-SITE SUB-TOTAL:			\$4,225,500	\$3,568,960	\$656,540
TOTAL WATER TRANSMISSION MAIN			\$10,797,895	\$10,141,355	\$743,540

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Prepared by
 Michael Rodgers, Inc.

Page 6 of 17

Overall Summary of Improvements file
 Water

Project No. 220

Greenbriar
Table 5. Summary of Improvements (CIP)
Trunk Drain

Project	Segment	Description	Total Project Cost	Phase 1 Project Cost	Phase 2 Project Cost
On-Site					
Trunk Drain					
D1.1	On-Site	42" Drain Pipe	\$15,908	\$15,908	
D1.2	On-Site	42" Drain Pipe	\$15,088	\$15,088	
D1.3	On-Site	42" Drain Pipe	\$17,150	\$17,150	
D1.4	On-Site	42" Drain Pipe	\$15,948		\$15,948
D1.5	On-Site	42" Drain Pipe	\$67,319		\$67,319
D1.6	On-Site	36" Drain Pipe	\$85,848		\$85,848
D1.7	On-Site, Top 10 East Drain @ -5	48" HDPE	\$102,212	\$102,212	
D1.8	On-Site	42" Drain Pipe	\$216,967		\$216,967
D1.9	On-Site	48" Drain Pipe	\$66,013		\$66,013
D1.10	On-Site	36" Drain Pipe	\$340,810		\$340,810
D1.11	On-Site	48" Drain Pipe	\$181,891		\$181,891
D1.12	On-Site	42" Drain Pipe	\$236,284		\$236,284
D1.13	On-Site	48" Drain Pipe	\$25,224		\$25,224
D1.14	On-Site	42" Drain Pipe	\$180,181		\$180,181
D1.15	On-Site	36" Drain Pipe	\$168,333		\$168,333
D1.16	On-Site	48" Drain Pipe	\$141,527		\$141,527
D1.17	On-Site	42" Drain Pipe	\$151,546		\$151,546
D1.18	On-Site	36" Drain Pipe	\$22,482		\$22,482
D1.19	On-Site	42" Drain Pipe	\$11,894		\$11,894
D1.20	On-Site	36" Drain Pipe	\$41,826		\$41,826
D1.21	On-Site	36" Drain Pipe	\$15,505		\$15,505
D1.22	On-Site	40" Drain Pipe	\$73,115		\$73,115
D1.23	On-Site	48" Drain Pipe	\$67,990		\$67,990
D1.24	On-Site	42" Drain Pipe	\$180,270		\$180,270
D1.25	On-Site	36" Drain Pipe	\$150,873		\$150,873
D1.26	On-Site	36" Drain Pipe	\$101,157		\$101,157
Trunk Drain Sub-Total			\$3,176,232	\$2,077,777	\$1,098,454

Project: 8435037

Greenbriar
Table 5. Summary of Improvements (CIP)
Trunk Drain

Project	Segment	Description	Total Project Cost	Phase 1 Project Cost	Phase 2 Project Cost
Detention Basin	On-Site	On-Site Detention Basin	\$9,352,768	\$9,352,768	
Detention Basin Outfall	On-Site	On-Site Detention Basin Outfall	\$79,957	\$79,957	
ON-SITE SUB-TOTAL:			\$9,432,725	\$9,432,725	
OFF-SITE					
0.5MVA DIO2	Off-Site Drainage	Add 0.5-C-5 Pumping to 100 1000 Pump Station for 2 and 3 and 4 and 5	\$1,707,750	\$1,707,750	
OFF-SITE SUB-TOTAL:			\$1,707,750	\$1,707,750	\$0
TOTAL TRUNK DRAIN			\$11,140,475	\$11,140,475	\$1,025,454

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 West Management

Final - 4/4/2007

Greenbriar
Table 5. Summary of Improvements (C&I)
Backbone Landscaping, Trails and Soundwalls

Project	Segment	Description	Total Project Costs	Phase 1 Project Cost	Phase 2 Project Cost
CIN-8112					
L1.1	Elk Horn Boulevard Landscape Corridor	Landscape 25' Corridor South of Elk Horn Blvd	\$482,278	\$482,278	
L2.1	Entry Road Landscape Corridor	Landscape 60' Wide Corridor West of the Entry Roadway	\$312,694	\$312,694	
L3.1	Phase 1 Freeway Buffer Landscape Corridor	Phase 1 Landscape Freeway Buffer North of Master Way	\$1,435,725	\$1,435,725	
L3.2	Phase 2 Freeway Buffer Landscape Corridor	Phase 2 Landscape Freeway Buffer South of Master Way	\$2,634,471		\$2,634,471
L4.1	Master Way Slope Bank	Landscape Master Way slope bank west of overpass.	\$450,920	\$450,920	
L5.1	Light Rail ROW	Interior Landscaping for LRT ROW Corridor	\$546,485		\$546,485
SW-1	Elk Horn Landscape Corridor Soundwall (12')	Perimeter Soundwalls Pursuant to the OEIR	\$469,800	\$469,800	
SW-2.1	Phase 1 Lane Tree Canal Walk (5')	Perimeter Soundwalls Pursuant to the DEIR	\$228,150	\$228,150	
SW-2.2	Phase 2 Lane Tree Canal Walk (5')	Perimeter Soundwalls Pursuant to the DEIR	\$121,634		\$121,634
SW-3.1	Phase 1 Highway 89 Soundwall (6')	Perimeter Soundwalls Pursuant to the DEIR	\$118,403	\$118,403	
SW-3.2	Phase 2 Highway 89 (12' Soundwall) (10')	Perimeter Soundwalls Pursuant to the DEIR	\$327,443		\$327,443
SW-4.1	Phase 1 Master Way Soundwall (8')	Perimeter Soundwalls Pursuant to the DEIR	\$175,000	\$175,000	
SW-4.2	Phase 2 Master Way Soundwall (6')	Perimeter Soundwalls Pursuant to the DEIR	\$658,175		\$658,175
TS1.1	Phase 1 Trail System - Open Space Buffer	(12' Pavement w/ 2' Shoulders each side)	\$254,136	\$254,136	
TS1.2	Phase 2 Trail System - Open Space Buffer	(10' Pavement w/ 2' Shoulders each side)	\$536,625		\$536,625
Subtotal CIN-8112			\$4,882,441	\$1,837,714	\$4,744,727

Prepared By: West Engineers, Inc.

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**Greenbriar
Draft Capital Improvement Program
Table 7. Summary of Improvements (CIP)
Detailed Summary of Costs, Reimbursements and Credits
Backbone Infrastructure and Improvements**

Item	Cost Detail		Reimbursement/Credit Detail				
	Estimated Cost	Estimated Reimb. / Credit	Net Cost	NINFFP	Metro Air Park Drainage Improvements	CFD 97-01 Sacramento (Water)	ESD-1 Frank Sewer
Roadway Infrastructure							
ON-SITE							
MEADOW WAY							
R2.1	\$4,672,000	\$1,356,348	\$3,315,652		\$1,356,348		
R10.1	\$1,012,500	\$0	\$1,012,500				
R19.2	\$2,025,000	\$0	\$2,025,000				
R19.3	\$1,350,000	\$0	\$1,350,000				
Collector Road							
R3.1	\$876,320	\$0	\$876,320				
Signalization							
S2	\$303,750	\$0	\$303,750				
S3	\$405,000	\$2	\$405,000				
Subtotal On-Site	\$10,844,370	\$1,356,348	\$9,288,222	\$0	\$1,356,348	\$0	\$0
OFF-SITE							
Elkborn Boulevard							
R1.1	\$5,185,852	\$2,093,453	\$3,091,999		\$2,093,453		
R22.1	\$1,068,156	\$1,045,872	\$22,284		\$1,045,872		
R22.2	\$32,400	\$0	\$32,400				
MEADOW WAY							
R2.2	\$9,273,936	\$5,307,895	\$2,068,041	\$1,325,000	\$3,982,895		
R2.3	\$105,272	\$0	\$105,272				
R2.4	\$27,000	\$0	\$27,000				
R2.5	\$33,750	\$0	\$33,750				
Overall Summary of Improvements							
Reim. Summary							

Prepared by
Wood Rodgers Inc.

Printed 6/1/2007

Backbone Infrastructure and Improvements

Item	Cost Detail		Reimbursements/Credit Detail					
	Estimated Cost	Estimated Reimb. / Credit	Net Cost	NMPPFP	Metro Air Pain	CFD 97-C1 Drainage Improvements	City of Sacramento (Water)	CSD-1 Trunk Sewer
Firewater Wastewater / Intersection								
R4 1a	\$1,179,900	\$1,134,364	\$45,536	\$615,208	\$519,156	Note: NM-PPFP at 34% + signal.		MAP at 44%
R4 1b	\$472,500	\$368,550	\$103,950	\$160,656	\$207,900	Note: NM-PPFP at 34%.		MAP at 44%
R20 1	\$229,500	\$0	\$229,500					
R21 1	\$141,750	\$0	\$141,750					
R23 1	\$141,750	\$0	\$141,750					
R24 1	\$639,900	\$0	\$639,900					
Intersection								
R4 3	\$533,250	\$0	\$533,250					
Fireway Segments								
R25 1	\$263,250	\$0	\$263,250					
Signalization								
S1	\$506,250	\$0	\$506,250					
S4	\$405,000	\$0	\$405,000		\$215,600			
S5	\$178,000	\$215,600	\$162,400					
S6	\$405,000	\$0	\$405,000					
S7	\$371,250	\$0	\$371,250					
S8	\$371,250	\$0	\$371,250					
Subtotal Off-Site	\$50,704,118	\$10,165,729	\$40,538,389	\$2,100,858	\$8,064,876	\$0	\$0	\$0
Total for Roadway	\$31,406,666	\$11,522,062	\$19,884,604	\$2,100,858	\$8,421,224	\$0	\$0	\$0

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Backbone Infrastructure and Improvements

Item	Cost Detail		Net Cost	Reimbursements/Credit Detail				
	Estimated Cost	Estimated Reimb / Credit		City of Sacramento (Water)	Metro Air Park	CFD 97.01 Drainage Improvements	City of Sacramento (Water)	CSD-1 Trunk Sewer
<u>Trunk Sewer</u>								
<u>ON-SITE</u>								
Lift Station & Force Mains								
S1.1	\$3,267,000	\$3,267,000	\$0					\$3,267,000
S2.1	\$74,621	\$74,621	\$0					\$74,621
Gravity Sewer								
S2.2	\$226,902	\$226,902	\$0					\$226,902
S2.3	\$298,405	\$298,405	\$0					\$298,405
Subtotal On-Site	\$3,866,928	\$3,866,928	\$0		\$0	\$0		\$3,866,928
<u>OFF-SITE</u>								
Gravity Sewer								
S3.1	\$2,581,875	\$3,366,235	(\$785,060)		\$785,060			\$2,581,875
Subtotal Off-Site	\$2,581,875	\$3,366,235	(\$785,060)		\$785,060	\$0		\$2,581,875
Total for Sewer	\$6,448,803	\$7,233,163	(\$785,060)		\$785,060	\$0		\$6,448,803

Overall Summary of Improvements.xls
Reimb Summary

Prepared by:
Wood Rodgers Inc.

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Backbone Infrastructure and Improvements

Item	Cost Detail		Reimbursement/Credit Detail					
	Estimated Cost	Estimated Reimb./ Credit	Net Cost	NNIPFP	Metric Air Park	CFD 97-01 Drainage Improvements	City of Sacramento (Water)	CSD #1 Trunk Sewer
Water Transmission Main								
ON-SITE								
W2.1	\$1,755,000	\$1,755,000	\$0				\$1,755,000	
W3.1	\$550,250	\$550,250	\$0				\$550,250	
W3.2	\$709,425	\$709,425	\$0				\$709,425	
W4.1	\$657,720	\$657,720	\$0				\$657,720	
W5.1	\$1,890,000	\$0	\$1,890,000					
Subtotal On-Site	\$5,572,395	\$3,682,395	\$1,890,000	\$0	\$0	\$0	\$3,682,395	\$0
OFF-SITE								
W1.1	\$644,560	\$644,560	\$0				\$644,560	
W1.2	\$1,578,420	\$1,578,420	\$0				\$1,578,420	
W1.3	\$666,520	\$666,520	\$0				\$666,520	
W2.2	\$1,134,000	\$1,134,000	\$0				\$1,134,000	
Subtotal Off-Site	\$4,225,500	\$4,225,500	\$0	\$0	\$0	\$0	\$4,225,500	\$0
Total for Water	\$9,797,895	\$7,907,895	\$1,890,000	\$0	\$0	\$0	\$7,907,895	\$0

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Backbone Infrastructure and Improvements

Item	Cost Detail		Net Cost	Reimbursement/Credit Detail			
	Estimated Cost	Estimated Reimb. / Credit		M299 Air Park	CFD 97-01 Drainage Improvements	City of Sacramento (Water)	USD-1 Trunk Sewer
Trunk Drain							
ON-SITE							
D1.1	\$252,968	\$0	\$252,968				\$0
D1.2	\$169,068	\$0	\$169,068				\$0
D1.3	\$170,150	\$0	\$170,150				\$0
D1.4	\$150,548	\$0	\$150,548				\$0
D1.5	\$63,319	\$0	\$63,319				\$0
D1.6	\$85,848	\$0	\$85,848				\$0
D1.7	\$102,219	\$0	\$102,219				\$0
D1.8	\$210,967	\$0	\$210,967				\$0
D1.9	\$66,013	\$0	\$66,013				\$0
D1.10	\$242,910	\$0	\$242,910				\$0
D1.11	\$182,891	\$0	\$182,891				\$0
D1.12	\$238,555	\$0	\$238,555				\$0
D1.13	\$251,224	\$0	\$251,224				\$0
D1.14	\$192,181	\$0	\$192,181				\$0
D1.15	\$188,937	\$0	\$188,937				\$0
D1.16	\$193,521	\$0	\$193,521				\$0
D1.17	\$153,588	\$0	\$153,588				\$0
D1.18	\$52,480	\$0	\$52,480				\$0
D1.19	\$77,894	\$0	\$77,894				\$0
D1.20	\$51,825	\$0	\$51,825				\$0
D1.21	\$110,903	\$0	\$110,903				\$0
D1.22	\$73,115	\$0	\$73,115				\$0
D1.23	\$61,990	\$0	\$61,990				\$0
D1.24	\$190,270	\$0	\$190,270				\$0
D1.25	\$150,873	\$0	\$150,873				\$0
D1.26	\$100,157	\$0	\$100,157				\$0

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Backbone Infrastructure and Improvements

Item	Cost Detail		Reimbursements/Credit Debits					
	Estimated Cost	Estimated Reimb. / Credit	Net Cost	NIHFFP	Metco Air Park	CFD 97-01 Drainage Improvements	City of Sacramento (Water)	CSD-1 Trunk Sewer
Dewater Basin D10.1	\$9,302,769	\$0	\$9,302,769					\$0
Dewater Basin Outfall D20.1	\$518,867	\$0	\$518,867					\$0
Subtotal On-Site	\$13,581,968	\$0	\$13,581,968	\$0	\$0	\$0	\$0	\$0
OFF-SITE								
D30.1 & D30.2	\$1,707,750	\$1,707,750	\$0			\$1,707,750		
Subtotal Off-Site	\$1,707,750	\$1,707,750	\$0	\$0	\$0	\$1,707,750	\$0	\$0
Total for Drainage	\$15,289,718	\$1,707,750	\$13,581,968	\$0	\$0	\$1,707,750	\$0	\$0

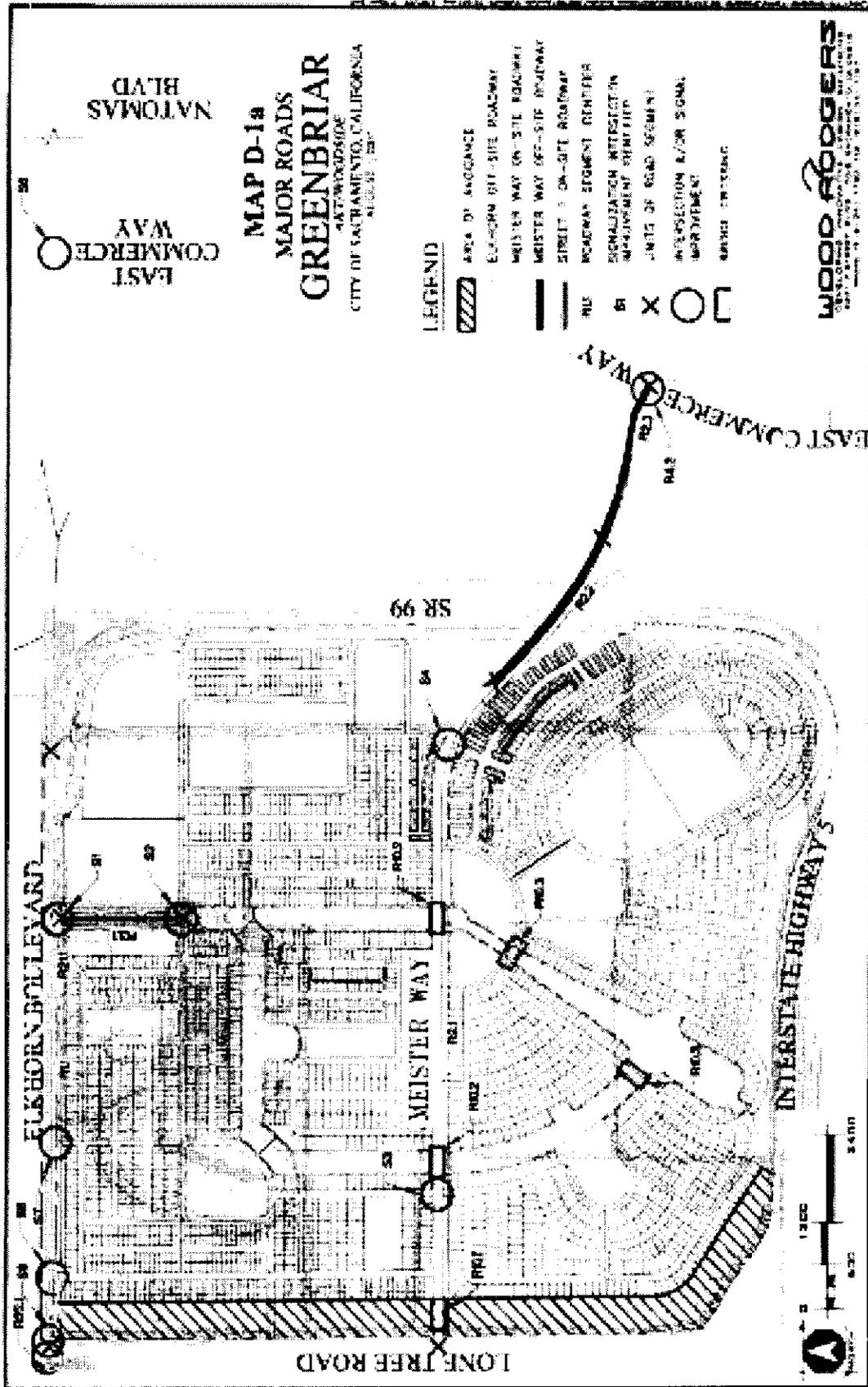
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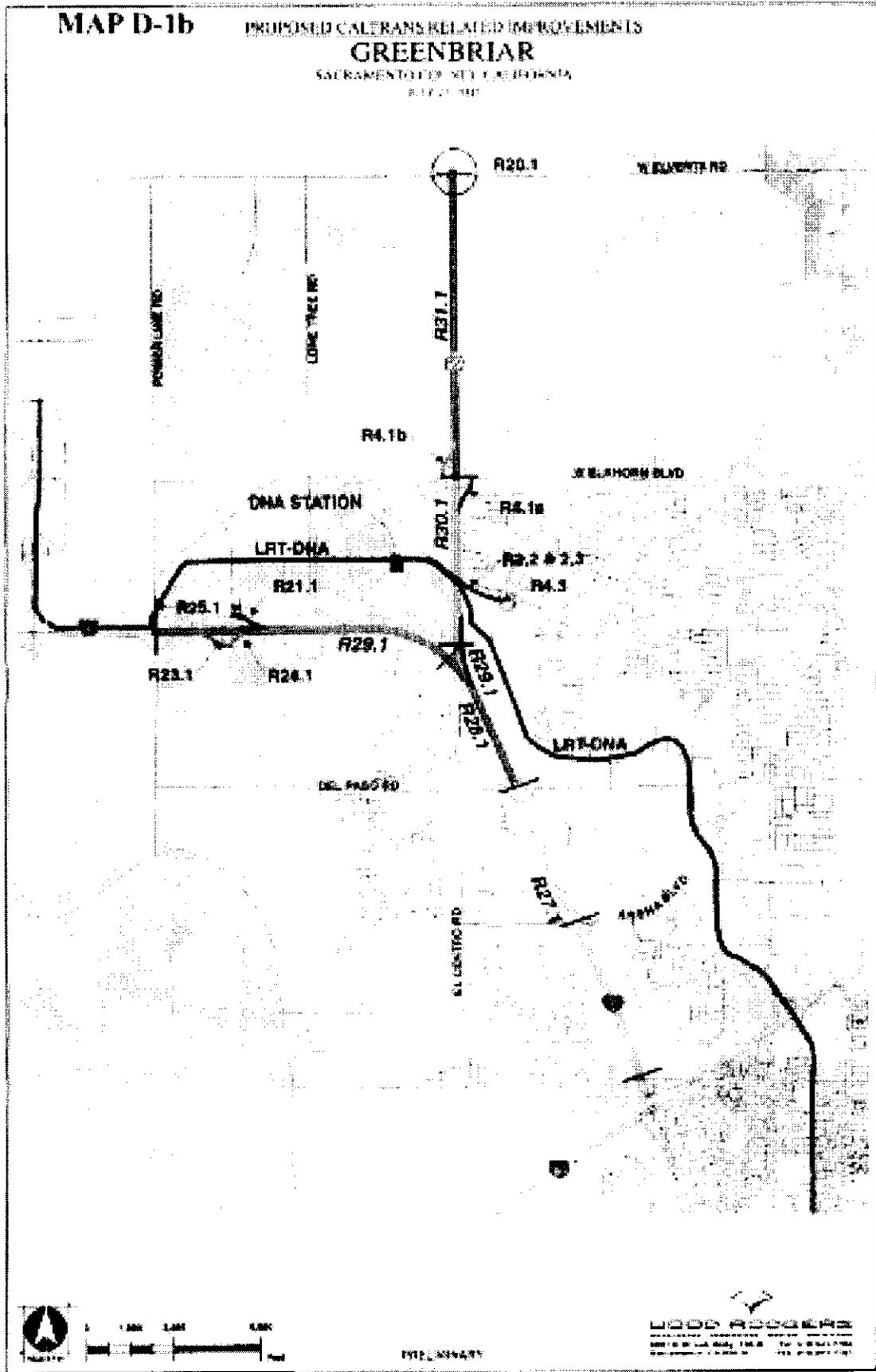
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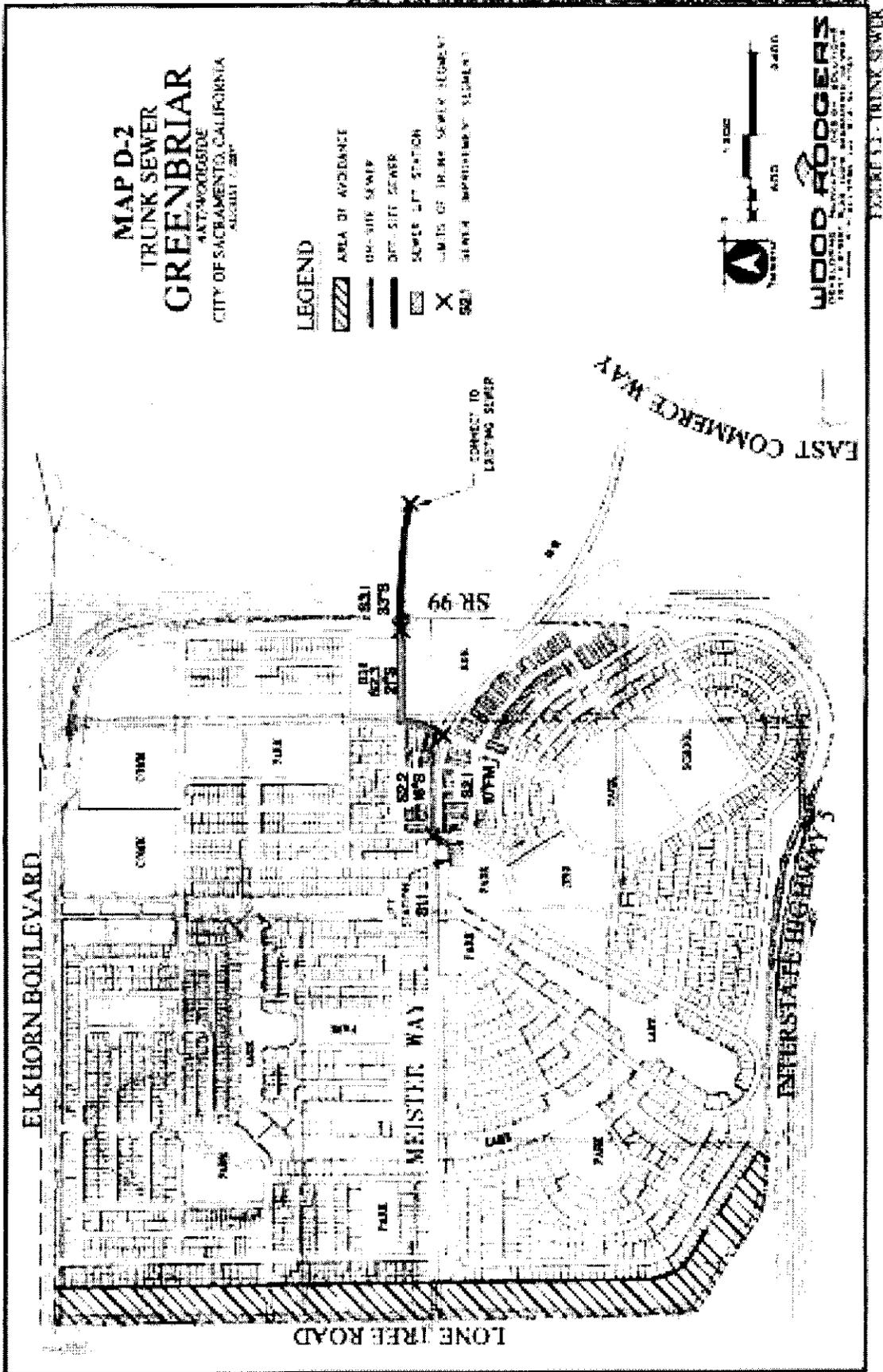
Backbone Infrastructure and Improvements

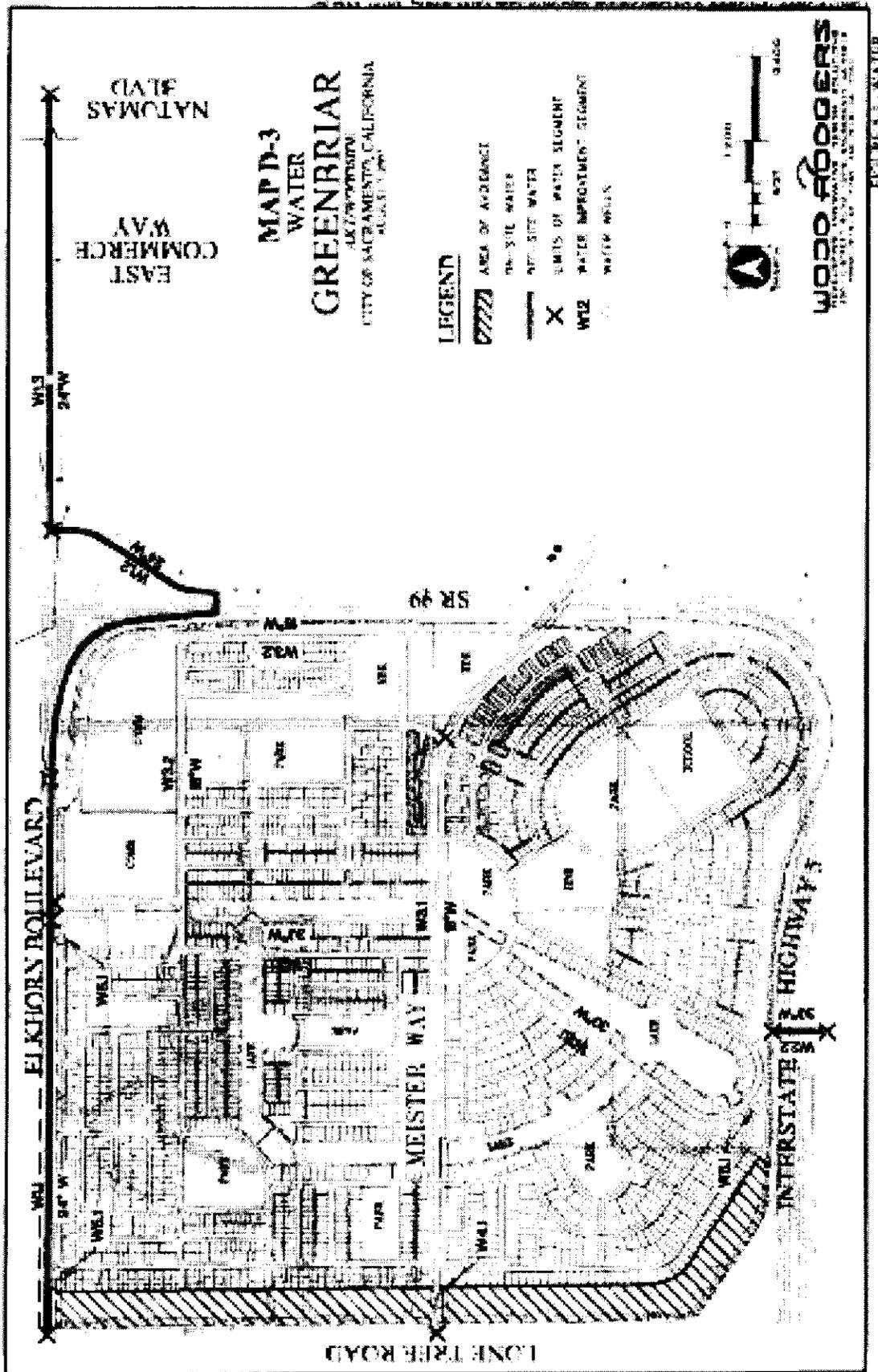
Item	Cost Detail		Net Cost	Reimbursement/Credit Detail			
	Estimated Cost	Estimated Reimb / Credit		Metra Air Park	CFD 97-01 Drainage Improvements	City of Sacramento (Waste)	CSD-1 Trunk Sewer
Backbone Landscaping, Trails and Soundwalls							
ON SITE							
L1.1	\$482,278	\$0	\$482,278				
L2.1	\$312,694	\$0	\$312,694				
L3.1	\$1,435,725	\$0	\$1,435,725				
L3.2	\$2,604,471	\$0	\$2,604,471				
L4.1	\$450,300	\$0	\$450,300				
L5.1	\$546,480	\$0	\$546,480				
SW-1	\$469,800	\$0	\$469,800				
SW-2.1	\$228,150	\$0	\$228,150				
SW-2.2	\$121,534	\$0	\$121,534				
SW-3.1	\$118,463	\$0	\$118,463				
SW-3.2	\$327,443	\$0	\$327,443				
SW-4.1	\$175,568	\$0	\$175,568				
SW-4.2	\$606,175	\$0	\$606,175				
TS1.1	\$254,138	\$0	\$254,138				
TS1.2	\$536,625	\$0	\$536,625				
Subtotal On-Site	\$8,882,441	\$0	\$8,882,441	\$0	\$0	\$0	\$0
OFF SITE							
Total for Landscaping	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Total for Landscaping	\$8,882,441	\$0	\$8,882,441	\$0	\$0	\$0	\$0
Grand Total	\$71,637,542	\$28,371,560	\$43,265,982	\$2,160,858	\$1,707,750	\$7,907,895	\$6,448,803

