



# City of Sacramento City Council

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**Meeting Date:** 4/21/2011

**Report Type:** Public Hearing

**Title:** Provence Project (P09-006) (Noticed on 4-8-11)

**Report ID:** 2011-00305

**Location:** District 1

**Recommendation:** Conduct a public hearing and upon conclusion, adopt 1) a Resolution adopting the Mitigated Negative Declaration and the Mitigation Monitoring Plan, 2) a Resolution approving the PUD Schematic Plan Amendment for the Natomas Crossing PUD (Area 3), and 3) a Resolution approving the Provence project.

**Contact:** David Hung, Associate Planner, (916) 808-5530, Community Development Department; Lindsey Alagozian, Senior Planner, (916) 808-2659, Community Development Department

**Presenter:** David Hung, Associate Planner, (916) 808-5530, Community Development Department

**Department:** Community Development Dept

**Division:** Planning

**Dept ID:** 21001221

**Attachments:**

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- 1-Description Analysis
  - 2-Background
  - 3-Vicinity Map
  - 4-Resolution CEQA
  - 5-Resolution PUD Schematic Plan Amendment
  - 6-Resolution Project Approval
  - 7-WALKSacramento Comment Letter
  - 8-Mitigated Negative Declaration

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**City Attorney Review**

Approved as to Form  
Sabina D. Gilbert  
4/8/2011 9:24:56 AM

**City Treasurer Review**

Prior Council Financial Policy Approval or  
Outside City Treasurer Scope  
Russell Fehr  
4/5/2011 10:10:21 AM

**Approvals/Acknowledgements**

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Department Director or Designee: Max Fernandez - 4/7/2011 3:42:47 PM

Assistant City Manager: Cassandra Jennings - 4/7/2011 6:23:10 PM

Eileen Teichert, City Attorney

Shirley Concolino, City Clerk  
Gus Vina, Interim City Manager

Russell Fehr, City Treasurer



## Background

**Background Information:** On June 24, 1997, the City Council approved a Development Agreement and Rezone (P96-084) to designate this area as the Natomas Crossing - Alleghany #3 Planned Unit Development (PUD). Also approved were development guidelines and a schematic plan for the PUD. On May 8, 1997, the Planning Commission approved a Tentative Master Parcel Map for the site (P96-084). The current project site is a small portion of the Natomas Crossing – Alleghany Area #3 Planned Unit Development area.

On June 25, 2002, the City Council approved a General Plan Amendment, Community Plan Amendment, and a Rezone (P01-028) to re-configure the land use designations for the overall Natomas Crossing – Alleghany Area #3 PUD area. Also approved were PUD development guidelines and schematic plan amendments. On June 6, 2002, the Planning Commission approved a Tentative Map, Subdivision Modifications, and a Special Permit for an office building (for a parcel south of this location).

On January 17, 2006, the City Council approved a PUD Schematic Plan Amendment (P05-079) to re-designate this site for multi-family development in the Natomas Crossing – Alleghany Area #3 Planned Unit Development (PUD). On October 13, 2006, the Planning Commission approved a Tentative Subdivision Map to create one 10.9± net acre condominium parcel and a PUD Special Permit to develop a 187 unit condominium complex in the Natomas Crossing – Area #3 PUD.

On February 22, 2007, the Planning Commission approved the entitlement to revoke the Special Permit to develop the 187 unit condominium complex and approved a Tentative Map, Subdivision Modification and Special Permit to develop a 187 unit townhouse complex (P06-194). The site is currently developed with five of the 187 units, along with interior streets and the pool and pool house.

**Public/Neighborhood Outreach and Comments:** The project was routed to the following neighborhood advisory groups: Heritage Park HOA; Natomas Community Association (NCA); North Natomas Alliance (NNA); North Natomas Community Association (NNCA); Natomas Park Master Association (NPMA); Regency Park Neighborhood Association (RPNA); SABA; Terrace Park Neighborhood Association; WALKSacramento; and Witter Ranch Neighborhood Association. No comments were received. WALKSacramento has provided comments for the project and is attached to the staff report. In response to the comments staff is conditioning that the final landscape and circulation plans be submitted to the Planning Division for review and comment prior to submittal for building permits. Staff has therefore addressed all of WALKSacramento's concerns.

**Employment Center Zoning:** The development of a multi-family residential use in the employment center zone is considered a non-primary use ancillary to the primary use intended for the greater area of the employment center zoned parcels within a Planned Unit Development. A maximum of twenty-five percent (25%) of the PUD net acreage

may be designated for and devoted to residential uses. The proposed project will not exceed the 25% residential threshold in the Natomas Crossing PUD. The Zoning Code allows an entitlement process to exceed the maximum allowable area of 25% residential use within a PUD if it exists within the specific area delineated as being bounded by the East Drain, I-5, Del Paso Road and Arena Boulevard. This geographic area contains approximately 340 net acres. The Zoning Code allows 25% of the defined area to be residential which equates to approximately 85 net acres residential use. The proposed project in combination with existing multi-family residential development will total approximately 72.28 net acres of residential development leaving a remainder of approximately 12.72 acres available for residential use, provided the appropriate findings can be made.

Within the defined geographic area which allows up to 25% residential use, the other approved or constructed multi-family complexes include the Bella Rose Condominiums, the Ashton Parc Apartments, the Tuscaro Apartments, Arena Seniors, and the Fairfield Apartments. With the previously approved project on the subject site, the total amount of acreage devoted to residential uses within the defined geographic area is 72.28 acres and thus will not exceed the maximum 25%, or 85 acres, as shown on the following table.

<b>Item</b>	<b>Area/Project</b>		<b>Net Acres</b>	<b>Description</b>
1	Natomas Crossing PUD	(EC-50, R-2B)-PUD	<16.9 na>	Fairfield Apartments (P01-014)
2	Natomas Crossing PUD	EC-50-PUD	<10.9 na>	Provence (P06-194)
3	Arena Corporate Center PUD	EC-40-PUD	<10.85 na>	Bella Rose Condominiums (P03-162)
4	Arena Corporate Center PUD	EC-40-PUD, AOS	<16.9 na>	Tuscaro Apartments (P98-042)
5	Arena Corporate Center PUD	EC-40-PUD	<8.23 na>	Ashton Parc Apartments (P04-240)
6	Arena Corporate Center PUD	EC-40-PUD, EC-80-PUD	<8.5 na>	Arena Seniors (P08-013)
	<b>Total Residential (in defined geographic area)</b>		<b>72.28 na</b>	
	<b>Total Net Acreage in defined area</b>		<b>340 net acres</b>	<b>Allowable acres for residential if criteria met = 85 na</b>
	<b>% Residential in Defined Area</b>		<b>21.3%</b>	

The following table shows a list of multi-family housing projects in the North Natomas Community Plan area, including apartments and condominiums:

<b>Location</b>	<b>Project Name</b>	<b>Type</b>	<b>Units</b>
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<b>West of Interstate 5</b>	Lofts (P02-084)	Apartment	188 (app'd)
	Atrium Court (P02-035)	Apartment	224 (app'd)
	Irongate (P98-071)	Apartment	280 (app'd)
	Terracina Meadows (P01-050)	Apartment	148 (app'd)
	Hurley Creek (P06-007)	Apartment	208 (app'd)
	Valencia Point (P05-212)	Apartment	168 (app'd)
	Cambay West (P03-047)	Apartment	216 (app'd)
	Duckhorn Village (P06-201)	Apartment	75 (app'd)
	Vista del Lago (P06-093)	Condominium	219 (app'd)
	Westlake Villas (P01-053)	Condominium	285 (app'd)
	Brias del Lago (P06-139)	Townhouses	126 (app'd)
		Subtotal:	2137
<b>East of Interstate 5</b>	Homecoming (P01-115)	Apartment	450 (app'd)
	Bella Rose (P03-162)	Condominium	201 (app'd)
	Ashton Parc (P04-240)	Apartment	168 (app'd)
	Villagio (P99-059)	Apartment	272 (app'd)
	Creekside Crossing (P05-177)	Condominium/ Townhouses	434 (app'd)
	Tuscaro (P98-042)	Apartment	296 (app'd)
	Granite Pointe (P01-014)	Apartment	384 (app'd)
	Terracina Gold (P99-142)	Apartment	280 (app'd)
	Natomas Field (P04-236)	Townhouses	211 (app'd)
	JMA/Laing Condos (P05-164)	Condominium	92 (app'd)
	JMA/St. Anton (P05-136)	Apartment	108 (app'd)
	McKenzie (P01-016)	Apartment	152 (app'd)
	Miramonte/Trovass (P99-082)	Apartment	440 (app'd)
	Carriage Lane I (P03-085)	Condominium	156 (app'd)
	Carriage Lane II (P04-167)	Condominium	39 (app'd)
	Provence (P06-194)	Townhouses	187 (app'd)
	Syrah (P02-132)	Condominium	245 (app'd)
	Amara (P04-087)	Condominium	200 (app'd)
	Natomas Park (P01-100)	Apartment	212 (app'd)
	Broadstone (P04-096)	Condominium	142 (app'd)
	Regency Park (P04-065)	Condominium	135 (app'd)
	Carefree (P00-005)	Apartment	500 (app'd)
	Northpointe (P03-046)	Apartment	180 (app'd)
	Terraces (P04-196)	Condominium	321 (app'd)
	Natomas Place (P06-124)	Apartment	135 (app'd)
	Natomas Market Rate (P08-047)	Condominium	120 (app'd)
	Vintage at Natomas Field (P05-116)	Apartment	200 (app'd)

	Hampton Village (P04-058)	Condominium	264 (app'd)
	Heritage Point (P07-035)	Condominium	229 (app'd)
	Arena Seniors (P08-013)	Apartment	240 (app'd)
		Subtotal:	6,993
<b>Total Multi-Family Units in North Natomas:</b>			<b>9,130</b>

There are a total of 9,130 approved multi-family dwelling units in the North Natomas Community Plan area as of February 2011. The project, if approved, will add an additional 51 units of multi-family housing.

The proposed condominium use is compatible with the adjacent/proposed future uses within the PUD, and the site can be adequately served by public facilities, transit and open space. This project is a high density development and has 21 dwelling units per net acre; however, circulation is eased by the fact that the development is bordered by three streets and allows easy access to the surrounding area. In conclusion, staff supports the project and finds the proposed project is in compliance with the goals and policies of the North Natomas Community Plan.

***Natomas Basin Habitat Conservation Plan (NBHCP):*** The 1994 North Natomas Community Plan required the development and implementation of a Habitat Conservation Plan as mitigation for development in North Natomas. In 1997, the NBHCP was approved by the City of Sacramento, USFWS, and CDFG.

The NBHCP is a conservation plan supporting application for incidental take permits (ITP's) under Section 10(a)(1)(B) of the Endangered Species Act and under Section 2081 of the California Fish and Game Code. The purpose of the NBHCP is to promote biological conservation while allowing urban development and continuation of agriculture within the Natomas Basin. The NBHCP establishes a multi-species conservation program to mitigate the expected loss of habitat values and incidental take of protected species that would result from urban development, operation of irrigation and drainage systems, and rice farming. The goal of the NBHCP is to preserve, restore, and enhance habitat values found in the Natomas Basin.

To support the issuance of an ITP, an Environmental Assessment was prepared by the USFWS for the National Environmental Policy Act requirement and a Negative Declaration was prepared by the City of Sacramento for the California Environmental Quality Act (CEQA) requirement. The USFWS and CDFG issued ITP's to the City of Sacramento. The NBHCP and ITP were subsequently challenged, and on August 15, 2000, the United States District Court, Eastern District, ruled that the ITP was invalid and an EIS was required for the project. Based on this ruling, the City of Sacramento and Sutter County jointly prepared a revised NBHCP and an Environmental Impact Report/Environmental Impact Statement (EIR/EIS) for use by the USFWS and CDFG. The USFWS is the lead federal agency for the preparation of the EIS and the City of Sacramento and Sutter County are co-lead agencies for the preparation of the EIR. The Sacramento City Council adopted the revised NBHCP and EIR/EIS on May 13, 2003. On June 27, 2003 the USFWS issued a new Incidental Take Permit for the

NBHCP for development within the Natomas Basin. This project is subject to the requirements of the revised HCP/ITP. HCP fees have been paid and the site has been graded, thereby complying with the requirements of the HCP/ITP.

**Mixed Income Inclusionary Housing Ordinance:** The project is allowed in the EC zone due to amendments to the Community Plan (Resolution 2002-047) and the EC Zone (Ordinance 2002-001) adopted in 2002. These amendments allow for the residential component of EC development within the geographic area bounded by the East Drain, Interstate 5, Del Paso Road and Arena Boulevard to exceed 25% within the individual PUD with the approval of a special permit; the subject site is within the above referenced geographic area.

Per section 17.190.070(E) of the Zoning Ordinance, any residential project in the North Natomas community plan area which is the subject of a development agreement executed on or before June 20, 2000 shall be exempted from the inclusionary housing component, unless subsequent to June 20, 2000 the residential project requires the approval of one or more legislative entitlements or amendments to legislative entitlements which are major rather than minor, in which case the residential project shall not be exempt from inclusion of the inclusionary housing component. Per section 17.190.020 of the Zoning Ordinance, "minor legislative entitlements" means legislative entitlements or amendments to legislative entitlements which satisfy one or more of the following:

1. Entitlements that do not result in any of the following criteria as defined by the North Natomas community plan target average densities: a net loss of residential acreage; a net loss of acreage of land designated for high density residential (HDR) or medium density residential (MDR) development, unless the HDR total residential units replace the loss of MDR residential units; or a net loss of total residential units;
2. Entitlements that are the result of, and required by, amendments to public facilities or roadways designated in the North Natomas community plan; provided further that the entitlements are limited to addressing the amendments required by the city or other public agency; or
3. Entitlements that are limited to amendments to a previously approved PUD schematic plan, tentative map, or PUD development guidelines, provided that the amendments do not result in a loss of more than five (5) percent between the density of the proposed project and the density of the previously approved project.

The project site was under a Development Agreement approved prior to June 20, 2000. Staff has found that the 2002 amendments to the Community Plan and the EC Zone as well as the required PUD Schematic Plan Amendment for the project fall within the definition of a minor legislative entitlement per the criteria discussed above; the project does not result in a net loss of residential acreage and the entitlements are limited to amendments to a previously approved PUD schematic plan, Plan Review and Special Permits. As a result, the project is exempt from the Mixed Income Housing Ordinance under section 17.190.170.

**Multi-Family Residential Design Guidelines:** The Multi-Family Residential Design Guidelines were approved by the City Council on August 5, 2000 (Resolution CC2000-487). This document articulates design principles for multi-family residences to assist the Planning Commission, City Council, City staff and project planners and designers by identifying the City’s design criteria for multi-family development. The intent is to achieve well-designed projects to enhance the community’s overall value and appearance. The project is generally consistent with the Multi-Family Residential Design Guidelines as identified in the building design section of this staff report.

**Project Design:** The following discussion outlines the entitlements required for the project including the PUD Schematic Plan Amendment, the Tentative Map, the Special Permit for condominium development and the Special Permit Modification to the previous approval.

**Land Use**

***PUD Schematic Plan Amendment***

The applicant is proposing a PUD Schematic Plan Amendment to designate a 233-unit condominium complex on 10.8 net acres within the Natomas Crossing PUD. Applicants wishing to obtain entitlements in order to proceed to the construction phase of development are required to submit a Schematic Plan along with other drawings delineating the anticipated developments proposed in the near future for a site. The existing Schematic Plan depicts the site with 187 townhouse units. The proposed Schematic Plan Amendment delineates the use and density of the project under consideration. Since the PUD Schematic Plan Amendment reflects an increase in density of more than 10% of the existing condition, the project requires approval of the City Council.

<b>Table 4A: Existing schematic plan summary for project</b>						
<b>Project Designation</b>	<b>Proposed Land Use Designation</b>	<b>Gross Acres</b>	<b>Net Acres</b>	<b>Units</b>	<b>Buildings</b>	<b>Density</b>
Carriage Lane 3	Residential (Townhouses)	13.9	10.9	188	38	17.2 units per net acre

<b>Table 4B: Proposed schematic plan summary for project</b>						
<b>Project Designation</b>	<b>Proposed Land Use Designation</b>	<b>Gross Acres</b>	<b>Net Acres</b>	<b>Units</b>	<b>Buildings</b>	<b>Density</b>
Provence (P09-006)	Residential (Condos)	13.9	10.8	233	134	22 units per net acre
Provence (P06-194)	Residential (Townhouses)		0.2	5	5	

The development of a multi-family residential in the employment center zone is considered a non-primary use ancillary to the primary use intended for the greater area of the employment center zoned parcels within a Planned Unit Development. Staff finds that the PUD Schematic Plan Amendment conforms to policies of the General Plan and North Natomas Community Plan to provide adequate housing sites and opportunities for all households and to promote efficient development within a new growth area. Furthermore, the PUD Schematic Plan Amendment will not be injurious to the public welfare, nor to other properties in the vicinity of the development in that the project is compatible with adjacent developments and the site will be developed according to the requirements of the PUD Guidelines.

***Tentative Map design***

Map Design: The tentative map proposes to subdivide 182 residential lots and 38 common lots (per approval of file P06-194) into 28 residential condominium lots, 5 private street lots, 16 private drive lots and one landscape lot on 10.8 net acres. The tentative map design is summarized below:

<b>Table 5: Map Design Summary</b>		
<b>Lot Number:</b>	<b>Total Net Acreage:</b>	<b>Use:</b>
1 - 28	7.7	Condominium Units
A – P	1.0	Private Drives
Q - U	1.8	Private Streets
V	0.3	Landscape Lot

The project creates condominium parcels which consist of an undivided interest in common in a portion of real property coupled with a separate interest in space called a unit, the boundaries of which are described on a recorded final map, parcel map, or condominium plan in sufficient detail to locate all boundaries thereof. [Civil Code Section 1350 (f)].

Vehicular Circulation and Parking: Access to the project site is provided at Benefit Way to the north and Advantage Court to the west. The main vehicular access is the northern driveway and secondary vehicular access is provided to the west. The units along Advantage Court provide direct pedestrian access to the sidewalks along this street. Pedestrian connections are also provided from the interior of the site to the sidewalks along Benefit Way and East Commerce Way.

Pedestrian Circulation: Existing sidewalk and rolled curb are found at the frontage on East Commerce Way, Benefit Way and Advantage Court. The project does not impact or change existing circulation in and around the site.

Walls and Fencing: Existing fencing at the pool area will remain. New fencing is to be

installed all along the southern perimeter of the site. Fencing abutting the vacant adjacent parcel to the west shall be provided when the site is developed. As required by the Fire Department, a gate will be installed at the emergency vehicle access entrance at East Commerce Way.

On December 15, 2010, the Subdivision Review Committee, with all ayes, voted to recommend approval of the proposed Tentative Map, subject to the conditions of approval as found in Attachment 1.

In evaluating tentative maps, the City Council is required to make the following findings:

- A. None of the conditions described in Government Code Section 66474, subsection (a) through (g), inclusive, exist with respect to the proposed subdivision;
- B. The proposed subdivision, together with the provisions for its design and improvement, is consistent with the City General Plan and Title 16 Subdivisions of the City Code, which is a specific plan of the City (Gov. Code §66473.5);
- C. The discharge of waste from the proposed subdivision into the existing community sewer system will not result in a violation of the applicable waste discharge requirements prescribed by the California Regional Water Quality Board, Central Valley Region, in that existing treatment plants have a design capacity adequate to service the proposed subdivision (Gov. code §66474.6);
- D. The design of the proposed subdivision provides, to the extent feasible, for future passive or natural heating and cooling opportunities (Gov. Code §66473.1);
- E. The City Council has considered the effect of the approval of this tentative subdivision map on the housing needs of the region and has balanced these needs against the public service needs of its residents and available fiscal and environmental resources (Gov. Code §66412.3).

Staff finds that the Tentative Map is consistent with the policies of the General Plan and Title 16 of the City Code. The site is physically suitable for the type of development proposed and suited for the proposed density. The design of the subdivision and the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife their habitat, and the design of the subdivision and the type of improvements will not conflict with easements, acquired by the public at large, for access through or use, of, property within the proposed subdivision. The project will not overly burden the sewer system, nor will it preclude future passive or natural heating and cooling opportunities.

The Carriage Lane III development project, previously approved for this site, has a Private Recreational Facilities Agreement (City Agreement 2006-0298). The Agreement

allows 5% parkland dedication credit in exchange for the development of a private pool for the use of all residents in the 187 unit Carriage Lane III subdivision. The Agreement is specific to the Carriage Lane III project and is not transferable to this project. The applicant for the Provence project has requested a new Private Recreational Facilities Agreement. The new Provence Private Recreational Facilities Agreement will be for the development of a private pool for the use of all residents in the 233 unit development, plus the five existing model homes from the Carriage Lane III project. Pursuant to Section 16.64.120 of the Sacramento City Code and at the time of the hearing on the tentative subdivision map, the Planning Commission made a recommendation in favor of the request for a Private Recreational Facilities Agreement to the City Council. Staff supports a new Private Recreational Facilities Agreement, with the remainder of the parkland dedication obligation to be fulfilled through the payment of in lieu fees.

### ***Special Permit***

The applicant proposes to develop 223 condominium units on approximately 10.8 net acres in the Employment Center Planned Unit Development (EC-50-PUD) zone. Section 17.192 of the Zoning Code permits new condominium developments with the issuance of a special permit. In evaluating special permit proposals of this type, the City Council is required to make the following findings:

1. A special permit shall be granted upon sound principles of land use.  
Staff finds that the proposed development is an appropriate land use that will have positive contribution to the surrounding area, in that the project site is in close proximity to future commercial and open space uses and that the site will be well served by auto, bicycle, and pedestrian linkages.
2. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.  
Staff finds that the proposed condominium development site and building design are consistent with the Natomas Crossing PUD Guidelines and the Multi-Family Residential Design Principles and will not be detrimental to public health, safety or welfare.
3. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.  
The proposed project is consistent with the goals and policies of the employment center designation in the General Plan and the North Natomas Community Plan.

Staff believes that the current proposal is a well designed project in that it provides various housing types and different architectural variations, and it provides usable private and public outdoor spaces for the residents. The proposed project should have a positive contribution to the surrounding area.

### **Access, Circulation and Parking**

Vehicular access to the project site is provided through three driveways: one off of Benefit Way and two off of Advantage Court. Additional pedestrian connections are provided from the units to the sidewalks along Advantage Court, Benefit Way, and East Commerce Way.

<b>Table 6: Vehicular Parking</b>			
<b>Use</b>	<b>Required Parking</b>	<b>Proposed Parking</b>	<b>Difference</b>
Condominiums	233 (one per unit)	410	+177

Two types of parking will be available at this complex: garage spaces within the individual units and open spaces for residents and guests. The parking areas comply with the Multi-Family Residential Design Guidelines in that: the majority of the surfaced parking areas are located away from the adjacent public roadways; parking areas are generally located away from street corners; and landscaping and walkways are provided between buildings and paved parking areas. According to the Sacramento City Code (Section 17.192), a minimum of 1 parking space per unit is required. Therefore, the proposal is required to provide a minimum of 233 parking spaces. The applicant is proposing 410 parking spaces on site, consisting of 371 garage spaces and 39 open spaces. It is staff’s opinion that the 371 garage spaces and the 48 extra parking spaces are not excessive, in that: each unit has either a one-car or a two-car garage; minimal on-street parking is allowed on the surrounding streets; and the open parking is provided throughout the site.

The proposal is also consistent with the Natomas Crossing PUD guidelines, which state that where reasonable, locate parking lots away from the primary adjacent roadways, behind buildings, or within the buildings as structured parking. (p. 57)

### **Height, Bulk and Setbacks**

<b>Table 7: Height and area standards</b>			
<b>Standard</b>	<b>Allowance</b>	<b>Proposed</b>	<b>Deviation?</b>
Height	Maximum four stories	Two to three stories	No
East Commerce Way	Minimum of 12’-6” Maximum of 15’-0”	12’-6” to 15’-0”	No
Benefit Way	Minimum of 17’-6” Maximum of 23’-0”	17’-6”	No
Advantage Court	15’-0”	15’-0”	No

Southern Property Line	15'-0"	15'-0"	No
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As indicated above, the project meets or exceeds all applicable height and area requirements.

### Building design, signage and landscaping

The condominium buildings are two and three stories in height which is within the four-story maximum building height allowed in the Employment Center 50 zone. The exterior building materials will consist of integral color stucco, stone veneer, plaster corbels, plaster window trims, metal railing details, and decorative shutters. Roofing is proposed to be tiles. Vinyl windows, fiberglass entry doors and metal sectional garage doors are also used. The applicant is proposing two housing types, the Urban Villas and the Courts.

The Urban Villas are a housing type located at the northernmost and southernmost portions of the site. All three unit types are three stories tall and have ground floor living area adjacent to two-car garages. In some instances, unit types 1 and 2 are attached by a deck in between the buildings. All unit entries face onto a pedestrian paseo or a public street, and all garages are accessed at the private drives. All building side elevations facing a street shall be enhanced with window openings, trims and decorative elements.

The Courts are a housing type located at the central portion of the site and at the East Commerce Way frontage. All Court buildings are three stories in height and other than Plan 2/3 and 5/7, all buildings have partial living area on the ground floor. Some of the Court buildings contain two-story elements to alleviate the massing. All units have an entry that faces onto a public street, a private street or a pedestrian paseo, and all garages are accessed at the private drives. Plans 1, 2, 4, 6, 8, 9, 10 and 11 are shown with one-car garages while Plans 3, 5 and 7 are shown with two-car tandem garages. All building side elevations facing a street shall be enhanced with window openings, trims and decorative elements.

The following is a general summary of the building types:

Building Type	Building Count	Unit Count	Maximum Height	Number of Floors
Urban Villa 1	7	7	32'-0"	3
Urban Villa 1 & 2	17	34	32'-0"	3
Urban Villa 3	21	21	32'-0"	3
Court – Plan 1 / 4	16	32	33'-0"	3
Court – Plan 2 / 3	14	28	33'-0"	3
Court – Plan 5 / 7	31	62	35'-0"	3
Court – Plan 6 / 9	11	22	34'-0"	3
Court – Plan 8	7	7	33'-0"	3

Court – Plan 10 / 11	10	20	32'-0"	3
Total	134	233	n/a	n/a

The proposal is required to be consistent with the following Natomas Crossing PUD Guidelines:

- Front-on buildings are encouraged. Avoid sound walls, replace with mounds and other sound absorption features. (p. 22)
- Architectural facades should provide visual interest and scale to the adjacent streets. Avoid overly monotonous facades that do not have relief, shadow, or textural changes at the pedestrian level. (p. 57)
- Provide windows that look out to the adjacent streetscapes and parking lot areas. (p. 57)
- Orient building entrances toward the adjacent streetscape and celebrate the connection between public and private uses. (p. 57)
- Buildings should be located close to the public utility easement (PUE). (p. 78)
- Residential buildings should have pedestrian access and visual orientation to the adjacent roadways and/or open space features. (p. 85)
- Residential buildings shall be oriented on the site to create interesting and safe common open space areas that promote neighborly interaction. (p. 85)

The residential buildings are consistent with the Multi-Family Residential Design Guidelines in that the buildings are arranged to provide functional public and private outdoor spaces, and pedestrian orientation is encouraged in the allocation of space, building size and placement, and open space design. The buildings provide windows and active spaces to enhance security and visual interest. A variety in architecture is provided and the buildings vary in roof form, mass, shape and materials to create variations in plans. The applicant will be required to provide exterior lighting of residential quality and the design for this proposal, consistent with the pedestrian and light poles for this proposal. All signage will be required to conform to the Natomas Crossing – Area #3 PUD Guidelines and the City’s Sign Ordinance, where applicable.

A preliminary landscaping plan for the project is provided; the applicant has also provided playground shade structure and paseo concept plans. Trees have been planted between a separated sidewalk and the street along the public streets. The proposed landscaping on site will consist of shade trees, flowering ornamental trees, shrubs, and ground cover. The proposed landscaping is required to be consistent with the North Natomas Development Guidelines – Plant Species and the Natomas Crossing PUD Guidelines – Roadway master Plan Matrix (Table 4). The parking areas will be required to comply with the City’s Tree Shading Ordinance, requiring 50 percent tree shading within 15 years.

The proposed landscaping is consistent with the Multi-Family Residential Design Guidelines, in that: the exterior site design and landscaping provide functional recreational spaces and community site amenities; the exterior spaces are designed to enhance the overall appearance and compatibility of this development by providing

privacy, buffering and daylight, and to provide a pleasant transition to the street.

### ***Special Permit Major Modification***

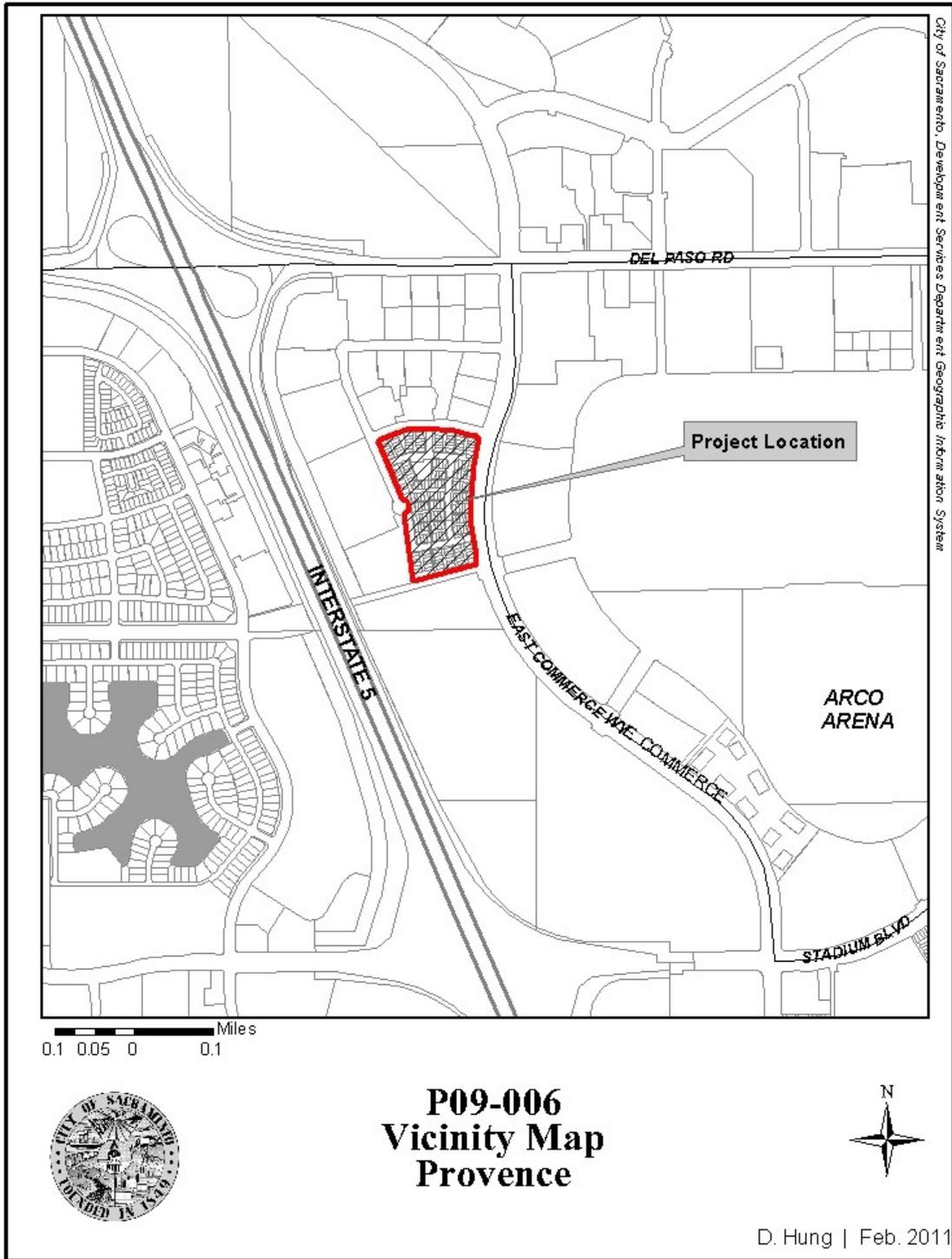
The previous approval on the site is for 187 townhouse units (P06-194). The site is currently developed with five of the 187 units, along with interior streets and the pool and pool house. A Special Permit Major Modification is required to amend the previous approval to allow the construction of a total of 233 condominiums on the remaining portion of the site. In evaluating special permit proposals of this type, the City Council is required to make the following findings:

1. A special permit shall be granted upon sound principles of land use.  
Staff finds that the proposed condominium development is a better designed project than the previous approval on the site; the project provides more architectural variations and both private and public outdoor spaces for the occupants.
2. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.  
The project will not be detrimental to the public health, safety or welfare since the buildings will be constructed to meet code standards. Residential developments of this type have been approved in many areas of the city. Staff finds that the proposed development is consistent with the Natomas Crossing PUD Guidelines and the Multi-Family Residential Design Principles.
3. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.  
The proposed project is consistent with the employment center and residential policies of the General Plan and the North Natomas Community Plan.

Staff believes that the current proposal is a better project in that it provides various housing types and different architectural variations, and it provides usable private and public outdoor spaces for the residents. The proposed project is also consistent with the goals and policies of the General Plan.



### Attachment 3: Vicinity Map





## RESOLUTION NO.

Adopted by the Sacramento City Council

### **ADOPTING THE MITIGATED NEGATIVE DECLARATION AND THE MITIGATION MONITORING PROGRAM FOR THE PROVENCE PROJECT (P09-006)**

#### **BACKGROUND**

- A. On March 24, 2011, the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve with conditions the Provence Project.
- B. On April 21, 2011, the City Council conducted a public hearing, for which notice was given pursuant Sacramento City Code Section 17.200.010 (C)(2)(a, b, and c) and received and considered evidence concerning the Provence Project.

#### **BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:**

Section 1. The City Council finds as follows:

- A. The Project initial study was prepared to analyze whether the Project was described in the Master EIR and whether the Project would cause any significant additional environmental effects (project-specific effects) that were not analyzed in the Master EIR for the 2030 General Plan.
- B. The Initial Study concluded that the Project was described in the Master EIR, and identified mitigation in the Master EIR that would apply to the Project. The Initial Study identified mitigation measures that were incorporated to revise the project before the environmental document was released for public review pursuant to CEQA Guidelines Section 15073 in order to avoid or mitigate the identified effects to a level of insignificance. (CEQA Guidelines Section 15178(b)). As part of the Master EIR process, the City incorporated all feasible mitigation measures or feasible alternatives appropriate to the project as set forth in the Master EIR (CEQA Guidelines Section 15177(d)).
- C. The above review concluded that there is no substantial evidence that the Project as revised and conditioned would have a significant effect on the environment. A Mitigated Negative Declaration (MND) for the Project was then completed, noticed and circulated in accordance with the requirements of the California Environmental Quality Act (CEQA), the State CEQA Guidelines and the Sacramento Local Environmental Procedures as follows:

1. On May 8, 2009 a Notice of Intent to Adopt the MND (NOI) dated May 8, 2009 was circulated for public comments for 20 days. The NOI was sent to those public agencies that have jurisdiction by law with respect to the proposed project and to other interested parties and agencies, including property owners within 500 feet of the boundaries of the proposed project. The comments of such persons and agencies were sought.

2. On May 8, 2009, the NOI was published in the Daily Recorder, a newspaper of general circulation, and the NOI was posted in the office of the Sacramento County Clerk.

Section 2. The City Council has reviewed and considered the information contained in the MND, including the Initial Study, the revisions and conditions incorporated into the Project, and the comments received during the public review process and the hearing on the Project. The City Council has determined that the MND constitutes an adequate, accurate, objective and complete review of the environmental effects of the proposed project.

Section 3. Based on its review of the MND and on the basis of the whole record, the City Council finds that the MND reflects the City Council's independent judgment and analysis and that there is no substantial evidence that the Project will have a significant effect on the environment.

Section 4. The City Council adopts the MND for the Project.

Section 5. Pursuant to CEQA section 21081.6 and CEQA Guidelines section 15074, and in support of its approval of the Project, the City Council adopts a Mitigation Monitoring Program to require all reasonably feasible mitigation measures be implemented by means of Project conditions, agreements, or other measures, as set forth in the Mitigation Monitoring Program.

Section 6. Upon approval of the Project, the City's Environmental Planning Services shall file or cause to be filed a Notice of Determination with the Sacramento County Clerk and, if the project requires a discretionary approval from any state agency, with the State Office of Planning and Research, pursuant to section 21152(a) of the Public Resources Code and section 15075 of the State EIR Guidelines adopted pursuant thereto.

Section 7. Pursuant to Guidelines section 15091(e), the documents and other materials that constitute the record of proceedings upon which the City Council has based its decision are located in and may be obtained from, the Office of the City Clerk at 915 I Street, Sacramento, California. The City Clerk is the custodian of records for all matters before the City Council.

Section 8. Exhibit A is a part of this Resolution.

Table of Contents:

Exhibit 4A: Mitigation Monitoring Program – 6 pages

# Exhibit 4A: Mitigation Monitoring Plan

## MITIGATION MONITORING PLAN

FOR

PROVENCE (P09-006)

**TYPE OF ENVIRONMENTAL DOCUMENT:**  
INITIAL STUDY/ NEGATIVE DECLARATION

### PREPARED FOR:

CITY OF SACRAMENTO, COMMUNITY DEVELOPMENT DEPARTMENT

DATE:

DATE

### ADOPTED BY:

CITY OF SACRAMENTO  
PLANNING COMMISSION

DATE:

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ATTEST:

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**Provence (P09-006)**  
**MITIGATION MONITORING PLAN**

This Mitigation Monitoring Plan (MMP) has been required by and prepared for the City of Sacramento Community Development Department, Environmental Planning Services, 300 Richards Boulevard, Sacramento, CA 95811, pursuant to CEQA Guidelines Section 21081.6.

**SECTION 1: PROJECT IDENTIFICATION**

**Project Name / File Number:** Provence (P09-006)

**Owner/Developer- Name:** Kevin Smith  
RC Natomas LLC

**Address:** 4060 Campus Drive, Suite 100  
Newport Beach, CA 92660  
916-838-6651

**Project Location / Legal Description of Property (if recorded):**

The proposed project site is located southwest of the intersection of East Commerce Way and Benefit Way. The proposed project is located within the North Natomas Community Plan area of the City of Sacramento. The project site includes Assessor's Parcel Number 225-2330-002 thru 076, 225-2680-002 thru -083 and 225-2690-002 thru -069.

**Project Description:**

The proposed project consists of development entitlements to construct 237 condominium units on the project site. The proposed project seeks a modification to the previous approval for 187 units of townhouse development (P06-194) of which five homes have been constructed. The proposed project requires a PUD Schematic Plan Amendment to re-designate the use and allowable density of the site, a Tentative Map, a Special Permit for alternative ownership housing and a Special Permit Modification to amend the previous approval on the site (P06-194).

**SECTION 2: GENERAL INFORMATION**

The Plan includes mitigation for Aesthetics, Cultural Resources, Hydrology and Noise. The intent of the Plan is to prescribe and enforce a means for properly and successfully implementing the mitigation measures as identified within the Initial Study for this project. Unless otherwise noted, the cost of implementing the mitigation measures as prescribed by this Plan shall be funded by the owner/developer identified above. This Mitigation Monitoring Plan (MMP) is designed to aid the City of Sacramento in its implementation and monitoring of mitigation measures adopted for the proposed project.

The mitigation measures have been taken from the Initial Study and are assigned the same number they have in the document. The MMP describes the actions that must take place to implement each mitigation measure, the timing of those actions, and the entities responsible for implementing and monitoring the actions. The developer will be responsible for fully understanding and effectively implementing the mitigation measures contained within the MMP. The City of Sacramento will be responsible for ensuring compliance.

**Provence (P09-006)  
MITIGATION MONITORING PLAN**

**MITIGATION AGREEMENT**

PROJECT NAME / FILE NUMBER: Provence (P09-006)  
OWNER/DEVELOPER/APPLICANT: Kevin Smith  
RC Natomas LLC  
Address: 4060 Campus Drive, Suite 100  
Newport Beach, CA 92660  
916-838-6651

I, <sup>RC</sup> ~~NATHAN S. BIRCHALL, NATOMAS LLC~~ owner/developer/applicant, agree to amend the project application P09-006 to incorporate the attached mitigation measures as identified in the Initial Study for the project. I understand that by agreeing to these mitigation measures, all identified potentially significant environmental impacts should be reduced to below a level of significance, thereby enabling the Environmental Coordinator to prepare an Addendum to the previously approved Mitigated Negative Declaration of environmental impact for the above referenced project.

