



# REPORT TO COUNCIL 12 City of Sacramento

915 I Street, Sacramento, CA 95814-2604  
www. CityofSacramento.org

CONSENT  
February 19, 2008

Honorable Mayor and  
Members of the City Council

**Title:** El Camino Walgreens (P07-116)

**Location/Council District:** 830 El Camino Avenue, APN 275-0054-007, 275-0054-025, 275-0054-026, & 275-0054-027, (District 2)

**Recommendation:** 1) Review: a) a **Resolution** approving the environmental exemption under California Environmental Quality Act (CEQA) Guidelines Section 15332 (Infill Developments); b) an **Ordinance** approving a rezoning from Standard Single-Family (R-1) to General Commercial (C-2) relating to the El Camino Walgreens project; c) a **Resolution** approving a Special Permit and Variances for the El Camino Walgreens project; and 2) pass for publication the Ordinance title as required by the Sacramento City Charter 32c to be adopted February 26, 2008.

**Contact:** Steven Kerr, Junior Planner, (916) 808-5416; Stacia Cosgrove, Senior Planner, (916) 808-7110.

**Presenters:** Not Applicable

**Department:** Development Services

**Division:** Current Planning

**Organization No:** 4885

## Description/Analysis

**Issue:** The applicant is requesting approval of entitlements to allow the construction of an 11,420 square foot retail store with a pharmacy and drive-through service on 1.26± acres. The proposed project will require the demolition of an existing building, which is currently vacant and was previously used for auto sales. The applicant proposes to merge three parcels into one parcel which requires a rezone of a portion of the project site from Standard Single-Family (R-1) to General Commercial (C-2) to create one uniformly zoned parcel which is consistent with the intended use.

**Policy Considerations:** The project is consistent with the General Plan Update

Vision and Guiding Principles, and the General and Community Plan by providing redevelopment of underutilized infill land along a transportation and commercial corridor which will offer goods and services for the daily needs of adjacent residential areas.

**Committee/Commission Action:** The Planning Commission heard the project on February 14, 2008 and forwarded a recommendation for approval for the entitlements for the El Camino Walgreens project. The Design Review staff approved the project on January 29, 2008.

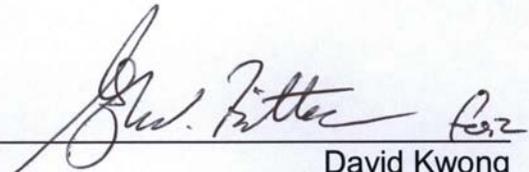
**Environmental Considerations:** The Development Services Department, Environmental Planning Services Division has reviewed this project and determined that it is exempt from the provisions of the California Environmental Quality Act (CEQA) under Section 15332 (Class 32 Infill Development) which consists of a project that is consistent with the General Plan and zoning regulations, is located within the City limits, is located on a project site of not greater than five acres in size substantially surrounded by urban uses and that has no habitat value, will not have significant effects relating to traffic, noise, air quality, or water quality and can be adequately served by utilities and public services.

**Rationale for Recommendation:** Staff supports the request to rezone from R-1 to C-2 since it will provide consistency between the zoning and the proposed use. This recommendation for approval is based on the project's consistency with the General Plan policies encouraging commercial uses which offer goods and services for the daily needs of the adjacent residential areas as well as the General Plan Update Vision which promotes strategic development of vacant, underutilized, and infill land, especially along transportation and commercial corridors, to improve the city's economic outlook.

**Financial Considerations:** This project has no fiscal considerations.

**Emerging Small Business Development (ESBD):** No goods or services are being purchased under this report.

Respectfully Submitted by:



David Kwong  
Planning Manager

Approved by: William Thomas  
William Thomas  
Director of Development Services

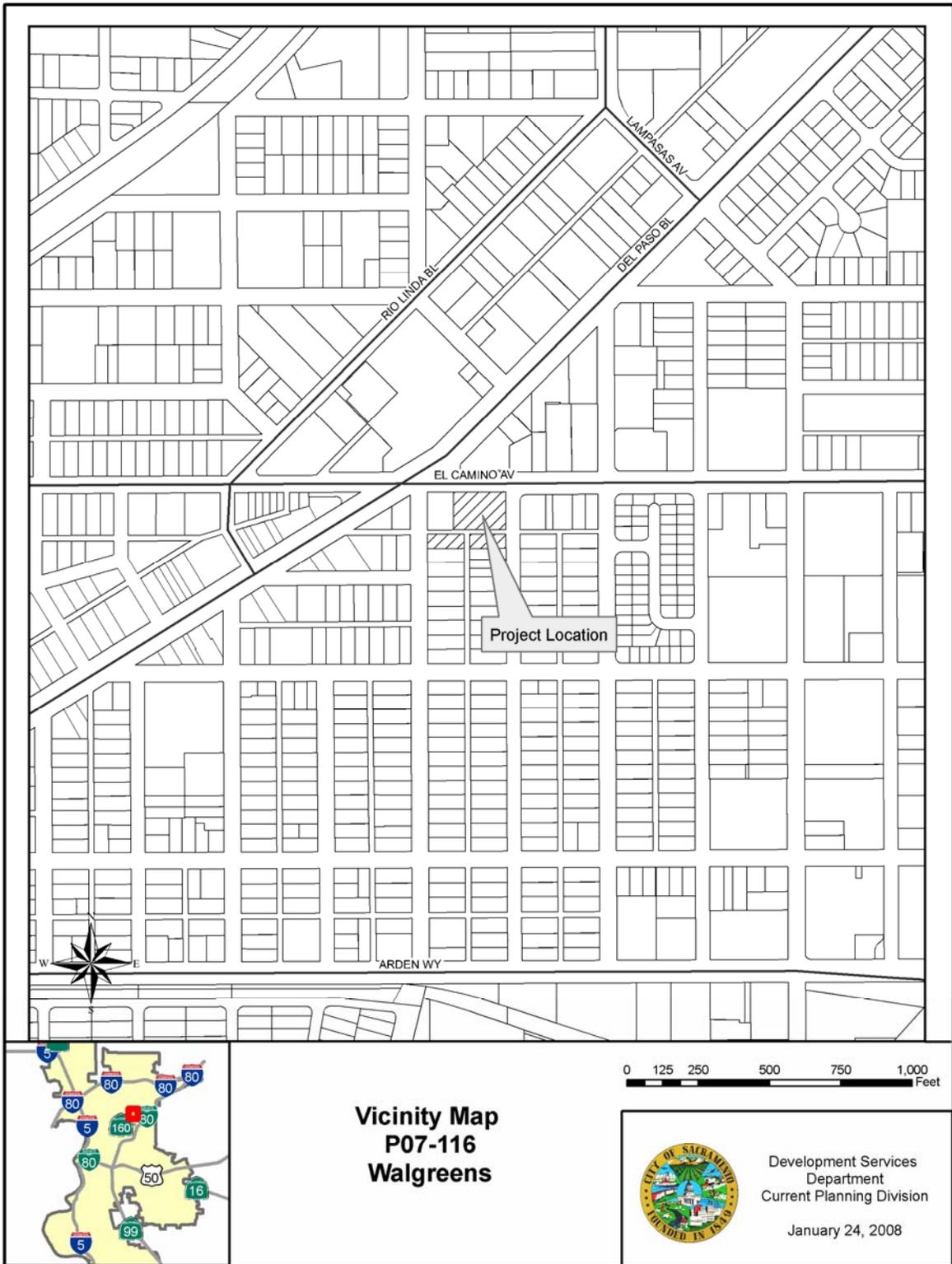
Recommendation Approved:

*for* Ray Kerridge  
Ray Kerridge  
City Manager

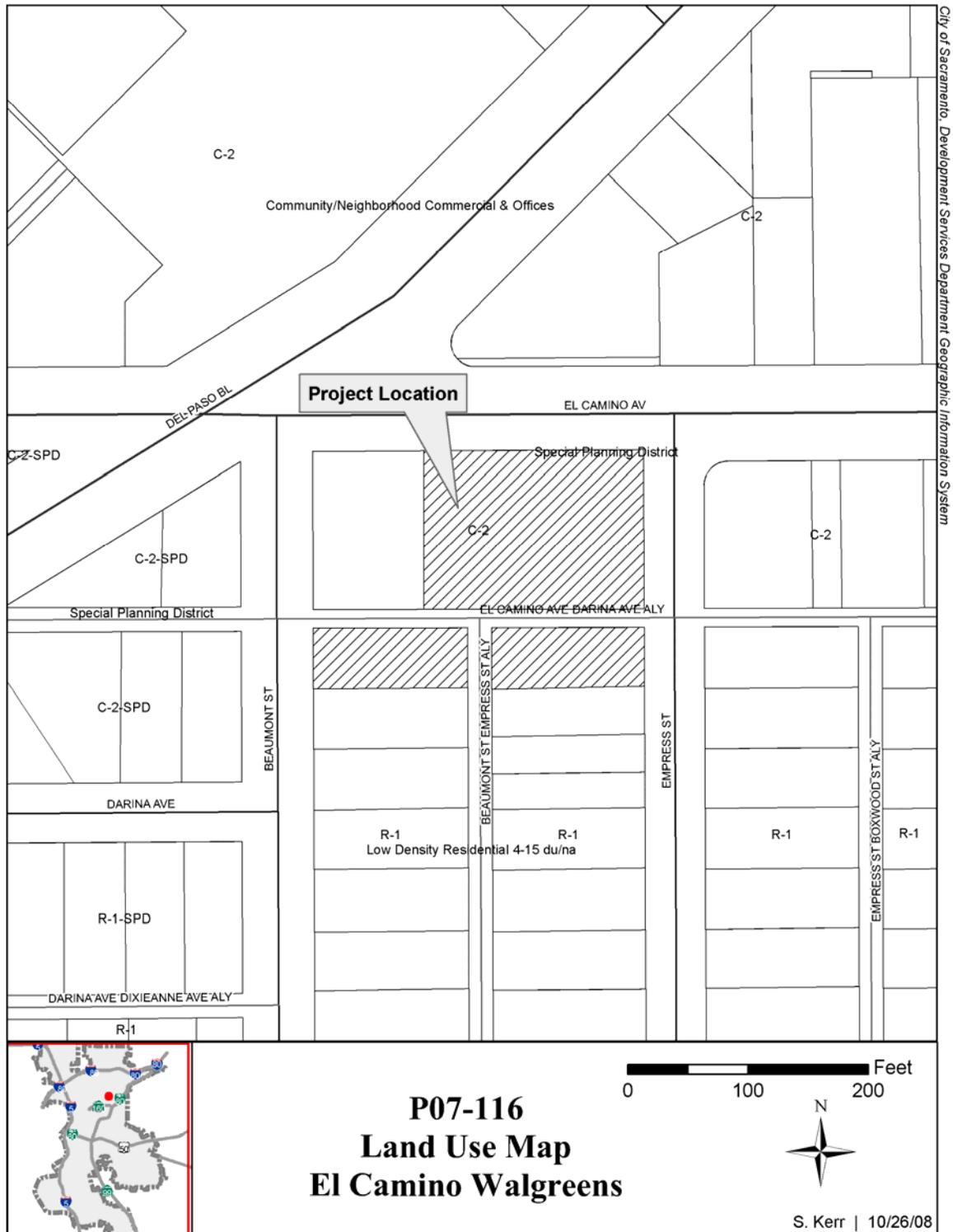
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**Attachment 1 – Vicinity Map**



Attachment 2 – Land Use and Zoning Map



**Attachment 3 – Background**

**Background Information:** The applicant is requesting approval of entitlements to allow the construction of an 11,420 square foot retail store with a pharmacy and drive through service. Staff notified all property owners within 500 feet of the site for this public hearing and received no opposition from the surrounding neighborhood. The proposed project will require the demolition of an existing building, which is currently vacant and was previously used for auto sales.

The applicant is requesting the approval of entitlements which include rezoning a portion of the property from the Standard Single-Family (R-1) zone to the General Commercial (C-2) zone, a Special Permit for Drive Through service, and Variances to reduce the required front and street-side setbacks, to reduce the required vehicle stacking distance for a drive-through, and to reduce the required drive aisle maneuvering width.

The two currently residential zoned parcels within the proposed project site were previously developed as off-site parking for the adjacent uses along El Camino Avenue. On December 11, 1980, the Planning Commission approved a Special Permit (P-9240) to develop an 11 space parking lot on the residentially zoned parcel at the south east corner of the subject site, for the Burger King which was being constructed at the time; this building is the one proposed for demolition as part of the currently proposed project. On May 22, 1986, the Planning Commission approved a Special Permit (P86-148) to develop a 14 space parking lot on the residentially zoned parcel at the south west corner of the subject site for the Round Table Pizza restaurant that was locating in the building on the south east corner of El Camino Avenue and Beaumont Street, this building is now the check cashing facility which is not a part of the currently proposed project.

<b>Table 1: Project Information</b>
<b>General Plan designation:</b> Special Planning District, and Low Density Residential 4-15 du/na
<b>N. Sac Community Plan designation:</b> Special Planning District, and Residential 4-8 du/na
<b>Existing zoning of site:</b> C-2 (General Commercial) and R-1 (Standard Single-Family)
<b>Existing use of site:</b> Vacant
<b>Property area:</b> 1.26 gross acres

**Public/Neighborhood Outreach and Comments:** Staff notified the following community groups: Del Paso Heights Neighborhood Improvement Association, Woodlake Neighborhood Association, and North Sacramento Redevelopment Advisory Committee (RAC). Design Review staff presented the project at a North Sacramento Redevelopment Advisory Committee meeting and received positive feedback. Additionally, all property owners within five hundred (500) feet of the project site received a public hearing notice for this project. City staff received no phone calls or comments about this project.

**Environmental Considerations:** The Development Services Department, Environmental Planning Services Division has reviewed this project and determined that it is exempt from the provisions of CEQA (the California Environmental Quality Act) under Section 15332 (Class 32 Infill Development) which consists of a project that is consistent with the General Plan and zoning regulations, is located within the City limits, is located on a project site of not greater than five acres in size substantially surrounded by urban uses and that has no habitat value, will not have significant effects relating to traffic, noise, air quality, or water quality and can be adequately served by utilities and public services.

**Policy Considerations:** The proposed project site currently consists of three parcels. The main parcel with frontage on El Camino Avenue is zoned General Commercial (C-2) and the two parcels to the rear are zoned Standard Single-Family Residential (R-1). Alleys separate each of the three existing parcels. The two smaller R-1 zoned parcels are paved and were used as additional customer parking for the commercial uses which previously occupied the C-2 zoned parcels to the north. A lot merger and alley abandonments are being concurrently processed as parts of this project. At the direction of staff, the applicant is requesting to re-designate the residentially zoned portions of the project to General Commercial (C-2) to create one consistently zoned parcel.

General Plan: The C-2 zoned parcel within the project site which fronts El Camino Avenue is designated Special Planning District and the two rear parking lot parcels are designated Low Density Residential 4-15 dwelling units per net acre. A General Plan Amendment is not required because the parcels are less than 5 acres. The proposed project is consistent with the goals and policies as set forth in the General Plan such as the project would “offer goods and services for the daily needs of adjacent residential areas.” (Sec 4-10) The project would also “actively promote the continued vitality and diversification of the local economy, and expand employment opportunities for City residents.” (Sec 1-32)

North Sacramento Community Plan (NSCP): The C-2 zoned parcel within the project site lies within an area defined in the NSCP as a special planning district. This district is comprised of properties abutting both sides of El Camino Avenue between Del Paso Boulevard to the west and the Southern-Pacific Railroad line to the east. The NSCP recognizes this district as being underutilized and having excellent potential for positive growth and revitalization given its location along a major thoroughfare and its proximity to the light rail stations (NSCP, p. 32-33). It goes further by acknowledging that private investment in the area will most likely be slow unless the City shows clear support of new development that makes good use of land in the district area (p. 33) The existing parking lots at the rear of the site are designated Residential 4-8 dwelling units per acre in the NSCP, the parking lots were established under previous approvals and no change in use is proposed.

When it was drafted and approved in 1984, the NSCP prescribed the development of a Special Planning District (SPD) Zone which would establish land use regulations and development standards for the area (p. 34) This SPD Zone was never established, however. Therefore we must analyze the project in light of its consistency with the issues and objectives that the NSCP does contain in its discussion of the project area. In this case, the proposal has the potential to be a catalyst for further redevelopment of the area

and would be highly visible as it fronts on El Camino Avenue and is visible from Del Paso Boulevard. In addition, the project is consistent with the NSCP goals for commercial development by providing a shopping place that is convenient for the daily shopping needs of residents in the immediate neighborhood area and strengthening shopping facilities within the Del Paso – El Camino business district to serve the community (p. 18-19). The project also helps achieve the NSCP objectives of attracting new businesses that reduce storefront vacancies and create new jobs, and enhance the economic vitality of the community (p. 20).