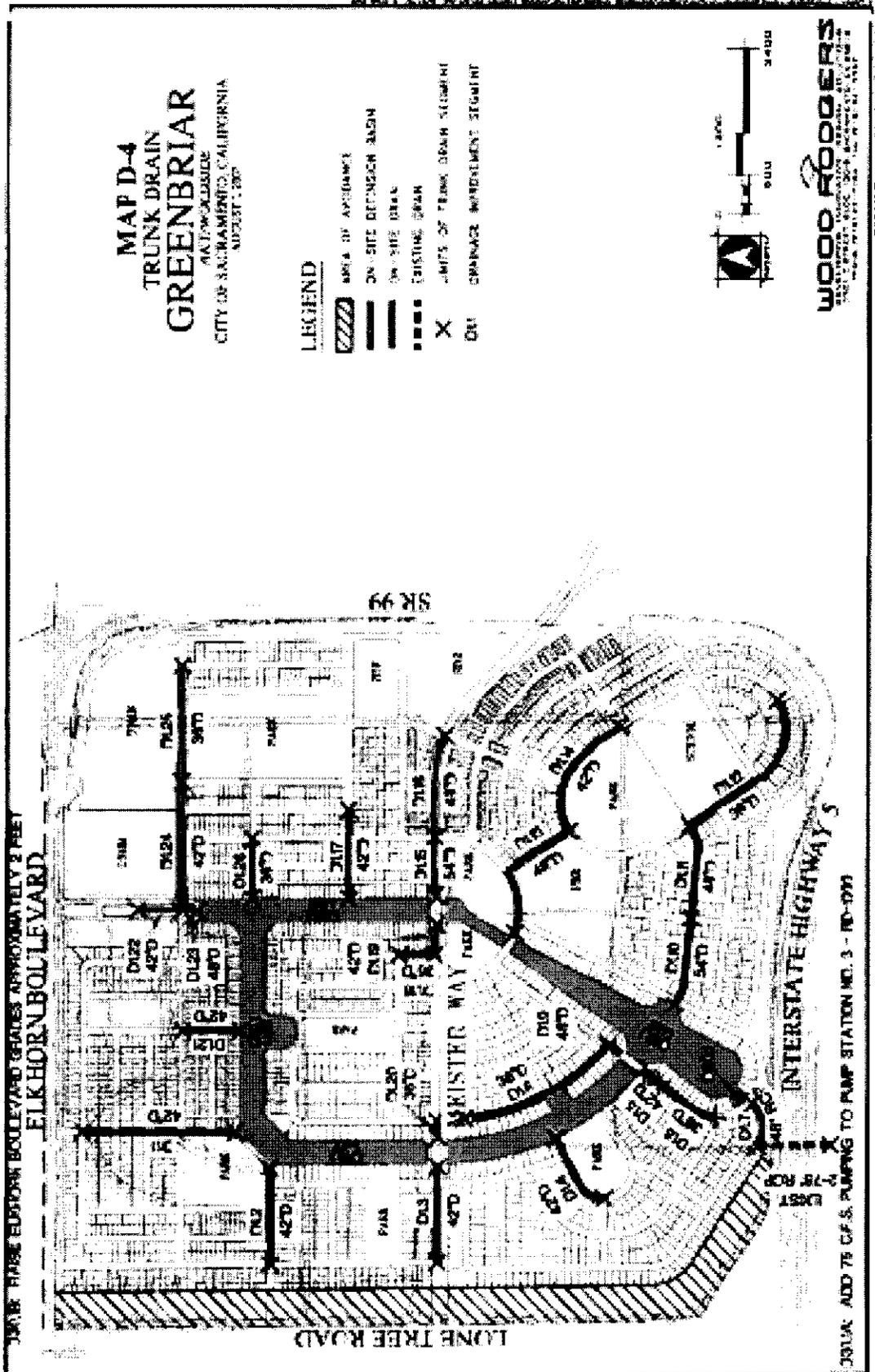
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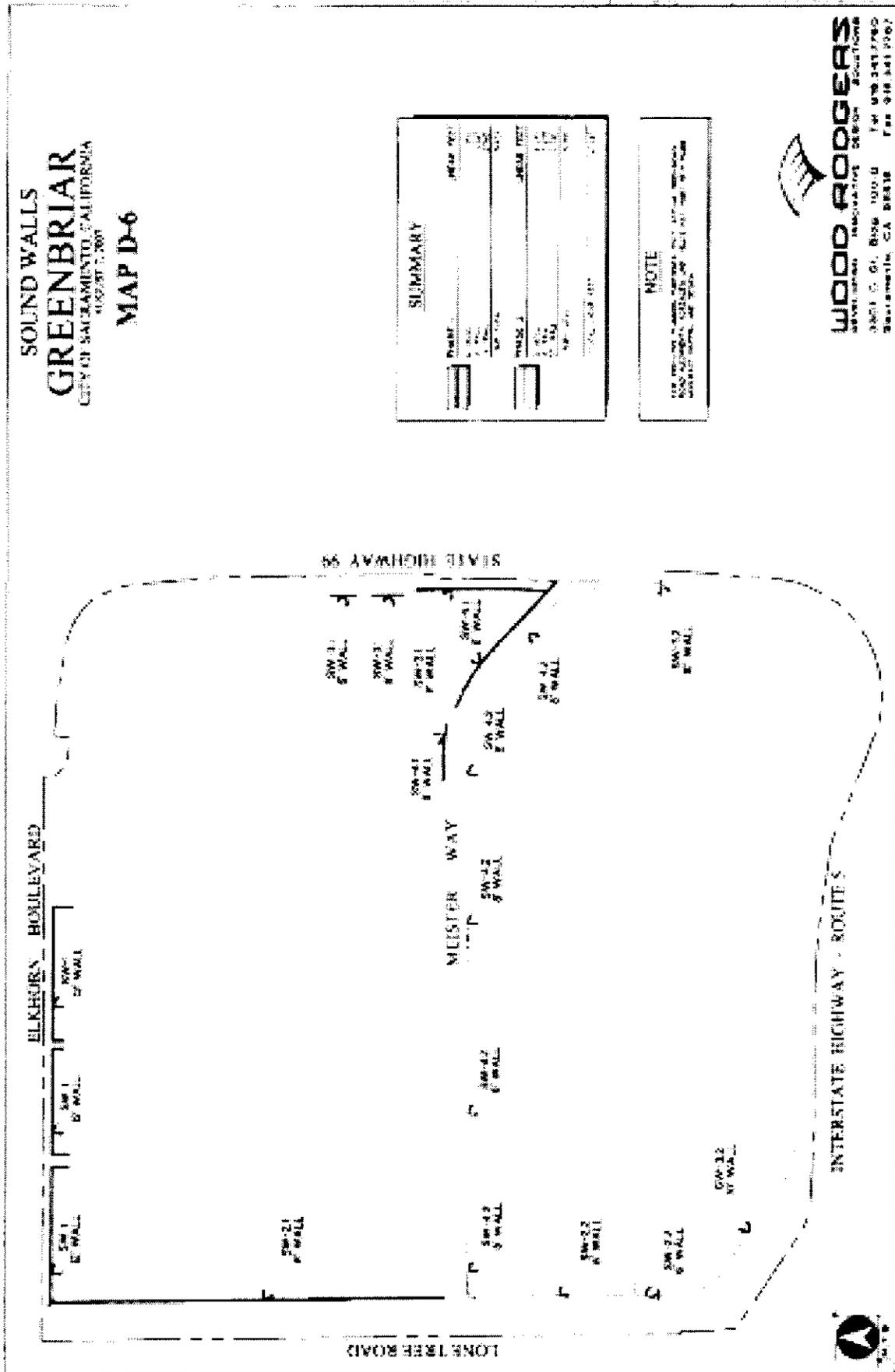














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APPENDIX E

CFD No. 97-01 BUY-IN CALCULATION
CFD No. 97-01 CREDITABLE FACILITIES

**Greenbriar Annexation
CFD 97-01**

Issue: What is the "catch-up" tax amount for the Greenbriar Annexation to annex into CFD 97-01?

Assumptions: Gross acres = 577
 Annexation Year = 10 (FY 2007)
 Parcels drains to the West of I-5
 Parcels within the Finance Plan Area designated in the 1994 Community Plan
 Parcels currently not in CFD 97-01
 Parcels are undeveloped
 Parcels are unmapped

Solution: Maximum Special Tax for Undeveloped Parcels West of I-5
 Land Use Category 5 (Tentative Map Parcels or Unmapped Parcels)
 Gross Acres = 577

Fiscal Year	Rate (per gross acres)	Maximum Special Tax Amount
1998	\$350.00	\$201,950.00
1999	\$357.00	\$205,989.00
2000	\$364.14	\$210,108.78
2001	\$371.42	\$214,310.96
2002	\$378.85	\$218,597.17
2003	\$386.43	\$222,969.17
2004	\$394.16	\$227,428.50
2005	\$402.04	\$231,977.07
2006	\$410.08	\$236,616.61
2007	\$418.28	\$241,348.94
Totals =	\$3,832.40	\$2,211,296.16

Conclusion: Based on the assumptions provided above and based on the information provided about in the 'Assignment and Collection of Catch-up Tax' in the Final Report and Rate and Method of Apportionment (RMA) for CFD 97-01, the catch-up tax amount to annex into CFD 97-01 for the Greenbriar Annexation is **\$2,211,296.16**.

Note: Rate increases at 2.0 % per year

Printed 8/7/2007

**Greenbriar - AKT/Woodside
Capital Improvement Plan
Opinion of Probable Cost**

D30.1 & D30.2

Off-Site Drainage

Add 30-CFS Pumping to RD 1000 Pump Station No. 3 and Raise Elkhorn 2'

<u>Item</u>	<u>Quantity</u>	<u>Unit</u>	<u>Estimated Unit Cost</u>	<u>Total</u>
<u>Pump Station No. 3 Upgrades</u>				
30 CFS Pump Upgrade	75	cfs	\$15,000.00	\$1,125,000
<u>Elkhorn Boulevard</u>				
Raise approx. 2'	4,000	#	\$35.00	\$140,000
Sub-Total				\$1,265,000
35% Engineering & Contingency (for estimated costs)				\$442,750
Grand Total				\$1,707,750

*Note: This cost is creditable against CFD 97-01



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APPENDIX F

MAINLINE FREEWAY-WIDENING
OPINION OF PROBABLE COSTS

Greenbriar
 CIP Estimate
 Opinion of Probable Cost

DRAFT EXHIBIT A
 13-Jul-07

**Mainline Freeway Widening
 Summary**

<u>Item</u>	<u>Existing Lanes</u>	<u>Proposed Lanes</u>	<u>Total Est. Cost</u>	<u>Project Est. Share</u>
I-5 (I-80 to Del Paso)	6	8	\$8,016,506	\$228,983
I-5 (Del Paso to 99/70)	4	6	\$8,587,587	\$243,985
I-5 (99/70 to Power Line)	4	6	\$16,316,415	\$168,912
H 99/70 (I-5 to Elkhorn Blvd)	4	6	\$4,723,173	\$251,450
H 99/70 (Elkhorn Blvd to Elverta Road)	4	6	\$8,587,587	\$153,228
North I-5 to North 99/70 Ramp	1	2	\$1,288,128	\$59,135
Total			\$46,519,866	\$1,135,804

1. The cost for these improvements are derived from the approved Caltrans Project Study Report (PSR) titled "Elkhorn Blvd Interchange Modification, Elverta Road Interchange and Meeker Way Overcrossing" dated June 1999
2. The cost index from 1999 to 2007 is based on California State Department of Transportation, Summary, Price Index for Selected Highway Construction Items, First quarter Ending March 31, 2007, Prepared by the Division of Engineering Services, May 10, 2007
3. The Cumulative Plus Project Peak Hour Traffic values are based on the Table 6.1-40 of the Re-calculated Draft EIR dated June 2007 Table 6.1-38

Printed 9/15/2007

Greenbriar
CIP Estimate
Opinion of Probable Cost

DRAFT EXHIBIT B
13-Jan-07

Mainline Freeway Widening
Determine Cost Per Mile for Mainline Widening
Cost based on Caltrans PSR dated 1999 for Highway 99/70 Improvements:

<u>Item</u>	<u>Quantity</u>	<u>Unit</u>	<u>Unit Cost</u>	<u>Total</u>
<u>Determine Cost Per Mile for Mainline Widening</u>				
1. Widen 99/70 1-Lane each direction (I-5 to Elverta Road) (Based on PSR)	1	LS	\$ 6,973,000	\$ 6,973,000
2. Revised Total Based On Construction Index Increase (Based on Caltrans Price Index, Prepared May 10, 2007)	1.414	Multiplier		\$ 9,859,822
3. 15% Engineering, Inspection and Construction Management				\$ 3,450,938
Total Construction 1-Lane Each Direction				\$ 13,310,760
<u>Per Mile Calculation</u>				
A. Length (I-5 to Elverta)	3.1	Miles		
B. Pro rata cost per mile (2-lanes)				\$ 4,293,783
B. Pro rata cost per mile (1-lane) @ 50%				\$ 2,146,891

Notes:

- The cost for these improvements are derived from the approved Caltrans Project Study Report (PSR) titled "Elkhorn Blvd Interchange Modification, Elverta Road Interchange and Meeker Way Overcrossing" dated June 1999.
- The cost index from 1999 to 2007 is based on California State Department of Transportation, Summary, Price Index for Selected highway Construction Items, First quarter Ending March 31, 2007, Prepared by the Division of Engineering Services, May 10, 2007.

Greenbriar
CIP Estimate
Opinion of Probable Cost

DRAFT EXHIBIT C
13-Jul-07

R27.1
I-5 (I-80 to Del Paso)
Widening 2 to 6 Lanes

Item	Quantity	Unit	Unit Cost	Total
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Determine Cost for I-5 Widening (I-80 to Del Paso Road)

Cost per Lane Mile
(See Machine Cost Per Mile Worksheet) **\$2,146,897**

Widen I-5 (I-80 to Del Paso)

Local Number of Lanes	6	Lanes		
Proposed Number of Lanes	8	Lanes		
Delta Widening	2	Lanes		
Cost Per Mile of Widening	2	Lanes/Mile	\$2,146,897	\$4,293,793
Total Estimated Cost	2.1	Miles	\$4,293,793	\$9,016,966

Calculate Fair Share Percentage (Use Cumulative Plus Project Method)

Note: Use Traffic Study Volumes I-5 (I-80 to Arroyo Blvd)

AM Peak Fair Share Percentage

1. NB Plus Project AM Peak Hour Traffic (From Traffic Study)	10,527	Trips		
2. SB Plus Project AM Peak Hour Traffic (From Traffic Study)	7,412	Trips		
3. Total Plus Project AM Peak Hour Traffic (NB and SB)	17,939	Trips		
4. NB No Project AM Peak Hour Traffic (From Traffic Study)	10,254	Trips		
5. SB No Project AM Peak Hour Traffic (From Traffic Study)	7,001	Trips		
6. Total No Project AM Peak Hour Traffic (NB and SB)	17,255	Trips		
7. AM Project Traffic (3-6 above)	444	Trips		
8. AM Project Percentage (7 / 3 above)	2.46%			

PM Peak Fair Share Percentage

1. NB Plus Project PM Peak Hour Traffic (From Traffic Study)	7,856	Trips		
2. SB Plus Project PM Peak Hour Traffic (From Traffic Study)	11,238	Trips		
3. Total Plus Project PM Peak Hour Traffic (NB and SB)	19,094	Trips		
4. NB No Project PM Peak Hour Traffic (From Traffic Study)	7,621	Trips		
5. SB No Project PM Peak Hour Traffic (From Traffic Study)	10,196	Trips		
6. Total No Project PM Peak Hour Traffic (NB and SB)	17,817	Trips		
7. PM Project Traffic (3-6 above)	449	Trips		
8. PM Project Percentage (7 / 3 above)	2.34%		Use	

Calculate Fair Share Cost

Fair Share Cost	7.54%	\$9,016,966	\$228,983
Total Project Share			\$228,983

Notes

1. The Peak Hour Traffic values are based on the Table 6.1-40 of the Reconstructed Draft EIR dated June 2007, Table 6.1-36.

Revised 10/20/07

Greenbriar
 CIP Estimate
 Division of Protective Cost

DRAFT EXHIBIT D
 13-Jul-07

R26.1
 I-9 (Del Paso to SR70)
 Widening 4 to 8 Lanes

Item	Quantity	Unit	Unit Cost	Total
Delimiting Cost for I-9 Widening (Del Paso Road to SR70)				
Cost per Lane Mile (Base Machine Cost Per Mile Worksheet)			\$2,146,587	
Widen I-9 (I-99 to Del Paso)				
Exist Number of Lanes	4	Lanes		
Proposed Number of Lanes	8	Lanes		
Delta Widening	4	Lanes		
Cost Per Mile of Widening	4	Lanes/Mile	\$2,146,587	\$8,587,587
Total Estimated Cost	1.0	Miles	\$8,587,587	\$8,587,587

Calculate Fair Share Percentage (Use Computation Plus Project Volumes)
 Note: Use Traffic Study Volumes to (North of Del Paso Road)

AM Peak Fair Share Percentage

1. NB Plus Project AM Peak Hour Traffic (From Traffic Study)	9,665	Trips		
2. SB Plus Project AM Peak Hour Traffic (From Traffic Study)	6,339	Trips		
3. Total Plus Project AM Peak Hour Traffic (NB and SB)	16,004	Trips		
4. NB No Project AM Peak Hour Traffic (From Traffic Study)	9,668	Trips		
5. SB No Project AM Peak Hour Traffic (From Traffic Study)	6,160	Trips		
6. Total No Project AM Peak Hour Traffic (NB and SB)	15,828	Trips		
7. AM Project Traffic (1-6 above)	387	Trips		
8. AM Project Percentage (7/3 above)	2.35%			

PM Peak Fair Share Percentage

1. NB Plus Project PM Peak Hour Traffic (From Traffic Study)	8,478	Trips		
2. SB Plus Project PM Peak Hour Traffic (From Traffic Study)	10,243	Trips		
3. Total Plus Project PM Peak Hour Traffic (NB and SB)	18,721	Trips		
4. NB No Project PM Peak Hour Traffic (From Traffic Study)	8,248	Trips		
5. SB No Project PM Peak Hour Traffic (From Traffic Study)	9,997	Trips		
6. Total No Project PM Peak Hour Traffic (NB and SB)	18,245	Trips		
7. PM Project Traffic (1-6 above)	476	Trips		
8. PM Project Percentage (7/3 above)	2.54%		Use	

Calculate Fair Share Cost

Fair Share Cost	2.94%		\$8,587,587	\$2493,995
Total Project Share				\$2493,995

Notes

1. The Peak Hour Traffic values are based on the Table 6.1-4D of the Recirculated Draft EIR dated June 2007 Table 6.1-3E

Greenbriar
 CIP Estimate
 Opinion of Probable Cost

DRAFT EXHIBIT E
 15-Jul-07

R29.1
 I-5 (SR70 to Power Line)
 Widening 4 to 8 Lanes

Item	Quantity	Unit	Unit Cost	Total
Determine Cost for I-5 Widening (SR70 to Power Line Road)				
Cost per Lane Mile (See Material Cost Per Mile Worksheet)			\$2,146,897	
Widen I-5 (I-88 to Del Paso)				
Existing Number of Lanes	4	Lanes		
Proposed Number of Lanes	8	Lanes		
Delta Widening	4	Lanes		
Cost Per Mile of Widening	4	Lanes/Mile	\$2,146,897	\$8,587,597
Total Estimated Cost	1.0	Miles	\$8,587,597	\$16,316,412

Calculate Fair Share Percentages (Use Cumulative Plus Project Volumes)
 Note: Use Traffic Study Volumes I-5 (East of Power Line Road)

AM Peak Fair Share Percentage:

1. NB Plus Project AM Peak Hour Traffic (From Traffic Study)	5,221	Trips
2. SB Plus Project AM Peak Hour Traffic (From Traffic Study)	2,222	Trips
3. Total Plus Project AM Peak Hour Traffic (NB and SB)	10,000	Trips
4. NB No Project AM Peak Hour Traffic (From Traffic Study)	4,222	Trips
5. SB No Project AM Peak Hour Traffic (From Traffic Study)	1,753	Trips
6. Total No Project AM Peak Hour Traffic (NB and SB)	4,975	Trips
7. AM Project Traffic (3-6 above)	50	Trips
8. AM Project Percentage (7 / 3 above)	0.46%	

PM Peak Fair Share Percentage:

1. NB Plus Project PM Peak Hour Traffic (From Traffic Study)	3,606	Trips
2. SB Plus Project PM Peak Hour Traffic (From Traffic Study)	2,280	Trips
3. Total Plus Project PM Peak Hour Traffic (NB and SB)	11,236	Trips
4. NB No Project PM Peak Hour Traffic (From Traffic Study)	3,673	Trips
5. SB No Project PM Peak Hour Traffic (From Traffic Study)	2,269	Trips
6. Total No Project PM Peak Hour Traffic (NB and SB)	11,981	Trips
7. PM Project Traffic (3-6 above)	75	Trips
8. PM Project Percentage (7 / 3 above)	0.63%	Less

Calculate Fair Share Cost:

Fair Share Cost	0.67%	\$8,316,415	\$108,912
Total Project Share			\$108,912

Notes:

- 1. The Peak Hour Traffic values are based on the Table B-1-42 of the Recirculated Draft EIR dated June 2007 Table B-1-36.

Greenbriar
 CIP Estimate
 Comparison of Probable Cost

DRAFT EXHIBIT F
 13-JUL-07

R38.1
 H 9970 (I-5 to Elkhorn Blvd)
 Widening A to B Lanes

Item	Quantity	Unit	Unit Cost	Total
Determine Cost for H 9970 Widening I-5 to Elkhorn Blvd				
Cost per Lane Mile (See Markise Cost Per Mile Worksheet)			\$2,146,897	
Widening I-5 I-80 to Del Paso				
Existing Number of Lanes	4	Lanes		
Proposed Number of Lanes	6	Lanes		
Delta Widening	2	Lanes		
Cost Per Mile of Widening	2	Lanes/Mile	\$2,146,897	\$4,293,795
Total Estimated Cost	1.1	Miles	\$4,293,795	\$4,723,323

Calculate Fair Share Percentage (Use Cumulative Plus Project Volume)
 Note: Use Traffic Study Volumes for 9970 I-5 to Elkhorn Blvd

AM Peak Fair Share Percentage

1. NB Plus Project AM Peak Hour Traffic (From Traffic Study)	4,171	Trips	
2. SB Plus Project AM Peak Hour Traffic (From Traffic Study)	2,133	Trips	
3. Total Plus Project AM Peak Hour Traffic (NB and SB)	7,293	Trips	
4. NB No Project AM Peak Hour Traffic (From Traffic Study)	3,900	Trips	
5. SB No Project AM Peak Hour Traffic (From Traffic Study)	2,947	Trips	
6. Total No Project AM Peak Hour Traffic (NB and SB)	6,895	Trips	
7. AM Project Traffic (1-6 above)	355	Trips	
8. AM Project Percentage (7 / 3 above)	4.87%		

PM Peak Fair Share Percentage

1. NB Plus Project PM Peak Hour Traffic (From Traffic Study)	3,312	Trips	
2. SB Plus Project PM Peak Hour Traffic (From Traffic Study)	2,629	Trips	
3. Total Plus Project PM Peak Hour Traffic (NB and SB)	5,941	Trips	
4. NB No Project PM Peak Hour Traffic (From Traffic Study)	3,061	Trips	
5. SB No Project PM Peak Hour Traffic (From Traffic Study)	3,417	Trips	
6. Total No Project PM Peak Hour Traffic (NB and SB)	6,496	Trips	
7. PM Project Traffic (1-6 above)	413	Trips	
8. PM Project Percentage (7 / 3 above)	6.36%		Use

Calculate Fair Share Cost

Fair Share Cost	6.36%	\$4,723,323	\$301,493
Total Project Share			\$301,493

Notes:

1. The Peak Hour Traffic values are based on the Table G 1-40 of the Recirculated Draft EIR dated June 2007, Table G 1-36.

Project 05-03900

Greenbriar
 CIP Estimate
 Optional Probable Cost

DRAFT EXHIBIT G
 13-Jul-07

R31.1
 H 9970 (Elkhorn Blvd to Elverta Road)
 Widening 4 to 6 Lanes

Item	Quantity	Unit	Unit Cost	Total
Determine Cost for H 9970 Widening (Elkhorn Blvd to Elverta Road)				
Cost per Lane Mile (See Marking Cost Per Mile Worksheet)			12,146,897	
Width 4-6 to 60 to 60 Feet				
Exist Number of Lanes	4	Lanes		
Proposed Number of Lanes	6	Lanes		
Delta Widening	2	Lanes		
Cost Per Mile of Widening	2	Lanes/Mile	\$2,548,797	\$4,281,793
Total Estimated Cost	2.0	Miles	\$4,281,793	\$4,281,793

Calculate Fair Share Percentage (Use Currents Plus Project Volume)
 Note: Use Traffic Study Volume H 99 Elkhorn Blvd to Elverta Road

AM Peak Hour Share Percentage

1. NB Plus Project AM Peak Hour Traffic (From Traffic Study)	2,272	Trips	
2. SB Plus Project AM Peak Hour Traffic (From Traffic Study)	2,628	Trips	
3. Total Plus Project AM Peak Hour Traffic (NB and SB)	4,900	Trips	
4. NB No Project AM Peak Hour Traffic (From Traffic Study)	2,231	Trips	
5. SB No Project AM Peak Hour Traffic (From Traffic Study)	2,278	Trips	
6. Total No Project AM Peak Hour Traffic (NB and SB)	4,509	Trips	
7. AM Project Traffic (1-6 above)	91	Trips	
8. AM Project Percentage (7 / 3 above)	1.78%		Use

PM Peak Hour Share Percentage

1. NB Plus Project PM Peak Hour Traffic (From Traffic Study)	2,756	Trips	
2. SB Plus Project PM Peak Hour Traffic (From Traffic Study)	1,756	Trips	
3. Total Plus Project PM Peak Hour Traffic (NB and SB)	4,512	Trips	
4. NB No Project PM Peak Hour Traffic (From Traffic Study)	2,698	Trips	
5. SB No Project PM Peak Hour Traffic (From Traffic Study)	2,554	Trips	
6. Total No Project PM Peak Hour Traffic (NB and SB)	4,700	Trips	
7. PM Project Traffic (1-6 above)	(188)	Trips	
8. PM Project Percentage (7 / 3 above)	-4.69%		Negative NA

Calculate Fair Share Cost

Fair Share Cost	1.78%	\$6,567,587	\$153,029
Total Project Share			\$153,029

Notes:

- The Peak Hour Traffic values are based on the Table 6.1-42 of the Recirculated Draft EIR dated June 2007, Table 6.1-3E

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Greenbriar
 CIP Estimate
 Opinion of Probable Cost

DRAFT EXHIBIT H
 13-Jul-07

PC11.1
 North I-5 to North 9970 Ramp
 Widening 1 to 2 Lanes

Item	Quantity	Unit	Unit Cost	Total
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Determine Cost for North I-5 to North 9970 Ramp

Cost per Lane Mile **\$2,146,897**
 (See Mainline Cost Per Mile Worksheet)

Widen I-5 (I-50 to Del Paso)

Exist. Number of Lanes	1	Lanes		
Proposed Number of Lanes	2	Lanes		
Delta Widening	1	Lanes		
Cost Per Mile of Widening	1	Lanes/Mile	\$2,146,897	\$2,146,897
Total Estimated Cost	0.6	Miles	\$2,146,897	\$1,288,138

Calculate Fair Share Percentage (Use Cumulative Plus Project Volume)

Note: Use Traffic Study Volumes I-5 North to 9970 North Off Ramp

AM Peak Fair Share Percentage

1. NB Plus Project AM Peak Hour Traffic (From Traffic Study)	3,875	Trips
2. NB No Project AM Peak Hour Traffic (From Traffic Study)	3,590	Trips
3. AM Project Traffic (3-6 above)	180	Trips
4. AM Project Percentage (3 / 1 above)	4.63%	

PM Peak Fair Share Percentage

1. NB Plus Project PM Peak Hour Traffic (From Traffic Study)	2,901	Trips
2. NB No Project PM Peak Hour Traffic (From Traffic Study)	2,565	Trips
3. PM Project Traffic (3-6 above)	218	Trips
4. PM Project Percentage (3 / 1 above)	7.71%	Use

Calculate Fair Share Cost

Fair Share Cost	7.71%	\$1,288,138	\$99,335
Total Project Share			\$99,335

Notes:

- The Peak Hour Traffic values are based on the Table 6.1-40 of the Recirculated Draft EIR dated June 2007, Table 6.1-36

ATTACHMENT 7 – INCLUSIONARY HOUSING PLAN RESOLUTION

RESOLUTION NO. 2007-XXXX
Adopted by the Sacramento City Council
Date

ADOPTING AN INCLUSIONARY HOUSING PLAN FOR THE GREENBRIAR PROJECT

BACKGROUND

- A. The Mixed Income Housing Policy adopted in the City of Sacramento Housing Element and required by the City's Mixed Income Housing Ordinance, requires that ten percent of the units in a residential development project be affordable to very low income households and five percent to low income households;
- B. The City Council conducted a public hearing on _____, 2007 concerning the above Inclusionary Housing Plan and based on documentary and oral evidence submitted at the public hearing, the Council hereby finds:
 - 1. The proposed Plan is consistent with Chapter 17.190 of the City Code which requires an Inclusionary Housing Plan setting forth the number, unit mix, location, structure type, affordability and phasing of the Inclusionary Units in the residential development;

BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:

- A. Section 1. The City Council adopts the Inclusionary Housing Plan for the Greenbriar project, attached hereto as Exhibit A.

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Exhibit A: Inclusionary Housing Plan – 7 Pages

EXHIBIT A – DRAFT INCLUSIONARY HOUSING PLAN



October 10, 2007

Arwen Wacht
Sacramento City Planning Department
915 I Street, 3rd Floor
Sacramento, CA 95814

Re: Greenbriar Complete and Acceptable Inclusionary Housing Plan

Dear Ms. Wacht:

I am writing in regards to the Greenbriar development project, project number IR04-463. As you know, this project is subject to the City of Sacramento Mixed Income Housing Ordinance, Chapter 17.190 of the City Code.

On October 2, 2007, pursuant to Section 17.190.110(A), the Developer, North Natomas 575 Investors, L.L.C., prepared an Inclusionary Housing Plan (Plan) and submitted it to the Sacramento Housing and Redevelopment Agency (SHRA). SHRA has reviewed this Plan and finds it complete and acceptable under the Ordinance.

SHRA is now forwarding this Plan to the City for final review and recommendation for adoption by the Planning Director, subject to the follow condition.

- The Developer shall enter into an affordable housing agreement with the Sacramento Housing and Redevelopment Agency which will be recorded against the entire development project prior to the recordation of the first final map.

Should you have any questions, please feel free to contact me at (916) 440-1353.

Sincerely,

A handwritten signature in black ink, appearing to read "Christine Weichert".

Christine Weichert
Assistant Director
Housing and Community Development

(Faint, illegible text, possibly a stamp or footer)

Greenbriar
Mixed Income Housing Ordinance Compliance Plan
October 2, 2007

Proposed Project

North Natomas 575 Investors, LLC, (the “Developer”) is the owner and developer of certain real property in the City of Sacramento [ASSUMES POST SOI AMENDMENT] on which it proposes to develop and construct the Greenbriar Project (the “Project”), a low, medium, and high density residential community. The approximately 577 gross acre Project is generally located northwest of the intersection of State Route 99 (SR 99) and Interstate 5 (I-5). The Project consists of a total of 2,991 dwelling units.

Mixed Income Housing Policy

The Project site is subject to the City’s Mixed Income Housing Policy. The Mixed Income Housing Policy adopted in the City of Sacramento Housing Element and required by the City’s Mixed-Income Housing Ordinance, City of Sacramento City Code Chapter 17.190 requires that ten percent (10%) of the units in a Residential Project be affordable to very low income households and five percent (5%) to low income households (collectively the “Inclusionary Requirement” and “Inclusionary Units”).