I also understand that the City of Sacramento will adopt a Mitigation Monitoring Plan (Plan) for this project. This Plan will be prepared by the Community Development Department, pursuant to the California Environmental Quality Act Guidelines Section 21081.6 and pursuant to Article III of the City's Local Administrative Procedures for the Preparation of Environmental Documents.

I acknowledge that this project, P09-006, would be subject to this Plan at the time the Plan is adopted. This Plan will establish responsibilities for the monitoring of my project by various City Departments and by other public agencies under the terms of the agreed upon mitigation measures. I understand that the mitigation measures adopted for my project may require the expenditure of owner/developer funds where necessary to comply with the provisions of said mitigation measures.

  
\_\_\_\_\_  
Signature (Owner/Developer/Applicant)

CFO  
\_\_\_\_\_  
Title

12/8/10  
\_\_\_\_\_  
Date

**Provence (P09-006)  
MITIGATION MONITORING PLAN**

Environmental Resource	Mitigation Measure	Responsible Entity	Compliance Milestone / Confirm Complete
Aesthetics	<b>Aesthetics 1:</b> Project outdoor lighting shall be oriented away from adjacent properties and shall not produce a glare or reflection on neighboring properties or adjacent streets or property.	Community Development Department	Prior to issuance of any grading or building permit, measures identified on plans shall be verified for compliance. The Community Development Department shall assure that measures are identified on construction plans and specifications and confirm compliance prior to issuance of any grading or building permit.
Cultural Resources	<b>CR-1:</b> In the event that any prehistoric subsurface archeological features or deposits, including locally darkened soil ("midden"), that could conceal cultural deposits, animal bone, obsidian and/or mortars are discovered during construction-related earth-moving activities, all work within 50 meters of the resources shall be halted, and the City shall consult with a qualified archeologist to assess the significance of the find. Archeological test excavations shall be conducted by a qualified archeologist to aid in determining the nature and integrity of the find. If the find is determined to be significant by the qualified archeologist, representatives of the City and the qualified archeologist shall coordinate to determine the appropriate course of action. All significant cultural materials recovered shall be subject to scientific analysis and professional museum curation. In addition, a report shall be prepared by the qualified archeologist according to current professional standards.	Community Development Department,  Native American Heritage Commission	Prior to issuance of any grading or building permit, measures identified on plans shall be verified for compliance. The Community Development Department shall assure that measures are identified on construction plans and specifications and confirm compliance prior to

**Provence (P09-006)  
MITIGATION MONITORING PLAN**

	<p><b>CR-2:</b> If a Native American site is discovered, the evaluation process shall include consultation with the appropriate Native American representatives.</p> <p>a. If Native American archeological, ethnographic, or spiritual resources are involved, all identification and treatment shall be conducted by qualified archeologists, who are certified by the Society of Professional Archeologists (SOPA) and/or meet the federal standards as stated in the Code of Federal Regulations (36 CFR 61), and Native American representatives, who are approved by the local Native American community as scholars of the cultural traditions.</p> <p>b. In the event that no such Native American is available, persons who represent tribal governments and/or organizations in the locale in which resources could be affected shall be consulted. If historic archeological sites are involved, all identified treatment is to be carried out by qualified historical archeologists, who shall meet either Register of Professional Archeologists (RPA), or 36 CFR 61 requirements.</p> <p><b>CR-3:</b> If a human bone or bone of unknown origin is found during construction, all work shall stop in the vicinity of the find, and the County Coroner shall be contacted immediately. If the remains are determined to be Native American, the coroner shall notify the Native American Heritage Commission, who shall notify the person most likely believed to be a descendant. The most likely descendant shall work with the contractor to develop a program for re-interment of the human remains and any associated artifacts. No additional work is to take place within the immediate vicinity of the find until the identified appropriate actions have taken place.</p>		<p>issuance of any grading or building permit.</p>
<p>Hydrology and Water Quality</p>	<p><b>Hydro 1</b> Construction and operation of the project shall not commence prior to recertification of the Natomas levees by the USACE and FEMA, and the subsequent removal of Natomas Basin from the 100-year floodplain and associated flood zone redesignation; or until FEMA redesignates the Natomas Basin with a flood zone designation that would permit development of the proposed project. The above measures shall terminate upon the first recertification of the levees by the U.S. Army Corps of Engineers.</p>	<p>Community Development Department,  United States Army Corp of Engineers</p>	<p>Prior to issuance of any grading or building permit, measures identified on plans shall be verified for compliance. The Community Development Department</p>

**Provence (P09-006)  
MITIGATION MONITORING PLAN**

	<p><b>Hydro 2:</b> The project applicant shall participate in a funding mechanism such as an assessment district established by SAFCA and/or the City for the purpose of implementing measures that would provide no less than 100-year flood protection including the North Natomas Area, or for that portion of the Natomas Basin requiring re-certification for 100-year flood protection including the Project site provided that such funding mechanism is (i) based on a nexus study; (ii) is regional in nature; (iii) is proportionate; (iv) complies with all applicable laws and ordinances; and (3) the requirements of the applicable FEMA zone and corresponding requirements under the City of Sacramento's Floodplain Ordinance shall be satisfied prior to the issuance of building permits for the project. Any future homeowners within the floodzone shall maintain federal flood insurance, as required under the applicable FEMA and City of Sacramento Floodplain Management Ordinance regulations. The above measures shall terminate upon the first recertification of the levees by the U.S. Army Corps of Engineers.</p>	<p>shall assure that measures are identified on construction plans and specifications and confirm compliance prior to issuance of any grading or building permit.</p>
<p>Noise</p>	<p><b>Noise 1:</b> STC 32 rated window assemblies shall be installed in all second and third floor windows with a direct view of East Commerce Way.</p> <p><b>Noise 2:</b> Each residential unit shall include a mechanical ventilation system to allow occupants to keep windows and doors closed to achieve isolation from exterior noise sources.</p>	<p>Community Development Department</p> <p>Prior to issuance of any grading or building permit, measures identified on plans shall be verified for compliance. The Community Development Department shall assure that measures are identified on construction plans and specifications and confirm compliance prior to issuance of any grading or building permit.</p>



## Attachment 5: Resolution – PUD Schematic Plan Amendment

### RESOLUTION NO.

Adopted by the Sacramento City Council

**APPROVING A PUD SCHEMATIC PLAN AMENDMENT FOR THE NATOMAS CROSSING PUD (AREA 3) TO DESIGNATE MULTI-FAMILY RESIDENTIAL USE FOR THE PROVENCE PROJECT (P09-006)(APN: 225-2330-002-0000 through 225-2330-076-0000, 225-2680-002-0000 through 225-2680-083-0000, and 225-2690-002-0000 through 225-2690-069-0000)**

#### BACKGROUND

- A. On March 24, 2011, the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve the PUD Schematic Plan Amendment for the Provence project.
- B. On April 21, 2011, the City Council conducted a public hearing, for which notice was given pursuant Sacramento City Code Section 17.200.010(C)(2)(a) and (c) (publication and mail 500'), and received and considered evidence concerning the Provence project.
- C. The proposed PUD Schematic Plan Amendment conforms to policies of the General Plan to provide adequate housing sites and opportunities for all households and to promote efficient development within a new growth area.
- D. The PUD Schematic Plan Amendment will not be injurious to the public welfare, nor to other properties in the vicinity of the development in that the project is compatible with adjacent developments and the site will be developed according to the requirements of the PUD Guidelines.

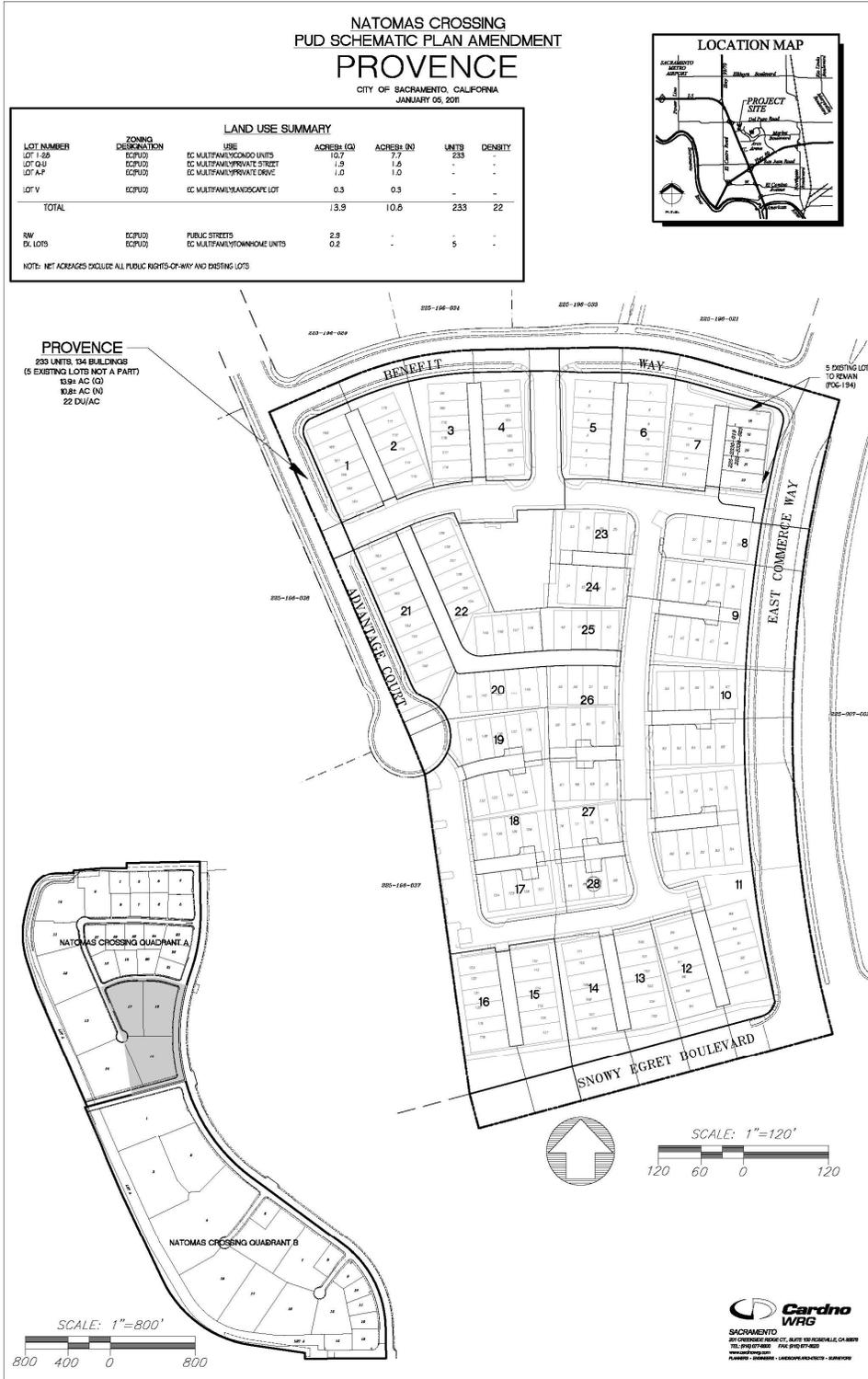
#### **BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:**

- Section 1. The PUD Schematic Plan Amendment for the Provence Project (as shown on the attached Exhibit) is approved.
- Section 2. Exhibit A is a part of this Resolution.

#### **Table of Contents:**

Exhibit 5A: PUD Schematic Plan Amendment – 1 page

# Exhibit 5A: PUD Schematic Plan Amendment





## Attachment 6: Resolution – Project Approval

### RESOLUTION NO.

Adopted by the Sacramento City Council

### ADOPTING FINDINGS OF FACT AND APPROVING THE PROVENCE PROJECT (P08-013)

#### BACKGROUND

- A. On March 24, 2011, the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve the Provence project.
- B. On April 21, 2011, the City Council conducted a public hearing, for which notice was given pursuant Sacramento City Code Section 17.200.010(C)(2)(a), (b), and (c) (publication, posting, and mail 500'), and received and considered evidence concerning the Provence project.

#### BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:

Section 1. Based on the verbal and documentary evidence received at the hearing on the Provence project, the City Council approves the project based on the findings of fact and subject to the conditions of approval as set forth below.

Section 2. The City Council approves the Project entitlements based on the following findings of fact:

**A. Tentative Map:** The Tentative Map to create 28 residential condominium lots, 5 private street lots, 16 private drive lots and one landscape lot on 10.8 net acres is **approved** based on the following findings of fact:

1. None of the conditions described in Government Code Section 66474, subsection (a) through (g), inclusive, exist with respect to the proposed subdivision as follows:
  - a. The proposed subdivision, together with the provisions for its design and improvement, is consistent with the City's General Plan, all applicable community and specific plans, and Title 16 of the City Code, which is a specific plan of the City;
  - b. The site is physically suitable for the type of development proposed and

suited for the proposed density;

c. The design of the subdivision and the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife their habitat;

d. The design of the subdivision and the type of improvements are not likely to cause serious public health problems;

e. The design of the subdivision and the type of improvements will not conflict with easements, acquired by the public at large, for access through or use, of, property within the proposed subdivision.

2. The proposed subdivision, together with the provisions for its design and improvement, is consistent with the City General Plan and Title 16 Subdivisions of the City Code, which is a specific plan of the City (Gov. Code §66473.5);
3. The discharge of waste from the proposed subdivision into the existing community sewer system will not result in a violation of the applicable waste discharge requirements prescribed by the California Regional Water Quality Board, Central Valley Region, in that existing treatment plants have a design capacity adequate to service the proposed subdivision (Gov. code §66474.6);
4. The design of the proposed subdivision provides, to the extent feasible, for future passive or natural heating and cooling opportunities (Gov. Code §66473.1);
5. The City Council has considered the effect of the approval of this tentative subdivision map on the housing needs of the region and has balanced these needs against the public service needs of its residents and available fiscal and environmental resources (Gov. Code §66412.3).

**B. Special Permit:** The Special Permit to develop 233 condominium units in the Employment Center 50 Planned Unit Development (EC-50-PUD) is **approved** based on the following findings of fact:

1. A special permit shall be granted upon sound principles of land use.  
The proposed development is an appropriate land use that will have positive contribution to the surrounding area, in that the project site is in close proximity to future commercial and open space uses and that the site will be well served by auto, bicycle, and pedestrian linkages.
2. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.  
The proposed condominium development site and building design are consistent with the Natomas Crossing PUD Guidelines and the Multi-Family Residential Design Principles and will not be detrimental to public health, safety or welfare.

3. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.  
The proposed project is consistent with the goals and policies of the employment center designation in the 2030 General Plan and the North Natomas Community Plan.

**C. Special Permit Major Modification:** The Special Permit to amend previous approval for 187 townhouse units per P06-194 is **approved** based on the following findings of fact:

1. A special permit shall be granted upon sound principles of land use.  
The proposed condominium development is a better designed project than the previous approval on the site; the project provides more architectural variations and both private and public outdoor spaces for the occupants.
2. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.  
The project will not be detrimental to the public health, safety or welfare since the buildings will be constructed to meet code standards. Residential developments of this type have been approved in many areas of the city. Staff finds that the proposed development is consistent with the Natomas Crossing PUD Guidelines and the Multi-Family Residential Design Principles.
3. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.  
The proposed project is consistent with the employment center and residential policies of the 2030 General Plan and the North Natomas Community Plan.

Section 3. The City Council approves the Project entitlements subject to the following conditions of approval:

### **Conditions of Approval**

A. The **Tentative Map** to create 28 residential condominium lots, 5 private street lots, 16 private drive lots and one landscape lot on 10.8 net acres is hereby approved subject to the following conditions:

**NOTE: These conditions shall supersede any contradictory information shown on the Tentative Map or any contradictory provisions in the PUD guidelines approved for this project (P01-028). The design of any improvement not covered by these conditions or the PUD Guidelines shall be to City standard.**

**GENERAL:** All Projects

1. Pay off existing assessments, or file the necessary segregation requests and fees to segregate existing assessments.
2. Pursuant to City Code Section 16.40.190, indicate easements on the Final Map to allow for the placement of centralized mail delivery units. The specific locations for such easements shall be subject to review and approval of the Department of Transportation after consultation with the U.S. Postal Service.
3. The applicant shall participate in the North Natomas Financing Plan, adopted by Resolution No. 94-495 on August 9, 1994, and updated by Resolution No 2005-584 on August 2, 2005, and shall execute any and all agreements, which may be required in order to implement this condition.
4. Comply with the North Natomas Development Guidelines and the PUD guidelines approved for this project (P01-028) to the satisfaction of the Planning Director and the Department of Transportation.
5. Private reciprocal ingress, egress, maneuvering and parking easements are required for future development of the area covered by this Tentative Map. The applicant shall enter into and record an Agreement For Conveyance of Easements with the City stating that a private reciprocal ingress/egress, maneuvering, and parking easement shall be conveyed to and reserved from each common access lots (A, B, C, D, E, F, H, I, J, K, L, M, N, O, P) to each individual Lot along the common Lot, and from all private street lots (G, Q, R, S, T and U) at no cost, at the time of sale or other conveyance of either parcel.
6. Comply with requirements included in the Mitigation Monitoring Plan developed by, and kept on file in, the Planning Division Office (P01-028).
7. Meet all conditions of the existing PUD (P01-028) unless the condition is superseded by a Tentative Map condition.
8. Meet all conditions of the development agreement.
9. Show all continuing and proposed/required easements on the Final Map.
10. Multiple Final Maps may be recorded. Prior to recordation of any Final Map all infrastructure/improvements necessary for the respective Final Map must be in place to the satisfaction of the Departments of Utilities, and Department of Transportation.
11. Prior to submittal of improvement plans for this project, the developer's design consultant(s) shall participate in a pre-design conference with City staff. The

purpose of this conference is to allow City staff and the design consultants to exchange information on project design requirements and to coordinate the improvement plan review process. Contact the Department of Transportation, Plan Check Engineer at 808-7915 to schedule the conference. It is strongly recommended that the conference be held as early in the design process as possible.

**Department of Transportation: Streets (Anis Ghobril, DOT, 808-5367)**

12. The applicant shall be responsible for the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk fronting the property along East Commerce Way, Benefit Way and Advantage Court per City standards and to the satisfaction of the Department of Transportation. This shall include any needed street lighting.
13. The design and placement of walls, fences, signs and Landscaping near public intersections and private streets shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height. The area of exclusion shall be determined by the Department of Transportation.
14. This project shall require street lighting. There is an existing street lighting system around this project area. Improvements of right-of-way may require modification to the existing system. Electrical equipment shall be protected and remain functional during construction. The applicant shall provide acorn lighting as required along East Commerce Way to the satisfaction of the Department of Transportation.
15. The applicant shall make provisions for bus stops, shelters, etc. to the satisfaction of Regional Transit.
16. The applicant shall dedicate (if necessary) and construct bus turn-outs for all bus stops adjacent to the subject site to the satisfaction of the Department of Transportation.

**Department of Transportation: Private Streets (DOT)**

17. The applicant shall repair/reconstruct any deteriorated curb, gutter and sidewalks along the private streets to the satisfaction of the Department of Transportation.

**PUBLIC/PRIVATE UTILITIES (Yujean Kim, SMUD, (916) 732-5027)  
(Salam Khan, SASD, (916) 876-6094)**

18. Dedicate a standard 12.5 foot public utility easement (PUE) for underground and overhead facilities and appurtenances adjacent to all street right of ways.
19. Dedicate all private drives and 5-feet adjacent thereto, as public utility easement for underground and overhead facilities and appurtenances.
20. The owner or developer must disclose to future or potential owners the existing 69KV electrical facilities.
21. Sewer lines are already constructed and accepted by SASD. Any addition or modification to the SASD's sewer system shall be required to the satisfaction of SASD. SASD Design Standards apply to sewer construction or modification. (SASD)
22. While reconfiguring the lots, it must be ensured that each parcel shall have a separate connection to SASD public sewer system. If there is more than one building in any single parcel and the parcel is not proposed for split, then each building on that parcel shall have a separate connection to a private on-site sewer line or SASD public sewer line. (SASD)
23. Sewer easements have already been recorded for SASD sewer line installed within the project area. However, any modification to these sewer easements, if required, shall to the satisfaction of SASD. (SASD)
24. The subject project owner(s) and successors in interest thereof, shall be responsible for repair and/or replacement of all non-asphalt and/or enhanced surface treatments of streets and drives (such as stamped/colored/decorative concrete, concrete pavers, etc.) within these easements damaged by District maintenance and repair operations, including landscaping, channelization's, lighting, fountain area, sidewalk, and any other appurtenances conflicting therein. This requirement shall be set forth in easement grant documents and be a covenant running with the land, be responsibility of successors in interest in future land transfers and divisions and by language approved by the District. The District will only replace asphalt and standard concrete roadway/driveway disturbed due to maintenance/repair of its sewer line. If the repair is of decorative or stamped concrete, the District will only replace with standard concrete. (SASD)
25. Additional SASD/SRCSD Sewer impact fee (one time connection fee) may be required before issuance of building permits. Applicant should contact the Fee Quote Desk at (916) 876-6100 for sewer impact fee information. (SASD)

**CITY UTILITIES** (Jesus Reyes, Dept. of Utilities, 808-1721)

26. Applicant/Association shall execute an Agreement with the Department of Utilities (DOU) to assume ownership of the existing 8" public water line currently

located in the existing private drives. The Agreement shall be to the satisfaction of the DOU, Fire Department and the City Attorney.

27. Install 8" water meters and 8" RP back flow prevention assemblies at the two points of service (one at Benefit Way and the other one at Advantage Court) to the satisfaction of Department of Utilities. Meters shall be special meters suitable for use in combination Fire and Domestic water systems.
28. Applicant shall pay appropriate tap, meter and development fees associated with the installation of the special meters.
29. Water services for the existing townhouses (APN's: 225-2330-019, 020, 021, 022 & 023) shall be disconnected from the existing 8" water main and re-connected to the existing 12" public water main in East Commerce Way or Benefit Way. City will abandon the existing water services and install new water service taps to the point of service for fee. (Note: The existing water services may remain connected to the existing water main if the townhouses/lots become part of the Provence Master Association).
30. Abandon existing "Public Water Easements" (PWE) relating to the existing on-site public water main/appurtenances.
31. Execute and record a Utility Service Agreement (USA) approved by the City Attorney's Office and Department of Utilities.
32. If required, construct public water, sewer, drainage and service connections to the satisfaction of DOU.
33. All existing easements that are to remain and all existing right-of-ways shall be shown on the Final Map.
34. All onsite streets, drives, common areas, storm drain & water facilities shall be private facilities. Prior to or concurrent with the recording of the final map a homeowners association (HOA) shall be formed and C.C. & R.s shall be approved by the City and recorded assuring maintenance of the private streets, private drives, storm drainage facilities, water facilities and common areas. Private easements shall be dedicated for these facilities. The private street and drive maintenance shall include all pavement, curb, gutter and v-gutter.
35. Concurrent with the recordation of the final map, the applicant shall enter into and record an Agreement for Conveyance of Easements with the City, in a form acceptable to the City Attorney, requiring that private easements be granted, as needed, for drainage, water and sanitary sewer at no cost at the time of sale or other conveyance of any lot. A note stating the following shall be placed on the Final Map: "The lots created by this map shall be developed in accordance with recorded agreement for conveyance of easements # (Book\_\_\_\_, Page\_\_\_\_)."

**FIRE** (King Tunson, Fire Department, 808-1358)

36. Maintenance agreements shall be provided for the interior roadways of the proposed project and for the fire protection systems. The agreement shall be record with the Public Records Office having jurisdiction and shall provide for the following:
- a. Provisions for the necessary repair and maintenance of the roadway surface.
  - b. Removal of vegetation overgrowing the roadway and infringing on the roadway clear vertical height of thirteen feet six inches (13'6") and/or width of twenty feet (20').
  - c. Provisions for the maintenance, repair, and/or replacement of NO PARKING-FIRE LANE signage or striping.
  - d. Unrestricted use of and access to the roadways covered by the agreements.
  - e. Provisions for the control of vehicle parking in prohibited areas and a mechanism for the removal of vehicles illegally parked.
  - f. Maintenance and timely repair of all fire protection systems, including but not limited to hydrants, fire alarm systems and fire sprinklers.
37. Provide the required fire hydrants in accordance with CFC 508 and Appendix C, Section C105. Hydrant spacing shall be decreased where T courts are used. Hydrants shall be provided halfway between each T court, on one side of the street, and to the satisfaction of the Fire Department. *Due to the project increasing the number of units from 187 to 237, additional hydrants may be required.*

**PPDS:** Parks (Raymond Costantino, Parks Department, 808-8826)

38. **Payment of In-lieu Park Fee:** Pursuant to Sacramento City Code Chapter 16.64 (Parkland Dedication) the applicant shall pay to City an in-lieu park fee in the amount determined under SCC §§16.64.040 and 16.64.050 equal to the value of land prescribed for dedication under 16.64.030 and not satisfied by dedication. (See Advisory Note).
39. **Maintenance District:** The applicant shall initiate and complete the formation of a parks maintenance district (assessment or Mello-Roos special tax district), or annex the project into an existing parks maintenance district. The applicant shall pay all city fees for formation of or annexation to a parks maintenance district. (Contact Public Improvement Financing, Special Districts Project Manager. In assessment districts, the cost of neighborhood park maintenance is equitably spread on the basis of special benefit. In special tax districts, the cost of neighborhood park maintenance is spread based upon the hearing report, which specifies the tax rate and method of apportionment.).
40. **Multi-Use Trail:** A multi-use trail and adjacent landscaping shall be dedicated

and constructed as specified below and in compliance with the Park Planning & Development Services (PPDS) "Multi-Use Trail Design Guidelines" available by contacting PPDS.

- 1 The City 2010 Bikeway Master Plan identifies an off-street multi-use trail on the southern portion of the project site, north of Snowy Egret Boulevard. The applicant shall comply with the Bikeway Master Plan and construct a multi-use trail in compliance with PPDS guidelines.
  - 2 The applicant shall submit and obtain PPDS approval of the alignment and design of the multi-use trail prior to submitting improvement plans for the trail.
  - 3 The proposed multi-use trail shall comply with Class I bike trail standards, including regulatory signage, as defined in Chapter 1000 of State Department of Transportation Highway Design Manual. The trail shall be a minimum 8' of asphalt concrete paving, with clear, graded shoulders that are a minimum of 2' in width. Shoulders should be decomposed granite or an alternate material approved by PPDS. Pavement sections shall be 3" minimum asphaltic concrete over 6" min of aggregate base, with a centerline stripe (refer to PPDS Trail detail and specification).
  - 4 Vehicular access controls shall be placed at the entrance to all access points to the trail (refer to PPDS details and specifications for approved designs). Access to the trail via Lots CC, DD and EE, along Colmars, Bastille and Garonne Walks is encouraged.
  - 5 Wherever possible and as approved by PPDS and the Department of Utilities, multi-use trails shall be designed as joint-use with utility service roads utilizing the service roads aggregate base as the trail's aggregate base course. Applicant shall design the pavement to meet all required design loads.
  - 6 Where a multi-use trail is located adjacent to any embankment with a greater than 4:1 slope, the Applicant shall, at his expense, install a post-and-cable fence along the top of the embankment, between the embankment and the multi-use trail.
  - 7 Residential lots adjoining the trail should be fenced w/ open tubular steel fencing.
  - 8 The Applicant shall disclose the location of the planned multi-use trail to all future/potential owners of parcels within the subdivision.
41. **New Private Facility Credits:** The Carriage Lane III (P05-079) development project, previously approved for this site, has a Private Facilities Agreement (City Agreement No. 2006-0298) which allowed 5% parkland dedication credit in

exchange for the development of a private pool for the use of all residents in the then 187 unit subdivision. That Agreement is not transferable to this project. Prior to recording a Final Map, Applicant may opt to enter into a new private facilities agreement for the 238 units (including the 5 existing units), in which case the following shall apply.

City Code Chapter 16.64, Sections 16.64.100, 110 and 120 address granting of private recreation facility credits. The city may grant credits for privately owned and maintained open space or local recreation facilities, or both, in planned developments as defined in Section 11003 of the Business and Professions Code, condominiums as defined in Section 783 of the Civil Code, and other common interest developments. Such credit, if granted in acres, or comparable in lieu fees, shall not exceed twenty-five (25) percent of the dedication or fees, or both, otherwise required under this chapter and no more than five percent per category of open space or recreational facilities described in this Chapter under 16.64.100.

Should the applicant elect to request City consideration of private facilities credit, contact PPDS at least three (3) months prior to recordation of Final Map. The Private Facilities Agreement will require City Council approval and must be in place prior to recordation of the Final Map.

## **MISCELLANEOUS**

42. Title to any property required to be dedicated to the City in fee shall be conveyed free and clear of all rights, restrictions, easements, impediments, encumbrances, liens, taxes, assessments or other security interests of any kind (hereafter collectively referred to as "Encumbrances"), except as provided herein. The applicant shall take all actions necessary to remove any and all Encumbrances prior to approval of the Final Map and acceptance of the dedication by City, except that the applicant shall not be required to remove Encumbrances of record, including but not limited to easements or rights-of-way for public roads or public utilities, which, in the sole and exclusive judgment of the City, cannot be removed and/or would not interfere with the City's future use of the property. The applicant shall provide title insurance with the City as the named beneficiary assuring the conveyance of such title to City.
43. Form a Homeowner's Association. CC&R's shall be approved by the City and recorded assuring maintenance of private roadway(s). The Homeowner's Association shall maintain all private streets, common lights, common landscaping and common areas.

## **ADVISORY NOTES:**

The following advisory notes are informational in nature and are not a requirement of this Tentative Map:

44. Prior to the issuance of any building permits, provide the City with a copy of the certificate of payment of any school fees for the applicable school district(s). (DOT)
45. If unusual amounts of bone, stone, or artifacts are uncovered, work within 50 meters of the area will cease immediately and a qualified archaeologist shall be consulted to develop, if necessary, further mitigation measures to reduce any archaeological impact to a less than significant effect before construction resumes. A note shall be placed on the final improvement plans referencing this condition. (DOT)
46. Dedicate slope easements and right-of-way necessary, for the Snowy Egret Boulevard overcrossing to the satisfaction of the Department of Transportation. The appropriate slope easements will be determined during the plan check phase of the improvement plans for this map. The Snowy Egret overcrossing shall line up with the street on the west side of I-5, to the satisfaction of the Department of Transportation. If a retaining wall is used in the construction of the overcrossing, then an engineering design shall be submitted to the satisfaction of the Department of Transportation. (DOT)
47. SASD policy prohibits gates that prevent access within sewer easements unless SASD standards for accessibility through gates are met. (SASD)
48. Any use of SASD sewer easements, which is not compatible or interferes with the construction, reconstruction, operation, maintenance, or repair of the SASD's sanitary sewer(s), shall not be allowed. Each proposed use shall be reviewed and approved in writing by the District Engineer prior to the use of the easement by the Grantor. This includes landscaping. (SASD)
49. A sewer study entitled Carriage Lane III was approved and the sewer infrastructure for this project has been constructed and accepted by SASD. (SASD)
50. As per City Code, the applicant will be responsible to meet his/her obligations regarding: (Parks)
  - 1 Title 16, 16.64 Park Dedication / In Lieu (Quimby) Fees, due prior to approval of the final map. The Quimby in-lieu fee due for this project is estimated at \$401,610, if there is no new private facilities agreement, or \$382,866 if a new private facilities agreement is approved by City Council before the final map. Option 1 is based on 46 new multi-family units (difference between 233 and 187) and an average land value of \$687,500 per acre for the North Natomas Planning Area, plus an additional 20% for off-site park infrastructure improvements (which totals \$333,960), plus reimbursable to City of the 5% Quimby credit received on P05-079 for 187 units (\$67,650). Any change in these factors will change the amount of the Quimby fee due. The final fee is

calculated using factors at the time of payment.

- 2 Option 2 is based on 233 multi-family units and an average land value of \$687,500 per acre for the North Natomas Planning Area, plus an additional 20% for off-site park infrastructure improvements (which totals 1,691,580), less 5% Credit for new Private Facilities Agreement on 238 units for P06-006 (\$86,394), less Alleghany Credits applied to Quimby Obligation for P05-079 (\$1,289,970), plus reimbursable to City of the 5% Quimby credit received on P05-079 for 187 units (\$67,650). Any change in these factors will change the amount of the Quimby fee due. The final fee is calculated using factors at the time of payment.
- 3 Title 18, 18.44 Park Development Impact Fee, due at the time of issuance of building permit. The Park Development Impact Fee due for this project is estimated at \$712,514. This is based on 233 multi-family units at the rate of \$3,058 per unit. Any change in these factors will change the amount of the PIF due. The fee is calculated using factors at the time that the project is submitted for building permit.
- 4 Community Facilities District 2002-02, Neighborhood Park Maintenance CFD Annexation.

B. The **Special Permit** to develop 233 condominium units in the Employment Center 50 Planned Unit Development (EC-50-PUD) is hereby approved subject to the following conditions:

### **Planning**

- B1. This approval is for the construction of 233 condominium units per attached exhibits. Any change in the design, materials, or colors from this approval shall be submitted to the Planning Division for review and determination for further actions.
- B2. Final landscape and circulation plans shall be submitted to Planning Division for review and comment prior to submittal for building permits.
- B3. The applicant shall obtain all necessary building permits prior to commencement of construction.
- B4. Building plans and elevations shall be plotted that no two same elevations and no two color schemes are adjacent to one another. All building side elevations facing a street shall be enhanced with window openings, trims and decorative elements.

- B5. Provide an ownership association responsible for the care and maintenance of all common areas and common improvements and any other interest common to the condominium owners. Complete and true copies of all covenants, conditions and restrictions, articles of incorporation and by-laws shall be subject to review and approval by the city prior to occupancy as a condominium unit.
- B6. The homeowner's association shall conduct periodic inspections, not less than monthly, of the exterior of all buildings, trash enclosures and recreation facilities.
- B7. The homeowner's association shall establish and conduct a regular program of routine maintenance for the property. Such a program shall include common areas and scheduled repainting, replanting and other similar activities that typically require attention at periodic intervals but not necessarily continuous. Owner/Operator shall repaint or retreat all painted or treated areas at least once every 8 years; provided that the Planning Director may approve less frequent painting or re-treatment upon a determination that less frequent repainting or re-treatment is appropriate, given the nature of the materials used or other factors. The program shall be subject to review and approval by the Planning Director.
- B8. The homeowner's association shall maintain landscaping and irrigation in a healthy and serviceable condition.
- B9. The homeowner's association shall indicate and maintain all locations of parking stalls for handicapped/disabled access and strictly enforce rules related thereto.
- B10. Each condominium unit shall comply with the state of California's Noise Insulation Standards (California Amended Code Section 1092).
- B11. Each condominium unit shall provide the following:
  - a. A separate sewer service hookup; provided, that the planning commission may permit the use of common sewer lines that are oversized by one size or more, or which are hydraulically designed with the concurrence of the city engineer, finds the common sewer lines can adequately service the condominiums and that separate service hookups would not be feasible. For this provision, the Planning Commission is delegating the approval to the Planning Director subject to concurrence with the Public Works Director and the Utilities Department.
  - b. A separate water service hookup or shutoff; provided, that the planning commission may permit a single water system to service more than one condominium unit where shutoffs are provided wherever practicable and where the planning commission, with the concurrence of the city engineer, finds that the single water system can adequately service the condominiums and separate service hookups or shutoffs are not feasible. For this provision, the Planning Commission is delegating the approval to the Planning Director

subject to concurrence with the Public Works Director and the Utilities Department.

- c. A separate gas service where gas is a necessary utility.
- d. A separate electrical service, with separate meters and disconnects and ground fault interrupters where and as required by Building Code.

B12. Each unit of a condominium project and all commonly owned portions of a Condominium building shall comply with all applicable building code standards. Nothing herein shall be construed to prevent or prohibit the applicant or the city from providing or requiring building standards greater than those set forth in the Building Code where the greater standards are found to be necessary to carry out the purposes and objectives of this chapter. (Ord. 99-015 § 6-3-D)

B13. Landscaping & Walls and Fencing:

- a. The project shall comply with the City's Tree Shading Ordinance which requires 50 percent shading of the parking area within 15 years.
- b. Install two 15-gallon trees per lot frontage, with the average spacing of 30' on center, as measured along the entire length of the street.
- c. All landscaping and planting shall conform to City standards for sight line requirements at intersections and driveways.
- d. All mechanical equipment shall be located within enclosed cabinets or screened by landscaping and/or screening/fencing.
- e. In order to provide adequate surveillance opportunities, all *plants and shrubs* are to be maintained at maximum height of thirty inches (30"); the lowest branch height shall be at least six feet (6'). Decorative planting shall be maintained so as not to obstruct or diminish lighting level throughout the project.
- f. Walls and fences shall conform to City standards for sight line requirements at intersections and driveways.

B14. The applicant shall comply with the City's Recycling Ordinance (Section 17.72).

B15. Signage:

- a. All detached signs shall be monument-type, constructed of masonry with finish materials and colors, which are consistent with building architecture. One detached monument sign is permitted and may be located within a landscape setback area but no closer than 10 feet from public right-of-way.

- b. Attached signage shall consist of address numbers only.
- c. All signage shall comply with the Natomas Crossing PUD signage criteria; a sign permit shall be obtained prior to construction of any sign.

B16. Lighting:

- a. The type and location of the outdoor lighting (building, parking lot, walkway, etc.) must be approved by the Planning Director prior to issuance of a building permit. Lighting shall be provided in garage areas and each building address number shall be illuminated.
- b. Project lighting shall be provided as follows: one foot-candle of minimum maintained illumination per square foot of parking space and exterior walkways/sidewalks during hours of darkness and 0.25 foot-candle of minimum maintained illumination per square foot of surface on any interior walkway, alcove, passageway, etc., from one-half hour before dusk to one-half hour after dawn. All light fixtures are to be vandal-resistant.
- c. Per Section 17.68.030(B), exterior lighting, if provided, shall reflect away from residential areas and public streets.
- d. Fixtures shall be unobtrusive and complementary to the architectural design of the building. Lighting shall be designed so as not to produce hazardous and annoying glare to motorists and building occupants, adjacent residents, or the general public.

B17. The applicant shall comply with the approved Mitigation Monitoring Plan (P09-006) on file at the Planning Division.

B18. The final playground shade structure and sculptures specifications shall be submitted for review and approval by the Planning Director prior to the issuance of building permit.

B19. The final building color palette shall be submitted for review and approval by the Planning Director prior to the issuance of building permit.

B20. The applicant shall paint electrical meters/cabinets, telephone connection boxes and other utility appurtenances to match the building to which they are attached.

B21. *If security becomes a concern and if deemed necessary by the Police Department*, the applicant shall contract with a reputable security firm to provide on-site security, night and day.

- B22. Prior to the issuance of any building permits, the applicant shall provide the City with a copy of the certificate of payment of school fees for the applicable school district(s).
- B23. This approval does not include any vehicular gates. Any proposal for vehicular gates will require an additional Special Permit subject to approval by the Planning Commission.
- B24. Condominiums addressing shall be to City standards.
- B25. The pool and tot lot shall be used by tenants and their guests only.
- B26. Auto repairs are not permitted at any time, except in emergencies, such as flat tire or dead battery.
- B27. Visitor parking shall be strictly enforced.

#### **Department of Transportation**

- B28. The applicant shall be responsible for the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk fronting the property along East Commerce Way, Benefit Way and Advantage Court per City standards and to the satisfaction of the Department of Transportation. This shall include any needed street lighting.
- B29. This project shall require street lighting. There is an existing street lighting system around this project area. Improvements of right-of-way may require modification to the existing system. Electrical equipment shall be protected and remain functional during construction. The applicant shall provide acorn lighting as required along East Commerce Way to the satisfaction of the Department of Transportation.
- B30. The design and placement of walls, fences, signs and Landscaping near public intersections and private Streets shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height. The area of exclusion shall be determined by the Department of Transportation.
- B31. The applicant shall repair/reconstruct any deteriorated curb, gutter and sidewalks along the private streets to the satisfaction of the Department of Transportation.
- B32. Form a Homeowner's Association. CC&R's shall be approved by the City and recorded assuring maintenance of private roadway(s). The Homeowner's

Association shall maintain all private streets, common lights, common landscaping and common areas.

