

RESOLUTION NO. 2008-059

Adopted by the Sacramento City Council

January 29, 2008

ESTABLISHING THE GREENBRIAR PLANNED UNIT DEVELOPMENT GUIDELINES AND SCHEMATIC PLAN, LOCATED SOUTHWEST OF THE INTERSECTION OF ELKHORN BOULEVARD AND HIGHWAY 99, IN SACRAMENTO, CALIFORNIA (M05-046 / P05-069)

BACKGROUND

- A. The Planning Commission conducted public hearings on October 11, 2007 and November 8, 2007, and the City Council conducted public hearings on January 22, 2008 and January 29, 2008 concerning the above plan amendment and based on documentary and oral evidence submitted at the public hearing, the Council hereby finds:
1. The PUD establishment conforms to the General Plan; and
 2. The PUD establishment meets the purposes and criteria stated in the City Zoning Ordinance in that the PUD assures that new development is healthy and of long-lasting benefit to the community and the City; and
 3. The PUD establishment will not be injurious to the public welfare, nor to other property in the vicinity of the development and will be in harmony with the general purposes and intent of the Zoning Ordinance in that the PUD ensures that development will be well-designed, and that the residential, open space, and commercial uses will not create a negative impact on adjacent uses.

BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:

- Section 1 The City Council of the City of Sacramento, in accordance with the City Code, Chapter 17.180, approves the Greenbriar Planned Unit Development Guidelines and Schematic Plan (as shown on the attached Exhibits A and B).

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Exhibit A: Greenbriar PUD Schematic Plan Exhibit – 1 Page

Exhibit B: Greenbriar PUD Guidelines Exhibit - 86 Pages

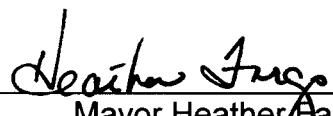
Adopted by the City of Sacramento City Council on January 29, 2008 by the following vote:

Ayes: Councilmembers Cohn, Fong, Hammond, McCarty, Pannell, Sheedy, Tretheway, Waters, and Mayor Fargo.

Noes: None.

Abstain: None.

Absent: None.



Mayor Heather Fargo

Attest:


Shirley Concolino, City Clerk

Exhibit A: Greenbriar PUD Schematic Plan Exhibit

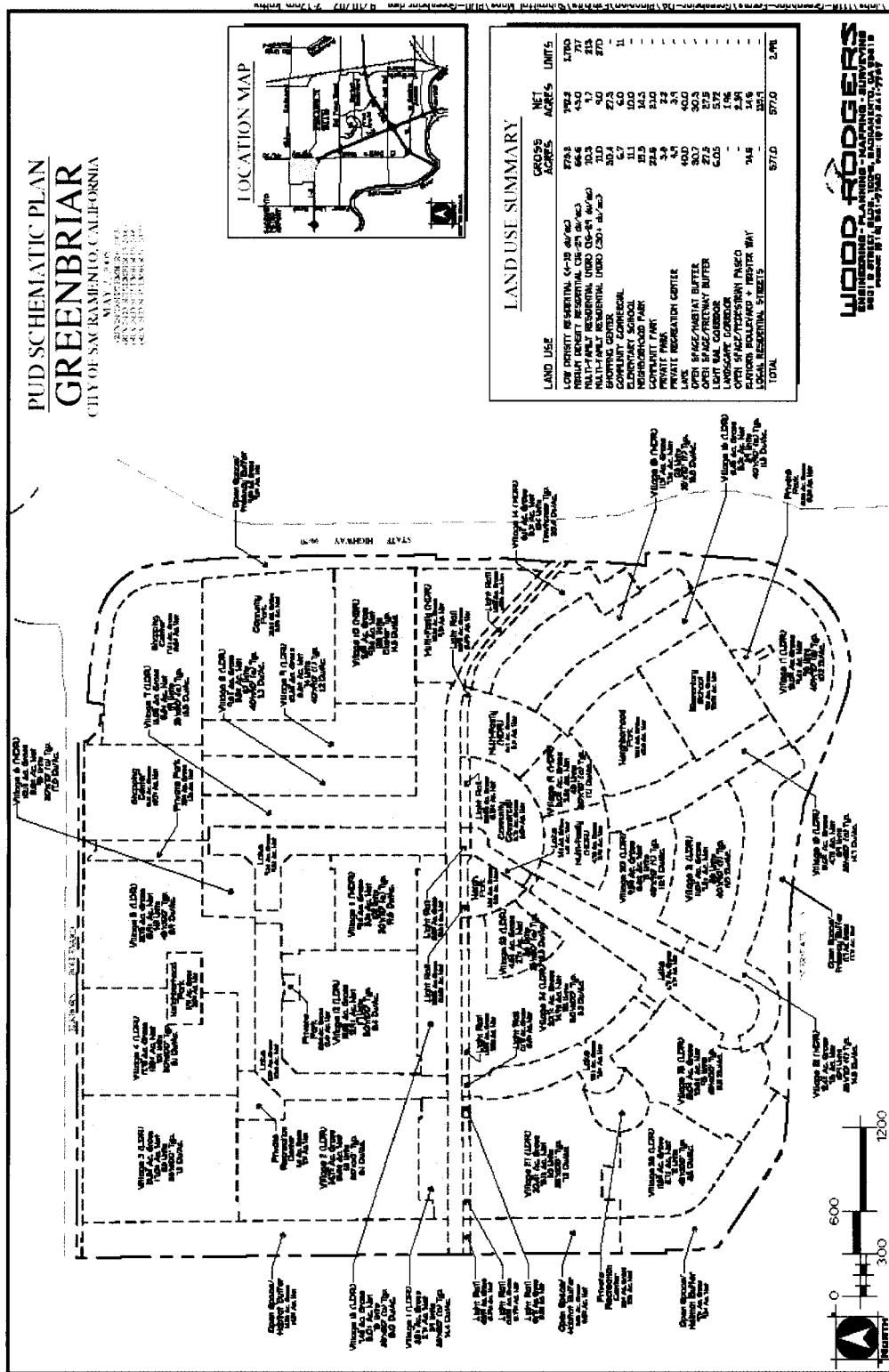


Exhibit B: Greenbriar PUD Guidelines Exhibit

GREENBRIAR

planned unit development (pud) guidelines

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Santa Ana, CA, 92705

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Section 1 – Overview

1.1 Project Context, Goals and Objectives

The Greenbriar Planned Unit Development (PUD) is located at the northwest intersection of Interstate 5 and Highway 99/Wormedly west and north of the North Valleys Community Plan area in the City of Sacramento. The PUD is bordered by Elkhorn Boulevard to the north, the Metro Air Park employment center to the west, Interstate 5 to the south, and State Highway 99/70 to the east. The approximately 577 acre Greenbriar project is a planned community comprised of single- and multi-family residential neighborhoods, parks, open space, natural habitat, an elementary school, light rail, and neighborhood-serving commercial areas. The Greenbriar PUD strives to achieve two primary goals. Each of these goals will be pursued using specific design objectives which are listed below.

- To capitalize on the setting and location of the site for the benefit of future residents:**
- Utilize parks, lakes and circulation design to enhance the unique character of each neighborhood.
 - Develop a public transportation system that encourages light rail ridership by centrally

locating the Transit Station adjacent to higher density residential neighborhoods and providing a high degree of walkability within the PUD.

To unify the neighborhoods visually and functionally:

- Utilize a consistent set of design standards and details throughout the Greenbriar PUD to develop a sense of place for the neighborhoods.
 - Create a fluid circulation system which interlinks areas within the neighborhoods to one another.
 - Integrate the PUD through the design and location of a convenient and functional light rail system.
- With Greenbriar's proximity to the existing City of Sacramento job base and the neighboring Metro Air Park employment center, The Sacramento International Airport, Interstate 5 and Highway 99/W90, as well as the site's location of the periphery of the North Valleys Community Plan, the PUD is strongly positioned to be a focal community within the City of Sacramento. In order to accomplish this type of new development, development guidelines and standards must be carefully crafted to allow for flexible retail development, compact design, unique street scenes and unified design among the varied and diverse housing types. It is for these reasons that this PUD Guidelines has been created and why this community will be a unique and model community in the greater Sacramento region.

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local context map



Exhibit #1 - Local Context Map

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The purpose of these PUD Guidelines is to guide future development within the Greenbriar PUD area. This document is organized into seven (7) Sections as follows:

- The **PUD Overview Section** of the PUD Guidelines provides the local context for the proposed project. The Community Plan Principles and established Goals, Policies and Objectives for the plan Area. The Overview Section provides a detailed summary of the planned development Zoning and Land Use designators.
- The **Transit Orientation and Mobility Section** addresses the Transit-Oriented Development (TOID) characteristics and the planned extension of regional transit, non-vehicular mobility, and vehicle circulation.
- The **Housing Section** of the PUD Guidelines discusses the Housing Alternatives including Market rate housing, Affordable housing and senior housing. This section sets forth housing Development Standards to ensure consistency in the quality and character of the residential environment.
- The **Commercial Uses Section** discusses the Design and Function of commercial uses, including the Village and Community Commercial sites. This section sets forth Commercial Development Standards to ensure consistency in the quality and character of the commercial environment within Greenbriar.

- The Parks, Open Space and Recreation Section discusses the design, uses and access/relationship of the Parks, Open Space Greenbelts, Lake, and the Natural and Open Space buffers.
- The **Implementation Section** outlines the processes that will implement the orderly development of the PUD area.
- The **Design Guidelines Section** which extrusses the site-specific design of the single-family residential, multifamily residential, commercial and retail village areas.

1.2 Future Submittals

These Greenbriar Planned Unit Development (PUD) Guidelines identify the standards necessary to implement successful and sustainable development. This document presents standards to guide the existing uses and general development of the PUD, and establishes an appropriate context for the application of more specific local level design standards. Future applications will be gauged according to standards prescribed by detailed Design Guidelines and Planning Director or Planning Commission oversight as necessary.

This PUD Guidelines document acknowledges that the City Planning Commission and City Council shall review and the City Council shall consider for adoption, a future chapter of these design guidelines (Section 7 Design Guidelines) which is intended to prescribe

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detailed and comprehensive design standards addressing single-family and multi-family residential design, commercial design, and transit village design. In addition to these subjects, the Design Guidelines may include other optional topics as needed such as walls, signage and landscape, or public facility and park and open space design to describe and convey the overall design concept and implementation of the PUD's vision.

The filing for compilation and subsequent approval of the "blue" section 7 Design Guidelines is intended to be coordinated with the City's review and consideration of the Greenbriar Master Plan Tentative Subdivision Map and Development Agreement.

The Greenbriar PUD is subject to the City's Schematic PUD Development Application process in place of the line of subsequent applications and approvals under or as specified in Section 6.3 of these Guidelines. Please see Section 6.3 [Consistency Review and Planning Director Directive] for more detail.

1.3 PUD Schematic Plan

The Greenbriar PUD Schematic Plan is consistent with the policies for development as set forth in the City's General Plan. The PUD Schematic Plan locates a neighborhood-serving Shopping Center along Bithorn Boulevard and Greerador Boulevard (at the main project entry), a Community Commercial site co-

located with the planned RT Transit Station, located centrally in the heart of the neighborhood along Master Way, and provides varied housing opportunities in close proximity to the Transit Station, shopping and daily services, employment opportunities, the elementary school and various park and recreation facilities.

The PUD Schematic Plan is comprised of eleven different single-family lot size/product types to accommodate various income levels and lifestyle choices within the plan area. Additionally, the PUD provides the opportunity for housing within the Community Commercial site and high-density attached housing, including one senior housing site, in close proximity to transit opportunities.

The Greenbriar PUD schematic plan provides many varied recreation opportunities in the location of both publicly and privately owned active and passive parks and open spaces. One large Community Park, three Neighborhood Parks, three private parks, two private recreation centers, numerous pocket parks, pedestrian paths, and running paths, ranging in size and on elementary school are located within the PUD. These amenities are well-linked by residential streets and trails. Additionally, the PUD boasts an expansive lake with a pedestrian-friendly lakeside or promenade around the entire perimeter of the lake, and a wide "new way" buffered/open space corridor (along the south and east

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project boundaries enhanced with restoration tools
and landscaping.

The PUD Schenck Plan also includes a wide Wildlife Corridor/delta buffer adjacent to the western project boundary that will be privately maintained in perpetuity for the preservation of threatened and/or endangered species. Due to the possible presence of rare or threatened species, physical access to the habitat buffer will be restricted and an on-going maintenance plan will be established.

The following pages illustrate the Greenbriar PUD Schenck Plan and the summary in detail.