Pursuant to City Code Section 17.190.110 (B), an Inclusionary Housing Plan (“Plan”) must be approved prior to or concurrent with the approval of legislative or adjudicative entitlements for the Project. City Code Section 17.190.030 sets forth the guidelines for the number, unit mix, location, structure type, affordability and phasing of the Inclusionary Units in the Project. This document constitutes the Plan, and, as supplemented and amended from time to time, is intended to begin implementation of the Inclusionary Requirement for the Project. All future approvals for the Project shall be consistent with this Inclusionary Housing Plan.

The Inclusionary Requirement for the Project will be set forth in more detail in the Inclusionary Housing Agreement executed by Developer and the Sacramento Housing and Redevelopment Agency (“SHRA”) and recorded against all the residential units in the Residential Project. The Inclusionary Housing Agreement shall be executed no later than the approval of the master parcel map (i.e., Final Map) for the Residential Project and shall be recorded concurrently with the master parcel map. The Inclusionary Housing Agreement will describe with particularity the site and building schematics, phasing and income and sales restrictions for the construction and financing of the Inclusionary Units, pursuant to City Code Section 17.190.110 (C). The Inclusionary Housing Agreement shall be consistent with this Plan.

Number of Inclusionary Units

The Developer, or its successors and assignees, shall construct or cause to be constructed a number of dwelling units affordable to Very Low Income Households (“Very Low Income Units”) and Low Income Households (“Low Income Units”) as defined in the Sacramento City Code Section 17.190.020, equal to ten percent (10%) and five percent (5%) of the total number of housing units approved for the Project, respectively.

Based on the current Project proposal of 2,991 residential units, the Inclusionary Requirement for the Project is 299 Very Low Income Units (10%) and 150 Low Income Units (5%).

Total Number of Residential Units within Project:	100%	2,991 Units
Very Low Income Units:	10%	299 Units
Low Income Units:	5%	150 Units
Total Number of Inclusionary Units:	15%	449 Units

If the Project approvals are amended to increase the number of units in the Project, this Plan will be amended to reflect a number of equal to ten percent (10%) of the increased total residential units in the amended entitlements for Very Low Income Units and five percent (5%) for Low Income Units. If the Project approvals are amended to decrease the number of residential units in the Project, this Plan will be amended to reflect a number equal to ten percent (10%) of the decreased total residential units in the amended entitlements for Very Low Income Units and five percent (5%) for Low Income Units. However, after a building permit has been issued for a structure to contain Inclusionary Units, those Units will be constructed and maintained as Inclusionary Units pursuant to the terms of Chapter 17.190 of the City Code regardless of any subsequent reduction in the number of approved total residential units.

Type and Location of Inclusionary Units

Based on the current Project proposal of 2,991 residential units, the Inclusionary Housing Units shall consist of 449 total rental units divided between Lots A, C and D of the proposed Project and shall be retained by the Developer, its heirs, successors, or assigns as rental units affordable to Low and Very Low Income households. If the total unit count of the Project changes, then the new number of Inclusionary Units will be adjusted upward or downward accordingly. The Developer, its heirs, successors, or assigns shall retain the units for a minimum of 30 years and shall ensure that they are professionally managed by an accredited property management company.

Inclusionary Units shall be located on-site within Lots A, C and D of the Greenbriar Project as part of senior and family oriented multi-family residential developments.

Lot A, consisting of approximately 9.7 net acres, is designated as a Medium Density Residential ("MDR") and zoned R-3-PUD, which will allow up to 29-units-per acre. Lot C, consisting of approximately 5.1 net acres, is designated as a Medium Density Residential ("MDR") and zoned R-3-PUD, which will allow up to 29-units-per acre. Lot D, consisting of approximately 3.9 Net acres, is designated as Medium Density Residential ("MDR") and zoned R-3-PUD, which will allow up to 29-units-per acre. These densities may be increased in the future pursuant to State density bonus law. The buildings will have up to four stories and measure up to 55 feet to the top of the roof, as allowed under the R-3-PUD zoning designation.

It is anticipated that the approximately 449 rental units will be spread across the three parcels. Approximate rental unit types and bedroom counts are as follows:

Approximate rental unit types, bedroom counts and unit sizes for Greenbriar:

Affordability Orientation	Approx. Unit Count	Inclusionary Units	Unit Distribution	Unit Type	Approx. Unit Size
Family Affordable (Lot A)	199	132 very low and 67 low units distributed proportionally by unit type/size	0% to 30%	1 Bedroom	550sf to 800sf
			30% to 70%	2 Bedroom	700sf to 1,000sf
			20% to 40%	3 Bedroom	1,000sf to 1,300sf
Senior Affordable (Lot C)	142	95 very low and 47 low units distributed proportionally by unit type/size	0% to 30%	1 Bedroom	550sf to 800sf
			30% to 70%	2 Bedroom	700sf to 1,000sf
			20% to 40%	3 Bedroom	1,000sf to 1,300sf
Family Affordable (Lot D)	108	72 very low and 36 low units distributed proportionally by unit type/size	60% to 80%	1 Bedroom	550sf to 800sf
			20% to 40%	2 Bedroom	700sf to 1,000sf
TOTAL	449	299 very low 150 low			

Affordability Requirements

The Inclusionary Units will be offered as rental units. These rental units will meet the requirements of City Code Section 17.190.030 regarding tenure and tenancy, number and affordability of units, their location, timing of development, unit sizes, exterior appearance and development standards. The rental units will be available to low and very low income households. Monthly Affordable Rents (including utility allowances) of the Inclusionary Units shall be restricted to Low and Very Low Income Households. A unit whose occupancy is restricted to a Very Low Income Household has a monthly rent that does not exceed one-twelfth of thirty percent (30%) of fifty percent (50%) of the Sacramento area median income, adjusted for family size. A unit whose occupancy is restricted to a Low Income Household has a monthly rent that does not exceed one-twelfth of thirty percent (30%) of eighty percent (80%) of the Sacramento area median income, adjusted for family size. Median income figures are those published annually by the United States Department of Housing and Urban Development. With respect to each Inclusionary Unit, the affordability requirements of this Plan shall continue for no less than thirty (30) years from the time the project is in compliance. Compliance is reached after ninety (90) days have passed with the project at ninety percent (90%) occupancy.

Incentives

The Developer or builder may seek incentives, assistance, or subsidies pursuant to City Code Section 17.190.040 such as the allowance for fee reductions for those units fulfilling the Inclusionary Requirement. The Developer will work with the City to determine the fee reductions and other incentives available.

Marketing

The Developer shall be required to market Inclusionary Units in the same manner as non-Inclusionary Units. Such marketing activities may include newspaper and internet advertisements, toll free project information numbers, and on- or off-site project signage.

Phasing of Development of Inclusionary Units

The Inclusionary Units shall be developed concurrently with the development of the remaining units in the Project, as may be further defined in Sacramento City Code Section 17.190.030(D). The nature of the concurrency is defined by a series of linkages between approvals of the market rate units and the development of the Inclusionary Units.

Market Rate Housing/Inclusionary Unit Linkages

The following describes the relationship of market rate development activity to the Inclusionary Unit development activity. These milestones are outlined to ensure that the development of affordable units occurs concurrent with development of market rate units:

- The Inclusionary Housing Plan shall be approved concurrent with the approval of the Project's Tentative Subdivision Map.
- The Inclusionary Housing Agreement shall be executed prior to the recording of the large lot map (or "Parcel Map") for the Residential Project.
- The Inclusionary Housing Agreement shall be recorded concurrently with the recordation of the large lot map (or "Parcel Map") for the Residential Project.
- Inclusionary Units shall generally be built concurrently with the market-rate units within the Project consistent with the following provision.
- Up to 65% of the market rate for sale residential building permits may be issued prior to issuance of building permits for all buildings containing inclusionary units. The remaining 35% of market rate for sale residential building permits may be issued after issuance of all building permits for the buildings containing inclusionary units.
- Marketing of inclusionary units within the Project shall occur concurrently with the marketing of market rate units.

Amendment and Administration of Inclusionary Housing Plan

The Planning Director, with the advice of the Executive Director of SHRA, shall administer this Inclusionary Housing Plan. The Planning Director may make minor administrative amendments to the text of this Plan as provided in Sacramento City Code Section 17.190.110(B)(2)(d).

ATTACHMENT 8 - GENERAL PLAN AMENDMENT RESOLUTION

RESOLUTION NO. 2007-XXXX

Adopted by the Sacramento City Council

Date

AMENDING THE GENERAL PLAN LAND USE MAP RELATING TO THE GREENBRIAR PANHANDLE PROJECT (M05-046 / P05-069)

BACKGROUND

- A. The Planning Commission conducted a public hearing on _____, 2007, and the City Council conducted a public hearing on _____, 2007 concerning the above plan amendment and based on documentary and oral evidence submitted at the public hearing, the Council hereby finds:
1. The proposed land use amendment is compatible with the surrounding land uses;
 2. The subject site is suitable for single-family residential, multi-family residential, parks/open space, schools, and commercial land uses; and
 3. The proposal is generally consistent with the policies of the General Plan.

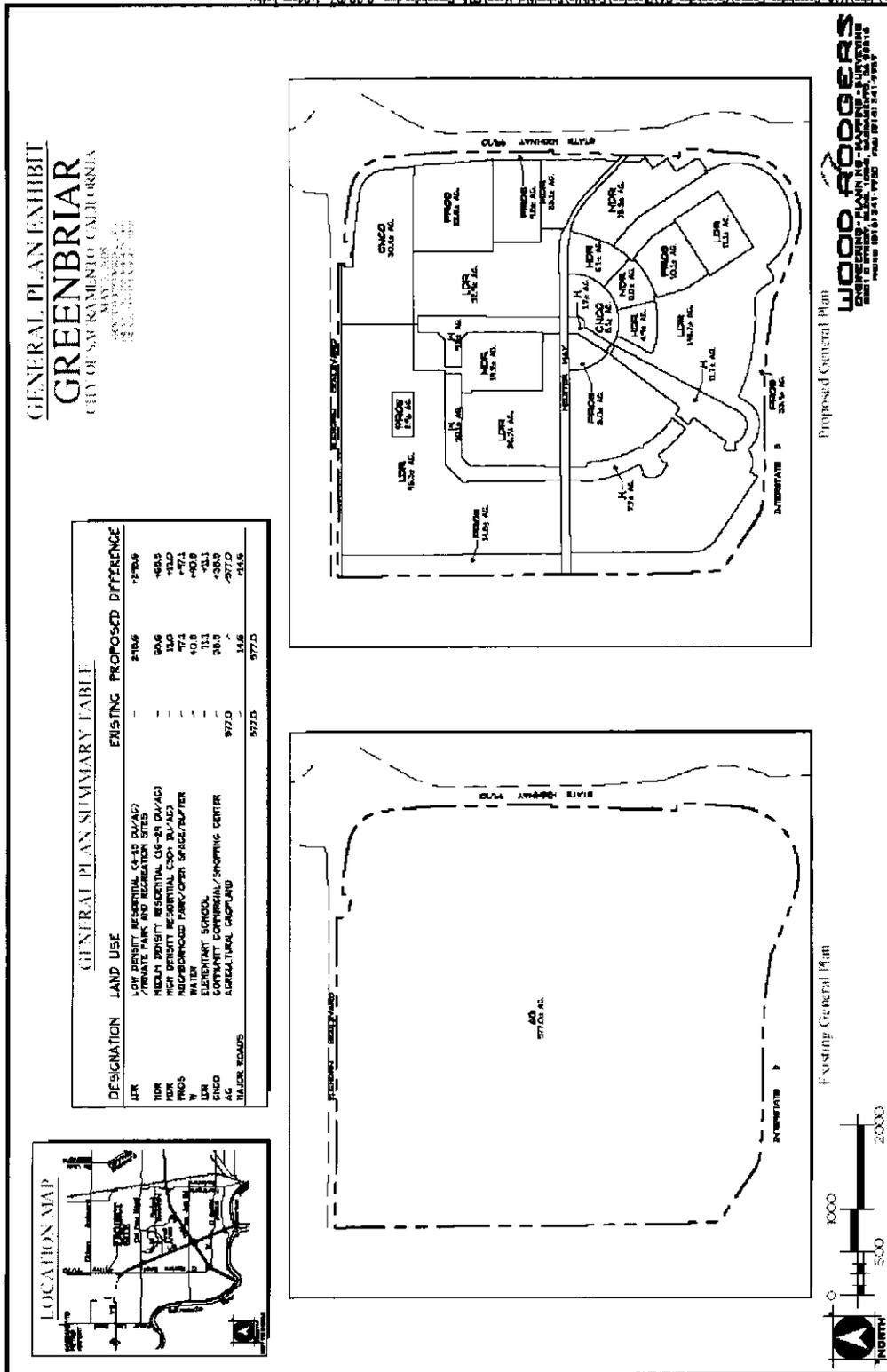
BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:

- Section 1. The City Council adopts the General Plan Amendment for the property, as described on the attached Exhibit A, and the property is hereby designated on the General Plan land use map for 309.7± acres of Low Density Residential, 65.6± acres of Medium Density Residential, 11.0± acres of High Density Residential, 97.1± acres of Parks-Recreation-Open Space, 40.5± acres of Water, 38.5± acres of Community/ Neighborhood Commercial and Offices, 107.4± acres of Parks-Recreation-Open Space, and 14.6± acres as Major Roadways (APN: 201-0300-049, -067, -068, -069, -070, -071, -076, -077, -079, -080, -081, -083, and -085)

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Exhibit A: General Plan Amendment Exhibit – 1 Page

Exhibit A – General Plan Amendment Exhibit



ATTACHMENT 9 – DRAFT PREZONE ORDINANCE

ORDINANCE NO.

Adopted by the Sacramento City Council
Date

PREZONING CERTAIN REAL PROPERTY STANDARD SINGLE-FAMILY PLANNED UNIT DEVELOPMENT (R-1-PUD), SINGLE-FAMILY ALTERNATIVE PLANNED UNIT DEVELOPMENT (R-1A-PUD), MULTI-FAMILY PLANNED UNIT DEVELOPMENT (R-2B-PUD), MULTI-FAMILY PLANNED UNIT DEVELOPMENT (R-3-PUD) ZONE, AGRICULTURE-OPEN SPACE PLANNED UNIT DEVELOPMENT (A-OS-PUD), LIMITED COMMERCIAL PLANNED UNIT DEVELOPMENT (C-1-PUD), AND SHOPPING CENTER PLANNED UNIT DEVELOPMENT (SC-PUD) ZONES (M05-046 / P05-069) (LOCATED SOUTHWEST OF THE INTERSECTION OF HIGHWAY 99 AND ELKHORN BOULEVARD)

BACKGROUND

- A. The Planning Commission reviewed this proposal on November 8, 2007 and voted to forward the rezone to City Council with a recommendation for denial (the Planning Commission vote was five ayes, three noes, one recusal, and zero abstentions).

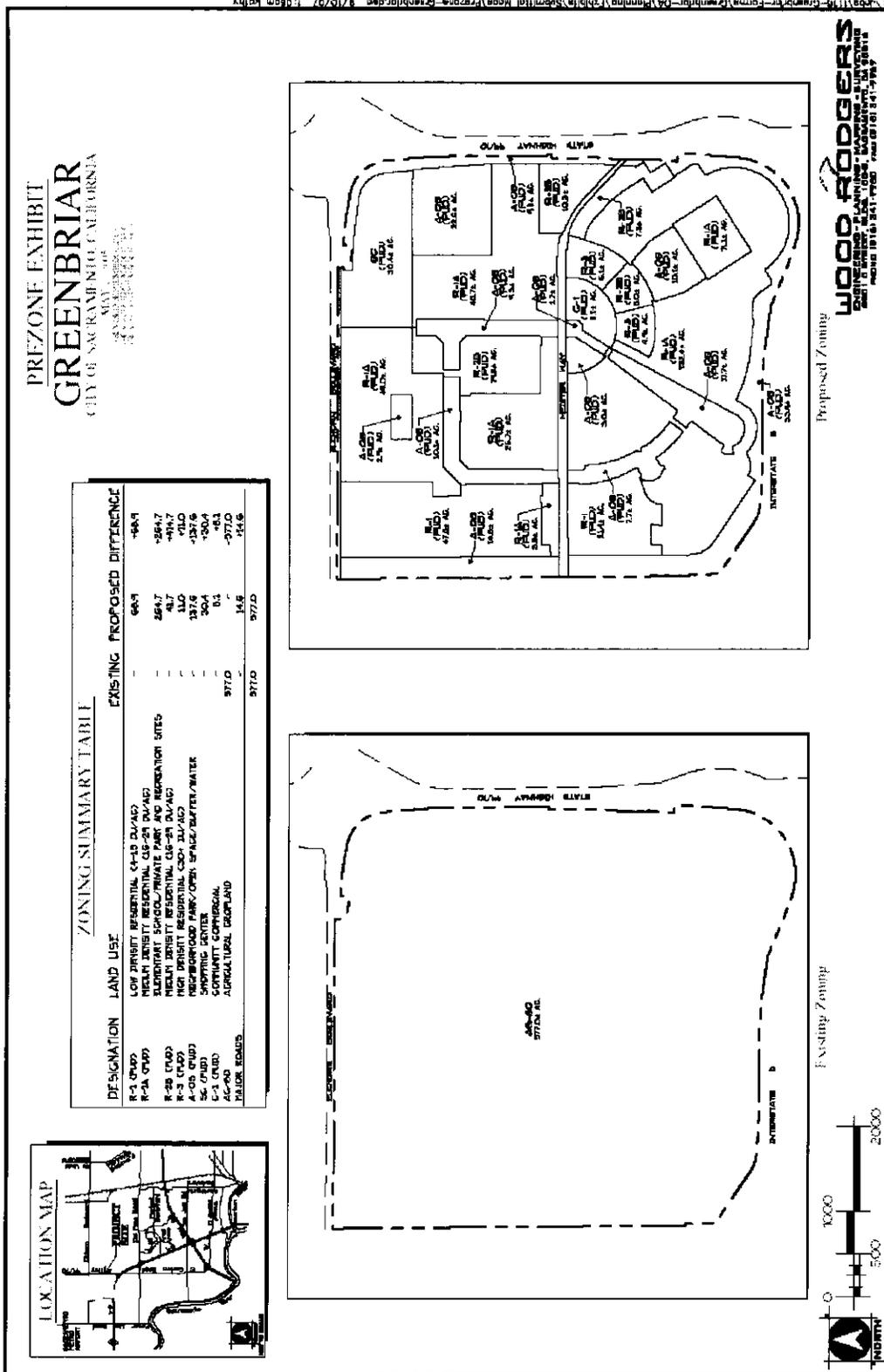
BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:

Section 1. The prezoning designations for the properties described by APNs below and as shown and described in the attached Exhibit A are approved.

APNs: 201-0300-049, -067, -068, -069, -070, -071, -076, -077, -079, -080, -081, -083, and -085

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Exhibit 1: Prezone Exhibit – 1 page



ATTACHMENT 10 – PUD GUIDELINES AND SCHEMATIC PLAN RESOLUTION

RESOLUTION NO. 2007-XXXX

Adopted by the Sacramento City Council

Date

ESTABLISHING THE GREENBRIAR PLANNED UNIT DEVELOPMENT GUIDELINES AND SCHEMATIC PLAN, LOCATED SOUTHWEST OF THE INTERSECTION OF ELKHORN BOULEVARD AND HIGHWAY 99, IN SACRAMENTO, CALIFORNIA (M05-046 / P05-069)

BACKGROUND

- A. The Planning Commission conducted a public hearing on November 8, 2007, and the City Council conducted a public hearing on _____, 2007 concerning the above plan amendment and based on documentary and oral evidence submitted at the public hearing, the Council hereby finds:
1. The PUD establishment conforms to the General Plan; and
 2. The PUD establishment meets the purposes and criteria stated in the City Zoning Ordinance in that the PUD assures that new development is healthy and of long-lasting benefit to the community and the City; and
 3. The PUD establishment will not be injurious to the public welfare, nor to other property in the vicinity of the development and will be in harmony with the general purposes and intent of the Zoning Ordinance in that the PUD ensures that development will be well-designed, and that the residential, open space, and commercial uses will not create a negative impact on adjacent uses.

BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:

- Section 1 The City Council of the City of Sacramento, in accordance with the City Code, Chapter 17.180, approves the Greenbriar Planned Unit Development Guidelines and Schematic Plan (as shown on the attached Exhibits A and B).

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- Exhibit A: Greenbriar PUD Schematic Plan Exhibit – 1 Page
Exhibit B: Greenbriar PUD Guidelines Exhibit - 1 Page

Exhibit A: Greenbriar PUD Schematic Plan Exhibit

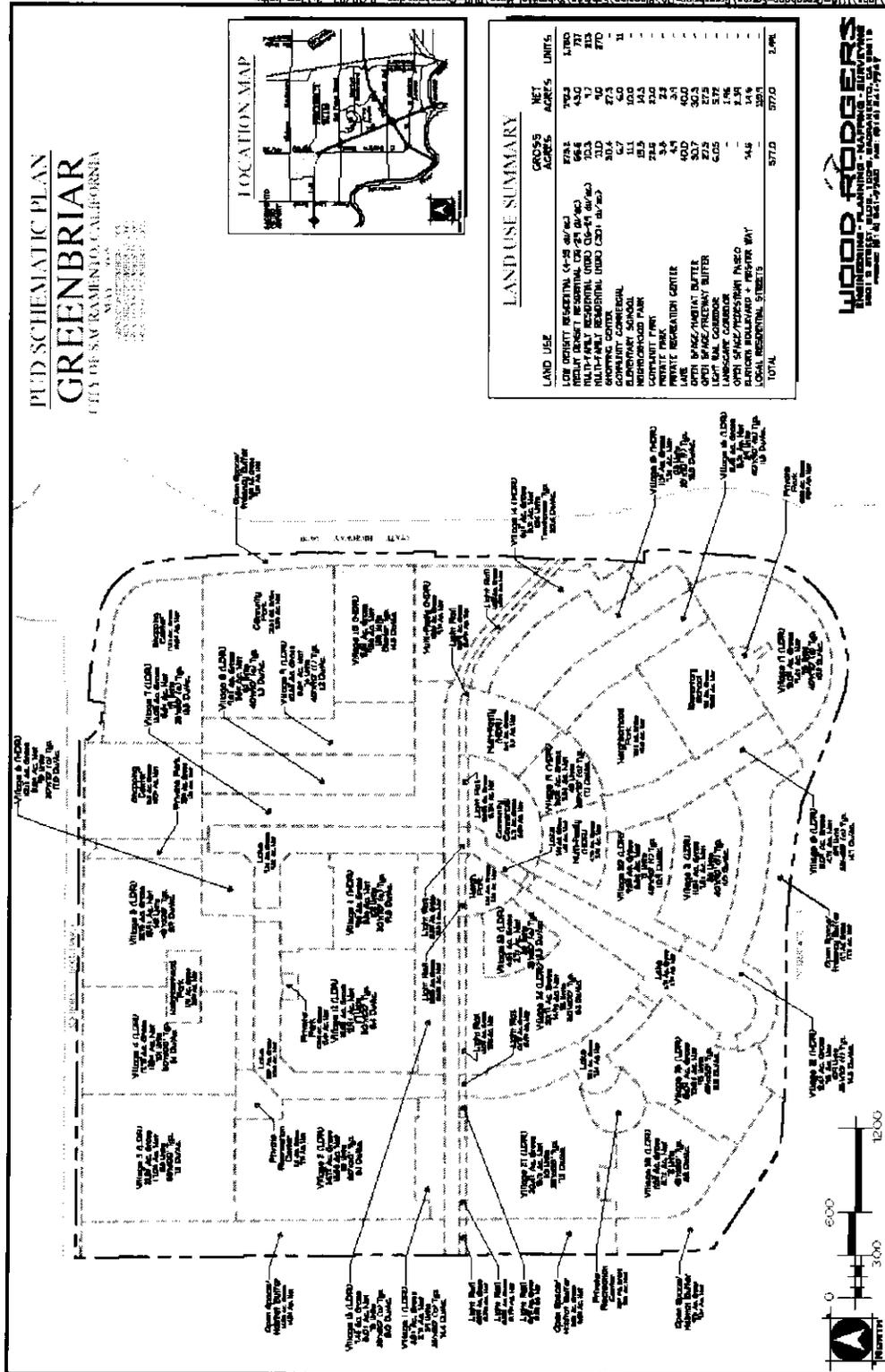


Exhibit B: Greenbriar PUD Guidelines Exhibit

Greenbriar

planned unit development (pud) guidelines

planning commission draft
November 8, 2007

Greenbriar
PUD Guidelines

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Planning Commission Draft

November 8, 2007

page 2

**Greenbriar
PUD Guidelines**

project team

jurisdiction

City of Sacramento
915 I Street, 3rd Floor
Sacramento, CA, 95814

owner/applicant/developer

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Remy, Thomas, Moose & Manley

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Sacramento, CA, 95814

land planning, civil engineering, surveying

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Sacramento, CA 95825

landscape architecture

HRP LandDesign

3242 Halladay, Suite 203
Santa Ana, CA 92705

**Greenbriar
PUD Guidelines**

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Greenbriar
PUD Guidelines

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Greenbriar PUD Guidelines

section 1 – Overview

locating the Transit Station adjacent to higher density residential neighborhoods and providing a high degree of walkability within the PUD.

1.1 Project Context, Goals and Objectives

- To unify the neighborhoods visually and functionally:**
- Utilize a consistent set of design standards and details throughout the Greenbriar PUD to develop a sense of place for the neighborhoods.
 - Create a fluid circulation system which interlinks areas within the neighborhoods to one another.
 - Unify the PUD through the design and location of a convenient and functional light rail system.

The Greenbriar Planned Unit Development (PUD) is located at the northwest intersection of Interstate 5 and Highway 99/70 immediately west and north of the North Natomas Community Plan area in the City of Sacramento. The PUD is bordered by Elkhorn Boulevard to the north, the Metro Air Park employment center to the west, Interstate 5 to the south, and State Highway 99/70 to the east. The approximately 577 acre Greenbriar project is a planned community comprised of single- and multi-family residential neighborhoods, parks, open space, natural habitat, an elementary school, light rail transit and neighborhood-serving commercial areas. The Greenbriar PUD strives to achieve two primary goals. Each of these goals will be pursued using specific design objectives which are listed below.

With Greenbriar's proximity to the existing City of Sacramento job base and the neighboring Metro Air Park employment center, The Sacramento International Airport, Interstate 5 and Highway 99/70, as well as the site's location at the periphery of the North Natomas Community Plan, the PUD is strongly positioned to be a focal community within the City of Sacramento. In order to accomplish this type of new development, development guidelines and standards must be carefully crafted to allow for flexible retail development, compact design, unique street scenes and unified design among the varied and diverse housing types. It is for these reasons that this PUD Guidelines has been created and why this community will be a unique and model community in the greater Sacramento region.

To capitalize on the setting and location of the site for the benefit of future residents:

- Utilize parks, lake and circulation design to enhance the unique character of each neighborhood.
- Develop a public transportation system that encourages light rail ridership by centrally

Greenbriar
PUD Guidelines

local context map



Exhibit #1 – Local Context Map

Greenbriar

PUD Guidelines

The purpose of these PUD Guidelines is to guide future development within the Greenbriar PUD area. This document is organized into 6 Sections as follows.

- The **PUD Overview Section** of the PUD Guidelines provides the local context for the proposed project, the Community Plan Principles and established Goals, Policies and Objectives for the Plan Area. The Overview Section provides a detailed summary of the planned development, Zoning and Land Use designations.
- The **Transit Orientation and Mobility Section** addresses the Transit-Oriented Development (TOD) characteristics and the planned extension of Regional Transit, Non-Vehicular Mobility, and Vehicular Circulation.
- The **Housing Section** of the PUD Guidelines discusses the Housing Alternatives including Market Rate Housing, Affordable Housing, and Senior Housing. This section sets forth Housing Development Standards to ensure consistency in the quality and character of the residential environment.
- The **Commercial Uses Section** discusses the Design and Function of commercial uses, including the Village and Community Commercial sites. This section sets forth Commercial Development Standards to ensure consistency in the quality and character of the commercial environment within Greenbriar.

- The **Parks, Open Space and Recreation Section** discusses the design, uses and access/relationship of the Parks, Open Space, Greenbriar Lake, and the Habitat and Open Space Buffers.
- The **Implementation Section** outlines the processes that will implement the orderly development of the PUD area.

1.2 Future Submittals

These Greenbriar Planned Unit Development (PUD) Guidelines identify the standards necessary to implement successful and quality-image development. This document presents standards to guide the zoning, uses and general development of the PUD, and establishes an appropriate context for the application of more specific tract-level design standards. Future applications will be gauged according to standards prescribed by detailed Design Guidelines, and Planning Director or Planning Commission oversight as necessary.

This PUD Guidelines document acknowledges that the City Planning Commission shall review and consider for adoption a separate yet complementary Design Guidelines document intended to prescribe detailed and comprehensive design standards addressing the following: walls, signage, landscape, single-family and multi-family residential design, commercial site design, and public facility and park/open space design. In addition to these subjects, the Design Guidelines may

Greenbriar PUD Guidelines

include other optional topics as needed to describe and convey the overall design concept and implementation of the PUD's vision.

The timing for completion and subsequent approval of the Future Design Guidelines is intended to be coordinated with the City's review and consideration of the Greenbriar Master Parcel Map, Tentative Subdivision Map and Development Agreement.

The Greenbriar PUD is subject to the City of Sacramento's PUD Development Application process in place at the time of subsequent applications and approvals and/or as specified in Section 6.3 of these Guidelines. Please see Section 6.3 (Consistency Review and Planning Director Discretion) for more detail.

1.3 PUD Schematic Plan

The Greenbriar PUD Schematic Plan is consistent with the patterns for development as set forth in the City's General Plan. The PUD Schematic Plan locates a neighborhood-serving Shopping Center along Elkhorn Boulevard and Greenbriar Boulevard (at the main project entry), a Community Commercial site centrally located with the planned RT Transit Station sited in the heart of the neighborhood along Meister Way, and provides varied housing opportunities in close proximity to the Transit Station, shopping and daily services, employment opportunities, the

elementary school and various park and recreation facilities.

The PUD Schematic Plan is comprised of eleven different single-family lot sizes/product types to accommodate various income levels and lifestyle choices within the plan area. Additionally, the PUD provides the opportunity for housing within the Community Commercial site and high-density attached housing, including one senior housing site, in close proximity to transit opportunities.

The Greenbriar PUD Schematic Plan provides many varied recreation opportunities in the location of both publicly- and privately-owned active and passive parks and open spaces. One large Community Park, three Neighborhood Parks, three private parks, two private recreation centers, numerous pocket parks, pedestrian paseos and marina parks (ranging in size), and an elementary school are located within the PUD. These amenities are well-linked by residential streets and trails. Additionally, the PUD boasts an expansive lake with a pedestrian-friendly lakewalk or promenade around the entire perimeter of the lake, and a wide freeway buffer/open space corridor (along the south and east project boundaries) enhanced with pedestrian trails and landscaping.

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The PUD Schematic Plan also includes a wide Wildlife Corridor/Habitat Buffer adjacent to the western project boundary that will be privately maintained in perpetuity for the preservation of threatened and/or endangered species. Due to the possible presence of rare or threatened species, physical access to the Habitat Buffer will be restricted and an on-going maintenance plan will be established.

The following pages illustrate the Greenbriar PUD Schematic Plan and Land Use Summary in detail.