## **Fire**

B33. Install gate at emergency vehicle access at East Commerce Way.

## **Utilities**

B34. Applicant/Association shall execute an Agreement with the Department of Utilities (DOU) to assume ownership of the existing 8" public water line currently located in the existing private drives. The Agreement shall be to the satisfaction of the DOU, Fire Department and the City Attorney.

B35. Install 8" water meters and 8" RP back flow prevention assemblies at the two points of service (one at Benefit Way and the other one at Advantage Court) to the satisfaction of Department of Utilities. Meters shall be special meters suitable for use in combination Fire and Domestic water systems.

B36. Applicant shall pay appropriate tap, meter and development fees associated with the installation of the special meters.

B37. Water services for the existing townhouses (APN's: 225-2330-019, 020, 021, 022 & 023) shall be disconnected from the existing 8" water main and re-connected to the existing 12" public water main in East Commerce Way or Benefit Way. City will abandon the existing water services and install new water service taps to the point of service for fee. (Note: The existing water services may remain connected to the existing water main if the townhouses/lots become part of the Provence Master Association)

B38. Abandon existing "Public Water Easements" (PWE) relating to the existing on-site public water main/appurtenances.

B39. All water connections shall comply with the City of Sacramento's Cross Connection Control Policy.

B40. Per Sacramento City Code, water meters shall be located at the point of service which is located at the back of curb for separated sidewalks or the back of walk for connected sidewalks or at a location acceptable to DOU.

B41. All onsite streets, drives, common areas, storm drain & water facilities shall be private facilities. A homeowners association (HOA) shall be formed and C.C. & R.s shall be approved by the City and recorded assuring maintenance of the private streets, private drives, storm drainage facilities, water facilities and common areas. Private easements shall be dedicated for these facilities. The private street and drive maintenance shall include all pavement, curb, gutter and

v-gutter. If required by the Department of Utilities (DOU), the responsible maintenance agency shall enter into and record an agreement with the City regarding the maintenance of these facilities. The agreement shall be to the satisfaction of the DOU and the City Attorney.

- B42. Prior to issuance of a building permit, the owners and ownership association shall enter into a utility service agreement with the City to receive such utility services at points of service designated by the DOU. Such agreement shall provide, among other requirements, for payment of all charges for the storm drainage and water services, shall authorize discontinuance of utility services at the City's point(s) of service in the event that all or any portion of such charges are not paid when and as required, shall require compliance with all relevant utility billing and maintenance requirements of the City, and shall be in a form approved by the City Attorney.
- B43. Per City Code, the Subdivider may not develop the project in any way that obstructs, impedes, or interferes with the natural flow of existing off-site drainage that crosses the property. The project shall construct the required public and/or private infrastructure to handle off-site runoff to the satisfaction of the DOU. If private infrastructure is constructed to handle off-site runoff, the applicant shall dedicate the required private easements and/or, at the discretion of the DOU, the applicant shall enter into and record an Agreement for Maintenance of Drainage with the City, in a form acceptable to the City Attorney.
- B44. The proposed development is located within Sacramento Area Sewer District (SASD) Satisfy all SASD requirements.
- B45. A grading plan showing existing and proposed elevations is required. All lots/parcels shall be graded so that drainage does not cross property lines or private drainage easements shall be dedicated. Adjacent off-site topography shall also be shown to the extent necessary to determine impacts to existing surface drainage paths. At a minimum, one-foot off-site contours within 100 feet of the project boundary are required (per Plate 2, page 3-7 of the City Design and Procedures Manual). No grading shall occur until the grading plan has been reviewed and approved by the DOU.
- B46. This project is greater than 1 acre in size; therefore, the project is required to comply with the State "NPDES General Permit for Stormwater Discharges Associated with Construction Activity" (State Permit). To comply with the State Permit, the applicant will need to file a Notice of Intent (NOI) with the State Water Resources Control Board (SWRCB) and prepare a Stormwater Pollution Prevention Plan (SWPPP) prior to construction. A copy of the State Permit and NOI may be obtained from [www.swrcb.ca.gov/stormstr/construction.html](http://www.swrcb.ca.gov/stormstr/construction.html). The SWPPP will be reviewed by the DOU prior to issuing a grading permit. The following items shall be included in the SWPPP: (1) vicinity map, (2) site map, (3) list of potential pollutant sources, (4) type and location of erosion and

sediment BMP's, (5) name and phone number of person responsible for SWPPP and (6) certification by property owner or authorized representative.

- B47. The applicant must comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinance. This ordinance requires the applicant to show erosion and sediment control methods on the subdivision improvement plans. These plans shall also show the methods to control urban runoff pollution from the project site during construction.
- B48. Post construction (permanent), stormwater quality control measures shall be incorporated into the development to minimize the increase of urban runoff pollution caused by development of the area. Since the project is in an area served by a regional water quality control facility, only source control measures are required. Storm drain message and signage is required at all drain inlets. Improvement plans must include the source controls measures selected for the site. Refer to the latest edition of the "Guidance Manual for On-Site Stormwater Quality Control Measures" for appropriate source control measures.

Advisory notes for the **Special Permit**:

1. Since December 8, 2008, the proposed project has been in a 100-year flood plain, designated as an AE zone by the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Maps (FIRMs). The base flood elevation ("BFE") for the Natomas Basin is 33' feet above sea level. The DOU expects the area to be taken out of the 100-year floodplain in 2012. Contact Nancy Dorfer (916-808-3539) for more information and updates on this issue. City Code Chapter 15.104 Floodplain Management Regulations require that any new construction of and/or substantial improvement to any structure located in Zone AE must have the lowest floor, including the basement, elevated a minimum of one (1) foot above the BFE. Non-residential structures have the option of flood proofing to one (1) foot above the BFE in lieu of the elevation requirement.
2. Unless otherwise approved by the Department of Utilities (DOU), all sanitary sewer, storm drain, water, and flood control improvements shall be in place and fully functioning prior to issuance of any building permits within the subject area.
3. **Many projects within the City of Sacramento require on-site booster pumps for fire suppression and domestic water systems.** Prior to design of the subject project, the DOU suggests that the applicant request a water supply test to determine what pressure and flows the surrounding public water distribution system can provide to the site. This information can then be used to assist the engineers in the design of the on-site fire suppression system.

Section 4. Exhibits A through L are a part of this Resolution.

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Exhibit 6G: East Commerce Views Looking Southwest & West – Without Trees – 2 pages  
Exhibit 6H: Courts – House Plans and Elevations – 62 pages  
Exhibit 6I: Urban Villas – House Plans and Elevations – 14 pages  
Exhibit 6J: Building Data Charts – 2 pages  
Exhibit 6K: Elevation Variation Plotting and Color Schemes – 12 pages  
Exhibit 6L: Playground Shade Structure and Paseo Concept Plans – 3 pages



Exhibit 6B: Existing Condition

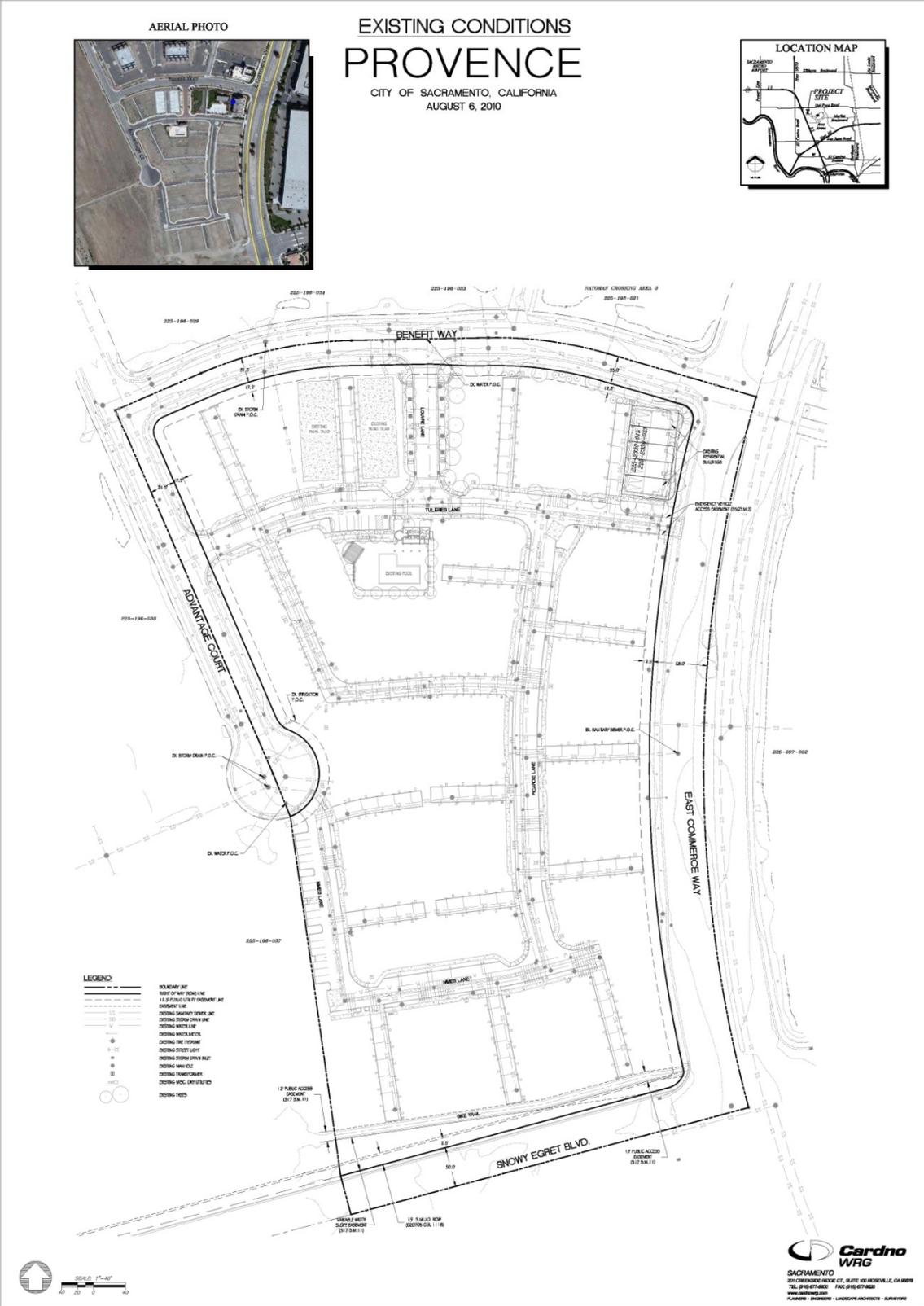








Exhibit 6F: East Commerce Views Looking Southwest & West – With Trees



Note: Different monitors and printers may skew colors. Please reference the color boards provided for "true color and contrast."

Building: 53  
 Type: 8  
 Elevation: A  
 Color Scheme: 3

Building: 52  
 Type: 8  
 Elevation: C  
 Color Scheme: 4

Building: 51  
 Type: 6-9  
 Elevation: A  
 Color Scheme: 1

Building: 46  
 Type: 6-9  
 Elevation: B  
 Color Scheme: 2

Building: 45  
 Type: 1-4  
 Elevation: C  
 Color Scheme: 3



**PROVENCE**

2/24/2011

Courts - East Commerce Way looking Southwest - With Trees



Note: Different monitors and printers may skew colors. Please reference the color boards provided for "true color and contrast."

Building: 53  
 Type: 8  
 Elevation: A  
 Color Scheme: 3

Building: 52  
 Type: 8  
 Elevation: C  
 Color Scheme: 4

Building: 51  
 Type: 6-9  
 Elevation: A  
 Color Scheme: 1

Building: 46  
 Type: 6-9  
 Elevation: B  
 Color Scheme: 2

Building: 45  
 Type: 1-4  
 Elevation: C  
 Color Scheme: 3



**PROVENCE**

2/24/2011

Courts - East Commerce Way Looking West - With Trees

Exhibit 6G: East Commerce Views Looking Southwest & West – Without Trees



Note: Different monitors and printers may skew colors. Please reference the color boards provided for "true color and contrast."

Building: 53  
Type: 8  
Elevation: A  
Color Scheme: 3

Building: 52  
Type: 8  
Elevation: C  
Color Scheme: 4

Building: 51  
Type: 6-9  
Elevation: A  
Color Scheme: 1

Building: 46  
Type: 6-9  
Elevation: B  
Color Scheme: 2

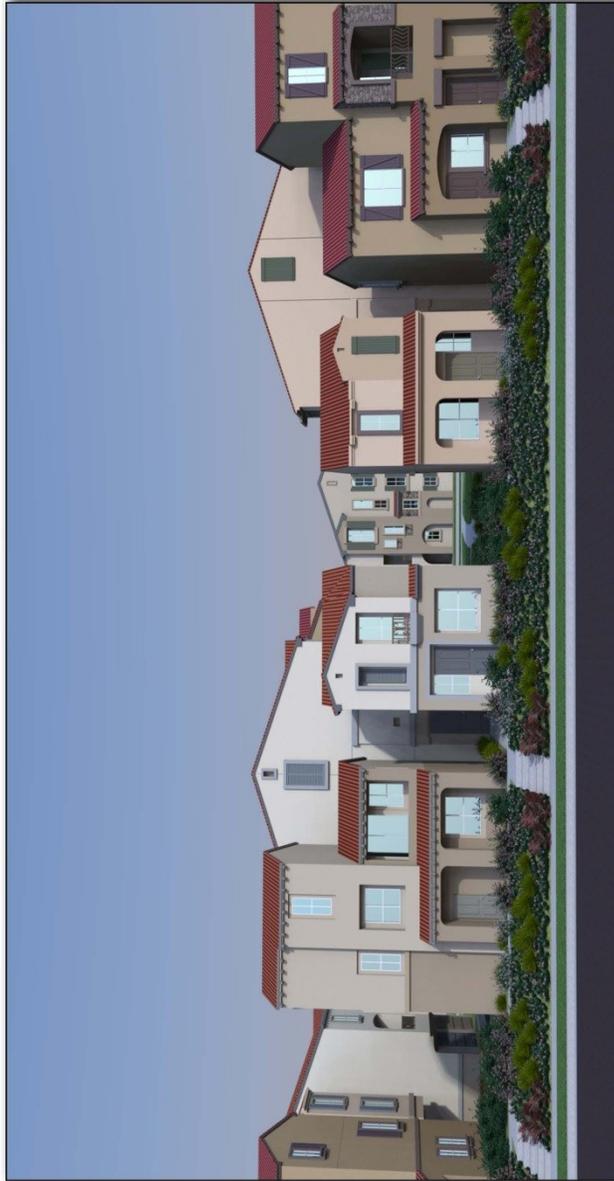
Building: 45  
Type: 1-4  
Elevation: C  
Color Scheme: 3



**PROVENCE**

2/24/2011

Courts - East Commerce Way looking Southwest -Without Trees



Note: Different monitors and printers may skew colors. Please reference the color boards provided for "true color and contrast."

Building: 53  
 Type: 8  
 Elevation: A  
 Color Scheme: 3

Building: 52  
 Type: 8  
 Elevation: C  
 Color Scheme: 4

Building: 51  
 Type: 6-9  
 Elevation: A  
 Color Scheme: 1

Building: 46  
 Type: 6-9  
 Elevation: B  
 Color Scheme: 2

Building: 45  
 Type: 1-4  
 Elevation: C  
 Color Scheme: 3

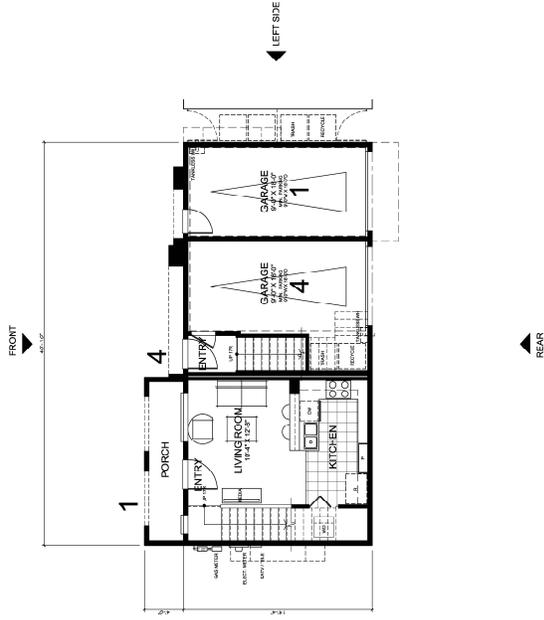
**PROVENCE**

2/24/2011

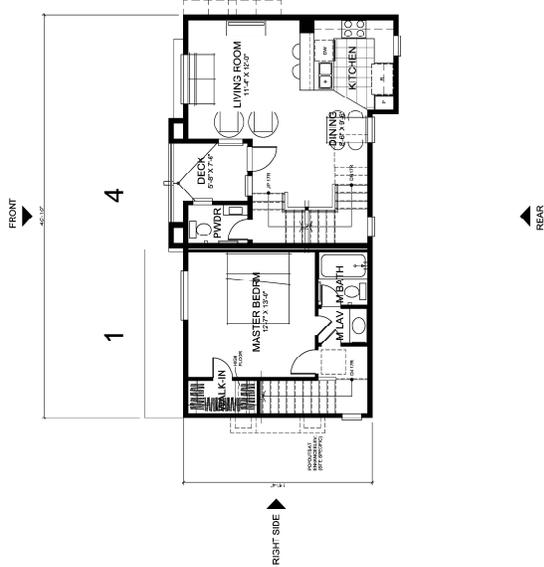


Courts - East Commerce Way looking West - Without Trees

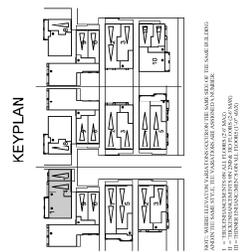
# Exhibit 6H: Courts – House Plans and Elevations



FIRST FLOOR



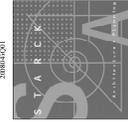
SECOND FLOOR



NOTE: THESE ELEVATIONS WERE DEVELOPED BY THE ARCHITECT AS PART OF THE PRELIMINARY DESIGN AND ARE SUBJECT TO CHANGE WITHOUT NOTICE. THE ARCHITECT ASSUMES NO LIABILITY FOR ANY ERRORS OR OMISSIONS IN THESE ELEVATIONS. THE ARCHITECT'S RESPONSIBILITY IS LIMITED TO THE DESIGN AND CONSTRUCTION OF THE BUILDING AS SHOWN ON THESE ELEVATIONS.

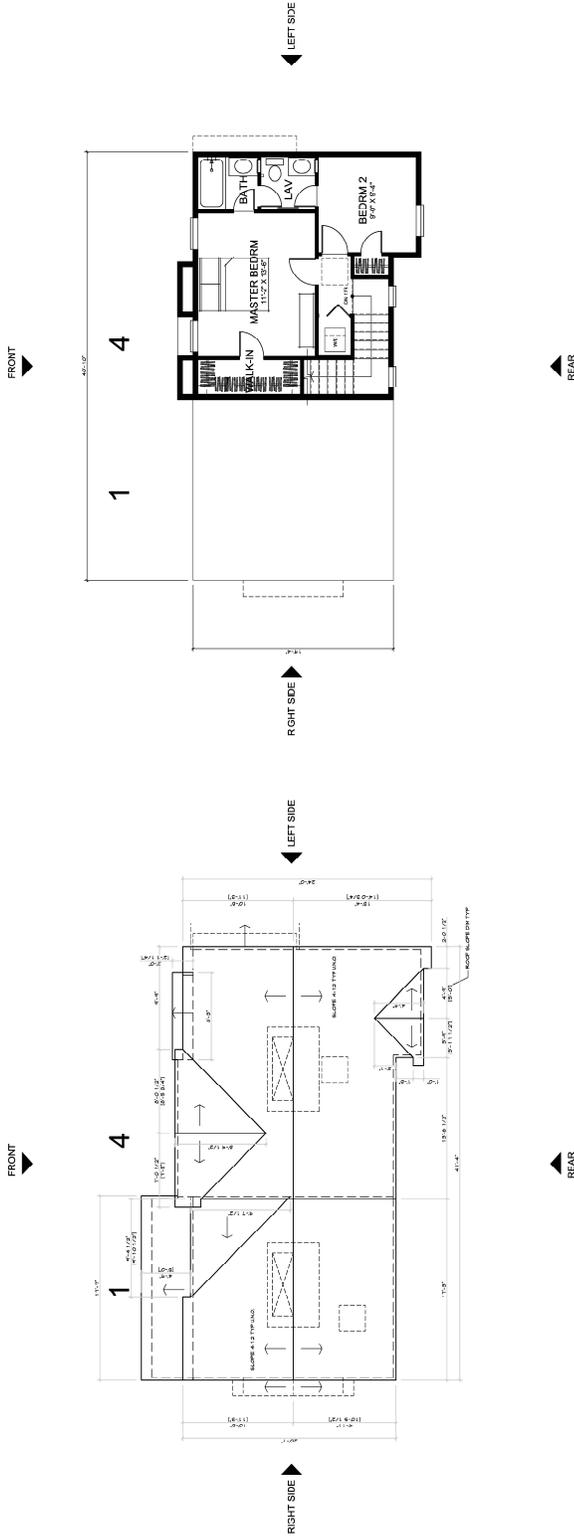
UNIT 1 & 4  
FIRST / SECOND FLOOR PLANS  
ELEVATION STYLE - "A"

11" = 1'-0"  
**PROVENCE - COURTS**  
RC NATOMAS, LLC



1.5.1

AREA ANALYSIS	
FIRST FLOOR	326.37 SF
SECOND FLOOR	326.37 SF
<b>TOTAL UNIT AREA</b>	<b>652.74 SF</b>
COMMON AREA	188.77 SF
<b>TOTAL COMMON AREA</b>	<b>188.77 SF</b>
<b>TOTAL AREA</b>	<b>841.51 SF</b>
UNIT 1	
FIRST FLOOR	307.74 SF
SECOND FLOOR	307.74 SF
<b>TOTAL UNIT AREA</b>	<b>615.48 SF</b>
COMMON AREA	274.03 SF
<b>TOTAL COMMON AREA</b>	<b>274.03 SF</b>
<b>TOTAL AREA</b>	<b>889.51 SF</b>



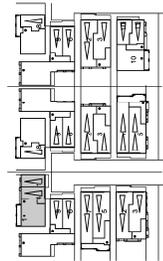
THIRD FLOOR

ROOF PLAN

UNIT 1 & 4  
THIRD FLOOR / ROOF PLAN  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC

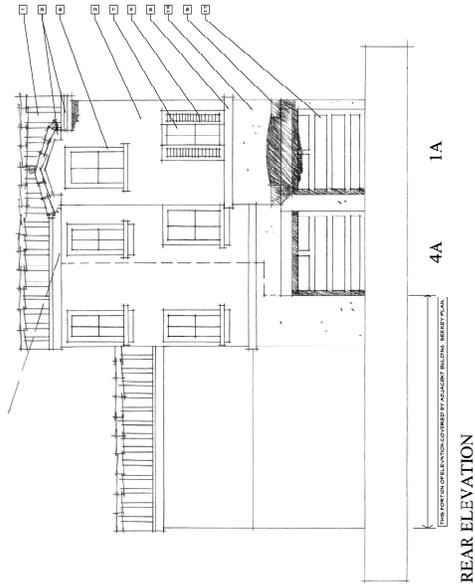
KEYPLAN



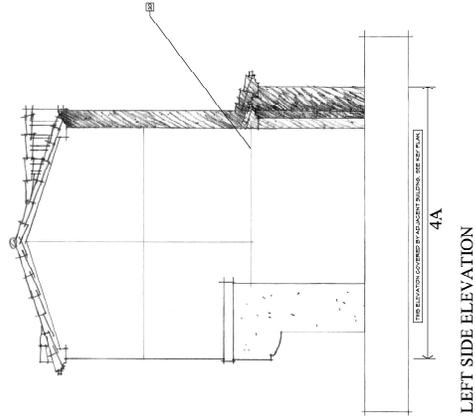
NOT: WHERE THE PLAN SHOWS CONNECTIONS TO THE MAIN SET OF THE SAME FLOORING AND THE SAME STYLE, THE PARTITION OR FINISHES NUMBER IS THE SAME AS THE MAIN SET. THE PARTITION OR FINISHES NUMBER IS THE SAME AS THE MAIN SET. THE PARTITION OR FINISHES NUMBER IS THE SAME AS THE MAIN SET.

AREA ANALYSIS	
UNIT 1	1,000 S.F.
UNIT 4	1,000 S.F.
STAIR FLOOR	500 S.F.
TOTAL LIVING AREA	2,000 S.F.
STAIR FLOOR	500 S.F.
TOTAL AREA	2,500 S.F.
UNIT 1	
STAIR FLOOR	500 S.F.
UNIT 4	1,000 S.F.
TOTAL LIVING AREA	1,500 S.F.
STAIR FLOOR	500 S.F.
TOTAL AREA	2,000 S.F.





REAR ELEVATION



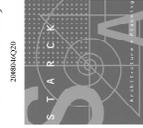
LEFT SIDE ELEVATION

# A

UNIT 1 & 4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

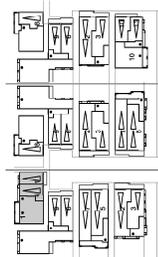
1:1/8" = 1'-0"

PROVENCE - COURTS  
RC NATOMAS, LLC



15.11

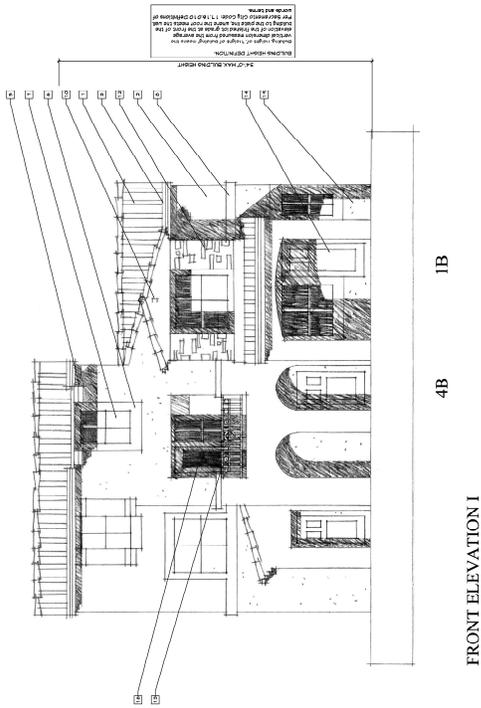
## KEYPLAN



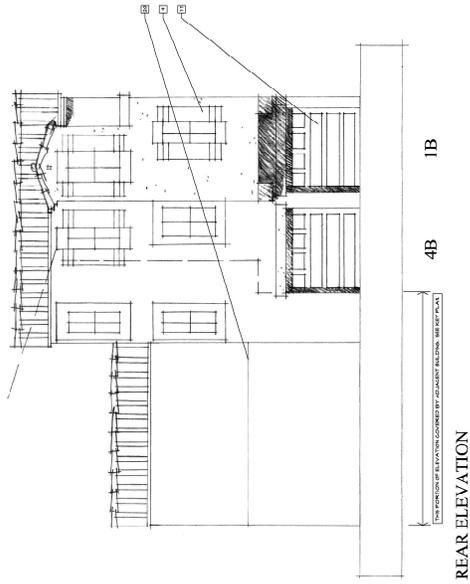
NOTE: UNITS 1 & 4 SHOWN IN SHADING TO INDICATE LOCATION OF THE COURT ELEVATIONS.  
NOTE: THE SHADING IS FOR INFORMATION ONLY AND DOES NOT INDICATE THE EXACT LOCATION OF THE COURT ELEVATIONS.  
1 - THIS UNIT IS SHOWN IN SHADING TO INDICATE LOCATION OF THE COURT ELEVATIONS.  
2 - THIS UNIT IS SHOWN IN SHADING TO INDICATE LOCATION OF THE COURT ELEVATIONS.

## TYPICAL BUILDING MATERIALS

- ☐ INTEGRAL COLOR TILE FLOOR
  - ☐ INTEGRAL COLOR PLASTER
  - ☐ PAINT
  - ☐ RAISE BOARD
  - ☐ PLASTER
  - ☐ PLASTER WINDOW TRIM
  - ☐ VINYL WINDOW
  - ☐ PLASTER TRIM
  - ☐ DECORATIVE SHUTTER
  - ☐ DECORATIVE PLASTER PROJECTION
  - ☐ METAL WINDOW FRAME
  - ☐ SCOTTISH GRANITE
  - ☐ METAL PAUL GUARDRAIL
  - ☐ FIREGLASS ENTRY DOOR
  - ☐ BUILDING SLAB DOOR
  - ☐ FRENCH DOOR
  - ☐ METAL SECTIONAL GARAGE DOOR WITH WINDOW
  - ☐ METAL SECTIONAL GARAGE DOOR WITH WINDOW
  - ☐ HEAVY TEXTURE STUCCO
  - ☐ EXPANSION JOINT
  - ☐ PLASTER BATTERED PAUL
- \* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



FRONT ELEVATION I

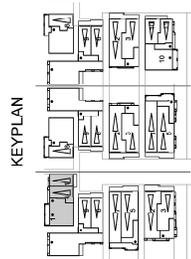


REAR ELEVATION

# B

SEE 'A' ELEVATIONS FOR SIDES

UNIT 1 & 4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'



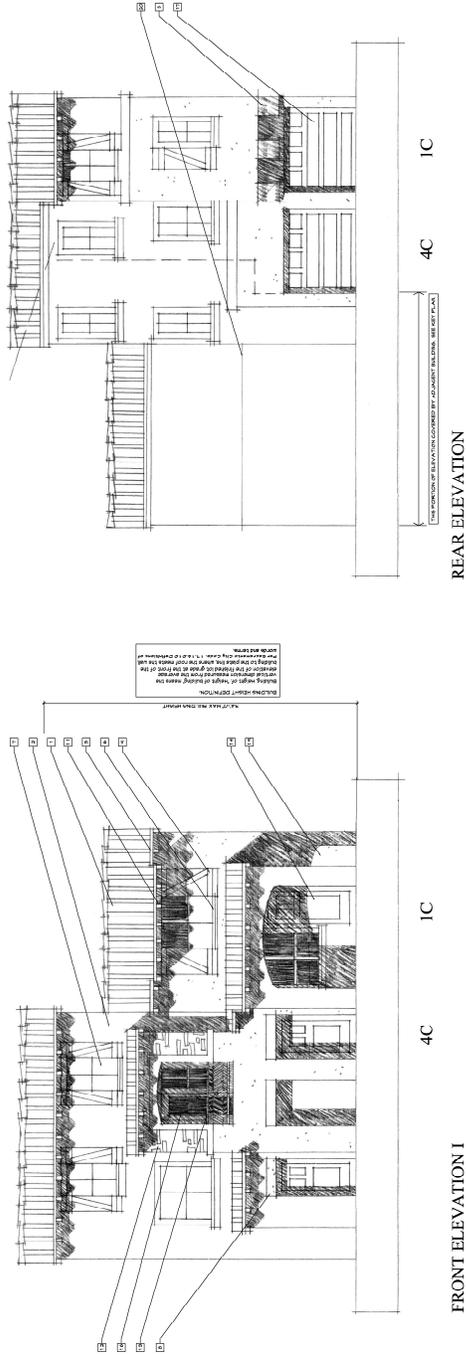
NOTE: ALL UNITS IN THIS DEVELOPMENT MUST BE IN ACCORDANCE WITH THE CODES AND REGULATIONS APPLICABLE TO THE PROJECT AND THE STATE OF CALIFORNIA. THE VARIATIONS AND DIMENSIONS IN THIS KEYPLAN ARE SUBJECT TO THE VARIATIONS AND DIMENSIONS IN THE ARCHITECTURAL DRAWINGS AND SPECIFICATIONS. THE DIMENSIONS IN THIS KEYPLAN SHALL BE THE DIMENSIONS OF THE EXTERIOR WALLS UNLESS OTHERWISE NOTED.

## PROVENCE - COURTS RC NATOMAS, LLC



15.11

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERGRAL COLOR TILE FLOOR
<input type="checkbox"/>	INTERGRAL COLOR PLASTER
<input type="checkbox"/>	PANSLA
<input type="checkbox"/>	RAISE BOARD
<input type="checkbox"/>	PLASTER BOARD
<input type="checkbox"/>	PLASTER INSOCK TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	WOOD TRIM
<input type="checkbox"/>	WOOD TRIM/GRINDER
<input type="checkbox"/>	METAL KAIL / GUANERAIL
<input type="checkbox"/>	FRINGELASS ENTRY DOOR
<input type="checkbox"/>	BUILDING SLASH DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL RANGE HOOD WITH WINDOW
<input type="checkbox"/>	METAL SECTIONAL RANGE HOOD WITH WINDOW
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED PA.L
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



FRONT ELEVATION I

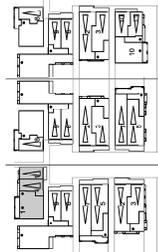
REAR ELEVATION



SEE 'A' ELEVATIONS FOR SIDES

UNIT 1 & 4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

KEYPLAN



NOTE: ALL UNIT 1 & 4 UNITS ARE LOCATED ON THE EAST SIDE OF THE COURT BUILDING. UNITS 1 & 4 ARE LOCATED ON THE WEST SIDE OF THE COURT BUILDING. UNITS 1 & 4 ARE LOCATED ON THE WEST SIDE OF THE COURT BUILDING. UNITS 1 & 4 ARE LOCATED ON THE WEST SIDE OF THE COURT BUILDING.

**TYPICAL BUILDING MATERIALS**

- INTEGRAL COLOR TILE FLOOR
- INTEGRAL COLOR PLASTER
- PAINT
- RAISE BOARD
- PLASTER
- PLASTER INSULATION
- VINYL WINDOW
- PLASTER TRIM
- DECORATIVE SHUTTER
- DECORATIVE PLASTER PROJECTION
- PLASTER
- SPONGE TEXTURE GROUT
- METAL PAIL / GUARDRAIL
- FIRE-RATED GLASS ENTRY DOOR
- BUILDING SLAB DOOR
- FRENCH DOOR
- METAL SECTIONAL GARAGE DOOR WITH WINDOW
- METAL SECTIONAL GARAGE DOOR WITH WINDOW
- HEAVY TEXTURE STUCCO
- EXPANSION JOINT
- PLASTER BATTERED PAINT

\* ALL METAL AND WOOD ELEMENTS TO BE PAINTED

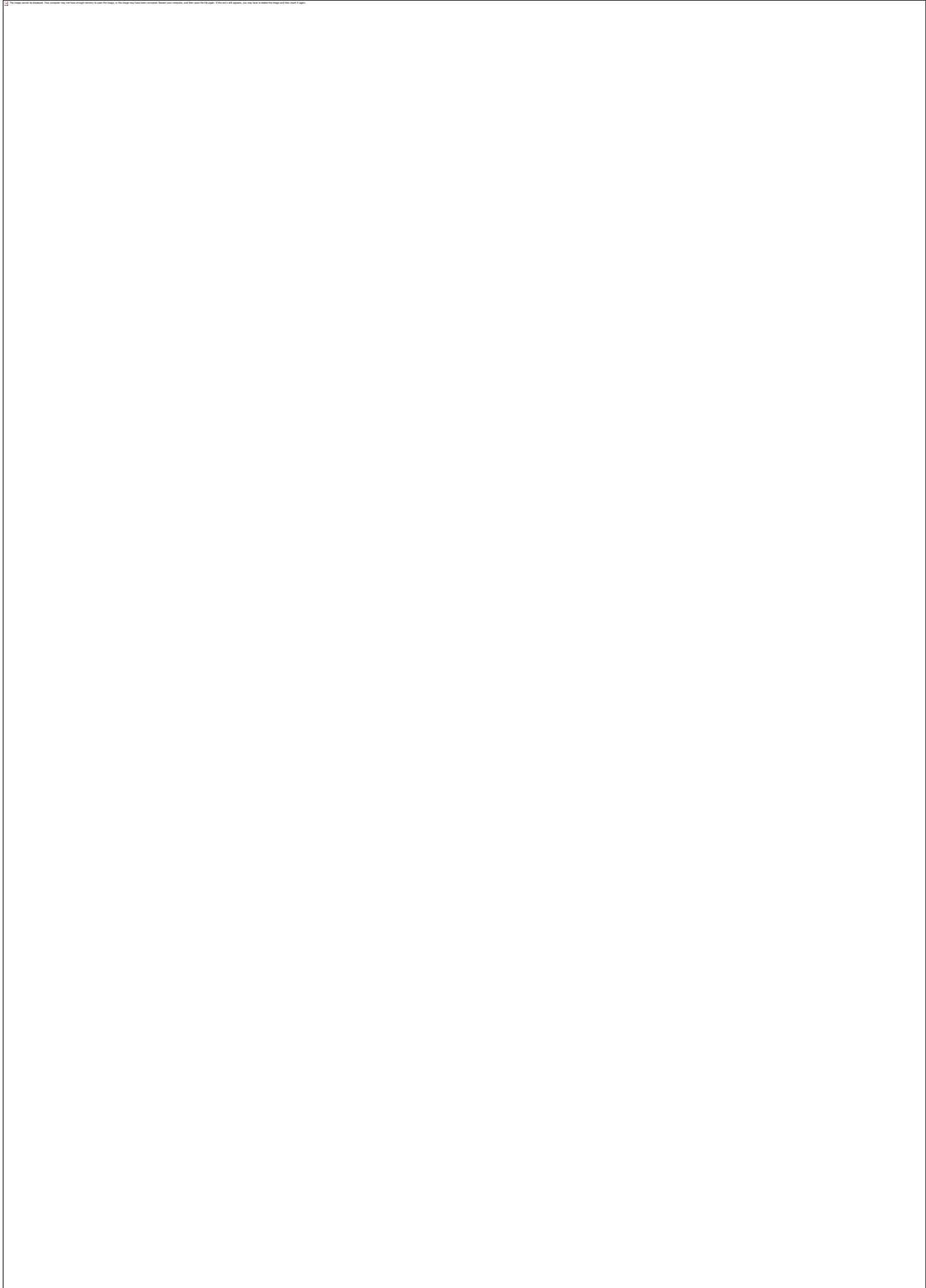
PROVENCE - COURTS  
RC NATOMAS, LLC



1.5.11

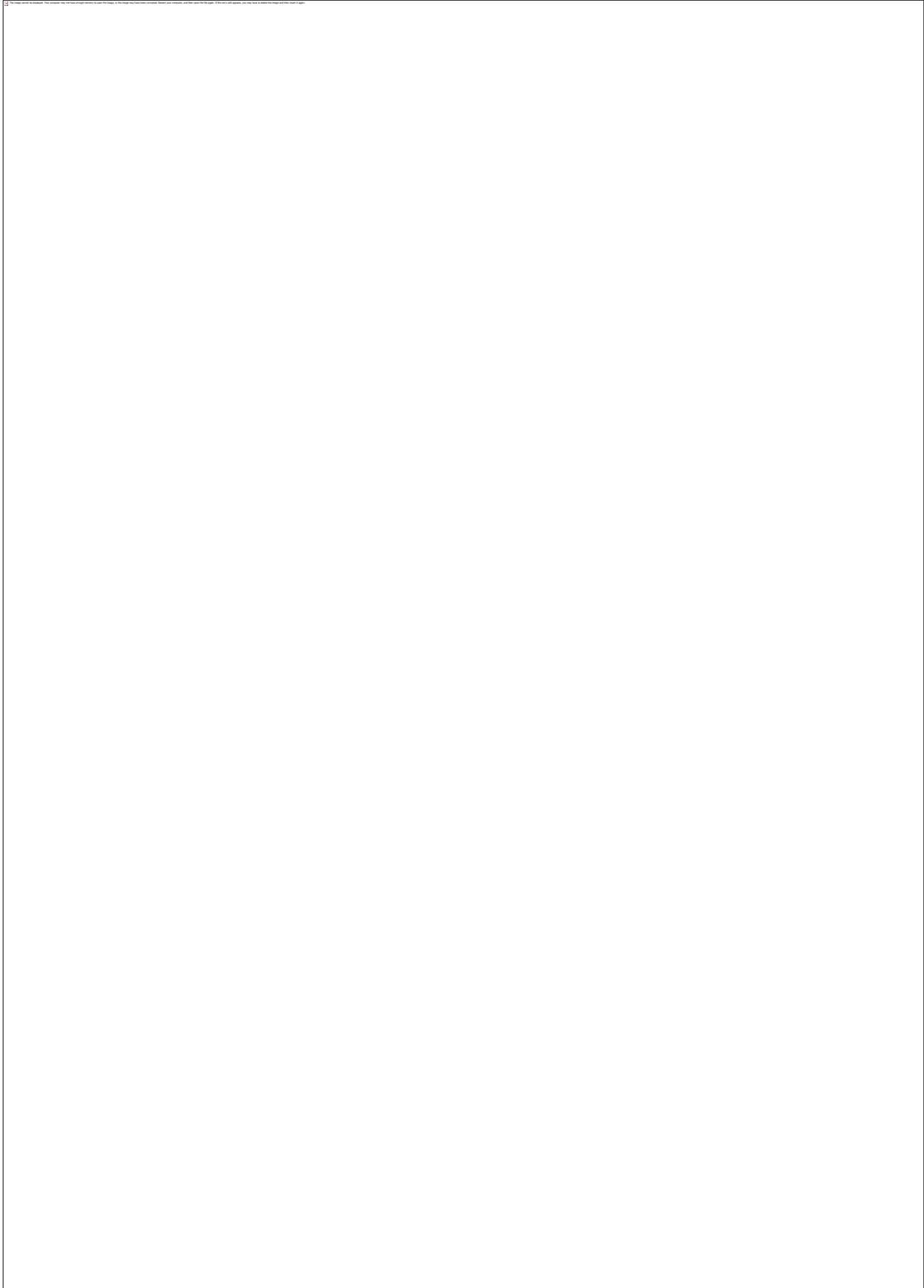


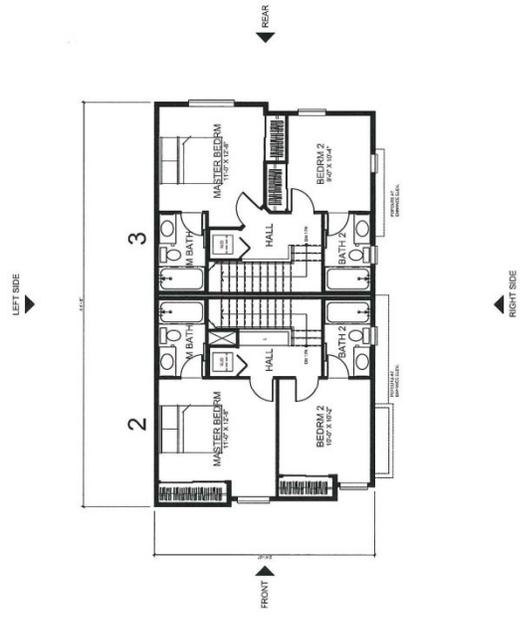




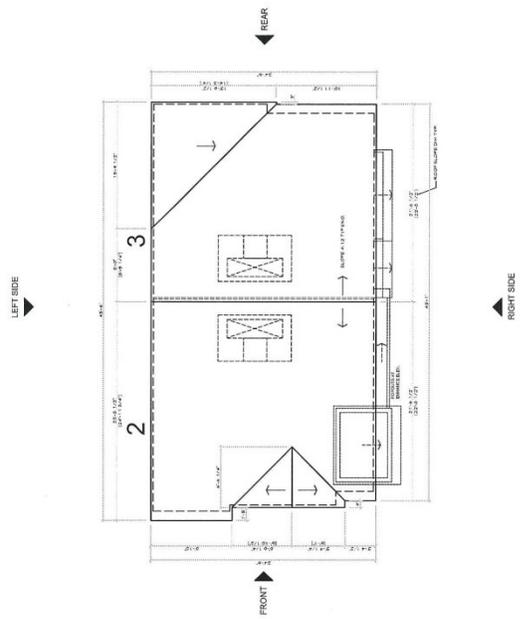








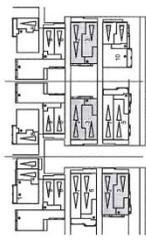
ROOF PLAN



THIRD FLOOR

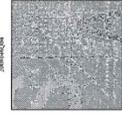
UNIT 2 & 3  
THIRD FLOOR / ROOF PLAN  
ELEVATION STYLE - X

KEYPLAN



NOTE: THESE ELEVATIONS AND CONSTRUCTION SHALL BE THE SAME AS SHOWN ON THE GENERAL CONTRACT DOCUMENTS.  
1. ALL DIMENSIONS SHALL BE AS SHOWN ON THE GENERAL CONTRACT DOCUMENTS.  
2. ALL DIMENSIONS SHALL BE AS SHOWN ON THE GENERAL CONTRACT DOCUMENTS.  
3. ALL DIMENSIONS SHALL BE AS SHOWN ON THE GENERAL CONTRACT DOCUMENTS.