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PUD schematic plan

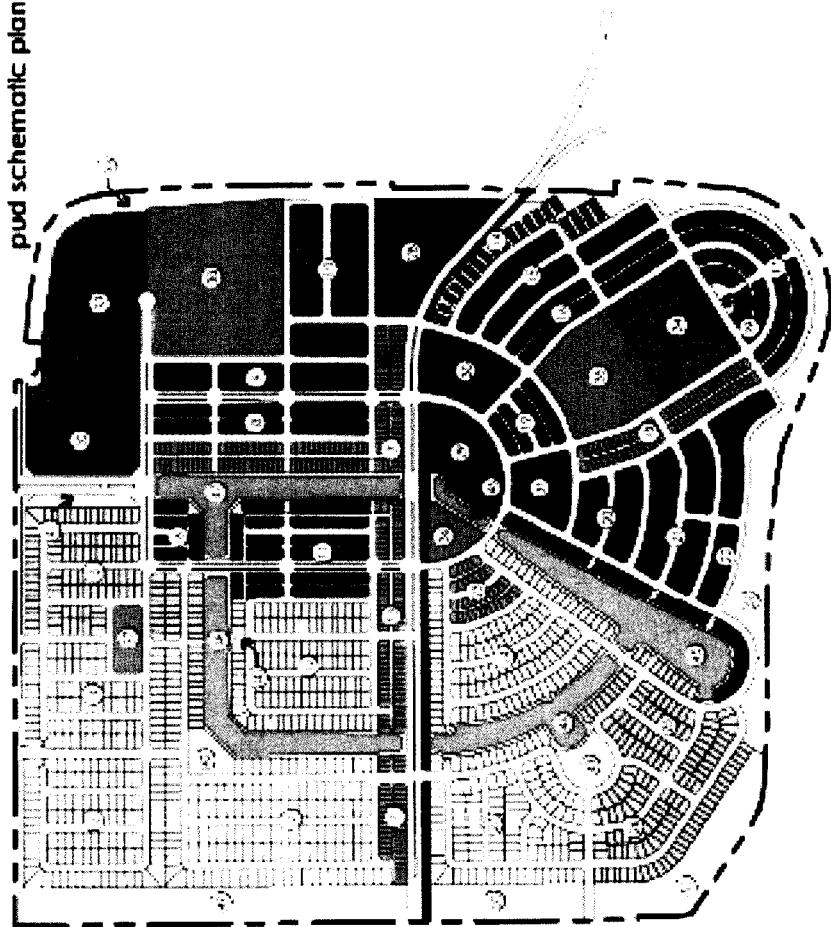


Exhibit #2 – PUD Schematic Plan

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land use summary table

location/lot no.	general use	use	gross lot	gross lot	gross lot
V100-2	.18*	low density residential	160' x 110'	24.7	18.6
V100-3 & 27	.18*	low density residential	155' x 100'	3.3	3.3
V100-4 & 24	.18*	low density residential	150' x 100'	3.3	3.3
V100-5 & 6, 28	.18*	low density residential	145' x 100'	3.3	3.3
V100-6 & 21	.18*	low density residential	140' x 100'	3.3	3.3
V100-8 & 7	.18*	low density residential	140' x 100'	3.3	3.3
V100-17 & 18 & 23	.18*	low density residential	125' x 80' A1	3.8	3.8
V100-15 & 22	.18*	medium density residential (35,000 sf)	125' x 80' A1	3.8	3.8
V100-6 & 10 & 9	.18*	medium density residential (30,000 sf)	125' x 80' A1	3.8	3.8
V100-1	.18*	medium density residential (20,000 sf)	125' x 80' A1	3.8	3.8
V100-4	.18*	medium density residential (15,000 sf)	125' x 80' A1	3.8	3.8
SAI-11	.18*	medium density residential (10,000 sf)	125' x 80' A1	3.8	3.8
SAI-12	.18*	medium density residential (5,000 sf)	125' x 80' A1	3.8	3.8
SAI-13	.18*	medium density residential (2,000 sf)	125' x 80' A1	3.8	3.8
SAI-14	.18*	medium density residential (1,000 sf)	125' x 80' A1	3.8	3.8
SAI-15	.18*	medium density residential (500 sf)	125' x 80' A1	3.8	3.8
SAI-16	.18*	medium density residential (250 sf)	125' x 80' A1	3.8	3.8
SAI-17	.18*	medium density residential (100 sf)	125' x 80' A1	3.8	3.8
SAI-18	.18*	medium density residential (50 sf)	125' x 80' A1	3.8	3.8
SAI-19	.18*	medium density residential (25 sf)	125' x 80' A1	3.8	3.8
SAI-20	.18*	medium density residential (10 sf)	125' x 80' A1	3.8	3.8
SAI-21	.18*	medium density residential (5 sf)	125' x 80' A1	3.8	3.8
SAI-22	.18*	medium density residential (2 sf)	125' x 80' A1	3.8	3.8
SAI-23	.18*	medium density residential (1 sf)	125' x 80' A1	3.8	3.8
SAI-24	.18*	medium density residential (0.5 sf)	125' x 80' A1	3.8	3.8
SAI-25	.18*	medium density residential (0.25 sf)	125' x 80' A1	3.8	3.8
SAI-26	.18*	medium density residential (0.125 sf)	125' x 80' A1	3.8	3.8
SAI-27	.18*	medium density residential (0.0625 sf)	125' x 80' A1	3.8	3.8
SAI-28	.18*	medium density residential (0.03125 sf)	125' x 80' A1	3.8	3.8
SAI-29	.18*	medium density residential (0.015625 sf)	125' x 80' A1	3.8	3.8
SAI-30	.18*	medium density residential (0.0078125 sf)	125' x 80' A1	3.8	3.8
SAI-31	.18*	medium density residential (0.00390625 sf)	125' x 80' A1	3.8	3.8
SAI-32	.18*	medium density residential (0.001953125 sf)	125' x 80' A1	3.8	3.8
SAI-33	.18*	medium density residential (0.0009765625 sf)	125' x 80' A1	3.8	3.8
SAI-34	.18*	medium density residential (0.00048828125 sf)	125' x 80' A1	3.8	3.8
SAI-35	.18*	medium density residential (0.000244140625 sf)	125' x 80' A1	3.8	3.8
SAI-36	.18*	medium density residential (0.0001220703125 sf)	125' x 80' A1	3.8	3.8
SAI-37	.18*	medium density residential (0.00006103515625 sf)	125' x 80' A1	3.8	3.8
SAI-38	.18*	medium density residential (0.000030517578125 sf)	125' x 80' A1	3.8	3.8
SAI-39	.18*	medium density residential (0.0000152587890625 sf)	125' x 80' A1	3.8	3.8
SAI-40	.18*	medium density residential (0.00000762939453125 sf)	125' x 80' A1	3.8	3.8
SAI-41	.18*	medium density residential (0.000003814697265625 sf)	125' x 80' A1	3.8	3.8
SAI-42	.18*	medium density residential (0.0000019073486328125 sf)	125' x 80' A1	3.8	3.8
SAI-43	.18*	medium density residential (0.00000095367431640625 sf)	125' x 80' A1	3.8	3.8
SAI-44	.18*	medium density residential (0.000000476837158203125 sf)	125' x 80' A1	3.8	3.8
SAI-45	.18*	medium density residential (0.0000002384185791015625 sf)	125' x 80' A1	3.8	3.8
SAI-46	.18*	medium density residential (0.00000011920928950078125 sf)	125' x 80' A1	3.8	3.8
SAI-47	.18*	medium density residential (0.000000059604644750390625 sf)	125' x 80' A1	3.8	3.8
SAI-48	.18*	medium density residential (0.0000000298023223751953125 sf)	125' x 80' A1	3.8	3.8
SAI-49	.18*	medium density residential (0.00000001490116118759765625 sf)	125' x 80' A1	3.8	3.8
SAI-50	.18*	medium density residential (0.000000007450580593798828125 sf)	125' x 80' A1	3.8	3.8
SAI-51	.18*	medium density residential (0.0000000037252902968994140625 sf)	125' x 80' A1	3.8	3.8
SAI-52	.18*	medium density residential (0.00000000186264514844720703125 sf)	125' x 80' A1	3.8	3.8
SAI-53	.18*	medium density residential (0.000000000931322574223603515625 sf)	125' x 80' A1	3.8	3.8
SAI-54	.18*	medium density residential (0.0000000004656612871118017578125 sf)	125' x 80' A1	3.8	3.8
SAI-55	.18*	medium density residential (0.00000000023283064355590085046875 sf)	125' x 80' A1	3.8	3.8
SAI-56	.18*	medium density residential (0.00000000011641532177795004252441 sf)	125' x 80' A1	3.8	3.8
SAI-57	.18*	medium density residential (0.00000000005820766088897502126221 sf)	125' x 80' A1	3.8	3.8
SAI-58	.18*	medium density residential (0.00000000002910383044448751013111 sf)	125' x 80' A1	3.8	3.8
SAI-59	.18*	medium density residential (0.00000000001455241522224375050555 sf)	125' x 80' A1	3.8	3.8
SAI-60	.18*	medium density residential (0.00000000000727620761112187525277 sf)	125' x 80' A1	3.8	3.8
SAI-61	.18*	medium density residential (0.00000000000363810380556093762638 sf)	125' x 80' A1	3.8	3.8
SAI-62	.18*	medium density residential (0.00000000000181905192778046881319 sf)	125' x 80' A1	3.8	3.8
SAI-63	.18*	medium density residential (0.00000000000090952596389023440659 sf)	125' x 80' A1	3.8	3.8
SAI-64	.18*	medium density residential (0.00000000000045476298194511720329 sf)	125' x 80' A1	3.8	3.8
SAI-65	.18*	medium density residential (0.00000000000022738149097255860164 sf)	125' x 80' A1	3.8	3.8
SAI-66	.18*	medium density residential (0.00000000000011369074548627930082 sf)	125' x 80' A1	3.8	3.8
SAI-67	.18*	medium density residential (0.00000000000005684537274313965041 sf)	125' x 80' A1	3.8	3.8
SAI-68	.18*	medium density residential (0.00000000000002842268637156982521 sf)	125' x 80' A1	3.8	3.8
SAI-69	.18*	medium density residential (0.00000000000001421134318578491261 sf)	125' x 80' A1	3.8	3.8
SAI-70	.18*	medium density residential (0.00000000000000710567159289245631 sf)	125' x 80' A1	3.8	3.8
SAI-71	.18*	medium density residential (0.00000000000000355283579644622816 sf)	125' x 80' A1	3.8	3.8
SAI-72	.18*	medium density residential (0.00000000000000177641789822311408 sf)	125' x 80' A1	3.8	3.8
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SAI-119	.18*	medium density residential (0.00000000000000000000000000001262 sf)	125' x 80' A1	3.8	3.8
SAI-120	.18*	medium density residential (0.00000000000000000000000000000631 sf)	125' x 80' A1	3.8	3.8
SAI-121	.18*	medium density residential (0.00000000000000000000000000000316 sf)	125' x 80' A1	3.8	3.8
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SAI-125	.18*	medium density residential (0.00000000000000			

**GREENBRIAR
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Section 2 - Transit-Oriented and Mobility

2.1 Principles and Objectives

- The Greenbriar Transit-oriented development [TO] embraces the following general design principles.
- Balanced mix of land uses [mixed uses]
 - Pedestrian scale design [walkability]
 - Connected community [variety of movement modes]
 - Reduced impact of the automobile [travel trip and walk]
 - Authentic architecture [reflecting tradition]
 - California train depot motifs enlivened with touches of traditional Sacramento neighborhood architecture]

2.2 Transit-Oriented Development

For organizational purposes these guidelines divide the TO into two primary parts, the mixed use portions and the rail station.

2.2.a LRT Station Location and Adjacent Land Uses

A community commercial facility is planned adjacent to the future Greenbriar light rail transit [LRT] station. As LRT stations are prime generators of pedestrian traffic and activity, typical elements of a TOJ are a plaza, community and railroad serving uses. For example, retail, community space, housing or office, parking and the station itself. In the case of the future Greenbriar TOJ, all of the typical elements are present along with the additional conveniences of an adjacent lake and park.

The area around an LRT station [including the street] should be thought of as a single community space or room. Elements of this room shall include:

- Sidewalks and pedestrian paths in the immediate vicinity of LRT station at least ten [10] feet in width.
- Walked pedestrian crossing of station in a color contrasting the street and of high quality materials in keeping with the design of the station.
- Station area with lush landscaping accompanied by drought tolerant materials
- Pairs of other tall trees planted at the station so that they may be marked from a distance.
- Street trees that provide shade and visual interest in the vicinity of the station.

**GREENBRIAR
PUD Guidelines**

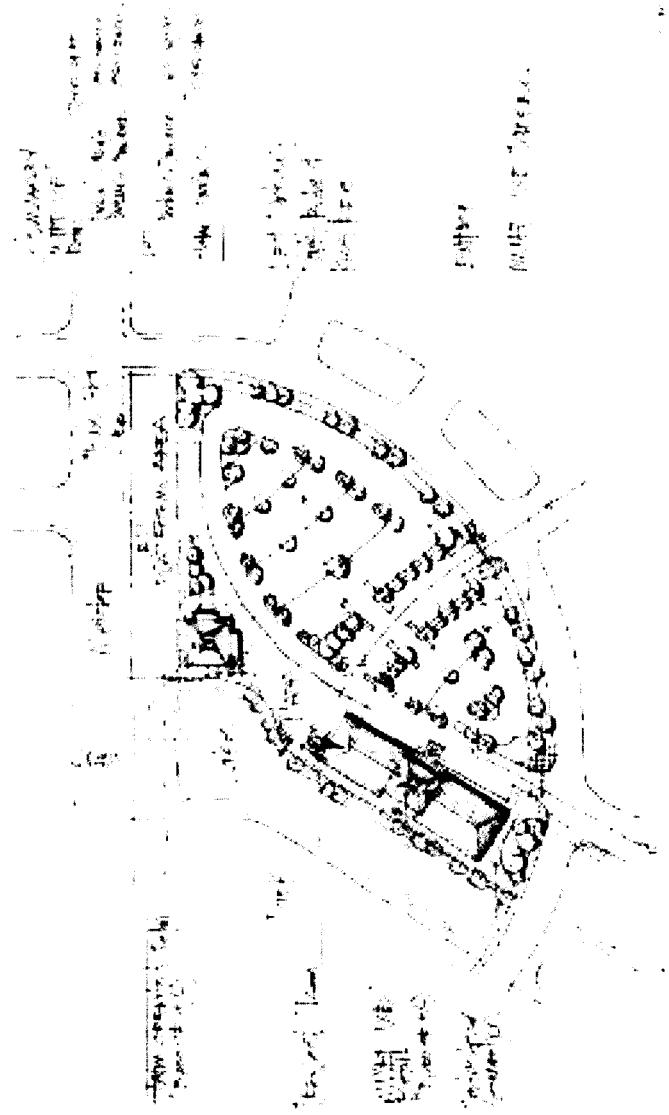


Exhibit #4 - TOD Commercial Site - Concept Plan

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2.2.b Mixed-Use Design

Mixed-use may be horizontal or vertical in design. The mixed-use component includes the plaza, mixed-use structures, mixed-use parking, residential parking and pedestrian/bicycle circulation.

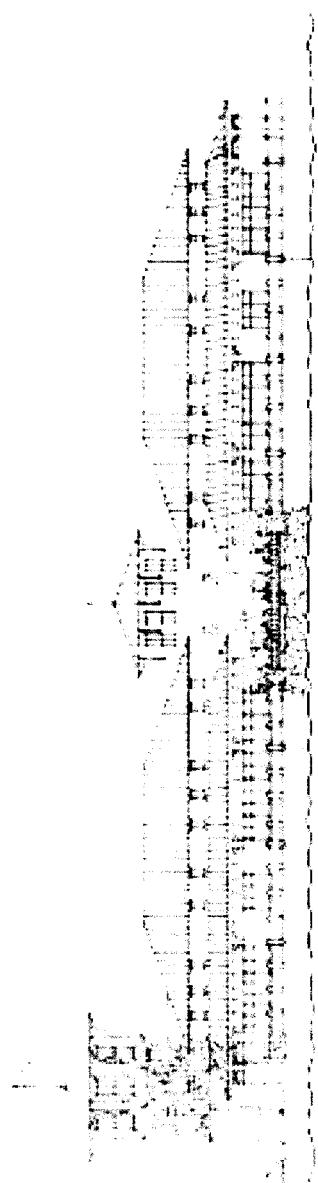
Buildings on the site may have retail on the ground floor such as a specialty grocery market, wine shop, station or specialty gift/grocery stores. Upper floors may include residential uses, office space or a bed and breakfast.

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**Exhibit #5 - One-Story Commercial Building at TOD
Restaurant/Retail/Office-Concept Plan**

LRT stations are generally low in height and long approximately 300 to 400 feet in length and approximately 60 feet in width. LRT stations are comprised of a grade-level platform, canopies, seating, fare vending equipment, landscaping and signage. At this location a kiss-and-ride area, bus drop-off area and parking for approximately 150 to 200 vehicles will be provided. Kiss-and-ride refers to RT riders who are dropped off by another driver at the RT station on their daily commute. No permanent parking is to be provided in the kiss-and-ride area. It is anticipated that a bus stop will be located in the immediate vicinity of the station.



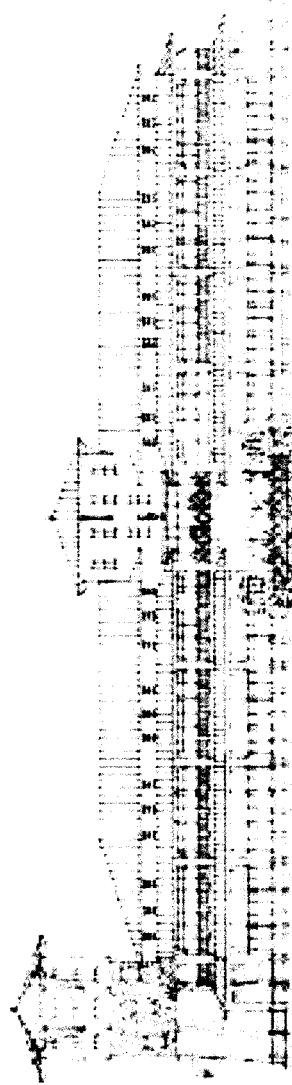
Exhibit #6 - Light Rail Transit Station

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*Exhibit #7 - Two Story Commercial Building at TOD
(Office over Restaurant/Retail)-Concept Plan*

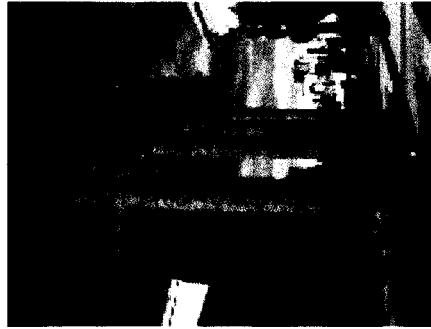


Exhibit #9 - Pedestrian/Mixed-Use Circulation



Exhibit #8 - Light Rail Transit Station

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PUD Guidelines**



**Exhibit #10 – Light Rail Station at TOD
(Station/Restaurant/Retail)-Concept Plan**

2.2.c TOD Development Standards

The currently commercial designation allows a mixture of retail/commercial and housing opportunities in a single area of the project. Mixed housing opportunities proposed on this site include townhouses, lofts or mixed-use buildings with ground-floor retail and second/third floor offices and/or housing, housing may be tenant or for-sale.

Permitted Uses (General)

The City of Sacramento's code for C-1 zoning shall prevail with the following explicit addition:

- Farmers Market

Architectural Treatment

The TOD's architectural style shall be a contemporary interpretation of traditional Sacramento styles. This means paying homage to traditional forms while embracing new materials and technologies.

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The TOD's architecture shall reflect the indoor-outdoor living style that is a hallmark of Northern California. Walkable outdoor spaces should be included in the design of structures along with the extensive use of passive solar elements such as awnings, pergolas and trellises.

- experience that includes one or more of the following: interlocking boulders, oaks, overhangs and/or other overhead structures.
- Entries shall be made prominent through the use of architectural details and accents such as arches, planter boxes, overhead boulders and trellised doors.
- Rock-mounted communications equipment shall be screened or placed behind roof parapet walls so as not to be visible to motorists and pedestrians.
- A broad use of a variety of building, accent and roof colors is encouraged.

Street trees should be primary large shade trees while palm trees or other tall trees will be used to mark the location of the station from the roadway and surrounding neighborhoods subject to approval of Jutan Tree Services.

The following design guidelines apply:

- All structures shall have four-sided architectural treatments.
- All structures shall have paved outdoor patios and/or seating areas located adjacent to at least one side of the building.
- Glass storefronts are encouraged along pedestrian ways.
- All building developments with pedestrian occupancy shall be designed with pedestrian scaled details and amenities such as display windows, awnings, trellises and others.
- Each structure shall be architecturally and stylistically connected to the overall pedestrian

Plaza

A pedestrian-oriented plaza shall be built as part of the TOD. This plaza shall be designed as an intensively used gathering place. Spaces shall be designed for public gathering and to accommodate activities such as resting, eating, strolling and people watching.

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Exhibit #11 - Plaza

Guidelines for the plaza include:

- High degree of applied design
- Adjacency to lift station
- Adjacency or accessibility to the lake.
- Adjacency to the Pedestrian Promenade.

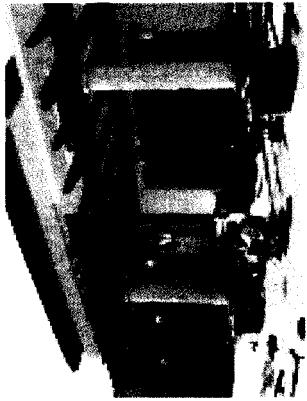


Exhibit #12 - Pedestrian Plaza

- Adjacency to, or at least on one side, some of the community-serving uses such as retail, commercial or restaurant uses.
- Potted seating may approach into the plaza.
- A range of ample seating opportunities.
- Good visibility to/from surrounding streets.
- Good visibility from the parking lot.
- Opportunities for pedestrians to shelter in inclement weather.
- Secure bicycle parking and storage.
- Providing area maps for use of RT patrons and million-area visitors.
- Providing electrical outlets throughout the plaza to facilitate mobile vendors, outdoor performances and the mobile/wireless high-tech population.

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door of the station and adjacent residential users. Specific guidelines include:

- Pairs of either tall trees planted at the station so it can be seen or marked from a distance (subject to approval of Urban Tree Services)
- Station area lushly landscaped with drought tolerant plants and materials.
- Street trees for shade and visual interest particularly in the vicinity of the station.



Exhibit #14 - Community icon



Exhibit #13 - Pedestrian Plaza

Landscape Concept

As LRT stations are prime generators of people and activity, the area immediately adjacent to and including the station should be thought of as a single room or space. Landscaping helps define the edges of the room and, in the case of the Greenbriar TCD, serves as a place marker, looking outwards to the front

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Community Icon

A community icon should be constructed as part of the plaza or lake-facing structures. This community icon is envisioned as a vertical element such as a tower, beacon or other art piece. The icon shall be lighted at night.

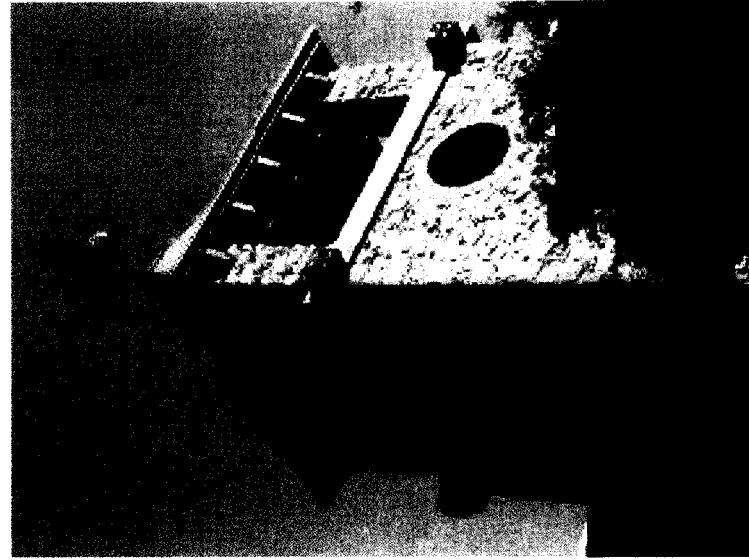


Exhibit #16 - Community Icon

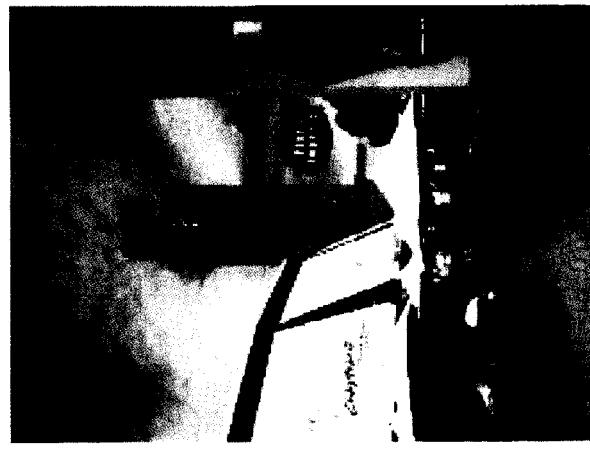


Exhibit #15 - Community Icon Tower

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Canopies

Canopies are traditionally used to provide shelter for passengers from the elements. Shelters and canopies will be found on the LRT station platform itself as well as in the adjacent plaza.