General Plan Update Vision and Guiding Principles: While the City's General Plan is being updated, the City Council has adopted a vision for the future of the City, as well as several guiding principles to help guide the update and achieve this vision. This was done to ensure that new developments submitted during the ongoing update comply with the goals and policies that are being incorporated into the General Plan through the update. The applicable guiding principles that this proposal complies with include:

1. Promote developments that foster accessibility and connectivity between areas and safely and efficiently accommodate a mixture of cars, transit, bicyclists, and pedestrians.
2. Promote strategic development of vacant, underutilized, and infill land, especially along transportation and commercial corridors, to improve the city's economic outlook.

In addition to being consistent with these principles, the proposal is not contrary to any of the other approved principles of the General Plan Update Vision.

### **Project Design:**

The applicant is requesting entitlements to develop an 11,420 square foot retail store with a pharmacy and drive-through service. The proposed development will require several approvals including a Rezone, a Special Permit for drive-through service, a Variance to reduce the required front and street-side setbacks, a Variance to reduce the required vehicle stacking distance for a drive-through, and a Variance to reduce the required drive aisle maneuvering width.

### **Land Use**

#### *Rezone*

The applicant is requesting to rezone a portion of the project site from R-1 to C-2. Staff finds that the proposed rezone is consistent with other surrounding uses and supports the rezone as the portions of the lot which are currently zoned for residential are already developed as parking spaces for the commercial uses that previously occupied the project site.

## Access, Circulation and Parking

The proposed project includes driveway access points along Beaumont Street (left in, left out only), El Camino Avenue (right in, right out only), and Empress Street (full access).

### *Special Permit for drive-through service facility*

Pursuant to Section 17.24.050 of the Zoning Code, drive-through facilities are permitted subject to approval of a Special Permit. The location of the drive-through will not conflict with internal circulation or impede ingress and egress to the site. The configuration of the drive-through is pedestrian friendly, allowing foot traffic to access the rest of the site without conflicting with the drive-through lane. The proposed project has been reviewed by the Development Engineering Division and it has been determined that, as designed, the project will not contribute to increased congestion on streets adjacent to the property, or impede circulation within the parking lot or impede pedestrian movement.

The Zoning Ordinance establishes the following standards for drive-through service facilities that are to be used in reviewing the adequacy of project design:

1. A minimum stacking distance of one hundred eighty (180) feet shall be provided to each pick-up window or automated machine.

The proposed stacking distance for the drive-through lane is 70 feet. Further discussion is below in the Variance section of the report.

2. A facility with separate ordering point(s) and pick up window(s) shall provide stacking space for at least four vehicles in advance of each ordering point and stacking space for at least four vehicles between each ordering point and pick-up window.

As proposed, there is only one pick-up and ordering window. This project will provide stacking space for at least four vehicles from the window.

3. Entrances to drive-through lanes shall be at least twenty-five (25) feet from driveways entering a public or private street or alley.

The entrance to the drive-through lane is more than 25 feet from each of the site's driveways.

4. Drive-through service facilities shall not be considered as justification for reducing the number of required parking spaces.

The proposal includes more parking spaces than are required. Therefore, the drive-through will have no impact for reducing the number of required parking spaces.

5. The minimum width of each drive-through lane shall be eleven (11) feet. The entrance to the lane and the direction of traffic flow shall be clearly designated by signs and pavement marking or raised curbs.

The minimum width of the drive-through lane is 11 feet, meeting the minimum requirements.

6. A solid six-foot high masonry sound wall shall be constructed on the property boundary when the site is contiguous to residentially zoned or used property(ies).

There are existing six-foot high masonry sound walls on the southern property boundary contiguous to residential properties. The project is conditioned to repair or replace any deteriorated sections of the existing masonry walls.

7. Operation of the drive-through service facility shall be restricted to be between the hours of seven a.m. and ten p.m. when the site is contiguous to residentially zoned or used property(ies) unless the planning commission approves different hours of operation during the review of the special permit.

This project is conditioned to restrict the drive-through hours of operation to between 7 AM and 10 PM.

#### *Variance for substandard drive-through length*

Pursuant to section 17.24.050 Footnote 44(b)(i), a minimum stacking distance of one hundred eighty (180) feet shall be provided to each pick-up window or automated machine. The proposal calls for a drive-through lane of approximately 70 feet long with one pick-up window. To reduce the minimum stacking distance from 180 feet to 70 feet, the applicant must obtain a Variance.

It is staff's opinion that granting a variance for this application would not bestow a special privilege upon the property owner for a number of reasons. The unique shape of the lot poses a significant challenge to meet the required 180 feet drive-through lane while still maintaining adequate internal traffic circulation throughout the site. Staff is in favor of having the drive-through lane entrance toward the rear of the building while allowing the main entrance to be adjacent to El Camino Avenue, enabling a safe walkable site while reducing additional pedestrian conflicts with the drive-through. In addition, the Development Engineering Division has reviewed the plans and indicated that a 70 foot drive-through lane is adequate in this case. Pharmacy drive-throughs such as the one proposed differ from typical "fast-food" drive-throughs because there is no order board or separate order window, rather the customers are either only dropping off or picking up a prescription. According to Walgreens corporate office, based on historical use at other Walgreens, the drive-through is expected to have approximately 5-6 vehicle trips per hour during peak hours with no more than three cars stacked up at a time. The drive-through meets all other applicable standards and will not cause harm to public safety. For these reasons, staff feels that in this case, the findings can be made in support of granting the variance.

#### *Variance for substandard drive-aisle width*

Pursuant to section 17.64.030, a minimum maneuvering width of 26 feet shall be provided for 90 degree parking spaces. The proposal calls for a minimum drive aisle widths of 25 feet off of Beaumont Street, 23 feet off of El Camino Avenue, and 24 feet off of Empress

Street. To reduce the minimum drive aisle distance the applicant must obtain a Variance.

In this case, staff supports the Variance to reduce the minimum drive aisle width because it allows for additional sidewalk and planter space along El Camino Avenue and Empress Street and the proposed parking space depths exceed the minimum requirement. The original proposal located the building a couple of feet closer to the corner of El Camino Avenue and Empress Street. However, the Development Engineering Division requires that the improvements on the corner be reconstructed to have a 32 foot radius, therefore to allow for ample sidewalk and planter widths staff recommended moving the building back slightly. Walgreens requires that their parking stalls are 19 feet deep, while the Zoning Code requires a minimum of 18 feet deep and a minimum drive aisle width of 26 feet. The Development Engineering Division staff was involved in this conversation and stated that they support reducing the drive aisle widths to accommodate a better pedestrian environment along the street frontages because with the 19 foot deep parking spaces, the total length of the parking area off of El Camino Avenue would be reduced by only one foot, the parking area off of Empress Street would be reduced by one foot, and the total width of the parking area off of Beaumont Street would not be affected. The Development Engineering Division has reviewed the proposed parking layout and indicated that they would and have supported similar variances, and that the parking meets all other applicable standards and will not cause harm to public safety.

<b>Table 2: Parking</b>			
<b>Use</b>	<b>Required Parking</b>	<b>Proposed Parking</b>	<b>Difference</b>
Retail, etc.	31	44	+13

<b>Table 2a: Bicycle Parking</b>			
<b>Total parking provided</b>	<b>Required bicycle parking</b>	<b>Provided bicycle parking</b>	<b>Difference</b>
44	2	0	-2

With the exception of bicycle parking, the project meets or exceeds parking requirements.

Vehicle Parking: The proposal is required to provide a minimum of 1 space per every 400 square feet for the first 9,600 square feet and 1 space for every 250 square feet of commercial use there after. The applicant is proposing an 11,420 square foot building; therefore the proposal is required to provide a minimum of 31 parking spaces. The applicant is proposing a total of 44 parking spaces.

Bicycle Parking: The Sacramento City Code, Section 17.64.050, requires one (1) bicycle parking space for every twenty (20) required vehicle parking spaces. The proposal is

required to provide a minimum of 31 parking spaces; therefore the applicant is required to provide two bicycle parking spaces with at least one being a Class I facility. The current project site plan does not indicate where the bicycle parking will be located on site. The proposal will be conditioned to provide a minimum of two bicycle parking spaces within close proximity to the proposed building.