PROVENCE - COURTS  
RC/NATOMAS, LLC

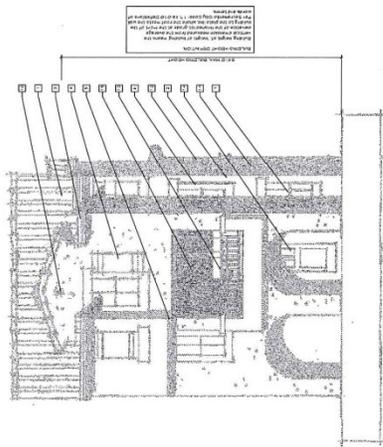


13.111

AREA ANALYSIS	
UNIT 2	1,234.56
UNIT 3	1,234.56
TOTAL	2,469.12
TOTAL AREA	
UNIT 2	1,234.56
UNIT 3	1,234.56
TOTAL	2,469.12
TOTAL AREA	
UNIT 2	1,234.56
UNIT 3	1,234.56
TOTAL	2,469.12

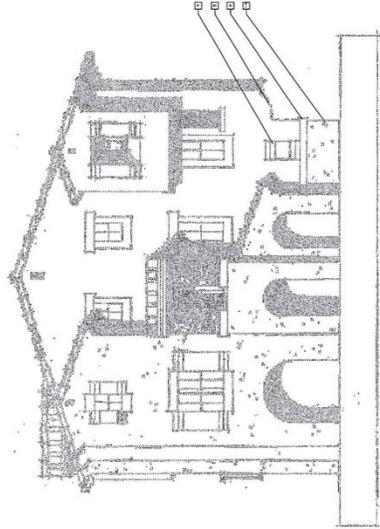






2B 3B

FRONT ELEVATION

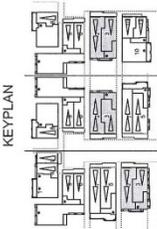


3B

ENHANCED RIGHT SIDE ELEVATION I

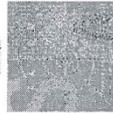
**B**

UNIT 2 & 3  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'



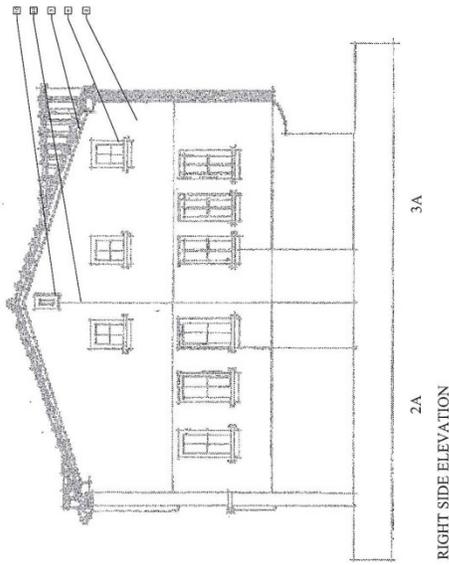
NOTES: 1. REFER TO THE ARCHITECTURAL PLAN FOR THE NAME AND SIZE OF THE UNIT YOU CHOOSE.  
2. REFER TO THE ARCHITECTURAL PLAN FOR THE NAME AND SIZE OF THE UNIT YOU CHOOSE.  
3. REFER TO THE ARCHITECTURAL PLAN FOR THE NAME AND SIZE OF THE UNIT YOU CHOOSE.

PROVENCE - COURTS  
RC NATOMAS, LLC

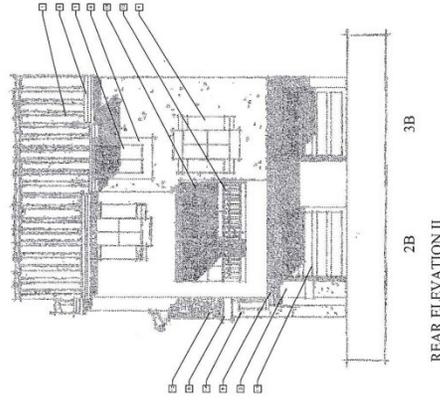


1:1/11

TYPICAL BUILDING MATERIALS	
[ ]	INTERNAL COLOUR TILE ROOF
[ ]	PAINT - COLOUR PLASTER
[ ]	PAINT BOARD
[ ]	PLASTER CORBEL
[ ]	PLASTER TRIM
[ ]	VINYL FINISH
[ ]	PLASTER TRIM
[ ]	DECORATIVE SWITCH
[ ]	PLASTER FINISH
[ ]	PLASTER FINISH
[ ]	STONE PATTERN GROUND
[ ]	METAL PAINT / GRANULAR
[ ]	WOOD GRASS DOOR
[ ]	FRENCH DOOR
[ ]	METAL SECTIONAL GARAGE DOOR WITH FINISH
[ ]	METAL PAINT / GRANULAR
[ ]	EXPANSION JOINT
[ ]	PLASTER BATTERED FINISH
[ ]	*ALL METAL AND ROOF ELEMENTS TO BE PAINTED



2A  
RIGHT SIDE ELEVATION

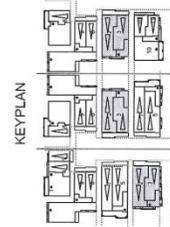


2B  
REAR ELEVATION II

# A,B

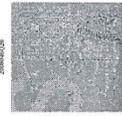
SEE 'A' ELEVATIONS FOR LEFT SIDE

UNIT 2 & 3  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A' & 'B'



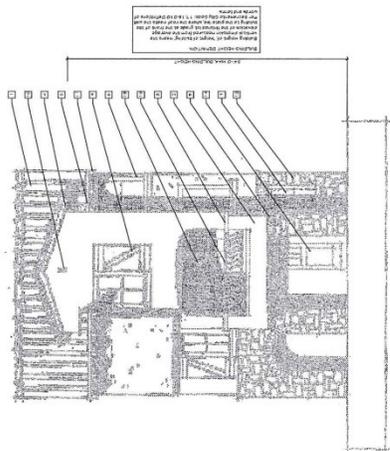
WITH THESE ELEVATIONS AND SECTION SHOWN, THE CLIENT SHALL BE RESPONSIBLE FOR THE FINAL MATERIALS SELECTION. THE CLIENT SHALL BE RESPONSIBLE FOR THE FINAL MATERIALS SELECTION. THE CLIENT SHALL BE RESPONSIBLE FOR THE FINAL MATERIALS SELECTION.

## PROVENCE - COURTS RC NATOMAS, LLC



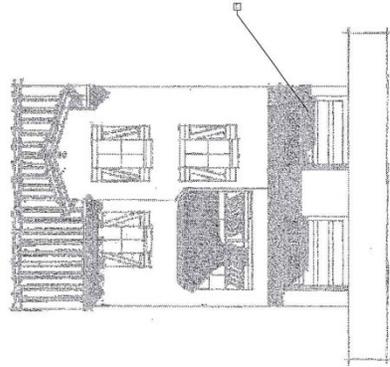
1:5/11

TYPICAL BUILDING MATERIALS	
☐	INTERNAL COLOR TILE ROOF
☐	INTERNAL COLOR PLASTER
☐	PAVING
☐	FRAME BOARD
☐	PLASTER CORNICE
☐	PLASTER CORNICE
☐	UNGLAZED WINDOW
☐	PLASTER TRIM
☐	DECORATIVE SWITCHER
☐	STONE PATTERN PROJECTION
☐	STONE PATTERN VOISER
☐	METAL RAIL / HANDRAIL
☐	METAL RAIL / HANDRAIL
☐	FRANCE DOOR
☐	METAL SECTIONAL SHAMERE DOOR WITH WINDOWS
☐	HEAVY TEXTURE STYCO
☐	EXPANSION JOINT
☐	PLASTER BATTERED PILL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



2C 3C

FRONT ELEVATION



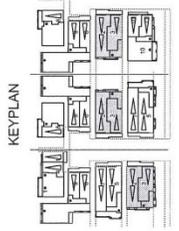
2C 3C

REAR ELEVATION



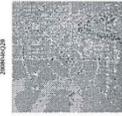
SEE 'A' ELEVATIONS FOR STANDARD SIDES

UNIT 2 & 3  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - C



NOTE: THESE ELEVATIONS REPRESENT ONLY THE EXTERIOR OF THE BUILDING. THE INTERIOR OF THE BUILDING IS NOT SHOWN. ALL DIMENSIONS ARE IN FEET AND INCHES. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.

PROVENCE - COURTS  
RC NATOMAS, LLC

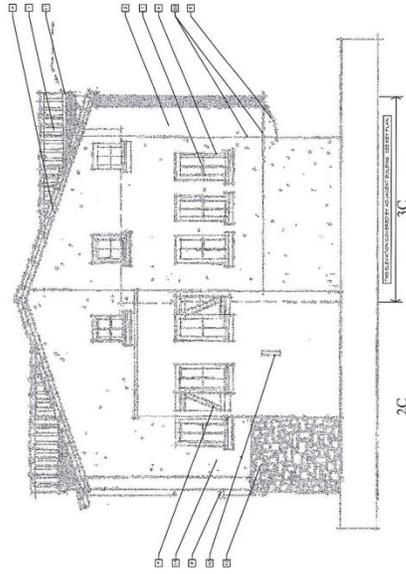


1.5.11

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR TILE ROOF
<input type="checkbox"/>	INTERNAL COORDINATING
<input type="checkbox"/>	PAVING
<input type="checkbox"/>	BASE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WITH
<input type="checkbox"/>	VINYL FINISH
<input type="checkbox"/>	PLASTER WITH
<input type="checkbox"/>	DECORATIVE BRICK
<input type="checkbox"/>	PLASTER WITH
<input type="checkbox"/>	PLASTER WITH
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL PANEL / GRANITE
<input type="checkbox"/>	BRICK
<input type="checkbox"/>	BRICK
<input type="checkbox"/>	BRICK
<input type="checkbox"/>	METAL SECTIONAL SHAMISE DOOR WITH FINISH
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED FINISH
<input type="checkbox"/>	* ALL METAL AND ROOF ELEMENTS TO BE PAINTED







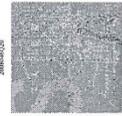
ENHANCED RIGHT SIDE ELEVATION III



SEE 'A' ELEVATIONS FOR STANDARD SIDES

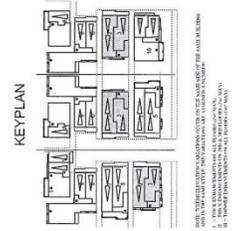
UNIT 2 & 3  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - C

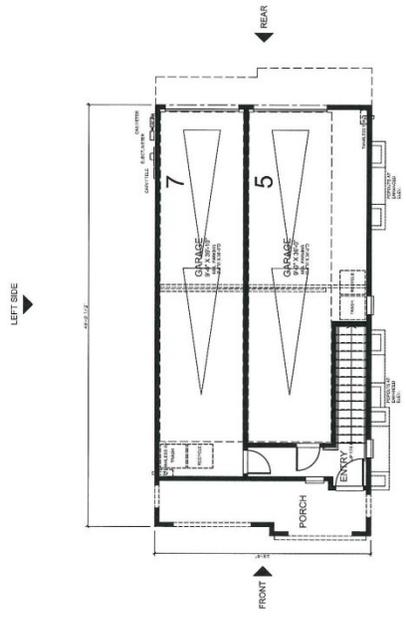
PROVENCE - COURTS  
RC NATOMAS, LLC



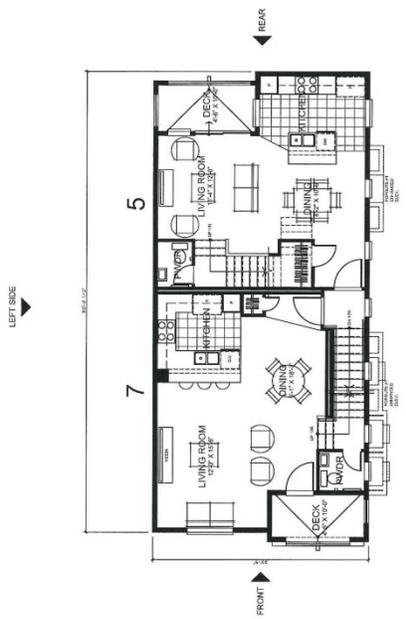
1.1.11

TYPICAL BUILDING MATERIALS	
☐	INTERNAL COLOR OF TILE ROOF
☐	INTERNAL COORDINATION
☐	PAVING
☐	RAISE BOARD
☐	PLASTER CORNICE
☐	PLASTER FINISH
☐	PLASTER TRIM
☐	PLASTER WINDOW
☐	PLASTER SHIFTER
☐	PLASTER PARTITION
☐	PLASTER TAIL
☐	STONE PATTERN VENEER
☐	METAL RAIL / GRABBAR
☐	METAL RAIL / HANDRAIL
☐	METAL RAIL / STAIR
☐	FRENCH DOOR
☐	METAL SECTIONAL SHAMBE DOOR (RT-1) FINISH
☐	METAL SECTIONAL SHAMBE DOOR (RT-2) FINISH
☐	HEAVY TEXTURE STUCCO
☐	EXPANSION JOINT
☐	PLASTER BATTERED PALL
* ALL METAL AND ROOF ELEMENTS TO BE PAINTED	





FIRST FLOOR



SECOND FLOOR

UNIT 5 & 7  
FIRST / SECOND FLOOR PLANS  
ELEVATION STYLE - 'A'



NOTE: THESE PLANS ARE FOR INFORMATION ONLY. THE OWNER SHALL BE THE FINAL AUTHORITY.  
1. ALL DIMENSIONS ARE IN FEET AND INCHES.  
2. ALL DIMENSIONS ARE TO FACE UNLESS NOTED OTHERWISE.  
3. ALL DIMENSIONS ARE TO FACE UNLESS NOTED OTHERWISE.

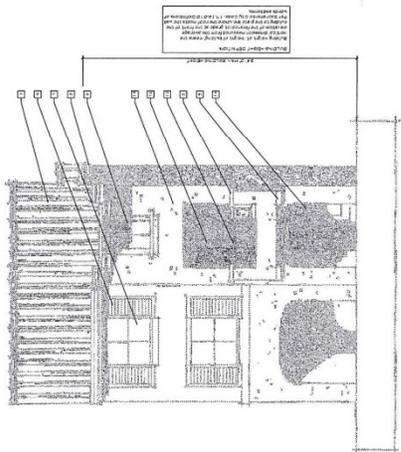
PROVENCE - COURTS  
RC NATOMAS, LLC



1:5.11

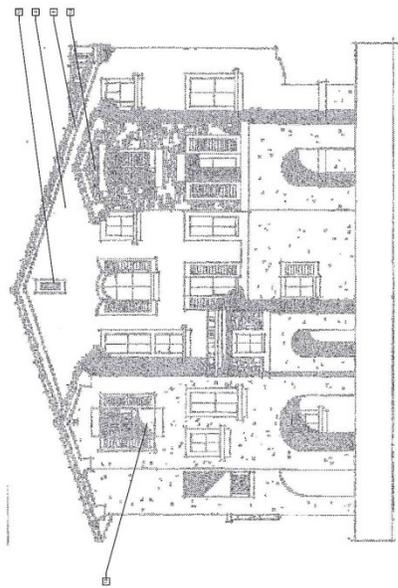
UNIT	ANALYSIS	DATE
UNIT 5	FOUNDATION	08/01/2017
UNIT 5	STRUCTURE	08/01/2017
UNIT 5	MECHANICAL	08/01/2017
UNIT 5	ELECTRICAL	08/01/2017
UNIT 5	PLUMBING	08/01/2017
UNIT 5	FINISHES	08/01/2017
UNIT 5	LANDSCAPE	08/01/2017
UNIT 5	CONCRETE	08/01/2017
UNIT 5	PAINT	08/01/2017
UNIT 5	GLASS	08/01/2017
UNIT 5	IRONING	08/01/2017
UNIT 5	STAIRS	08/01/2017
UNIT 5	ELEVATORS	08/01/2017
UNIT 5	MECHANICAL	08/01/2017
UNIT 5	ELECTRICAL	08/01/2017
UNIT 5	PLUMBING	08/01/2017
UNIT 5	FINISHES	08/01/2017
UNIT 5	LANDSCAPE	08/01/2017
UNIT 5	CONCRETE	08/01/2017
UNIT 5	PAINT	08/01/2017
UNIT 5	GLASS	08/01/2017
UNIT 5	IRONING	08/01/2017
UNIT 5	STAIRS	08/01/2017
UNIT 5	ELEVATORS	08/01/2017





5A 7A

FRONT ELEVATION



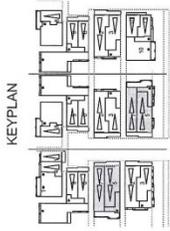
7A

5A

ENHANCED RIGHT SIDE ELEVATION I

A

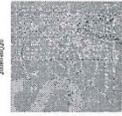
UNIT 5 & 7  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - A



KEYPLAN

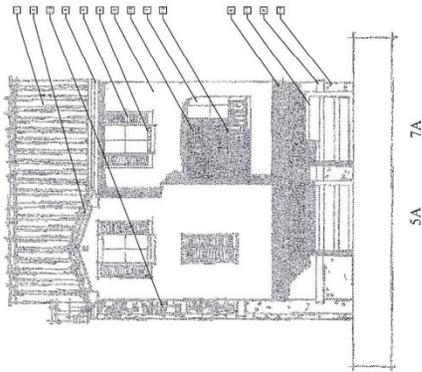
NOTE: UNIT 5 IS A CONDO AND UNIT 7 IS A CONDO. THE UNIT 5 AND 7 ARE SHOWN IN THE KEYPLAN. THE UNIT 5 AND 7 ARE SHOWN IN THE KEYPLAN. THE UNIT 5 AND 7 ARE SHOWN IN THE KEYPLAN.

PROVENCE - COURTS  
RC NATOMAS, LLC

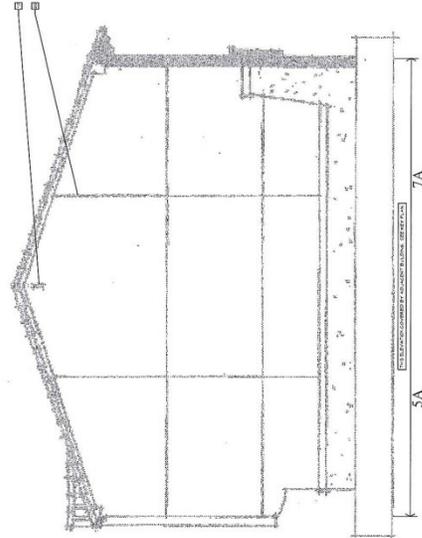


1:1/11

TYPICAL BUILDING MATERIALS	
INTERNAL COLOR OF TILE ROOF	1
EXTERNAL COLOR OF ROOF	2
PLASTER	3
PAINT	4
PAINT BOARD	5
PLASTER CORNICE	6
PLASTER TRIM	7
WOOD TRIM	8
PLASTER TRIM	9
WOOD TRIM	10
WOOD TRIM	11
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WOOD TRIM	95
WOOD TRIM	96
WOOD TRIM	97
WOOD TRIM	98
WOOD TRIM	99
WOOD TRIM	100



REAR ELEVATION



STANDARD LEFT SIDE ELEVATION

A

UNIT 5 & 7  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'



KEYPLAN

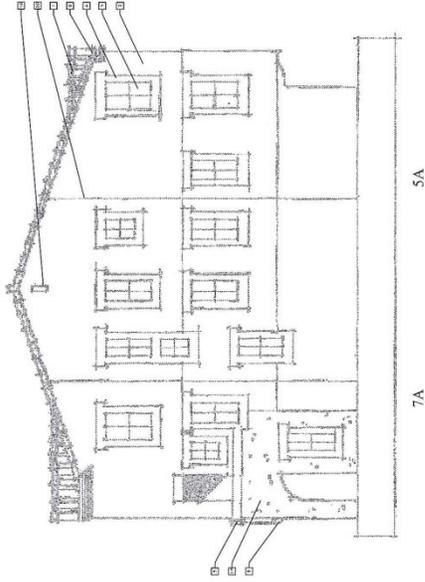
NOTE: REFER TO THE ARCHITECTURAL DRAWINGS FOR THE NAME AND SIZE OF THE UNIT AND THE TYPE OF THE UNIT. THE UNIT NAME AND SIZE ARE INDICATED IN THE ARCHITECTURAL DRAWINGS. THE UNIT NAME AND SIZE ARE INDICATED IN THE ARCHITECTURAL DRAWINGS. THE UNIT NAME AND SIZE ARE INDICATED IN THE ARCHITECTURAL DRAWINGS.

PROVENCE - COURTS  
RC NATOMAS, LLC



1:11

TYPICAL BUILDING MATERIALS	
☐	INTERNAL COLOUR OF TILE ROOF
☐	INTERNAL COLOUR PLASTER
☐	PAVING
☐	PAVE BOARD
☐	PLASTER CORNICE
☐	WOOD BRICKWORK FINISH
☐	PLASTER FINISH
☐	DECORATIVE SHUTTER
☐	WOODEN PLASTER PROJECTION
☐	STONE PATTERN VENEER
☐	METAL PANEL / HANDRAIL
☐	WOODEN DOOR
☐	METAL DOOR
☐	METAL SECTIONAL GARAGE DOOR WITH FINISH
☐	EXPANSION JOINT
☐	PLASTER BATTERED PILLAR

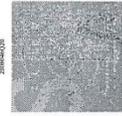


7A  
STANDARD RIGHT SIDE ELEVATION  
5A

A

UNIT 5 & 7  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - A

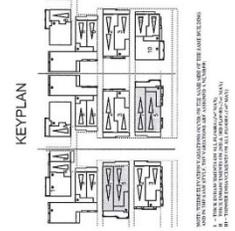
PROVENCE - COURTS  
RC NATOMAS, LLC

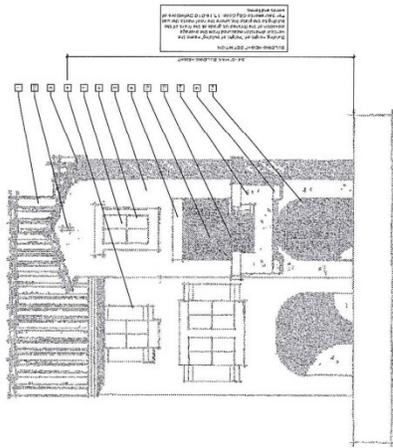


1.5.11

**TYPICAL BUILDING MATERIALS**

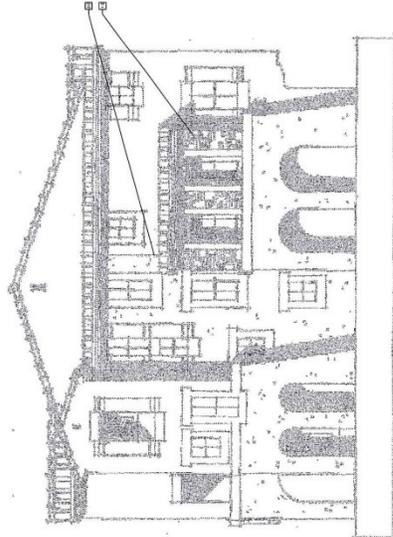
- INTERNAL COLOR TILE ROOF
- METAL COLOR PLASTER
- PAINT
- PAINT BOARD
- PLASTER CORNICE
- PLASTER FINISH
- VINYL WINDOW
- PLASTER TRIM
- DECORATIVE BRICK
- METAL PATENT PROJECTION
- BRICK PATENT PROJECTION
- STONE PATENT VESSEL
- METAL PAINT GUARDRAIL
- METAL PATENT GUARDRAIL
- BRICK PATENT GUARDRAIL
- FRENCH DOOR
- METAL SECTIONAL SHAMALE DOOR WITH WINDOWS
- METAL PATENT SHAMALE DOOR WITH WINDOWS
- EXPANSION JOINT
- PLASTER BATTERED FINISH
- FULL METAL AND ROOF ELEMENTS TO BE PAINTED





5B 7B

FRONT ELEVATION



7B

5B

ENHANCED RIGHT SIDE ELEVATION I

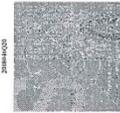
# B

SEE 'A' ELEVATIONS FOR SIDES

UNIT 5 & 7  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'

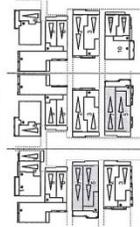
1/4" = 1'-0"

PROVENCE - COURTS  
RC NATOMAS, LLC



1:1 (H)

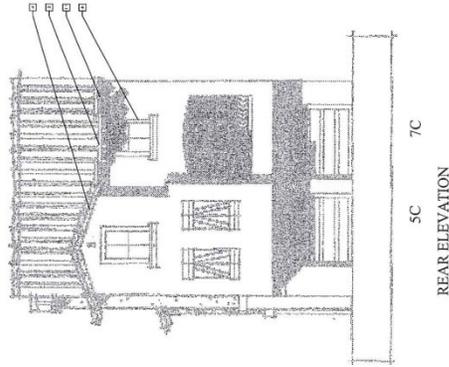
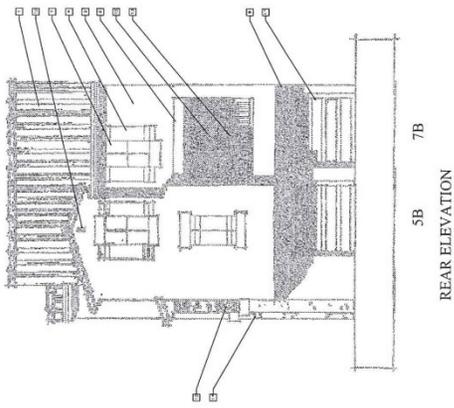
## KEYPLAN



NOTES: 1. THIS KEY PLAN IS A GENERAL REFERENCE ONLY. IT DOES NOT SHOW THE EXACT DIMENSIONS OF THE UNITS. 2. THE UNITS SHOWN IN THIS KEY PLAN ARE SUBJECT TO CHANGE WITHOUT NOTICE. 3. THE UNITS SHOWN IN THIS KEY PLAN ARE SUBJECT TO CHANGE WITHOUT NOTICE.

TYPICAL BUILDING MATERIALS	
☐	METAL COLOR TILE ROOF
☐	CONCRETE
☐	PLASTER
☐	PLASTER BOARD
☐	PLASTER CORREL
☐	PLASTER TRIM
☐	VINYL WINDOW
☐	PLASTER TRIM
☐	DISCREETIVE SWITCH
☐	PLASTER PROJECTION
☐	PLASTER TAIL
☐	STONE PATTERN VENEER
☐	METAL PAINT / ALUMINUM
☐	METAL PAINT / ALUMINUM
☐	FRENCH DOOR
☐	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
☐	HEAVY TEXTURE STUCCO
☐	EXPANSION JOINT
☐	PLASTER BATTERED PILL
☐	*ALL METAL AND ROOF ELEMENTS TO BE PAINTED

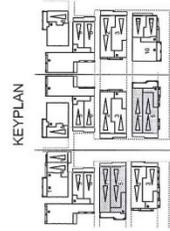




# B,C

UNIT 5 & 7  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B' & 'C'

PROVENCE - COURTS  
RC NATOMAS, LLC

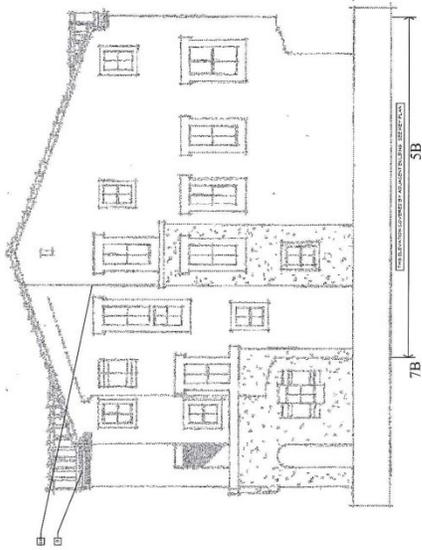


1. ALL WALLS AND CEILING SHALL BE FINISHED WITH 1/2\"/>

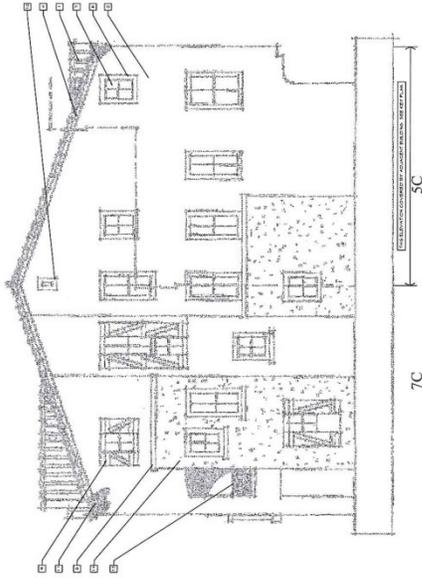
TYPICAL BUILDING MATERIALS	
1	INTERNAL COLOR TILE ROOF
2	EXTERIOR COLORED PLASTER
3	PAVING
4	BASE BOARD
5	PLASTER CORNICE
6	INTERIOR FINISH
7	UNIV. FINISH
8	PLASTER TRIM
9	DECORATIVE SWITCHEL
10	PLASTER MOLDING
11	BAFFLE TAIL
12	SPINE PATTERN VENEER
13	METAL KNUIT / ORNAMENTAL
14	FRANCE DOOR
15	FRANCE DOOR
16	METAL SECTIONAL SHAMISE DOOR WITH FINISHES
17	HEAVY-TASK STYCO
18	EXPANSION JOINT
19	PLASTER LANTERN PALL
20	* ALL METAL AND WOOD ELEMENTS TO BE PAINTED







RIGHT SIDE ELEVATION III

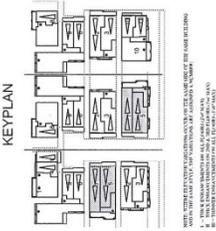


RIGHT SIDE ELEVATION III

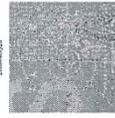
# B,C

SEE 'A' ELEVATIONS FOR STANDARD SIDES

UNIT 5 & 7  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - B & C



PROVENCE - COURTS  
RC NATOMAS, LLC

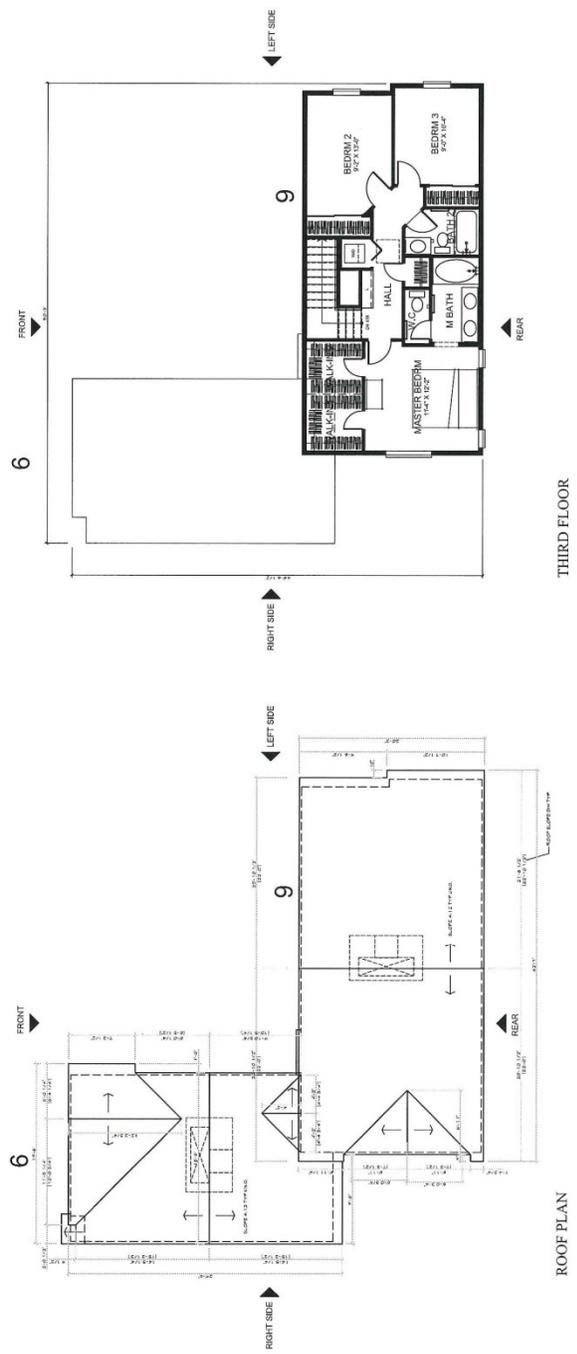


1:1/11

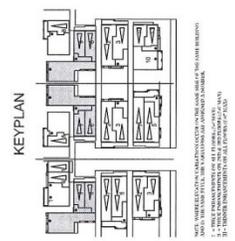
TYPICAL BUILDING MATERIALS	
□	INTERNAL COLOR TILE ROOF
□	ASPHALT/FLY ASH ROOFING
□	PAINT
□	BASE BOARD
□	PLASTER CORBEL
□	WOOD TRIM
□	VINYL WINDOW
□	PLASTER TRIM
□	DECORATIVE SWITCHER
□	PLASTER PROJECTION
□	PLASTER TAIL
□	SPRINKLER PATTERN VENEER
□	METAL PAINT / GRANULAR
□	METAL PAINT / GLOSS
□	FRENCH DOOR
□	METAL SECTIONAL SHAMANE DOOR WITH WINDOWS
□	METAL SECTIONAL SHAMANE DOOR WITH WINDOWS
□	HEAVY METAL STYCOG
□	EXPANSION JOINT
□	PLASTER BATTERED PILL







UNIT 6 & 9  
THIRD FLOOR / ROOF PLAN  
ELEVATION STYLE - 'A'



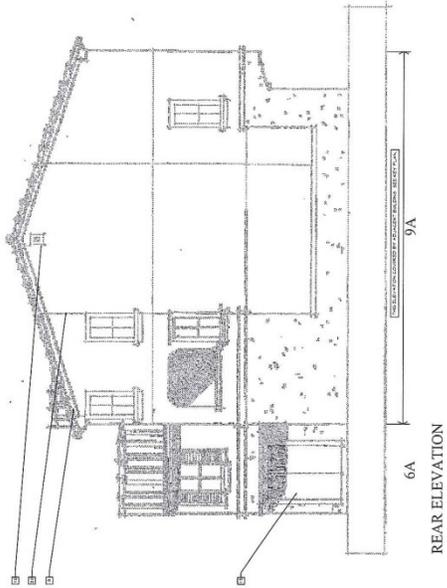
PROVENCE - COURTS  
RC NATOMAS, LLC



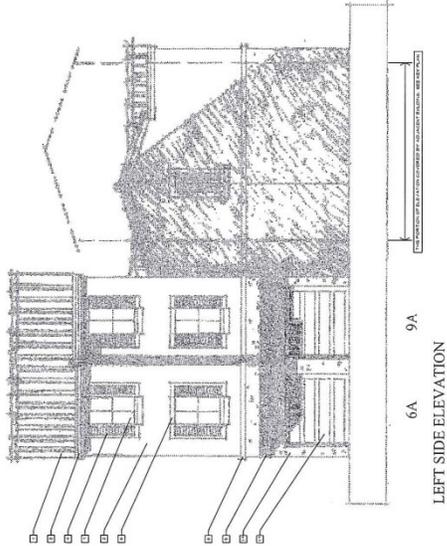
AREA ANALYSIS

THIRD FLOOR	1007.49 S.F.
ROOF PLAN	1007.49 S.F.
TOTAL FLOOR AREA	2014.98 S.F.
NET AREA	1007.49 S.F.
UNIT 9	
THIRD FLOOR	75.35 S.F.
ROOF PLAN	75.35 S.F.
TOTAL FLOOR AREA	150.70 S.F.
NET AREA	75.35 S.F.
UNIT 6	
THIRD FLOOR	954.14 S.F.
ROOF PLAN	954.14 S.F.
TOTAL FLOOR AREA	1908.28 S.F.
NET AREA	954.14 S.F.