Mid-term canopies traditionally occupy a small portion of the platform area. Both platform and plaza canopies shall reflect traditional California iron depot mall's shelters and canopies shall be of sufficient size and quantity to provide shelter and protection during inclement weather and heat. Canopies should also be included on both of the bus on-street facility.



Exhibit #17 - Building Canopy



Exhibit #18 - Lake Frontage

Lake Frontage

A significant amount of the TOD fronts the lake. These guidelines place structures along the majority of this lake frontage.

Design elements include:

- No parking adjacent to the lake shall be parallel.
- All parking shall be separated from the lake with buildings or open space equal to the average depth of buildings fronting the lake.
- All structures adjacent to the lake shall provide the option for public entries on both the lake and parking sides of the structure.
- A landscaped pedestrian promenade will be provided along the lake edge adjacent to lake facing developments. This lake fronting promenade surrounds the RTI. The plaza, lake fronting buildings and neighborhoods to the south. The landscaped pedestrian promenade is recommended to be stored generally with circulation around the lake edge.
- Outdoor seating/drinking areas facing the lake and pedestrian way are strongly encouraged.
- Outdoor seating/drinking areas built over the lake are encouraged.

Street Frontage

Structures in the TOD do not typically have service drives or loading zones except in the event of a multi-story, mixed-use structure. Except for multi-story, mixed-use buildings, service will be from parking areas during off-hours. The following guidelines apply:

- Service functions shall be screened from general public view by building design and layout. Screen walls attached to buildings and plantings or property lines.
- Design of storage and service areas shall be compatible with and in the case of loading and service areas integral to building architecture.
- Waste storage and collection areas shall be designed with materials, finishes and colors from the adjacent building. Metal gates are required.
- Trash enclosures are not permitted in the street right-of-way or street-side landscape setback.

2.3 Non-Vehicular Mobility

In addition to the planned extension of the Regional Transit (RT) light rail Corridor through the plan area, the PUD provides a varied network of both on and off street pedestrian pathways and trails allowing for safe and convenient non-vehicular travel throughout and within the PUD.

GREENBRIAR PUD Guidelines

2.3.a Light Rail Corridor

Currently, Regional Transit [RT] does not exist within the area; however extension of existing facilities is planned along the south side of Weisler Way. This rail system will connect the site directly connecting the plan area to Downtown Sacramento through the North Sacramento Community Plan area to the east and to the Sacramento International Airport through Metrolink to the West.

The transit station is centrally located within the heart of the Greenbriar PUD area adjacent to high-density residential and community commercial land uses. Appropriate pedestrian friendly bus shelters will be appropriately located to ensure safe and convenient pedestrian use. The placement of medium and high density residential uses, including the provision of car-free housing sites, will allow for convenient pedestrian access to work and walkable routes.

2.3.b On-Street Pedestrian and Bicycle Mobility

The PUD provides a varied network of both on- and off-street pedestrian pathways and trails allowing for safe and convenient non-vehicular travel throughout and within the PUD. All arterial and collector streets have striped Class III bike lanes. Nearly all sidewalks within the PUD's streets are detached from the street edge and separated from the street by a landscape planter of varying width depending upon the street facility. These pedestrian-friendly streets will provide a safe walkable

route to everywhere in the PUD area under a dense canopy of shade trees.

2.3.c Off-Street Pedestrian and Bicycle Mobility

The open space/freeway buffer along the south and west sides of the plan area will provide Class I trail facilities within a wide landscaped open space area. Additionally, Greenbriar Lake offers an extensive trail system in the form of a 27' wide lakeside or promenade that surrounds the entire perimeter of the lake. These trail facilities will in conjunction, facilitate circular movement within the community without ever stepping onto any roadway.

2.4 Vehicular Circulation

The design of the Greenbriar PUD circulation pattern utilizes an efficient system of streets. Pedestrian and Regional Transit facilities to serve the plan area. The internal street system is designed such that a 'block' or 'grid' system is achieved. Various pedestrian pathways are designed to provide non-vehicular transportation opportunities throughout the project site in all directions linking the project's many park sites. This creation of a well-connected hierarchy of roadways allows for the efficient movement of vehicular traffic and encourages biking, walking and other human powered transportation alternatives to the automobile.

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2.4.a Street and Alley Standards

The intent of the circulation system design is to provide a functional and efficient system that balances the needs of automobiles with other forms of transportation including walking and bicycling. In addition to circulation requirements, the street system will be designed for aesthetics and will be an element of the overall project's identity.

Arterial and collector streets are the principal streets that serve the through traffic within the PUD. The arterial street within the PUD is Elkhorn Boulevard located along the northern boundary of the plan area. Master Way serves the plan area as a collector roadway and bisects the PUD providing an east/west connection through the plan area.

The internal residential streets provide connections into and through the PUD and connect residential neighborhoods with the field centers, elementary school, and the various park and open space facilities thus allowing residents to travel within and throughout the PUD without traversing along the arterial and collector roadways. Where necessary, traffic control devices will be installed that will facilitate timely and safe pedestrian crossings.

The following pages describe and illustrate the Overall Circulation Plan and individual Street Sections utilized within the Greenbriar PUD. The exhibits are color-coded for convenience and clarity.

Please note: This section and the associated exhibits are illustrative by nature to illustrate the various street sections. For actual approved street sections see the Tentative Subdivision Map and Master Tentative Subdivision Map. If and where there are discrepancies between these exhibits and the Tentative Maps, the Tentative Maps shall prevail.

2.4.b Parking Standards

Parking at the TCO site accommodates residents of both the ST station and the community-serving uses within this mixed-use TCO. Parking shall be shared between the light rail users and the community commercial development.

Parking requirements shall be calculated on the assumption that parking shall be shared between the mixed-use and light rail portions of this project. Each portion shall calculate the required amount of parking based on City of Sacramento's requirements.

Parking for the mixed-use portion of the TCO shall be lighted and landscaped according to the standards presented in these guidelines.

Housing is included in the mixed-use portion of the TOD, separate parking shall be identified and set aside for purely residential users.

2.4.c Park/Plaza Facilities

The Sacramento Regional Transit Authority (RTA) is a proponent of the City's light rail line that will serve Green Valley Station. Parking spaces should be provided in conjunction with the needs of RTA in close proximity of the light rail station. The parking area shall be lighted for night use and landscaped according to City standards. A pedestrian walkway will traverse the parking lot, continuing off the plaza.

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circulation exhibit

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|--------------------------|---|
| <input type="checkbox"/> | the exterior boundary |
| <input type="checkbox"/> | the interior boundary |
| <input type="checkbox"/> | the median way lies in front of |
| <input type="checkbox"/> | bridge over river |
| <input type="checkbox"/> | Kennedy bridge |
| <input type="checkbox"/> | canal master ways |
| <input type="checkbox"/> | the reservoir ends |
| <input type="checkbox"/> | the express bus is starting to run |
| <input type="checkbox"/> | the source of acidic water |
| <input type="checkbox"/> | the oil factor of sand |
| <input type="checkbox"/> | the oil fields show? |
| <input type="checkbox"/> | the residence collector, rear sheet |
| <input type="checkbox"/> | the residence collector is at the edge |
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| <input type="checkbox"/> | the primary residence |
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| <input type="checkbox"/> | the primary residence along bridge |
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| <input type="checkbox"/> | the primary element of structure |
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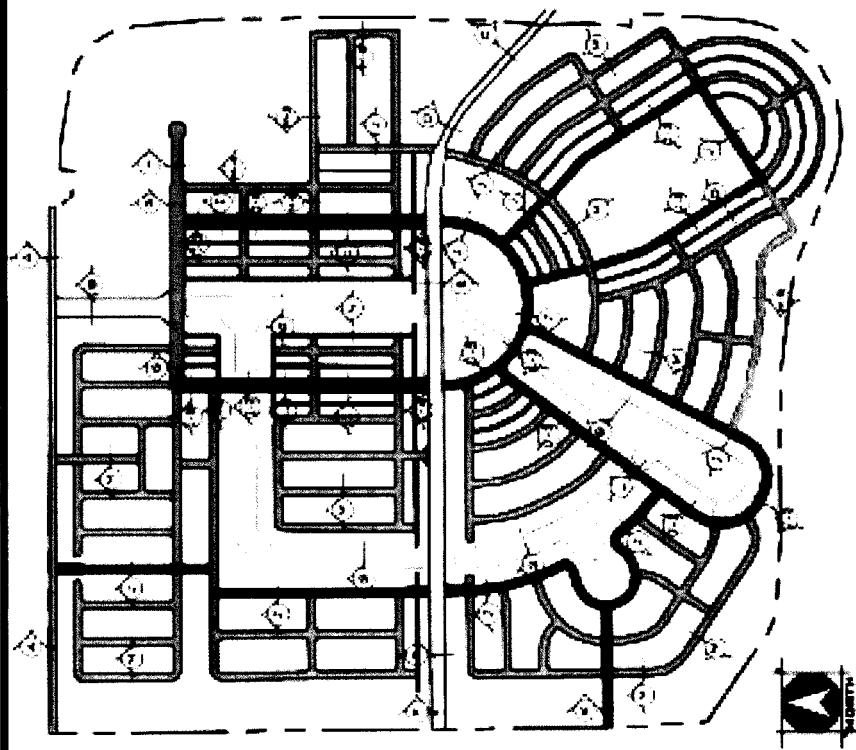


Exhibit #19 - Overall Circulation Plan

For final consideration by City Council on January 29, 2008

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Birkhorn Boulevard

Birkhorn Boulevard is the main oriented street serving the plan area and is located along the northern property boundary of the PUD. Overhead poweline facilities are planned for the south side of Birkhorn Boulevard within the 25' P.U.E.

- This facility is comprised of the following components:
 - 135' ultimate right-of-way
 - 26' landscaped median разделено left turn lane
 - three travel lanes (12', 12' and 11') in each direction
 - 6' unpeaked bike lanes
 - 8.5' landscape planter with 6' separated sidewalk on each side
 - 25' P.U.E. (landscape corridor on the south side of the street)
 - parking is not permitted

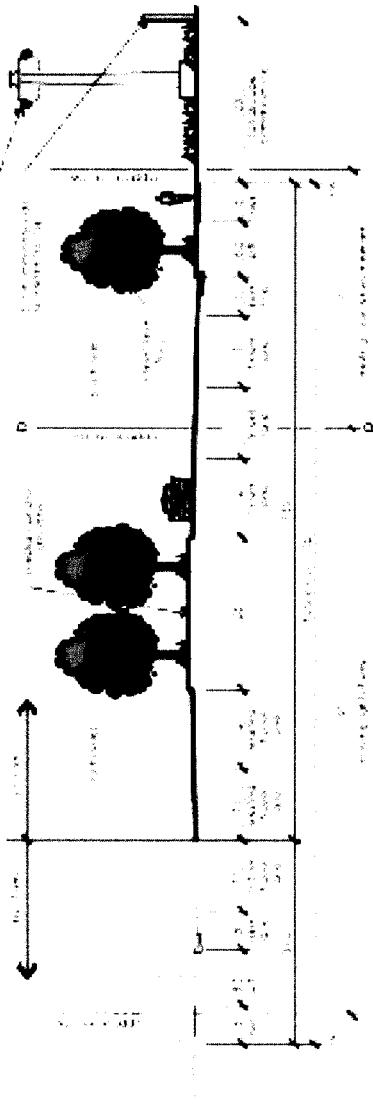


Exhibit #20 - Birkhorn Boulevard

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**GREENBRIAR
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Greenbriar Boulevard (main project entry)

Greenbriar Boulevard is the main project entry street serving the plan area from the north along Greenbriar Boulevard. This 4-lane '��' (lucky) is designed with a very generous landscape section with landscape corridor and wide pedestrian-friendly sidewalks on each side.

- This '��' is comprised of the following components:
- 13' ultrawide right of way
 - 30' landscaped median/double left turn lane
 - two travel lanes (12' and 11') in each direction
 - 8' striped bike lane
 - 12.5' landscaped corridor with 10' separated sidewalk on each side
 - 12.5' landscape corridor with 10' PWE on both sides
 - Parking is not permitted

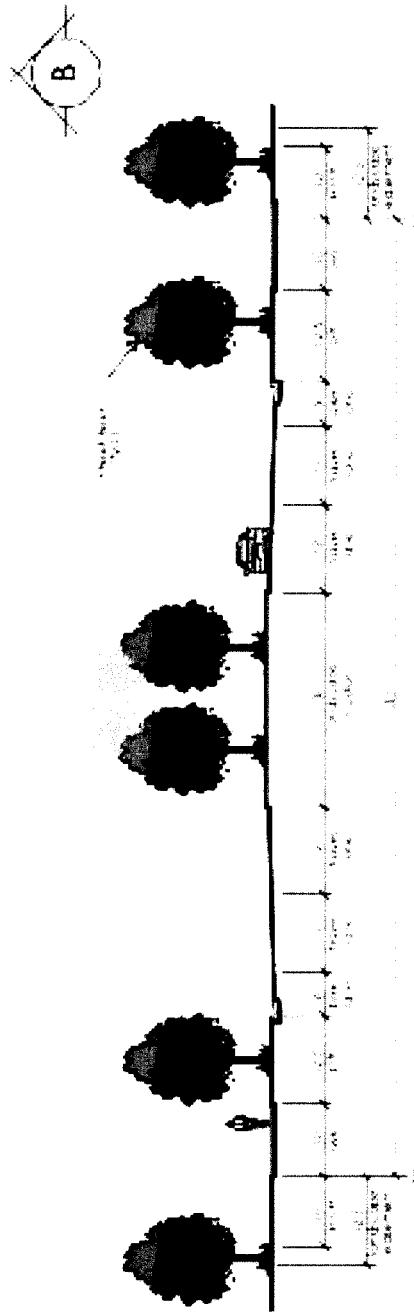


Exhibit #21 - Greenbriar Boulevard (main entry)

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**GREENBRIAR
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**Meister Way Flyover at State Highway 99/70-Bridge
Overcrossing**

This portion of Meister Way is located at the project's confluence with the Meister Way over crossing state highway 99/70.

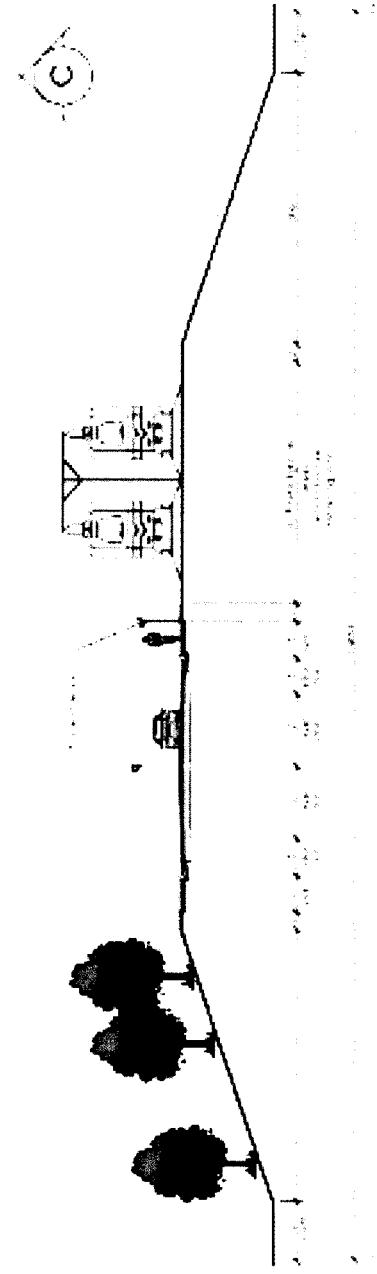


Exhibit #22 - Meister Way Flyover at State Highway 99/70 Bridge Overcrossing

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**GREENBRIAR
PUD Guidelines**

Melster Way - 56' ROW

This portion of Melster Way is located near the easternmost portion of the Plan area, just west of State Highway 99/70, and is designed consistent with the existing Melster Way in the North Stationers Community Plan Area.

This roadway is comprised of the following components:

- 56' right of way
- 12' landscaped median/turn lane
- one travel lane [11'] in each direction
- 6' striped bike lanes
- 5' dedicated sidewalk
- parking is not permitted



Exhibit #23 - Melster Way - 56' ROW

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**GREENBRIAR
PUD Guidelines**

Meister Way - 106' ROW

Meister Way is an east west oriented collector roadway that links the plan to Metric Air Park on the west and the North Naturals Community Plan area on the east. This roadway facility is 106' ROW through the majority of the plan area but reduces in width as it moves easterly in the plan area.

- This roadway is comprised of the following components:
 - 108' ultimate right-of-way
 - 12' landscaped median/centerline
 - two travel lanes (12' and 11') in each direction
 - 6' striped bike lanes
 - 7' parking lane on north side only
 - 8.5' landscape planter with 10' separated sidewalk on each side
 - 6' detached sidewalk
 - parking is permitted on the north side only
 - no residential driveway access allowed

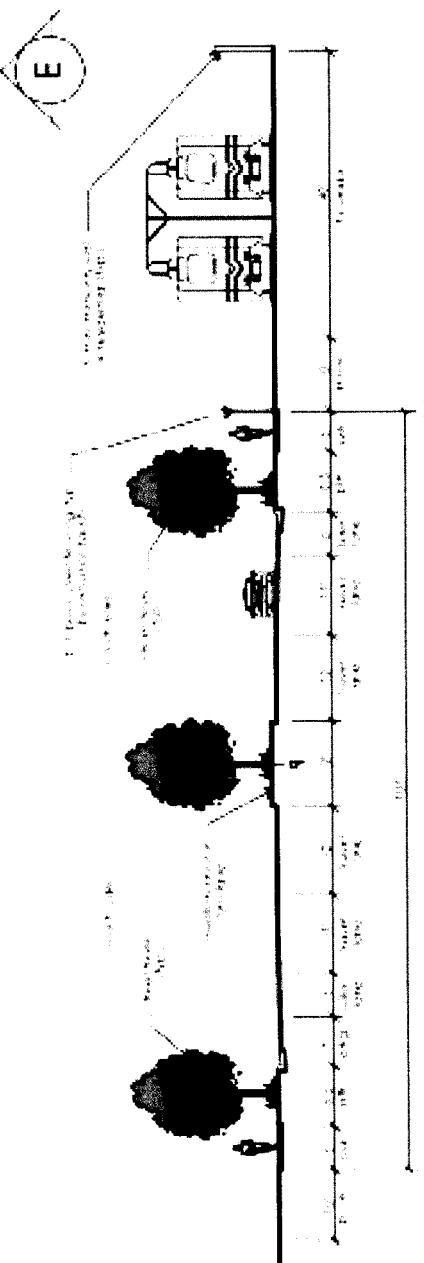


Exhibit #24 - Meister Way - 106' ROW

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**GREENBRIA
PUD Guidelines**

**Meister Way at Wildlife Corridor/Faux Bridge
106' ROW**

This portion of Meister Way occurs at the plan areas' westernmost boundary where it crosses the Wildlife Corridor-related buffer and at all bridge crossings of the lake feature.