### Height, Bulk and Setbacks

<b>Standard</b>	<b>Required</b>	<b>Proposed</b>	<b>Deviation?</b>
Height	35'	32'	no
Front setback	15'	3'	-12'
Side setback	0'	72'	no
Street side setback	5'	3'	-2'
Rear setback	15'	66'-6"	no
Lot coverage	<40,000 SF	11,420 SF	no

#### *Variance to reduce setbacks*

As indicated in Table 3 above, the proposal deviates from the front and street side setback requirements. A Variance is required in order to deviate from setback requirements. Staff supports the request to reduce the setbacks because the intention of locating the building as close to the corner as possible is to create a safe, welcoming environment that is at a scale that will encourage pedestrian activity. By locating the building on the corner and the parking in the back, the building becomes the focal point of the site. The project site is within walking distance from residences in the Del Paso neighborhood, several bus routes, and approximately 1500 feet from the Royal Oaks light rail station. Many nearby buildings along and near Del Paso Boulevard are also located close to the street with parking in the rear, which contributes to the General Plan Update Vision of creating a lively pedestrian friendly corridor. It is staff's opinion that granting a Variance for this application would not constitute a special privilege upon the property owner because staff has and would support projects under similar circumstances since it promotes the goals and policies of the General Plan and Central City Community Plan to promote pedestrian access.

### Building design, signage and landscaping

Design Review staff approved the project (DR07-0280) on January 29, 2008. The Design Director Conditions of Approval have been included in Attachment 2.

#### *Building Design*

The applicant is proposing to construct an 11,420 square foot commercial building. The proposed project is in a transitional area and has the potential to encourage further rehabilitation and development in the vicinity. The proposed building has a large tower feature which identifies the customer entrance. Awnings, light fixtures, and windows are included to help make pedestrian pathways safe and inviting. Building materials include brick veneer and painted cement plaster. A screenwall will be provided along the east elevation to screen the receiving area adjacent to Empress Street.

### *Signage*

Chapter 15.148 of the Zoning Code allows corner parcels within the C-2 zone to have two detached monument signs and four attached signs. The applicant is not requesting variances for additional signage at this time. All signage is required to have a sign permit, final signage designs shall be submitted to Design Review staff and the City Sign Coordinator for final review and approval.

### *Landscaping*

The Zoning Ordinance requires that trees be planted and maintained throughout the surface parking lot to ensure that, within 15 years after establishment of the parking lot, at least 50% of the parking area will be shaded. Areas to be shaded include the parking area and any driveways or maneuvering areas utilized or accessed by the vehicles using the parking spaces. The submitted landscape plan indicates that the project will comply with the 50% shading requirement for parking areas and driveways.

**RESOLUTION NO. 2008-\_\_\_\_\_**

ADOPTED BY THE SACRAMENTO CITY COUNCIL

**DETERMINING PROJECT EXEMPT FROM REVIEW UNDER THE CALIFORNIA ENVIRONMENTAL QUALITY ACT (EL CAMINO WALGREENS, P07-116)**

**BACKGROUND**

- A. On February 14, 2008, the City Planning Commission conducted a public hearing on the El Camino Walgreens project (hereafter referred to as "Project"), and forwarded the Project to the City Council with a recommendation to approve with conditions.
- B. On February 26, 2008, the City Council conducted a public hearing, for which notice was given pursuant to Sacramento City Code Sections 16.24.097, 17.204.020(C), 17.208.020(C), 17.212.035, and 17.200.010(C)(2)(a, b, and c) (publication, posting, and mail 500'), and received and considered evidence concerning the Project.

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:**

Section 1. Based on the determination and recommendation of the City's Environmental Planning Services Manager and the oral and documentary evidence received at the hearing on the Project, the City Council finds that the Project is exempt from review under Section 15332 Infill Development of the California Environmental Quality Act Guidelines as follows:

- a. The project complies with the General Plan designation and all applicable policies of the General Plan and South Sacramento Community Plan, as well as with the applicable zoning regulations;
- b. The proposed development occurs within City limits on a project site of no more than five (5) acres substantially surrounded by urban uses;
- c. The project site has no value as habitat for endangered, rare or threatened species;
- d. Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality; and
- e. The site can be adequately served by all required utilities and public services.

**Attachment 5 – Rezone Ordinance**

**ORDINANCE NO. 2008-\_\_\_\_\_**

ADOPTED BY THE SACRAMENTO CITY COUNCIL

**AMENDING TITLE 17 OF THE SACRAMENTO CITY CODE (THE ZONING CODE) BY REZONING CERTAIN REAL PROPERTY FROM STANDARD SINGLE-FAMILY RESIDENTIAL (R-1) TO GENERAL COMMERCIAL (C-2) (EL CAMINO WALGREENS P07-116) (APN: 275-0054-025 & 275-0054-007)**

BE IT ENACTED BY THE COUNCIL OF THE CITY OF SACRAMENTO:

**SECTION 1**

Title 17 of the Sacramento City Code (the Zoning Code) is amended by rezoning the property shown in the attached Exhibit A, generally described, known and referred to as El Camino Walgreens (APN: 275-0054-025 and 275-0054-007) and consisting of 0.29± acres, from Standard Single Family Residential (R-1) to General Commercial (C-2).

**SECTION 2**

Rezoning of the property as shown in the attached Exhibit A, by the adoption of this Ordinance, will be considered to be in compliance with the requirements for the rezoning of the property described in the Zoning Code, as amended, as those procedures have been affected by recent court decisions.

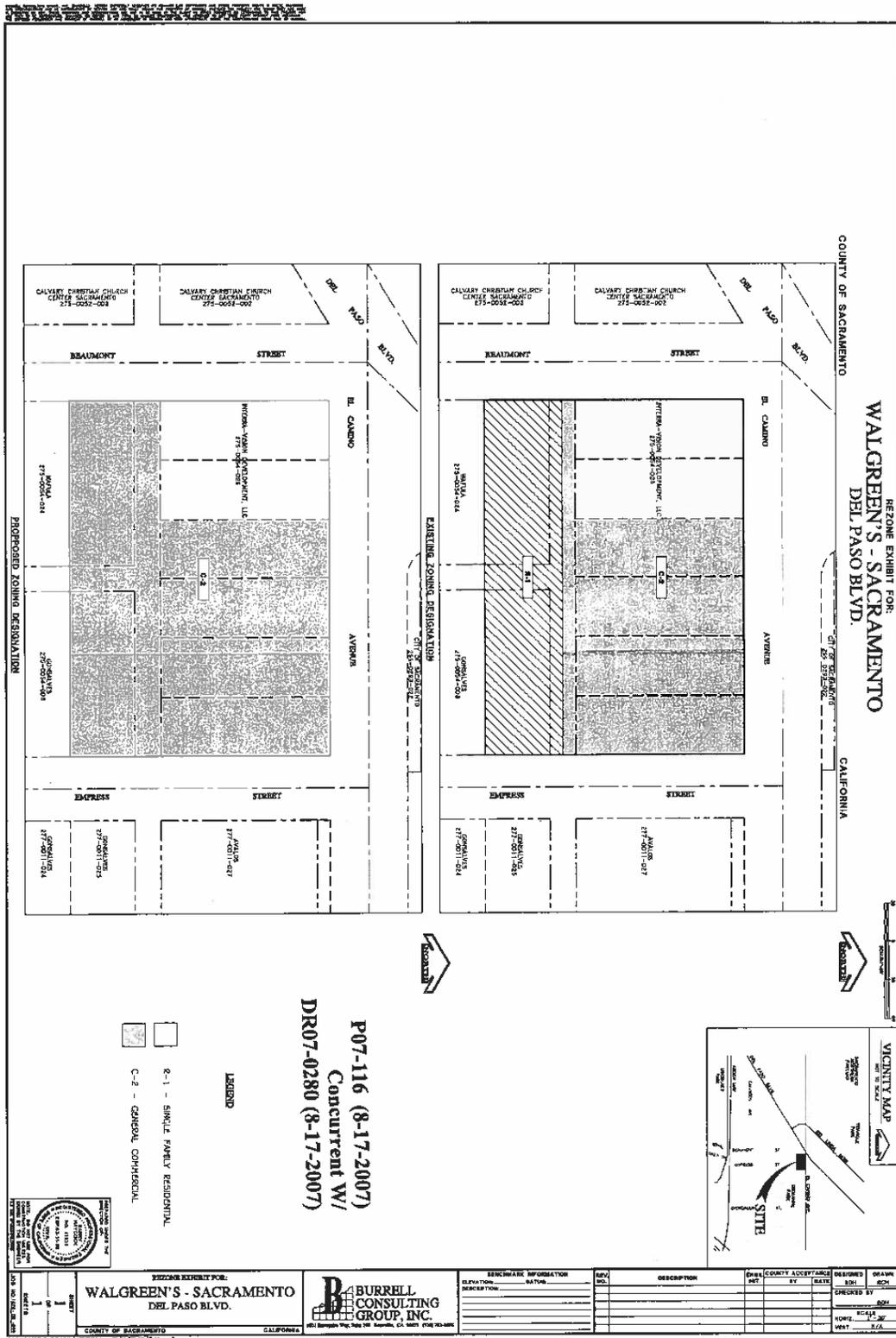
**SECTION 3**

The City Clerk of the City of Sacramento is hereby directed to amend the official zoning map, which is a part of the Zoning Code, to conform to the provisions of this Ordinance.

