

City of Sacramento

915 I Street, Sacramento, CA 95814-2604

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Staff Report
August 24, 2010

**Honorable Mayor and
Members of the City Council**

Title: Regional Transit Green Line Project Update

Location/Council District: District 1

Recommendation: Receive and file

Contact: Azadeh Doherty, Principal Planner, (916) 808-3137

Presenters: Michael Wiley, General Manager/CEO- Regional Transit District

Department: Transportation

Division: Office of the Director

Organization No: 15001041

Description/Analysis

Issue: Sacramento Regional Transit District is embarking on a community review process to develop a shared vision of how to best integrate the Green Line light rail extension to the Airport into the Natomas communities. The community review will take place August 25-28, 2010 and will allow participants several opportunities to review plans as they are being refined. Topics of discussion will include design concepts for tracks and stations as well as terminus for the second phase of the Green Line. The first phase of the extension, from Downtown to the River District (Richards Boulevard), broke ground in October 2009.

Policy Considerations: The City's General Plan has adopted the following policies:

M 1.2.1- Multimodal Choices. The City shall promote the development of an integrated, multi-modal transportation system that offers attractive choices among modes including pedestrianways, public transportation, roadways, bikeways, rail, waterways, and aviation and reduces air pollution and greenhouse gas emissions.

M 1.2.3 - Multimodal Access. The City shall promote the provision of multimodal access to activity centers such as commercial centers and corridors, employment centers, transit stops/stations, airports, schools, parks, recreation areas, and tourist attractions.

Environmental Considerations: This report concerns administrative activities that will not have an impact on the environment and do not constitute a "project" as defined by CEQA [CEQA Guidelines Section 15061 (b)(3); 15378 (b) (2)].

Sustainability Considerations: Not applicable.

Other: None.

Commission/Committee Action: None.

Rationale for Recommendation: Information only.

Financial Considerations: None.

Emerging Small Business Development (ESBD): Not applicable.

Respectfully Submitted by: Francesca L. Halbakken

Francesca L. Halbakken
Operations Manager

Approved by: Jerry Way

Jerry Way
Director of Transportation

Recommendation Approved:

Gus Vina
Gus Vina
Interim City Manager

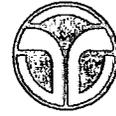
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Attachment:
1 PowerPoint Presentation Pg. 3



Downtown
Natomas
Airport

Green Line to the Airport

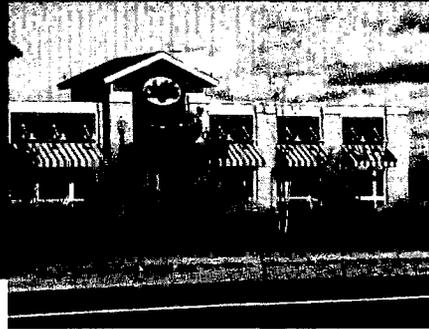
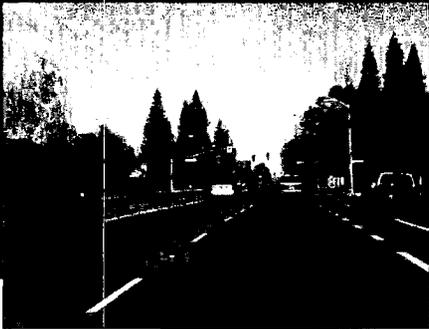