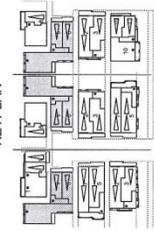
REAR ELEVATION



LEFT SIDE ELEVATION

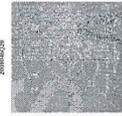
A

UNIT 6 & 9  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - A



NOTE: 1. REFER TO THE ARCHITECTURAL DRAWINGS FOR THE EXTERIOR FINISHES OF THE UNIT BUILDING.  
2. THIS DRAWING IS A PRELIMINARY DRAWING AND IS SUBJECT TO CHANGE.  
3. THIS DRAWING IS FOR INFORMATION ONLY AND IS NOT TO BE USED FOR CONSTRUCTION.

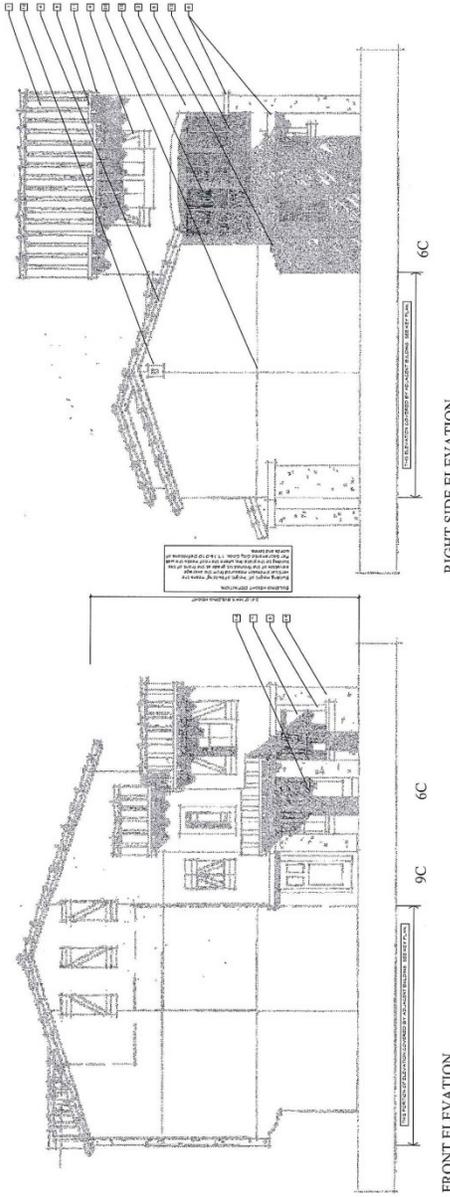
PROVENCE - COURTS  
RC NATOMAS, LLC



1:1

TYPICAL BUILDING MATERIALS	
[Symbol]	INTERNAL COLOR OF TILE ROOF
[Symbol]	INTERNAL COLOR OF PLASTER
[Symbol]	PAINT
[Symbol]	PAVE BOARD
[Symbol]	PLASTER CORNICE
[Symbol]	PLASTER FINISH
[Symbol]	PLASTER TRIM
[Symbol]	PLASTER TRIM
[Symbol]	DECORATIVE SWITCHER
[Symbol]	ROOF TILE
[Symbol]	ROOF TILE PLASTER PROJECTION
[Symbol]	STONE PATTERN VENEER
[Symbol]	METAL RAIL / HANDRAIL
[Symbol]	METAL RAIL / HANDRAIL
[Symbol]	BRASS DOOR
[Symbol]	BRASS DOOR
[Symbol]	METAL SECTIONAL SHUTTER DOOR (KIT + FINISH)
[Symbol]	HEAVY TEXTURE STUCCO
[Symbol]	EXPANSION JOINT
[Symbol]	PLASTER BATTERED FINISH



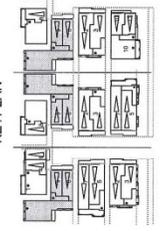


RIGHT SIDE ELEVATION

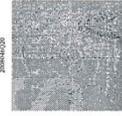
FRONT ELEVATION

C

UNIT 6 & 9  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - C



PROVENCE - COURTS  
RC NATOMAS, LLC



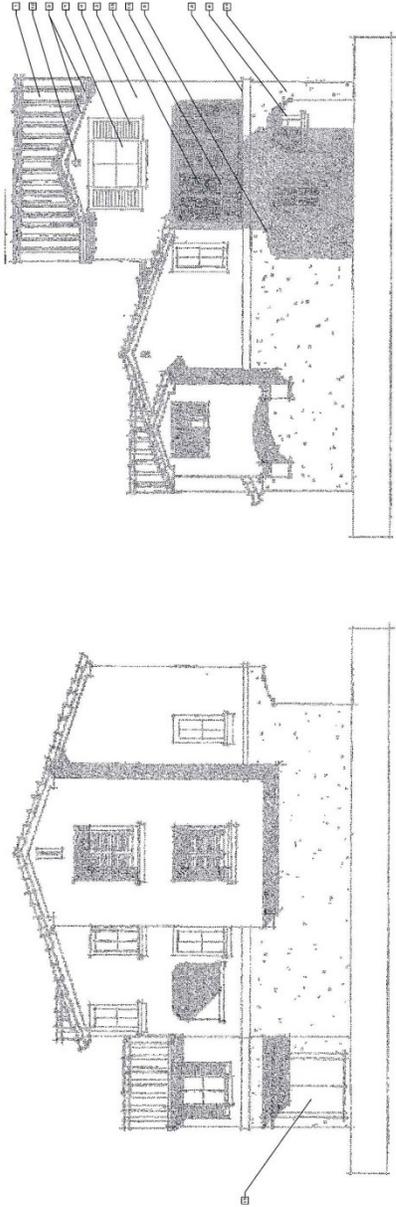
1.5.11

**TYPICAL BUILDING MATERIALS**

- INTERNAL GYPSUM TILE ROOF
- EXTERNAL GYPSUM PLASTER
- FINISH
- BASE BOARD
- PLASTER CORNICE
- 1/2" GYPSUM BOARD
- 1/2" GYPSUM BOARD
- PLASTER TRIM
- DECORATIVE FINISH
- PLASTER
- PLASTER PRODUCTION
- STONE PATTERN VENEER
- METAL PAINT / SANDRILL
- METAL SECTIONAL SHAMERE DOOR WITH ENDS
- METAL SECTIONAL SHAMERE DOOR WITH ENDS
- EXPANSION JOINT
- PLASTER BATTERED WALL

\* ALL METAL AND MOOSE ELEMENTS TO BE PAINTED





6A

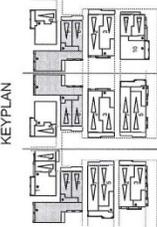
ENHANCED RIGHT SIDE ELEVATION II

9A

REAR ELEVATION II

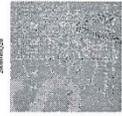
A

UNIT 6 & 9  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - A



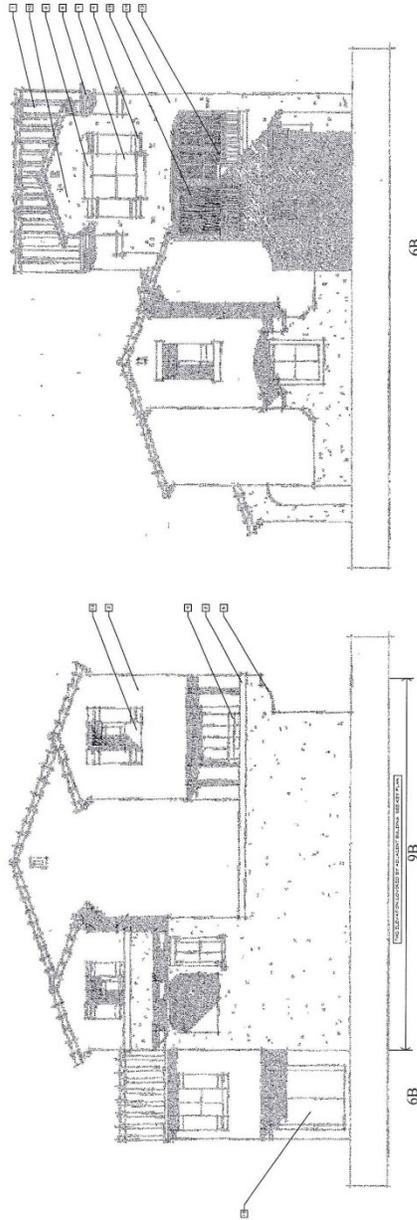
NOTE: UNIT 6 IS CURRENTLY OCCUPIED BY THE BANK SUBJECT TO THE BANK'S TERMS.  
1. THE EXTERIOR SURFACE IS TO BE PAINTED.  
2. THE EXTERIOR SURFACE IS TO BE PAINTED.  
3. THE EXTERIOR SURFACE IS TO BE PAINTED.

PROVENCE - COURTS  
RC NATOMAS, LLC



1.01

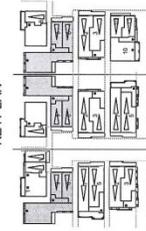
TYPICAL BUILDING MATERIALS	
1	INTERNAL COLOR OF FLOORING
2	INTERNAL COLOR OF PLASTER
3	PAINT
4	PAINT BOARD
5	PLASTER CORNELL
6	PLASTER TRIM
7	VINYL WINDOW
8	PLASTER TRIM
9	DECORATIVE SWITCHEL
10	PLASTER TRIM
11	PLASTER TRIM
12	STORE PATTERNS VENER
13	INTERNAL WALL / GROUNDWALL
14	INTERNAL WALL / GROUNDWALL
15	INTERNAL WALL / GROUNDWALL
16	INTERNAL WALL / GROUNDWALL
17	INTERNAL WALL / GROUNDWALL
18	INTERNAL WALL / GROUNDWALL
19	INTERNAL WALL / GROUNDWALL
20	INTERNAL WALL / GROUNDWALL
21	INTERNAL WALL / GROUNDWALL
22	INTERNAL WALL / GROUNDWALL
23	INTERNAL WALL / GROUNDWALL
24	INTERNAL WALL / GROUNDWALL
25	INTERNAL WALL / GROUNDWALL
26	INTERNAL WALL / GROUNDWALL
27	INTERNAL WALL / GROUNDWALL
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92	INTERNAL WALL / GROUNDWALL
93	INTERNAL WALL / GROUNDWALL
94	INTERNAL WALL / GROUNDWALL
95	INTERNAL WALL / GROUNDWALL
96	INTERNAL WALL / GROUNDWALL
97	INTERNAL WALL / GROUNDWALL
98	INTERNAL WALL / GROUNDWALL
99	INTERNAL WALL / GROUNDWALL
100	INTERNAL WALL / GROUNDWALL



# B

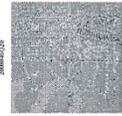
SEE 'A' ELEVATIONS FOR STANDARD SIDES

UNIT 6 & 9  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'



NOTE: REFER TO SECTION 05050 FOR THE FULL RANGE OF FINISHES AVAILABLE FOR THIS BUILDING.  
1. SEE THE FINISH SCHEDULE FOR ALL FINISHES AND MATERIALS.  
2. FINISHES ARE SUBJECT TO AVAILABILITY AND CHANGE WITHOUT NOTICE.  
3. FINISHES ARE SUBJECT TO THE FINISHES LISTED IN THE FINISH SCHEDULE.

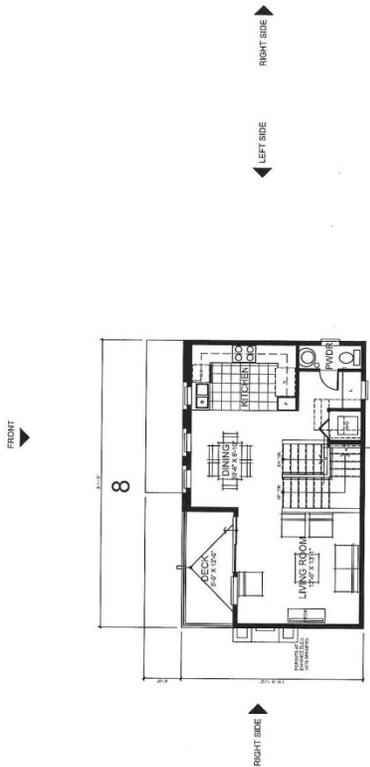
PROVENCE - COURTS  
RC NATOMAS, LLC



1.1.1

TYPICAL BUILDING MATERIALS	
1	INTERIOR COLOR 'B' TILE ROOF
2	INTERIOR COLOR PLASTER
3	PAINT
4	PAVE BOARD
5	PLASTER CORNICE
6	PLASTER FINISH
7	VINYL WINDOW
8	PLASTER TRIM
9	DECORATIVE SWITCHER
10	PLASTER FINISH
11	PLASTER FINISH
12	STORE PATTERNS VENEER
13	METAL HALL / CORRIDOR
14	FRANCE PATTERNS VENEER
15	FRENCH DOOR
16	METAL SECTIONAL SHAMISE DOOR WITH WINDOWS
17	HEAVY TEXTURE STUCCO
18	EXPANSION JOINT
19	PLASTER BATTERED FINISH
20	* ALL METAL AND WOOD ELEMENTS TO BE PAINTED





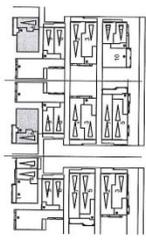
SECOND FLOOR

FIRST FLOOR

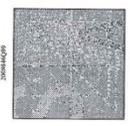
UNIT 8  
FIRST / SECOND FLOOR PLANS  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC

KEY PLAN



NOTES:  
1. THESE PLANS SHALL BE USED IN CONJUNCTION WITH THE ARCHITECTURAL ELEVATION STYLE 'A' AND THE FINISH SCHEDULE.  
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION.  
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION.  
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION.



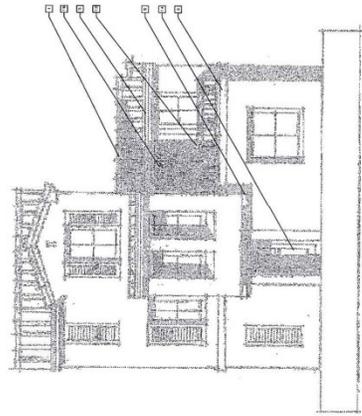
AREA ANALYSIS	
FLOOR AREA	267.41 S.F.
CEILING AREA	267.41 S.F.
WALL AREA	304.00 S.F.
DOOR AREA	100.00 S.F.
WINDOW AREA	100.00 S.F.
TOTAL FLOOR AREA	1038.82 S.F.
TOTAL WALL AREA	1038.82 S.F.
TOTAL WINDOW AREA	1038.82 S.F.
TOTAL DOOR AREA	1038.82 S.F.
TOTAL AREA	1038.82 S.F.





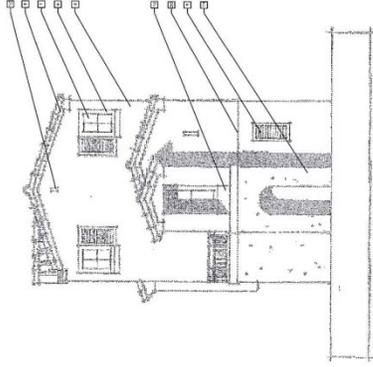






8A

FRONT ELEVATION II



ENHANCED RIGHT SIDE ELEVATION I

A

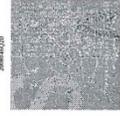
UNIT 8  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'



KEY PLAN

NOTE: THE KEY PLAN IS FOR INFORMATION ONLY AND IS NOT TO BE USED FOR CONSTRUCTION. THE ARCHITECT IS NOT RESPONSIBLE FOR THE ACCURACY OF THE KEY PLAN. THE KEY PLAN IS FOR INFORMATION ONLY AND IS NOT TO BE USED FOR CONSTRUCTION. THE ARCHITECT IS NOT RESPONSIBLE FOR THE ACCURACY OF THE KEY PLAN.

PROVENCE - COURTS  
RC NATOMAS, LLC



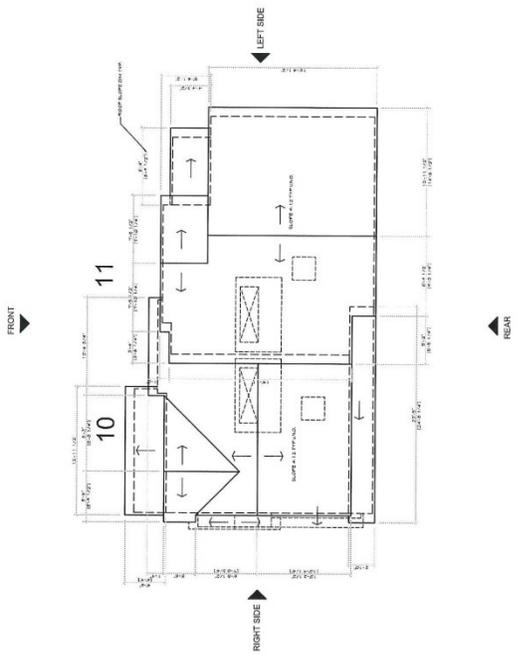
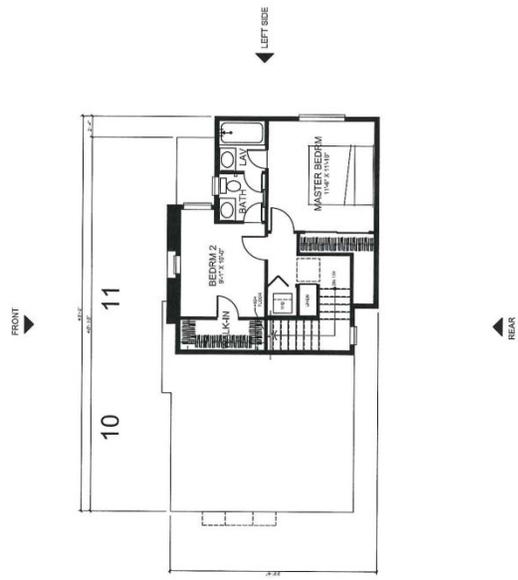
1.11

TYPICAL BUILDING MATERIALS	
1	INTERIOR WALL COLOUR: TYLE ROOF
2	INTERIOR WALL COLOUR: GYPSUM BOARD
3	PAVING
4	BASEBOARD
5	PLASTER CORNICE
6	WALL FINISH: GYPSUM BOARD
7	WALL FINISH: PLASTER
8	WALL FINISH: PLASTER
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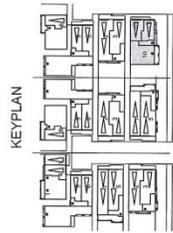




UNIT 10 & 11  
 THIRD FLOOR / ROOF PLAN  
 ELEVATION STYLE - 'A'

PROVENCE - COURTS  
 RC NATOMAS, LLC

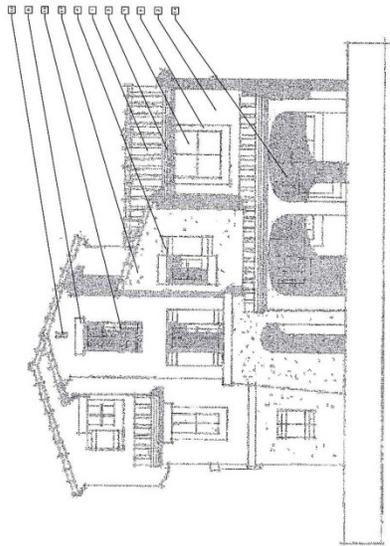
AREA ANALYSIS	
AREA 10	50.00 S.F.
AREA 11	50.00 S.F.
TOTAL FLOOR AREA	100.00 S.F.
TOTAL ROOF AREA	100.00 S.F.
TOTAL AREA	200.00 S.F.
TOTAL PERIMETER	180.00 S.F.
TOTAL VOLUME	100.00 S.F.
TOTAL FLOOR AREA	100.00 S.F.
TOTAL ROOF AREA	100.00 S.F.
TOTAL AREA	200.00 S.F.
TOTAL PERIMETER	180.00 S.F.
TOTAL VOLUME	100.00 S.F.



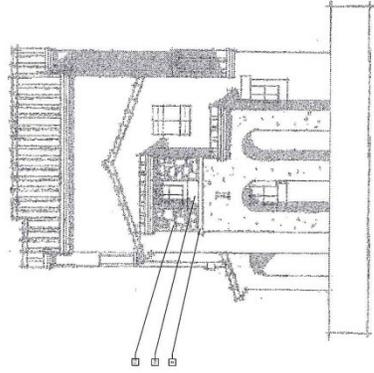
NOTES: 1. THIS PLAN IS A PRELIMINARY DESIGN AND SHOULD NOT BE USED FOR CONSTRUCTION WITHOUT THE ARCHITECT'S APPROVAL. 2. ALL DIMENSIONS ARE IN FEET AND INCHES. 3. THE ARCHITECT IS NOT RESPONSIBLE FOR THE ACCURACY OF THE DATA PROVIDED BY THE CLIENT.







11B 10B  
FRONT ELEVATION I

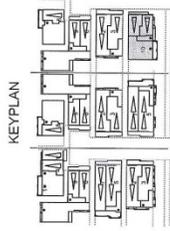


10B  
RIGHT SIDE ELEVATION I

# B

UNIT 10 & 11  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'

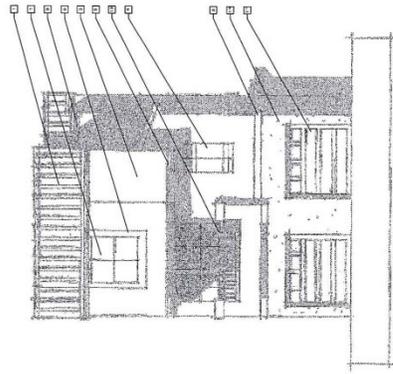
PROVENCE - COURTS  
RC NATOMAS, LLC



KEY PLAN

NOT TO SCALE  
1. THIS DRAWING IS A PROFESSIONAL ARCHITECTURAL DRAWING AND IS NOT TO BE USED FOR ANY OTHER PURPOSE.  
2. THIS DRAWING IS THE PROPERTY OF THE ARCHITECT AND IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF THE ARCHITECT.  
3. THE ARCHITECT ASSUMES NO LIABILITY FOR ANY DAMAGE TO PERSONS OR PROPERTY ARISING FROM THE USE OF THIS DRAWING.

TYPICAL BUILDING MATERIALS	
1	INTERIOR COLOR BY TELE ROOM
2	PAINT
3	WOOD PANELING
4	BASE BOARD
5	WOOD TRIM
6	PLASTER
7	PLASTER TRENCH
8	PLASTER TRENCH
9	PLASTER TRENCH
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10B 11B  
LEFT SIDE ELEVATION

# B

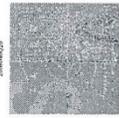
SEE 'A' ELEVATION FOR REAR

UNIT 10 & 11  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'



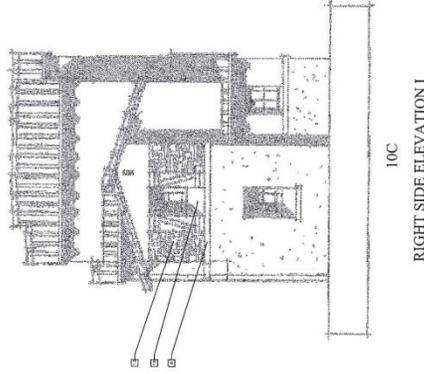
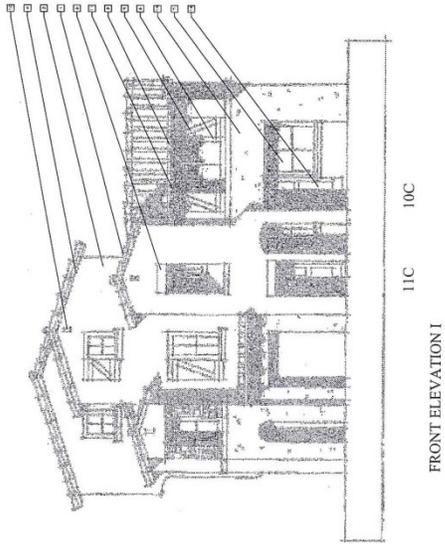
1. THIS UNIT IS TO BE CONSTRUCTED AS PER THE ARCHITECT'S DESIGN.  
2. THIS UNIT IS TO BE CONSTRUCTED AS PER THE ARCHITECT'S DESIGN.  
3. THIS UNIT IS TO BE CONSTRUCTED AS PER THE ARCHITECT'S DESIGN.

## PROVENCE - COURTS RC NATOMAS, LLC



1.11

TYPICAL BUILDING MATERIALS	
1	CONCRETE, COLORED & TEXTURED
2	INTERIOR COLORED PAINT
3	PAVING
4	BASE BOARD
5	WOOD TRIM
6	PLASTER AND/OR TRIM
7	VINYL WINDOW
8	PLASTER TRIM
9	WOOD TRIM
10	WOOD TRIM PLASTER PROJECTION
11	WOOD TRIM
12	STONE PATTERNS VESSEL
13	WOOD TRIM
14	WOOD TRIM PLASTER PROJECTION
15	WOOD TRIM
16	WOOD TRIM PLASTER PROJECTION
17	WOOD TRIM
18	WOOD TRIM PLASTER PROJECTION
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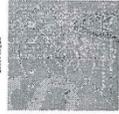
C

UNIT 10 & 11  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'



NOTE: THIS KEY PLAN IS FOR INFORMATION ONLY AND DOES NOT REPRESENT THE FINAL DESIGN.  
 1. THIS KEY PLAN IS FOR INFORMATION ONLY AND DOES NOT REPRESENT THE FINAL DESIGN.  
 2. THIS KEY PLAN IS FOR INFORMATION ONLY AND DOES NOT REPRESENT THE FINAL DESIGN.  
 3. THIS KEY PLAN IS FOR INFORMATION ONLY AND DOES NOT REPRESENT THE FINAL DESIGN.

PROVENCE - COURTS  
RC NATOMAS, LLC



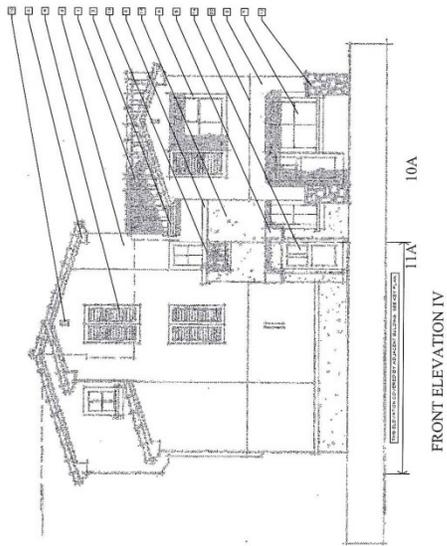
1111

**TYPICAL BUILDING MATERIALS**

- INTERNAL WALLS - GYP BOARD
- INTERNAL COLOR PLASTER
- PASCA
- GYP BOARD
- GYP BOARD
- PLASTER WINDOW TRIM
- PVC WINDOW
- PLASTER TRIM
- DISCREETIVE PLASTER PROTECTION
- RAFTER TAIL
- GYPSUM PATTERNS/STAIR
- METAL LATH
- METAL LATH ENTRY ZONE
- BUILDING GLASS DOOR
- FINISH DOOR
- FINISH DOOR
- HEAVY TYPE STUCCO
- PLASTER PATTERNS
- PLASTER PATTERNS

\* ALL METALS AND ROOF ELEMENTS TO BE PAINTED

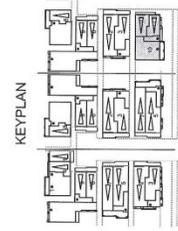




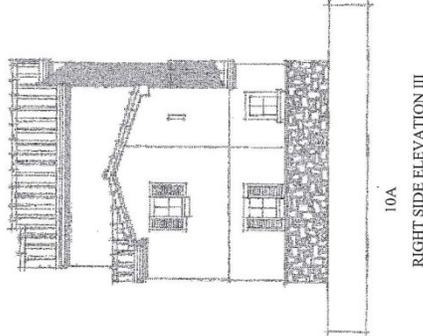
**A**

UNIT 10 & 11  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC



WITH THESE AND OTHER INFORMATION, THE ARCHITECT HAS PREPARED THESE ELEVATIONS.  
THESE ELEVATIONS REPRESENT THE ARCHITECT'S INTERPRETATION OF THE INFORMATION PROVIDED.  
THE ARCHITECT HAS CONDUCTED VISUAL CHECKS OF THE ELEVATIONS FOR ACCURACY.  
THE ARCHITECT HAS CONDUCTED VISUAL CHECKS OF THE ELEVATIONS FOR ACCURACY.



**TYPICAL BUILDING MATERIALS**

- INTERNAL COLOR TILE BACKUP
- INTERNAL COLOR PLASTER
- FASCIA
- RAFTER BOARD
- RAFTER BOARD
- PLASTER WINDOW TRIM
- PLASTER WINDOW
- PLASTER TRIM
- PLASTER
- DECORATIVE PLASTER PROJECTION
- RAFTER TAIL
- STORE FRONTAL VENEER
- INTERIOR GLASS ENTRY DOOR
- INTERIOR GLASS DOOR
- SLIPING PLASTER SILL
- HEAVY TEXTURE STUCCO
- PLASTER
- PLASTER LATTICED PALL

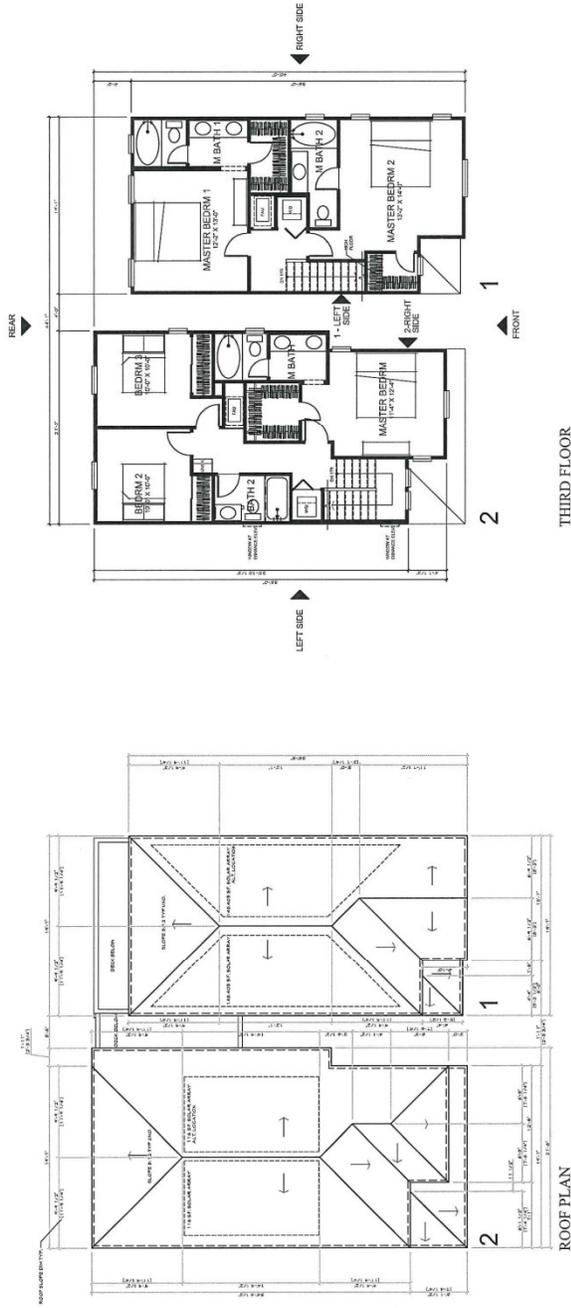
\* ALL MP'S AND ROOF ELEMENTS TO BE PAINTED









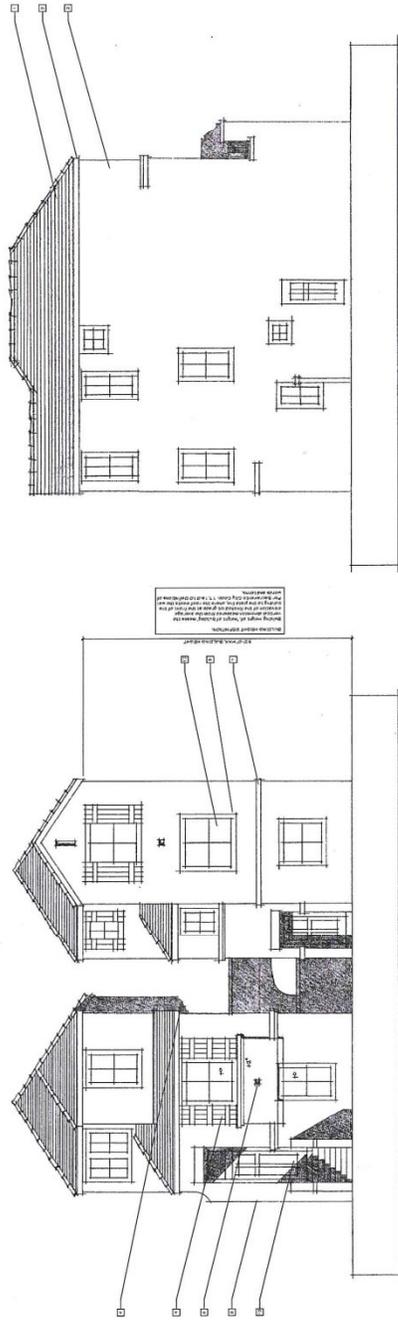


UNIT 1 & 2  
 THIRD FLOOR / ROOF PLAN  
 ELEVATION STYLE - 'A'

PROVENCE - URBAN VILLAS  
 RC NATOMAS, LLC



AREA ANALYSIS	
UNIT 1	
FIRST FLOOR	304.95 S.F.
THIRD FLOOR	682.88 S.F.
STAIRS	10.00 S.F.
COMMON AREA	309.37 S.F.
STAIRS	10.00 S.F.
UNIT AREA	1,317.20 S.F.
UNIT 2	
FIRST FLOOR	304.38 S.F.
THIRD FLOOR	721.25 S.F.
STAIRS	10.00 S.F.
COMMON AREA	309.37 S.F.
STAIRS	10.00 S.F.
UNIT AREA	1,354.99 S.F.
UNIT 3	
FIRST FLOOR	411.41 S.F.
THIRD FLOOR	311.70 S.F.
STAIRS	10.00 S.F.
COMMON AREA	428.27 S.F.
STAIRS	10.00 S.F.
UNIT AREA	1,171.38 S.F.