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pud schematic plan

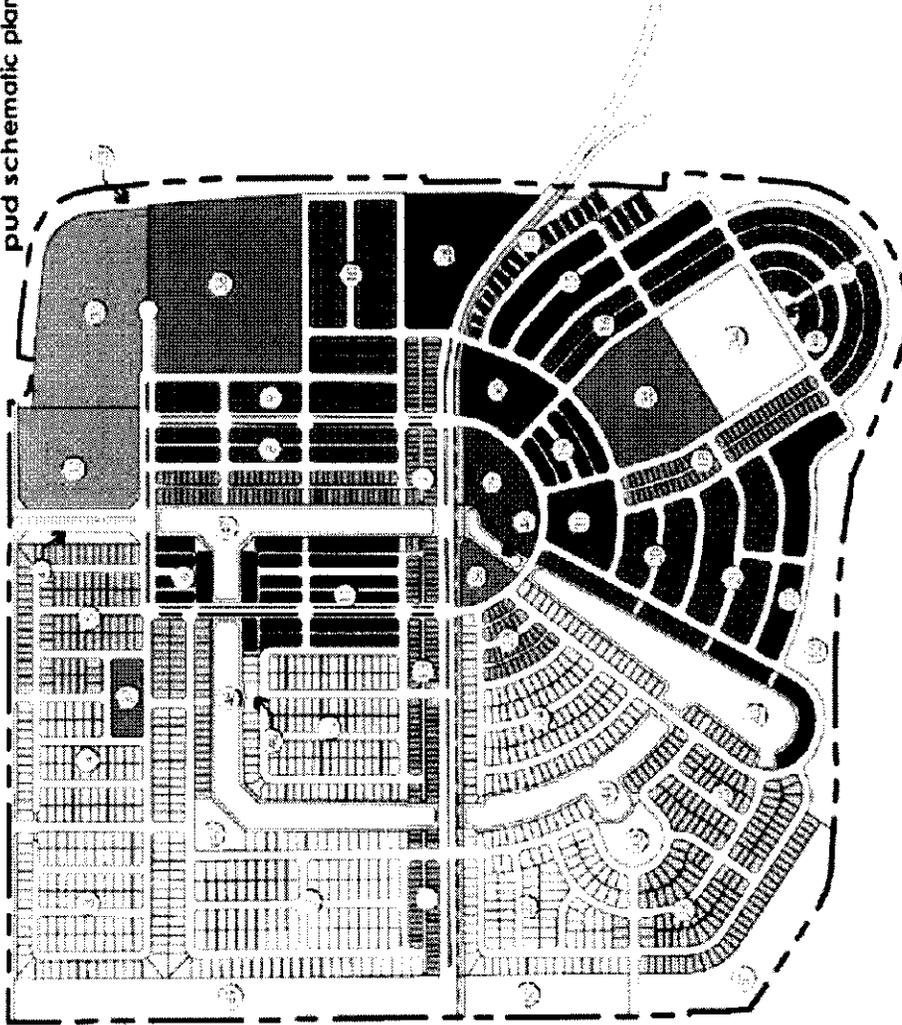


Exhibit #2 – PUD Schematic Plan

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land use summary table

zoning no. / lot no.	general plan:	use:	acres/ft.	feet/ft.	units
village 2	LDR	low density residential (50'x100')	24.7	35.6	13
village 3 & 27	LDR	low density residential (55'x100')	43.2	52.1	235
village 4, 12 & 24	LDR	low density residential (55'x100')	57.2	47.3	340
village 5, 26 & 28	LDR	low density residential (55'x100')	49.5	35.2	307
village 9, 20 & 21	LDR	low density residential (40'x90'-E)	50.8	21.0	732
village 8, 14 & 17	LDR	low density residential (45'x90'-A)	33.1	20.1	277
village 7, 13, 16 & 33	LDR	low density residential (45'x90'-A)	36.8	23.9	336
village 5 & 25	MDR	medium density residential (55'x20' F)	23.5	14.3	222
village 6, 11 & 19	MDR	medium density residential (50'x20'-A)	24.2	13.4	245
village 10	NDR	medium density residential (16'x100')	12.8	9.5	136
village 14	MDR	medium density residential (low-rises)	6.1	5.1	104
lot a	MLK	multi-family residential	10.2	9.7	23
lot c	MDR	multi-family residential	11.0	9.0	153
lot d	MDR	multi-family residential	13.0	9.0	177
lot e	MDR	multi-family residential	13.1	10.7	-
lot f	MDR	multi-family residential	17.2	16.5	-
lot b	CNCD	community center	6.7	6.0	-
lot g	CNCD	community commercial	13.1	10.0	-
lot h	LDR	elementary school	3.0	14.2	-
lot i	PROS	public neighborhood park	22.2	21.0	-
lot k	PROS	community park	3.5	2.7	-
lots a & p	LDR	private park	4.9	3.9	-
lots d & j	W	lake	40.0	40.0	-
lots y & z	PROS	open space/treeway buffer	27.5	27.5	-
lots w, x & x	PROS	open space/habitat buffer	30.7	30.3	-
lots a & t	PROS	light rail corridor	0.61	0.77	-
lots b & cc	LDR	light rail corridor	1.0	0.91	-
lots d & 9b	W	light rail corridor	0.42	0.42	-
lot ee	LDR	light rail corridor	1.30	1.27	-
lot fh	LDR	light rail corridor	0.60	0.75	-
lot i	CNCD	light rail corridor	0.37	0.49	-
lot j	MDR	light rail corridor	1.1	1.05	-
lots a, w, ll, llm	LDR	landscape lot/peDESTIAN cubes	-	0.03	-
lots 00, 01, U, wv-22	MDR	landscape lot/peDESTIAN cubes	-	1.68	-
one-gigg, Tupp	MDR	landscape lot/peDESTIAN cubes	-	0.44	-
lots w, v, d, d, d	MDR	landscape lot/peDESTIAN cubes	-	1.94	-
lots k, ll, llm, jn	MDR	landscape lot/peDESTIAN cubes	-	1.46	-
elkhorn, oakleaved & meister way	MDR	landscape lot/peDESTIAN cubes	-	11.59	-
local road, junia, steech	MDR	landscape lot/peDESTIAN cubes	-	14.6	-
total			577.0	577.0	2,991

landscaping corridor lot/peDESTIAN cubes are included in gross acreages of above parcels.
gross acreages include streets & landscape corridors. net acreages include minor residential streets only.

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section 2 – Transit-Oriented and Mobility

2.1 Principles and Objectives

The Greenbriar transit-oriented development (TOD) embraces the following general design principles:

- Balanced mix of land uses (mixed uses)
- Pedestrian scale design (walkability)
- Connected community (variety of movement modes)
- Reduced impact of the automobile (transit, trail, and walks)
- Authentic architecture (reflecting traditional California train depot motifs enlivened with touches of traditional Sacramento neighborhood architecture)

2.2 Transit-Oriented Development

For organizational purposes these guidelines divide the TOD into two primary parts: the mixed-use portions and the rail station.

2.2.a LRT Station Location and Adjacent Land Uses

A community commercial facility is planned adjacent to the future Greenbriar Light Rail Transit (LRT) Station. As LRT stations are prime generators of pedestrian traffic and activity, typical elements of a TOD are a plaza, community and patron-serving uses (for example, retail, community space, housing or office), parking and the station itself. In the case of the future Greenbriar TOD, all of the typical elements are present along with the additional amenities of an adjacent lake and park.

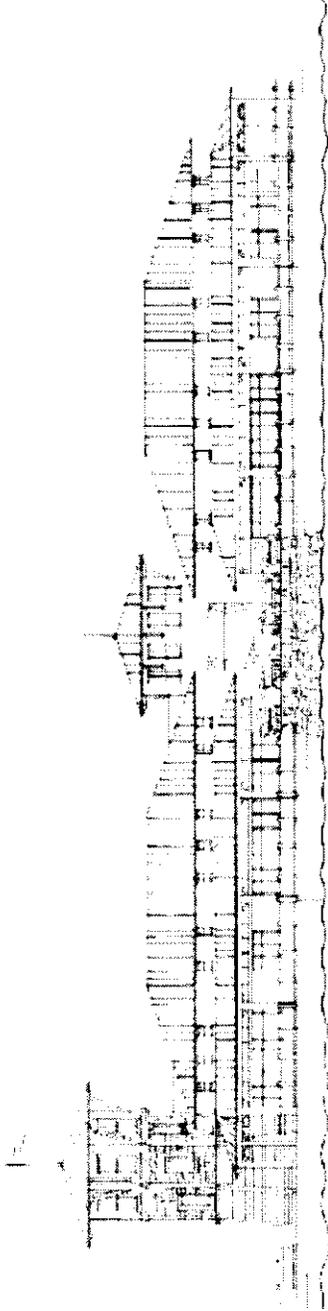
The area around an LRT station (including the street) should be thought of as a single community space or room. Elements of this room shall include:

- Sidewalks and pedestrian paths in the immediate vicinity of LRT station at least ten (10) feet in width.
- Marked pedestrian crossing at stations in a color contrasting the street and of high quality materials in keeping with the design of the station.
- Station area with lush landscaping accompanied by drought tolerant materials.
- Palms or other tall trees planted at the station so that the LRT may be marked from a distance.
- Street trees that provide shade and visual interest in the vicinity of the station.

2.2.b Mixed-Use Design

Mixed-use may be horizontal or vertical in design. The mixed-use component includes the plaza, mixed-use structures, mixed-use patron/residential parking and pedestrian/bicycle circulation.

Buildings on this site may have retail on the ground floor such as a specialty grocery market, wine shop, salon or specialty gifts/housewares shops. Upper floors may include residential uses, office space or a bed and breakfast.



**Exhibit #5 – One-Story Commercial Building at TOD
Restaurant/Retail/Office-Concept Plan**

LRT stations are generally low in height and long, approximately 380 to 400 feet in length and approximately 60 feet in width. LRT stations are comprised of a grade-level platform, canopies, seating, fare vending equipment, landscaping and signage. At this location, a kiss-and-ride area, bus drop-off area and parking for approximately 150 to 200 vehicles will be provided. Kiss-and-ride refers to LRT riders who are dropped off by another driver at the LRT station for their daily commute. No permanent parking is to be provided in the kiss-and-ride area. It is anticipated that a bus stop(s) will be located in the immediate vicinity of the station.

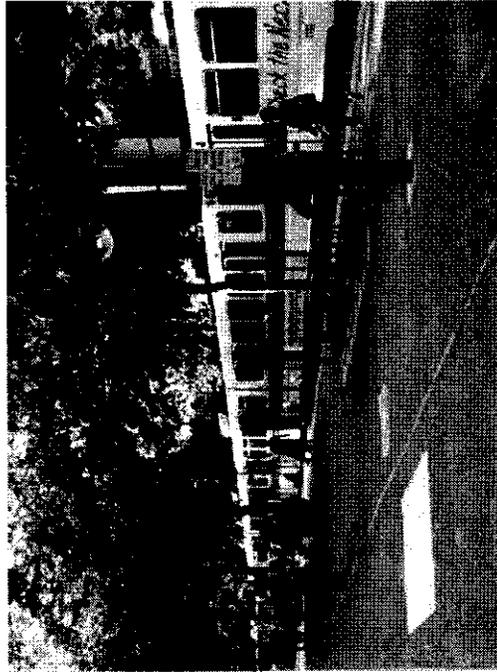


Exhibit #6 – Light Rail Transit Station

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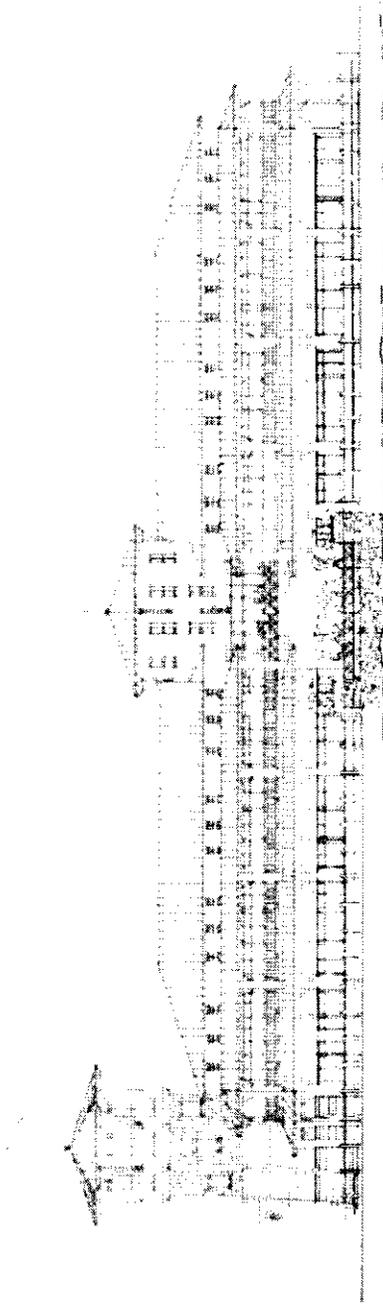


Exhibit #7 - Two-Story Commercial Building at TOD
(Office over Restaurant/Retail)-Concept Plan



Exhibit #8 - Light Rail Transit Station



Exhibit #9 - Pedestrian/Mixed-Use Circulation

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PUD Guidelines**



**Exhibit #10 – Light Rail Station at TOD
(Station/Restaurant/Retail)-Concept Plan**

2.2.c TOD Development Standards

The community commercial designation allows a mixture of retail, commercial and housing opportunities in a single area of the project. Typical housing opportunities, if used on this site, include townhouses, flats or mixed-use buildings with ground-floor retail and second-/third-floor offices and/or housing. Housing may be for rent or for sale.

The mixed-use element of this TOD may be developed initially as single use or may transition over time to higher densities and multiple uses once the LRT station is in place.

Permitted Uses (General)

The City of Sacramento's code for C-1 zoning shall prevail with the following explicit addition:

- Farmers Market

Architectural Treatment

The TOD's architectural style shall be a contemporary interpretation of traditional Sacramento styles. This means paying homage to traditional forms while embracing new materials and technologies.

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The TOD's ambience shall reflect the indoor-outdoor living style that is a hallmark of Northern California. Usable outdoor spaces should be included in the design of structures along with the extensive use of passive solar elements such as awnings, pergolas and trellises.

Street trees should be primary large shade trees while palm trees or other tall trees will be used to mark the location of the station from the freeway and surrounding neighborhoods, subject to approval of Urban Tree Services.

The following design guidelines apply:

- All structures shall have four-sided architectural treatments.
- All structures shall have paved outdoor patios and/or seating areas located adjacent to at least one side of the building.
- Glass storefronts are encouraged along pedestrian ways.
- All building elevations with pedestrian adjacency shall be designed with pedestrian-scaled details and amenities such as display windows, awnings, trellises and arbors.
- Each structure shall be architecturally and stylistically connected to the overall pedestrian

experience that includes one or more of the following: intermittent trellises, arbors, awnings and/or other overhead structures.

- Entries shall be made prominent through the use of architectural details and accents such as arches, planters, overhead balconies and special doors.
- Roof-mounted communications equipment shall be screened or placed behind roof parapet walls so as not to be visible to motorists and pedestrians.
- Liberal use of a variety of building, accent and trim colors is encouraged.

Plaza

A pedestrian-oriented plaza shall be built as part of the TOD. This plaza shall be designed as an intensively used gathering place. Spaces shall be designed for public gathering and to accommodate activities such as resting, eating, strolling and people watching.

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Exhibit #11 – Plaza

Guidelines for the plaza include:

- High degree of applied design.
- Adjacency to LRT station.
- Adjacency or accessibility to the Lake.
- Adjacency to the Pedestrian Promenade.

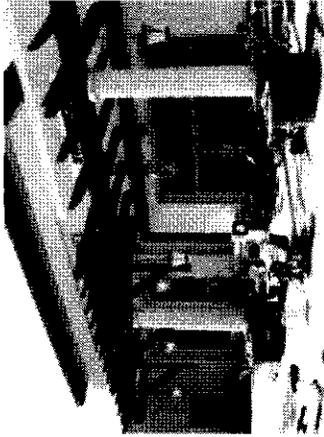


Exhibit #12 – Pedestrian Plaza

- Adjacency to, on at least on one side, some of the community-serving uses such as retail, commercial or restaurant uses.
- Patron seating may encroach into the plaza.
- A range of ample seating opportunities.
- Good visibility to/from surrounding streets.
- Good visibility from the train.
- Visibility from the parking lot.
- Opportunities for pedestrians to shelter in inclement weather.
- Secure bicycle parking and storage.
- Providing area maps for use of LRT patrons and station-area visitors.
- Providing electrical outlets throughout the plaza to facilitate mobile vendors, outdoor performances and the mobile/wireless high tech population.

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door of the station and adjacent retail/commercial uses. Specific guidelines include:

- Palms or other tall trees planted at the station so it can be seen or marked from a distance (subject to approval of Urban Tree Services).
- Station area lushly landscaped with drought-tolerant plants and materials.
- Street trees for shade and visual interest, particularly in the vicinity of the station.

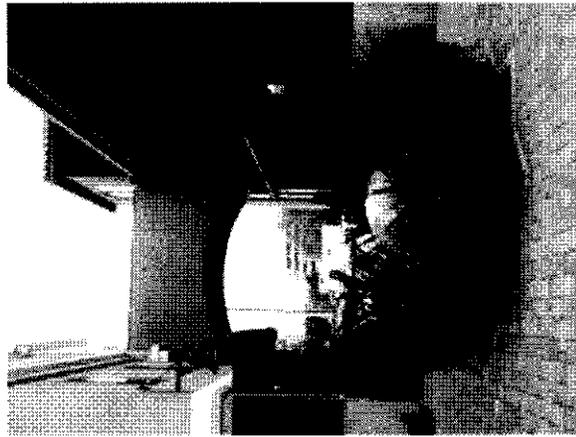


Exhibit #13 – Pedestrian Plaza

Landscape Concept

As IRT stations are prime generators of people and activity, the area immediately adjacent to and including the station should be thought of as a single room or space. Landscaping helps define the edges of the room and, in the case of the Greenbriar TOD, serves as a place marker, leading patrons to the front



Exhibit #14 – Community Icon

Community Icon

A community icon should be constructed as part of the plaza or lake-facing structures. This community icon is envisioned as a vertical element such as a tower, beacon or other art piece. The icon shall be lighted at night.

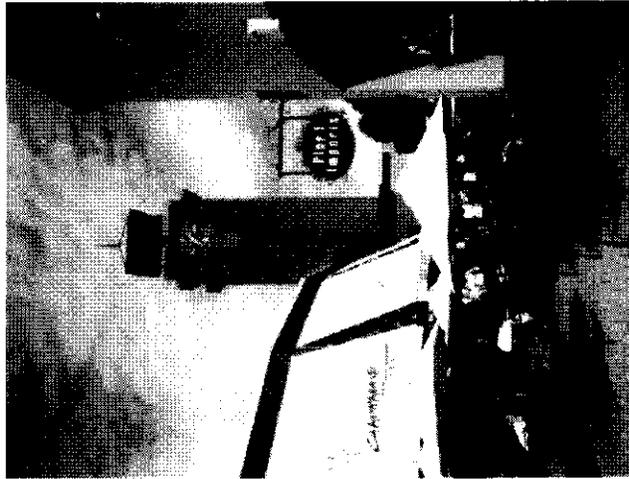


Exhibit #15 – Community Icon/Tower

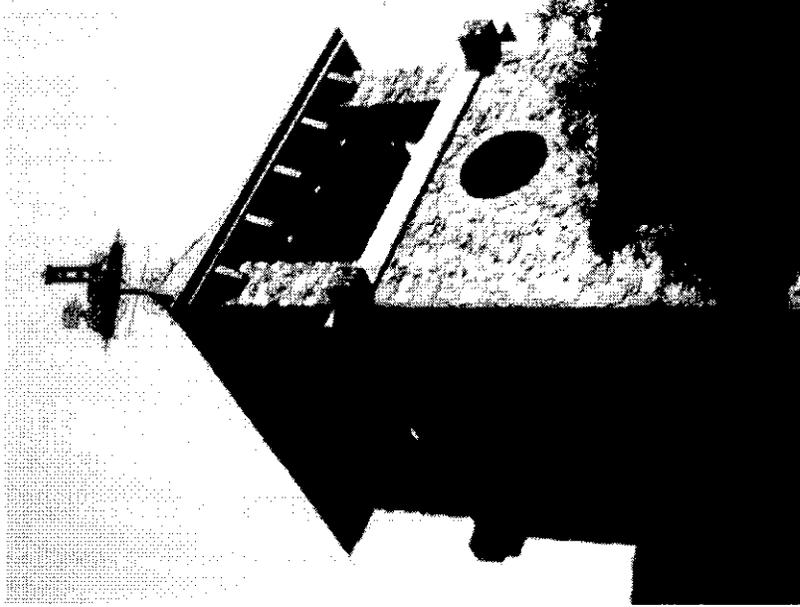


Exhibit #16 – Community Icon

Greenbriar PUD Guidelines

Canopies

Canopies are traditionally used to provide shelter for pedestrians from the elements. Shelters/canopies will be found on the LRT station platform itself as well as in the adjacent plaza.



Exhibit # 17 - Building Canopy

Platform canopies traditionally occupy a small percent of the platform area. Both platform and plaza canopies shall reflect traditional California train depot motifs. Shelters and canopies shall be of sufficient size and quantity to provide LRT riders protection during inclement weather and heat. Canopies should also be included as part of the kiss-and-ride facility.



Exhibit # 18 - Lake Frontage

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Lake Frontage

A significant amount of the TOD fronts the lake. These guidelines place structures along the majority of this lake frontage.

Design elements include:

- No parking adjacent to the lake shall be permitted.
- All parking shall be separated from the lake with buildings or open space equal to the average depth of buildings fronting the lake.
- All structures adjacent to the lake shall provide the option for patron entries on both the lake and parking sides of the structure.
- A landscaped pedestrian promenade will be provided along the lake edge adjacent to lake-facing development. This lake fronting landscaped pedestrian promenade shall connect the LRT, the plaza, lake-fronting buildings and neighborhoods to the south. The landscaped pedestrian promenade is recommended to be shared generally with circulation around the lake edge.
- Outdoor seating/dining areas facing the lake and pedestrian way are strongly encouraged.
- Outdoor seating/dining areas built over the lake are encouraged.

Street Frontage

Structures in the TOD do not typically have service drives or loading zones except in the event of a multi-story, mixed-use structure. Except for multi-story, mixed-use buildings, service will be from parking areas during off-hours. The following guidelines apply:

- Service functions shall be screened from general public view by building design and layout, screen walls attached to buildings and plantings at property lines.
- Design of storage and service areas shall be compatible with and, in the case of loading and service areas, integral to building architecture.
- Waste storage and collection areas shall be designed with materials, finishes and colors from the adjacent building. Metal gates are required.
- Trash enclosures are not permitted in the street right-of-way or street-side landscape setback.

2.3 Non-Vehicular Mobility

In addition to the planned extension of the Regional Transit (RT) Light Rail Corridor through the plan area, the PUD provides a varied network of both on- and off-street pedestrian pathways and trails allowing for safe and convenient non-vehicular travel throughout and within the PUD.

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2.3.a Light Rail Corridor

Currently, Regional Transit (RT) does not exist within the area; however extension of existing facilities is planned along the south side of Meister Way. This rail system will bisect the site, directly connecting the plan area to Downtown Sacramento through the North Natomas Community Plan area to the east and to the Sacramento International Airport through Metro Airpark to the West.

The transit station is centrally located within the heart of the Greenbriar PUD area adjacent to high density residential and Community Commercial land uses. Proposed pedestrian friendly bus shelters will be appropriately located to ensure safe and convenient pedestrian use. The placement of medium- and high density residential uses, including the provision of seniors housing sites, will allow for convenient pedestrian access via safe and walkable routes.

2.3.b On-Street Pedestrian and Bicycle Mobility

The PUD provides a varied network of both on- and off-street pedestrian pathways and trails allowing for safe and convenient non-vehicular travel throughout and within the PUD. All arterial and collector streets have striped Class II bike lanes. Nearly all sidewalks within the PUD's streets are detached from the street edge and separated from the street by a landscape planter of varying width depending upon the street facility. These pedestrian-friendly streets will provide a safe walkable

route to everywhere in the PUD area under a dense canopy of shade trees.

2.3.c Off-Street Pedestrian and Bicycle Mobility

The open space/freeway buffer along the south and west sides of the plan area will provide Class I trail facilities within a wide landscaped open space area. Additionally, Greenbriar Lake offers an extensive trail system in the form of a 22' wide lakewalk or promenade that surrounds the entire perimeter of the lake. These trail facilities will, in conjunction, facilitate circular movement within the community without ever stepping onto any roadway.

2.4 Vehicular Circulation

The design of the Greenbriar PUD circulation system utilizes an efficient system of streets, pedestrian, and Regional Transit facilities to serve the plan area. The internal street system is designed such that a 'block' or 'grid' system is achieved. Various pedestrian pathways are designed to provide non vehicular transportation opportunities throughout the project site in all directions linking the projects' many park sites. This creation of a well-connected hierarchy of roadways allows for the efficient movement of vehicular traffic and encourages biking, walking, and other human-powered transportation alternatives to the automobile.

Greenbriar PUD Guidelines

2.4.a Street and Alley Standards

The intent of the circulation system design is to provide a functional and efficient system that balances the needs of automobiles with other forms of transportation including walking and bicycling. In addition to circulation requirements, the street system will be designed for aesthetics and will be an element of the overall project's identity.

Arterial and collector streets are the principal streets that serve the through traffic within the PUD. The arterial street within the PUD is Elkhorn Boulevard located along the northern boundary of the plan area. Meister Way serves the plan area as a collector roadway and bisects the PUD providing an east-west connection through the plan area.

The internal residential streets provide connections into and through the PUD and connect residential neighborhoods with the retail centers, elementary school, and the various park and open space facilities thus allowing residents to travel within and throughout the PUD without traveling along the arterial and collector roadways. Where necessary, traffic control devices will be installed that will facilitate timely and safe pedestrian crossings.

The following pages describe and illustrate the Overall Circulation Plan and individual Street Sections utilized within the Greenbriar PUD; the exhibits are color-coded for convenience and clarity.

Please note: This section and the associated exhibits are illustrative by nature to illustrate the various street sections; For actual approved street sections see the Tentative Subdivision Map and Master Tentative Subdivision Map. If and where there are discrepancies between these exhibits and the Tentative Maps, the Tentative Maps shall prevail.

2.4.b Parking Standards

Parking at the TOD site accommodates patrons of both the LRT station and the community serving uses within this mixed-use TOD. Parking shall be shared between the light rail users and the community commercial development.

Parking requirements shall be calculated on the assumption that parking shall be shared between the mixed-use and light rail portions of this project. Each portion shall calculate the required amount of parking based on City of Sacramento's requirements.

Parking for the mixed-use portion of the TOD shall be lighted and landscaped according to the standards presented in these guidelines.

If housing is included in the mixed-use portion of the TOD, separate parking shall be identified and set aside for purely residential users.

2.4.c Park/Ride Facilities

The Sacramento Regional Transit Authority (RTA), operator of the City's light rail line that will serve Greenbriar Station. Parking spaces should be provided in conjunction with the needs of RTA in close proximity of the light rail station. The parking area shall be lighted for night use and landscaped according to City standards. A pedestrian walkway will traverse the parking lot, culminating at the plaza.

**Greenbriar
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circulation exhibit

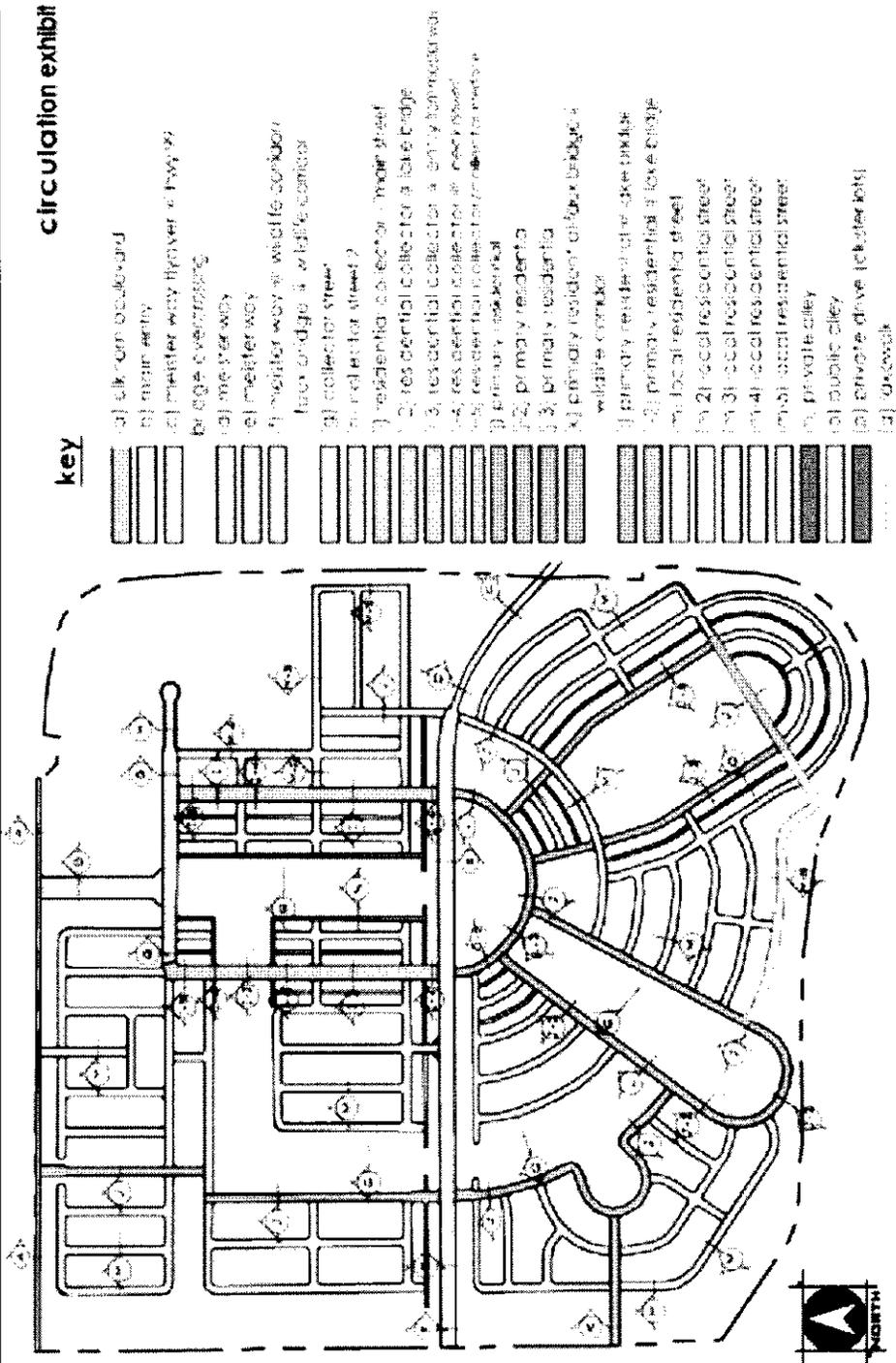


Exhibit #19 - Overall Circulation Plan

Greenbriar PUD Guidelines

Elkhorn Boulevard

Elkhorn Boulevard is the main arterial street serving the plan area and is located along the northern property boundary of the PUD. Overhead powerline facilities are planned for the south side of Elkhorn Boulevard within the 25' P.U.E.

- This facility is comprised of the following components.
- 135' ultimate right-of-way
 - 26' landscaped median/double left turn lane
 - three travel lanes (12', 12' and 11') in each direction
 - 6' striped bike lanes
 - 8.5' landscape planter with 6' separated sidewalk on each side
 - 25' P.U.E. /landscape corridor on the south side of the street
 - parking is not permitted

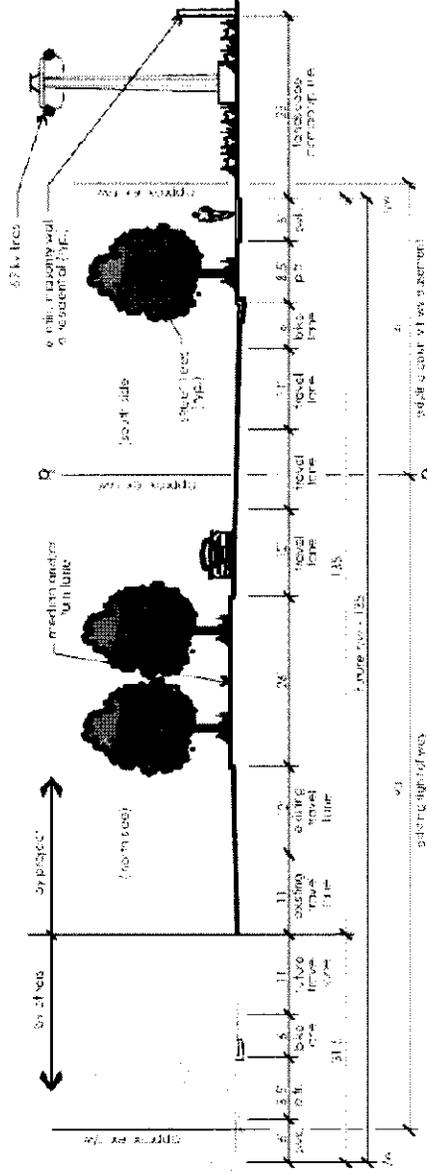


Exhibit #20 - Elkhorn Boulevard

Greenbriar PUD Guidelines

Greenbriar Boulevard (main project entry)

Greenbriar Boulevard is the main project entry street serving the plan area from the north along Elkhorn Boulevard. This 4-lane facility is designed with a very generous landscaped median with landscape corridors and wide pedestrian friendly sidewalks on each side.