Regional Transit

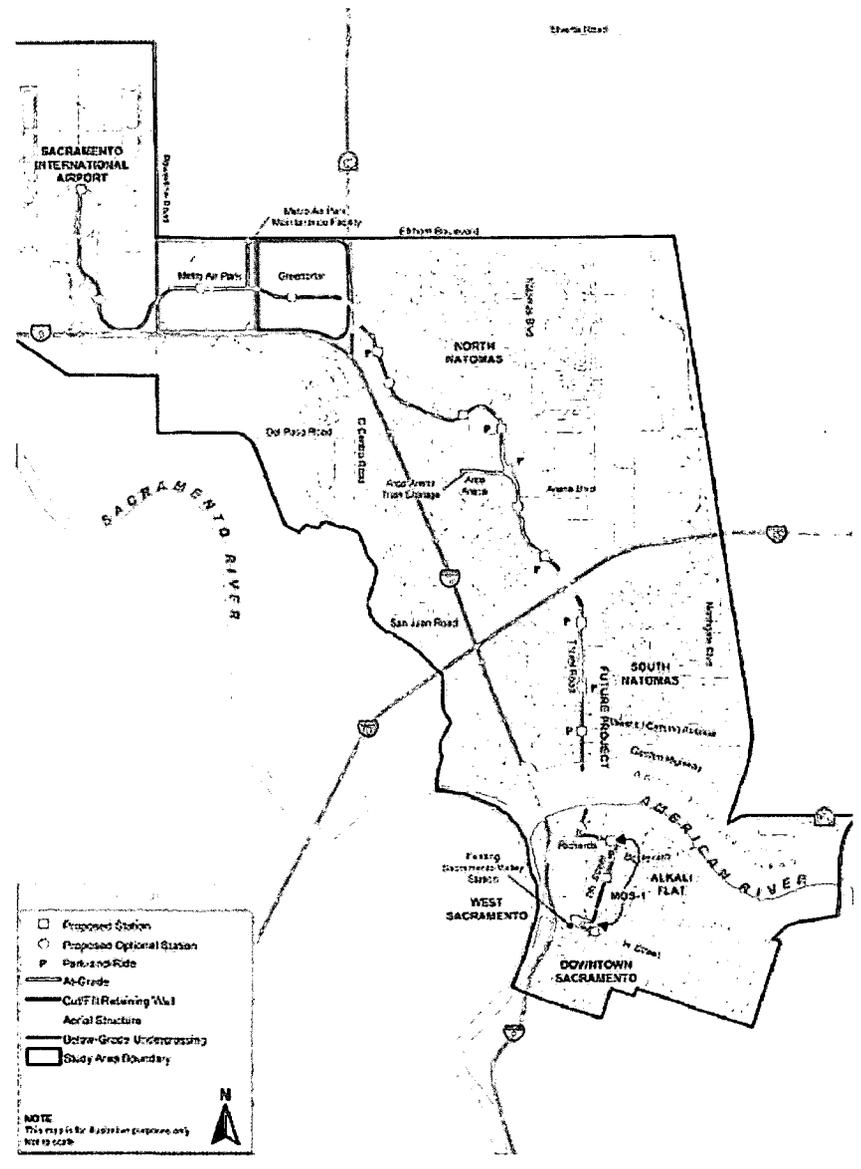
Advancing the Green Line LRT Project

Status Update

August 24, 2010



Planning History



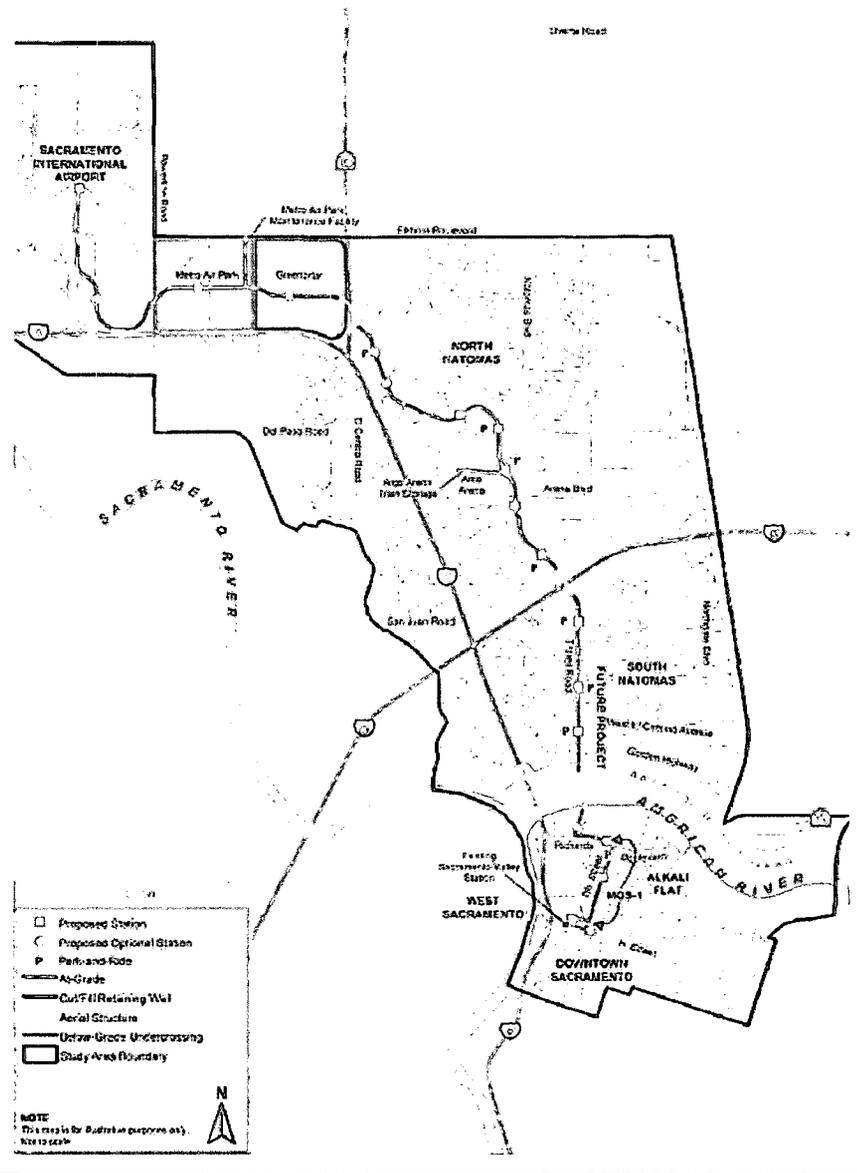
- Multiple studies since 1980s
- 2001-2003 Alternatives Analysis evaluated:
 - Two transit modes
 - Five alignments
- LPA adopted by RT Board 2003
- Included in
 - Community and General Plans
 - SACOG Metropolitan Transportation Plan
- Certification of Program EIR on April 28, 2008