**Table of Contents:**

Exhibit A – Rezone Exhibit

Exhibit A – Rezone Exhibit



Attachment 6 – Resolution – Findings of Fact

**RESOLUTION NO. 2008-\_\_\_\_\_**

ADOPTED BY THE SACRAMENTO CITY COUNCIL

**ADOPTING FINDINGS OF FACT AND APPROVING THE EL CAMINO  
WALGREENS PROJECT (P07-116)**

**BACKGROUND**

- A. On February 14, 2008, the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve with conditions the El Camino Walgreens Project.
- B. On February 26, 2008, the City Council conducted a public hearing, for which notice was given pursuant to Sacramento City Code Sections 16.24.097, 17.204.020(C), 17.208.020(C), 17.212.035, and 17.200.010(C)(2)(a, b, and c) (publication, posting, and mail 500'), and received and considered evidence concerning the El Camino Walgreens Project.

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL  
RESOLVES AS FOLLOWS:**

Section 1. Based on the verbal and documentary evidence received at the hearing on the El Camino Walgreens project, the City Council approves the Project entitlements based on the findings of fact and subject to the conditions of approval as set forth below.

**C.** The **Special Permit** to allow a drive-through pharmacy service for an approximately 11,420 square foot retail store in the General Commercial (C-2) zone **is approved** subject to the following Findings of Fact:

- a. Granting of the Special Permit is based upon sound principles of land use in that the proposed drive-through service facility would be incidental to the underlying commercial use and shall adhere to all development standards as set forth by the City's Zoning Ordinance;
- b. Granting the Special Permit would not be detrimental to the public health, safety, or welfare, nor result in the creation of a nuisance in that the proposed drive-through service facility is a compatible use in the General Commercial (C-2) zone. Furthermore, the project site is of sufficient size and shape to accommodate setbacks, landscaping and lighting which will protect the privacy of residential neighbors and ensure adequate open space and access to light and air;

- c. The proposed project would be consistent with the commercial land use policies of the General Plan and Zoning Ordinance;
- d. The design and location of the facility will not contribute to increased congestion on public or private streets or alleys adjacent to the subject property;
- e. The design or location of the facility will not impede access to or exit from the parking lot serving the business, impair normal circulation within the parking lot or impede pedestrian movement; and
- f. The design and location of the facility will not create a nuisance for adjacent properties.

**D, E, & F.** The **Variations** to reduce the required front and street-side setbacks, allow a drive-through of less than 180 feet, and reduce the required drive aisle maneuvering width **are approved** subject to the following Findings of Fact:

- a. Granting the variance does not constitute a special privilege extended to an individual applicant in that the building's location at the corner, the unique shape of the lot, and the corner reconstruction are unique circumstances and a variance would be and has been granted to other property owners facing similar circumstances.
- b. Granting the variance request does not constitute a use variance in that retail store uses are permitted on lots within the General Commercial (C-2) zone.
- c. The proposed project will not be injurious to public welfare, nor to property in the vicinity of the applicant in that the development, as conditioned, will create a safe environment for customers and area residents and the variance will not adversely affect the adjacent properties.
- d. Granting the variance is in harmony with the general purpose and intent of the zoning code and will not adversely affect the general plan or specific plans of the city, or the open space regulations.

### **Conditions of Approval**

**C.** The **Special Permit** to allow a drive-through pharmacy service for an approximately 11,420 square foot retail store in the General Commercial (C-2) zone **is approved** subject to the following conditions:

### **General**

- C1.** Applicant shall obtain all necessary building and/or encroachment permits prior to commencing construction.
- C2.** The project shall comply with all Design Review conditions of approval (DR07-0280).
- C3.** The project shall be constructed in accordance with the attached plans (Exhibits A-E). Any modification to the project shall be subject to review and approval by Planning Division staff prior to the issuance of building permits.
- C4.** Unless specified by any condition below, this project shall be developed and constructed in full compliance with the Zoning Ordinance.
- C5.** The property owner shall comply with the tree shading guidelines set forth in the Zoning Code.
- C6.** A minimum of two bicycle parking facilities shall be provided. At least one bicycle parking facility shall be Class I. Bicycle parking shall be located in view of building entrances or in view of windows and/or security stations.
- C7.** Final design of signage shall be reviewed and approved by Design Review staff and the City Sign Coordinator prior to issuance of a sign permit. All signage shall have a sign permit and shall comply with the sign ordinance.
- C8.** The existing six-foot high masonry sound walls on the southern property boundary contiguous to residential properties shall be repaired or replaced where deteriorated.
- C9.** A Parcel Merger shall be completed prior to the issuance of building permits.
- C10.** Operation of the drive-through service facility shall be restricted to be between the hours of seven a.m. and ten p.m.

### **Development Engineering**

- C11.** Construct standard improvements as noted in these conditions pursuant to section 16.48.110 of the City Code. Improvements shall be designed and constructed to City standards in place at the time that the Building Permit is issued. All improvements shall be designed and constructed to the satisfaction of the Development Engineering Division. Any public improvement not specifically noted in these conditions shall be designed and constructed to City Standards.
- C12.** The applicant shall reconstruct frontage improvements on El Camino Avenue. Frontage improvements shall include a 6-ft sidewalk and 5'9" planter and vertical curb and gutter. However, where trees are to be planted, the planter shall be widened by 1-ft and the sidewalk shall be reduced by 1-ft. The design and construction of such improvements, including any necessary transitions from

existing improvements, shall be to the satisfaction of the Development Engineering Division.

- C13.** The applicant shall reconstruct frontage improvements on Empress Street to a modified 53-ft right-of-way street cross-section. Frontage improvements shall consist of a 6-ft sidewalk, a 6.5-ft planter and vertical curb and gutter as well as striping/re-striping of the street centerline. The design and construction of such improvements, including any necessary transitions from existing improvements, shall be to the satisfaction of the Development Engineering Division.
- C14.** The applicant shall dedicate a 3-ft pedestrian easement for the portion of the sidewalk along El Camino Avenue and Empress Street that shall be constructed outside of the right-of-way line.
- C15.** The applicant shall reconstruct the sidewalk along Beaumont Street adjacent to the subject property to meet current A.D.A. requirements and City standards to the satisfaction of the Development Engineering Division. The design and construction of such improvements, including any necessary transitions from existing improvements, shall be to the satisfaction of the Development Engineering Division.
- C16.** The applicant shall relocate any existing utility pole(s) within and adjacent to the subject property, if necessary, to the satisfaction of the Development Engineering Division.
- C17.** All new driveways shall be designed and constructed to City Standards to the satisfaction of the Development Engineering Division. Any existing driveway(s) within the subject property that are not shown on the approved site plan shall be removed to the satisfaction of the Development Engineering Division.
- C18.** The minimum throat distance for the El Camino Avenue driveway shall be 47-ft, and the minimum throat distance for the Empress Street driveway shall be 20-ft (throat distance is that distance a vehicle can move from the public right-of-way into a given site before encountering a conflict with parking stalls, aisles, etc).
- C19.** The applicant shall reconstruct the round corner at the intersection of Empress Street and West El Camino Avenue adjacent to the subject property to meet current A.D.A. requirements and City standards to the satisfaction of the Development Engineering Division.
- C20.** The site plan shall conform to A.D.A. requirements in all respects. This shall include the replacement of any curb ramp that does not meet current A.D.A. standards.
- C21.** The applicant must apply for and obtain City Council approval for the abandonment of the existing alley between Beaumont Street and Empress Street adjacent to the subject property.

- C22.** The applicant shall satisfy the conditions of approval of the abandonment prior to obtaining any Building Permits.
- C23.** The applicant shall pay a fair share contribution to fund the future construction of the traffic signal at the intersection of El Camino Avenue and Boxwood Street, East of the subject property.
- C24.** The applicant shall apply for and obtain a revocable Encroachment Permit for the proposed awnings that will be protruding over the right-of-way adjacent to the subject property.
- C25.** The design of walls fences and signage near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height at maturity. The area of exclusion shall be determined by the Development Engineering Division.