- This facility is comprised of the following components:
- 106' ultimate right-of-way
 - 12' median/turn lane
 - two travel lanes [12' and 11'] in each direction
 - 4' multi-use/bike lane
 - 7' parking lane
 - 11' dedicated sidewalk on each side

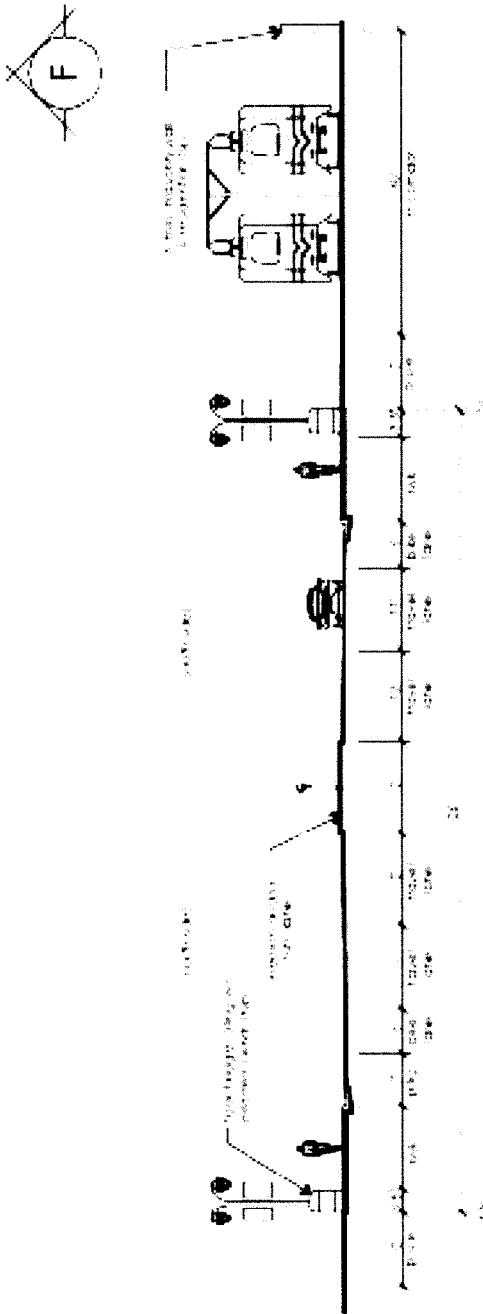


Exhibit #25 - Meister Way at WildWe Corridor/Faux Bridge 106' ROW

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**GREENBRIAR
PUD Guidelines**

Collector Street 1 - 95' ROW

Greenbriar Boulevard (the main project entry) terminates in this Collector street, which extends both west and east, dispersing traffic into and tunneling traffic out of the PUD area. This totality is comprised of the following components:

- 95' ultimate right-of-way
- two travel lanes (12' and 11') in each direction
- 6' striped bike lanes
- 12' median/turn lane
- 7.5' landscape planter with 5' separated sidewalk on each side
- parking is not permitted

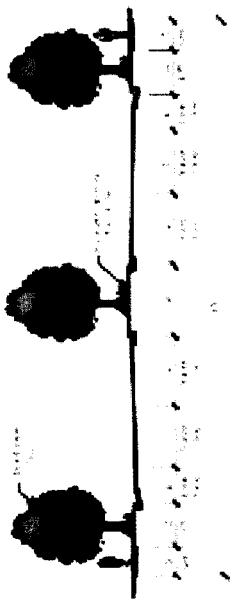


Exhibit #26 - Collector Street 1 - 95' ROW

Collector Street 2 - 45.5' ROW

This Collector street is an extension of the 45.5' R.O.W. collector east into the plan area and ultimately terminates into the Community park. This collector functions as a secondary access into the Village Commercial site and as a primary entry into the Community park. This totality is comprised of the following components:

- 45.5' ultimate right-of-way
- one 11' travel lane each way
- 6' striped bike lanes
- 7' parking lanes
- 7.5' landscape planter with 5' separated sidewalk on north side (at commercial frontage)
- 5' allecreted sidewalk (no planter) on south side (at community park frontage)
- parking is permitted

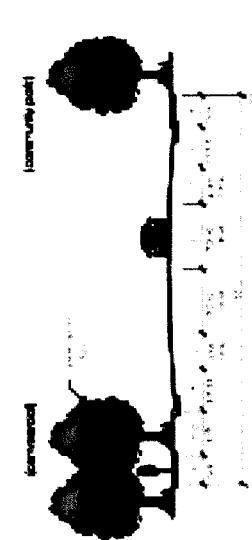


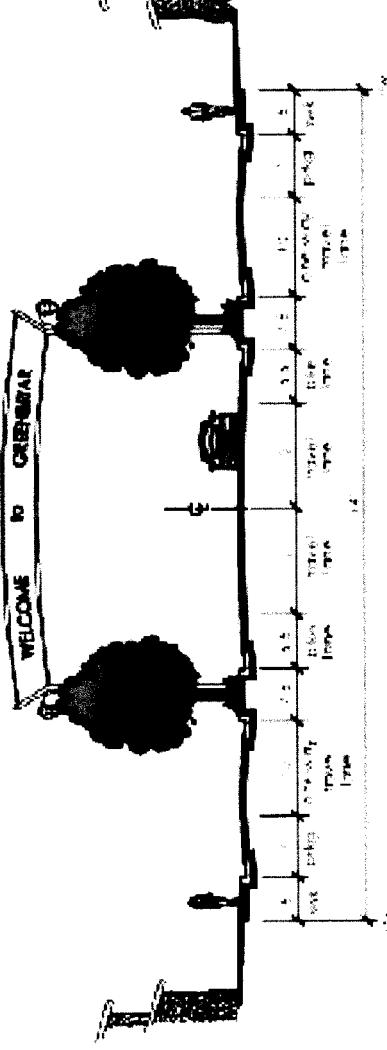
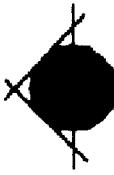
Exhibit #27 - Collector Street 2 - 45.5' ROW

**GREENBRIAR
PUD Guidelines**

Residential Collector "Main Street" - 94' ROW

This "Main Street" facility occurs in two places within the PUD area, and provides direct north/south access through and within the northern portion of plan area. This facility is specifically designed to separate through or commuter traffic from local traffic serving the adjacent front-on residential units. This traffic-carrying facility replaces the need for a typical large 4-lane collector with soundwalls with back-on residences that would divide the neighborhood, but rather promotes a more pedestrian-friendly walkable and aesthetically superior alternative.

- This facility is comprised of the following components:
 - 9'4" ultimate right-of-way
 - one 12' travel lane in each direction
 - 5.5' striped bike lanes
 - 7.5' landscape planters
 - separated 10' one way travel lane (on each side)
 - 7' parking lanes
 - 5' attached sidewalks
 - Parking is permitted



**Exhibit #28 - Residential Collector "Main Street" - 94'
ROW**

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**GREENBRIAR
PUD Guidelines**

Residential Collector "Main Street" at Lake Bridge 60' ROW

This Main Street 'facility' occurs only where the residential collector crosses the lake feature. At these crossings, the 10' travel lanes and associated parking lane are no longer needed so they are removed. This 'facility' is comprised of the following components:

- 60' ultralite right-of-way
- one 12' travel lane in each direction
- 5.5' striped bike lanes
- 9' attached sidewalk on each side
- parking is not permitted

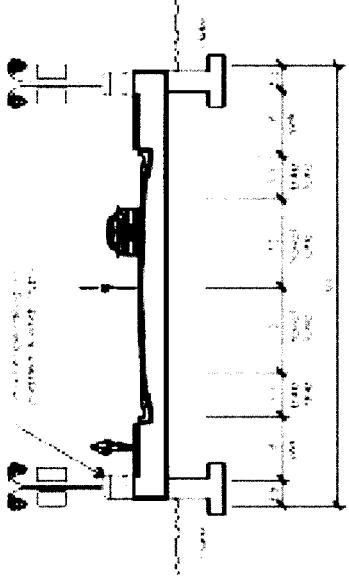


Exhibit #29 - Residential Collector at Lake Bridge - 60' ROW

(For Final Consideration by City Council on January 29, 2008)

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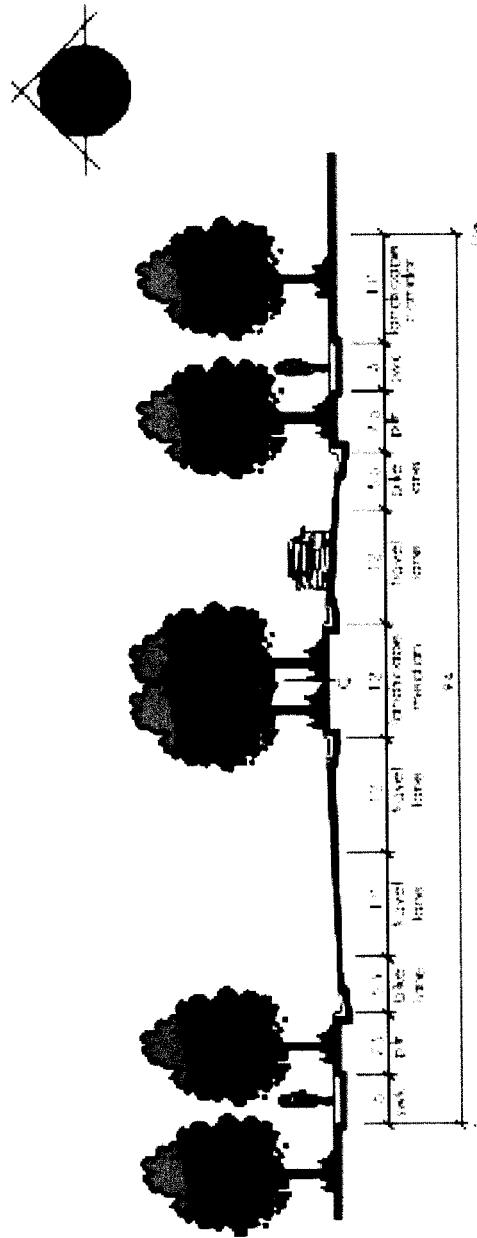
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**GREENBRIAR
PUD Guidelines**

Residential Collector "Main Street" - Entry at Meister Way - 94' ROW

This "Main Street" facility occurs in two places within the PUD area where this roadway intersects with Meister Way. At this location, the residential serving transects roads units are not necessary and instead have been replaced with a southbound left turn lane and wide landscape median that separates traffic.

- This facility is comprised of the following components:
- 94' ultralite right-of-way
 - one northbound 11'2" travel lane and two southbound 11'2" and 11'1" travel lanes
 - 12' landscape median
 - 5'5" striped bike lanes
 - 7'5" landscape planters
 - 5' detached sidewalks
 - 11'1" landscape corridor on east side of street



**Exhibit #30 - Residential Collector "Main Street" -
Entry at Meister Way - 94' ROW**

(For Final Consideration by City Council on January 29, 2008)

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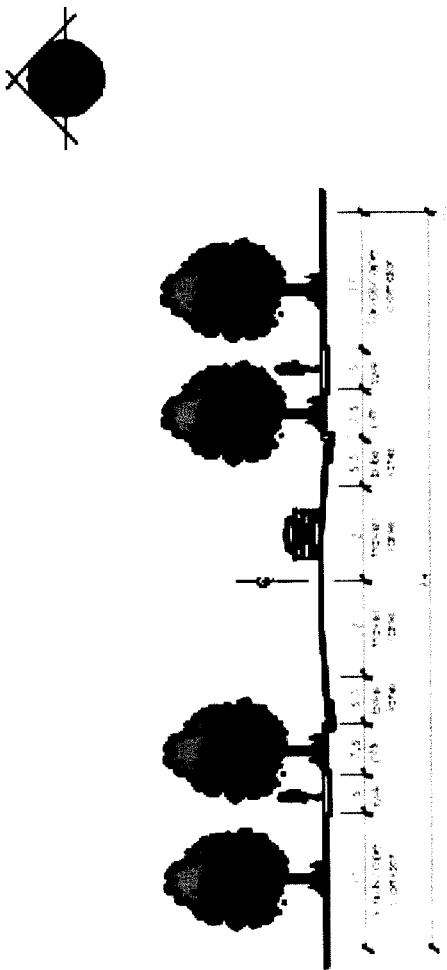
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**GREENBRIAR
PUD Guidelines**

Residential Collector "Main Street" - 94' ROW

This "Main Street" facility section occurs at intersections of east/west streets where the residential serving function road is not necessary and has been removed to minimize overall movement width, and therefore pedestrian crossing distances at intersections.

- Pedestrian crossing distances at intersections.
- 94' utility & comprised of the following components:
 - 94' ultimate right-of-way
 - one 12' travel lane in each direction
 - 5' striped bike lanes
 - 7.5' landscape planter
 - 5' detached sidewalk
 - 17' landscape planter
 - parking is not permitted



**Exhibit #31 - Residential Collector "Main Street"
"Neckdown" at east/west street intersections - 94' ROW**

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**GREENBRIAR
PUD Guidelines**

Residential Collector "Main Street" - 94' ROW
This "Main Street" facility occurs in one place within the PUD. This facility is designed for the location where the residential-zoning one-way lane is not necessary on one side of the street.

- This facility is comprised of the following components:
- 9' 4" ultralite right of way
 - One 12' travel lane in each direction
 - 5.5' striped bike lanes
 - 7.5' landscape planters
 - 6' detached sidewalk [west side of street]
 - 17' landscape corridor [west side of street]
 - 10' one-way travel lane [east side of street]
 - 7' parking lane on east side of street only
 - 5' attached sidewalk [east side of street]

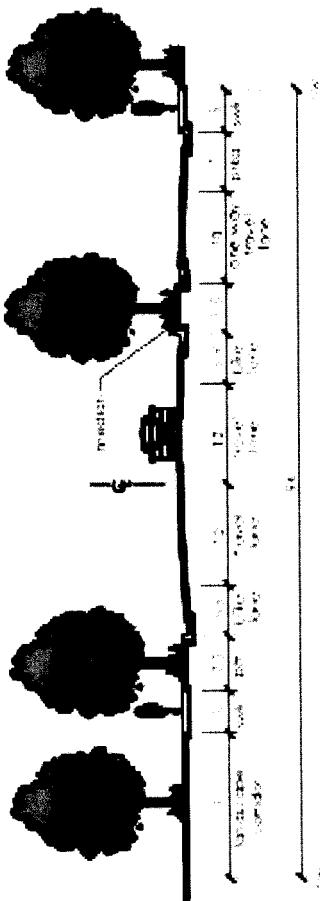


Exhibit #32 - Residential Collector "Main Street"
Separated 10' one-way travel lane and 7' parking on
east side of street - 94' ROW

[For Final Consideration by City Council on January 29, 2008]

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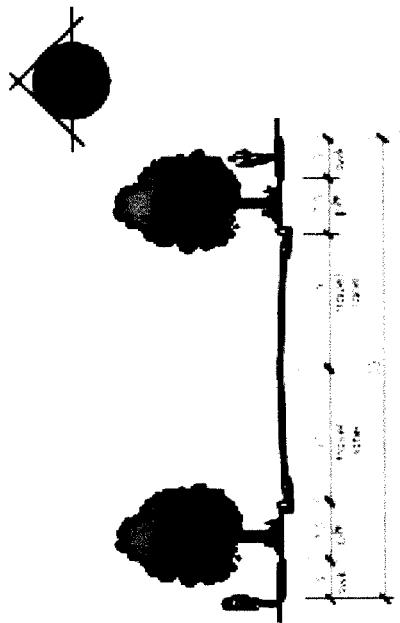
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**GREENBRIAR
PUD Guidelines**

Primary Residential - 55' ROW

Primary residential streets within the PUD area are the main connector streets within the residential street network. These street trees will have a wider landscape planter to accommodate a larger and more prominent street tree which will serve to orient and define the curvilinear. The overall pavement width is the same as the local residential streets. This facility is comprised of the following components:

- 55' utilizable right-of-way
- one 15' travel lane in each direction
- 7.5' landscape planter with 5' separated sidewalk on each side
- parking is permitted



Primary Residential - 60' ROW

This primary residential street is consistent with the 55' R.O.W. Primary residential street however the pavement with is 5' wider which is necessary to accommodate underground infrastructure. This facility is comprised of the following components:

- 60' utilizable right-of-way
- one 17.5' travel lane in each direction
- 7.5' landscape planter with 5' separated sidewalk on each side
- parking is permitted

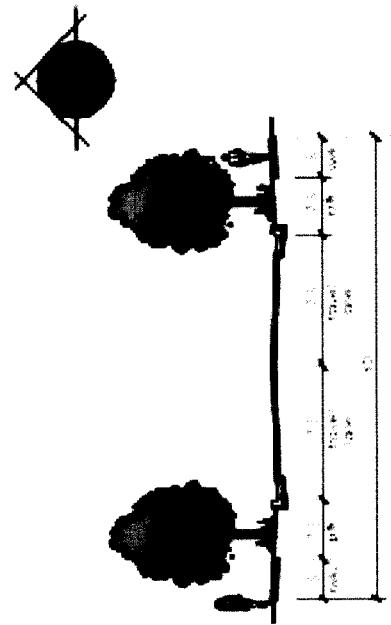


Exhibit #33 - Primary Residential - 55' ROW

Exhibit #34 - Primary Residential - 60' ROW

For Final Consideration by City Council on January 29, 2008

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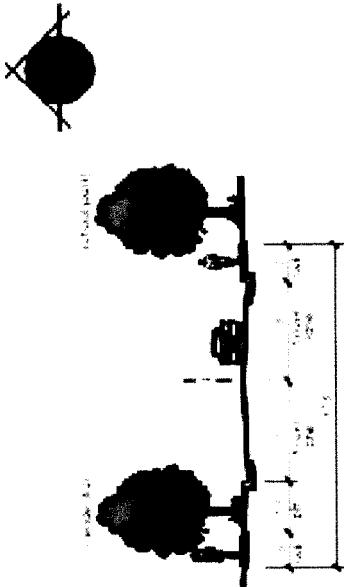
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**GREENBRIAR
PUD Guidelines**