**Downtown
Natomas
Airport**

Green line to the Airport

Locally Preferred Alternative



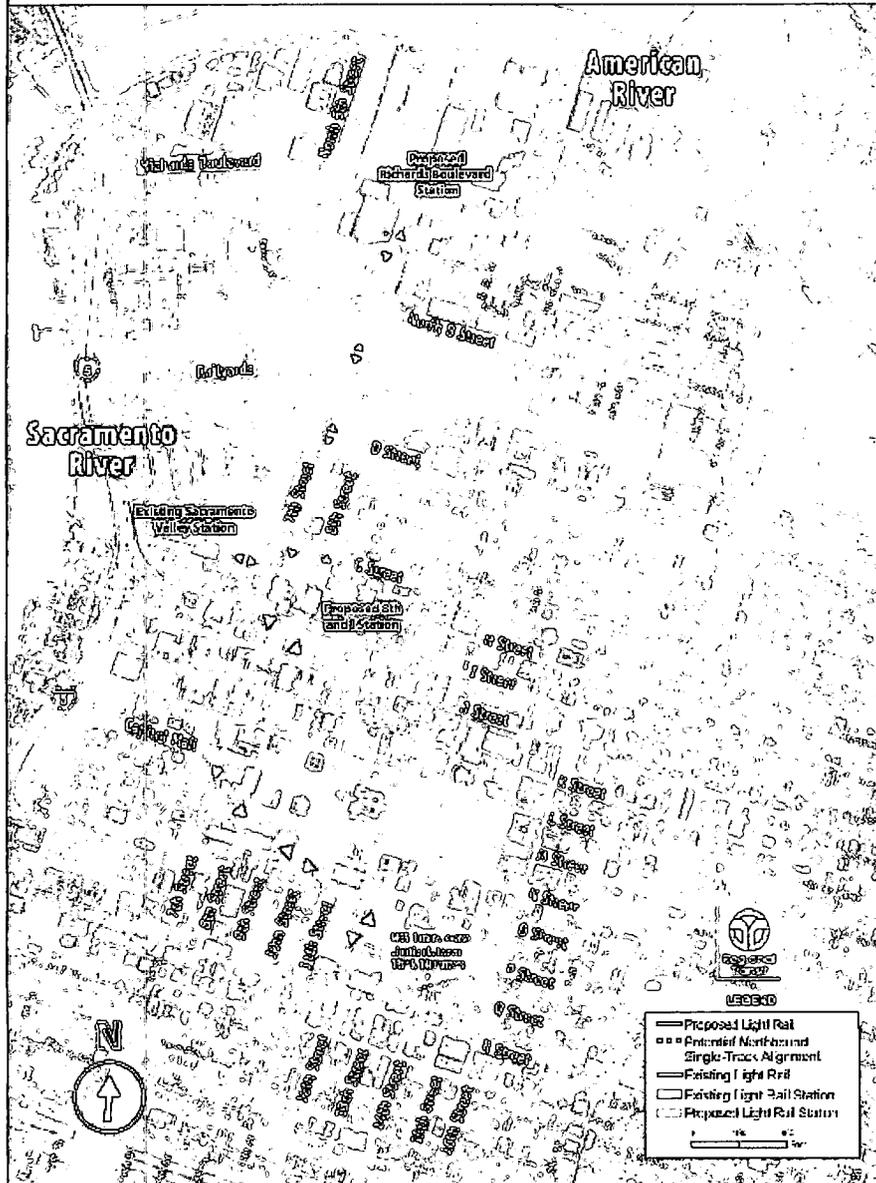
- Double-Track LRT
- Downtown to Airport
- Truxel Alignment
- 22 Light Rail Vehicles
- Maintenance Facility
- Cost: \$785M (2006 \$)



Downtown
Natomas
Airport

Green Line to the Airport

Green Line to River District



- Final EIR Certified 4/09
- 30% Design Completed 7/09
- 1st Design-Build LRT Extension for Regional Transit
- Design-Build Started 11/09
- Opening Scheduled late January 2011



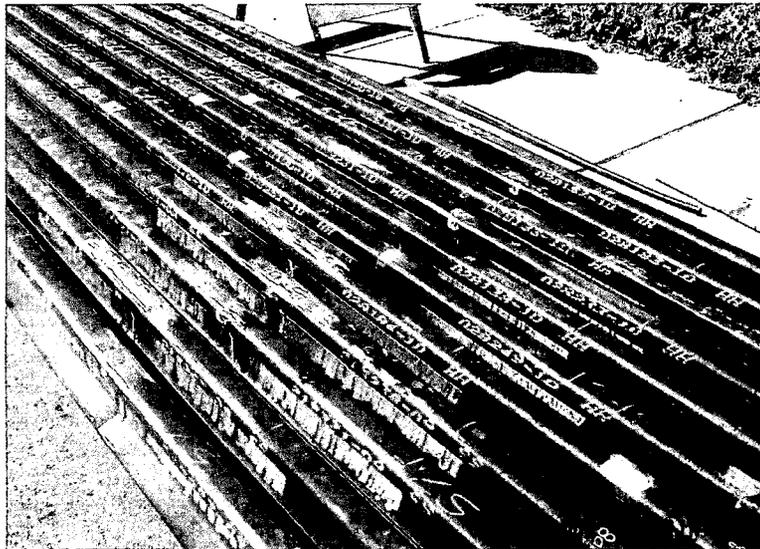
Downtown
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Green Line to the Airport