This facility is comprised of the following components.

- 133' ultimate right-of-way
- 30' landscaped median/double left turn lane
- two travel lanes (12' and 11') in each direction
- 6' striped bike lane
- 12.5' landscape planter with 10' separated sidewalk on each side
- 12.5' landscape corridor with 10' P.U.E on both sides
- parking is not permitted

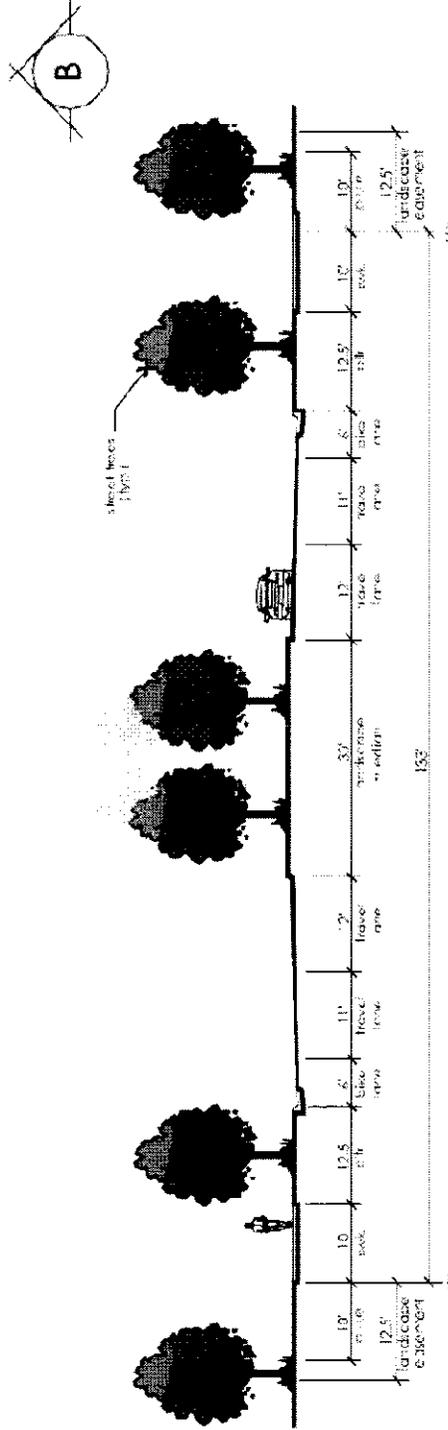
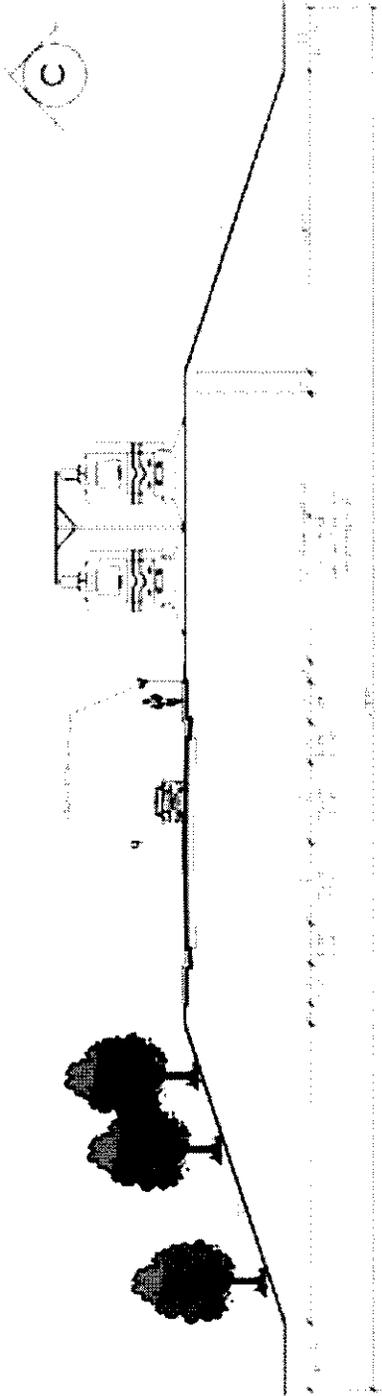


Exhibit #21 - Greenbriar Boulevard (main entry)

**Meister Way Flyover of State Highway 99/70-Bridge
Overcrossing**

This portion of Meister Way is located at the projects' easternmost boundary where Meister Way over crosses State Highway 99/70.

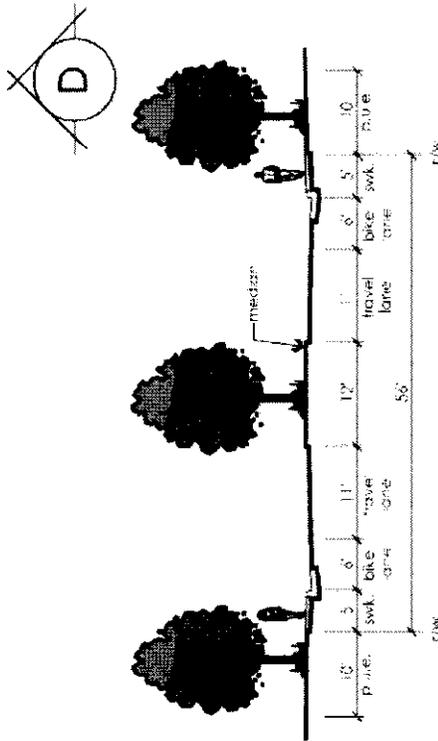


**Exhibit #22 - Meister Way Flyover at State Highway
99/70 Bridge Overcrossing**

Greenbriar PUD Guidelines

Meister Way – 56' ROW

This portion of Meister Way is located near the easternmost portion of the plan area, just west of State Highway 99/70, and is designed consistent with the existing Meister Way in the North Natomas Community Plan Area.



This facility is comprised of the following components.

- 56' right-of-way
- 12' landscaped median/left turn lane
- one travel lane (11') in each direction
- 6' striped bike lanes
- 5' attached sidewalk
- parking is not permitted

Exhibit #23 - Meister Way – 56' ROW

Greenbriar PUD Guidelines

Meister Way – 106' ROW

Meister Way is an east-west oriented collector roadway that links the plan to Metro Air Park on the west and the North Natomas Community Plan area on the east. This roadway facility is 106' ROW through the majority of the plan area but reduces in width as it moves easterly in the plan area.

This facility is comprised of the following components.

- 106' ultimate right-of-way
- 12' landscaped median/left turn lane
- two travel lanes (12' and 11') in each direction
- 6' striped bike lanes
- 7' parking lane on north side only
- 8.5' landscape planter with 10' separated sidewalk on each side
- 6' detached sidewalk
- parking is permitted on the north side only
- no residential driveway access allowed

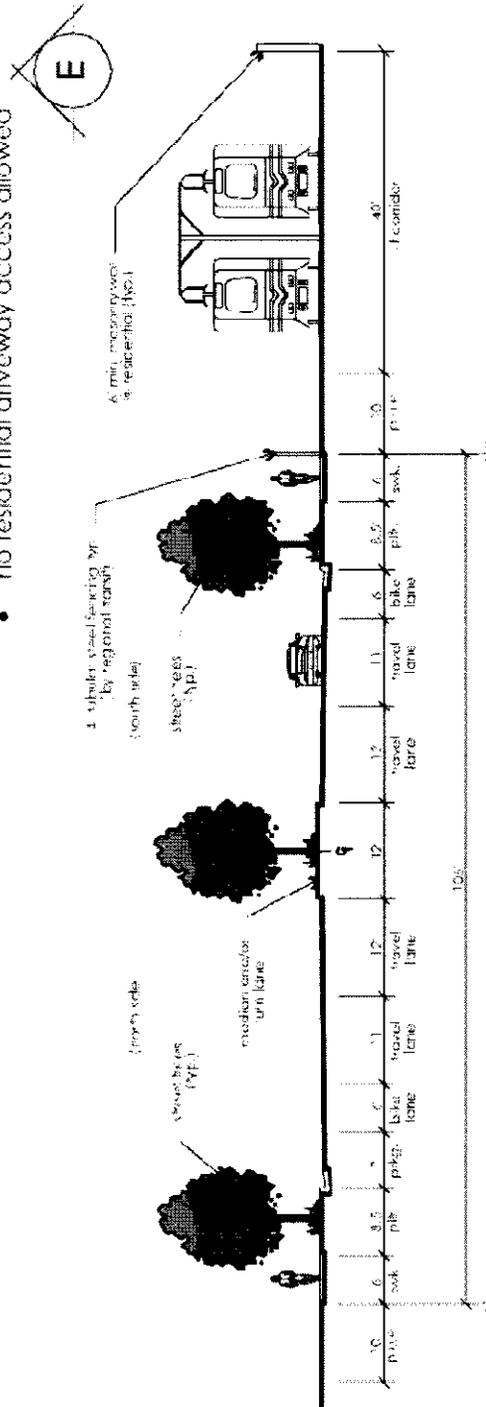


Exhibit #24 - Meister Way – 106' ROW

Greenbriar PUD Guidelines

Meister Way at Wildlife Corridor/Faux Bridge

This portion of Meister Way occurs at the plan areas' westernmost boundary where it crosses the Wildlife Corridor/Habitat Buffer and at all bridge crossings of the lake feature.

This facility is comprised of the following components.

- 106' ultimate right-of-way
- 12' median/left turn lane
- two travel lanes (12' and 11') in each direction
- 6' striped bike lane
- 7' parking lane
- 11' attached sidewalk on each side

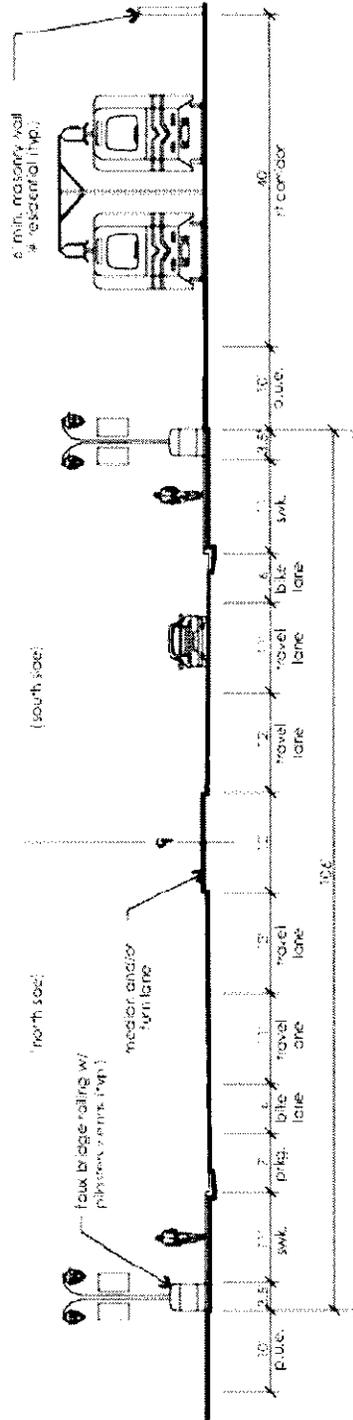
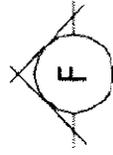


Exhibit #25 - Meister Way at Wildlife Corridor/Faux Bridge 106' ROW

Greenbriar PUD Guidelines

Collector Street 1 – 95' ROW

Greenbriar Boulevard (the main project entry) terminates into this Collector Street, which extends both west and east dispersing traffic into, and tunneling traffic out of, the PUD area. This facility is comprised of the following components:

- 9.5' ultimate right-of-way
- 12' median/left turn lane
- two travel lanes (12' and 11') in each direction
- 6' striped bike lanes
- 7.5' landscape planter with 5' separated sidewalk on each side
- parking is not permitted

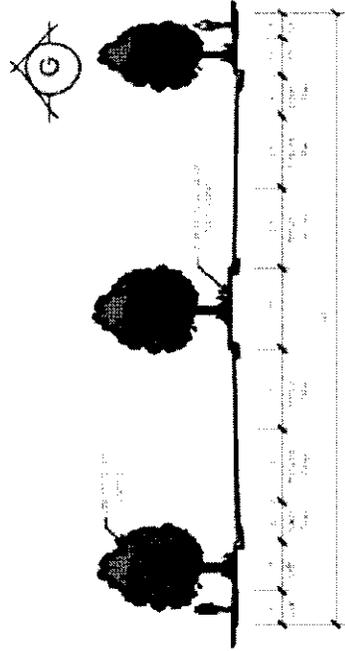


Exhibit #26 - Collector Street 1 – 95' ROW

Collector Street 2 – 65.5' ROW

This collector street is an extension of the 65.5' R.O.W. collector east into the plan area and ultimately terminates into the Community Park. This collector functions as a secondary access into the Village Commercial site and as a primary entry into the Community Park. This facility is comprised of the following components:

- 65.5' ultimate right-of-way
- one 11' travel lane each way
- 6' striped bike lanes
- 7' parking lanes
- 7.5' landscape planter with 5' separated sidewalk on north side (at commercial frontage)
- 5' attached sidewalk (no planter) on south side (at community park frontage)
- parking is permitted

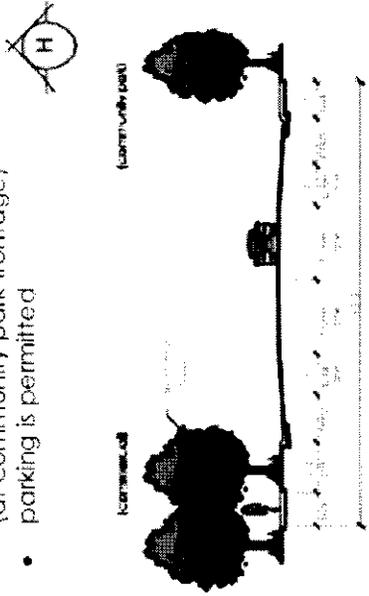


Exhibit #27 - Collector Street 2 –65.5' ROW

Greenbriar PUD Guidelines

Residential Collector "Main Street" – 94' ROW

This "Main Street" facility occurs in two places within the PUD area and provides direct north/south access through and within the northern portion of plan area. This facility is specifically designed to separate through or commuter traffic from local traffic serving the adjacent front-on residential units. This traffic-calming facility replaces the need for a typical large 4-lane collector with soundwalls with back-on residences that would divide the neighborhood, but rather promotes a more pedestrian-friendly, walkable and aesthetically superior alternative.

This facility is comprised of the following components.

- 94' ultimate right-of-way
- one 12' travel lane in each direction
- 5.5' striped bike lanes
- 7.5' landscape planters
- separated 10' one-way travel lane (on each side)
- 7' parking lanes
- 5' attached sidewalks
- parking is permitted

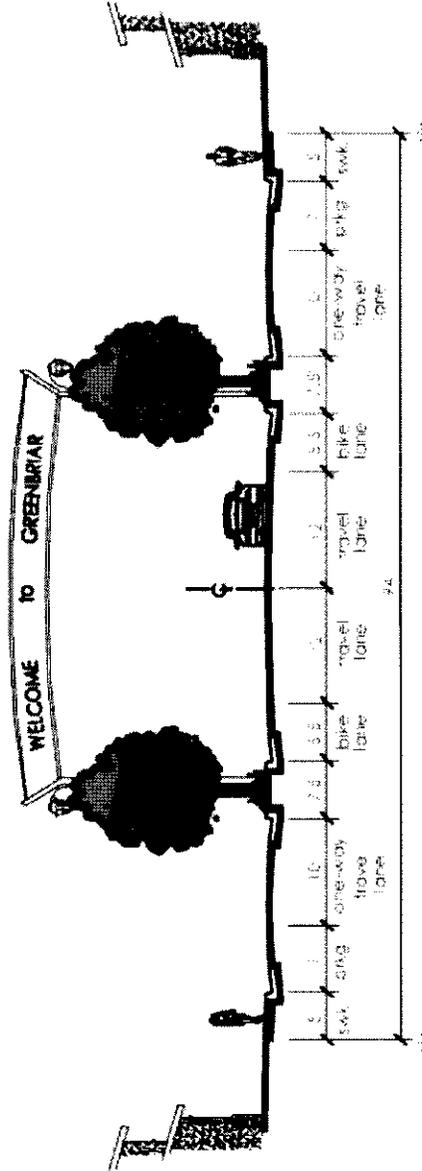
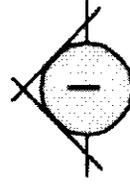


Exhibit #28 - Residential Collector "Main Street" – 94' ROW

Greenbriar
PUD Guidelines

Residential Collector "Main Street" at Lake Bridge 60' ROW

This Main Street facility occurs only where the residential collector crosses the lake feature. At these crossings, the 10' travel lanes and associated parking lane are no longer needed so they are removed. This facility is comprised of the following components.

- 60' ultimate right-of-way
- one 12' travel lane in each direction
- 5.5' striped bike lanes
- 9' attached sidewalk on each side
- parking is not permitted

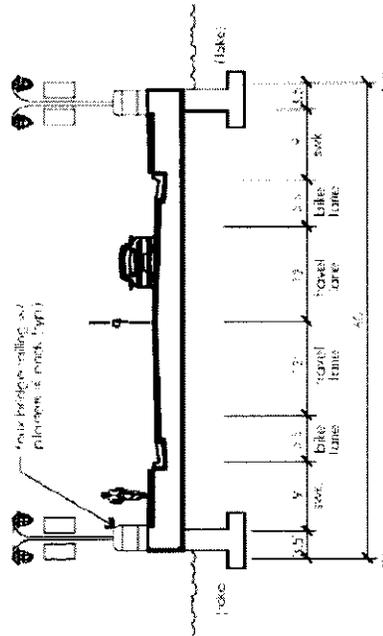
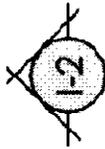


Exhibit #29 - Residential Collector at Lake Bridge - 60' ROW

Greenbriar PUD Guidelines

Residential Collector "Main Street" – Entry at Meister Way - 94' ROW

This "Main Street" facility occurs in two places within the PUD area where this roadway intersects with Meister Way. At this location, the residential-serving frontage roads units are not necessary and instead have been replaced with a southbound left turn lane and wide landscape median that separates traffic.

This facility is comprised of the following components.

- 94' ultimate right-of-way
- one northbound (12') travel lane, and two southbound (12' and 11') travel lanes
- 12' landscape median
- 5.5' striped bike lanes
- 7.5' landscape planters
- 5' detached sidewalks
- 11' landscape corridor on east side of street

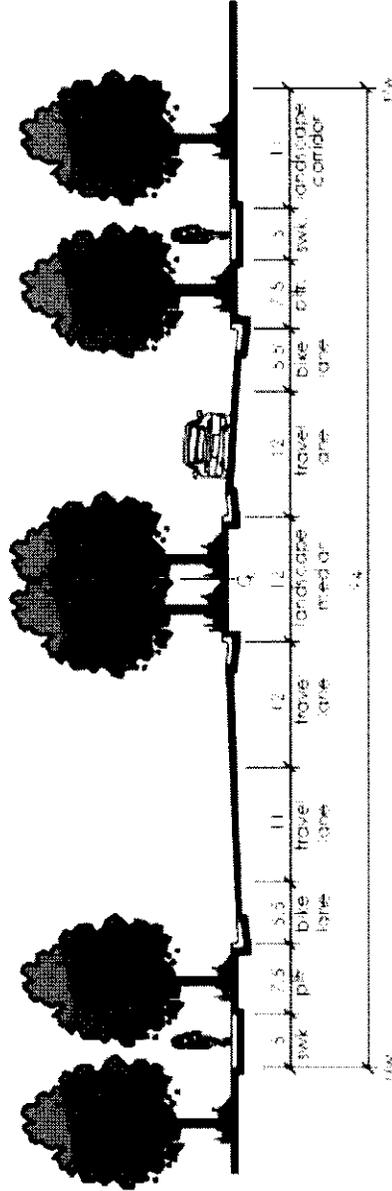
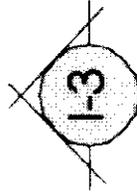


Exhibit #30 - Residential Collector "Main Street" – Entry at Meister Way - 94' ROW

Greenbriar PUD Guidelines

Residential Collector "Main Street" - 94' ROW

This "Main Street" facility section occurs at intersections of east/west streets where the residential-serving frontage road is not necessary and has been removed to minimize overall pavement width, and therefore pedestrian crossing distances, at intersections.

This facility is comprised of the following components.

- 94' ultimate right-of-way
- one 12' travel lane in each direction
- 5.5' striped bike lanes
- 7.5' landscape planter
- 5' detached sidewalk
- 17' landscape planter
- parking is not permitted

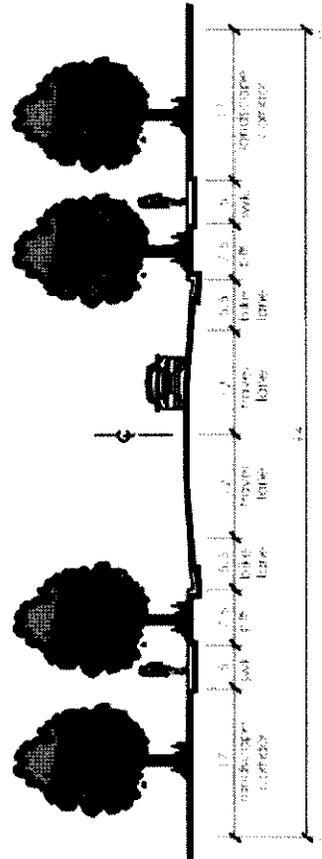
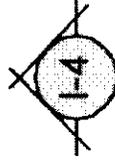


Exhibit # 31 - Residential Collector "Main Street" - "Neckdown" at east/west street intersections - 94' ROW

Greenbriar

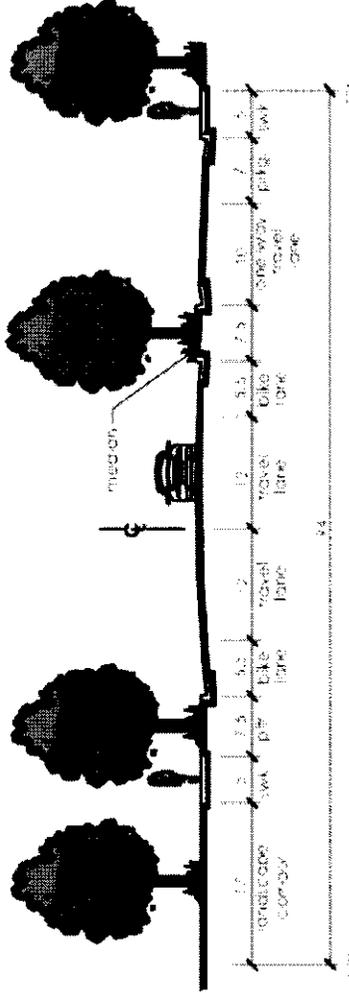
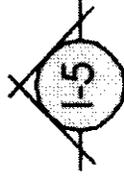
PUD Guidelines

Residential Collector "Main Street" – 94' ROW

This "Main Street" facility occurs in one place within the PUD. This facility is designed for the location where the residential-serving one-way lane is not necessary on one side of the street.

This facility is comprised of the following components.

- 94' ultimate right-of-way
- one 12' travel lane in each direction
- 5.5' striped bike lanes
- 7.5' landscape planters
- 5' detached sidewalk (west side of street)
- 17' landscape corridor (west side of street)
- 10' one-way travel lane (east side of street)
- 7' parking lane on east side of street only
- 5' attached sidewalk (east side of street)



**Exhibit #32 - Residential Collector "Main Street"
Separated 10' one-way travel lane and 7' parking on
east side of street – 94' ROW**

Greenbriar PUD Guidelines

Primary Residential – 55' ROW

Primary residential streets within the PUD area are the main connector streets within the residential street network. These street trees will have a wider landscape planter to accommodate a larger and more prominent street tree, which will serve to orient and define the community. The overall pavement width is the same as the local residential streets. This facility is comprised of the following components:

- 55' ultimate right of way
- one 15' travel lane in each direction
- 7.5' landscape planter with 5' separated sidewalk on each side
- parking is permitted

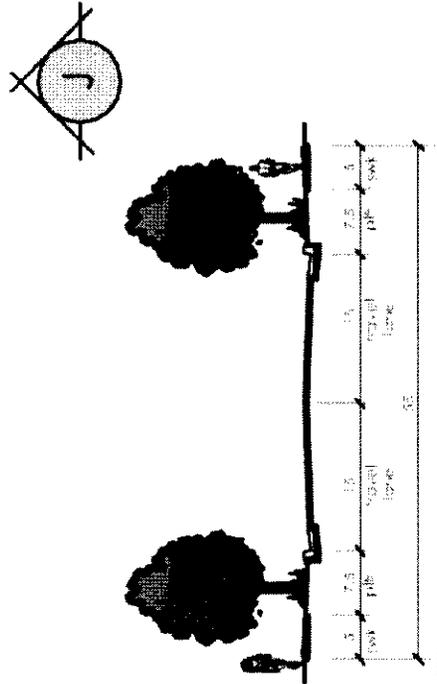


Exhibit #33 - Primary Residential – 55' ROW

Primary Residential – 60' ROW

This primary residential street is consistent with the 55' R.O.W. primary residential street however the pavement width is 5' wider which is necessary to accommodate underground infrastructure. This facility is comprised of the following components:

- 60' ultimate right-of-way
- one 17.5' travel lane in each direction
- 7.5' landscape planter with 5' separated sidewalk on each side
- parking is permitted

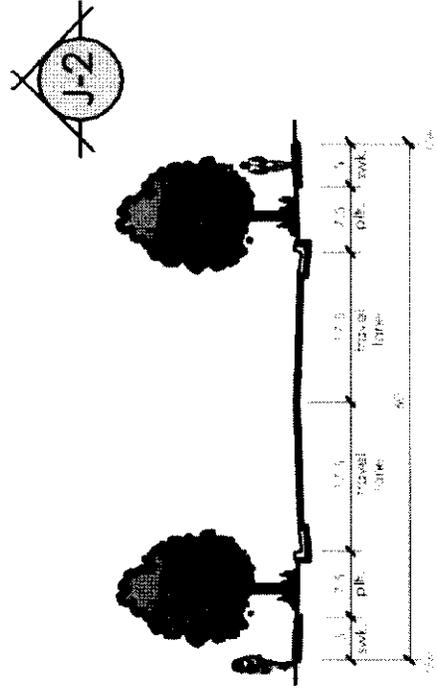


Exhibit #34 - Primary Residential – 60' ROW

**Greenbriar
PUD Guidelines**

Primary Residential – 47.5' ROW

This primary residential street is consistent with the 55' R.O.W. primary residential street however the separated sidewalks are located only on the side of the street adjacent to the residential uses. On the school/park/open space side of the street the sidewalk is attached and the 7.5' parkway has been removed. This facility is comprised of the following components.

- 47.5' ultimate right-of-way
- one 15' travel lane in each direction
- 7.5' landscape planter with 5' separated sidewalk on residential side only
- parking is permitted

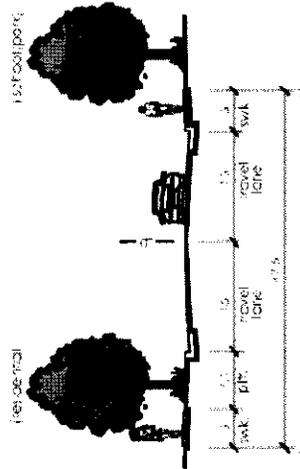
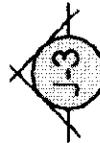


Exhibit #35 - Primary Residential – 47.5' ROW

Primary Residential – Faux Bridge at Wildlife Corridor – 55' ROW

This primary residential street occurs only where primary residential streets cross the Wildlife Corridor/Habitat Buffer. This facility is comprised of the following components.

- 55' ultimate right-of-way
- one 15' travel lane in each direction
- 9' attached sidewalk on each side
- no parking is permitted

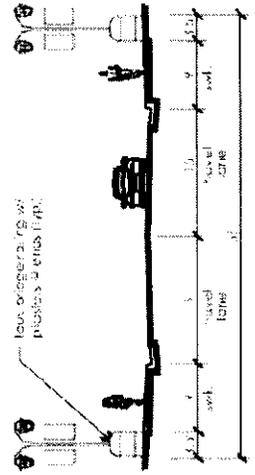
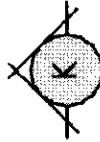


Exhibit #36 - Primary Residential – Faux Bridge at Wildlife Corridor – 55' ROW

Greenbriar PUD Guidelines

Primary Residential at Lake Bridge – 60' ROW

This primary residential street occurs only where the 60' R.O.W. primary residential street crosses the lake facility. This facility is comprised of the following components.

- 60' ultimate right-of-way
- one 17.5' travel lane in each direction
- 9' attached sidewalk on each side
- parking is not permitted

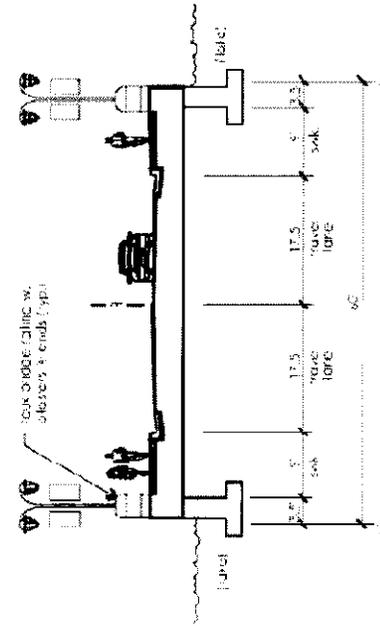
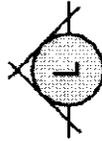


Exhibit #37 - Primary Residential at Lake Bridge – 60' ROW

Primary Residential at Lake Bridge – 55' ROW

This primary residential street occurs only where the 55' R.O.W. primary residential street crosses the lake facility. This facility is comprised of the following components.

- 55' ultimate right-of-way
- one 15' travel lane in each direction
- 9' attached sidewalk on each side
- parking is not permitted

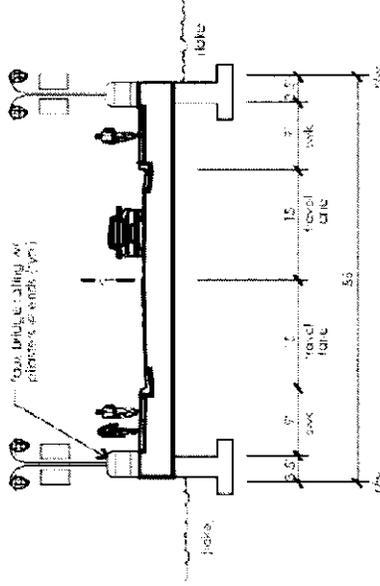


Exhibit #38 - Primary Residential at Lake Bridge – 55' ROW

Greenbriar PUD Guidelines

Local Residential Street – 53' ROW

This local residential street is the predominant street within the plan area in terms of occurrence and serves the vast majority of the residential land uses. This facility is comprised of the following components.