Primary Residential - 47.5' ROW

This Primary residential street is consistent with the 55' P.O.W. Primary residential street however the separated sidewalks are located only on the side of the street adjacent to the residential uses. On the school/park/open space side of the street the sidewalk is attached and the 7.5' roadway has been removed. This facility is comprised of the following components:

- 47.5' multi-lane right-of-way
- one 15' travel lane in each direction
- 7.5' landscape planter with 5' separated sidewalk on residential side only
- parking is permitted



Primary Residential - Faux Bridge at Wildlife Corridor - 55' ROW

This Primary residential street occurs only where Primary residential street cross the Wildlife Corridor Corridor Buffer. This facility is comprised of the following components:

- 55' Unpaved right-of-way
- one 15' travel lane in each direction
- 9' allocated sidewalk on each side
- no parking is permitted

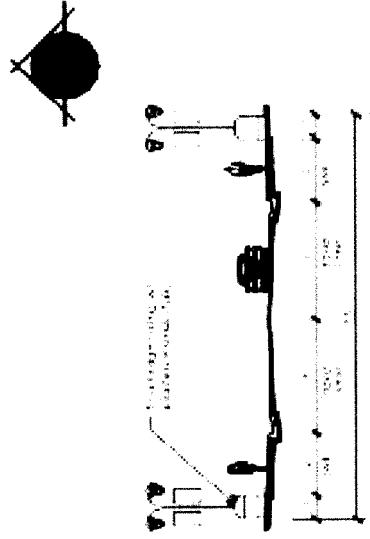


Exhibit #35 - Primary Residential - 47.5' ROW

Exhibit #36 - Primary Residential - Faux Bridge at Wildlife Corridor - 55' ROW

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**GREENBRIAR
PUD Guidelines**

Primary Residential at Lake Bridge - 60' ROW

This primary residential street occurs only where the 40' R.O.W. primary residential street crosses the lake facility. This facility is comprised of the following components:

- 40' ultralite right-of-way
- one 17.5' travel lane in each direction
- 9' allocated sidewalk on each side
- parking is not permitted

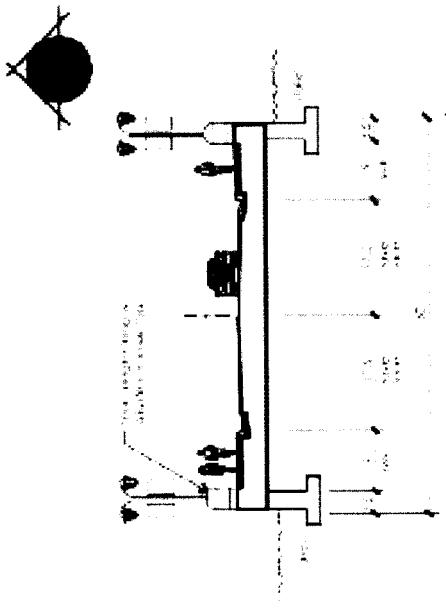


Exhibit #37 - Primary Residential at Lake Bridge - 60' ROW

Primary Residential at Lake Bridge - 55' ROW

This primary residential street occurs only where the 55' R.O.W. primary residential street crosses the lake facility. This facility is comprised of the following components:

- 55' ultralite right-of-way
- one 15' travel lane in each direction
- 9' allocated sidewalk on each side
- parking is not permitted

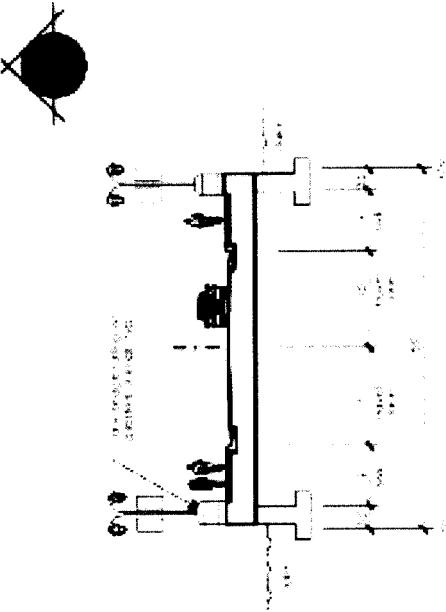


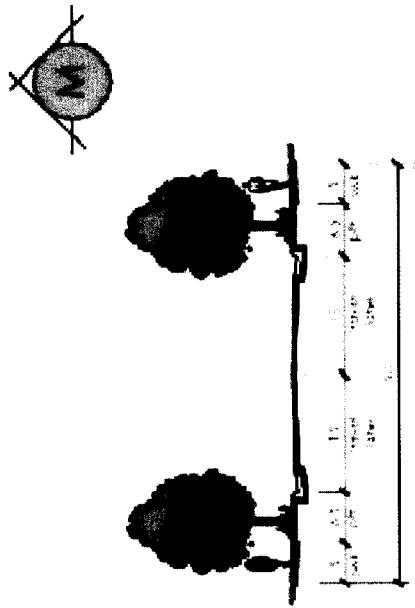
Exhibit #38 - Primary Residential at Lake Bridge - 55' ROW

**GREENBRIAR
PUD Guidelines**

Local Residential Street - 53' ROW

This local residential street is the preferred street within the plan area in terms of occurrence and serves the vast majority of the residential land uses. This "utility" is comprised of the following components:

- 53' ultimate right-of-way
- one 15' travel lane in each direction
- 6' landscape planter with 5' separated sidewalk on each side
- parking is permitted



Local Residential Street - 58' ROW

This local residential street is designed with the same features as the typical 53' R.O.W. local residential street; however the overall paved run width is 5' wider to accommodate underground infrastructure. This "utility" is comprised of the following components:

- 58' ultimate right-of-way
- one 17.5' travel lane in each direction
- 6' landscape planter with 5' separated sidewalk on each side
- parking is permitted

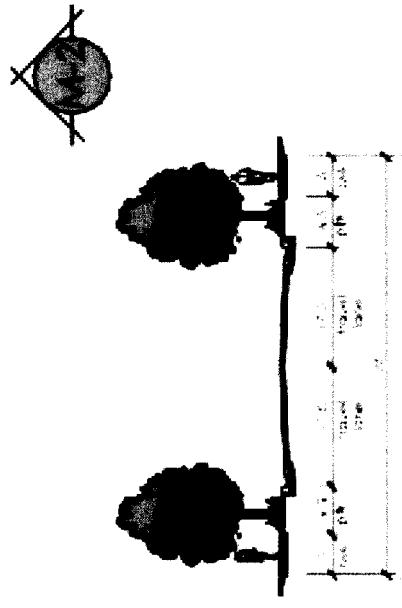


Exhibit #39 - Local Residential Street - 53' ROW

Exhibit #40 - Local Residential Street - 58' ROW

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**GREENBRIAR
PUD Guidelines**

Local Residential Street - 51.5' ROW

This local residential street is designed with the same features as the hybrid 52' R.O.W. local residential street; however, the landscape planter and separated sidewalk are located only on one side of the street adjacent to the residential uses. This local residential street is located immediately south of the community park and the distance from face of curb to face of curb is 35' instead of 30' to accommodate underground infrastructure. This locality is comprised of the following components:

- 51.5' ultralite right-of-way
- one travel lane in each direction
- 6.5' landscape planter with 5' separated sidewalk on residential side only
- 5' attached walk along the park frontage
- parking is paralled

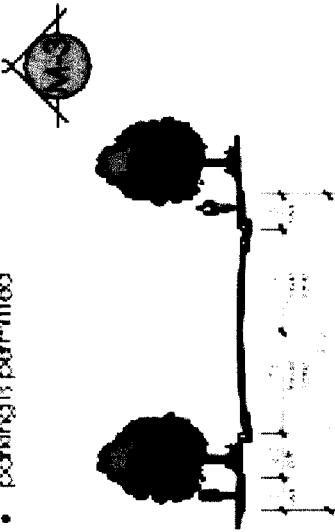


Exhibit #41 - Local Residential Street - 51.5' ROW

Local Residential Street - 46.5' ROW

This local residential street is designed with the same features as the hybrid 52' R.O.W. local residential street; however, the landscape planter and separated sidewalk are located only on one side of the street adjacent to the residential uses. This local residential street is located on the west side of the community park. This locality is comprised of the following components:

- 46.5' ultralite right-of-way
- one travel lane in each direction
- 6.5' landscape planter with 5' separated sidewalk on residential side only
- 5' detached sidewalk along the park frontage
- parking is paralled



Exhibit #42 - Local Residential Street - 46.5' ROW

**GREENBRIAR
PUD Guidelines**

Local Residential Street - 42' ROW

This local residential street is designed with the same features as the typical 33' R.O.W. local residential street; however, the landscape planter and separated sidewalk are located on the residential side of the street only. The sidewalk and planter on the other side of the street are replaced by a meandering pedestrian trail located within the open space "freeway buffer". This facility is comprised of the following components:

- 47' ultra-wide right-of-way
- one travel lane in each direction
- 6.5' landscape planter with 5' separated sidewalk on residential side only
- parking is permitted

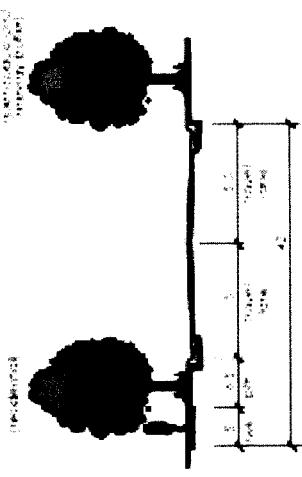
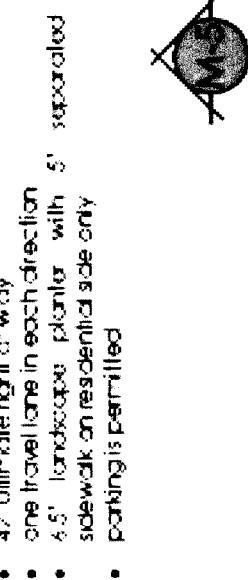


Exhibit #43 - Local Residential Street - 42' ROW

Private Drive - 22' ROW

This private drive is designed with the same features as the typical 22' R.O.W. public drive. There is 5' wide to accommodate underground infrastructure on each side. This facility is comprised of the following components:

- 22' ultra-wide right-of-way
- 3' from edge of road overhang to edge of root overhanging [includes 5' utility easement on each side]
- surfaced drained [no drain pipes]
- parking is not permitted

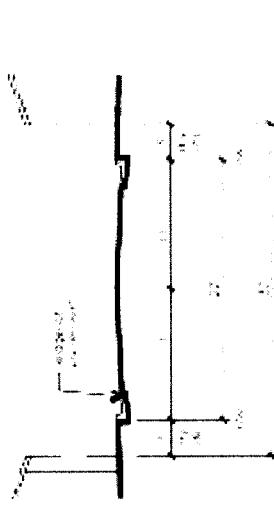


Exhibit #44 - Private Drive - 22' ROW

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**GREENBRIAR
PUD Guidelines**

Private Alley - 23' ROW

This 23' private alley is located where alley accessed residential uses do not front onto public streets but rather they front onto the lake feature infrastructure serving these residential uses will be located in the private alley. This facility is comprised of the following components:

- 23' ultimate right-of-way
- asphalt
- 33' term edge of roof overhang to edge of roof overhang (includes 5' dry utility easement on each side)
- surfaced drained (no drain pipes)
- parking is not permitted

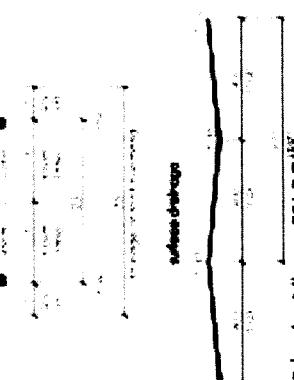


Exhibit #45 - Private Alley - 23' ROW

Public Alley - 22' ROW

This 22' public alley is located where alley accessed residential uses front onto public streets. Infrastructure serving these residential uses will be located in the public street. This facility is comprised of the following components:

- 22' ultimate right-of-way
- 32' from edge of roof overhang to edge of roof overhang (includes 5' dry utility easement on each side)
- surfaced drained (no drain pipes)
- parking is not permitted



Exhibit #46 - Public Alley - 22' ROW

22' Lakeside Section

This 22' Lakeside section is located adjacent to the lake/detention basin. This section shows the relationship between the major parts of the lakeside components and the lake. This locality is comprised of the following components:

- 22' wide pedestrian and landscaped planter
- 8' pedestrian walk [providing pedestrian circulation around the lake]
- Back yard access to lots that are adjacent to the lake

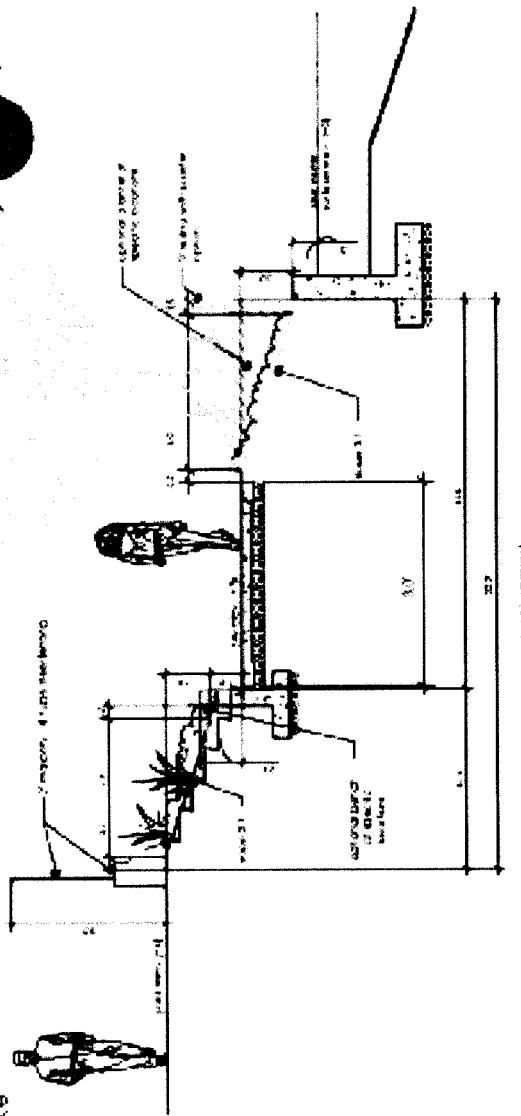


Exhibit #47 - 22' Lakeside Section

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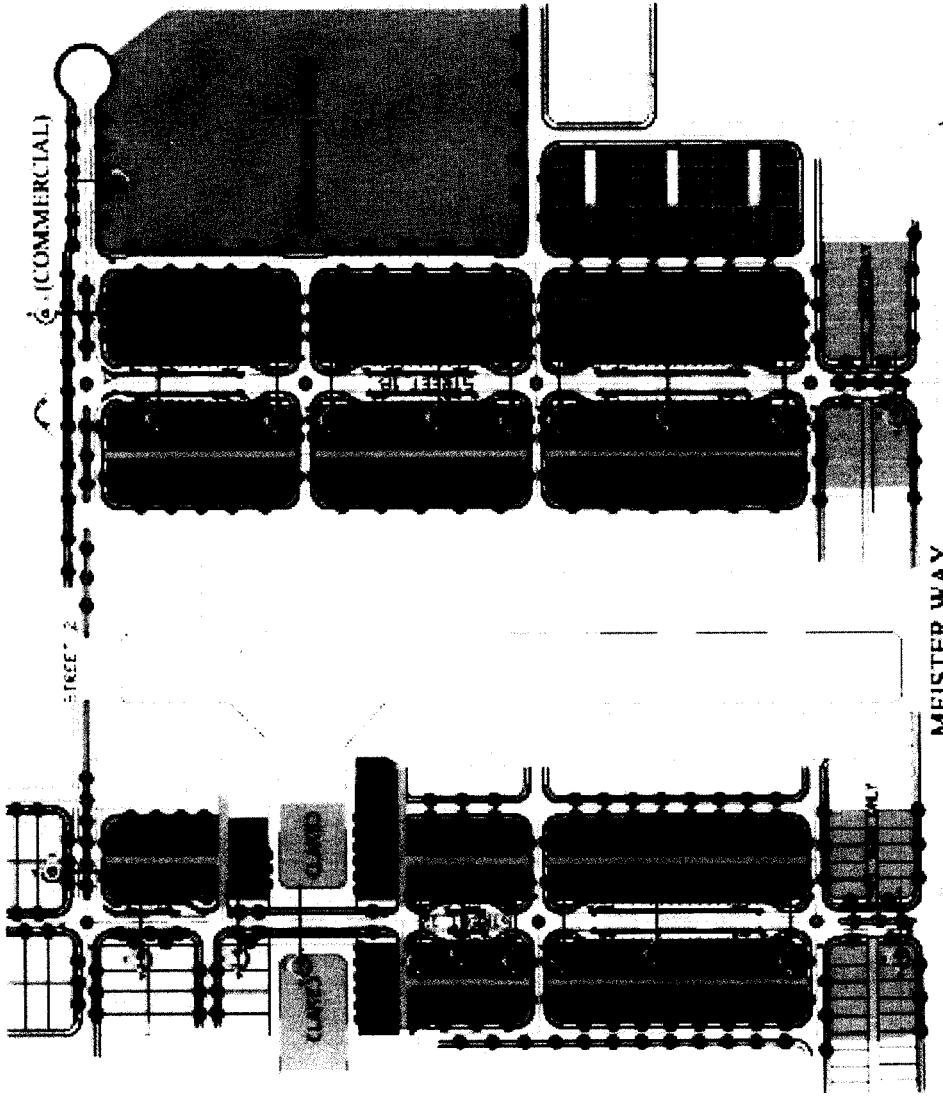


Exhibit #48 -
"Main Street"
Circulation

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located narrow lot homes, cluster development, small lots homes and traditional lots homes.

Section 3 - Housing

3.1 Principles and Objectives

3.1.a Balanced Mix of Land Uses

Greenbriar provides a balanced mix of land uses, including a variety of single family and multi-family housing types, commercial uses, parks and open space. This mix of land uses not only addresses the needs of Greenbriar residents but they also address the needs of the larger community. The entire Greenbriar community is oriented around a series of waterways and trails that provide opportunities for walking, running and recreational uses. Two duck houses serve as community gathering locations and offer further recreational uses such as pools and fitness facilities. Greenbriar is a TCO that takes advantage of the Meister Way at extension and Gwinnett Station stop centrally located within the project.