Green Line to River District



- Construction activities have been focused along 7th Street

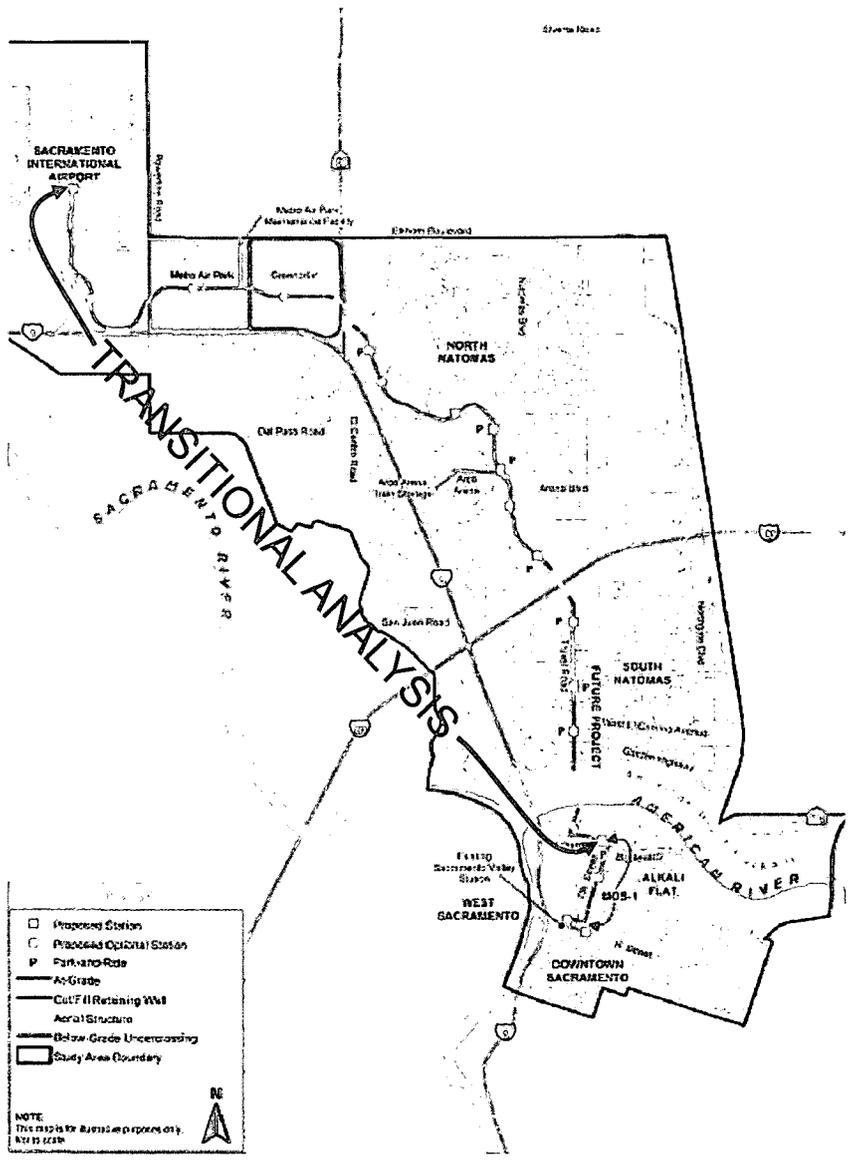


Transitional Analysis



For Green Line past 7th & Richards

- Identify most cost effective 2nd phase (capital and O+M costs, ridership)
- For segments of the LPA adopted with design options, recommend preferred option as basis for Cost-Effectiveness Index (CEI)
- Develop timeline and action plan for re-entering FTA New Starts Process

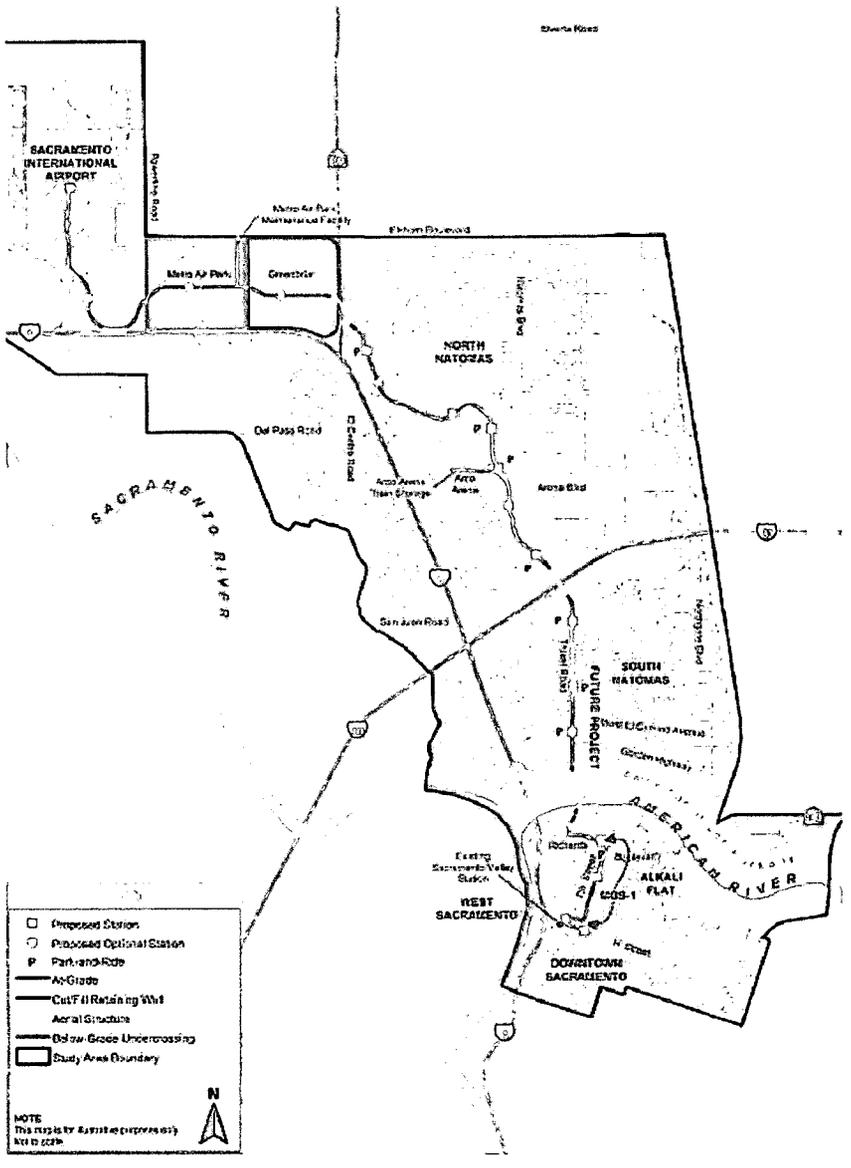




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Green Line to the Airport

Transitional Analysis



- Risk Assessment and Value Engineering at kick-off generated many ideas for cost savings.
- Project Review Workshop last summer identified areas for focused study.