- 53' ultimate right of way
- one 15' travel lane in each direction
- 6.5' landscape planter with 5' separated sidewalk on each side
- parking is permitted

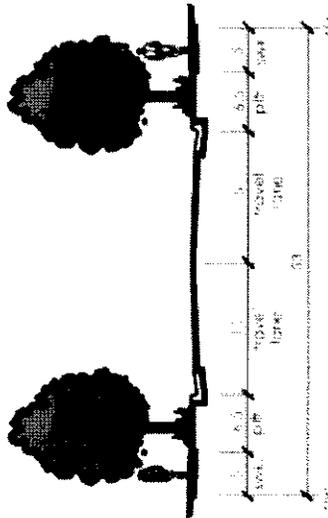
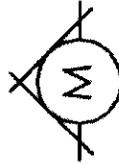


Exhibit #39 - Local Residential Street – 53' ROW

Local Residential Street – 58' ROW

This local residential street is designed with the same features as the typical 53' R.O.W. local residential street however the overall pavement width is 5' wider to accommodate underground infrastructure. This facility is comprised of the following components.

- 58' ultimate right-of-way
- one 17.5' travel lane in each direction
- 6.5' landscape planter with 5' separated sidewalk on each side
- parking is permitted

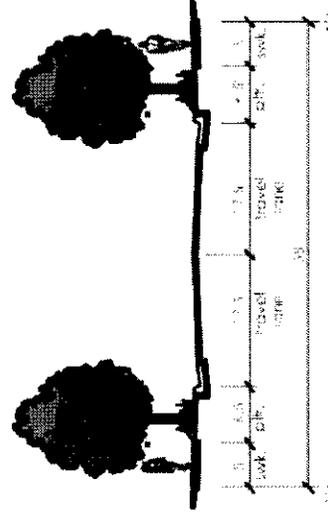
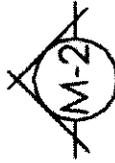


Exhibit #40 - Local Residential Street – 58' ROW

Greenbriar PUD Guidelines

Local Residential Street – 51.5' ROW

This local residential street is designed with the same features as the typical 53' R.O.W. local residential street however the landscape planter and separated sidewalk are located only on one side of the street adjacent to the residential uses. This local residential street is located immediately south of the community park and the distance from face of curb to face of sidewalk is 35' instead of 30' to accommodate underground infrastructure. This facility is comprised of the following components.

- 51.5' ultimate right-of-way
- one travel lane in each direction
- 6.5' landscape planter with 5' separated sidewalk on residential side only
- 5' attached walk along the park frontage
- parking is permitted

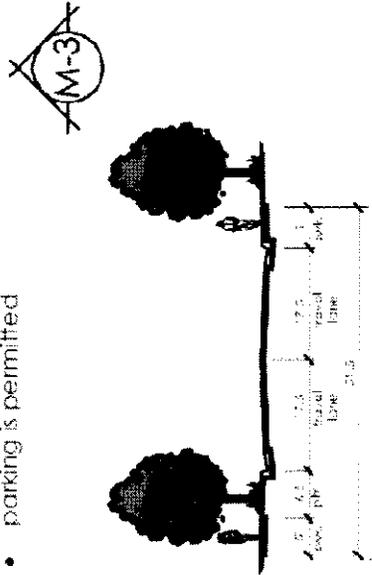


Exhibit #41 - Local Residential Street – 51.5' ROW

Local Residential Street – 46.5' ROW

This local residential street is designed with the same features as the typical 53' R.O.W. local residential street however the landscape planter and separated sidewalk are located only on one side of the street adjacent to the residential uses. This local residential street is located on the west side of the community park. This facility is comprised of the following components.

- 46.5' ultimate right of way
- one travel lane in each direction
- 6.5' landscape planter with 5' separated sidewalk on residential side only
- 5' attached sidewalk along the park frontage
- parking is permitted

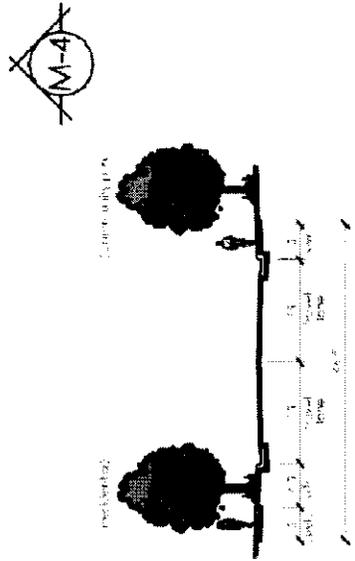


Exhibit #42 – Local Residential Street - 46.5' ROW

Greenbriar PUD Guidelines

Local Residential Street – 42' ROW

This local residential street is designed with the same features as the typical 53' R.O.W. local residential street however the landscape planter and separated sidewalk are located on the residential side of the street only. The sidewalk and planter on the other side of the street are replaced by a meandering pedestrian trail located within the open space/freeway buffer. This facility is comprised of the following components.

- 42' ultimate right-of-way
- one travel lane in each direction
- 6.5' landscape planter with 5' separated sidewalk on residential side only
- parking is permitted

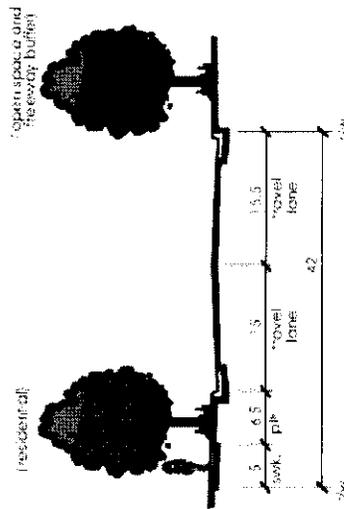
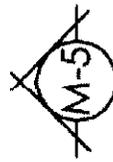


Exhibit #43 - Local Residential Street – 42' ROW

Private Drive – 22' ROW

This private drive is designed with the same features as the typical 22' R.O.W. public alley. There is a 5' wide to accommodate underground infrastructure on each side. This facility is comprised of the following components.

- 22' ultimate right-of-way
- 32' from edge of roof overhang to edge of roof overhang (includes 5' dry utility easement on each side)
- surfaced drained (no drain pipes)
- parking is not permitted

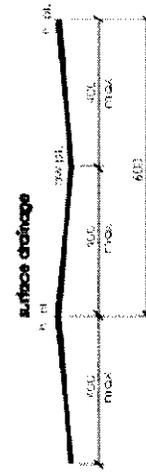
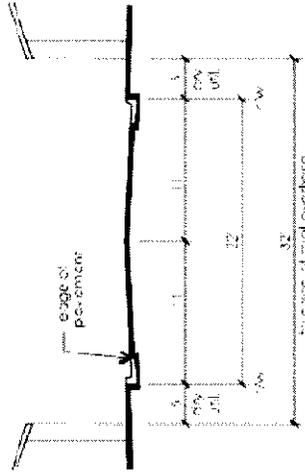


Exhibit #44 – Private Drive – 22' ROW

Greenbriar PUD Guidelines

Private Alley – 23' ROW

This 23' private alley is located where alley accessed residential uses do not front onto public streets but rather they front onto the lake feature. Infrastructure serving these residential uses will be located in the private alley. This facility is comprised of the following components:

- 23' ultimate right-of-way
- asphalt
- 33' from edge of roof overhang to edge of roof overhang (includes 5' dty utility easement on each side)
- surfaced drained (no drain pipes)
- parking is not permitted

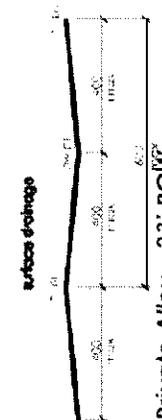
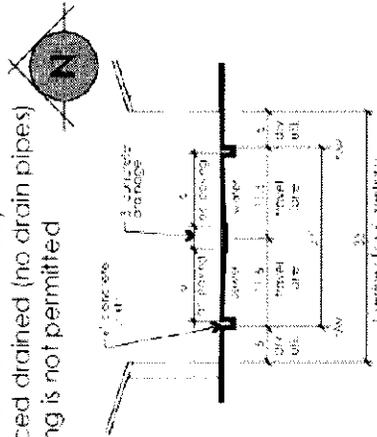


Exhibit #45 - Private Alley – 23' ROW

Public Alley – 22' ROW

This 22' public alley is located where alley accessed residential uses front onto public streets. Infrastructure serving these residential uses will be located in the public streets. This facility is comprised of the following components:

- 22' ultimate right-of-way
- 32' from edge of roof overhang to edge of roof overhang (includes 5' dry utility easement on each side)
- surfaced drained (no drain pipes)
- parking is not permitted

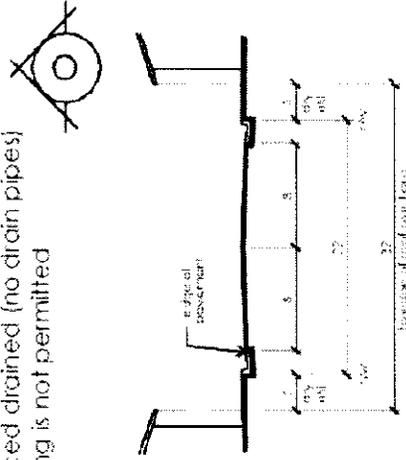


Exhibit #46 - Public Alley – 22' ROW

22' Lakewalk Section

This 22' Lakewalk section is located adjacent to the lake/detention basin. This section shows the relationship between the rear yards of the lake-adjacent lots and the lake. This facility is comprised of the following components:

- 22' wide pedestrian and landscape planter
- 8' pedestrian walk (providing pedestrian circulation around the lake)
- Back yard access to lots that are adjacent to the lake

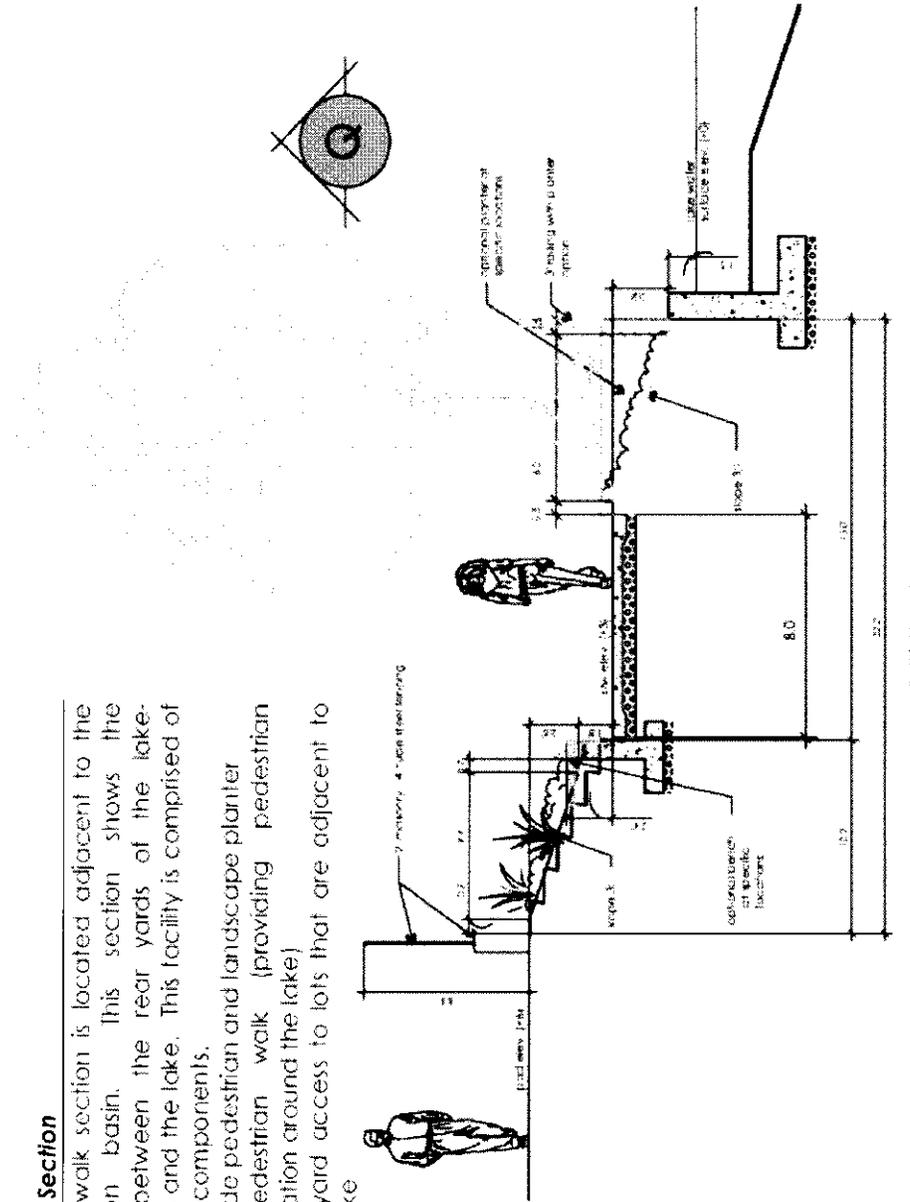


Exhibit #47 – 22' Lakewalk Section

Greenbriar
PUD Guidelines

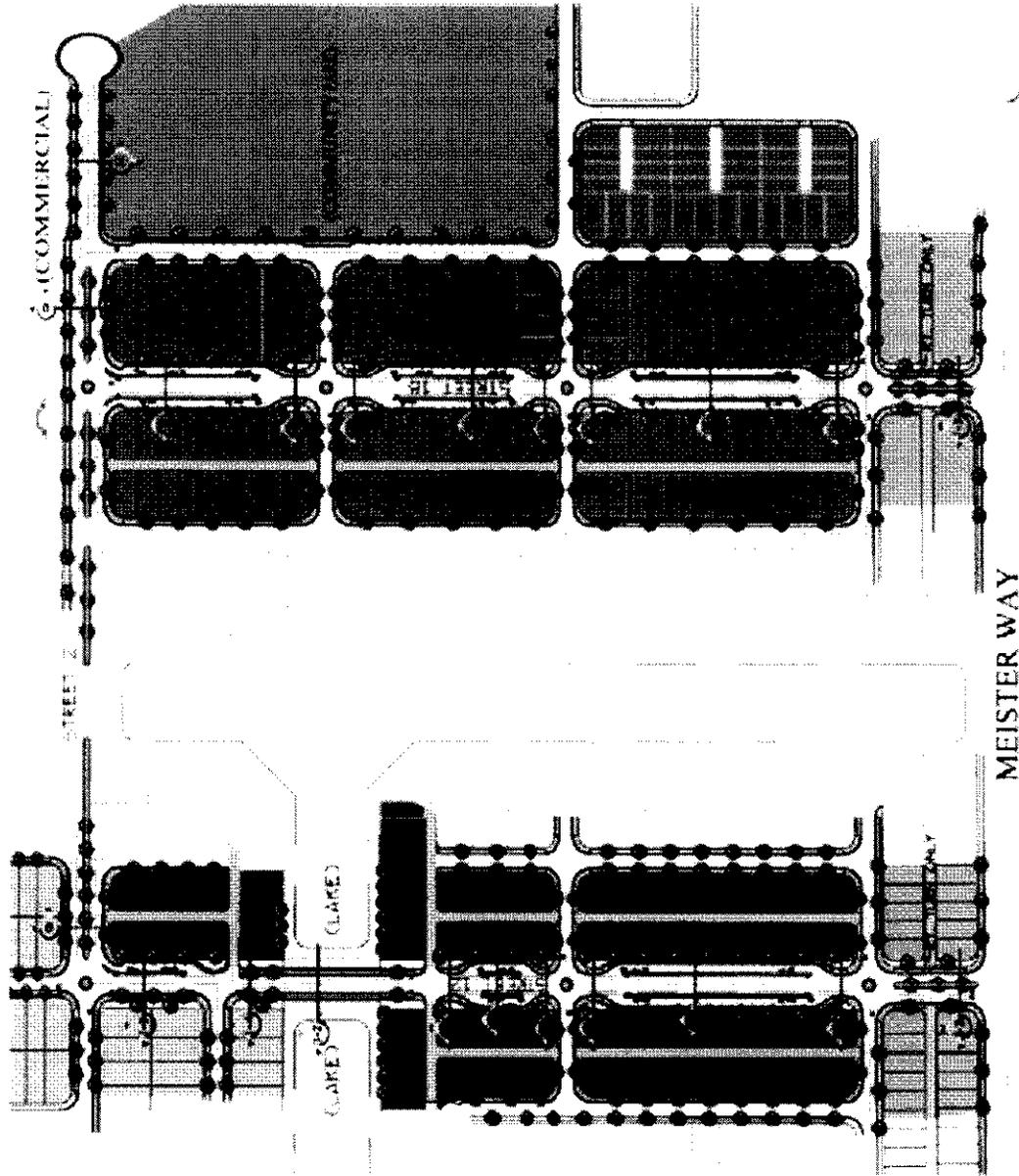


Exhibit #48 -
"Main Street"
Circulation

Greenbriar
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loaded narrow lot homes, cluster development, small lot homes and traditional lot homes.

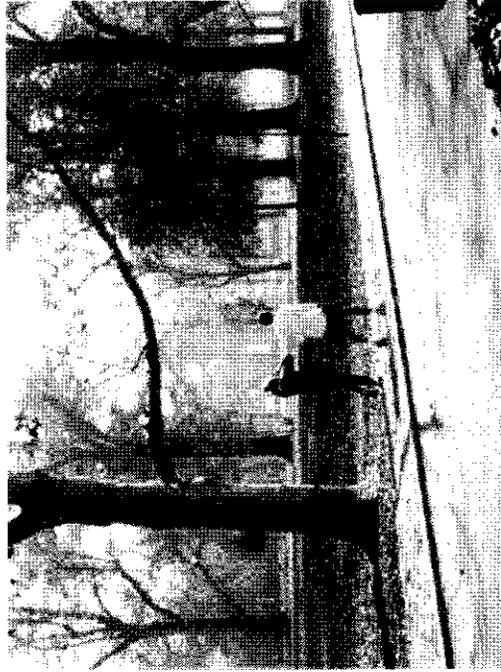


Exhibit #49 – Pedestrian Scale

section 3 – Housing

3.1 Principles and Objectives

3.1.a Balanced Mix of Land Uses

Greenbriar provides a balanced mix of land uses, including a variety of single family and multi family housing types, commercial uses, parks and open space. This mix of land uses not only addresses the needs of Greenbriar residents, but they also address the needs of the larger community. The entire Greenbriar community is oriented around a series of waterways and lakes that provide opportunities for walking, running and recreational uses. Two clubhouses serve as community gathering locations and offer further recreational uses such as pools and fitness facilities. Greenbriar is a TOD that takes advantage of the Meister Way RT extension and Greenbriar Station stop, centrally located within the project.

3.1.b Distinct Neighborhoods

Greenbriar's residential housing is organized into distinct neighborhoods. These neighborhoods will provide a diversity of housing types that will include apartments, senior housing, attached town homes, alley and front-

Greenbriar
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Exhibit #50 – Pedestrian Scale



Exhibit #51 – Authentic Architecture

Greenbriar PUD Guidelines

3.1.c Authentic Architecture

Greenbriar will distinguish itself by looking to classic Sacramento architecture for inspiration. Turn of the century neighborhoods like Curtis Park, McKinley Park and Land Park display a range of architectural styles thematically joined by the time in which they were built. Greenbriar will borrow from these vibrant street scenes and use architectural styles that are appropriate to Sacramento.



Exhibit # 52 – Authentic Architecture

3.2 Housing Alternatives

3.2.a Market Rate

The Greenbriar PUD offers a great variety and diversity of housing options dispersed throughout the plan area. The plan provides eleven different and distinct low and medium-density single-family lot sizes and options to meet the growing needs of the Sacramento region. These market-rate home-ownership opportunities are located throughout the PUD to appeal to an economic, age and lifestyle diverse community of future residents. These single-family lots include both traditional and non-traditional lot sizes, alley-load lots, cluster lots and detached townhomes, and all in close proximity to the planned extension of regional transit (RT).

3.2.b Senior Housing

The Greenbriar PUD provides for multi-family uses as well as a location for seniors-only attached housing located within a short walk to the RT transit station and commercially-planned lands. The senior housing site located across the street from the community commercial site and adjacent transit station. In recognition of the special needs of the PUD's senior population, the senior housing site was carefully planned in this location to encourage walking as an attractive option for the adjacent neighbors for their daily needs and recreation.

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This 2-to-3 story seniors housing site will be planned with a centrally-located community garden as well as a recreation area for exercise and interaction between the residents. The architecture will be designed to blend in with the adjacent single-family residential areas. Additionally, special attention will be paid to planning the crosswalk locations as well as to parking areas and drive aisle locations so as to minimize potential conflicts between pedestrians and motorists.

3.3 Development Standards

3.3.a Density

Residential densities will vary throughout Greenbriar but will be categorized in keeping with the City of Sacramento's zoning code. See Sacramento Municipal Code Title 17, Division II, Chapter 17.20, Zoning Districts for detailed information. Where there are discrepancies between these guidelines and the Sacramento Municipal Code, these guidelines shall prevail. Where these Guidelines are silent, the Sacramento Municipal Code shall prevail.

3.3.b Single-Family Development Standards

The Greenbriar PUD provides for varied single-family residential opportunities to attract a diverse population. Eleven different single-family residential home-ownership opportunities are located within the PUD to appeal to an economically- and lifestyle-diverse

community of future residents. These single-family lot sizes/product types are listed below and described in detail on the exhibits on the following pages.

- 60'x110' Conventional (Front-Loaded) Lots
- 55'x100' Conventional (Front-Loaded) Lots
- 50'x100' Conventional (Front-Loaded) Lots
- 45'x100' Conventional (Front-Loaded) Lots
- 40'x90' Conventional (Front-Loaded) Lots
- 40'x90' Alley-Loaded Lots
- 35'x80' Alley-Loaded Lots
- 35'x70' Conventional (Front-Loaded) Lots
- 30'x70' Alley-Loaded Lots
- Cluster/Courtyard Lots
- Townhomes

The Typical Development Standards Exhibits illustrated in this PUD Guidelines outline the typical lot and setback requirements needed for the single-family product types listed above. The exhibits illustrate and list particular detailed information to accommodate a particular product envisioned for the PUD area. This information may include, but is not limited to the following items.

- Minimum Lot Size
- Minimum Interior Lot Width
- Minimum Corner Lot Width
- Minimum Lot Depth
- Maximum Lot Coverage
- Minimum Front Yard Setback

- Minimum Garage Setback
- Minimum Interior Side Yard Setbacks
- Minimum Corner Side Yard Setbacks
- Minimum Alley / Rear Yard Setback
- Lot Type (front or alley loaded)
- Maximum Building Height
- Public Utility Easement

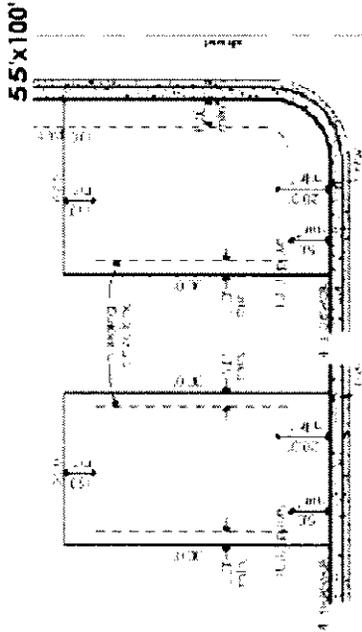
3.3.c Multi-Family Development Standards

Lots A, C and D are subject to the residential densities of the City of Sacramento's Zoning Code, with the exception that lots C and D are permitted a maximum building height of 58' and a maximum of 4 stories. See Sacramento Municipal Code Title 17, Division II, Chapter 17.20, Zoning Districts for detailed information. Where there are discrepancies between these guidelines and the Sacramento Municipal Code, these guidelines shall prevail. Where these Guidelines are silent, the Sacramento Municipal Code shall prevail.

3.3.d Permitted Uses

Land uses in Greenbriar shall comply with Sacramento Municipal Code land use regulation, Title 17, Division II, Chapter 17.24, Land Use Regulations contains detailed information about permitted uses under all conditions.

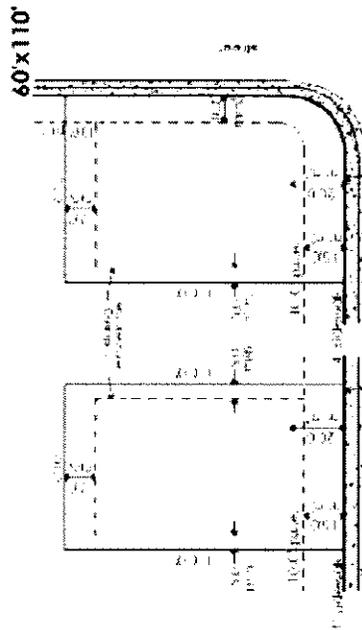
Greenbriar
PUD Guidelines



lot requirements	setback requirements
min. front	min. front
min. side	min. side
min. rear	min. rear
min. corner	min. corner
min. driveway	min. driveway
min. garage	min. garage
min. deck	min. deck
min. pool	min. pool
min. fence	min. fence
min. wall	min. wall
min. sign	min. sign
min. utility	min. utility
min. other	min. other

NOT TO SCALE
 Minimum setbacks shown on lot plan

Exhibit #53 – Typical Development Exhibit 60' x 110'



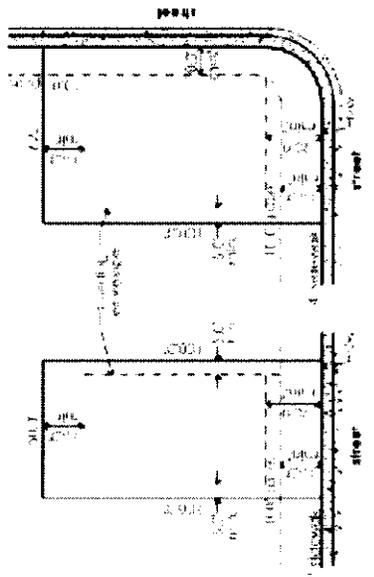
lot requirements	setback requirements
min. front	min. front
min. side	min. side
min. rear	min. rear
min. corner	min. corner
min. driveway	min. driveway
min. garage	min. garage
min. deck	min. deck
min. pool	min. pool
min. fence	min. fence
min. wall	min. wall
min. sign	min. sign
min. utility	min. utility
min. other	min. other

NOT TO SCALE
 Minimum setbacks shown on lot plan

Exhibit #54 – Typical Development Exhibit 55' x 100'

Greenbriar
PUD Guidelines

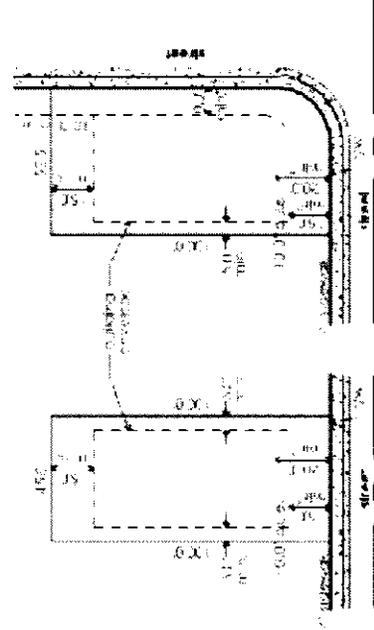
50'x100'



lot requirements	setback requirements
min. lot size	min. front setback
min. building width	min. front to garage
min. corner width	min. side
min. depth	min. rear setback
ACCESS	min. rear to garage
6'	5'

NOT TO SCALE
* setbacks from back of lot

45'x100'



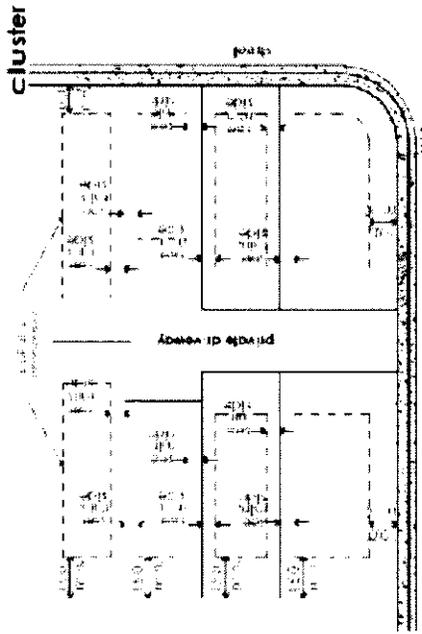
lot requirements	setback requirements
min. lot size	min. front setback
min. building width	min. front to garage
min. corner width	min. side
min. depth	min. rear setback
ACCESS	min. rear to garage
6'	5'

NOT TO SCALE
* setbacks from back of lot

Exhibit #55 – Typical Development 50'x100'

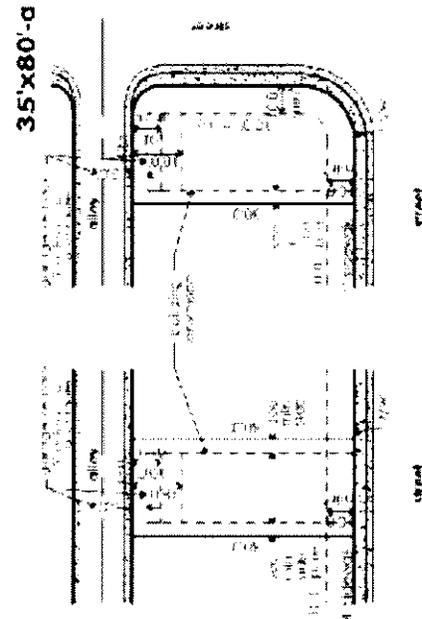
Exhibit #56 – Typical Development 45'x100'

Greenbriar
PUD Guidelines



of requirements	setback requirements
min. street	min. front setback
min. side of width	min. front setback
min. corner setback	min. side setback
min. of width	min. side setback
ACCESS	min. side setback
to back driveway	min. side setback

NOT TO SCALE
Dimensions are from back of block



of requirements	setback requirements
min. street	min. front setback
min. side of width	min. front setback
min. corner setback	min. side setback
min. of width	min. side setback
ACCESS	min. side setback
to back driveway	min. side setback

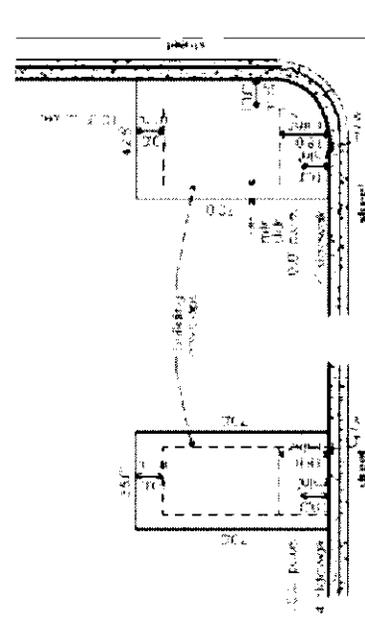
NOT TO SCALE
Dimensions are from back of block

Exhibit #60 – Typical Development Cluster

Exhibit #59 – Typical Development 35'x80'-alley

Greenbriar
PUD Guidelines

35'x70'-f



lot requirements

- min. steps 3
- min. front setback 15'
- min. corner setback 10'
- min. side setback 10'
- min. rear setback 10'

ACCESS
10'

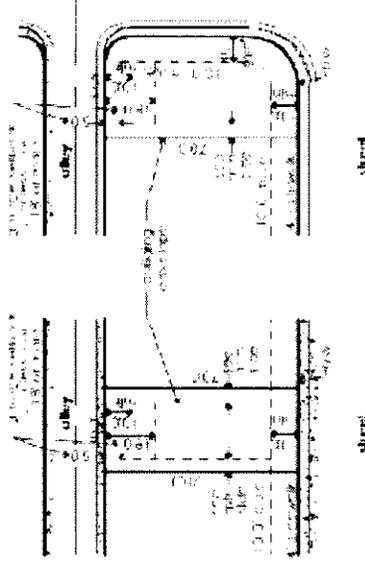
NOT TO SCALE

includes driveway and setbacks

setback requirements

- min. front setback 15'
- min. front setback 10'
- min. side setback 10'
- min. rear setback 10'
- min. rear setback 10'

30'x70'-d



lot requirements

- min. steps 2
- min. front setback 15'
- min. corner setback 10'
- min. side setback 10'
- min. rear setback 10'

ACCESS
10'

NOT TO SCALE

includes front driveway

setback requirements

- min. front setback 15'
- min. front setback 10'
- min. side setback 10'
- min. rear setback 10'
- min. rear setback 10'

Exhibit #61 – Typical Development 35'x70'-front

Exhibit #62 – Typical Development 30'x70'-alley

section 4 – Commercial Uses

4.1 Village Commercial

4.1.a Principles and Objectives

Site planning in Greenbriar's commercial area should result in a varied street scene along its main circulation streets that will be interesting for both pedestrians and motorists. Varied facade treatments for each individual tenant are encouraged, but all should be complementary in sum.