2A FRONT ELEVATION

1A RIGHT SIDE ELEVATION

A

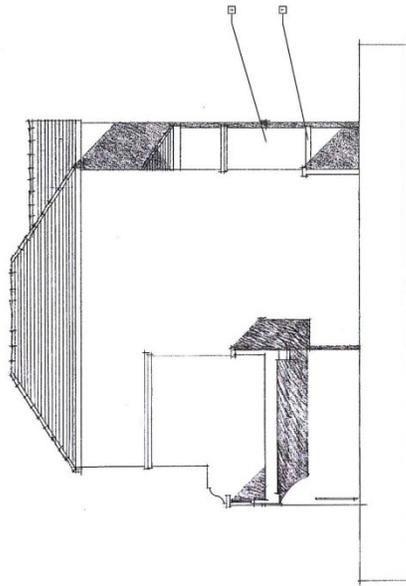
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 1A & 2A

PROVENCE - URBAN VILLAS  
RC NATOMAS, LLC

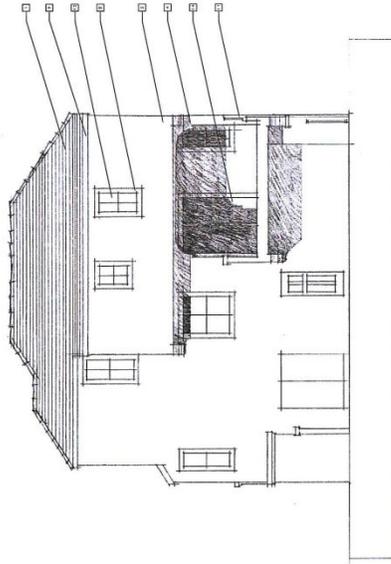


TYPICAL BUILDING MATERIALS	
INTERNAL COLOR PLASTER ROOF	
INTERNAL COLOR PLASTER	
2" FASCIA	
PLASTER CORBEL	
PLASTER TRIM	
SEGMENTED PLASTER PROJECTION	
PLASTER TRIM	
PLASTER BATTERED PILL	
SEGMENTED PLASTER	
METAL PAIL / GUMERAL	
VINYL SIDING	
SEGMENTED PLASTER FLOOR	
BURNING GLASS DOOR	
METAL SECTIONAL SHAMANE DOOR	
* ALL METAL AND FLOOR ELEMENTS TO BE PAINTED	





1A LEFT SIDE ELEVATION

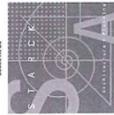


2A RIGHT SIDE ELEVATION

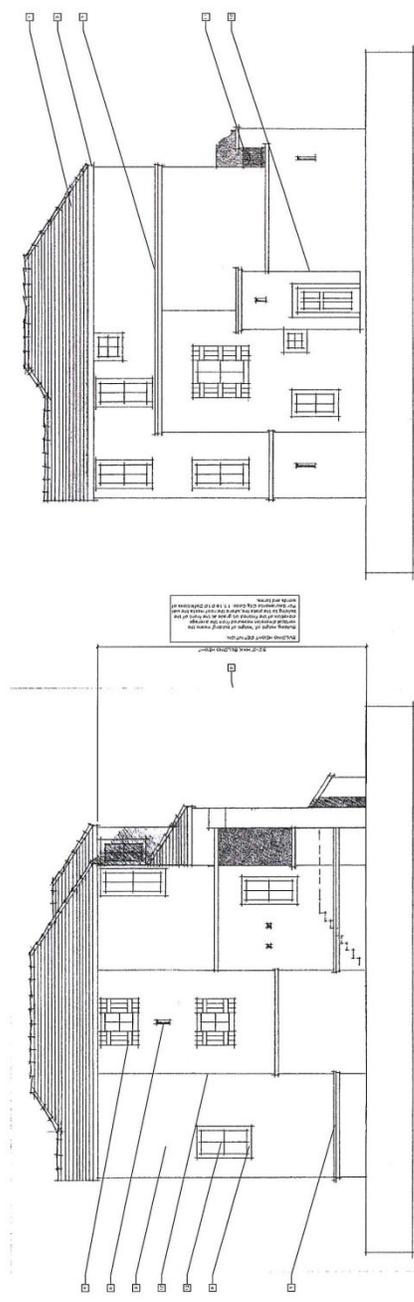
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EXTERIOR ELEVATIONS  
ELEVATION STYLE - 1A & 2A

PROVENCE - URBAN VILLAS  
RC NATOMAS, LLC



TYPICAL BUILDING MATERIALS	
□	INTERNAL COLOR PLASTER ROOF
□	INTERNAL COLOR PLASTER
□	2" FASCIA
□	PLASTER CORNER
□	PLASTER TRIM
□	RESISTANT PLASTER PROJECTION
□	PLASTER TRIM
□	PLASTER BATTERED WALL
□	PLASTER BATTERED CORNER
□	EXPANSION JOINT
□	METAL PANEL / SCHEDULE
□	W/VT. FINISH
□	W/VT. FINISH DOOR
□	W/VT. FINISH DOOR
□	METAL SECTIONAL SHAMAKE DOOR
□	METAL AND WOOD ELEMENTS TO BE PAINTED



2A  
2A ENHANCED LEFT SIDE ELEVATION

1A ENHANCED RIGHT SIDE ELEVATION

# A

EXTERIOR ELEVATIONS  
ELEVATION STYLE - '1A & 2A'

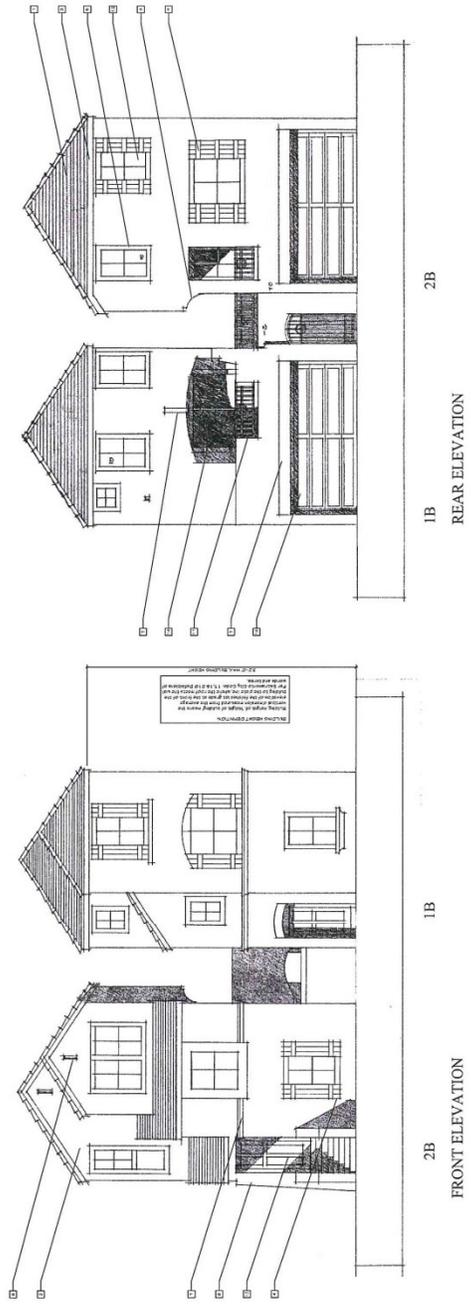
## PROVENCE - URBAN VILLAS RC NATOMAS, LLC



TYPICAL BUILDING MATERIALS

INTERNAL COLOR PLASTER ROOF
INTERNAL COLOR PLASTER
2" FASCIA
PLASTER CORNICE
PLASTER TRIM
EXPOSITIVE PLASTER PROJECTION
PLASTER TRIM
PLASTER BATTERED FINISH
EXPANSION JOINT
EXPANSION JOINT
METAL RAIL / HANDRAIL
VINYL FINISH
METAL RAIL / HANDRAIL
SLIDING GLASS DOOR
METAL SECTIONAL SHAKES DOOR

\* ALL METAL AND WOOD SURFACES TO BE PAINTED



# B

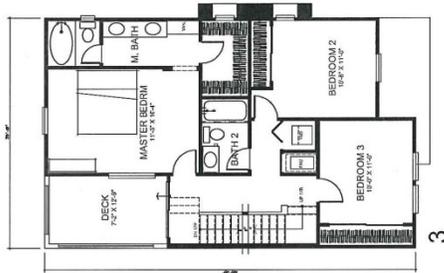
SEE 'A' ELEVATIONS FOR SIDES  
 EXTERIOR ELEVATIONS  
 ELEVATION STYLE - '1B & 2B'

PROJECT OF  
**PROVENCE - URBAN VILLAS**  
 RC NATOMAS, LLC  
 20060970

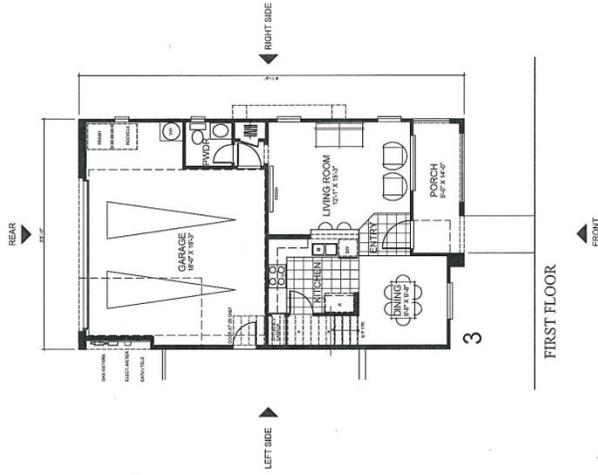


TYPICAL BUILDING MATERIALS	
☐	INTERNAL COLOR PLASTER ROOF
☐	INTERNAL COLOR PLASTER
☐	2" FASCIA
☐	WOOD SHAKES
☐	PLASTER FINISH TRIM
☐	DECORATIVE PLASTER PROJECTION
☐	PLASTER TRIM
☐	PLASTER BATTERED PILL
☐	WOOD SHAKES
☐	EXPANSION JOINT
☐	METAL RAIL / HANDRAIL
☐	WOOD SHAKES
☐	METAL AND BRASS DOOR
☐	BLINDING GLASS DOOR
☐	METAL SECTIONAL SHAMANE DOOR
* ALL METAL AND BRASS ELEMENTS TO BE PAINTED	





SECOND FLOOR



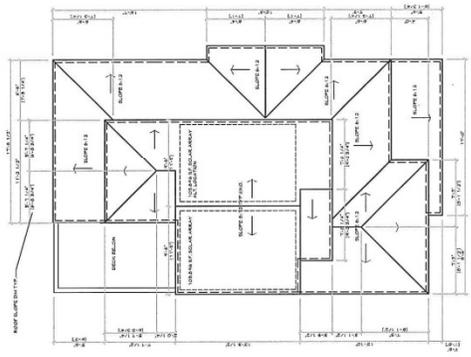
FIRST FLOOR

UNIT 3  
FIRST / SECOND FLOOR PLANS  
ELEVATION STYLE - 'A'

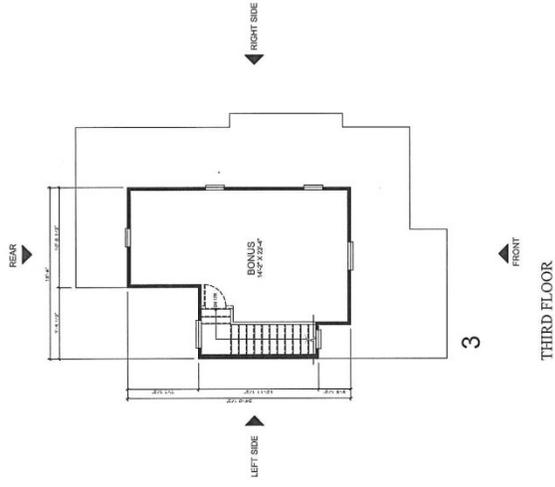
PROVENCE - URBAN VILLAS  
RC NATOMAS, LLC



AREA ANALYSIS	
FIRST FLOOR	304.58 S.F.
SECOND FLOOR	304.58 S.F.
TOTAL FLOOR AREA	609.16 S.F.
DECK	78.00 S.F.
PORCH	78.00 S.F.
TOTAL DECK AND PORCH AREA	156.00 S.F.
FIRST FLOOR	304.58 S.F.
SECOND FLOOR	304.58 S.F.
TOTAL FLOOR AREA	609.16 S.F.
DECK	78.00 S.F.
PORCH	78.00 S.F.
TOTAL DECK AND PORCH AREA	156.00 S.F.
FIRST FLOOR	414.18 S.F.
SECOND FLOOR	304.58 S.F.
TOTAL FLOOR AREA	718.76 S.F.
DECK	78.00 S.F.
PORCH	78.00 S.F.
TOTAL DECK AND PORCH AREA	156.00 S.F.



ROOF PLAN



3

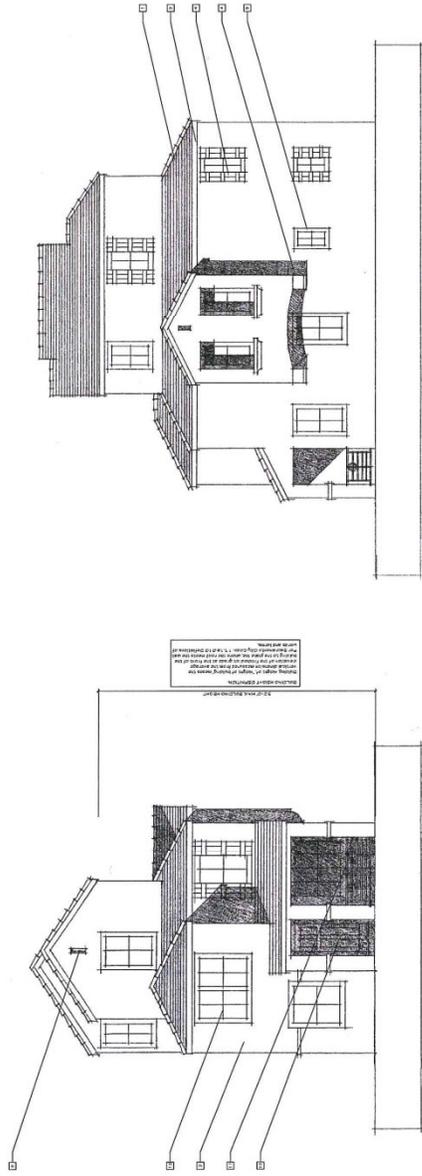
THIRD FLOOR

UNIT 3  
THIRD FLOOR / ROOF PLAN  
ELEVATION STYLE - A

PROVENCE - URBAN VILLAS  
RC NATOMAS, LLC



AREA ANALYSIS	
UNIT 1	AREA
FIRST FLOOR	10,114.87 SF
SECOND FLOOR	10,114.87 SF
THIRD FLOOR	10,114.87 SF
TOTAL FLOOR AREA	30,344.61 SF
STAIRS	39,978.34 SF
ROOF AREA	2,202.73 SF
UNIT 2	AREA
FIRST FLOOR	10,114.87 SF
SECOND FLOOR	10,114.87 SF
THIRD FLOOR	10,114.87 SF
TOTAL FLOOR AREA	30,344.61 SF
STAIRS	39,978.34 SF
ROOF AREA	2,202.73 SF
UNIT 3	AREA
FIRST FLOOR	10,114.87 SF
SECOND FLOOR	10,114.87 SF
THIRD FLOOR	10,114.87 SF
TOTAL FLOOR AREA	30,344.61 SF
STAIRS	39,978.34 SF
ROOF AREA	2,202.73 SF



FRONT ELEVATION

ENHANCED RIGHT SIDE ELEVATION

NOTES:  
 1. REFER TO SHEET 3-A FOR MATERIAL SCHEDULES.  
 2. REFER TO SHEET 3-B FOR FINISH SCHEDULES.  
 3. REFER TO SHEET 3-C FOR WINDOW SCHEDULES.  
 4. REFER TO SHEET 3-D FOR DOOR SCHEDULES.  
 5. REFER TO SHEET 3-E FOR ROOFING SCHEDULES.

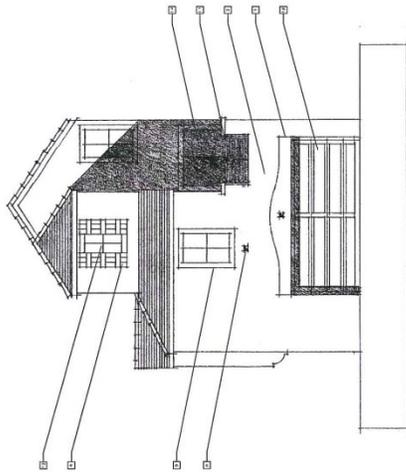
# A

EXTERIOR ELEVATIONS  
 ELEVATION STYLE - 3A

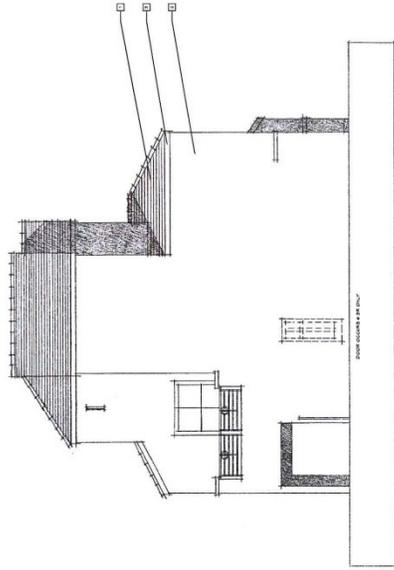
## PROVENCE - URBAN VILLAS RC NATOMAS, LLC



TYPICAL BUILDING MATERIALS	
1	INTERNAL GORE PLASTER ROOF
2	INTERNAL GORE PLASTER
3	PA FASCIA
4	PLASTER CORNICE
5	PLASTER TRIM
6	DECORATIVE PLASTER PROJECTION
7	PLASTER TRIM
8	PLASTER BATTERED PILL
9	EXPANSION JOINT
10	EXPANSION JOINT
11	METAL RAIL / BARRIER
12	METAL RAIL / BARRIER
13	METAL RAIL / BARRIER
14	METAL RAIL / BARRIER
15	METAL RAIL / BARRIER
16	METAL RAIL / BARRIER
17	METAL RAIL / BARRIER
18	METAL RAIL / BARRIER
19	METAL RAIL / BARRIER
20	METAL RAIL / BARRIER
21	METAL RAIL / BARRIER
22	METAL RAIL / BARRIER
23	METAL RAIL / BARRIER
24	METAL RAIL / BARRIER
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26	METAL RAIL / BARRIER
27	METAL RAIL / BARRIER
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29	METAL RAIL / BARRIER
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31	METAL RAIL / BARRIER
32	METAL RAIL / BARRIER
33	METAL RAIL / BARRIER
34	METAL RAIL / BARRIER
35	METAL RAIL / BARRIER
36	METAL RAIL / BARRIER
37	METAL RAIL / BARRIER
38	METAL RAIL / BARRIER
39	METAL RAIL / BARRIER
40	METAL RAIL / BARRIER
41	METAL RAIL / BARRIER
42	METAL RAIL / BARRIER
43	METAL RAIL / BARRIER
44	METAL RAIL / BARRIER
45	METAL RAIL / BARRIER
46	METAL RAIL / BARRIER
47	METAL RAIL / BARRIER
48	METAL RAIL / BARRIER
49	METAL RAIL / BARRIER
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51	METAL RAIL / BARRIER
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88	METAL RAIL / BARRIER
89	METAL RAIL / BARRIER
90	METAL RAIL / BARRIER
91	METAL RAIL / BARRIER
92	METAL RAIL / BARRIER
93	METAL RAIL / BARRIER
94	METAL RAIL / BARRIER
95	METAL RAIL / BARRIER
96	METAL RAIL / BARRIER
97	METAL RAIL / BARRIER
98	METAL RAIL / BARRIER
99	METAL RAIL / BARRIER
100	METAL RAIL / BARRIER



REAR ELEVATION



LEFT SIDE ELEVATION

A

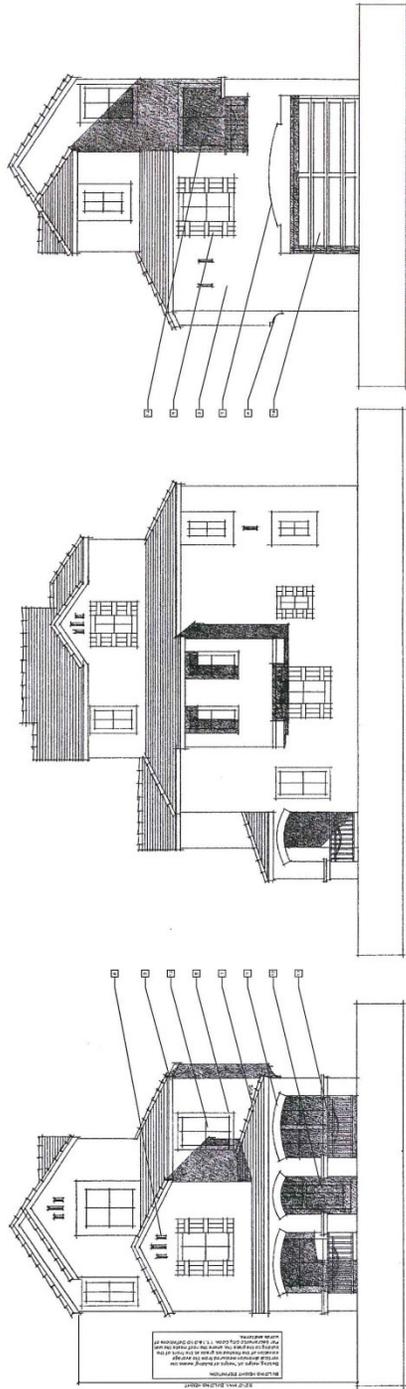
EXTERIOR ELEVATIONS  
ELEVATION STYLE - '3A'

PROVENCE - URBAN VILLAS  
RC NATOMAS, LLC



TYPICAL BUILDING MATERIALS	
1	INTERNAL GABLES, GUTTER ROOF
2	INTERNAL GABLE PLASTER
3	2" FASCIA
4	PLASTER CORNICE
5	EXTERIOR PLASTER
6	EXTERIOR PLASTER PROJECTION
7	PLASTER TRIM
8	PLASTER BATTERED FINN
9	EXTERIOR BRICK
10	METAL PAUL / SANDRAL
11	VINYL SIDING
12	EXTERIOR STAIR FLOOR
13	EXTERIOR STAIR DOOR
14	METAL SECTIONAL SHAMANE DOOR

\* ALL METAL AND FLOOR ELEMENTS TO BE PAINTED



FRONT ELEVATION

ENHANCED RIGHT SIDE ELEVATION

REAR ELEVATION

# B

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 3B'

PROVENCE - URBAN VILLAS  
RC NATOMAS, LLC



TYPICAL BUILDING MATERIALS	
1	INTERIOR COLORED FLAT TILE ROOF
2	WOODEN PLASTER
3	2X4 FRAMA
4	PLASTER GORDEL
5	PLASTER WINDOW TRIM
6	PLASTER WINDOW PROTECTION
7	PLASTER TRIM
8	PLASTER BATTERED PILL
9	DECORATIVE SAWTOOTH
10	METAL SKYLIGHT / WINDOW
11	WHTC WINDOW
12	FIBERGLASS ENTRY DOOR
13	METAL SECTIONAL SHAMANE DOOR
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

Exhibit 6J: Building Data Charts

PROVENCE - 5088676.00

**Structure Square Footage**

**Existing Structure Square Footage**

PRODUCT	TOTAL LIVING AREA	GARAGE	TOTAL AREA
Unit 1B	1279 SF	429 SF	1708 SF
Unit 2A	1351 SF	429 SF	1780 SF
Unit 3A	1265 SF	429 SF	1694 SF
Unit 2RB	1351 SF	429 SF	1780 SF
Unit 1RC	1279 SF	429 SF	1708 SF

Total Existing Structure Square Footage: **4,730 SF** NOT A PART (P06-194)

**Proposed Structure Square Footage**

PRODUCT	TOTAL LIVING AREA	GARAGE	DECK/ PORCH	GROSS AREA PER BLDG.	BLDGS. PER PLAN	TOTAL BLDG. AREA
<b>Courts</b>						
Plan 1/4	1623 SF	402 SF	119 SF	2143 SF	16	34290 SF
Plan 2/3	1806 SF	803 SF	114 SF	2724 SF	14	38133 SF
Plan 5/7	2154 SF	861 SF	118 SF	3133 SF	31	97121 SF
Plan 6/9	2477 SF	473 SF	137 SF	3087 SF	11	33957 SF
Plan 8	1242 SF	215 SF	145 SF	1602 SF	7	11217 SF
Plan 10/11	1793 SF	425 SF	140 SF	2359 SF	10	23585 SF
<b>Urban Villas</b>						
Unit 1	1634 SF	380 SF	136 SF	2150 SF	7	15050 SF
Unit 1&2	3315 SF	761 SF	313 SF	4389 SF	17	74613 SF
Unit 3	1702 SF	437 SF	174 SF	2313 SF	21	48573 SF

Total Proposed Structure Square Footage: **376538 SF**

**Lot Coverage**

PRODUCT	COVERAGE PER BUILDING	BLDGS. PER PLAN	TOTAL BLDG. COVERAGE
<b>Courts</b>			
Plan 1/4	893 SF	16	14288 SF
Plan 2/3	1024 SF	14	14336 SF
Plan 5/7	1178 SF	31	36518 SF
Plan 6/9	1205 SF	11	13255 SF
Plan 8	743 SF	7	5201 SF
Plan 10/11	1057 SF	10	10570 SF
<b>Urban Villas</b>			
Unit 1	760 SF	7	5320 SF
Unit (1&2)	1540 SF	17	26180 SF
Unit 3	1009 SF	21	21190 SF

Total Building Coverage Area: **146858 SF**  
 Total Residential/Landscape Lot Area: **348,480 SF (8.0AC)**  
 Total Lot Coverage Percentage: **42%**

ATTACHMENT A

## Building Heights & Materials

### Proposed Building Heights

PRODUCT	MAX. BUILDING HEIGHT	NUMBER OF FLOORS
Courts - Plan 1/4	33' - 0" max	3
Courts - Plan 2/3	33' - 0" max	3
Courts - Plan 5/7	35' - 0" max	3
Courts - Plan 6/9	34' - 0" max	3
Courts - Plan 8	33' - 0" max	3
Courts - Plan 10/11	34' - 0" max	3
Urban Villas	32' - 0" max	3

### Existing Building Heights

PRODUCT	MAX. BUILDING HEIGHT	NUMBER OF FLOORS
Unit 1B	19' - 3.5"	2
Unit 2A	19' - 3.5"	2
Unit 3A	19' - 3.5"	2
Unit 2RB	19' - 3.5"	2
Unit 1RC	19' - 3.5"	2

### Proposed Building Materials

Exterior Walls & Soffits, Trim: Integral color stucco, 2x fascia, plaster corbel, & plaster window trim rake board, rafter tail, stone pattern veneer,  
 Roofing: Composit MAXitile  
 Misc: metal rail/ guardrail, vinyl windows, decorative shutter, sliding glass door, fiberglass entry door, metal sectional garage door, metal sectional garage door with windows, and french door

### Existing Building Materials

Exterior Walls & Soffits, Trim: Stucco, Stucco over foam trim  
 Roofing: Class A Fire Retardent flat concrete tiles  
 Shutters: decorative Foam  
 Misc: metal exterior balcony guardrail  
 plywood mounted decorative clay pipe  
 decorative 3/4" round wrought iron



Bldg No	Bldg type	Areas	Br/Ba	Garage	Height	Elev Style	Color Scheme	Front	Right	Rear	Left
64	5-7	5: 1021 sf 7: 1133 sf	5: 2Br/2.5 Ba 7: 2Br/2.5 Ba	5: 2 car tandem 7: 2 car tandem	5: 34' 7: 34'	B	2		I		III
65	2-3	3: 903 sf	2: 2Br/2.5 Ba	3: 2 car tandem	4: 34'	B	2		I		III
66	1-4	1: 649 sf 4: 974 sf	1: 1 Br/1 Ba 4: 2 Br/1.5 Ba	1: 1 car 4: 1 car	1: 24' 4: 34'	C	1	I	II		
67	1-4	1: 649 sf 4: 974 sf	1: 1 Br/1 Ba 4: 2 Br/1.5 Ba	1: 1 car 4: 1 car	1: 24' 4: 34'	A	3	I			II
68	6-9	6: 1040 sf 9: 1437 sf	6: 2 Br/2.5 Ba 9: 3 Br/2.5 Ba	6: 1 car 9: 1 car	6: 24' 9: 34'	B	4				
69	2-3	2: 903 sf 3: 903 sf	2: 2Br/2.5 Ba 3: 2Br/2.5 Ba	2: 1 car 3: 1 car	2: 34' 3: 34'	C	1				
70	2-3	2: 903 sf 3: 903 sf	2: 2Br/2.5 Ba 3: 2Br/2.5 Ba	2: 1 car 3: 1 car	2: 34' 3: 34'	A	2				III
89	5-7	5: 1021 sf 7: 1133 sf	5: 2Br/2.5 Ba 7: 2Br/2.5 Ba	5: 2 car tandem 7: 2 car tandem	5: 34' 7: 34'	A	3				I
90	5-7	5: 1021 sf 7: 1133 sf	5: 2Br/2.5 Ba 7: 2Br/2.5 Ba	5: 2 car tandem 7: 2 car tandem	5: 34' 7: 34'	B	2				I
91	10-11	10: 1066 sf 11: 1066 sf	10: 1 Br/1 Ba 11: 2 Br/1.5 Ba	10: 1 car 11: 1 car	10: 24' 11: 34'	C	1	IV			I
92	1-4	1: 649 sf 4: 974 sf	1: 1 Br/1 Ba 4: 2 Br/1.5 Ba	1: 1 car 4: 1 car	1: 24' 4: 34'	A	4	II			I
93	1-4	1: 649 sf 4: 974 sf	1: 1 Br/1 Ba 4: 2 Br/1.5 Ba	1: 1 car 4: 1 car	1: 24' 4: 34'	C	3	II			I
94	2-3	2: 903 sf 3: 903 sf	2: 2Br/2.5 Ba 3: 2Br/2.5 Ba	2: 1 car 3: 1 car	2: 34' 3: 34'	B	1				III
96	5-7	5: 1021 sf 7: 1133 sf	5: 2Br/2.5 Ba 7: 2Br/2.5 Ba	5: 2 car tandem 7: 2 car tandem	5: 34' 7: 34'	A	4				I
97	10-11	10: 1066 sf 11: 1066 sf	10: 1 Br/1 Ba 11: 2 Br/1.5 Ba	10: 1 car 11: 1 car	10: 24' 11: 34'	B	4	IV			III
98	8	8: 1242 sf	8: 2 Br/2.5 Ba	8: 1 car	8: 24' 8: 34'	A	2	II			I
99	8	8: 1242 sf	8: 2 Br/2.5 Ba	8: 1 car	8: 24' 8: 34'	C	3	II			I
100	10-11	10: 1066 sf 11: 1066 sf	10: 1 Br/1 Ba 11: 2 Br/1.5 Ba	10: 1 car 11: 1 car	10: 24' 11: 34'	A	2	IV			III
101	2-3	2: 903 sf 3: 903 sf	2: 2Br/2.5 Ba 3: 2Br/2.5 Ba	2: 1 car 3: 1 car	2: 34' 3: 34'	A	1				
102	5-7	5: 1021 sf 7: 1133 sf	5: 2Br/2.5 Ba 7: 2Br/2.5 Ba	5: 2 car tandem 7: 2 car tandem	5: 34' 7: 34'	C	4				II
103	5-7	5: 1021 sf 7: 1133 sf	5: 2Br/2.5 Ba 7: 2Br/2.5 Ba	5: 2 car tandem 7: 2 car tandem	5: 34' 7: 34'	B	2				I
104	2-3	2: 903 sf 3: 903 sf	2: 2Br/2.5 Ba 3: 2Br/2.5 Ba	2: 1 car 3: 1 car	2: 34' 3: 34'	A	3				
105	10-11	10: 1066 sf 11: 1066 sf	10: 1 Br/1 Ba 11: 2 Br/1.5 Ba	10: 1 car 11: 1 car	10: 24' 11: 34'	C	1	IV			III
106	8	8: 1242 sf	8: 2 Br/2.5 Ba	8: 1 car	8: 24' 8: 34'	A	4	II			I
107	8	8: 1242 sf	8: 2 Br/2.5 Ba	8: 1 car	8: 24' 8: 34'	C	1	II			I
108	10-11	10: 1066 sf 11: 1066 sf	10: 1 Br/1 Ba 11: 2 Br/1.5 Ba	10: 1 car 11: 1 car	10: 24' 11: 34'	A	3	IV			III
109	10-11	10: 1066 sf 11: 1066 sf	10: 1 Br/1 Ba 11: 2 Br/1.5 Ba	10: 1 car 11: 1 car	10: 24' 11: 34'	B	2	I			I
110	5-7	5: 1021 sf 7: 1133 sf	5: 2Br/2.5 Ba 7: 2Br/2.5 Ba	5: 2 car tandem 7: 2 car tandem	5: 34' 7: 34'	A	2				I
111	2-3	2: 903 sf 3: 903 sf	2: 2Br/2.5 Ba 3: 2Br/2.5 Ba	2: 1 car 3: 1 car	2: 34' 3: 34'	B	4				III
112	1-4	1: 649 sf 4: 974 sf	1: 1 Br/1 Ba 4: 2 Br/1.5 Ba	1: 1 car 4: 1 car	1: 24' 4: 34'	A	2	I			II
113	1-4	1: 649 sf 4: 974 sf	1: 1 Br/1 Ba 4: 2 Br/1.5 Ba	1: 1 car 4: 1 car	1: 24' 4: 34'	B	3	I			I
114	10-11	10: 1066 sf 11: 1066 sf	10: 1 Br/1 Ba 11: 2 Br/1.5 Ba	10: 1 car 11: 1 car	10: 24' 11: 34'	A	2	IV			I
115	5-7	5: 1021 sf 7: 1133 sf	5: 2Br/2.5 Ba 7: 2Br/2.5 Ba	5: 2 car tandem 7: 2 car tandem	5: 34' 7: 34'	C	1				
116	5-7	5: 1021 sf 7: 1133 sf	5: 2Br/2.5 Ba 7: 2Br/2.5 Ba	5: 2 car tandem 7: 2 car tandem	5: 34' 7: 34'	A	4				I
117	5-7	5: 1021 sf 7: 1133 sf	5: 2Br/2.5 Ba 7: 2Br/2.5 Ba	5: 2 car tandem 7: 2 car tandem	5: 34' 7: 34'	C	1				I
118	5-7	5: 1021 sf 7: 1133 sf	5: 2Br/2.5 Ba 7: 2Br/2.5 Ba	5: 2 car tandem 7: 2 car tandem	5: 34' 7: 34'	B	2				I
119	6-9	6: 1040 sf 9: 1437 sf	6: 2 Br/2.5 Ba 9: 3 Br/2.5 Ba	6: 1 car 9: 1 car	6: 24' 9: 34'	A	4				II
120	8	8: 1242 sf	8: 2 Br/2.5 Ba	8: 1 car	8: 24' 8: 34'	C	1	III			II
121	2-3	2: 903 sf 3: 903 sf	2: 2Br/2.5 Ba 3: 2Br/2.5 Ba	2: 1 car 3: 1 car	2: 34' 3: 34'	B	3				II
122	2-3	2: 903 sf 3: 903 sf	2: 2Br/2.5 Ba 3: 2Br/2.5 Ba	2: 1 car 3: 1 car	2: 34' 3: 34'	A	2				II
123	5-7	5: 1021 sf 7: 1133 sf	5: 2Br/2.5 Ba 7: 2Br/2.5 Ba	5: 2 car tandem 7: 2 car tandem	5: 34' 7: 34'	C	4				I
124	5-7	5: 1021 sf 7: 1133 sf	5: 2Br/2.5 Ba 7: 2Br/2.5 Ba	5: 2 car tandem 7: 2 car tandem	5: 34' 7: 34'	B	3				I
125	6-9	6: 1040 sf 9: 1437 sf	6: 2 Br/2.5 Ba 9: 3 Br/2.5 Ba	6: 1 car 9: 1 car	6: 24' 9: 34'	C	1				II
126	1-4	1: 649 sf 4: 974 sf	1: 1 Br/1 Ba 4: 2 Br/1.5 Ba	1: 1 car 4: 1 car	1: 24' 4: 34'	A	4	III			II
127	1-4	1: 649 sf 4: 974 sf	1: 1 Br/1 Ba 4: 2 Br/1.5 Ba	1: 1 car 4: 1 car	1: 24' 4: 34'	C	2	III			II

Bldg No	Bldg type	Areas	Br/Ba	Garage	Height	Elev Style	Color Scheme	Front	Right	Rear	Left
128	6-9	6: 1040 sf 7: 1133 sf	6: 2 Br/2.5 Ba 7: 2Br/2.5 Ba	9: 1 car 7: 2 car tandem	6: 24' 7: 34'	B	3		II		
129	5-7	5: 1021 sf	5: 2Br/2.5 Ba	5: 1 car tandem	7: 34'	A	4		II		III
130	6-9	6: 1040 sf	6: 2 Br/2.5 Ba	6: 1 car	6: 24' 9: 34'	A	1		II		II
131	1-4	1: 649 sf	1: 1 Br/1 Ba	1: 1 car	1: 24' 4: 34'	B	3	I			
132	1-4	1: 649 sf	1: 1 Br/1 Ba	1: 1 car	1: 24' 4: 34'	C	2	II			
133	2-3	2: 903 sf	2: 2Br/2.5 Ba	2: 1 car	2: 34' 3: 34'	B	4		II		
134	5-7	5: 1021 sf	5: 2Br/2.5 Ba	5: 2 car tandem	7: 34'	C	3		I		
135	5-7	5: 1021 sf	5: 2Br/2.5 Ba	5: 2 car tandem	5: 34' 7: 34'	A	1		I		
136	1-4	1: 649 sf	1: 1 Br/1 Ba	1: 1 car	1: 24' 4: 34'	B	2		I		
137	2-3	2: 903 sf	2: 2Br/2.5 Ba	2: 1 car	2: 34' 3: 34'	A	4		I		
138	5-7	5: 1021 sf	5: 2Br/2.5 Ba	5: 2 car tandem	5: 34' 7: 34'	C	3				
139	5-7	5: 1021 sf	5: 2Br/2.5 Ba	5: 2 car tandem	5: 34' 7: 34'	B	2		I		
<b>Provence-Urban Villas</b>											
2008946											
1/10/2011											
<b>Elevation Variation Plotting</b>											
Notes: Areas are approximate, see design drawing for accurate plan areas											
<b>Color Plotting</b>											
Notes: Please refer to the Color Lists for scheme specifications (Schemes 4-6)											
Bldg No	Bldg type	Areas	Br/Ba	Garage	Height	Elev Style	Color Scheme	Front	Right	Rear	Left
1	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	B	5				
2	1-2	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32' 2: 32'	C	bidg1:6/bldg 2:4				
3	1	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32'	B	5				
4	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	AR	6				
5	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	B	5				
6	1-2	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32' 2: 32'	A	bidg1:4/bldg 2:6				
7	1	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32'	B	5				
8	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	AR	5				
9	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	B	6				
10	1-2	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32' 2: 32'	C	bidg1:4/bldg 2:5				
11	1-2	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32' 2: 32'	A	bidg1:6/bldg 2:4				
12	1-2	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32' 2: 32'	C	bidg1:6/bldg 2:5				
13	1-2	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32' 2: 32'	B	bidg1:4/bldg 2:6				
14	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	AR	5				
15	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	B	4				
16	1-2	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32' 2: 32'	A	bidg1:5/bldg 2:6				
17	1-2	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32' 2: 32'	B	bidg1:4/bldg 2:5				
18	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	A	6				
19	1-2	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32' 2: 32'	C	bidg1:5/bldg 2:4				
20	1	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32'	B	6				

Bldg No	Bldg type	Areas	Br/Ba	Garage	Height	Elev Style	Color Scheme	Front	Right	Rear	Left
21	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	AR	5				
22	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	B	6				
23	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	A	4				
24	1	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32'	C	5				
25	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	BR	4				
39	1-2	1: 1634 sf	1: 3 Br/3.5 Ba	2: 2 car tandem	1: 32'	C	blde1:6/hlde 2:5				
40	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	AR	4				
71	1-2	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32'	B	blde1:6/hlde 2:5				
72	1-2	1: 1634 sf	1: 3 Br/3.5 Ba	2: 2 car tandem	1: 32'	C	blde1:4/hlde 2:6				
73	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	AR	5				
74	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	B	4				
75	1-2	1: 1634 sf	1: 3 Br/3.5 Ba	2: 2 car tandem	1: 32'	A	blde1:5/hlde 2:6				
76	1	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32'	C	4				
77	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	AR	5				
78	1-2	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32'	C	blde1:4/hlde 2:6				
79	1-2	1: 1634 sf	1: 3 Br/3.5 Ba	2: 2 car tandem	1: 32'	B	blde1:5/hlde 2:4				
80	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	AR	6				
81	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	B	5				
82	1-2	1: 1634 sf	1: 3 Br/3.5 Ba	2: 2 car tandem	1: 32'	C	blde1:6/hlde 2:4				
83	1	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32'	B	5				
84	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	AR	6				
85	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	B	5				
86	1-2	1: 1634 sf	1: 3 Br/3.5 Ba	2: 2 car tandem	1: 32'	A	blde1:4/hlde 2:6				
87	1	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32'	C	5				
88	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	AR	4				

**PROVENCE – COURTS / RC NATOMAS, LLC**  
**COLOR SCHEMES / SAAP NO. 2008046**  
**January 03, 2011**

COLOR SCHEMES	• PLASTER 1 (LOWER)	• PLASTER 2 (UPPER)	• PLASTER TRIM • PLASTER WINDOW TRIM • DECORATIVE PLASTER • PROJECTION	• FASCIA • RAKE BOARD • ENTRY DOOR • GARAGE DOOR • FRENCH DOOR	• SHUTTER	• METAL RAIL / GUARDRAIL	• STONE PATTERN VENEER	• COMPOSITE'S ROOF TILE
1	CL 2923	CL 2911	CL 3163	CL 3165	CL 3105	CL 3226	ADANTE	CLAY
2	CLC 1250	CLC 1249	CL 2934	CL 2955	CL 2996	CL 3226	UMBRIA	RED FLASHED
3	CL 2814	CL 2813	CL 2506	CL 2507	CL 2496	CL 3226	MESETA	CLAY FLASHED
4	CL 2924	CL 2913	CL 2914	CL 3014	CL 3016	CL 3226	PADOVA	CLAY FLASHED

ALL COLOR TO BREAK AT INSIDE CORNERS. RAIN GUTTERS AND DOWNSPOUTS TO BE PAINTED TO MATCH ADJACENT SURFACE – I.E., GUTTERS TO MATCH FASCIA. DOWNSPOUTS TO MATCH STUCCO.

- PLASTER BY EXPO STUCCO - 16/20 SAND FINISH AND HEAVY TEXTURE PLASTER (HEAVY DASH).
- PAINT IS BY FRAZEE (CLW/CLC/CLV/CL 0000).
- STONE PATTERN VENEER: COLOR / STONE TYPE / TEXTURE SIMILAR TO ELDORADO FIELDLEDGE.
- ROOF TILE COMPOSITE 'S' TILE MAXILITE P10 BY MAXITILE.
- Fourth Color Scheme added 12/07/10
- Changes highlighted 01/03/11, per Jim Murar's request

**PROVENCE – URBAN VILLAS / RC NATOMAS, LLC**  
**COLOR SCHEMES / SAAP NO. 2008046**  
**June 17, 2010**

COLOR SCHEMES	• PLASTER 1 (LOWER)	• PLASTER 2 (UPPER)	• PLASTER TRIM • WINDOW TRIM • DECORATIVE PLASTER • PROJECTION	• FASCIA • ENTRY DOOR • GARAGE DOOR	• METAL RAIL / GUARDRAIL • SHUTTER	• FLAT COMPOSITE ROOF TILE
4	CL 2802	CL 2792	CL 2846	CL 2925	CL 2676	FLASHED BROWN
5	CL 1873	CL 2762	CL 2785	CL 2866	CL 2887	OXFORD GRAY
6	CL 2834	CL 2923	CL 2835	CL 2936	CL 2937	FLASHED OXFORD

ALL COLOR TO BREAK AT INSIDE CORNERS. RAIN GUTTERS AND DOWNSPOUTS TO BE PAINTED TO MATCH ADJACENT SURFACE – I.E., GUTTERS TO MATCH FASCIA, DOWNSPOUTS TO MATCH STUCCO.

- PLASTER BY EXPO STUCCO - 16/20 SAND FINISH.
- PAINT IS BY FRAZEE (CLW/CLC/CLV/CL 0000).
- ROOF TILE COMPOSITE FLAT TILE MaxiSHAKE / MaxiSLATE BY MAXITILE.