**Downtown
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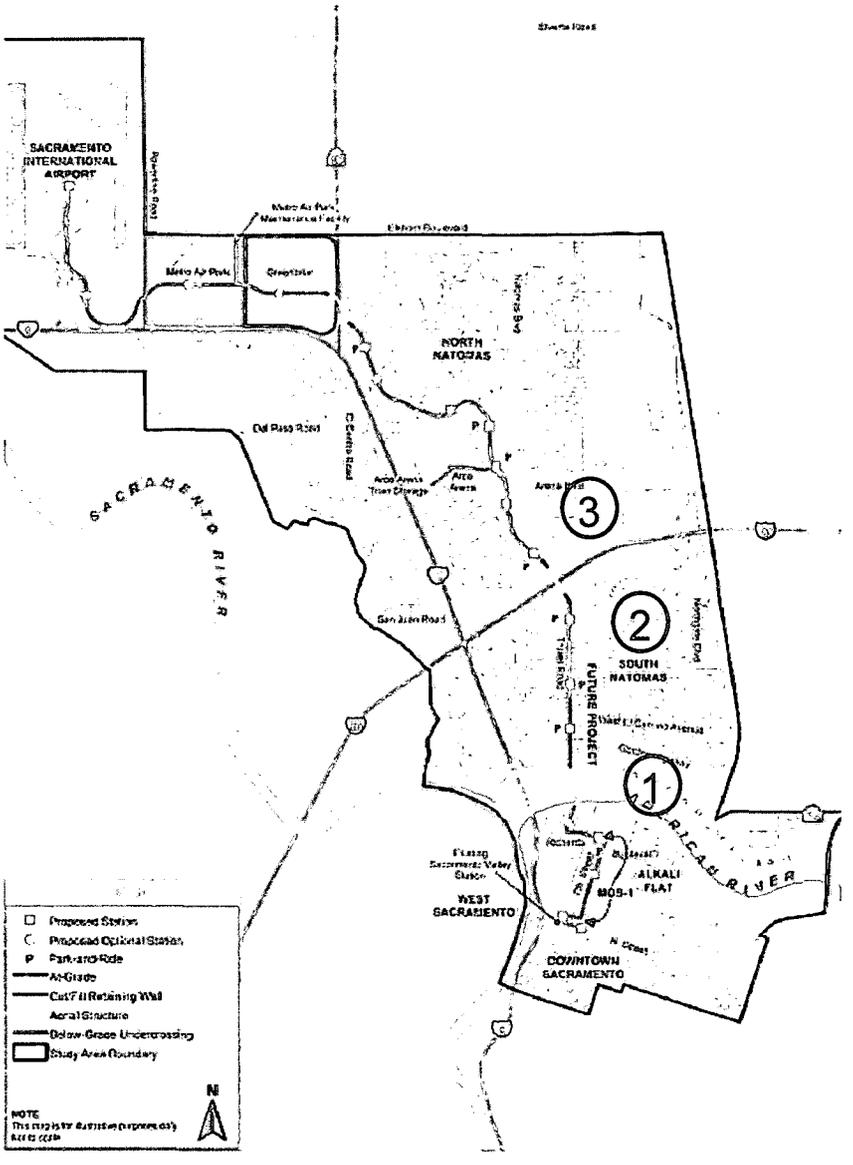
Green Line to the Airport

LPA Design Options



Regional Transit

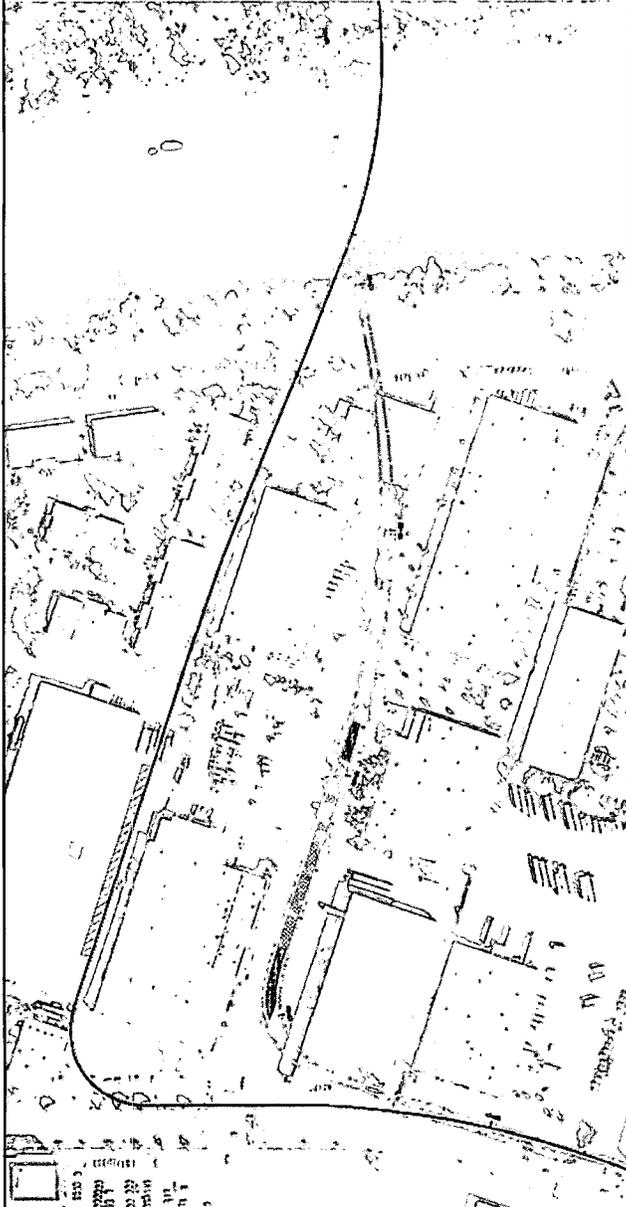
1. Sequoia Pacific – American River Bridge
2. South Natomas (17 options-- combinations of mixed-flow, exclusive median, double and single-track shared left turns, roundabouts, and staggered platforms)
3. I-80/Gateway Park Blvd (9 options—east, west, median, north, south, centered)