Building massing should relate to the street and pedestrian walkways to invite visitors into shops. Special attention paid to pedestrian circulation, sheltered walkways, outdoor cafe seating and plaza areas that protect pedestrians from sun or inclement weather is encouraged.

Anchor and sub-anchor retail building architecture should be compatible with the entire Greenbriar community character and reflect the selected architectural styles. Building designs should incorporate a variety of scale, massing, materials and colors to minimize the visual impact of "big box" retail spaces.

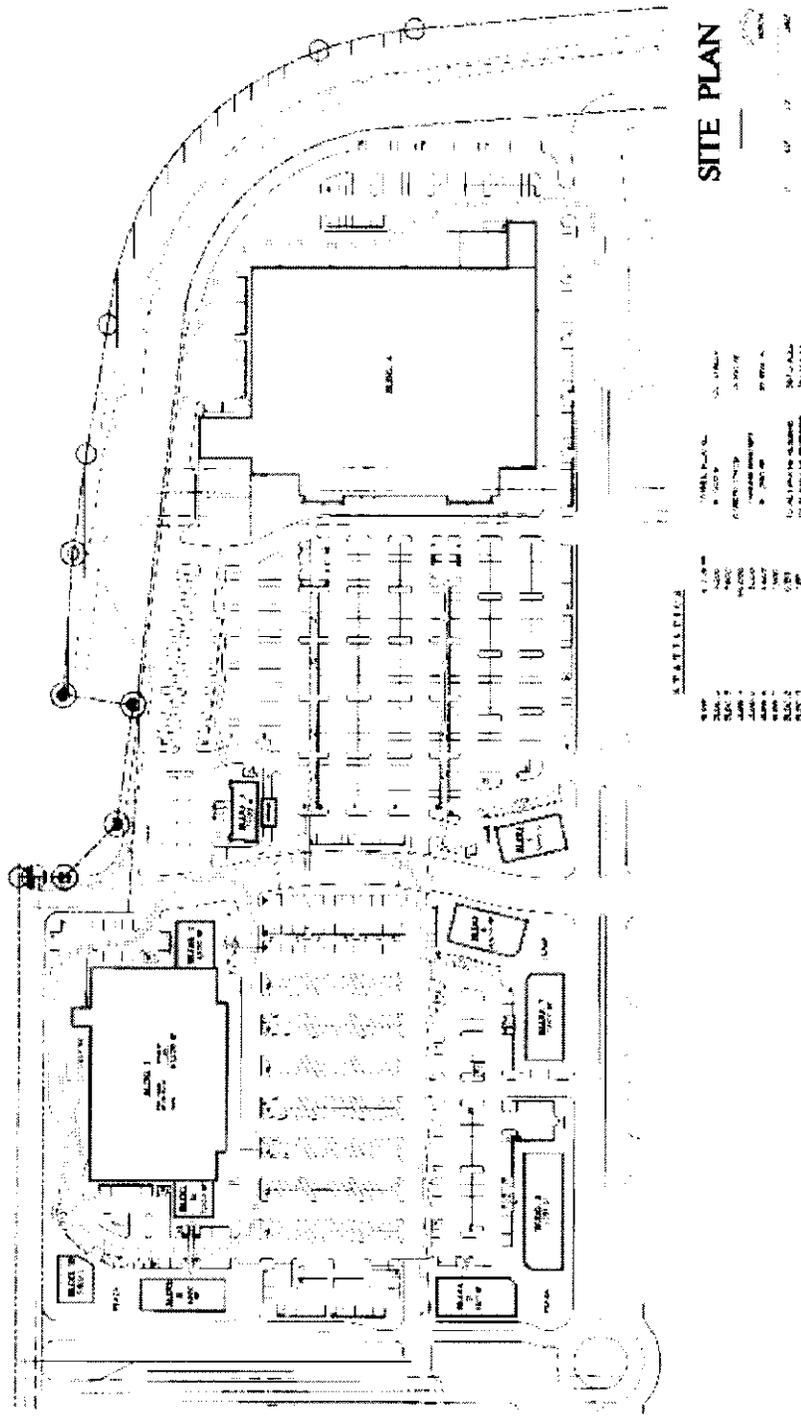
4.2 Community Commercial

4.2.a Large Format Retail

The two large community-serving retail spaces in the commercial area will serve both Greenbriar and the surrounding Sacramento community.

The largest community-serving retail building (Building 4) is estimated at 145,000 square feet and will be the single largest retail building in the project. This is an appropriate size for a large home improvement store or general merchandiser to serve as the anchor retail tenant. It is important to break this building's facades into distinct elements as much as possible and employ the basic architectural techniques of varying color, scale and materials to avoid typical "mega-store" appearance.

Greenbriar
PUD Guidelines



STATISTICS

ITEM	QUANTITY	UNIT	TOTAL
BLOCK 1	1	SQ. FT.	10,000
BLOCK 2	1	SQ. FT.	10,000
BLOCK 3	1	SQ. FT.	10,000
BLOCK 4	1	SQ. FT.	10,000
PARKING	100	SPACES	100
WALKWAYS	100	LINEAL FT.	100
LANDSCAPING	100	SQ. FT.	100
UTILITIES	100	LINEAL FT.	100
TOTAL			

SITE PLAN

Exhibit #63 - Village Commercial Site Plan-Concept Plan

Planning Commission Draft

November 8, 2007

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Greenbriar
PUD Guidelines

Building 1 is estimated at 67,000 square feet and is an appropriate size to accommodate a large community-serving grocery. It may also be broken up into several sub-major type retailers. Similar to Building 4, special care should be taken to adjust the lines and facades of the store to the look and feel of Greenbriar's overall architecture through the use of varied massing, scaling for pedestrian-friendly spaces and varying building materials.



Exhibit #64 - Neighborhood-Serving Commercial



Exhibit #65 - Neighborhood Icon Commercial

4.2.b Neighborhood-Serving Commercial

Neighborhood-serving commercial spaces along the western and southern edges of the commercial area have the most potential to express individual, contextually sensitive street scenes. Whenever possible, Sacramento's classic community architecture should be reflected.

**Greenbriar
PUD Guidelines**

4.3.b Massing and Architectural Treatment

Commercial Style

The overall style of the Greenbriar's commercial location should employ eclectic use of traditional materials and forms to create architectural flavor. Forms, proportions and materials should create visually pleasing buildings able to bridge the gap between residential housing and the more modern buildings surrounding the site. Varied materials and styles within building facades are encouraged to reflect Sacramento's classic community architecture. Focal points and view corridors throughout the project should invite visitors from one point to another within the project and are critical to creating an inviting, human-scaled environment.

Colors

Colors should be selected to offer distinction and individuality to each building and tenants in larger buildings. Bold or saturated hues are encouraged as long as they are not garish or obtrusive. Colors should bring together selected project materials and be selected to complement other stone, concrete, wood and fabrics.



Exhibit #66 – Neighborhood-Serving Commercial

Development Standards

4.3.a Floor Area Ratios

Development standards are as per City of Sacramento Municipal Code.

Greenbriar PUD Guidelines

Roofs

Roofs and roof forms should be consistent with the overall architectural theme of the Greenbriar PUD. Individual roof elements placed in key locations along buildings should convey the built-over-time concept in conjunction with building forms. Pedestrian areas should be enhanced by shed and gable roof elements extending into pedestrian areas for cover and shade. Additional elements are allowed such as fabric and metal awnings, trellises, etc. Dormer elements are also encouraged for an added layer of detail and shadow.

Cornices

Cornice elements should be applied and should articulate basic building forms appropriately while providing differential between individual tenants. Varied cornice elements tend to reinforce the built over time concept. When used, cornices should provide contrast of color and material to wall areas beneath. Cornice elements should not be of such size or quantity that they become a dominant repetitive or overwhelming architectural feature.

Wall Transitions

A variety of elements should be used to create wall transitions between buildings and tenant spaces and careful consideration should be given to walls adjacent to and oriented toward open spaces. Color and texture are basic elements of interest while towers and

other details may be used in some cases to frame transition areas. Simple, intermediate elements that book end an area of wall are encouraged. Whenever possible, color and simple traditional material changes are encouraged to break wall areas into visually pleasing proportions.

Building Corners

Building corners present an opportunity to simply enhance visual anchoring of individual structures. Presenting building corners as focal points to surrounding areas within the project is encouraged. Thoughtful treatments of building corners provide changes in scale, color and material, as well as an opportunity to introduce windows as a simple focal detail.

Canopies and Awnings

Judicious use of canopies and awnings is encouraged. These classic architectural details add an additional layer of interest to building facades. A variety of materials may be used including canvas, corrugated metal, wood trellises and shed or gable roof forms. Canopy and awning elements should also provide cover at pedestrian walkways wherever possible. These covered elements should also be placed to encourage the play of shadows against buildings.

Greenbriar PUD Guidelines

Towers

Tower elements may be considered as appropriate to the style of these buildings. When situated and massed properly, towers can enhance visual interest. These elements can serve as a connection between individual buildings as focal points and transitional spaces. Towers should provide a change in scale, color and material, and use windows as well.

Vertical elements should not be limited to towers. The appropriate and tasteful use of chimney elements and finials is also encouraged.

Windows

Shape, size and placement of windows are important elements that lend positive, yet simple character to the overall theme of the project. Window size and proportion should be appropriate to individual building style. Window forms may vary between individual tenant spaces and buildings to subtly reflect the built-over-time concept. Windows, especially at a pedestrian level, are encouraged in overall building design. Consideration of design elements like shutters, canopies, recesses, iron and other elements should be used to enhance windows and add variety.

Building Materials

Materials should reflect the style and overall impression of any building. Materials used for this project should also reflect quality, care and lend to the impression of overall theme.

Greenbriar

PUD Guidelines

Encouraged Materials

Interest and complexity in building design is encouraged. Both contemporary and more traditional approaches to building form and articulation will provide variety, interest and vitality appropriate for these activity areas. Encouraged materials include:

- Smooth stucco finishes
- Style-appropriate stone
- Wrought iron
- Complementary-colored canvas awnings
- Wood trellises
- Tile roof elements
- Split-face block
- Wood columns and beams in key location
- Pre-cast stone lintels, heads and sills
- Metal roof elements
- Decorative sheet metal gutters and downspouts, collectors if and where appropriate
- Wood shutters
- Individually articulated window elements
- Fill-up construction that uses imaginative forming techniques to add texture and shadow to otherwise unarticulated walls

Discouraged Materials

- Heavy "knock-down" / "Spanish Lace" stucco finishes
- Coritived stone veneers
- Unfinished fill-up wall panels
- Large unbroken window walls
- Exposed precision (flat) concrete block walls
- Exposed aggregate walls

Any architectural wood element may be substituted for high quality fiber cement or other manufactured elements as long as quality is maintained and no departures are made from these architectural style guidelines.

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Greenbriar PUD Guidelines

section 5 – Parks, Open Space and Habitat Conservation

threatened species, physical access to the habitat buffer will be restricted and on-going maintenance plan will be established. Interpretive signage regarding the habitat buffer's special site characteristics, plant and animal inhabitants and on-going maintenance efforts may be posted to promote the education of the residents and the larger community.

5.1 Greenbriar Parks

The Greenbriar PUD Schematic Plan provides many varied recreation opportunities in the location of both publicly- and privately-owned active and passive parks and open spaces. One large Community Park, three Neighborhood Parks, three private parks, two private recreation centers, and numerous pocket parks, pedestrian paseos and marina parks, ranging in size, and an elementary school are located within the PUD and are well-linked by residential streets and trails. Additionally, the PUD boasts an expansive lake with a pedestrian-friendly (and non-gated) lakewalk or promenade around the entire perimeter of the lake as well as a wide freeway buffer/open space corridor (along the south and east project boundaries) enhanced with pedestrian trails and landscaping.

The PUD Schematic Plan also includes a wide wildlife corridor/habitat buffer on the western project boundary that will be privately maintained in perpetuity for the preservation of threatened and/or endangered species. Due to the possible presence of rare or

The Overall Parks and Open Space Plan highlights the inclusion of Greenbriar Lake that will accommodate the sites overland drainage needs in a constructed linear lake feature. Greenbriar Lake winds through the PUD area and provides community-wide open space and a major regional pedestrian and bicycle circulation and recreation facility. The drainage component will be designed to efficiently carry flows through the site as well as be an aesthetically pleasing 'naturalized' lake corridor. Alongside the lake is a 22' wide lakewalk or promenade that will extend along the entire perimeter of the lake and will provide circulation and recreation amenities to PUD residents.

The following pages illustrate the Overall Parks and Open Space Plan and highlights the locations of the public facilities versus the private facilities. Additionally, the following pages describe and illustrate the location of various PUD area freeway and habitat buffers as well as their intended use(s) and access opportunities and constraints.

Greenbriar
PUD Guidelines

overall park & open space exhibit

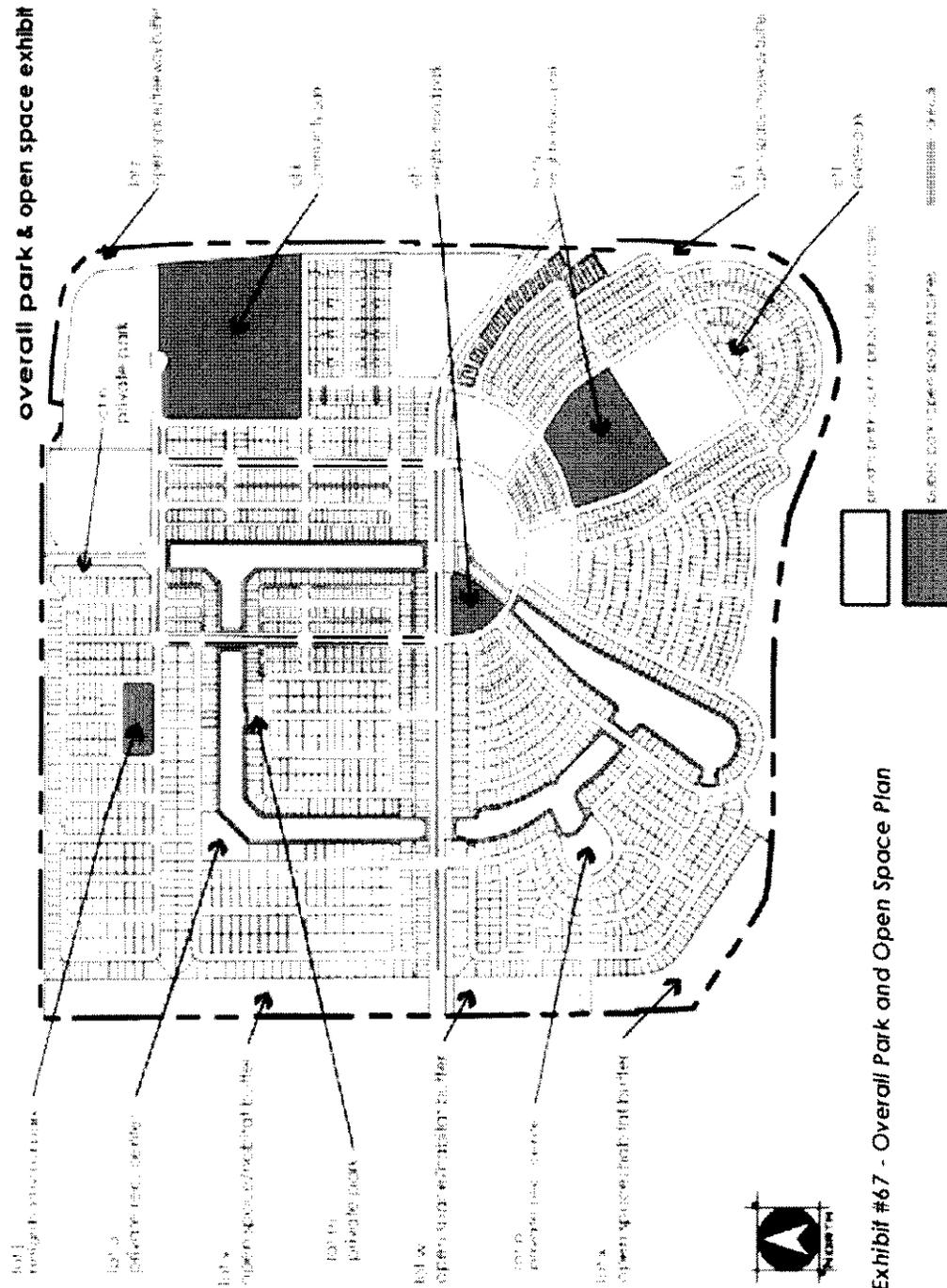


Exhibit #67 - Overall Park and Open Space Plan

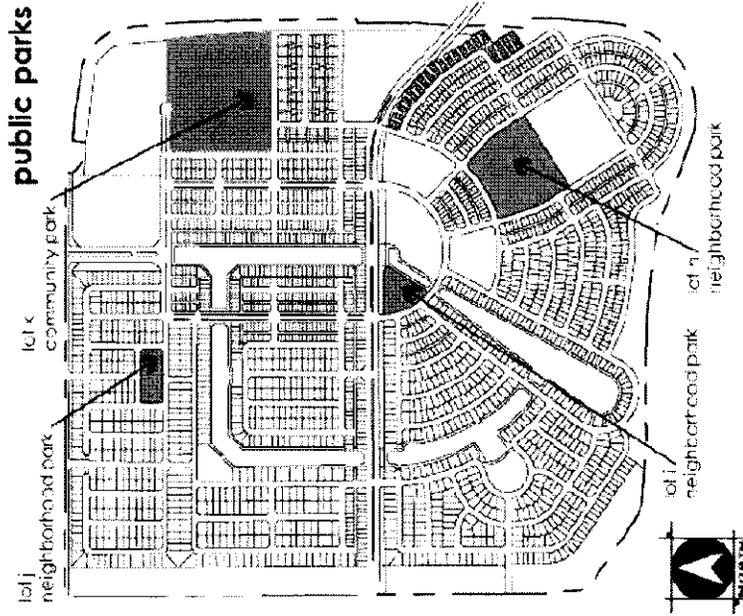


Exhibit #68 - Public Parks

5.1.a Public Parks

The Greenbriar PUD proposes a variety of public park land within the PUD; this includes a total of 4 park sites including two small neighborhood parks, a large neighborhood park co-located adjacent the elementary school site and a large community park.

The parks will be developed in accordance with park master plans developed by the Department of Parks and Recreation, Parks Planning and Development Services (PPDS). Future park plans may include, but are not limited to, tot lots, picnic areas, large open fields, ball fields, parking lots, restroom structures and lighted ball fields in the Community Park. In addition, the community park will incorporate a 16' wide multi-use trail (12' wide trail with 2' shoulders) that will meander throughout the park site and will connect to the adjacent trail system located in the open space/freeway buffer. Landscaping along the multi-use trail will include a combination of shrubs, groundcover, and trees. Vehicular access controls will be placed at the entrance to all trail access points to prohibit automobile access, yet allow maintenance and emergency vehicle access, to the trail.

parks & recreation facilities

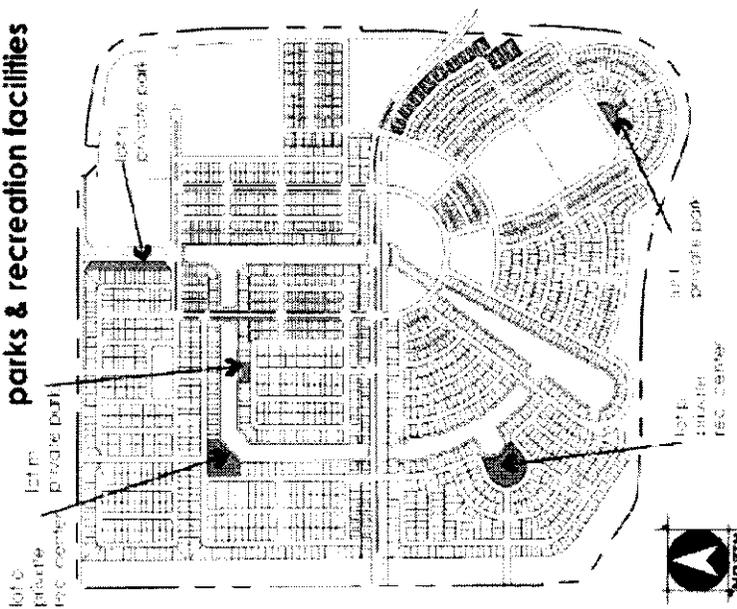


Exhibit #69 - Private Parks and Recreation Facilities

5.1.b HOA Private Parks

The Greenbriar PUD proposes a variety of private park and recreation lands within the PUD; this includes a total of 5 park sites including three small pocket parks and two private recreation center sites.

The parks will be constructed by the developer and maintained by a Homeowners Association (HOA). Future private park and recreation center site plans may include, but are not limited to, lot lots and picnic grounds, trails and grassy open areas. Two of the sites will also incorporate such uses as recreation center buildings with kitchens, banquet rooms and bathrooms, fitness areas and swimming pools.

Three of the five private park sites are thoughtfully planned at lake's edge to encourage neighbors to use the lakewalk trail to gain access to the sites. (The lakewalk amenities are described in more detail in the following section.) Landscaping of these private facilities will include a combination of shrubs, groundcover, trees and bark mulch.

The recreation center facilities are provided for use by all residents within the PUD however will be restricted from use by the general public. The private parks and recreation sites contribute to the PUD's overall parkland dedication requirement as described in the previous section.

Greenbriar PUD Guidelines

5.2 Greenbriar Lake

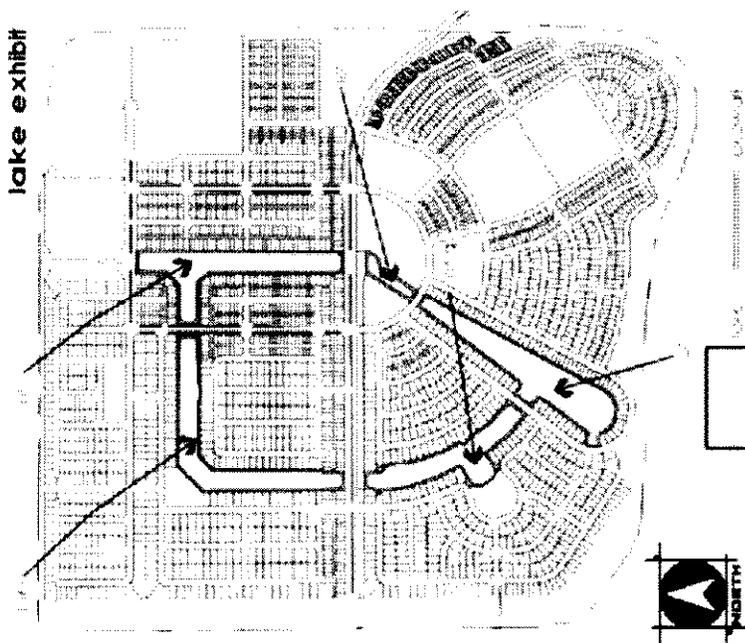
A signature feature of the PUD area is the inclusion of a large lake feature that winds through the site creating a sense of place and neighborhood orientation mechanism for the community. In addition to its aesthetic qualities, the lake will provide for stormwater detention and water quality functions for the PUD. The Greenbriar Lake feature encompasses approximately 40 net acres in land area and is located in the western and central portion of the plan area.

Nearly all of the single-family residential housing products offered in the Greenbriar PUD have direct access to the lake in terms of adjacency for locating model home sites. The lake is virtually surrounded by single-family residential housing in both back-on conventional situations as well as front-on alley-loaded configurations. Additionally, the PUD's two private recreation facilities, various park facilities and the Community Commercial site are also sited along the Lake's frontage.

A key feature of the PUD is the inclusion of a wide lakewalk or pedestrian promenade. This 22' lakewalk section is located adjacent to Greenbriar Lake and will provide residents with a safe and convenient non-vehicular route through and within the plan area. This overall 22' wide lakewalk facility is comprised of the following components including all 8' wide minimum

pedestrian walk providing pedestrian circulation around the lake, lush landscaping and benches located where appropriate and back-yard access (and frontyard access in some cases) for lots that are adjacent to the lake.

Exhibit #70 - Greenbriar Lake



Greenbriar
PUD Guidelines

pedestrian paseo concepts

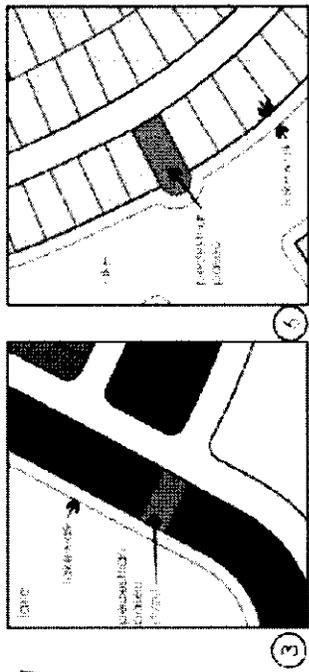
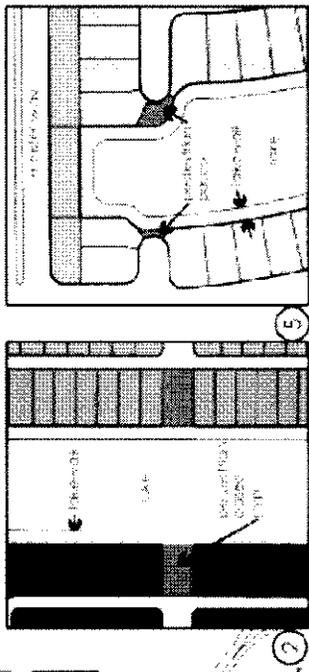
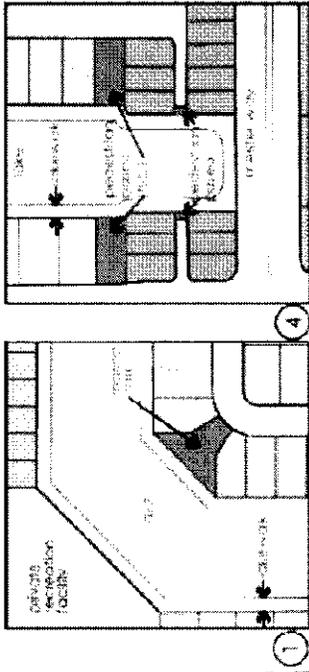
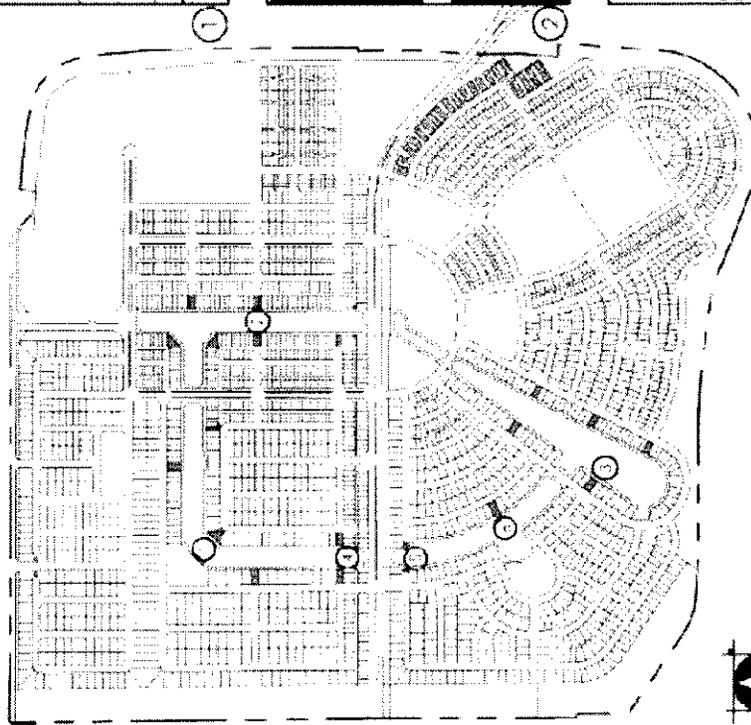


Exhibit #71 - Lake Access of Pedestrian Paseos

Greenbriar

PUD Guidelines

5.2.a Lake Access and Pedestrian Paseos

Greenbriar Lake is encompassed along its entire frontage by a 22' wide lakewalk or pedestrian boardwalk that is envisioned as a fully landscaped area with a wide 8' minimum pedestrian/bicycle trail, benches for sitting and people-watching and lots of shade trees, shrubs and groundcover.

Plant materials along the lake frontage and lake access paseos will be carefully planned to maximize visibility and safety as well as to be 'waterfowl resistant' to minimize the harborage and foraging of Sacramento's local waterfowl species. Large tree species will be chosen for their evergreen and deciduous qualities as well as for their growth habit and maintenance requirements. Deciduous shade trees will be primarily chosen to provide cooling shade during the summer months yet allow the winter sun to penetrate the lakewalk trail in the summer thereby providing a temperate climate for walking and biking enthusiasts year-round.

Pedestrian access to the promenade will be from a series of pedestrian paseos and marina parks located at regular intervals within the adjacent residential neighborhoods. These pedestrian access lots are envisioned to be well-landscaped areas with paths, benches, shade structures and similar amenities. To facilitate east/west access across the northern portion of Greenbriar Lake, pedestrian bridges are planned

that will connect, orient and unify the community. The bridges will extend across the lake terminating at the pedestrian paseos or marina parks and will be a signature architectural feature of the PUD area.

5.3 Freeway Buffer/Open Space

Consistent with development to the east within the North Natomas Community Plan, the Greenbriar PUD provides for a large open space/freeway buffer along the south and east sides of the plan area.

The open space/freeway buffer averages 100' in width and will be developed in accordance with trail guidelines developed by PPDD and will be well-landscaped with trees, shrubs and groundcovers, and possibly with turf in some areas as well. A 16' wide multi-use trail will be provided along the entire length of the open space/freeway buffer that will provide off-street recreation from the south area of the plan, up to the community park, and eventually beyond the plan area north of Elkhorn Boulevard. Future improvement plans will include a grade-separated crossing under the Meister Way freeway overcrossing for the expeditious, and uninterrupted, north-south movement of cyclists. Access controls will be placed at the entrance to all trail access points to prohibit automobile access, yet allow maintenance and emergency vehicle access.

**Greenbriar
PUD Guidelines**

5.4 Wildlife Corridor/Habitat Buffer

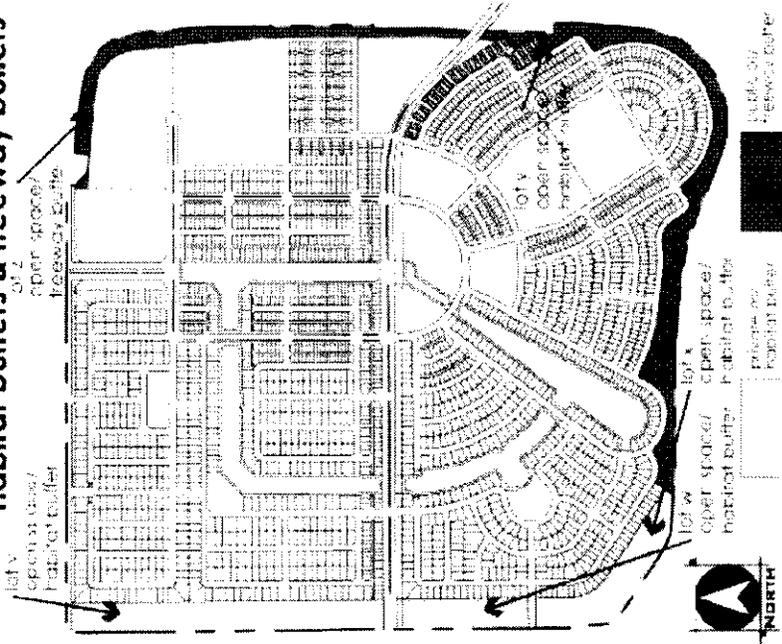
The west side of the plan area contains a drainage swale referred to as the Lone Tree Canal that is home and habitat to California Giant Garter Snake. The plan area provides for a large open space/habitat buffer along the west side of the plan for the conservation of site's federally protected species and their associated upland habitat.