### **Utilities**

- C26.** Any new domestic water services shall be metered. Only one domestic water service is allowed per parcel. Excess services shall be abandoned to the satisfaction of the DOU. (Note: There is an existing 6" water main in the alley south of property addresses 800 & 840 El Camino Ave. There is an existing 6" water main in Empress St.)
- C27.** An on-site surface drainage system is required and shall be connected to the street drainage system by means of a storm drain service tap unless otherwise approved by the DOU. The on-site system shall be designed so the 10-year HGL is a minimum of 6-inches below the onsite drain inlets. All on-site systems shall be designed to the standard for private storm drainage systems (per Section 11.12 of the Design and Procedures Manual).
- C28.** A drainage main extension is required for the connection of the on-site drainage system to the satisfaction of the Department of Utilities (DOU). (Note: There is an existing manhole at the intersection of Dixianne Ave. and Empress St.)
- C29.** Prior to submittal of improvement plans, a project specific drainage study is required for review and approval by the DOU. This project is in drainage shed 151. The applicant shall use the SWMM model for this project to size the drainage pipes and to determine the amount of detention volume required within oversized pipes within the public street section and/or landscape area or parking lot prior to overland release. There is a current drainage study done by Civil Solutions for the Evergreen Project (P06-028 & P06-029) of drainage shed 151. The applicant may coordinate with Civil Solutions to obtain a copy of this drainage study. Sufficient off-site and on-

site spot elevations shall be provided in the drainage study to determine the direction of storm drain runoff. (Note: There has been historic flooding at the intersection of Boxwood Street and Dixie Avenue.)

- C30.** The owner/developer shall properly abandon existing alleys and shall dedicate to the City, public utility easements at full widths of the private streets and alleys with public water, sanitary sewer and/or storm drain pipeline at no cost to the City. The easement shall include language assuring DOU personnel and maintenance vehicles unrestricted access to any easement at all times.
- C31.** A grading plan showing existing and proposed elevations is required. Adjacent off-site topography shall be shown to the extent necessary to determine impacts to existing surface drainage paths. No grading shall occur until the grading plan has been reviewed and approved by the Department of Utilities.
- C32.** The applicant must comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinance. This ordinance requires the applicant to prepare erosion and sediment control plans for both during and after construction of the proposed project, prepare preliminary and final grading plans, and prepare plans to control urban runoff pollution from the project site during construction.
- C33.** If the disturbed area is greater than 1 acre of property, then the project is required to comply with the State "NPDES General Permit for Stormwater Discharges Associated with Construction Activity" (State Permit). To comply with the State Permit, the applicant will is required to file a Notice of Intent (NOI) with the State Water Resources Control Board (SWRCB) and prepare a Stormwater Pollution Prevention Plan (SWPPP) prior to construction. A copy of the State Permit and NOI may be obtained at [www.swrcb.ca.gov/stormwtr/construction.html](http://www.swrcb.ca.gov/stormwtr/construction.html). The SWPPP will be reviewed by the Department of Utilities prior to issuing a grading permit or approval of improvement plans to assure that the following items are included: 1) vicinity map, 2) site map, 3) list of potential pollutant sources, 4) type and location of erosion and sediment BMPs, 5) name and phone number of person responsible for SWPPP, 6) certification by property owner or authorized representative.
- C34.** Post construction, stormwater quality control measures shall be incorporated into the development to minimize the increase of urban runoff pollution caused by development of the area. Since the project is not served by a regional water quality control facility, both source control and on-site treatment control measures are required. Contact DOU for a list of accepted proprietary devices if considered for treatment control. Improvement plans must include the source controls and on-site treatment control measures selected for the site. Refer to the latest edition of the "Guidance Manual for On Site Stormwater Quality Control Measures" for appropriate source control measures and on-site treatment control measures.

**F.** The **Variance** to reduce the required drive aisle maneuvering width **is approved** subject to the following conditions of approval:

**General**

- F1.** The project shall be constructed in accordance with the attached plans (Exhibits A-E). Any modification to the project shall be subject to review and approval by Planning Department staff prior to the issuance of building permits.

**ADVISORY NOTES:****Utilities**

1. Prior to design of the subject project, the Department of Utilities suggests that the applicant request a water supply test to determine what pressure and flows the surrounding public water distribution system can provide to the site. This information can then be used to assist the engineers in the design of the fire suppression and domestic water systems.
2. The proposed project is located in the Flood zone designated as **Shaded X** zone on the Federal Emergency Management Agency (FEMA) Federal Insurance Rate Maps (FIRMs) that have been revised by a letter of Map Revision effective February 18, 2005. Within the **Shaded X** zone, there are no requirements to elevate or flood proof.

**Fire**

3. All turning radii for fire access shall be designed as 35' inside and 55' outside.
4. Roads used for Fire Department access shall have an unobstructed width of not less than 20' and unobstructed vertical clearance of 13'6" or more.
5. Fire Apparatus access roads shall be designed and maintained to support the imposed loads of fire apparatus. (902.2.2.2)
6. Provide the required fire hydrants in accordance with CFC 903.4.2 and Appendix III-B, Section 5.
7. A reciprocal ingress egress agreement shall be provided for review by City Attorney for all shared driveways being used for Fire Department access.
8. Timing and Installation. When fire protection, including fire apparatus access roads and water supplies for fire protection, is required to be installed, such protection shall be installed and made serviceable prior to and during the time of construction.
9. Provide a water flow test. (Make arrangements at the Downtown Permit Center's walk-in counter: New City Hall, 915 I Street, 3<sup>rd</sup> Floor, Sacramento, CA, 95814.
10. Provide appropriate Knox access for the site.

11. Roads used for Fire Department access that are less than 28 feet in width shall be marked "No Parking Fire Lane" on both sides; roads less than 36 feet in width shall be marked on one side.
12. An automatic fire sprinkler system shall be installed in any portion of a building when the floor area of the building exceeds 4,999 square feet. Refer to City Code 15.36.1003, Amendment of Article 10, Section 1003, for exceptions.
13. Locate and identify Fire Department Connections (FDCs) on address side of building no further than 40 feet and no closer than 5 feet from a fire hydrant.