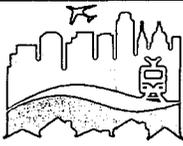
THE HOYT COMPANY

August 24, 2010

Sequoia Pacific-American River Bridge



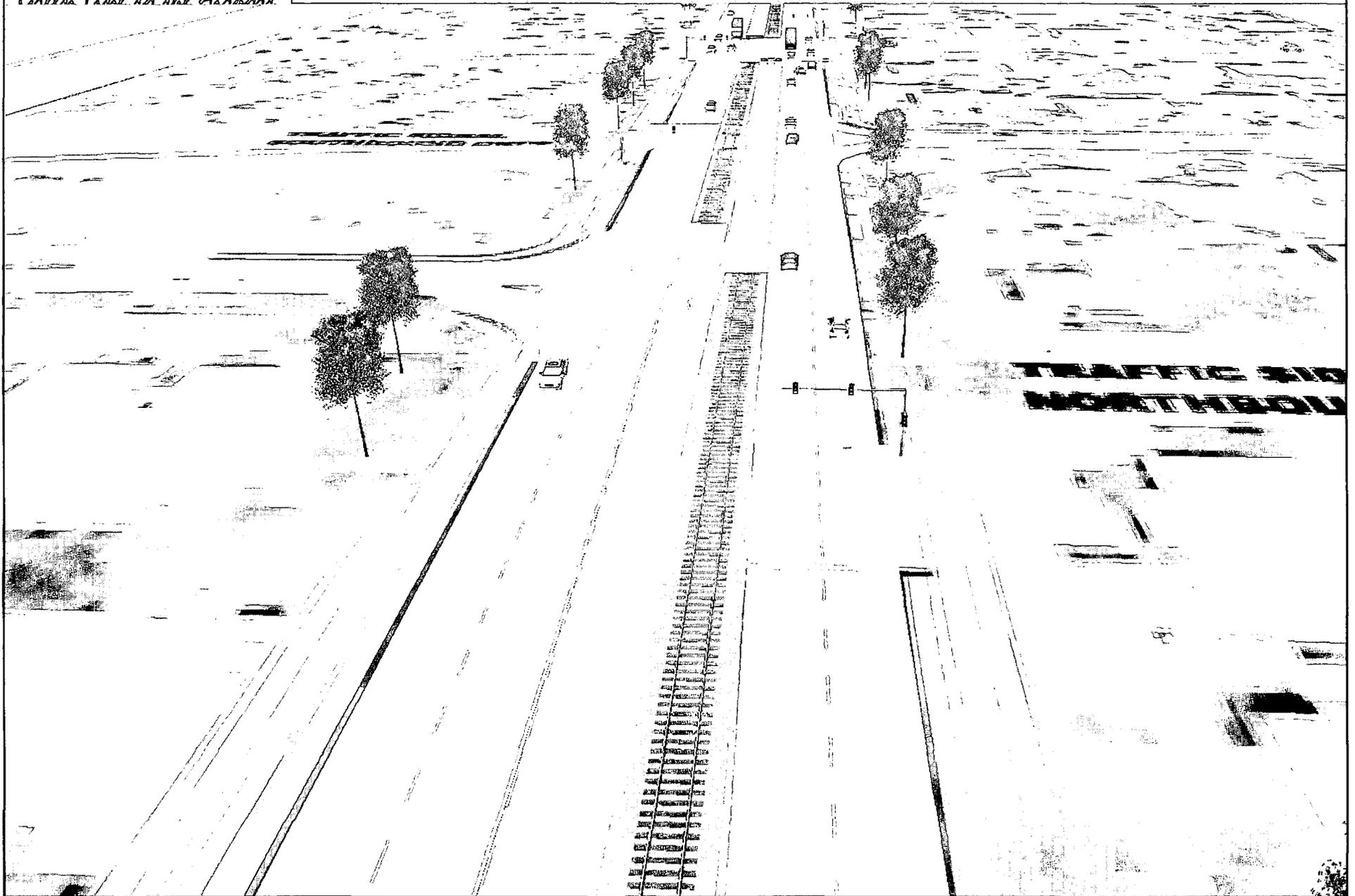
- City requested shifting tracks from railroad siding to Sequoia Pacific Boulevard for consistency with the River District Specific Plan (under development).
- Significant cost savings are possible by changing bridge type from segmental to pre-cast concrete or steel.



Downtown
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Green Line To The Airport