3.1.b Distinct Neighborhoods

Greenbriar's residential housing is organized into distinct neighborhoods. These neighborhoods will provide a diversity of housing types that will include apartments, senior housing, attached town homes, alley and front-



Exhibit #49 - Pedestrian Scale

**GREENBRIAR
PUD Guidelines**

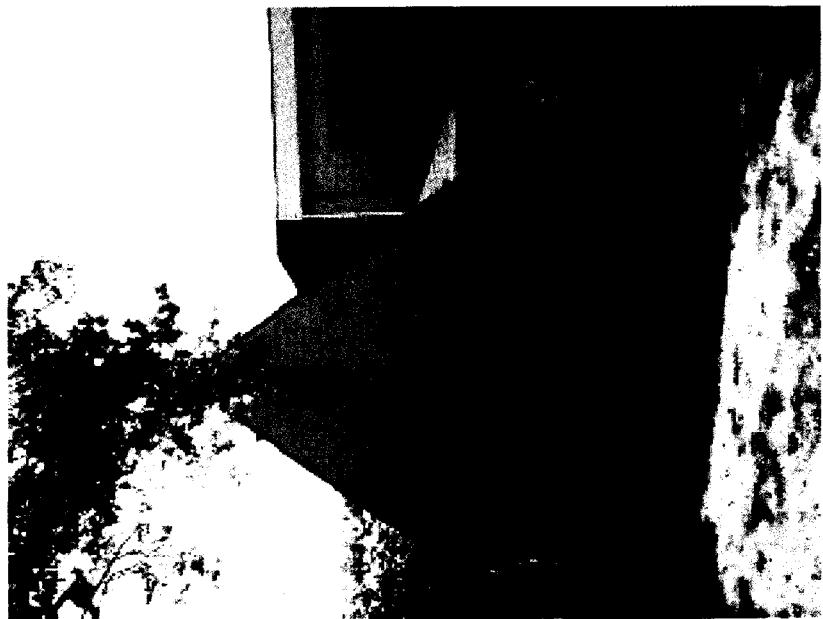


Exhibit #51 – Authentic Architecture



Exhibit #50 – Pedestrian Scale

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3.1.c Authentic Architecture

Greenbriar will distinguish itself by looking to classic Sacramento architecture for inspiration. Turn of the century neighborhoods like Curtis Park, McKinley Park and Land Park display a range of architectural styles that closely mirror by the time in which they were built. Greenbriar will borrow from these vibrant street scenes and use architectural styles that are appropriate to Sacramento.



Exhibit #52 - Authentic Architecture

3.2 Housing Alternatives

3.2.a Market Rate

The Greenbriar PUD offers a great variety and diversity of housing options dispersed throughout the plan area. The plan provides eleven different land densities and medium-density single-family lot sizes and options to meet the growing needs of the Sacramento region. These market-rate home-ownership opportunities are located throughout the PUD to appeal to an economic, age, and lifestyle diverse community of future residents. These single-family lots include both traditional and non-traditional lot sizes, alleywood lots, cluster lots and detached townhomes, and all in close proximity to the planned extension of regional transit [ST].

3.2.b Senior Housing

The Greenbriar PUD provides for multi-family uses as well as a location for senior-only allocated housing located within a short walk to the RT transit station and commercially-planned lands. The senior housing site is located across the street from the community commercial site and adjacent transit station. In recognition of the special needs of the PUD's senior population, the senior housing site was carefully planned in this location to encourage walking as an alternative option for the adjacent neighbors for their daily needs and recreation.

GREENBRIAR PUD Guidelines

This 2-to-3 story units housing site will be planned with a centrally located community garden as well as a recreation area for exercise and interaction between the residents. The architecture will be designed to blend in with the adjacent single-family residential areas. Additionally special attention will be paid to planning the crosswalk locations as well as to parking areas and drive aisle locations so as to minimize potential conflicts between residents and motorists.

3.3 Development Standards

3.3.a Density

Residential densities will vary throughout Greenbriar but will be categorized in keeping with the City of Sacramento's zoning code. See Sacramento Municipal Code Title 17, Division 1, Chapter 17.20, Zoning Districts for detailed information. Where there are discrepancies between these guidelines and the Sacramento Municipal Code, these guidelines shall prevail. Where these guidelines are silent, the Sacramento Municipal Code shall prevail.

3.3.b Single-Family Development Standards

The Greenbriar PUD provides for varied single-family residential opportunities to attract a diverse population. Seven different single-family residential home ownership opportunities are located within the PUD to appeal to an economically- and lifestyle-diverse

community of future residents. These single-family lot size/product types are listed below and described in detail on the exhibits on the following pages.

- 60'x110' Conventional [Front-coded] lots
- 55'x100' Conventional [Front-coded] lots
- 50'x100' Conventional [Front-coded] lots
- 45'x100' Conventional [Front-coded] lots
- 40'x90' Conventional [Front-coded] lots
- 40'x80' Alley-coded lots
- 35'x80' Conventional [Front-coded] lots
- 30'x70' Alley-coded lots
- Cluster/Court yards lots

- Townhomes

The Typified Development Standards Exhibits illustrated in this PUD Guidelines outline the typical lot and setback requirements needed for the single-family product types listed above. The exhibits illustrate and list particular detailed information to accommodate a particular product envisioned for the PUD area. This information may include but is not limited to the following items:

- Minimum Lot Size
- Minimum Rear Yard Lot Width
- Minimum Corner Lot Width
- Minimum Lot Depth
- Maximum Lot Coverage
- Minimum Front Yard Setback

- Minimum Garage Setback
- Minimum Rear Side Yard Setbacks
- Minimum Corner Side Yard Setbacks
- Minimum Alley / Rear Yard Setback
- Lot Type (front or rearly loaded)
- Maximum Building height
- Public Utility Boxacement

3.3.c Multi-Family Development Standards

Ch. A, C and D are subject to the residential densities of the City of Sacramento's Zoning Code, with the exception that lots C and D are permitted a maximum building height of 38' and a maximum of 4 stories. See Sacramento Municipal Code, Title 17, Division 1, Chapter 17.20.700(g) Matrix for detailed information, where there are discrepancies between these guidelines and the Sacramento Municipal Code, these guidelines shall prevail. Where these Guidelines are silent the Sacramento Municipal Code shall prevail.

3.3.d Permitted Uses

Land uses in Greenbriar shall comply with Sacramento Municipal Code land use regulation, Title 17, Division 1, Chapter 17.24, and use regulations contains detailed information about permitted uses under all conditions.

**GREENBRIAR
PUD Guidelines**

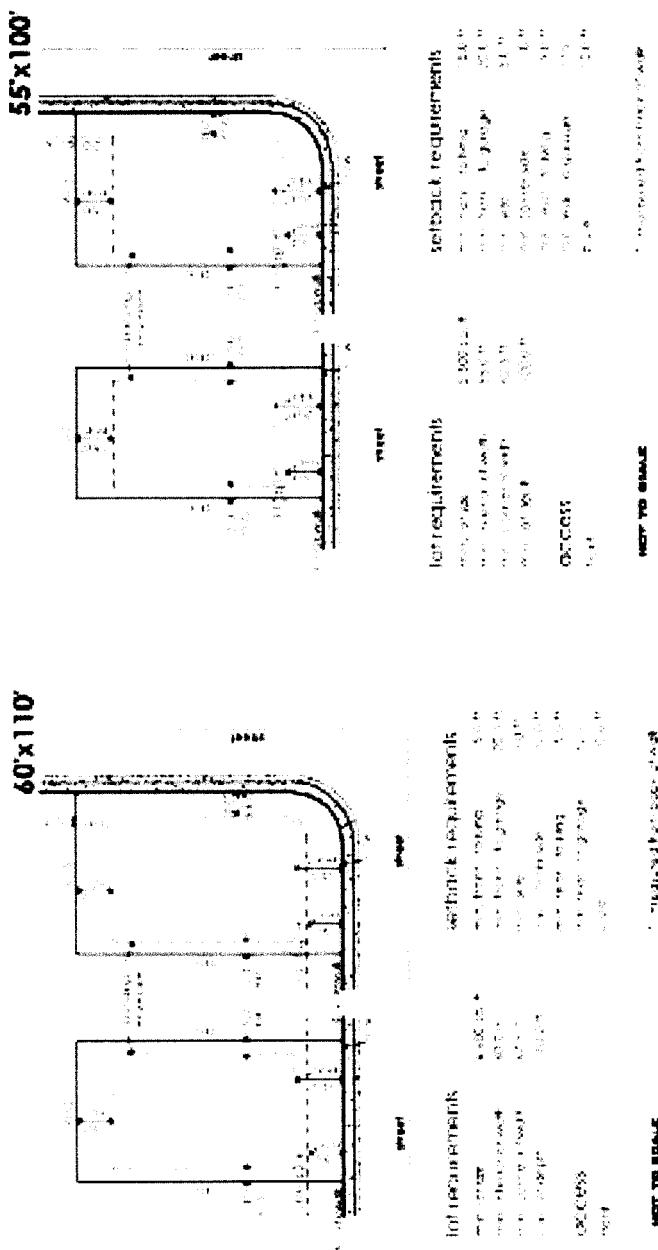


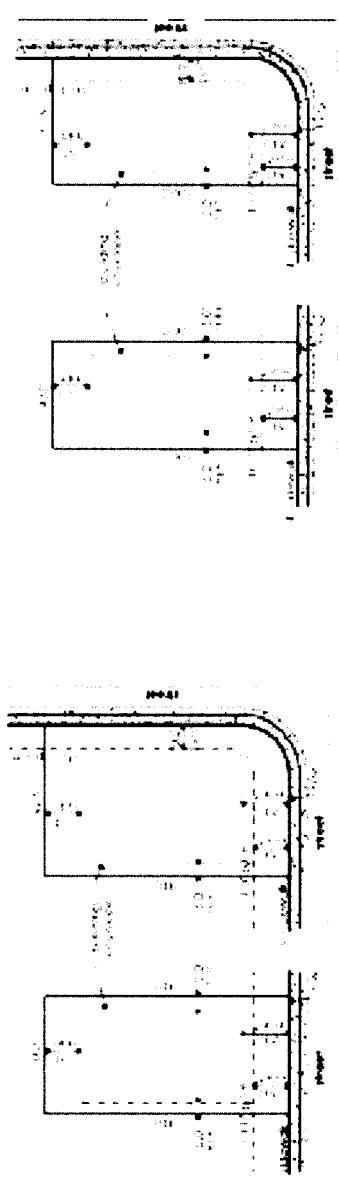
Exhibit #53 - Typical Development Exhibit 60'x110'

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**GREENBRIAR
PUD Guidelines**

Exhibit #55 – Typical Development 50'x100'



lot dimensions

lot size	width	depth
50' x 100'	50'	100'
50' x 100'	50'	100'
50' x 100'	50'	100'
50' x 100'	50'	100'

lot setbacks

setback requirement	width	depth
front	10'	50'
side	10'	50'
rear	10'	100'
end	10'	100'

setback requirements

setback requirement	width	depth
front	10'	50'
side	10'	50'
rear	10'	100'
end	10'	100'

NOTE TO DRAW

Dimensions shown are in feet.

Dimensions shown are in feet.

Exhibit #55 – Typical Development 50'x100'

Exhibit #56 – Typical Development 45'x100'

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**GREENBRIAR
PUD Guidelines**

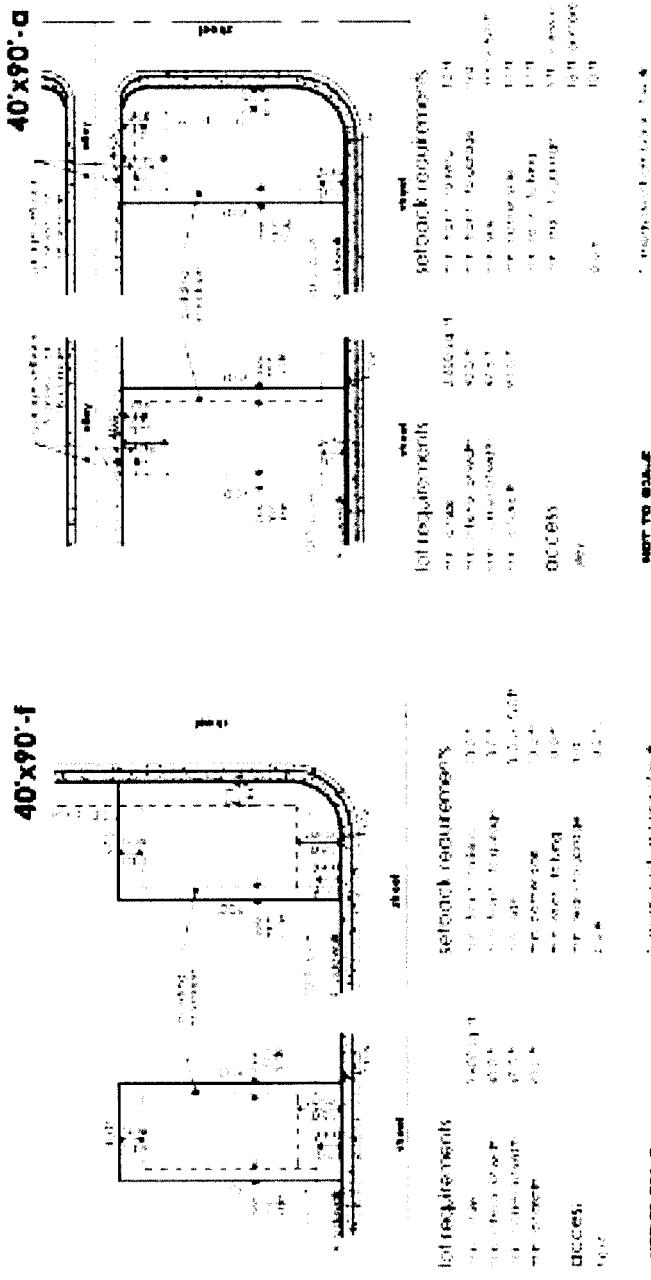


Exhibit #58 - Typical Development 40'x90'-front

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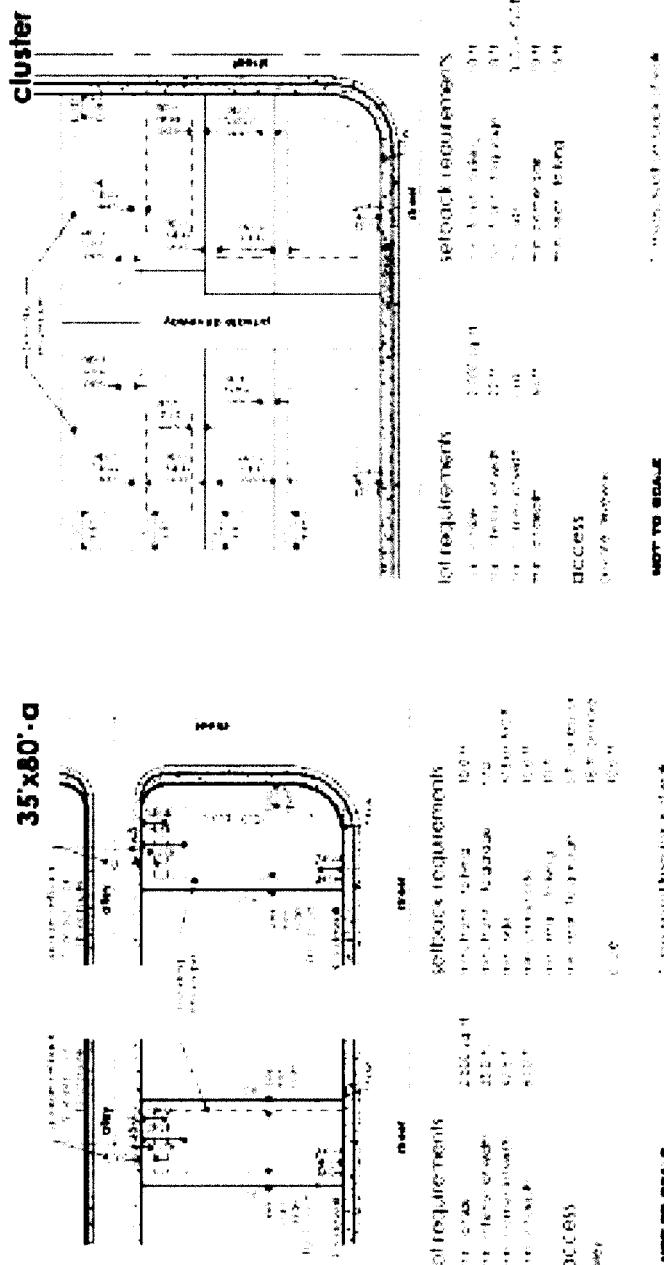


Exhibit #60 - Typical De vetopmen / Cluster

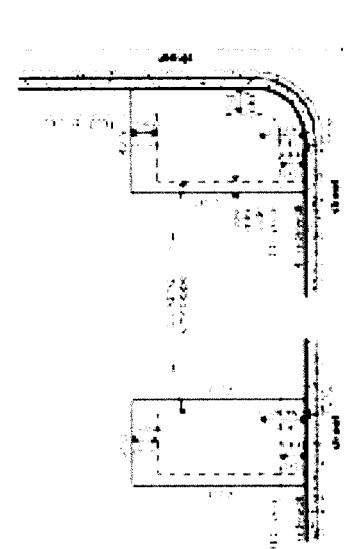
Exhibit A58 - Twinkie Devastation / 35180 - 4787

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**GREENSBRIAR
PUD Guidelines**

35'x70'-f



lot requirements

lot requirements	minimum dimensions
frontage	35'
depth or lot	70'
side property	15'
rear property	15'
garage	15' x 20'
driveway	15' x 20'
access	15'
deck	15' x 10'
porch	15' x 10'

setback requirements

setback (feet)	minimum required
front	10'
side	10'
rear	10'
garage	10'
driveway	10'
access	10'
deck	10'
porch	10'

NOTE TO SCALE:

1 foot = 100 feet

Exhibit #61 - Typical Development 35'x70'-f front

Exhibit #62 - Typical Development 30'x70'-alley

4 - Commercial Uses

4.1 Village Commercial

4.1.0 Principles and Objectives

Site planning in Greenbriar's commercial area should result in a walkable street scene along its main circulation streets that will be interesting for both pedestrians and motorists. Varied 'node' developments for each individual tenant are encouraged, but all should be complementary in sum.

Building massing should relate to the street and pedestrian walkways to invite visitors into shops. Special attention paid to pedestrian circulation, sheltered walkways, outdoor cafe seating and plaza areas that protect pedestrians from sun or inclement weather is encouraged.