**Building**

14. A minimum of two exits for the General Sales shall be provided.

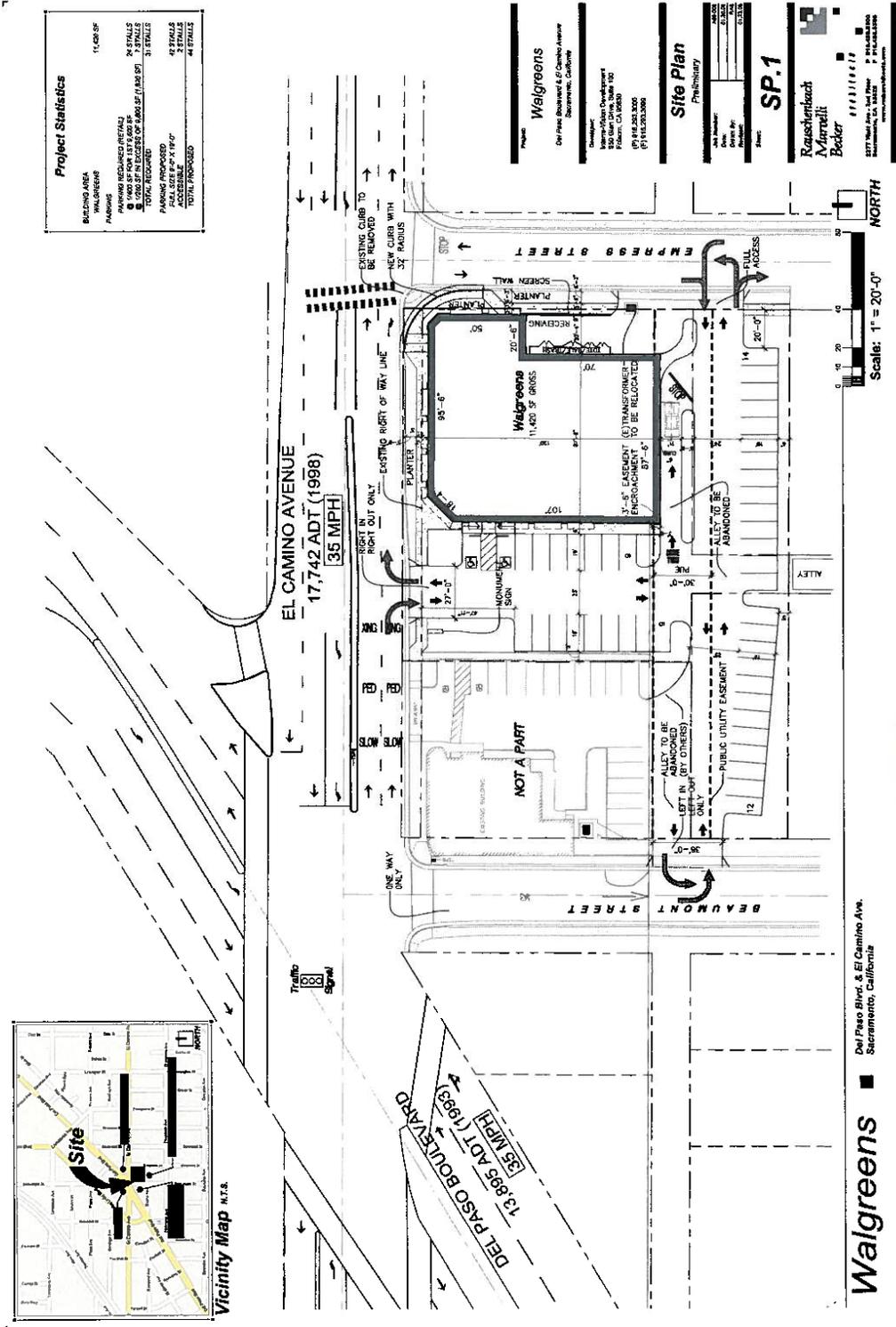
**Parks**

15. As per City Code, the applicant will be responsible to meet his/her obligations regarding Title 18, 18.44 Park Development Impact Fee, due at the time of issuance of building permit. The Park Development Impact Fee due for this project is estimated at \$1,841. This is based on 11,506 square feet of commercial retail / services at the commercial corridor infill rate of \$0.14 per square foot. Any change in these factors will change the amount of the PIF due. The fee is calculated using factors at the time that the project is submitted for building permit.

Table of Contents:

Exhibits A-E: Plans

Exhibit A – Site Plan



**Project Statistics**

REZONING AREA	11,420 SF
WALGREENS	
PARKING REQUIRED (NET)	31 STALLS
PARKING PROVIDED	31 STALLS
TOTAL REQUIRED	31 STALLS
TOTAL PROVIDED	31 STALLS
ACCESSIBLE	2 STALLS
TOTAL PROVIDED	44 STALLS



**Walgreens**  
 One Place Boulevard & El Camino Avenue  
 Sacramento, California

**Site Plan**  
 Preliminary

**SP.1**

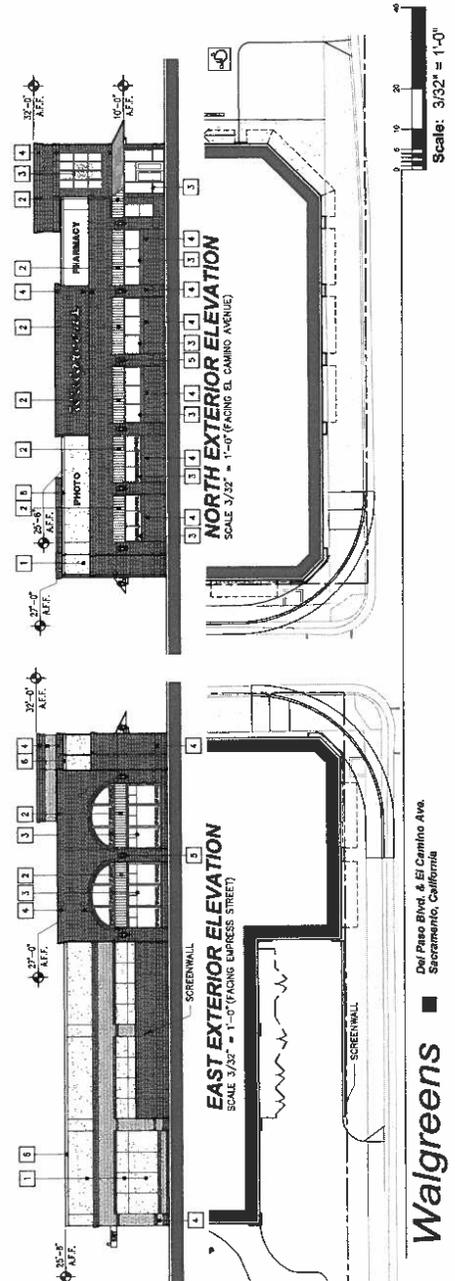
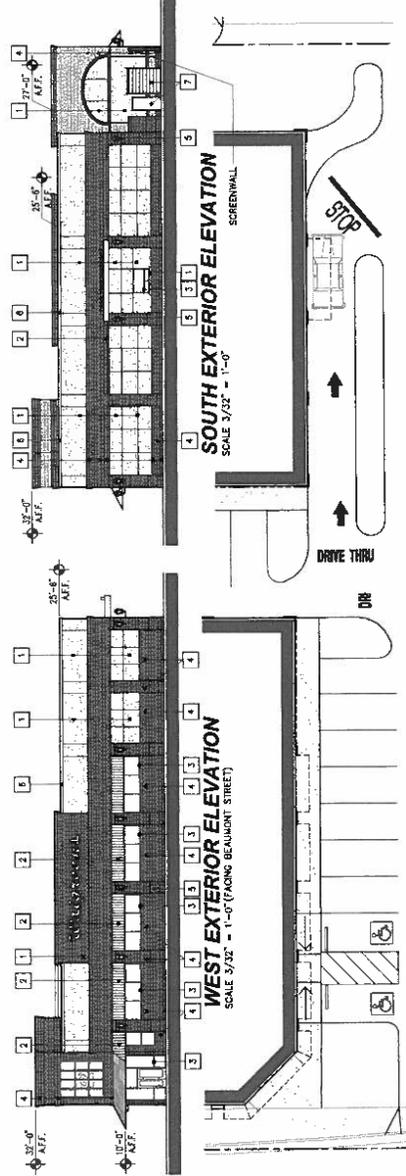
**Raushtentach**  
**Marrilli**  
**Becker**

1227 North Street, Suite 100  
 Sacramento, CA 95811  
 Tel: 916.441.1111  
 Fax: 916.441.1112  
 www.raushtentach.com

Exhibit B – Floor Plan



Material		Manufacturer/Style/Color	
1	Concrete Finish	Walgreens Panel	104023-8 Burnwood Beak
2	Weld Mesh	Walgreens Panel	Black
3	Weld Mesh	Walgreens Panel	Clear Anodized
4	Weld Mesh	Walgreens Panel	Walgreen Beak
5	Weld Mesh	Walgreens Panel	Walgreen Beak
6	Weld Mesh	Walgreens Panel	Walgreen Beak
7	Weld Mesh	Walgreens Panel	Walgreen Beak
8	Weld Mesh	Walgreens Panel	Walgreen Beak
9	Weld Mesh	Walgreens Panel	Walgreen Beak
10	Weld Mesh	Walgreens Panel	Walgreen Beak



**Walgreens**  
 Del Paso Boulevard, El Camino Avenue  
 Sacramento, California

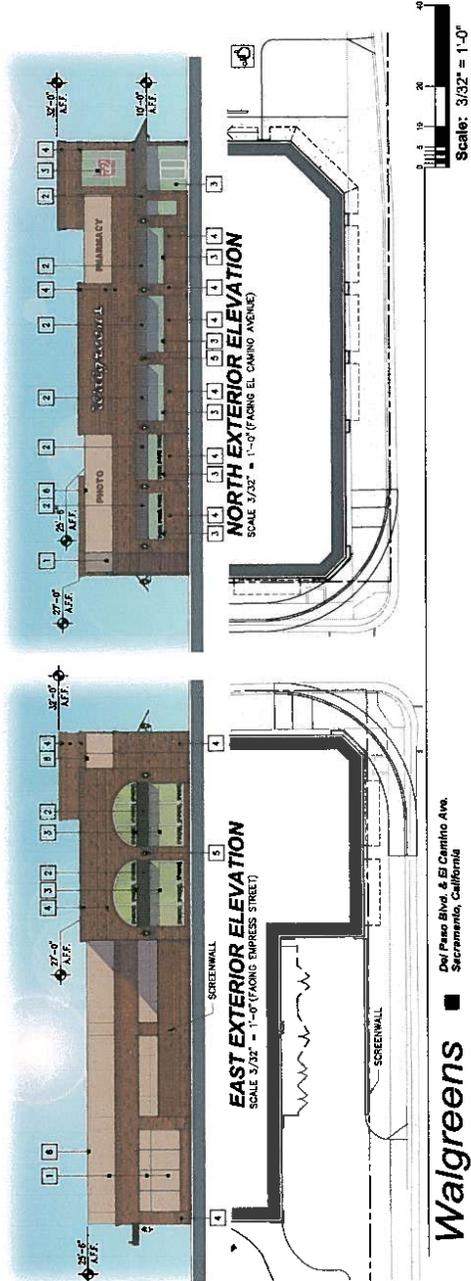
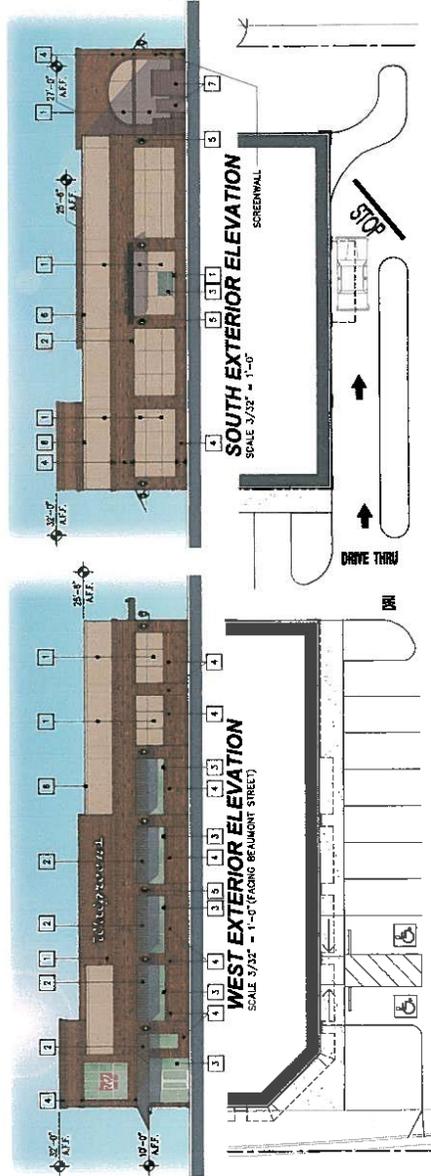
**Elevations**  
 Preliminary