PROVENCE - COURTS  
UNIT 1/4  
FRONT ELEVATION  
ELEVATION STYLE 'B'





PROVENCE - COURTS  
UNIT 2/3  
ENHANCED RIGHT ELEVATION  
ELEVATION STYLE 'B'





PROVENCE - COURTS  
UNIT 5/7  
ENHANCED RIGHT ELEVATION  
ELEVATION STYLE 'A'





PROVENCE - COURTS  
UNIT 6/9  
FRONT ELEVATION  
ELEVATION STYLE 'A'





PROVENCE - COURTS  
UNIT 8  
FRONT ELEVATION  
ELEVATION STYLE 'C'

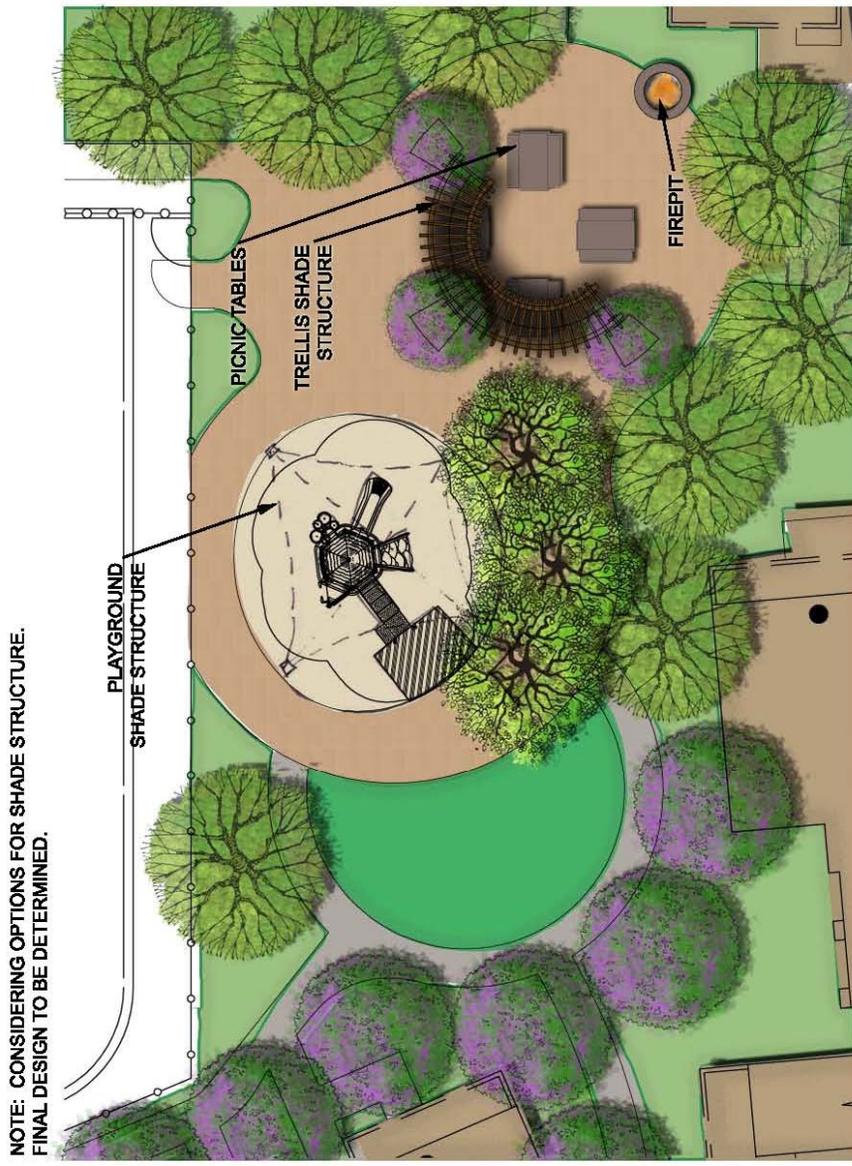




PROVENCE - COURTS  
UNIT 10/11  
FRONT ELEVATION  
ELEVATION STYLE 'C'



Exhibit 6L: Playground Shade Structure and Paseo Concept Plans



NOTE: CONSIDERING OPTIONS FOR SHADE STRUCTURE.  
FINAL DESIGN TO BE DETERMINED.

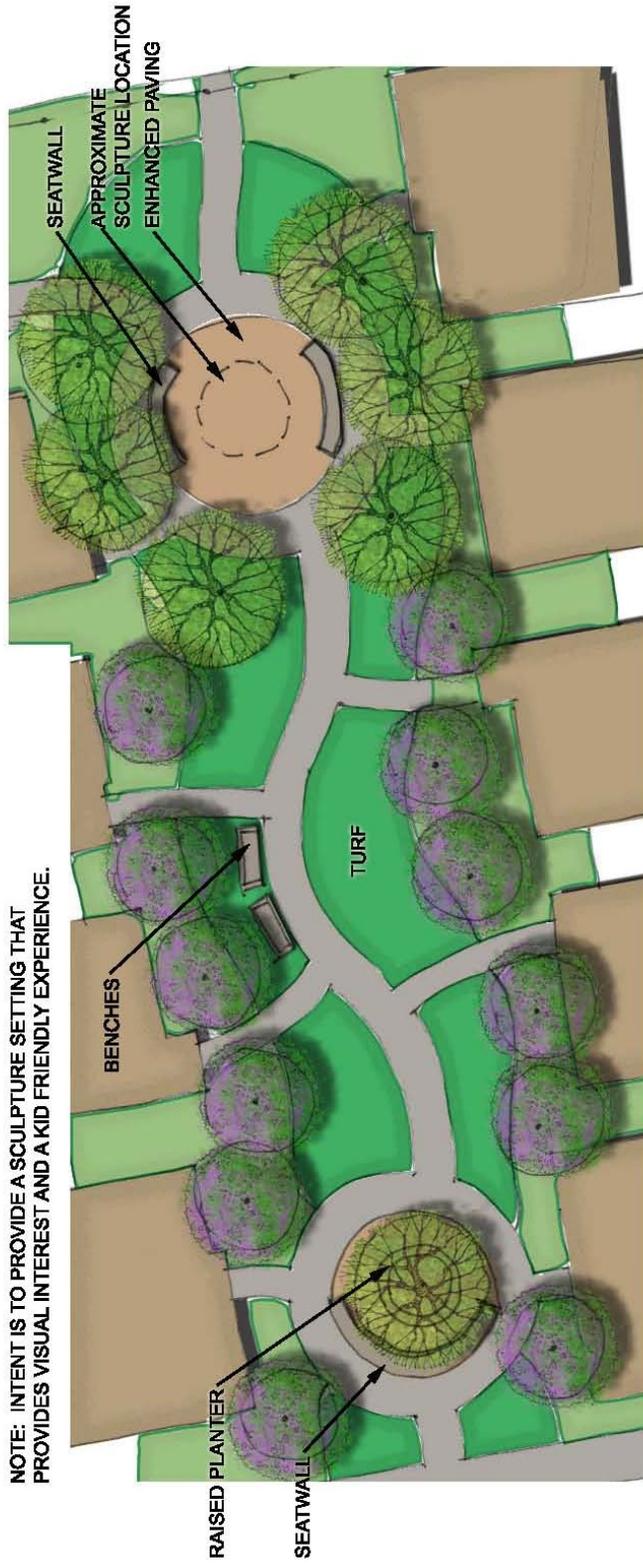
02/23/2011  
Cardno  
WRG

SACRAMENTO, CA  
PLAYGROUND SHADE EXHIBIT



**PROVENCE**  
RC NATOMAS, LLC

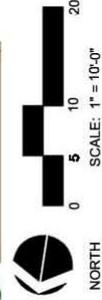
NOTE: INTENT IS TO PROVIDE A SCULPTURE SETTING THAT PROVIDES VISUAL INTEREST AND A KID FRIENDLY EXPERIENCE.



**PROVENCE**  
RC NATOMAS, LLC

02/22/2011  
Cardno  
W/R/G  
SACRAMENTO, CA  
SOUTH PASEO CONCEPT PLAN

NOTE: INTENT IS TO PROVIDE A SCULPTURE SETTING THAT PROVIDES VISUAL INTEREST AND A KID FRIENDLY EXPERIENCE.



08/22/2011  
**SACRAMENTO, CA**  
 Cardno  
 WRG

**PROVENCE**  
 RC NATOMAS, LLC

**NORTH PASEO CONCEPT PLAN**

## Attachment 7: WALKSacramento Comment Letter



March 15, 2011

David Hung  
Associate Planner  
Community Development Department  
300 Richards Boulevard, 3rd Floor  
Sacramento, CA 95811

**RE: Provence (P09-006)**

Dear Mr. Hung:

We appreciate the effort the applicant has made to improve the pedestrian circulation and provide the greatest opportunities for walking. The project site is adjacent to shopping, restaurants, and office – as such, it should be in demand by people that want a walkable residence north of the American River.

WALKSacramento would like to identify several pedestrian-friendly features of the proposed Provence residential project.

Tree shade on hardscape is important for providing pleasant pedestrian environment, and in the Sacramento region climate tree shade is crucial to creating a hospitable outdoor environment.

**The Provence project will provide an exceptionally tree-shaded environment for pedestrians.**

- Many of the private (alley) streets in this project include small trees such as Crape Myrtle and Crab Apple. The private streets that are not alleys have many mid-size trees within setbacks and street planters
- Shade trees currently stand within the street planters between the public streets and sidewalks on three sides of the project.
- The landscaped areas of the project have many small- and mid-size trees that will shade the walkways on the site.

**Pedestrians and cyclists will have six convenient points of access to the multi-use trail to be constructed along the southern edge of the site.**

- This number of access points on a trail segment of less than 500' is outstanding.
- Direct path-of-travel to the trail is provided for all residents and should be a great amenity for the future residents.

Related to these two beneficial components of the project mentioned above, there are two concerns that should be addressed.

1. The preliminary landscape plan seems to show a few trees planted on walkways. We would like to see all of the trees planted and the 50% shading requirement to actually be met. **We recommend staff review of the final landscape plan before occupancy.**
2. Special consideration should be given to the multi-use trail and the public sidewalk at the intersection of East Commerce Way and future Snowy Egret Way. The interface between bicyclists, pedestrians, and vehicles is critical for everyone's safety and comfort. **We recommend that the trail design be routed to the Alternate Modes Coordinator for review to insure the trailhead is designed correctly.**

*WALKSacramento* encourages people to walk and bicycle in their communities. The benefits include improved physical fitness, less motor vehicle traffic congestion, better air quality, and a stronger sense of cohesion and safety in local neighborhoods. *WALKSacramento* is working to support increased physical activity such as walking and bicycling in local neighborhoods as well as helping to create community environments that support walking and bicycling.

Thank you for your consideration of these comments and recommendations. If you have questions or need additional information, please contact me at (916) 446-9255 or [cholm@walksacramento.org](mailto:cholm@walksacramento.org).

Sincerely,



Chris Holm  
Project Analyst



Community Development  
DEPARTMENT  
  
PLANNING DIVISION



ENVIRONMENTAL PLANNING  
SERVICES  
916-808-8419  
FAX 916-808-1077

**MITIGATED NEGATIVE DECLARATION**

May 8, 2009

The City of Sacramento, California, a municipal corporation, does hereby prepare, declare, and publish this Mitigated Negative Declaration for the following described project:

**Provence (P09-006)** The proposed project consists of development entitlements to construct 237 condominium units on the project site. The 2030 General Plan land use designation for the project site is Employment Center Mid Rise.

The proposed project seeks a modification to the previous approval for 187 units of townhouse development (P06-194) of which five homes have been constructed. The proposed project requires a PUD Schematic Plan Amendment to re-designate the use and allowable density of the site, a Tentative Map, a Special Permit to develop 237 condominium units and a Special Permit Modification to amend the previous approval on the site (P06-194).

The Lead Agency is the City of Sacramento. The City of Sacramento, Community Development Department, reviewed the proposed project and, on the basis of the whole record before it, determined that the proposed project is consistent with the land use designation for the project site as set forth in the 2030 General Plan. The City prepared the attached Initial Study that identifies potentially new or additional significant environmental effects (project-specific effects) that were not analyzed in the 2030 General Plan Master EIR. The City will incorporate all feasible mitigation measures or feasible alternatives appropriate to the project as set forth in the Master EIR, and adopt project-specific mitigation measures in order to avoid or mitigate the identified effects to a level of insignificance. (CEQA Guidelines Sections 15177(d), 15178(b)(2)). This Mitigated Negative Declaration reflects the Lead Agency's independent judgment and analysis. An Environmental Impact Report is not required pursuant to the Environmental Quality Act of 1970 (Sections 21000, et seq., Public Resources Code of the State of California).

This Mitigated Negative Declaration was prepared pursuant to the California Environmental Quality Act (Public Resources Code Sections 21000 et seq.), CEQA Guidelines (Title 14, Sections 15000 et seq. of the California Code of Regulations), the Sacramento Local Environmental Regulations (Resolution 91-892) adopted by the City of Sacramento, and the Sacramento City Code. A copy of this document and all supportive documentation may be reviewed or obtained at the City of Sacramento, Development Services Department, 300 Richards Boulevard, 3<sup>rd</sup> Floor, Sacramento, CA 95811. The public counter is open from 8:00 am to 4:00 pm; Monday through Friday.

Environmental Services Manager, City of Sacramento,  
California, a municipal corporation

By: LE Buford

Date: 5/8/09

## **PROVENCE (P09-006)**

### **INITIAL STUDY/ MITIGATED NEGATIVE DECLARATION**

This Initial Study has been prepared by the City of Sacramento, Community Development Department, 300 Richards Boulevard, Third Floor, Sacramento, CA 95811, pursuant to the California Environmental Quality Act (Public Resources Code Sections 21000 *et seq.*), CEQA Guidelines (Title 14, Section 15000 *et seq.* of the California Code of Regulations) and the Sacramento Local Environmental Regulations (Resolution 91-892) adopted by the City of Sacramento.

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#### **ORGANIZATION OF THE INITIAL STUDY**

This Initial Study is organized into the following sections:

**SECTION I - BACKGROUND:** Provides summary background information about the project name, location, sponsor, and the date this Initial Study was completed.

**SECTION II - PROJECT DESCRIPTION:** Includes a detailed description of the proposed project.

**SECTION III - ENVIRONMENTAL CHECKLIST AND DISCUSSION:** This section reviews the project to determine whether it would have additional significant environmental effects (project-specific effects) that were not evaluated in the Master EIR for the 2030 General Plan.

**SECTION IV - ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:** Identifies which environmental factors were determined to have additional significant environmental effects.

**SECTION V - DETERMINATION:** Identifies the determination of whether environmental effects associated with development of the proposed project are significant, and what, if any, added environmental documentation may be required.

**REFERENCES CITED:** Identifies source materials that have been consulted in the preparation of the Initial Study.

## SECTION I - BACKGROUND

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Project Name and File Number: Provence (P09-006)

Project Location: The proposed project site is located southwest of the intersection of East Commerce Way and Benefit Way. The proposed project is located within the North Natomas Community Plan area of the City of Sacramento. The project site includes Assessor's Parcel Number 225-2330-002 thru-076, 225-2680-002 thru -083 and 225-2690-002 thru -069.

Project Applicant: Jen Brioschi  
WRG Design, Inc.  
201 Creekside Ridge Court, Suite 100  
Roseville, CA 95678

Project Planner: David Hung, Associate Planner  
300 Richards Boulevard  
(916) 808-5530

Environmental Planner: Kristin Ford, Assistant Planner  
300 Richards Boulevard  
(916) 808-8419

Date Initial Study Completed: May 8, 2009

This Initial Study was prepared in accordance with the California Environmental Quality Act (CEQA) (Public Resources Code Sections 1500 *et seq.*). The Lead Agency is the City of Sacramento.

The City of Sacramento, Community Development Department, has reviewed the proposed project and, on the basis of the whole record before it, has determined that the proposed project is consistent with the land use designation and the permissible densities and intensities of use for the project site as set forth in the 2030 General Plan. The proposed project is an anticipated subsequent project in the Master EIR prepared for the 2030 General Plan and was adequately described as included in the Master EIR. See CEQA Guidelines Section 15176(d).

The City has also determined that the discussions of cumulative impacts, growth inducing impacts, and irreversible significant effects in the 2030 General Plan Master EIR are adequate for the project. See CEQA Guidelines Section 15178 (a).

The City has prepared the attached Initial Study to identify any potential new or additional significant environmental effects (project-specific effects) that were not analyzed in the Master EIR for the 2030 General Plan. The Initial Study identifies any mitigation measures that will be incorporated to revise the project before the environmental document is released for public

review pursuant to CEQA Guidelines Section 15073 in order to avoid or mitigate the identified effects to a level of insignificance. (CEQA Guidelines Section 15178(b)).

As part of the Master EIR process, the City is required to incorporate all feasible mitigation measures or feasible alternatives appropriate to the project as set forth in the Master EIR (CEQA Guidelines Section 15177(d)) The Master EIR mitigation measures that are identified as appropriate are set forth in the applicable technical sections below.

This analysis incorporates by reference the general discussion portions of the 2030 General Plan Master EIR. (CEQA Guidelines Section 15150(a)). The Master EIR is available for public review at the City of Sacramento, Community Development Department, 300 Richards Boulevard, Third Floor, Sacramento, CA 95811, and on the City's web site at: [www.cityofsacramento.org/dsd/planning/environmental-review/eirs/](http://www.cityofsacramento.org/dsd/planning/environmental-review/eirs/).

The City is soliciting views of interested persons and agencies on the content of the environmental information presented in this document. Due to the time limits mandated by state law, your response must be sent at the earliest possible date, but no later than the 20-day review period ending May 28, 2009.

Please send written responses to:

Kristin Ford  
Community Development Department  
City of Sacramento  
300 Richards Blvd, 3<sup>rd</sup> Floor  
Sacramento, CA 95811  
Direct Line: (916) 808-8419  
FAX (916) 808-1077  
kford@cityofsacramento.org

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## SECTION II - PROJECT DESCRIPTION

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### Section II – Project Description

#### Introduction

The project site comprises approximately 13.9 acres in North Natomas in the City of Sacramento. The project site is located on the southwest intersection of East Commerce Way and Benefit Way. The subject property is identified by the Sacramento County Assessor's Office as parcel numbers 225-2330-002 thru-076, 225-2680-002 thru -083 and 225-2690-002 thru -069. The proposed site is bounded by commercial property to the east and north, and vacant land to the west and south.

#### Project Background

Natomas Crossing PUD is divided into three areas: Area 1 is located at the southeast corner of Truxel Road and Del Paso Road, Area 2 is generally located south of Arena Boulevard, east of Airport Road, west of Natomas East Main Drainage Canal and north of Fong Ranch area; and Area 3 is located east of Interstate 5, between Del Paso Road and San Juan Road. Total acreage for the Natomas Crossing PUD is 563 gross acres. The proposed project comprises a portion of Area 3.

The Natomas Crossing PUD has been the subject of prior environmental review. The City Council ratified a Negative Declaration (P01-028), adopted the Mitigation Monitoring Plan (MMP) and approved the necessary entitlements to reconfigure the land use designations and zoning for the 298 acre Natomas Crossing – Area 3 in 2002.

In 2005, the Planning Commission approved the Carriage Lane III Addendum (P05-079) to an adopted Negative Declaration and adopted a Mitigation Monitoring Plan. The proposed project site consisted of 38 buildings totaling 188 condominium units on 13.9 acres in the EC-50-PUD in the Natomas Crossing – Area 3.

In 2006, the City Council approved a PUD Schematic Plan Amendment (P05-079) to re-designate the proposed site for multi-family development. The Planning Commission later approved a Tentative Subdivision Map to create one condominium parcel and a PUD Special Permit to develop a 187-unit townhouse complex.

In 2007, the Planning Commission adopted the Provence (Carriage Lane III) Addendum and MMP, approved a Subdivision Modification and a request to revoke a PUD Special Permit to develop a 187-unit townhouse complex in the Employment Center Planned Unit Development (EC-50-PUD) zone (P05-079). The Planning Commission approved the request to revoke a PUD Special Permit in order to allow a new PUD Special Permit to develop a 187 unit alternative housing development (townhouses) on 10.9 acres in the EC-50-PUD zone.

The current application was submitted to develop 237 condominium units on the project site. The request is a modification to the previous approval for 187 units of townhouse development (P06-194) of which five homes are already constructed. The proposed project requires a PUD Schematic Plan Amendment to re-designate the use and allowable density of the site, a Tentative Map, a Special Permit to develop 237 condominium units and a Special Permit Modification to amend the previous approval on the site (P06-194).

## **Project Description**

The proposed project has been improved with five homes. The remainder of the project site is vacant, and infrastructure improvements (including underground utilities, streets, curbs and gutters) associated with the previously approved project (P06-164) have been installed and completed. The Final Master Parcel Map was approved and recorded in Book 317, page 11 on August 12, 2003. The proposed project would modify the previous approval of 187 units of townhouses to permit the development of 237 condominium units.

Prior environmental review evaluated the impacts of the previous projects. Mitigation measures related to site development and installation of subdivision improvements were implemented during construction.

## **Attachments**

Attachment A - Vicinity Map

Attachment B - Land Use and Zoning

Attachment C - Site Plan

Attachment D - Summer and Winter Emission Reports

## SECTION III – ENVIRONMENTAL CHECKLIST AND DISCUSSION

### LAND USE, POPULATION AND HOUSING, AGRICULTURAL RESOURCES

#### Introduction

The California Environmental Quality Act (CEQA) requires the Lead Agency to examine the effects of a project on the physical conditions that exist within the area that would be affected by the project. CEQA also requires a discussion of any inconsistency between the proposed project and applicable general plans and regional plans.

An inconsistency between the proposed project and an adopted plan for land use development in a community would not constitute a physical change in the environment. When a project diverges from an adopted plan, however, it may affect planning in the community regarding infrastructure and services, and the new demands generated by the project may result in later physical changes in response to the project.

In the same manner, the fact that a project brings new people or demand for housing to a community does not, by itself, change the physical conditions. An increase in population may, however, generate changes in retail demand or demand for governmental services, and the demand for housing may generate new activity in residential development. Physical environmental impacts that could result from implementing the proposed project are discussed in the appropriate technical sections.

This section of the initial study identifies the applicable land use plans and policies, and discusses any inconsistencies between these plans and the proposed project.

#### Discussion

The proposed project site is currently developed with five unoccupied residences and subdivision and utility infrastructure. The project site is currently zoned for Employment Center-50-PUD uses. The EC-50-PUD zone is a flexible zone for primarily employment generating uses in a pedestrian friendly setting with ample private and/or public open space. The EC zone also provides the opportunity for a variety and mix of supporting uses, including support retail, residential, and light industrial.

The 2030 General Plan land use designation for the project site is Employment Center Mid Rise. This designation provides for large mixed-use office/employment centers that include mid-rise office complexes, retail and service uses such as restaurants, dry-cleaners, gym/fitness centers, markets, hotels and office services, landscaped gathering places that include support uses, residential uses as a supportive mixed use or adjacent to large employment center and compatible public, quasi-public, and special uses. The project is consistent with the land use designation.

The proposed project would construct 237 condominium units on 13.9 acres. The development standards for Employment Center Mid Rise are: minimum density is 18.0 units per net acre; maximum density is 60 units per net acre. The proposed project and the existing 5 homes would total in 17.6 units per acre, which is below the minimum density. The proposed project is consistent with the 2030 General Plan Employment Center Mid Rise designation.

The proposed project site is not in agricultural production. No commercial agricultural operations exist in the project vicinity. The proposed project is located in an urbanized portion of the community, and currently includes connections to municipal water, sewer and storm drains. Extensions of utilities to the project site would not extend service to an area not previously served. The project would not directly or induce substantial growth in the project area. No housing units would be displaced or impacted by the proposed project.

Issues:	Effect remains significant with all identified mitigation	Effect can be mitigated to less than significant	No additional significant environmental effect
<b>1. <u>AESTHETICS, LIGHT AND GLARE</u></b>			
Would the proposal:			
A) Have a substantial adverse effect on a scenic vista?			X
B) Substantially damage scenic resources including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?			X
C) Substantially degrade the existing visual character or quality of the site and its surroundings?			X
D) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?		X	

**ENVIRONMENTAL SETTING**

The project site is located in North Natomas in the City of Sacramento. The project site is located approximately 400 feet east of Interstate 5 on the east side of East Commerce Way, north of Benefit Way, east of Advantage Way and Interstate 5. The proposed project site is approximately 1000 feet north of Del Paso Boulevard. The project site is not located in an adopted view corridor or a scenic vista.

**STANDARDS OF SIGNIFICANCE**

*Glare.* Glare is considered to be significant if it would be cast in such a way as to cause public hazard or annoyance for a sustained period of time.

*Light.* Light is considered significant if it would be cast onto oncoming traffic or residential uses.

**Answers to Checklist Questions**

**QUESTION A**

Because the project site is not located within an identified scenic corridor or viewshed, there would be no additional significant effects associated with the project.

**Question B**

The proposed project is not located in or nearby any scenic resources including trees, rock outcroppings and historic buildings within a state scenic highway. There would be no additional significant environmental effects that would result from project development and operation.

### QUESTION C

The proposed project would develop 237 condominium units on the project site. Five homes currently exist onsite. The project would change the visual character of the project site, but the change would be generally consistent with development characteristics in the surrounding area. The design of the project site would be subject to staff review and review by the Planning Commission.

The project would be required to comply with the City of Sacramento's guidelines for the development of structures, which would ensure that the appearance of the project is compatible with existing development in the project vicinity (Single Family Residential Design Principles, January 1998). No additional effects would result.

### QUESTION D

The proposed project includes construction of 237 condominium units on approximately 13.7 acres. Residential development is not typically considered to be a substantial source of glare, due to the limited height and the limited amount of reflective surface area (i.e., glass and metal surfaces). Lighting in the residential development would be subject to the City's zoning code and site review limiting outside lighting to fixtures that direct light downward to avoid spill to adjacent properties. Sacramento Municipal Code Section 15.80.020 requires that all lighting on residential structures shall be engineered so as to not to produce glare or stray light on adjacent properties. Section 17.68.030 requires that lighting shall be directed away from residential areas and public streets. These provisions are enforced by staff during the plan check process.

The project site is located in an urbanized area that includes various types of land uses, including residential, multi-family, and commercial and offices. The proposed project includes residential uses, and notwithstanding the implementation of the provisions of the City Code, could result in light intrusion and glare to the residential use. To ensure that the project lighting is reviewed during the development review, Mitigation Measure Aesthetics 1 will be implemented.

The proposed project would require improvements to the City's rights-of-way. These improvements include the installation of street lighting, as required by the Department of Transportation as a condition of approval. The lighting would be installed and shielded consistent with City standards. With the design and orientation of lighting in compliance with the City standards and Mitigation Measure Aesthetics 1, any additional significant environmental effects associated with light and glare would be less than significant.

### MITIGATION MEASURES

**Aesthetics 1:** Project outdoor lighting shall be oriented away from adjacent properties and shall not produce a glare or reflection on neighboring properties or adjacent streets or property.

### FINDINGS

All additional significant environmental effects of the project relating to Aesthetics can be mitigated to a less-than-significant level.

Issues:	Effect remains significant with all identified mitigation	Effect can be mitigated to less than significant	No additional significant environmental effect
<b>2. AIR QUALITY</b>			
<i>Would the proposal:</i>			
A) Conflict with or obstruct implementation of the applicable air quality plan?			X
B) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?			X
C) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions that exceed quantitative thresholds for ozone precursors)?			X
D) Exposure sensitive receptors to substantial pollutant concentrations?			X
E) Create objectionable odors affecting a substantial number of people?			X
F) Interfere with or impede the City's efforts to reduce greenhouse gas emissions?			X

**ENVIRONMENTAL SETTING**

The project area is located in the Sacramento Valley Air Basin, which is bounded by the Sierra Nevada on the east and the Coast Range on the west. Prevailing winds in the project area originate primarily from the southwest. These winds are the result of marine breezes coming through the Carquinez Straights. The marine breezes diminish during the winter months, and the winds from the north occur more frequently at this time. Air Quality within the project area and surround region is largely influenced by urban emission sources.

PM<sub>2.5</sub>: On December 22, 2008, the EPA administrator approved PM<sub>2.5</sub> nonattainment areas, which included Sacramento County and portions of Counties adjacent to Sacramento. The designations will become effective 90 days after publication in the Federal Register, sometime in April 2009. Plans for how areas will meet the health standards are due to EPA in April 2012. Areas must meet the health standards by April 2014, but the deadline can be extended to April 2019. The Sacramento Metropolitan Air Quality Management District will be working with the California Air Resources Board staff to update a technical assessment and modeling of Sacramento's PM<sub>2.5</sub> problems, update the inventory of PM<sub>2.5</sub> and precursor emission sources,

and determine whether existing controls are adequate to attain the federal PM<sub>2.5</sub> standards by 2014 or 2019. [

#### **STANDARDS OF SIGNIFICANCE**

The SMAQMD adopted the following thresholds of significance in 2002:

*Ozone and Particulate Matter.* An increase of nitrogen oxides (NOx) above 85 pounds per day for short-term effects (construction) would result in a significant impact. An increase of either ozone precursor, nitrogen oxides (NOx) or reactive organic gases (ROG), above 65 pounds per day for long-term effects (operation) would result in a significant impact (as revised by SMAQMD, March 2002). The threshold of significance for PM<sub>10</sub> is a concentration based threshold equivalent to the California Ambient Air Quality Standard (CAAQS). For PM<sub>10</sub>, a project would have a significant impact if it would emit pollutants at a level equal to or greater than five percent of the CAAQS (50 micrograms/cubic meter for 24 hours) if there were an existing or projected violation; however, if a project is below the ROG and NOx thresholds, it can be assumed that the project is below the PM<sub>10</sub> threshold as well (SMAQMD, 2004).

*Carbon Monoxide.* The pollutant of concern for sensitive receptors is carbon monoxide (CO). Motor vehicle emissions are the dominant source of CO in Sacramento County (SMAQMD, 2004). For purposes of environmental analysis, sensitive receptor locations generally include parks, sidewalks, transit stops, hospitals, rest homes, schools, playgrounds and residences. Commercial buildings are generally not considered sensitive receptors. Carbon monoxide concentrations are considered significant if they exceed the 1-hour state ambient air quality standard of 20.0 parts per million (ppm) or the 8-hour state ambient standard of 9.0 ppm (state ambient air quality standards are more stringent than their federal counterparts).

*Toxic Air Contaminants.* The project would create a significant impact if it created a risk of 10 in 1 million for cancer (stationary sources only).

#### **MITIGATION MEASURES FROM 2030 GENERAL PLAN MASTER EIR THAT APPLY TO THE PROJECT**

The following mitigation measures applicable to air quality were identified in the 2030 General Plan Master EIR, and will be applied to the project:

Greenhouse Gas Emissions and Climate Change: The Master EIR identified numerous policies included in the 2030 General Plan that addressed greenhouse gas emissions and climate change. See Draft MEIR, Chapter 8, and pages 8-49 et seq. The Master EIR is available for review at the offices of Development Services Department, 300 Richards Boulevard, 3<sup>rd</sup> Floor, Sacramento, CA during normal business hours, and is also available online at: <http://www.cityofsacramento.org/dsd/planning/environmental-review/eirs/>.

Policies identified in the 2030 General Plan include directives relating to sustainable development patterns and practices, and increasing the viability of pedestrian, bicycle and public transit modes. A complete list of policies addressing climate change is included in the Master EIR in Table 8-6, pages 8-50 et seq; the Final MEIR included additional discussion of greenhouse gas emissions and climate change in response to written comments. See changes to Chapter 8 at Final MEIR pages 2-19 et seq. See also Letter 2 and response.

The City ultimately determined that greenhouse gas emissions that would be generated by development consistent with the 2030 General Plan would be a significant and unavoidable cumulative impact. The discussion of greenhouse gas emissions and climate change in the Draft MEIR, Final MEIR and Errata 2 are incorporated by reference in this Initial Study. (CEQA Guidelines Section 15150)

## ANSWERS TO CHECKLIST QUESTIONS

### QUESTION A, B AND D

Operational Impacts: The URBEMIS 2007 9.2.4 model was used to calculate estimated emissions for the operation of the proposed project. Based on the estimated emissions from running the URBEMIS model, the proposed project is not likely to exceed the long-term operational emissions threshold of 65 lbs/day for ROG and NO<sub>x</sub>. Estimated ROG and NO<sub>x</sub> summer emissions for using the URBEMIS 2007 9.2.4 model were calculated to be approximately 30.21 lbs/day and 18.54 lbs/day, respectively, which is below the 65 lbs/day threshold. The estimated ROG and NO<sub>x</sub> winter emissions for using the URBEMIS 2007 9.2.4 model were calculated to be approximately 27.54 lbs/day and 27.92 lbs/day, respectively. See Attachment D for the URBEMIS calculations (summer and winter emission reports).

Project-Related Construction Impacts: The URBEMIS 2007 9.2.4 model was used to calculate estimated emissions for the construction of the proposed project. Based on the estimated emissions from running the URBEMIS model, the proposed project is not likely to exceed the short-term emissions threshold of 85 lbs/day for NO<sub>x</sub>. Estimated NO<sub>x</sub> summer and winter emissions using the URBEMIS 2007 9.2.4 model were calculated to be approximately 52.12 lbs/day, which is below the 85 lbs/day threshold.

As stated above, the URBEMIS 2007 9.2.4 model was utilized to calculate the estimated emissions of the project. Inputs were made to the model using the project unit count and acreage. All other default settings were used, minus wood hearths (Rule 417). Using the default settings provides a conservative estimate of the operational and construction emissions. The proposed project site was previously approved for development for which construction began that included grading, installation of utilities, and paving. Most of this work was completed under the prior project and new grading and paving would not need to occur except as needed for repair.

The SMAQMD 2004 Guide to Air Quality Assessment states on page 3-2 that if the project's NO<sub>x</sub> mass emissions from heavy-duty, mobile sources is determined not potentially significant using the recommend methodologies for estimated emissions (Manual Calculation, URBEMIS, and Roadway Construction Model), the Lead Agency may assume that exhaust emissions of other pollutants from operation of construction equipment and worker commute vehicles are also not significant. The URBEMIS 2007 model indicated that the project would not exceed the NO<sub>x</sub> threshold and, based on the guidance of the air district, the analysis of other criteria pollutant emissions is not included in this discussion.

Construction activities would be subject to the SMAQMD's Rule 403 on Fugitive Dust, which provides that contractors shall take every reasonable precaution not to cause or allow the emissions of fugitive dust from being airborne beyond the property line from which the emission originates, from any construction, handling or storage activity, or any excavation, grading, clearing of land or solid waste disposal operation. Reasonable precautions include, but are not limited to:

- the use of water or chemicals for control of dust, where possible, during construction operations (including roadways), or during the clearing of land;
- the application of asphalt, oil, water, or suitable chemicals on dirt roads, materials stockpiles, and other surfaces, which can give rise to airborne dusts;
- other means approved by the Air Pollution Control Officer.

Previous mitigation written in the 2001 Mitigated Negative Declaration for the Natomas Crossing Area - 3 requested the applicant to comply with the North Natomas Community Plan's requirement to prepare an Air Quality mitigation strategy that reduces ROG emissions by 50 percent project wide. The General Plan Master EIR addresses ROG in Policy ER 6.1.2.; stating if ROG operation thresholds are exceeded, design or operational features that reduce emissions equal to 15% shall be required. This proposed project does not exceed the thresholds discussed above. The proposed project does not conflict with or obstruct implementation of an air quality plan. The proposed project would be required to comply with the air quality standards as established by SMAQMD, and would result in a less than significant impact to air quality.

#### **QUESTION C**

The proposed project would not result in significant air quality impacts associated with short-term construction and long-term operation emissions of ozone-precursor pollutants (ROG and NO<sub>x</sub>) and airborne particulate matter (PM<sup>10</sup> and PM<sup>2.5</sup>). Ozone impacts are the result of the cumulative emissions from stationary, area, and mobile sources located within the region: as well as, transport from outside the region. Ozone is formed by the chemical reaction of the ozone-precursor pollutants ROG and NO<sub>x</sub> in the presences of sunlight, with the highest ozone concentrations occurring during the warmer summer months. The cumulative contributions of ozone-precursor pollutants from multiple sources result in severe ozone problems, which can adversely affect human health. The Sacramento Valley Air Basin (SVAB) is classified non-attainment for ozone and PM<sup>10</sup>.

For evaluation of cumulative ozone and PM impacts, the SMAQMD recommends that the project-level significance thresholds be relied upon for determination of cumulative air quality impacts. The proposed project does not generate emissions of either ozone precursor pollutants (i.e., ROG and NO<sub>x</sub>) or PM<sup>10</sup> and not would exceed the short-term or long-term thresholds. The project would not be considered to have a cumulatively considerable incremental contribution to a significant cumulative impact.

#### **QUESTION D**

The 2030 General Plan and MEIR include provisions to ensure that the City considers exposure to toxic air contaminants when approving new development in the vicinity of freeways and major roadways. In general, these concerns arise for developments within 500 feet of such roadways. The proposed project includes proposals for new residential units no closer than 500 feet to Interstate 5 and almost all residential units are well beyond the 500-foot distance. Development of the project site as proposed would not require a health risk assessment and would not generate any additional significant environmental effects for relating to toxic air contaminants.

#### **QUESTION E & F**

The City approved the 2030 General Plan on March 3, 2009 and became effective April 3, 2009. The City certified the Master EIR for the 2030 General Plan project at the same time.

The Master EIR includes extensive discussion of the potential effects of greenhouse gas (GHG) emissions. See for example:

Draft EIR: 6.1 Air Quality (Page 6.1-1)

Final EIR: City Climate Change master Response (Page 4-1)

Errata No. 2: Climate Change (Page 12)

These documents are available online at [www.cityofsacramento.org/dsd/planning/environmental-review/eirs/](http://www.cityofsacramento.org/dsd/planning/environmental-review/eirs/) and at the offices of the Development Services Department at 300 Richards Boulevard, Sacramento, California. The MEIR discussions regarding climate change are incorporated here by reference.

The Master EIR concluded that the greenhouse gas emissions that could be emitted by development that is consistent with the 2030 General Plan would be cumulatively considerable and unavoidable (Errata No. 2, Page 12).

Review of project greenhouse gas emissions is set forth below.

### **Short-term Construction Emissions**

During construction of the project, GHG's would be emitted from the operation of construction equipment and from worker and building supply vendor vehicles. CO<sub>2</sub> emissions resulting from construction of the project were estimated, using the URBEMIS2007 model, to be approximately 335.75 metric tons per year. These emissions would equate to approximately 0.000069 percent of California's total emissions (construction total period would not exceed two years).

### **Long-term Operation Emissions**

The largest source of GHG's associated with the proposed project would be on- and off-site motor vehicle use. CO<sub>2</sub> emissions, the primary GHG from mobile sources, are directly related to the quantity of fuel consumed. CO<sub>2</sub> emissions during operation of the project at full build out were estimated using URBEMIS2007. Results of the modeling showed the estimated CO<sub>2</sub> emissions generated by the project would be approximately 2830.66 metric tons per year, which equates to 0.00058 percent of California's total emissions.

The project site is located within the City's limits and has ready access to bus service. The North Natomas community includes a variety of retail and restaurant locations, helping to minimize vehicle trips.

Buildings constructed as part of the project would be required to comply with current California building codes that enforce energy efficiency. The project is consistent with the City's goals as set forth in the 2030 General Plan and MEIR relating to reduction of greenhouse gas emissions. The project would not impede the City's efforts to comply with AB32 requirements. As the project is consistent with the 2030 General Plan MEIR, no additional significant environmental effects relating to greenhouse gas emissions or climate change would occur from development of the project.

### **Findings**

The project would have no additional environmental effects relating to air quality.

Issues:	Effect remains significant with all identified mitigation	Effect can be mitigated to less than significant	No additional significant environmental effect
<b>3. BIOLOGICAL RESOURCES</b>			
Would the proposal result in impacts to:			
A) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?			X
B) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?			X
C) Have substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?			X
D) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?			X
E) Conflict with any local policies or ordinances protecting biological resources such as a tree preservation policy or ordinance?			X
F) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community conservation Plan, or other approved local, regional, or state habitat conservation plan?			X

**ENVIRONMENTAL SETTING**

The project site has been graded, subdivision infrastructure improvements (including utilities, streets, curbs and gutters) have been installed and five model homes have been constructed on the site. The entire site has been disturbed. The project site does not support habitat for any sensitive plant or animal species on the site. There are no wetlands present on the site.

## **STANDARDS OF SIGNIFICANCE**

For purposes of this environmental document, an impact would be significant if any of the following conditions or potential thereof, would result with implementation of the proposed project:

- Creation of a potential health hazard, or use, production or disposal of materials that would pose a hazard to plant or animal populations in the area affected;
- Substantial degradation of the quality of the environment, reduction of the habitat, reduction of population below self-sustaining levels of threatened or endangered species of plant or animal;
- Affect other species of special concern to agencies or natural resource organizations (such as regulatory waters and wetlands); or
- Violation of the Heritage Tree Ordinance (City Code 12.64.040).