Anchor and sub-anchor retail building architecture should be compatible with the entire Greenbriar community character and reflect the selected architectural styles. Building designs should incorporate a variety of scale, massing, materials and colors to minimize the visual impact of 'big box' retail spaces.

4.2 Community Commercial

4.2.0 Large Format Retail

The two large community serving retail spaces in the commercial area will serve both Greenbriar and the surrounding Sacramento community.

The largest community serving retail building (Building 4) is estimated at 145,000 square feet and will be the single largest retail building in the project. This is an appropriate size for a large home improvement store or general merchant to serve as the anchor retail tenant. It is important to break this building's 'footprint' into distinct elements as much as possible and employ the basic architectural techniques of varying color, scale and materials to avoid 'big box' mega-store' appearance.

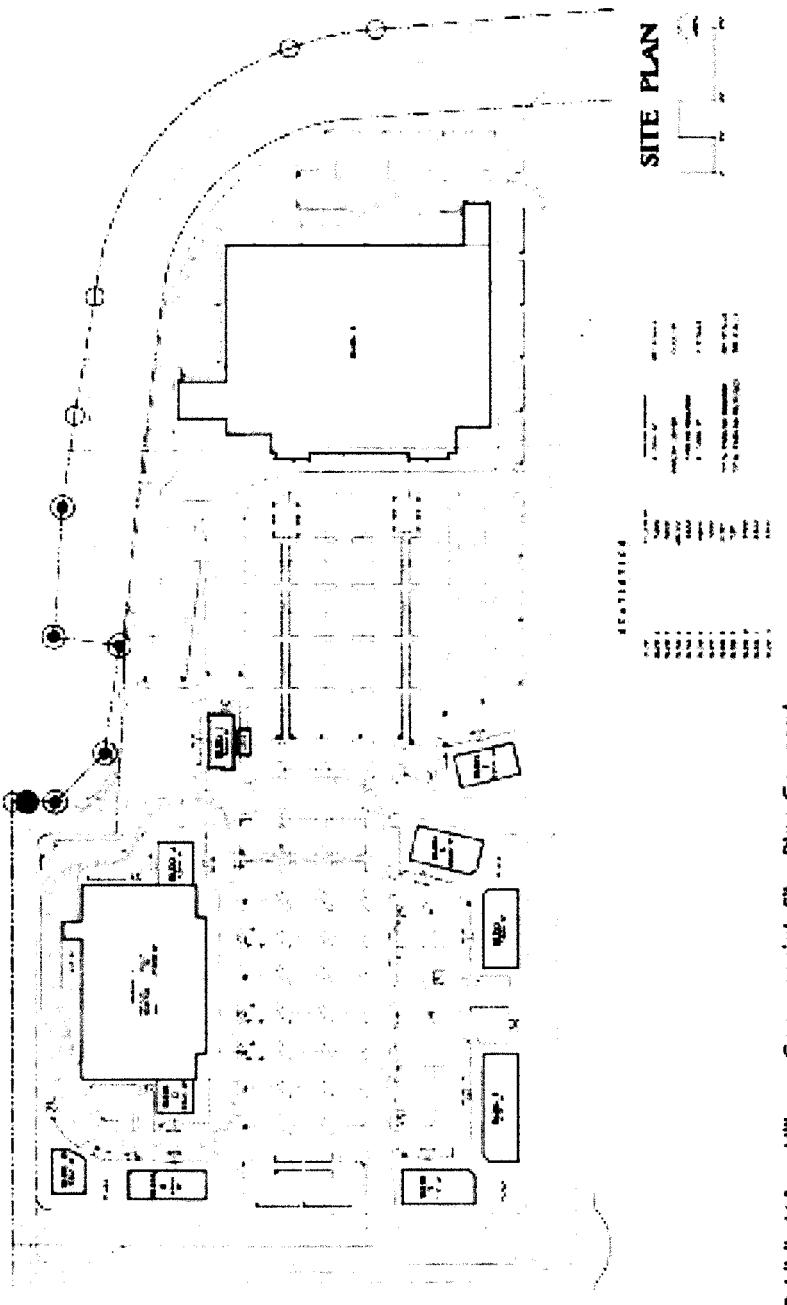


Exhibit #3 – Village Commercial Site Plan Concept Plan

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**GREENBRIAR
PUD Guidelines**

Building I is estimated at 67,000 square feet and is an appropriate size to accommodate a large community serving grocery. It may also be broken up into several sub-major hyperretailers. Similar to Building A, specific care should be taken to adjust the lines and facades of the store to the look and feel of Greenbriar's overall architecture through the use of varied massing, scaling for pedestrian friendly spaces and varying building materials.

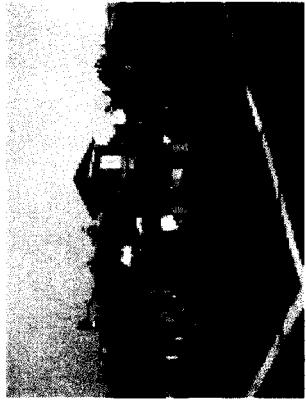


Exhibit #65 – Neighborhood Icon Commercial

4.2.b Neighborhood-Serving Commercial

Neighborhood-serving commercial spaces along the western and southern edges of the commercial area have the most potential to express individual, contextually sensitive street scenes. Whenever possible, Sacramento's classic community architecture should be reflected.

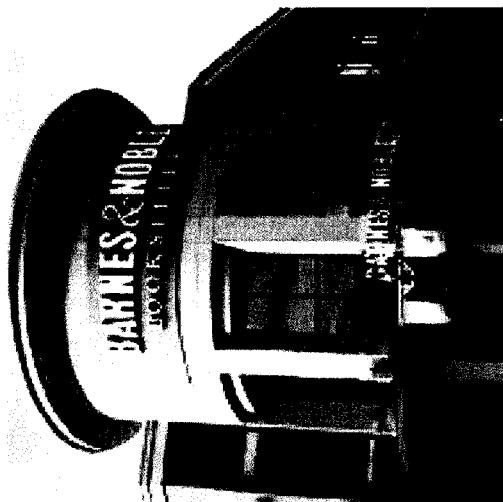


Exhibit #64 – Neighborhood-Serving Commercial

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4.3.b Massing and Architectural Treatment

Commercial Style

The overall style of the Greenbriar's commercial location should employ eclectic use of traditional materials and forms to create architecture flavor. Proportions and materials should create visually pleasing buildings able to bridge the gap between residential housing and the more modern buildings surrounding the site. Varied materials and styles within building footprints are encouraged to reflect Sacramento's classic community architecture. Food points and view corridors throughout the project should rule visual form one point to another within the project and are critical to creating an inviting human-scaled environment.

Colors

Colors should be selected to offer definition and individuality to each building and tenants in larger buildings. Bold or saturated hues are encouraged as long as they are not garish or obtrusive. Colors should bring together selected project materials and be selected to complement other stone, concrete, wood and fabrics.



Exhibit #66 - Neighborhood-Serving Commercial

Development Standards

4.3.c Roof Area Rations

Development standards are as per City of Sacramento Municipal Code

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GREEN BRICK PUD Guidelines

Roofs and roof ‘forms’ should be consistent with the overall architectural theme of the Greenbrick PUD individual roof elements placed in key locations along buildings should convey the built-over-time concept in conjunction with building form. Pedestrian areas should be enhanced by shed and gable roof* elements extending into pedestrian areas to cover and shade additional elements are allowed such as ‘dormer’ and multi-stringers, trellises, etc. Dormer elements are also encouraged for an added layer of detail and shadow.

Corices

Carice elements should be applied and should articulate basic building forms appropriately while providing differential between individual tenons. varied carice elements tend to reinforce the built over time concept. When used, carices should provide contrast of color and material to wall areas beneath. Carice elements should not be of such size or quantity that they become a dominant repellive or overwhelming architectural feature.

Wall Transitions

A variety of elements should be used to create wall transitions between buildings and lanai spaces and careful consideration should be given to walls adjacent to and oriented toward open spaces. Cedar and texture are basic elements of interest while towers and

other details may be used in some cases to frame horizontal areas. Simple, intermediate elements that break and an area of wall are encouraged. Whenever possible, color and simple traditional material changes are encouraged to break wall areas into visually pleasing proportions.

Building Corners

Building corners present an opportunity to greatly enhance visual anchoring of individual structures. Penetrating building corners or ‘toed’ points to surrounding areas within the project is encouraged. Thoughtful treatments of building corners provide changes in texture, color, and materials, as well as an opportunity to introduce windows as a simple focal detail.

Canopies and Awnings

Judicious use of canopies and awnings is encouraged. These classic architectural details add an additional layer of interest to buildings ‘ocodes’. A variety of materials may be used including canvas, corrugated metal, wood trellises and shed or gable roof forms. Canopy and awning elements should also provide cover at pedestrian walkways whenever possible. These covered elements should also be placed to encourage the play of shadows against buildings.

Towers

Tower elements may be considered as appropriate to the style of these buildings. When situated and massed properly, towers can enhance visual interest. These elements can serve as a connection between individual buildings or focal points and transitioned spaces. Towers should provide a change in scale, color and material, and use windows as well.

Vertical elements should not be limited to towers. The appropriate and tasteful use of chimney elements and friezes is also encouraged.

Windows

Shape, size and placement of windows are important elements that lend positive yet simple character to the overall theme of the project. Window size and proportion should be appropriate to individual building style. Window frames may vary between individual tenant spaces and buildings to subtly reflect the building's overall conceptual. Windows, especially at a pedestrian level, are encouraged in overall building design. Consideration of design elements like shutters, cornices, recesses, iron and other elements should be used to enhance windows and add variety.

Building Materials

Materials should reflect the style and overall impression of any building. Materials used for this project should also reflect quality, care and tend to the impression of overall theme.

**GREENBRIAR
PUD Guidelines**

Encouraged Materials

- Interest and creativity in building design is encouraged with creative and more traditional approaches to building form and articulation will provide variety, interest and visual appropriateness. These selectively chosen encouraged materials include:
 - Smooth stucco friezes
 - Style appropriate stone
 - Wrought iron
 - Complementary-colored cornices, eaves
 - Wood trusses
 - Flat roof elements
 - Skin-face block
 - Wood columns and beams in key location
 - Pre-cast stone trim, headers and sills
 - Metal roof elements
 - Decorative sheet metal gutters and downspouts.
 - Collectors!
 - Wood shutters
 - Individually articulated window elements
 - Tilt-up construction that uses imaginative forming techniques to add texture and shadow to otherwise untextured walls

Discouraged Materials

- Heavy "knock-down" / Spanish oak stucco friezes
- Continued stone veneers
- Unfinished tilt-up wall panels
- Large unbroken window walls
- Exposed pre-cast flat concrete block walls
- Exposed aggregate walls
- Any architectured wood element may be substituted for high quality terra cotta or other manufactured elements as long as quality is maintained and no depature are made from these architectured style guidelines.

**GREENBRIAR
PUD Guidelines**

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section 5 - Parks, Open Space and Habitat Conservation

5.1 Greenbriar Parks

The Greenbriar PUD Schematic Plan provides many varied recreation opportunities in the location of both publicly and privately-owned active and passive parks and open spaces. One large Community Park, three neighborhood parks, three private parks, two private recreation centers, and numerous pocket parks throughout the project and marina parks, ranging in size, and an elementary school are located within the PUD and are well-linked by residential streets and trails. Additionally, the PUD boasts an expansive lake with a pedestrian-friendly, long, non-gated, lakeside or promenade around the entire perimeter of the lake as well as a wide, tree-lined buffer/open space corridor along the south and east project boundaries, enhanced with pedestrian trails and landscaping.

The PUD Schematic Plan also includes a wide wildlife corridor/buffer on the western project boundary that will be privately maintained in perpetuity for the preservation of threatened and/or endangered species. Due to the possible presence of rare or

threatened species, physical access to the habitat buffer will be restricted and an on-going maintenance plan will be established. Interpretive signage regarding the habitat buffer's special site characteristics, plant and animal inhabitants and on-going maintenance efforts may be posted to promote the education of the residents and the larger community.

The Overall Parks and Open Space Plan highlights the inclusion of Greenbriar Lake that will occur middate the sites overall drainage needs in a constructed linear lake feature. Greenbriar Lake winds through the PUD area and provides community-wide open space and a major regional pedestrian and bicycle circulation and recreation facility. The drainage corridor will be designed to efficiently carry town through the site as well as be an aesthetically pleasing "naturalized" lake corridor. Alongside the lake is a 22' wide lakeside or promenade that will extend along the entire perimeter of the lake and will provide circulation and recreation facilities to PUD residents.

The following pages illustrate the Overall parks and Open Space Plan and highlights the locations of the public facilities versus the private facilities. Additionally, the following pages describe and illustrate the location of various PUD area freeway and habitat buffers as well as their intended uses(s) and access opportunities and constraints.

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**GENERIC
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Overall park & open space exhibit

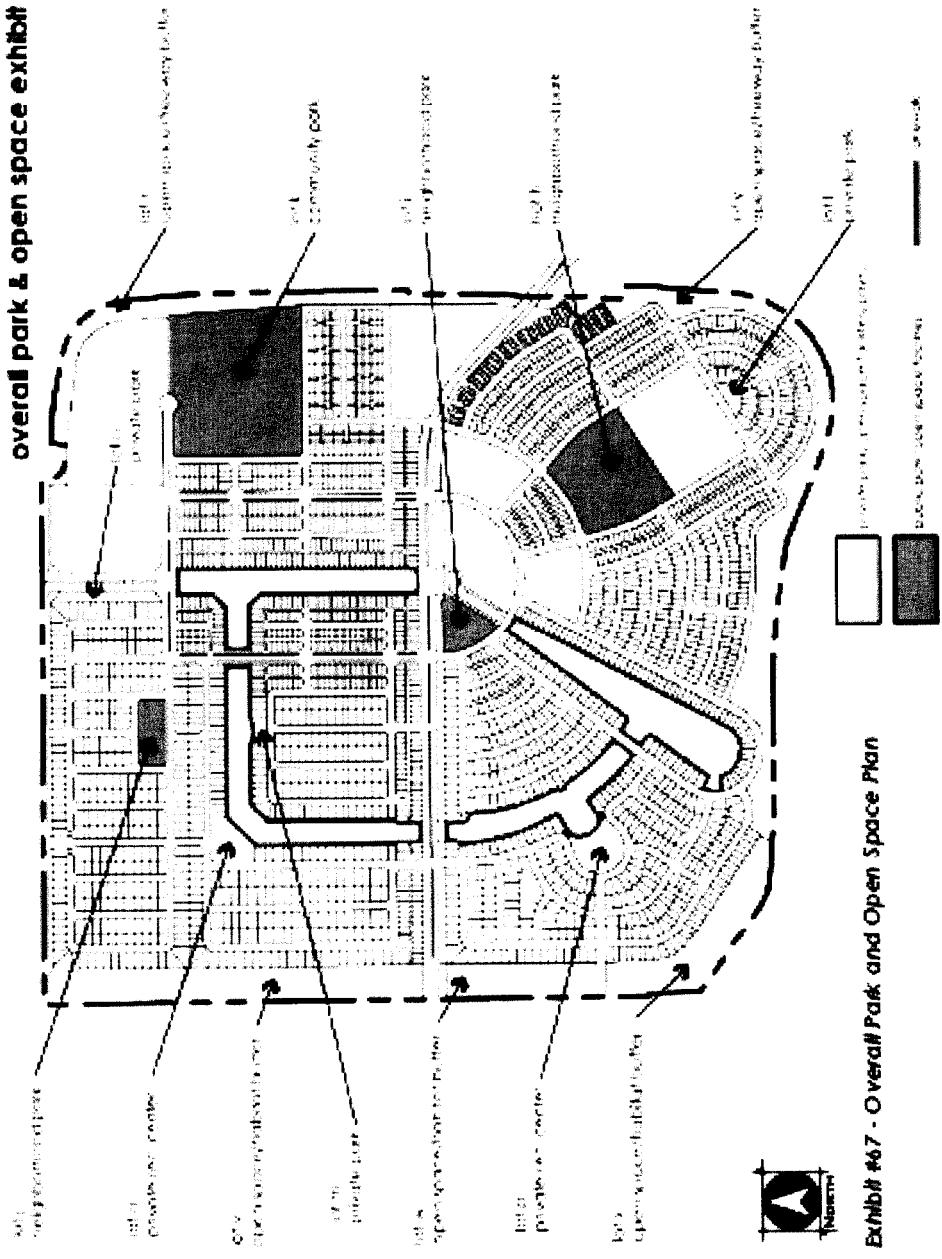


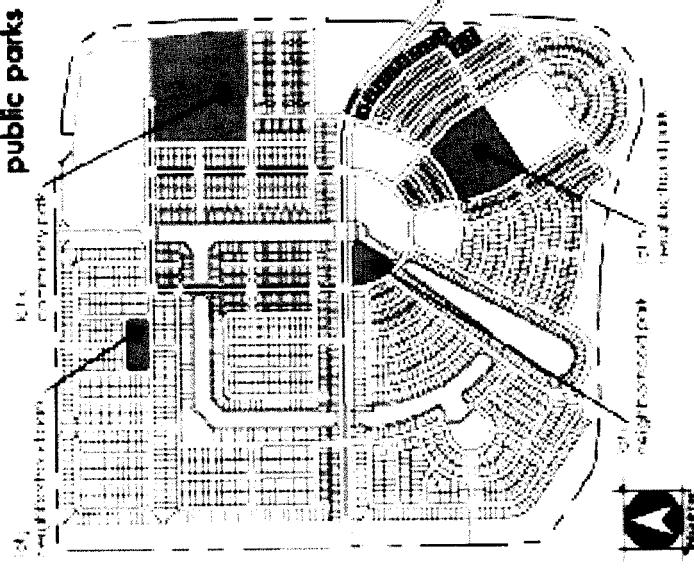
Exhibit #67 - Overall Park and Open Space Plan

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**GREENBRIAR
PUD Guidelines**

public parks



5.1.6 Public Parks

The Greenbriar PUD proposes a variety of public park land within the PUD. This includes a total of 4 park sites including two small neighborhood parks, a large neighborhood park co-located adjacent the elementary school site and a large community park.

The parks will be developed in accordance with park master plans developed by the Department of Parks and Recreation, Parks Planning and Development Services (PPDS). Future park plans may include, but are not limited to, ball lot picnic areas, large open fields, ball fields, parking lots, restrooms, structures and lighted baseball fields. In addition, the community park will incorporate a 6' wide multi-use trail (12' wide trail with 2' shoulders) that will meander throughout the park site and will connect to the exisiting trail system located in the open space/freeway buffer. Landscaping along the multi-use trail will include a combination of shrubs, groundcover, and trees. Vehicular access controls will be placed at the entrance to all trail access points to prohibit automobile access, yet allow maintenance and emergency vehicle access to the trail.