South Natomas Shared Left Turn

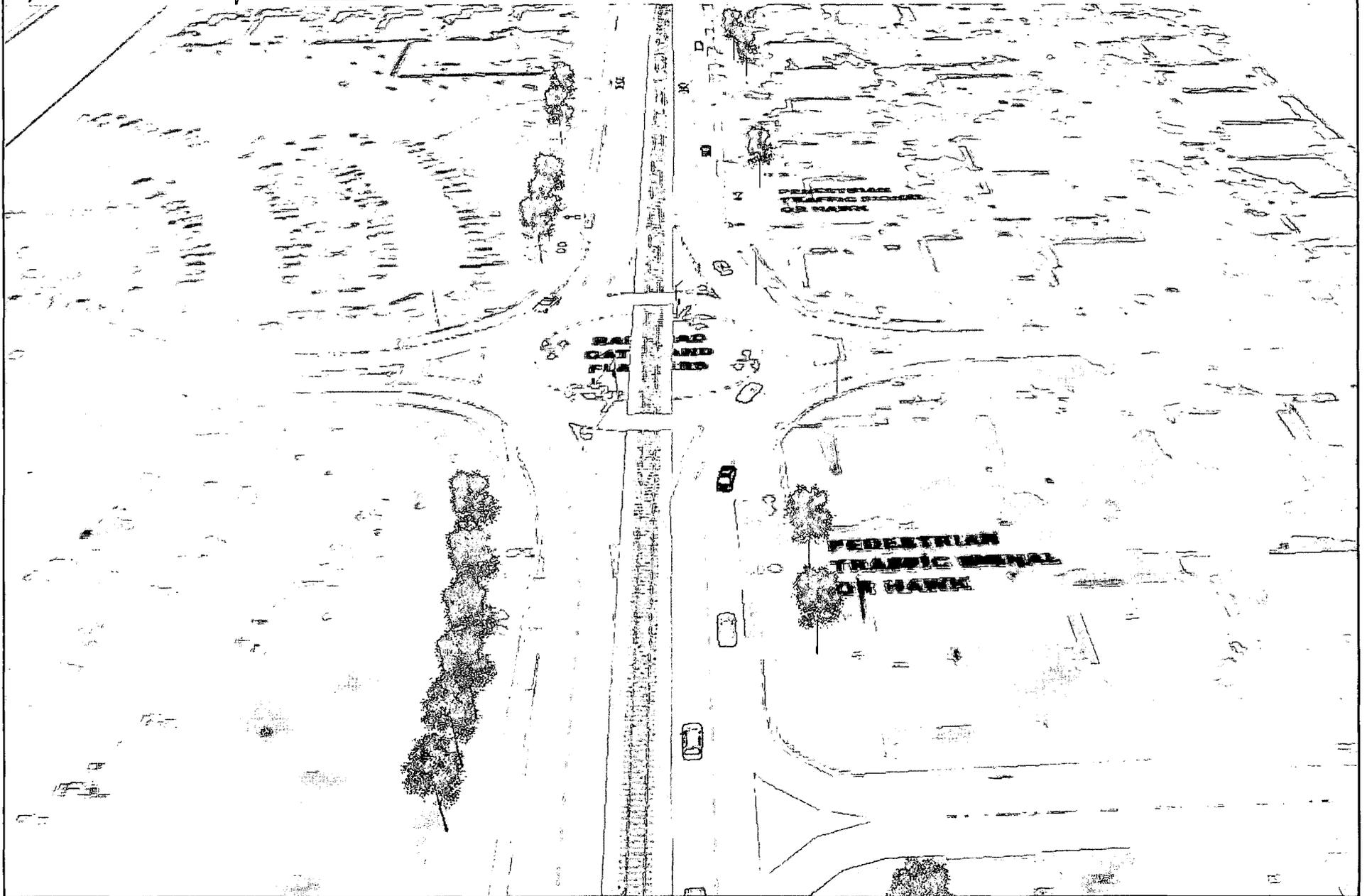




Downtown
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Green Line to the Airport

South Natomas Roundabout





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Green Line to the Airport

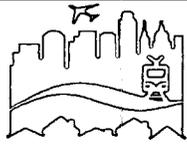
I-80 / Gateway Park Blvd



Regional Transit



- LRT at-grade at Gateway Park Blvd = 4 minute delay per car.
- Recommend grade separation.
- I-80 bridge designed for LRT in median.
- Over off-ramp, Gateway Park Blvd, and channel.
- Tracks in exclusive median by bridge widening or new bridge for bikes and peds to east.
- Elevated station with pedestrian bridges and elevators.



Downtown
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Green Line to the Airport

Ridership Improvements



Ridership estimate has doubled, and user benefit (travel time savings) improved by more than 50 percent:

- Changed Forecast Year from 2025 to 2035
- Added new roadways and bus feeders
- Adjusted bus stops and travel times
- Added development
- Modified baseline option
- Updated airport ground access model
- Adjusted downtown parking prices



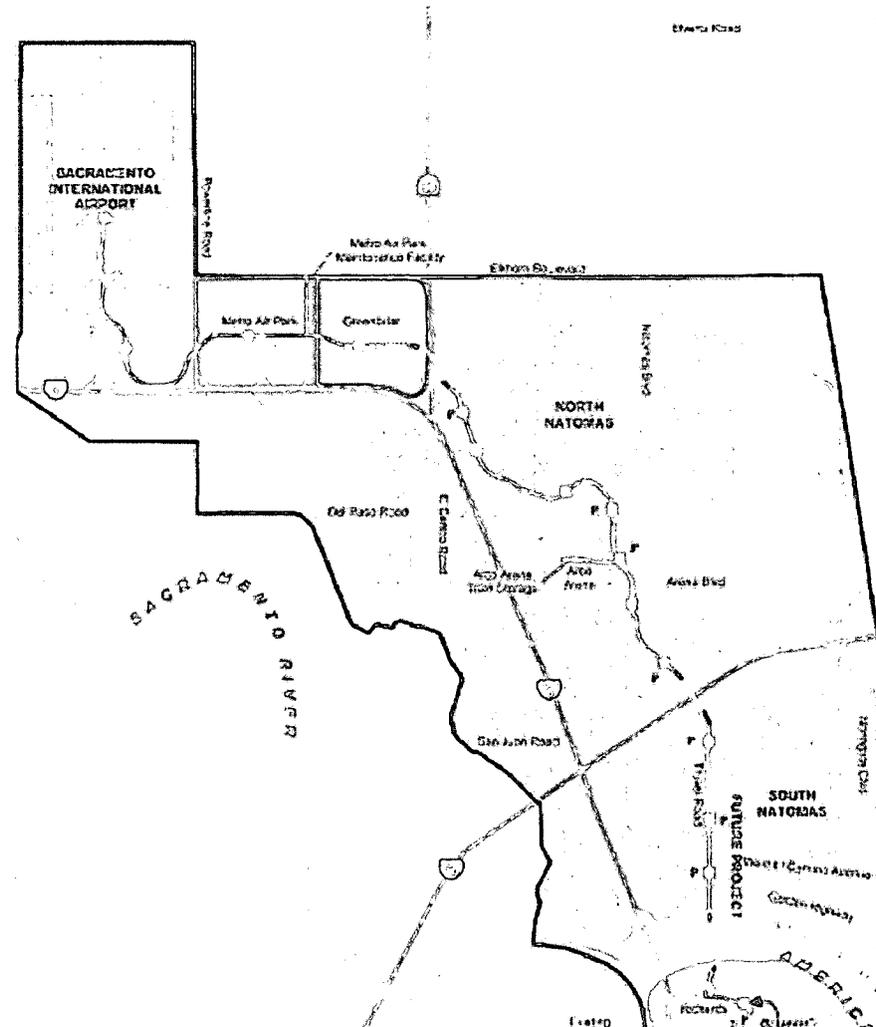
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Green Line to the Airport