**ELV.1**

**Rauschnbach Marnett Becker**  
 1075 MacArthur Blvd., Suite 100  
 Sacramento, CA 95833  
 916.441.1111

**Walgreens** ■ Del Paso Blvd. & El Camino Ave.  
 Sacramento, California

Material		Manufacturer/Style/Color	
1	General Finish	Walgreens Paint	DM002-2 Burnt Birch
2	Wood Siding	1/2" Siding	Black
3	Staircase	TED	Clear Anodized
4	WWS Window	1/2" Window	Redwood Blend
5	Light Fixture	Architectural Area Lighting or Equivalent	Transo Concrete AJ400 Black
6	Corridor	Walgreens Paint	DM002-2 Burnt Birch
7	Wood Doors	Walgreens Paint	281 White



**Walgreens**  
 2615 El Camino Avenue  
 Sacramento, California

**Elevations**  
 Preliminary

**ELV.1C**

Rosenbath  
 Marnell  
 Becker

3275 MacArthur Blvd. #1000  
 Sacramento, CA 95825

**Walgreens** ■ Del Paso Blvd. & El Camino Ave.  
 Sacramento, California





Exhibit E – Materials Sheet

Material	Manufacturer	Style/Color
1	Conard Plaster	*Walgreens Plaster
2	Joint Finishing	*Walgreens Plaster
3	Stucco	Stucco-8 Burnt Beak
4	Black Primer	Black
5	Black Primer	Black
6	Black Primer	Black
7	Black Primer	Black
8	Black Primer	Black

**NORTH EXTERIOR ELEVATION**  
SCALE 3/32" = 1'-0" (FRONT E. CAMINO AVENUE)

1 2 3 4 5 6 7 8

**Color & Material Board**  
Walgreens ■ Del Paso Blvd. & El Camino Ave.  
Sacramento, California

404-506 RAS 12.15.07  
Reuschlebach  
Murrell  
Baker  
4277 Camino El Camino  
Sacramento, CA 95820  
www.walgreens.com

Not to Scale

Attachment 7 – Design Director Conditions of Approval



CITY OF SACRAMENTO  
CALIFORNIA

DEVELOPMENT SERVICES  
DEPARTMENT

PHONE 916-808-5656

915 I STREET, 3<sup>rd</sup> Floor  
SACRAMENTO, CA 95814

FAX 916-808-7460

DESIGN DIRECTOR LEVEL PROJECT REVIEW

DR Number:	DR07-0280	Applicant:	Jerry Apiass
Address:	830 El Camino Ave.	Date Filed:	August 17, 2007
Description:	New Commercial Building	Date Approved:	January 29, 2008
APN:	275-0054-007,-025,-026,-027	Staff Contact:	Matthew Sites, 808-7646

DESIGN DIRECTOR CONDITIONS OF APPROVAL:

This project subject to Design Director level review has been deemed to be in substantial compliance with appropriate guidelines and is therefore approved at staff level with the following conditions of approval:

**Site Conditions:**

The design of the site (see plans attached) is hereby approved subject to the following conditions. These conditions must be met prior to the issuance of a building permit:

1. The buildings shall be sited as indicated in the report and exhibits.
2. The project shall have building setbacks and entries as indicated in the exhibits, and meet all Planning Commission Conditions of Approval.
3. Auto access and site layout shall be as indicated in the report and exhibits.
4. Landscape shall be provided per approved plans. Automatic irrigation shall be provided for all planting and landscaping. Site plan subject to Development Engineering and Urban Forest review to meet shade calculations. Street planters and sidewalk shall be provided per Development Engineering standards and reviewed by Development Engineering, Urban Forest, and Design Review staff prior to Building Permit submittal.
5. Trash enclosure shall match adjacent building. Prefinished metal doors shall be provided to match project color palette.
6. The Applicant shall submit final site lighting locations and cut sheets of light fixture design, material and color to Design Review staff for review and approval prior to Building Permit issuance.
7. Any site mechanical equipment proposed shall be screened as necessary to fit in with the design of the new building. Backflow prevention devices, SMUD boxes, etc., shall

be placed where not visible from street views, and screened from any pedestrian view. The Applicant shall submit final mechanical locations and screening to Design Review staff for review and approval prior to Building Permit issuance.

**Building Conditions:**

The design of the building (see plans attached) is hereby approved subject to the following conditions:

8. The design of the building shall be as indicated on the final plans and color and material board.
9. Final heights and massing shall be as indicated on the plans. The building elevations shall have a consistency of detail and quality as indicated on the plans.
10. Painted smooth fished cement plaster shall be provided per approved plans and color board.
11. Brick veneer shall be provided per approved plans. Brick coursing, sills and cornice shall be provided per approved plans. A high brick sill shall be provided at north and west elevations per approved plans.
12. Clear anodized aluminum storefront systems with clear glazing shall be provided per approved plans. Clear anodized aluminum storefront systems with translucent spandrel glazing shall be provided at east and north elevations per approved plans.
13. Clear anodized aluminum storefront doors with clear glazing shall be provided per approved plans.
14. New pre-finished steel doors shall be provided at the south elevation per approved plans.
15. Prefinished steel awnings with ornamental braces shall be provided per approved plans.
16. Lighting shall be provided to match existing style per approved plans.
17. Any roof mounted HVAC or mechanical equipment shall not be visible from any street or neighbor views. Roof mounted mechanical equipment shall not exceed the parapet wall height. All roof mounted HVAC or mechanical equipment shall be located towards the center of the structure and not be visible from any street views.

**Signage Conditions:**

The design of the signage (see plans attached) is hereby approved subject to the following conditions:

18. Internally illuminated individual channel letters shall be provided at the North and West

elevations per approved plans. Final details shall be submitted to Design Review and City Sign staff for review and approval prior to issuance of a sign permit.

19. Signage criteria for the project shall be submitted to Design Review staff and the City Sign Coordinator for review and approval prior to sign permit issuance.

**General Conditions:**

20. No building permit shall be issued until the expiration of the ten (10) business day request for reconsideration period. If reconsideration is requested, no permit shall be issued until final approval is received.
21. Any person dissatisfied with the staff action has the right to request reconsideration by the Design Director. Requests for reconsideration must be received within ten (10) business days of the staff action. The decision of the Design Director after reconsideration shall be final and shall not be subject to appeal.
22. All other notes and drawings on the final plans as submitted by the applicant are deemed conditions of approval. Any changes to the final set of plans stamped by Design Review staff shall be subject to review and approval prior to any changes. Applicant shall comply with all current building code requirements.
23. The approval shall be deemed automatically revoked unless required permits have been issued and construction begun within three years of the date of the approval. Prior to expiration, an extension of time may be granted by the Director upon written request of the applicant.
24. Final occupancy shall be subject to approval by Design Review Staff and shall include an on-site inspection.



**Matthew Sites, Associate AIA**  
Design Review Team Lead