EXHIBIT #68 - Public Parks

parks & recreation facilities

5.1b HOA Private Parks

The Greenbriar PUD proposes a variety of private park and recreation lands within the PUD. This includes a total of 5 park sites including three trail pocket parks and two private recreation center sites.

The parks will be constructed by the developer and maintained by a homeowners' association (HOA). Future private park and recreation center site plans may include, but are not limited to, ball fields and picnic grounds, trails and grassy open areas. Two of the sites will also incorporate such uses as recreation center buildings with kitchens, banquette rooms and bathrooms, fitness areas and swimming pools.

Three of the five private park sites are thoughtfully planned at lake's edge to encourage neighbors to use the lake walk trail to gain access to the sites. The lake walk amenities are described in more detail in the following section. Landscaping of these private facilities will include a combination of groundcover, trees and bank mulch.

The recreation center facilities are provided for use by all residents within the PUD however will be restricted from use by the general public. The private parks and recreation sites contribute to the PUD's overall parkland designation requirement as described in the previous section.

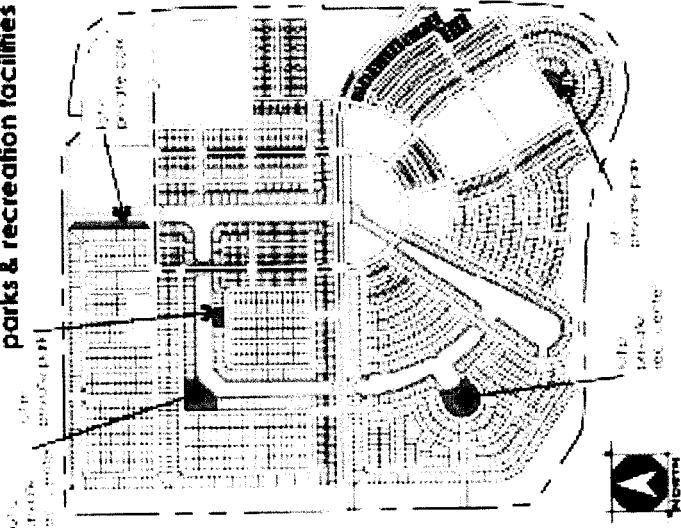


Exhibit #69 - Private Parks and Recreation Facilities

GREENBRIAR PUD Guidelines

5.2 Greenbriar Lake

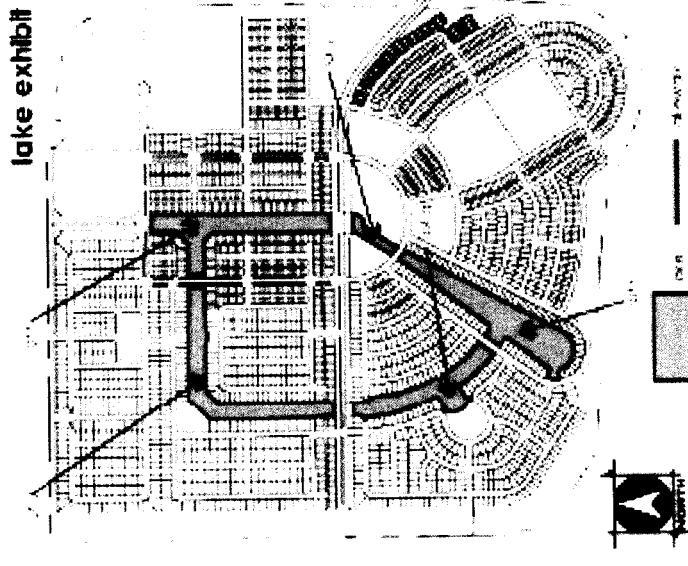
A signature feature of the PUD area is the inclusion of a large lake feature that winds through the site creating a sense of place and neighborhood orientation mechanism(s) for the community. In addition to its aesthetic qualities, the lake will provide a stormwater detention and water supply functions for the PUD. The Greenbriar Lake feature encompasses approximately 40 net acres in land area and is located in the western and central portion of the plan area.

Nearly all of the single-family residential housing products offered in the Greenbriar PUD have direct access to the lake in terms of adjacency or locating model home sites. The lake is virtually surrounded by single-family residential housing in both back-on conventional situations as well as front-on alley-loaded configurations. Additionally, the PUD's two private recreation facilities, various park facilities and the Community Centered site are also sited along the lake's shoreline.

A key feature of the PUD is the inclusion of a wide lakeside or pedestrian promenade. This 22' lakeside section is located adjacent to Greenbriar Lake and will provide residents with a safe and convenient non-vehicular route through and within the plan area. This overall 22' wide lakeside facility is comprised of the following components including off-street minimum

pedestrian walk providing pedestrian circulation around the lake, lush landscaping and benches located where appropriate and backyard access (and laneway access in some cases) for lots that are adjacent to the lake.

Exhibit #70 - Greenbriar Lake



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**GREENBRIAR
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pedestrian paseo concepts

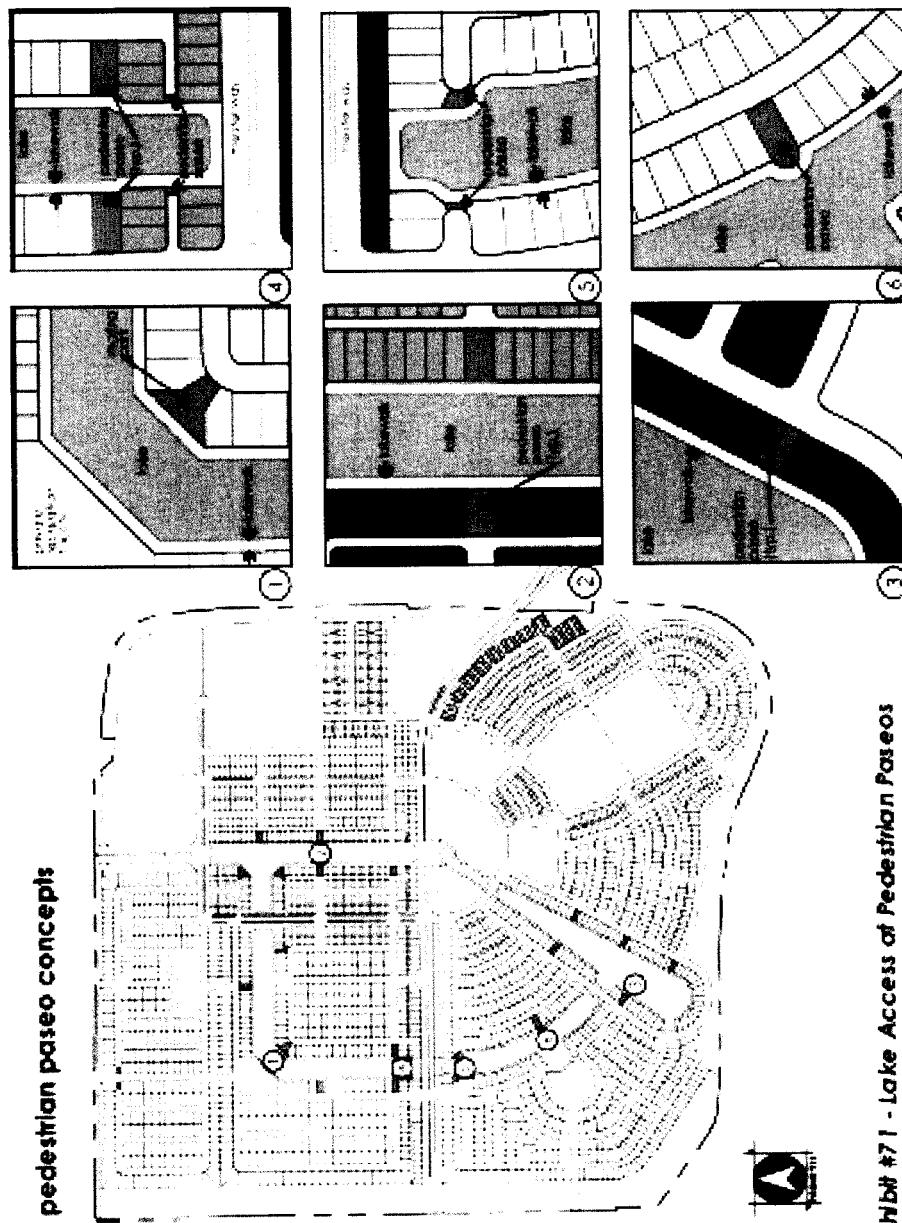


Exhibit #71 - Lake Access of Pedestrian Paseos

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GREENBRIAR PUD Guidelines

5.2.9 Lake Access and Pedestrian Passes
Greenbriar Lake is encircled along its entire Yarborough by a 72' wide lakewalk or pedestrian boardwalk that is envisioned as a fully landscaped area with a wide 8' minimum pedestrian/bicycle trail bordering the walking and people-watching and lots of shade trees, shrubs and groundcover.

Plant materials along the lake Yarborough and lake access passes will be carefully planned to maximize visibility and variety as well as to be "waterowl resistant" to minimize the harborage and "trapping" of Sacramento's local waterowl species. Large tree species will be chosen for their evergreen and deciduous qualities as well as for their growth habit and maintenance requirements. Deciduous shade trees will be primarily chosen to provide cooling shade during the summer months yet allow the winter sun to penetrate the lakewalk trail in the summer thereby providing a temperate climate for walking and biking enthusiasts year-round.

Pedestrian access to the promenade will be from a series of pedestrian passes and marina parks located at regular intervals within the adjacent residential neighborhoods. These pedestrian access lots are envisioned to be well landscaped areas with paths, benches, shade structures and similar amenities. To facilitate east/west access across the northern portion of Greenbriar Lake, pedestrian bridges are planned

that will connect, orient and unify the community. The bridges will extend across the lake terminating at the pedestrian passes or marina parks and will be a signature architectural feature of the PUD area.

5.3 Freeway Buffer/Open Space

Consistent with development to the east within the North Natomas Community plan, the Greenbriar PUD provides for a large open space/freeway buffer along the south and east sides of the plan area.

The open space/freeway buffer occupies 100' in width and will be developed in accordance with trail guidelines developed by 2003 and will be walk landscaped with trees, shrubs and groundcovers, and possibly with turf in some areas as well. A 16' wide multi-use trail will be provided along the entire length of the open space/freeway buffer that will provide off-street recreation from the south area of the plan up to the community park, and eventually beyond the plan area north of Elkham Boulevard. Future improvement plans will include a grade-separated crossing under the Master Way freeway overcrossing for the expeditious and uninterrupted north-south movement of cyclists. Access controls will be placed at the entrance to all trail access points to prohibit automobile access, yet allow maintenance and emergency vehicle access.

GREENBRIAR PUD Guidelines

5.4 Wildlife Corridor/Habitat Buffer

The west side of the plan area contains a drainage swale referred to as the One Tree Corridor that is home and habitat to California Giant Garter Snake. The plan area provides for a large open space habitat buffer along the west side of the plan for the conservation of site's federally protected species and their associated upland habitat.

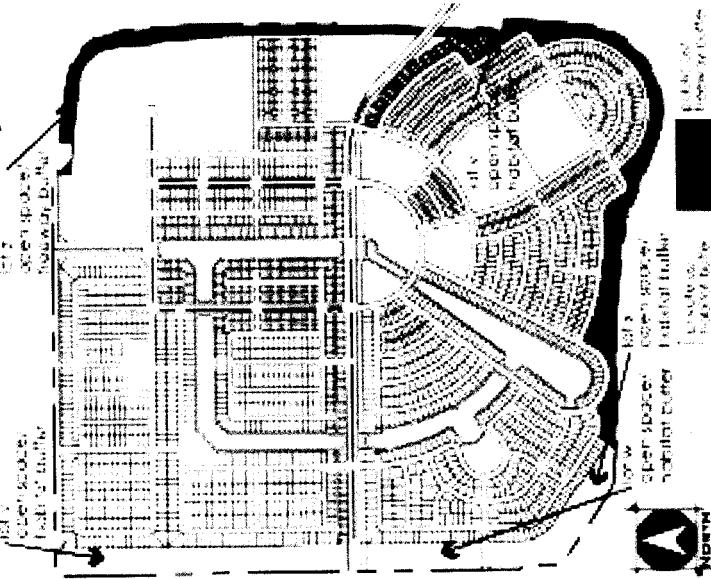
The Greenbriar PUD provides for on-site preservation and habitat protection of an approximately 250-acre wide corridor that includes the corral and approximately 200' 'ool' of adjacent uplands. The uplands within the One Tree Corridor will be converted to and managed as perennial grassland for the sole purpose of preserving the species' additional aquatic habitat for the giant garter snake will be created along the east bank of the corral.

The developed portion of the Greenbriar PUD will be designed to minimize potential conflicts with, and obstacles for, the movement and preservation of the species. Steel crossings over the corral have been minimized and the placement of ornamental wall/fence barrier will be installed between the residential uses and the open space/habitat buffer to ensure that the giant garter snakes do not stray into the development area and to prevent people and pets from entering into the preserve. This preservation is part

of the proposed specific habitat Conservation Plan (HCP) and will provide for, in perpetuity under a conservation easement, on-going north-south connectivity for the species movement and longevity.

Exhibit #72 - Freeway Buffer/Habitat Buffer

habitat buffers & freeway buffers



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section 6 -Implementation

6.1 Phasing

Construction of off-site infrastructure, on-site infrastructure and individual village development shall be phased in accordance with the Greenbriar Development Agreement Conditions of Approval and Finance Plan. Development of individual parcels and supporting infrastructure (i.e. roadways and utilities) may occur simultaneously or sequentially. The timing sequence shall be determined by economic and market conditions.

In the event Greenbriar development is constructed prior to an adjacent development, provisions for necessary infrastructure will be made in accordance with the Development Agreement. This will include provisions for construction of roadways to provide access to the development and the extension of necessary utilities and infrastructure such as water, sanitary sewer and storm drain facilities necessary to the development.

phasing exhibit

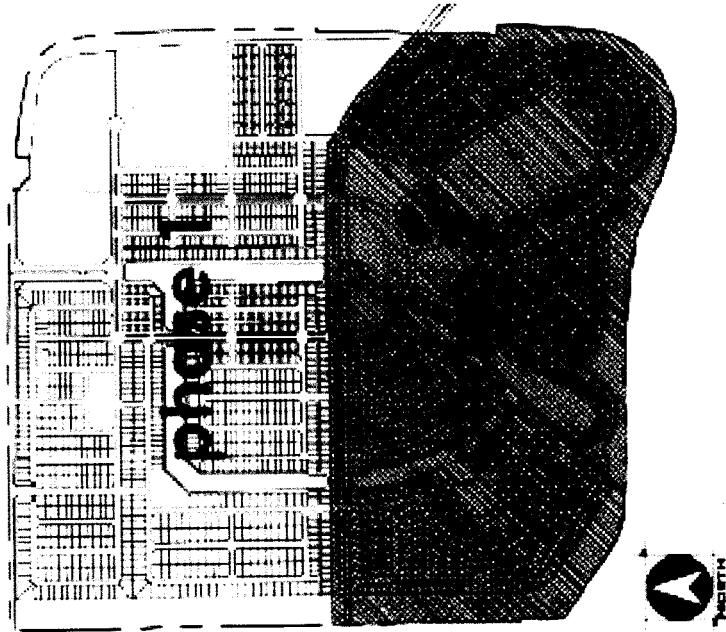


Exhibit #73 - Phasing Diagram

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GREENBRIAR PUD Guidelines

6.2 Master Developer Review Process

All development within the PUD area will be subject to Design review by the Greenbriar Design Review Committee. The purpose of the design review process is to ensure proper implementation and consistency with the intent of the Greenbriar PUD Guidelines including the future Section 7 Design Guidelines.

6.2.a Design Review Committee

All project proposals within the Greenbriar PUD must be submitted for review and approval by the Design Review Committee (DRC) prior to submittal to the City of Sooke for a site plan approval.

6.2.b Design Review Committee

A five [5] member Design Review Committee (DRC) will be established. The members of which will initially be appointed by the Developer. Ultimately, as the Greenbriar PUD is built out, the Developer's interests and control of the Design Review Committee will be replaced by members of the Greenbriar community. However initially the DRC members will be as follows:

- Three [3] of large members of the DEC will be appointed by the Developer
- Two [2] design professionals (i.e., planners, architects, landscape architects or other related consultants) appointed by the Developer
- For the purpose of conducting design review committee meetings, the chairperson will be selected from the committee by the members.

Other non-voting individuals may also attend meetings of the DEC.

Verification of Completed Review

The Design Review Committee shall issue a letter of either stamp of approval for the applicant to submit with the plans to the City of Sooke until further approvals.

6.3 Consistency Review and Planning Director Discretion

Specific applications for individual residential lots within the project shall be processed as follows: [1] The Planning Director shall review each individual application for consistency with both his Greenbriar PUD Guidelines and the Greater Victoria Residential Design Guidelines document. Lotting use and design standards shall be applied to each proposal to ensure consistency with the overall character envisioned for the PUD and/or individual neighbourhoods; [2] An application fails to meet the standards established in the Greenbriar PUD Guidelines and/or the Greenbriar Residential Design Guidelines document(s), the application shall be processed at the Planning Commission level. This protocol is intended to facilitate streamlined processing for proposals consistent with both the Greenbriar PUD Guidelines and Design Guidelines. It also permits flexibility and planning commission oversight for non-

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modified proposals, and proposals determined to substantially deviate from the standards prescribed in both documents. No portion of this protocol relies upon the applicability of the City's current or future Multi-family and/or single-family design principles documents.

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section 7 - Design Guidelines

7.1 Single-Family Residential Design

7.2 Multi-Family Design

7.3 Commercial Design

7.4 Transit Village Design

NOTE:

As stated in section 1.2 [future subsection] of these PUD Guidelines (and restated here for clarity) this PUD Guidelines document acknowledges that the City Planning Commission and City Council shall review, and the City Council shall consider for adoption, a future chapter of these design guidelines [this section / future chapter of these design guidelines] which is intended to prescribe detailed and comprehensive design standards addressing single-family and multi-family residential design, commercial design, and transit village design. In addition to these subjects, the Design Guidelines may include other optional topics as needed such as walk, signage and landscape, or public facility and park and open space design to describe and convey the overall design concept and implementation of the PUD's vision.

The timing for compilation and subsequent approval of the future section 7 Design Guidelines is intended to be coordinated with the City's review and consideration of the Greenbriar Master Zoned Map, Tentative Subdivision Map and Development Agreement.

G R E E N B R I A R
PUD Guidelines

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