Improve Ridership at Each Station



Station	Dist (mi) to next station	Projected Riders
8th & H/County Center	0.59	1034
Railyards (future)	0.42	6260
7th & Richards/Township 9	0.41	1940
Sequoia Pacific	0.90	1430
West El Camino	0.59	2330
Pebblestone Way	0.63	800
San Juan Road	0.73	2050
Gateway Park Blvd	0.63	3360
Arena Blvd	0.43	840
Arco Arena	0.40	1030
East Town Center	0.55	290
North Natomas Town Center	0.88	1400
Commerce Parkway	0.41	760
Club Center	1.13	420
Greenbriar	0.93	510
Metro Air Park	2.69	720
Airport		1260
TOTAL	12.32	26,434

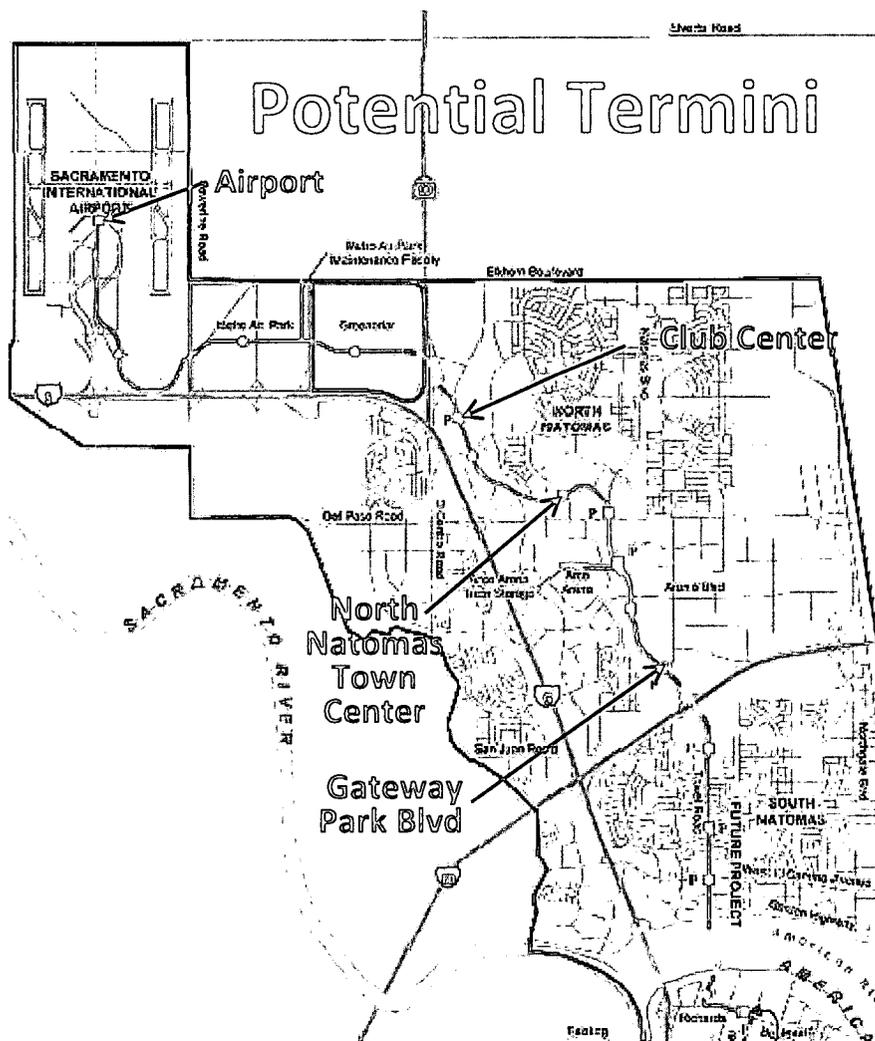




**Downtown
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Green Line to the Airport

Phasing Recommendation



- Finalize configuration
- Prepare capital and O+M cost for each potential termini
- Ridership run for each potential termini
- Cost-effectiveness for each potential termini
- Recommend project that will effectively compete for New Starts funding



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Green Line to the Airport

Amendment for Station Area Planning



- Opportunity to further increase ridership by focusing on the relationship between the station and the community, making sure that ridership is consistent with proposed development, and looking for opportunities to provide increased access by park-and-ride.
- Using latest development plans, evaluate stations and make recommendations to optimize ridership.
- Also includes exhibits for community workshops, outreach assistance, recommendations for siting the maintenance facility, and additional administrative expenses.

Next Steps

Station Area Planning

July/August  Regional Transit

Community Workshops

August 5, 12, 19 and 25
to 28

Estimates for Capital Cost, Operating
Cost, and Ridership

September/October

Transitional Analysis Report

October/November

Recommendations to RT Board

November

Request to FTA to Enter Preliminary
Engineering/Begin EIS

Immediately upon FTA
approval of higher
priority South
Sacramento Corridor
Phase 2 Final Design

Revised 7/22/10