The Greenbriar PUD provides for on-site preservation and habitat protection of an approximately 250-foot wide corridor that includes the canal and approximately 200 feet of adjacent uplands. The uplands within the Lone Tree Corridor will be converted to, and managed as, perennial grassland for the sole purpose of preserving the species. Additional aquatic habitat for the giant garter snake will be created along the east bank of the canal.

The developed portion of the Greenbriar PUD will be designed to minimize potential conflicts with, and obstacles for, the movement and preservation of the species. Street crossings over the canal have been minimized and the placement of a masonry wall/metal fencing barrier will be installed between the residential uses and the open space/habitat buffer to ensure that the giant garter snakes do not stray into the development area, and to prevent people and pets from entering into the preserve. This preservation is part

of the project-specific Habitat Conservation Plan (HCP) and will provide for, in perpetuity under a conservation easement, on-going north-south connectivity for the species movement and longevity.

**Exhibit #72 - Freeway Buffer/Habitat Buffer
habitat buffers & freeway buffers**



section 6 - Implementation

6.1 Phasing

Construction of off-site infrastructure, on-site infrastructure and individual village development shall be phased in accordance with the Greenbriar Development Agreement, Conditions of Approval and Finance Plan. Development of individual parcels and supporting infrastructure (i.e. roadways and utilities) may occur simultaneously or separately. The actual sequence shall be determined by economic and market conditions.

In the event Greenbriar development is constructed prior to an adjacent development, provisions for necessary infrastructure will be made in accordance with the Development Agreement. This will include provisions for construction of roadways to provide access to the development and the extension of necessary utilities and infrastructure such as water, sanitary sewer and storm drain facilities necessary to the development.

phasing exhibit

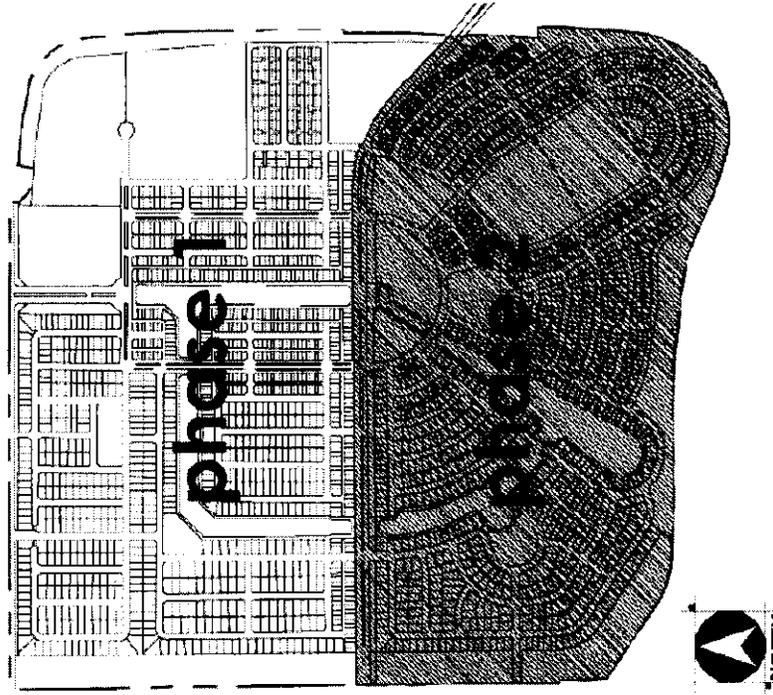


Exhibit #73 - Phasing Diagram

**Greenbriar
PUD Guidelines**

6.2 Master Developer Review Process

All development within the PUD area will be subject to Design Review prior to any required City Approvals. The purpose of the design review process is to ensure proper implementation of the Community Character Design Guidelines and consistency with the intent of the Greenbriar PUD Guidelines. The design review process will be conducted by a Greenbriar Design Review Committee that is appointed by the Developer.

6.2.a Design Review Committee

All project proposals within the Greenbriar PUD must be submitted for review and approval by the DRC prior to submittal to the City of Sacramento for approval. A five (5) member Design Review Committee (DRC) will be established and will consist of the members listed below. The DRC will be responsible to review all development plans for the Greenbriar Community to ensure consistency with these guidelines. In the early stages of community development this will be critical to establish the desired "village character" for Greenbriar.

- Three (3) at large members of the DRC will be appointed by the Developer.
- Two (2) design professionals (i.e. planners, architects, landscape architects or other related consultants) appointed by the Developer
- For the purpose of conducting design review committee meetings, the chairperson will be selected from the committee by the members.

Other non-voting individuals may also attend meetings of the DRC.

Verification of Completed Review

The Design Review Committee shall issue a letter and/or stamp of approval for the applicant to submit with the plans to the City of Sacramento for City level approvals.

6.3 Consistency Review and Planning Director Discretion

Specific applications for individual residential tracts within the project shall be processed as follows: (1) The Planning Director shall review each individual application for consistency with both this document (Greenbriar PUD Guidelines) and the Greenbriar Residential Design Guidelines document. Zoning, use and design standards shall be applied to each proposal to ensure consistency with the overall character envisioned for the PUD and for individual neighborhoods; (2) If an application fails to meet the standards established in the Greenbriar PUD Guidelines and/or the Greenbriar Residential Design Guidelines document(s), the application shall be processed at the Planning Commission level. This protocol is intended to facilitate streamlined application processing for proposals consistent with both the Greenbriar PUD Guidelines and Design Guidelines. It also permits flexibility and Planning Commission oversight for non

**Greenbriar
PUD Guidelines**

traditional proposals, and proposals determined to substantially deviate from the standards prescribed in both documents. No portion of this protocol relies upon the applicability of the City's current or future Multi-Family and/or Single-Family Design Principles documents.

ATTACHMENT 11 – ALUC OVERRIDE RESOLUTION

RESOLUTION NO.

Adopted by the Sacramento City Council

**RESOLUTION ADOPTING FINDINGS OF FACT
SUPPORTING THE CITY COUNCIL OVERRIDE OF THE
AIRPORT LAND USE COMMISSION DECISION THAT
PROVISIONS IN THE PROPOSED GREENBRIAR
PROJECT ARE INCONSISTENT WITH THE AIRPORT
COMPREHENSIVE LAND USE PLAN**

BACKGROUND

- A. The Board of Directors of the Sacramento Area Council of Governments (SACOG), sitting as the Airport Land Use Commission (ALUC) adopted the Comprehensive Land Use Plan for the Sacramento International Airport (CLUP) on May 20, 1999.
- B. The City of Sacramento received an application for development of the Greenbriar project, a 577 acre proposed development located north of Interstate 5, west of Highway 70/99, and east of Metro Air Park (Project). The City referred the Project application to ALUC for review for compatibility with the CLUP because a portion of the Project (405 acres) is within the Overflight Zone of the Sacramento International Airport. The Project proposal requests entitlements within the Overflight Zone for uses that include residential, commercial, mixed use, park and open space with water bodies, and a light-rail transit station.
- C. On December 7, 2005, ALUC staff provided its written review of the Project to the City of Sacramento's Planning Department. Of the three policy components of ALUC review; safety, noise, and height, ALUC's staff reviewed the Project for consistency with the CLUP safety policy only because the Project does not implicate the other components of review.
- D. ALUC found the residential and commercial uses to be compatible with the CLUP based upon the densities proposed for the Project.
- E. ALUC found the parks and open spaces within the Project to be compatible with the CLUP so long as such areas do not contain facilities that lead to high concentrations of people (an average density of 25 people per acre over a 24 hour period, and not to exceed 50 persons per acre at any time), such as ball fields and playgrounds.
- F. ALUC deferred to the Sacramento County Airport System (SCAS) and the Federal Aviation Administration (FAA) regarding whether the water bodies proposed for the Project would attract wildlife and create potential conflicts with aircraft (birdstrikes), and determined that the Project will either be considered (1)

compatible with the CLUP if the SCAS and FAA do not object to the proposed water features, or (2) incompatible if either of these two agencies object to the water features.

- G. ALUC found the Project's light rail station within the Overflight Zone inconsistent with the CLUP, which prohibits passenger terminals and stations within the Overflight Zone.
- H. Section 21676 subdivision (b) of the Public Utilities Code and Section 65302.3 of the Government Code provide that the City Council may, after a hearing and with a two-thirds vote, overrule the ALUC. The City Council must make specific findings that the disputed portion of the proposed Greenbriar Project is consistent with the public interest purposes stated in Public Utilities Code Section 21670.
- I. The City has considered long-range airport development plans. The City plans to support development of the airport over the next 20 years. The local land use planning and zoning actions will serve to protect runway approaches. The City's General Plan Circulation Element includes the following goal relevant to airport protection: "Goal A: Promote general, commercial and military aviation facilities within the parameters of compatible surrounding land uses. Aviation is an important segment of Sacramento's economic vitality. In order to function as they need to, each of the four separate airport facilities desires compatible land uses within certain radii of their runways and ground operations and within certain noise contour levels. The City recognizes these important factors in land use decision making."
- J. On _____, the City Council held a public hearing on [land use entitlements] The Council reviewed a proposed decision with findings to override the ALUC determination of Project inconsistencies with the CLUP, and authorized City staff to provide the proposed decision to override the ALUC.
- K. On _____, after consideration of issues regarding the light rail station, parks and open space, and water features proposed within the Overflight Zone as part of the Project, the EIR for the Project, testimony, and information provided at the hearing, the City Council approved the override and adopted this resolution.

BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:

- Section 1: As more specifically found below, the Project is consistent with the purposes of Section 21670 of the Public Utilities Code in that it prevents the creation of new safety problems and protects public health, safety, and welfare by ensuring the orderly expansion of the airport and includes the adoption of land use measures that minimize the public's exposure to excessive noise and safety hazards within areas around the public airport to the extent that these areas are not already devoted to incompatible uses.

- Section 2: The Project's proposal to develop parks and open space within the Overflight Zone is consistent with the purposes of the Airport Land Use Commission Law. As determined by ALUC, the proposed residential development is compatible with the CLUP based upon the densities proposed for the Project. The parks and open space will be developed to accommodate those residents, but will not be designed to attract people from outside the Greenbriar project area. Therefore, the proposed parks and open space are considered by the City Council to be compatible with the CLUP based upon the understanding that any difference in potential safety hazard associated with occupation of a residence or park site is negligible.
- Section 3: The Project proposal to develop water features within the Overflight Zone is consistent with the purposes of the Airport Land Use Commission Law. The open water on the Greenbriar site will not interfere with approaches to the airport runways and the Project employs safety measures or mitigation measures to protect the runways and prevent risks posed by hazardous wildlife. Neither SCAS nor FAA have objected to the proposed water features, and in fact the SCAS has provided written support. (See Final EIR, pages 4-238 to 4-239.)

Wildlife, specifically birds, have flown in and around the proposed Project area for decades, including between the Sacramento River and the Project site; there are no new threats to aircraft safety related to hazardous wildlife in the area that are associated with the proposed Project.

- Section 4: The Project's proposal to develop a light rail station within the Overflight Zone is consistent with the purposes of the Airport Land Use Commission Law. A direct connection exists between the juxtaposition of appropriate land uses and successful public transit service such as that identified for the planned Downtown-Natomas-Airport (DNA) line. Sacramento Regional Transit advises that 50 percent of the project justification rating for all federal transit funding for rail projects is based on land use criteria. Land use decisions made in the Sacramento region, particularly along the planned high capacity transit corridors and specifically within ¼ mile of planned rail and/or bus rapid transit stations, are not only critical to maximize ridership, but have also become critical to the Federal Transit Administration's ultimate decisions about these projects.

The Project proposes to develop high-density residential units within a ¼-mile radius of a future Greenbriar transit station. This high-density housing near a proposed transit station is critical in RT's quest, which the City supports, to receive funding from the FTA major capital investment programs, which funds all of the regional rail expansion programs.

The planned DNA line connects the Downtown and Natomas areas to the Sacramento International Airport. For a light rail line to serve effectively the airport, the rail line and at least one transit station will need to be

located near the airport, meaning that at least some of the transit station facilities will likely need to be constructed within the Overflight Zone.

Section 5: The decision of the Airport Land Use Commission is overruled insofar as it restricts the City's discretionary authority for approval of the Project. In overruling ALUC's decision, the City Council specifically finds that this action is in the public interest of the citizens of the City of Sacramento, and that this action promotes the protection of the public health, safety and welfare by ensuring the adoption of land use measures that minimize the public's exposure to excessive safety hazards within areas around public airports to the extent that such areas are not already devoted to incompatible uses.

Exhibit A

ALUC Letter of Consistency Determination

Coordination by Board
Council of
Agovernments

1435 L Street,
Suite 309
Sacramento, CA
95814

tel: 916.321.9000
fax: 916.321.9551
toll: 916.321.9550
www.sacog.org



December 7, 2005

Ms. Arwen Wacht
City of Sacramento Planning Department
915 L Street, 3rd Floor
Sacramento, CA 95814

Ms. Wacht:

On behalf of the Airport Land Use Commission (ALUC), I am submitting to you a letter, which serves as the ALUC's staff review of the Greenbriar Farms development application (City of Sacramento file P04-069, ALUC file 05-20). This letter is divided into four parts: (1) legal authority of the ALUC to review the Greenbriar Farms Development; (2) a summary of the proposed development application; (3) the ALUC's review; and (4) conclusions and next steps of the review.

- Albany*
- Colusa*
- Davis*
- El Dorado County*
- El Grove*
- Folsom*
- Galt*
- Grass Valley*
- Grayslake*
- Placer County*
- Maricopa*
- Beckley*
- Roseville*
- Sacramento*
- Sacramento County*
- Sutter County*
- West Sacramento*
- Woodland*
- Yuba*
- Yuba City*
- Yuba County*

I. ALUC Review Authority

The Sacramento Area Council of Governments (SACOG) serves as the Airport Land Use Commission for the counties of Sacramento, Sutter, Yuba and Yuba as described in the provisions of the California Public Utilities Code, Chapter 4, Article 3.5, Section 21670. In essence, ALUCs throughout the state are required to enforce the land use compatibility with publicly owned and operated airports. To do this, cities and counties within an ALUC's jurisdiction send development applications to the ALUC for review. The ALUC reviews the proposal based on adopted Comprehensive Land Use Plan (CLUP) for any airport affected by the development.

The ALUC reviews development applications for compatibility between proposed development and publicly used, owned or operated airports. Because of its location relative to Sacramento International Airport, the Greenbriar Farms application is subject to ALUC review based on the Sacramento International Airport Comprehensive Land Use Plan. The CLUP has three policy areas that each development application must pass: (1) height; (2) noise; and (3) safety. For the height policy, proposed users are evaluated based on the relationship of the height of proposed structures relative to their location to the airport. For noise, a determination is made whether the proposed land use is compatible with the noise impacts of the flight operations. For safety, the proposed land use must restrict high concentrations of people in potential flight safety hazard areas. The CLUP describes a methodology and/or list of land uses that are allowed within each policy area.

II. Proposed Greenbriar Farms Development

The Greenbriar project is located north of I-5, west of Highway 99, and east of Metro Air Park, as shown in Attachment #1. The overall size of the property is 577 acres. As will be explained later, the ALUC only has review authority on the land within the Overflight Zone, which is 405 acres. The proposal requests entitlements within the Overflight Zone for uses that include residential (various densities), commercial, mixed use, park and open space with water bodies, and a light-rail transit station. Specifically, the land uses proposed within this safety policy area are:

- 1932 units of single family residential
- 583 units of multi-family residential, of which 283 units are duplex, tri-plex or four-plex
- 102,300 square feet of commercial in larger commercial area (approximately 50,000 square feet for a grocery market, 22,300 square feet for a restaurant, and 30,000 square feet for retail shops)
- 54,000 square feet of commercial/office in smaller commercial area (14,000 square feet for a restaurant, 25,000 square feet for retail, and 15,000 square feet for office)
- 23 acres of park
- 40 acres of water bodies
- a light rail station that will serve the future Downtown-Natomas-Airport line
- 10 acres (approximately) for one public elementary school (located outside of Overflight Zone).

III. ALUC Review

Only one of the CLUP's three policy areas (height, noise and safety) may be applied to the review of the Greenbriar application: safety. Height is not applicable because there are no proposed structures that are close to penetrating any of the imaginary surfaces as set forth by the Federal Aviation Administration in Federal Aviation Regulation Part 77. Nor is this application subject to the CLUP's noise policies because the project site lies outside of the 60 Community Noise Equivalent Level (CNEL), which serves as the demarcation line for restricted development.

However, about 70% of the property (the western 405 acres of the 577 total acres), lies inside of the Overflight Zone of the CLUP, and therefore that portion of the property is subject to the CLUP's safety policies. The eastern quarter of the property is outside of the Overflight Zone boundary and, therefore, the ALUC has no standing in the review of that portion's application. This letter only applies to the land inside the Overflight Zone. Attachment #1 shows where that boundary lies. SACOG has verified that the boundary of the electronic files that the applicant used is accurate enough for review purposes.

Here is the review for each of the proposed land uses:

Residential Uses: The CLUP allows residential development (single family or multi-family) in the Overflight Zone with one condition: "uses compatible only if they do not result in a large concentration of people, which is defined as an

average density of greater than 25 persons per acre per hour during any 24 hour period, and not to exceed 50 persons per acre at any time for all land use types.” The calculations and findings for total persons allowed per hour and maximum at any time are addressed in more detail in a separate section below.

Commercial/Office Uses: The CLUP allows for office and most retail uses subject to the same density conditions stated above. Allowed uses with this condition include restaurants, grocery and drug stores, apparel and miscellaneous retail, and gas stations. The applicant has not stated the exact retail uses of the proposed retail or office areas, but they would all be subject to the same maximum density calculation.

However, the CLUP outright prohibits “regional shopping centers.” SACOG defines a regional shopping center as a retail area that draws residents from throughout the six-county region to shop because these products and retailers are not available at more localized scales. Most regional shopping centers contain at least 500,000 square feet. ALUC staff has determined that the footprint sizes of the both commercial areas are too small to warrant them as “regional shopping centers.”

Parks and Open Space: The CLUP allows for neighborhood parks and open space and natural areas if they do not exceed the maximum densities stated earlier and do not include “high intensity uses or facilities, such as structured playgrounds, ballfields or picnic pavilions”. The applicant does not specifically state whether the proposed parks will include these facilities, but they are not allowed.

Community and regional-wide parks are outright prohibited in the CLUP in the Overflight Area. The city of Sacramento’s park standards define community parks as 10 to 60 acres in size, and regional parks are significantly larger. Neighborhood parks are defined from 5 to 10 acres. The applicant proposes a series of parks all smaller than 10 acres in size, thus all would be considered neighborhood parks and, therefore, allowed with the condition stated above.

Water Body: The CLUP allows for natural water bodies if they meet three conditions: they do not exceed the maximum density threshold, they do not contain high intensity facilities, nor do they “result in the possibility that a water area may cause ground fog or result in a bird hazard.” The CLUP does not mention “man-made” or constructed water bodies, as in the case of this proposal, so the ALUC will treat this element of the application the same as “natural” water body. The ALUC will defer its evaluation of the bird hazard and ground fog to affected public agencies that are also reviewing this application: the Sacramento County Airport System and the Federal Aviation Administration. If both of these entities do not object to any bird or fog hazards, the ALUC will accept the proposed water bodies.

Light-Rail Station: The proposal calls for the light rail station within the Overflight Zone. The CLUP prohibits passenger terminals and stations within the Overflight Zone.

Public Elementary School: The elementary school proposed within the development is outside of the Overflight Zone, therefore it is not subject to the ALUC's review. However, because its proposed location is within 2 miles of an airport runway, state law (California Education Code 17215) requires the California Department of Transportation Division of Aeronautics to review and approve the school's location. The applicant has been given the appropriate contact information to follow-up to seek review by Caltrans; the ALUC will not track the approval process of this public facility.

Calculation and Findings of Average and Maximum Densities

As stated above, the CLUP's safety policy allows for commercial, office and residential uses in the Overflight Zone if, collectively between these uses, they do not result in a large concentration of people, which is defined as (criterion #1) an average density of greater than 25 persons per acre per hour during any 24 hour period, and (criterion #2) not to exceed 50 persons per acre at any time for all land use types.

The CLUP does not prescribe the methodology for determining whether a maximum density has been exceeded or not. However, the applicant has worked extensively with ALUC staff to establish such a methodology. In essence, the evaluation method agreed upon by both parties includes:

- for residential uses, number of residential units multiplied by the city of Sacramento's residents per household. For single-family detached residential development, 2.86 person per unit are used. For multi-family residential, 1.9 persons per unit are used, and for duplex, tri- and four-plexes, 2.26 persons per unit. These statistics are similar to those used in SACOG's Blueprint Project.
- for commercial and office uses, the Occupancy Loads of the California Building Code (2001, which is the current edition) determine the maximum persons per square foot, and therefore the maximum occupancies may be determined.

Attachment #2 provides a breakdown of the calculations used to determine estimated densities at any given time. The applicant and the ALUC collaborated on the development of this spreadsheet.

Criterion #1: The maximum "average" number of persons allowed within Overflight Zone during an "average hour" is:

- 25 persons per acre max. x 405 acres = 10,125 people

According to the calculations in Attachment A, the average amount is estimated to be about 8,000 people, which is an average density of about 20 persons per acre, and therefore this safety policy Criterion #1 is met.

Criterion #2: The maximum number of persons allowed within the Overflight Zone within any given time is:

- 50 persons per acre max. x 405 acres = 20,250 people

The calculations in Attachments A of approximately 11,300 people is far below the maximum threshold, and therefore safety policy Criterion #2 has been satisfactorily met.

Note: These findings for the maximum persons allowed were completed in the spirit in which the current CLUP for Sacramento International was written in 1994. This proposal meets both criteria using that document. Please note the current version of the California Airport Land Use Planning Handbook (2002) does not recommend concentrations of people within sub-areas of the greater development area. The Handbook provides the State of California's guidance to ALUC's throughout the state on standards. The Greenbriar proposal will have high concentrations of people above 25 person acre on an average hourly basis and above 50 persons per acre at times. The most notable place is surrounding the proposed light rail station (which is outright prohibited in the CLUP). In the spirit in which the current CLUP was written in 1994, the ALUC will consider this proposal compatible with the two density criteria.

IV. Summary of ALUC Review and Override Procedures

The summary finding of the ALUC's review are:

Finding #1: The residential and commercial uses are compatible with the CLUP based on the densities proposed.

Finding #2: Parks and open spaces are compatible with the CLUP as long as they do not contain facilities that lead to high concentrations of people, such as ballfields and playgrounds. If such facilities are desired, the applicant will need to obtain an override from the Sacramento City Council through the process described below.

Finding #3: The ALUC will defer to the Sacramento County Airport System and the Federal Aviation Administration regarding the water bodies proposed. If these two entities do not object to the proposed water features or the proposed wildlife and fog management techniques, the ALUC will consider this compatible with the CLUP. However, if either of these entities have objections, the ALUC will also have the same objections.

Finding #4: The proposed light rail station is NOT compatible with the CLUP. The applicant will need to seek an override from the Sacramento City Council as set forth in procedures described below for this.

Finding #5: Before a new public school is allowed within the 2 miles radius of Sacramento International Airport, the California Department of Transportation Aviation Division must provide approval. This is outside the jurisdiction of the ALUC.

Override Process

If the Sacramento City Council does not agree with ALUC Findings #2 or #4, the city may satisfy the consistency requirement by overriding either or both by a two-thirds vote. The overruling must, however, be made after a public hearing and must be based on specific findings that the proposed action is consistent with the purposes of the Airport Land Use Commission Law (California Public Utilities Code 21670).

If you have any questions about the contents of this letter, please let me know.

Sincerely,

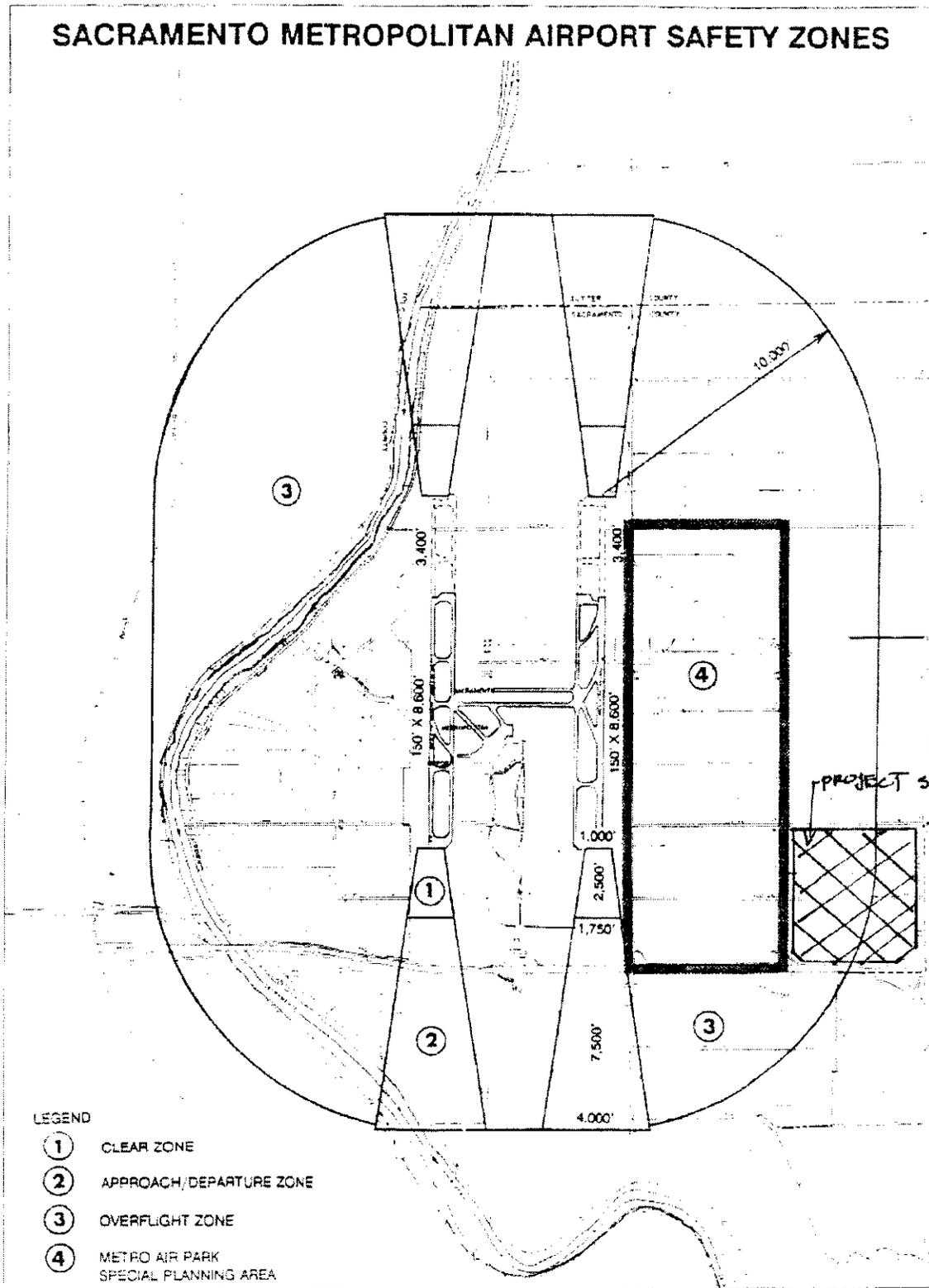
A handwritten signature in black ink, appearing to read 'Gregory Chew', written in a cursive style.

Gregory Chew, AICP
Sacramento Area Council of Governments/Airport Land Use Commission
(916) 340-6227

attachments

FIGURE 9

ATTACHMENT #1



Attachment 2: ALLUC Maximum Density Calculations for Greenbriar Farms Proposal
(December 2, 2005)

Greenbriar Safety Zone Densities - Persons per Hour			
2.5 Persons Per Acre Per Average Hour	405		
Gross acreage within safety zone	25		
ALLUC persons per acre per "average hour"	10125		
CLUP's Total allowable persons within safety zone (for average)			
RESIDENTIAL			
Assumptions:			
Persons per household - single family detached	2.86		
Persons per household - two-to-threeplex	2.26		
Persons per household - 5-plex (apartments)	1.9		
Calculations:			
Single family households - total # of persons	5626		
Duplex/tri or four-plex - total # of persons	340		
Multi-family households - total # of persons	570		
Total Maximum Persons Occupying Residential Units Occupied an average of 75% of the day	6096		
	4572		
COMMERCIAL			
Large Commercial Area			
Use	Area	Factor	Occup. Units
Market	50,000	45,000	1,500
Restaurant	22,300	9,000	300
Shops	30,000	17,840	15
Total Max Occupancy Load		4,460	200
		25,500	30
		4,500	300
			15
			3,000
Master Way Commercial Restaurant	14,000	11,200	15
Shops	25,000	2,800	200
Office	15,000	21,250	30
Total Max Occupancy Load		3,750	300
Total Maximum Persons Occupying Commercial Units Occupied for 48 hours per day		15,000	100
			1,632
			5225
			3501
PARKS**			
TOTAL PERSONS PER AVG HOUR WITHIN SAFETY ZONE	8072		
TOTAL ALLOWABLE PERSONS WITHIN SAFETY ZONE	10125		
TOTAL PERSONS PER ACRE PER AVG HOUR WITHIN SAFETY ZONE	20		
TOTAL ALLOWABLE PERSONS PER ACRE PER AVG HOUR WITHIN SAFETY ZONE	25		

Explanation:
per CLUP, page 36 and Appendix A-1

Source: City of Sacramento Planning Dept
Source: City of Sacramento Planning Dept
Source: City of Sacramento Planning Dept

1932 total detached units x 2.86
283 total duplex, tri or four-plex units x 2.26
300 total multi-family units x 1.9

For purposes of "averaging" - assume people at home 18 hours per day, on average

*Factor is population factor - persons per square foot maximums as set forth in Table 10-A of the 2001 California Building Code (2001 is current edition). Occupancy Load numbers are "maximum" persons per square foot derived from CA Building Code. Not shown on "Conceptual Plan," but included here to be conservative and provide flexibility later.

Only those commercial uses that fall within the safety line were analyzed in this section.

**Conceptual Plan shows 14,700 sq. ft. more analyzed here to be conservative and provide flexibility later.

For purposes of "averaging" - assume commercial units empty for 8 hours per day.

Population totals attributable to parks have no noticeable impact on overall density evaluation, due to the siting of "Regional" boxes outside of the safety line. See profile below.

Calculated figures below maximum allowed; this proposal exceeds this category.

50 Persons Per Acre Maximum Calculations

		<u>EXPLANATION</u>
Gross acreage within safety zone	40h	
Allowable persons	50	
Total maximum allowable persons within safety zone at any given time	20250	
RESIDENTIAL		
Total Maximum Persons Occupying Residential Units	6189h	per analysis above
COMMERCIAL		
Total Maximum Persons Occupying Commercial Units	5229	per analysis above
TOTAL MAXIMUM PERSONS WITHIN SAFETY ZONE AT ANY GIVEN TIME	11320	Calculated figure is below maximum allowed; this proposal passes this margin
TOTAL ALLOWABLE PERSONS WITHIN SAFETY ZONE	20250	

ATTACHMENT 12 -VICINITY MAP