## **ANSWERS TO CHECKLIST QUESTIONS**

### **QUESTION A THROUGH F**

The project site has been the subject of previous development applications, entitlements and environmental review. The site is included within the City of Sacramento's Permit Area under the Natomas Basin Habitat Conservation Plan, and the required fees have been paid. The site has been completely disturbed and developed, and the requested project entitlements would allow an increase in the number of residential units, but would not affect biological resources to any greater extent than current site development.

### **MITIGATION MEASURES**

None required.

### **FINDINGS**

The project would have no additional significant environmental effects relating to Biological Resources.

Issues:	Effect remains significant with all identified mitigation	Effect can be mitigated to less than significant	No additional significant environmental effect
<b>4. CULTURAL RESOURCES</b>			
<i>Would the proposal:</i>			
A) Cause a substantial adverse change in the significance of a historical resource as defined in § 15064.5?			X
B) Cause a substantial adverse change in the significance of an archaeological resource pursuant to § 15064.5?			X
C) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?			X
D) Disturb any human remains, including those interred outside of formal cemeteries?			X

**ENVIRONMENTAL SETTING**

The project site has been graded, subdivision infrastructure improvements (including utilities, streets, curbs and gutters) have been installed and five model homes have been constructed on the site. The entire site has been disturbed.

**STANDARDS OF SIGNIFICANCE**

Cultural resource impacts may be considered significant if the proposed project would result in one or more of the following:

1. Cause a substantial change in the significance of a historical or archaeological resource as defined in CEQA Guidelines Section 15064.5 or
2. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature. Answers to Checklist Questions

**QUESTIONS A THROUGH D**

The project site has been the subject of previous development applications, entitlements and environmental review. The project site has been completely disturbed and developed. No additional excavation would occur as part of the project entitlements. The requested project entitlements would allow an increase in the number of residential units, but would not affect cultural resources to any greater extent than current site development.

**MITIGATION MEASURES**

**CR-1:** In the event that any prehistoric subsurface archeological features or deposits,

including locally darkened soil (“midden”), that could conceal cultural deposits, animal bone, obsidian and/or mortars are discovered during construction-related earth-moving activities, all work within 50 meters of the resources shall be halted, and the City shall consult with a qualified archeologist to assess the significance of the find. Archeological test excavations shall be conducted by a qualified archeologist to aid in determining the nature and integrity of the find. If the find is determined to be significant by the qualified archeologist, representatives of the City and the qualified archeologist shall coordinate to determine the appropriate course of action. All significant cultural materials recovered shall be subject to scientific analysis and professional museum curation. In addition, a report shall be prepared by the qualified archeologist according to current professional standards.

**CR-2:** If a Native American site is discovered, the evaluation process shall include consultation with the appropriate Native American representatives.

- a. If Native American archeological, ethnographic, or spiritual resources are involved, all identification and treatment shall be conducted by qualified archeologists, who are certified by the Society of Professional Archeologists (SOPA) and/or meet the federal standards as stated in the Code of Federal Regulations (36 CFR 61), and Native American representatives, who are approved by the local Native American community as scholars of the cultural traditions.
- b. In the event that no such Native American is available, persons who represent tribal governments and/or organizations in the locale in which resources could be affected shall be consulted. If historic archeological sites are involved, all identified treatment is to be carried out by qualified historical archeologists, who shall meet either Register of Professional Archeologists (RPA), or 36 CFR 61 requirements.

**CR-3:** If a human bone or bone of unknown origin is found during construction, all work shall stop in the vicinity of the find, and the County Coroner shall be contacted immediately. If the remains are determined to be Native American, the coroner shall notify the Native American Heritage Commission, who shall notify the person most likely believed to be a descendant. The most likely descendant shall work with the contractor to develop a program for re-interment of the human remains and any associated artifacts. No additional work is to take place within the immediate vicinity of the find until the identified appropriate actions have taken place.

Implementation of these mitigation measures would reduce the impact to a less than significant level.

## FINDINGS

All additional significant environmental effects of the project relating to Cultural Resources can be mitigated to a less than significant level.

Issues:	Effect remains significant with all identified mitigation	Effect can be mitigated to less than significant	No additional significant environmental effect
5. <u>ENERGY</u> Would the proposal result in impacts to:			
A) Power or natural gas?			X
B) Use non-renewable resources in a wasteful and inefficient manner?			X
C) Substantial increase in demand of existing sources of energy or require the development of new sources of energy?			X

**STANDARDS OF SIGNIFICANCE**

*Gas Service.* A significant environmental impact would result if a project would require PG&E to secure a new gas source beyond their current supplies.

*Electrical Services.* A significant environmental impact would occur if a project resulted in the need for a new electrical source (e.g., hydroelectric and geothermal plants).

**ANSWERS TO CHECKLIST QUESTIONS**

**QUESTION A THROUGH C**

The project is located in an urbanized portion of the community, and the land use designation is consistent with the 29030 General Plan. The site has been planned for development, and has been included in planning by the affected utilities. Buildings would be constructed consistent with the requirements of Title 24, with the attendant energy standards, and there would be no use of non-renewable resources in a wasteful or inefficient manner.

**MITIGATION MEASURES**

None required.

**FINDINGS**

The project would have no additional significant environmental effects relating to Energy.

Issues:	Effect remains significant with all identified mitigation	Effect can be mitigated to less than significant	No additional significant environmental effect
<b>6. GEOLOGY AND SOILS</b>			
Would the project:			
A) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving: <ul style="list-style-type: none"> <li>i.) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.</li> <li>ii.) Strong seismic ground shaking?</li> <li>iii.) Seismic-related ground failure, including liquefaction?</li> <li>iv.) Landslides?</li> </ul>			X
B) Result in substantial soil erosion or the loss of topsoil?			X
C) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?			X
D) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?			X
E) Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?			X

**ENVIRONMENTAL SETTING**

*Geology and Seismicity.* Chapter 6.5 of the Master EIR for the 2030 General Plan discusses the geology and exposure to seismicity of the Sacramento region. While there are no known faults in the greater Sacramento region, faults in other areas of the state could result in seismic events.

**STANDARDS OF SIGNIFICANCE**

For the purposes of this analysis, an impact is considered significant if it allows a project to be built that will either introduce geologic or seismic hazards by allowing the construction of the project on such a site without protection against those hazards.

**ANSWERS TO CHECKLIST QUESTIONS**

**QUESTION A THROUGH E**

The MEIR discussed the potential for exposure to seismic and geologic events. The proposed project would be located in the North Natomas area of the community, and project review would include appropriate examination of soils on the project site. The project does not expose persons or property to risks that were not examined in the MEIR, and would not have additional significant environmental effects.

**MITIGATION MEASURES**

None required.

**FINDINGS**

The project would have no additional significant environmental effects relating to Geology and Soils.

Issues:	Effect remains significant with all identified mitigation	Effect can be mitigated to less than significant	No additional significant environmental effect
7. <u>HAZARDS</u>			
Would the project:			
A) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?			X
B) Create a significant hazard to the public or environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?			X
C) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?			X
D) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?			X
E) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport, or public use airport, would the project result in a safety hazard for people residing or working in the project area?			X
F) For a project within the vicinity of private airstrip, would the project result in a safety hazard for people residing or working in the project area?			X
G) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?			X
H) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?			X

## **ENVIRONMENTAL SETTING**

The project site is located in the North Natomas area of the City of Sacramento. The site and vicinity have been designated for urban development, and the area is experiencing continuing construction as sites are built out. The site and the vicinity are served with urban services.

## **STANDARDS OF SIGNIFICANCE**

For the purposes of this document, an impact is considered significant if the proposed project would:

- expose people (e.g., residents, pedestrians, construction workers) to existing contaminated soil during construction activities;
- expose people (e.g., residents, pedestrians, construction workers) to asbestos-containing materials or other hazardous materials; or
- expose people (e.g., residents, pedestrians, construction workers) to existing contaminated groundwater during dewatering activities.

## **ANSWERS TO CHECKLIST QUESTIONS**

### **QUESTION A THROUGH H**

As a site located in an urban area, the project site is fully served with urban services, including police, fire and stormwater drainage. The exposure to hazards experienced by persons and property on the site would be consistent with the level expected in normal urban development. The MEIR evaluated such exposure, and the project site, with the exception of exposure to flood risk (see Hydrology section below) would not have any additional significant environmental effects.

### **MITIGATION MEASURES**

None required.

### **FINDINGS**

The project would have no additional significant environmental effects relating to Hazards.

Issues:	Effect remains significant with all identified mitigation	Effect can be mitigated to less than significant	No additional significant environmental effect
8. <u>HYDROLOGY AND WATER QUALITY</u> Would the project:			X
A) Violate any water quality standards or waste or discharge requirements?			
B) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to level which would not support existing land uses or planned uses for which permits have been granted)?			X
C) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?			X
D) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?			X
E) Otherwise substantially degrade water quality?			X
F) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?		X	
G) Place within a 100-year flood hazard area structures which would impede or redirect flood flows?			X
H) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?		X	

## ENVIRONMENTAL SETTING

### STANDARDS OF SIGNIFICANCE

*Water Quality.* For purposes of this environmental document, an impact is considered significant if the proposed project would substantially degrade water quality and violate any water quality objectives set by the State Water Resources Control Board, due to increased sediments and other contaminants generated by consumption and/or operation activities.

*Flooding.* For purposes of this environmental document, an impact is considered significant if the proposed project substantially increases exposure of people and/or property to the risk of injury and damage in the event of a 100-year flood.

### ANSWERS TO CHECKLIST QUESTIONS

#### QUESTION A, B & E

The City is required to enforce water quality protection measures during construction, and these provisions have been fully implemented. The project site includes improvements to ensure that stormwater is adequately retained and treated prior to discharge, and no additional significant effects would result from the project regarding water quality.

#### QUESTION C & D

The project site has been graded and subdivision and utility improvements have been installed. Grading and site preparation were completed in compliance with the requirements enforced by the City of Sacramento. No net increase in runoff will occur as a result of enforcement of these requirements, and the project would have no additional significant effect.

#### QUESTION F & H

The proposed project site is located within a potential flood zone. The Sacramento River is located approximately two miles west of the project site, and the American River is approximately four miles south.

In December 2006, FEMA announced a revision to the Flood Insurance Rate Map (FIRM) containing the project site. Based on information provided by SAFCA and the USACE, FEMA found that the area constituted a Special Flood Hazard Area (SFHA). FEMA issued a FIRM revision with an updated AE designation for the Natomas Basin on December 8, 2008. The AE designation requires that all new structures be built above the 100-year flood level, which could be as much as 33 feet in some parts of Natomas.

The Master EIR evaluated the cumulative effects of flood and drainage. The proposed project is located in an area that has less than 100-year flood protection. This is a significant project-specific effect. The effect would be reduced to a less-than-significant level with implementation of Mitigation Measures Hydro 1 and Hydro 2, set forth below.

## MITIGATION MEASURES

The following mitigation measures would reduce the above impact to a **less than significant** level.

**Hydro 1:** Construction and operation of the project shall not commence prior to recertification of the Natomas levees by the USACE and FEMA, and the subsequent removal of Natomas Basin from the 100-year floodplain and associated flood zone redesignation; or until FEMA redesignates the Natomas Basin with a flood zone designation that would permit development of the proposed project. The above measures shall terminate upon the first recertification of the levees by the U.S. Army Corps of Engineers.

**Hydro 2:** The project applicant shall participate in a funding mechanism such as an assessment district established by SAFCA and/or the City for the purpose of implementing measures that would provide no less than 100-year flood protection including the North Natomas Area, or for that portion of the Natomas Basin requiring re-certification for 100-year flood protection including the Project site provided that such funding mechanism is (i) based on a nexus study; (ii) is regional in nature; (iii) is proportionate; (iv) complies with all applicable laws and ordinances; and (3) the requirements of the applicable FEMA zone and corresponding requirements under the City of Sacramento's Floodplain Ordinance shall be satisfied prior to the issuance of building permits for the project. Any future homeowners within the floodzone shall maintain federal flood insurance, as required under the applicable FEMA and City of Sacramento Floodplain Management Ordinance regulations. The above measures shall terminate upon the first recertification of the levees by the U.S. Army Corps of Engineers.

## FINDINGS

All additional significant environmental effects of the project relating to Hydrology and Water Quality would be mitigated to a less-than-significant level.

Issues:	Effect remains significant with all identified mitigation	Effect can be mitigated to less than significant	No additional significant environmental effect
<b>9. NOISE</b> Would the project result in:			
A) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?		X	
B) Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?			X
C) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?			X
D) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?			X
E) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?			X
F) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?			X

**ENVIRONMENTAL SETTING**

The project site is located in the North Natomas area of the City of Sacramento. The MEIR identified noise levels expected at the site in the range of 70 dB. (MEIR, Figure 6.8-10)

**STANDARDS OF SIGNIFICANCE**

Thresholds of significance are those established by the Title 24 standards and by the City's General Plan Noise Policies and the City Noise Ordinance. Noise and vibration impacts resulting from the implementation of the proposed project would be considered significant if they cause any of the following results:

- Exterior noise levels at the proposed project exceeding the upper value of the normally acceptable category for various land uses caused by noise level increases due to the project. (2030 General Plan, Table EC-1, 2009).
- Residential interior noise levels of  $L_{dn}$  45 dB or greater caused by noise level increases due to the project;
- Construction noise levels not in compliance with the City of Sacramento Noise Ordinance;
- Occupied existing and project residential and commercial areas are exposed to vibration peak particle velocities greater than 0.5 inches per second due to project construction;
- Project residential and commercial areas are exposed to vibration peak particle velocities greater than 0.5 inches per second due to highway traffic and rail operations; and
- Historic buildings and archaeological sites are exposed to vibration peak particle velocities greater than 0.25 inches per second due to project construction, highway traffic, and rail operations.

## ANSWERS TO CHECKLIST QUESTIONS

### QUESTION A

The primary noise source at the project site is Interstate 5. The majority of the project site is within the 65 dB to 70 dB noise contour for cumulative conditions.

The parcel to the west of the project site is currently proposed for development as a hotel, which would partially shield the project site from traffic noise. The project includes community space and swimming pool that are completely shielded by adjacent three-story residential units. Other open space within the project includes paseos and walkways that are likewise shielded from traffic noise.

Residential units with facades facing west could be exposed to traffic noise from Interstate 5, and interior spaces of such units could be exposed to noise that exceeds the applicable threshold. This is a significant impact.

Typical façade design and construction with prevailing industry practice would result in an exterior-to-interior noise reduction of 28 to 38 dB  $L_{dn}$  with windows and doors in the closed position. This would be sufficient to achieve interior noise levels that are less than the applicable threshold. Mitigation Measure Noise 1, below, required installation of windows rated STC 32, which would provide sufficient noise reduction to ensure that appropriate interior noise levels are achieved. Mitigation Measure Noise 2 requires installation of mechanical ventilation in residential units to ensure that residents have the ability to achieve isolation from exterior noise. With the implementation of these mitigation measures the effects would be reduced to a less-than significant level.

### QUESTION B THROUGH F

The project site has been completely graded and has been improved with subdivision and utility infrastructure. The project would not require additional grading, and construction noise impacts would be less than significant.

The project would generate noise commonly associated with residential activities, and any such impacts would be less than significant. The primary noise source in the area is Interstate 5, and mitigation measures identified for the project would reduce any such impacts to a less-than-significant level.

**MITIGATION MEASURES**

- Noise 1:** STC 32 rated window assemblies shall be installed in all second and third floor windows with a direct view of East Commerce Way.
- Noise 2:** Each residential unit shall include a mechanical ventilation system to allow occupants to keep windows and doors closed to achieve isolation from exterior noise sources.

**Findings**

All additional significant environmental effects of the project relating to Noise can be mitigated to a less-than-significant level.

Issues:	Effect remains significant with all identified mitigation	Effect can be mitigated to less than significant	No additional significant environmental effect
<b>10. PUBLIC SERVICES</b> Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:			X
A) Fire protection?			
B) Police protection?			X
C) Schools?			X
D) Parks?			X
E) Other public facilities?			X

**Environmental Setting**

**STANDARDS OF SIGNIFICANCE**

For the purposes of this report, an impact would be considered significant if the project resulted in the need for new or altered services related to fire protection, police protection, school facilities, roadway maintenance, or other governmental services.

**ANSWERS TO CHECKLIST QUESTIONS**

**QUESTION A THROUGH E**

The land use designation for the project is consistent with the 2030 General Plan land use designation. Impacts from development, including cumulative impacts, have been analyzed in the 2030 General Plan MEIR. The project does not include features that would generate unusual demands on public services and would not have any additional significant environmental effects.

**MITIGATION MEASURES**

None required.

**FINDINGS**

The project would have no additional significant environmental effects relating to Public Services.

Issues:	Effect remains significant with all identified mitigation	Effect can be mitigated to less than significant	No additional significant environmental effect
11. <u>RECREATION</u>			
A) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?			X
B) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?			X

**STANDARDS OF SIGNIFICANCE**

Impacts to recreational resources are considered significant if the proposed project would do either of the following:

- cause or accelerate substantial physical deterioration of existing area parks or recreational facilities; or
- create a need for construction or expansion of recreational facilities beyond what was anticipated in the General or Community Plan.

**ANSWERS TO CHECKLIST QUESTIONS**

**QUESTIONS A AND B**

The project would be required to dedicate land for parks consistent with the development practices of the City of Sacramento and the requirements of state law. The project does not include any unusual components that would require additional recreational facilities beyond those included within the City’s normal planning processes. The proposed development is consistent with the General Plan land use designation, and the impacts of the proposed project on recreational facilities have been adequately addressed in the MEIR.

**MITIGATION MEASURES**

None required.

**FINDINGS**

The proposed project would have no additional significant environmental effects on recreational resources.

Issues:	Effect remains significant with all identified mitigation	Effect can be mitigated to less than significant	No additional significant environmental effect
<b>12. TRANSPORTATION AND CIRCULATION</b>			
Would the project:			
A) Cause an increase in traffic which is substantial in relation to the existing traffic load and capacity of the street system (i.e., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections?			X
B) Exceed, either individually or cumulatively, a level of service standard established by the county congestion management agency for designated roads or highways?			X
C) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?			X
D) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?			X
E) Result in inadequate emergency access?			X
F) Result in inadequate parking capacity?			X
G) Conflict with adopted policies, plans, or programs supporting alternative transportations (e.g., bus turnouts, bicycle racks)?			X

**STANDARDS OF SIGNIFICANCE**

The standards of significance for Transportation utilize policies in the 2030 General Plan, Mobility Element and, when appropriate, standards used by regulatory agencies. For traffic flow on the freeway system, the standards of Caltrans have been used.

Roadway Segments

A significant traffic impact occurs for roadway segments when:

1. The traffic generated by a project degrades peak period Level of Service (LOS) from A,B,C or D (without the project) to E or F (with project); or

2. The LOS (without project) is E or F, and project generated traffic increases the Volume-to-Capacity Ratio (V/C ratio) by 0.02 or more.

### **Intersections**

A significant traffic impact occurs for intersections when:

1. The traffic generated by a project degrades peak period level of service from A, B, C or D (without project) to E or F (with project); or
2. The LOS (without project) is E or F, and project generated traffic increases the peak period average vehicle delay by five seconds or more.

### **Freeway Facilities**

Caltrans considers the following to be significant impacts:

- Off-ramps with vehicle queues that extend into the ramp's deceleration area or onto the freeway;
- Project traffic increases that cause any ramp's merge/diverge level of service to be worse than the freeway's level of service;
- Project traffic increases that cause the freeway level of service to deteriorate beyond level of service threshold defined in the Caltrans Route Concept Report for the facility; or
- The expected ramp queue is greater than the storage capacity.

### **Transit**

Impacts to the transit system are considered significant if the proposed project would:

- Adversely affect public transit operations or
- Fail to adequately provide for access to public transit.

### **Bicycle Facilities**

Impacts to bicycle facilities are considered significant if the proposed project would:

- Adversely affect bicycle travel, bicycle paths or
- Fail to adequately provide for access by bicycle.

### **Pedestrian Circulation**

Impacts to pedestrian circulation are considered significant if the proposed project would:

- adversely affect pedestrian travel, pedestrian paths or
- fail to adequately provide for access by pedestrians.

## **Parking**

Impacts to parking are considered significant if the proposed project would eliminate or adversely affect an existing parking facility, interfere with the implementation of a proposed parking facility, or result in an inadequate supply of parking.

## **ANSWERS TO CHECKLIST QUESTIONS**

### **QUESTIONS A THROUGH G**

The project site was approved in 2006 for the development 187 townhouses. The proposed project would construct 237 condominiums instead of the 187 townhouses. The change in trip generation between the two uses would have negligible impacts. The project is consistent 2030 General Plan land use designations and the densities and intensities of uses for the project site, and would pay all fees per the North Natomas Finance Plan.

The trips generated from the additional 50 condominiums would not affect the prior conclusion of no additional significant environmental affects considered in the Master Environmental Impact Report.

The nearest bus service is provided on Truxel Road (in-between Del Paso Boulevard and North Market Boulevard) by Regional Transit Route 11, 13 and 14. Route 11, 13 and 14 connects at the Arden/Del Paso light rail station, Blue Line which provides routes from the Watt/I-80 light rail station to the Meadowview light rail station. The proposed project would not interfere with existing modes of alternative transportation or decrease the level of service provided by Regional Transit or Conflict with adopted policies, plans, or programs supporting alternative transportations.

### **MITIGATION MEASURES**

None required.

### **FINDINGS**

The project would have no additional significant environmental effects relating to Transportation and Circulation.

Issues:	Effect remains significant with all identified mitigation	Effect can be mitigated to less than significant	No additional significant environmental effect
<b>13. UTILITIES AND SERVICE SYSTEMS</b>			
Would the project:			
A) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?			X
B) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?			X
C) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?			X
D) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?			X
E) Result in a determination by the wastewater treatment provider which serves or may serve the project's projected demand in addition to the provider's existing commitments?			X
F) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid water disposal needs?			X
G) Comply with federal, state, and local statutes and regulations related to solid waste?			X

**ENVIRONMENTAL SETTING**

The project site is located in the North Natomas area of the City of Sacramento. The site is fully served with urban services. The proposed development is consistent with the land use designation for the project site in the 2030 General Plan.

**STANDARDS OF SIGNIFICANCE**

For purposes of this environmental document, an impact is considered significant if the proposed project would:

- Result in a detriment to microwave, radar, or radio transmissions;
- Create an increase in water demand of more than 10 million gallons per day;
- Substantially degrade water quality;
- Generate more than 500 tons of solid waste per year; or
- Generate stormwater that would exceed the capacity of the stormwater system.

**ANSWERS TO CHECKLIST QUESTIONS**

**QUESTION A THROUGH G**

The proposed project is consistent with 2030 General Plan land use designation for the project site. The MEIR prepared for the 2030 General Plan evaluated the impacts of development as proposed under the 2030 General Plan. The proposed project includes no components that would generate environmental effects that were not considered in the MEIR.

**MITIGATION MEASURES**

None required.

**FINDINGS**

The project would have no additional significant environmental effects relating to Utilities and Service Systems.

**MANDATORY FINDINGS OF SIGNIFICANCE**

Issues:	Effect remains significant with all identified mitigation	Effect can be mitigated to less than significant	No additional significant environmental effect
<b>14. MANDATORY FINDINGS OF SIGNIFICANCE</b>			
A.) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?			X
B.) Does the project have impacts that are individually limited, but cumulatively considerable? (“Cumulatively considerable” means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.)			X
C.) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?			X

**Answers to Checklist Questions**

**QUESTIONS A THROUGH C**

The 2030 General Plan Master EIR evaluated the impacts of development as proposed under the 2030 General Plan. As an anticipated subsequent project identified and described in the Master EIR and consistent with the 2030 General Plan land use designation, density, and intensity of use for the project site, the proposed project includes no components that would generate environmental effects that were not considered in the MEIR.

## SECTION IV - ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

The environmental factors checked below would potentially be affected by this project.

X	Aesthetics		Hazards
	Air Quality	X	Noise
	Biological Resources		Public Services
X	Cultural Resources		Recreation
	Energy and Mineral Resources		Transportation/Circulation
	Geology and Soils		Utilities and Service Systems
X	Hydrology and Water Quality		
	None Identified		

## SECTION V - DETERMINATION

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**On the basis of the initial study:**

I find that the Lead Agency for the 2030 General Plan Master EIR is the same as the Lead Agency for the proposed project, and that the proposed project (a) is within the scope of, and identified and described in, the 2030 general Plan Master EIR as an anticipated subsequent project; (b) is consistent with the 2030 General Plan land use designation and the permissible densities and intensities of use for the project site; and (c), **would not** have any additional significant environmental effects. Mitigation measures from the Master EIR will be applied to the project as appropriate. No new additional mitigation or alternatives are required. Notice shall be provided pursuant to CEQA Guidelines Section 15087. (CEQA Guidelines Section 15177(b))

- X I find that the proposed project is identified and described in the 2030 General Plan Master EIR as an anticipated subsequent project; that the discussions of cumulative impacts, growth inducing impacts, and irreversible significant effects in the Master EIR are adequate for the proposed project; that the proposed project is consistent with the 2030 General Plan land use designation and the permissible densities and intensities of use for the project site and that the proposed project **would** have additional significant environmental effects. Mitigation measures from the Master EIR will be applied to the project as appropriate and additional mitigation to avoid or mitigate the identified effects to a level of insignificance is required as set forth in this Initial Study. A Mitigated Negative Declaration will be prepared and circulated for public comment. Feasible mitigation measures to avoid or mitigate the identified effect to a level of insignificance will be incorporated to revise the project before the negative declaration is circulated for public review. (CEQA Guidelines Section 15178(b))

I find that the proposed project was not identified and described in the 2030 General Plan Master EIR as an anticipated subsequent project. All applicable mitigation measures from the Master EIR have been incorporated in the project, and all environmental effects have been reduced to a less-than-significant level. The discussions of cumulative impacts, growth inducing impacts and irreversible significant effects in the Master EIR are adequate for the project. A Mitigated Negative Declaration will be prepared and circulated for public comment. Feasible mitigation measures will be incorporated to revise the project before the negative declaration is circulated for public review. (CEQA Guidelines Section 15178(b)(2))

I find that the proposed project was not identified and described in the 2030 general Plan Master EIR as an anticipated subsequent project.

I find that the proposed project may have a significant effect on the environment, and an environmental impact report is required.

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Signature

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Date

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Printed Name

## REFERENCES CITED

Air Resources Board, GHG Emission Inventory Summary (1990-2004)

[http://www.arb.ca.gov/app/ghg/ghg\\_sector\\_data.php](http://www.arb.ca.gov/app/ghg/ghg_sector_data.php)

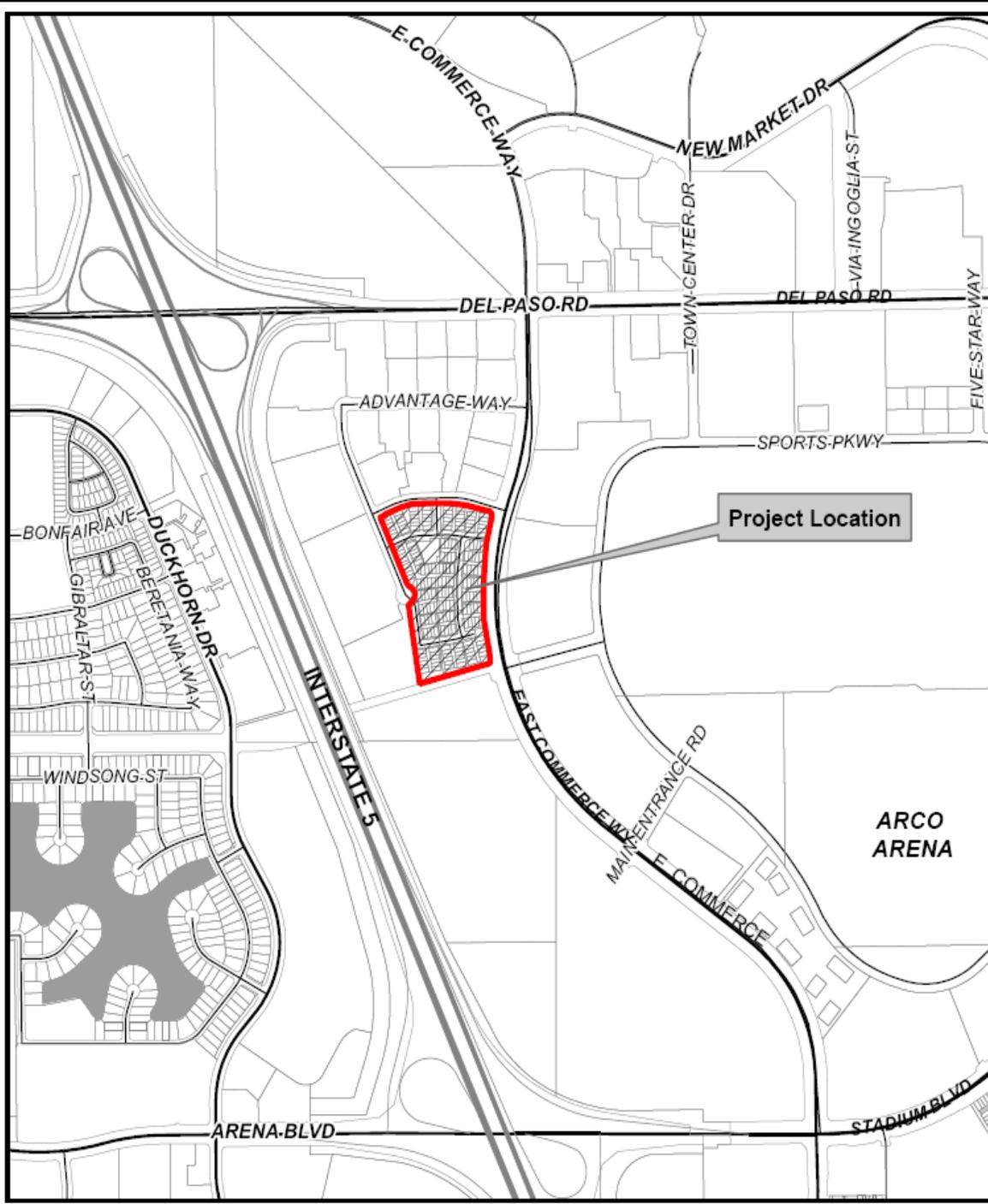
City of Sacramento. 2009 *2030 General Plan*.

City of Sacramento. 2008. *Sacramento 2030 General Plan Master Environmental Impact Report*

City of Sacramento, Department of Utilities. 2007. *Table 3-2 Stormwater Quality Control Measure Selection Matrix in the Stormwater Quality Design Manual*.

Institute for Transportation Engineers, Trip Generation 7<sup>th</sup> Edition

Sacramento Metropolitan Air Quality Management District (SMAQMD) 2004. *Guide to Air Quality Assessment in Sacramento County*.



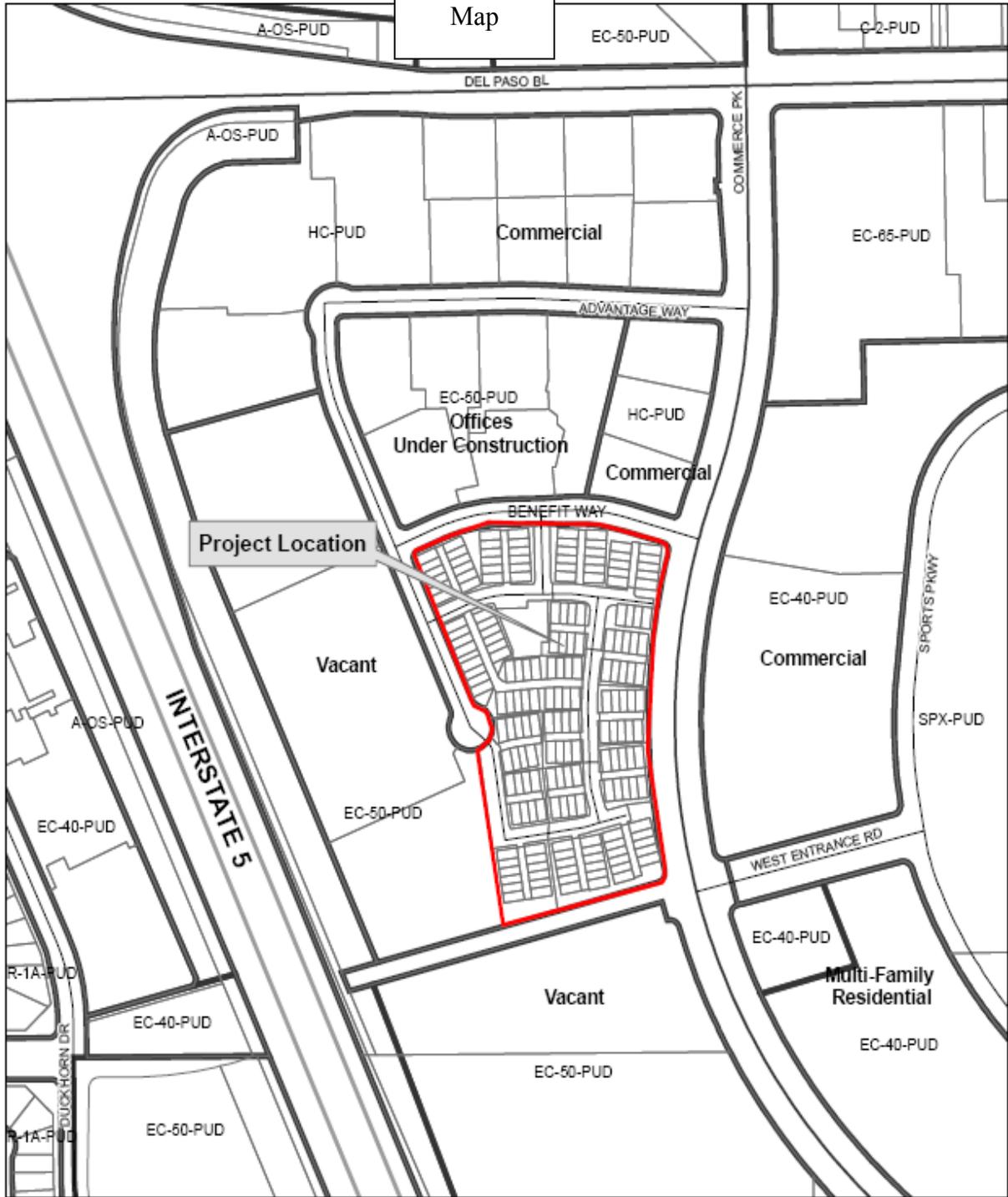
Attachment  
A  
Vicinity  
Map

# P09-006 Vicinity Map Provence

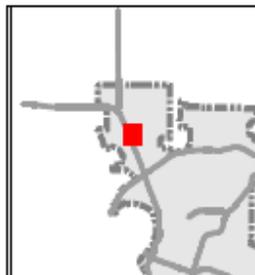


D. Hung | March 2009

Attachment  
B  
Land Use  
Map



City of Sacramento, Development Services Department Geographic Information System



**P09-006**  
**Land Use & Zoning Map**  
**Provence**

0 250 500 Feet



D. Hung | March 2009



Attachment D  
Summer and Winter Emission  
Reports

Page: 1

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Urbemis 2007 Version 9.2.4

Combined Summer Emissions Reports (Pounds/Day)

File Name:

Project Name: Provance

Project Location: Sacramento County AQMD

On-Road Vehicle Emissions Based on: Version : Emfac2007 V2.3 Nov 1 2006

Off-Road Vehicle Emissions Based on: OFFROAD2007

Summary Report:

CONSTRUCTION EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10 Dust</u>	<u>PM10 Exhaust</u>	<u>PM10</u>	<u>PM2.5 Dust</u>	<u>PM2.5 Exhaust</u>	<u>PM2.5</u>
2007 TOTALS (lbs/day unmitigated)	3.56	29.70	15.53	0.00	74.00	1.49	75.50	15.46	1.37	16.83
2008 TOTALS (lbs/day unmitigated)	282.64	52.12	52.80	0.03	74.12	2.99	77.11	15.50	2.75	18.24

AREA SOURCE EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
TOTALS (lbs/day, unmitigated)	12.58	2.28	2.51	0.00	0.01	0.01	2,893.02

OPERATIONAL (VEHICLE) EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
TOTALS (lbs/day, unmitigated)	17.63	16.26	197.93	0.15	24.12	4.67	15,169.08

SUM OF AREA SOURCE AND OPERATIONAL EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
TOTALS (lbs/day, unmitigated)	30.21	18.54	200.44	0.15	24.13	4.68	18,062.10

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## Construction Unmitigated Detail Report:

## CONSTRUCTION EMISSION ESTIMATES Summer Pounds Per Day, Unmitigated

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10 Dust</u>	<u>PM10 Exhaust</u>	<u>PM10</u>	<u>PM2.5 Dust</u>	<u>PM2.5 Exhaust</u>	<u>PM2.5</u>
Time Slice 11/30/2007-12/27/2007	<u>3.58</u>	<u>29.70</u>	<u>15.53</u>	<u>0.00</u>	<u>74.00</u>	<u>1.49</u>	<u>75.50</u>	<u>15.46</u>	<u>1.37</u>	<u>16.83</u>
Active Days: 20										
Fine Grading 11/30/2007-01/11/2008	3.58	29.70	15.53	0.00	74.00	1.49	75.50	15.46	1.37	16.83
Fine Grading Dust	0.00	0.00	0.00	0.00	74.00	0.00	74.00	15.45	0.00	15.45
Fine Grading Off Road Diesel	3.52	29.64	14.18	0.00	0.00	1.49	1.49	0.00	1.37	1.37
Fine Grading On Road Diesel	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Fine Grading Worker Trips	0.04	0.06	1.36	0.00	0.00	0.00	0.01	0.00	0.00	0.00
Time Slice 12/28/2007-12/31/2007	<u>3.58</u>	<u>29.70</u>	<u>15.53</u>	<u>0.00</u>	<u>74.00</u>	<u>1.49</u>	<u>75.50</u>	<u>15.46</u>	<u>1.37</u>	<u>16.83</u>
Active Days: 2										
Asphalt 12/28/2007-01/11/2008	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Paving Off-Gas	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Paving Off Road Diesel	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Paving On Road Diesel	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Paving Worker Trips	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Fine Grading 11/30/2007-01/11/2008	3.58	29.70	15.53	0.00	74.00	1.49	75.50	15.46	1.37	16.83
Fine Grading Dust	0.00	0.00	0.00	0.00	74.00	0.00	74.00	15.45	0.00	15.45
Fine Grading Off Road Diesel	3.52	29.64	14.18	0.00	0.00	1.49	1.49	0.00	1.37	1.37
Fine Grading On Road Diesel	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Fine Grading Worker Trips	0.04	0.06	1.36	0.00	0.00	0.00	0.01	0.00	0.00	0.00
Time Slice 1/1/2008-1/10/2008 Active Days: 8	3.35	28.06	14.83	0.00	74.00	1.41	75.42	15.46	1.30	16.76
Asphalt 12/28/2007-01/11/2008	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Paving Off-Gas	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Paving Off Road Diesel	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

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Paving On Road Diesel	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Paving Worker Trips	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Fine Grading 11/30/2007-01/11/2008	3.35	28.06	14.83	0.00	74.00	1.41	75.42	15.46	1.30	16.76
Fine Grading Dust	0.00	0.00	0.00	0.00	74.00	0.00	74.00	15.45	0.00	15.45
Fine Grading Off Road Diesel	3.31	28.00	13.56	0.00	0.00	1.41	1.41	0.00	1.30	1.30
Fine Grading On Road Diesel	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Fine Grading Worker Trips	0.04	0.06	1.27	0.00	0.00	0.00	0.01	0.00	0.00	0.00
Time Slice 1/11/2008-1/11/2008	8.46	<u>52.12</u>	<u>52.80</u>	0.03	<u>74.12</u>	<u>2.99</u>	<u>77.11</u>	<u>15.50</u>	<u>2.75</u>	<u>18.24</u>
Active Days: 1										
Asphalt 12/28/2007-01/11/2008	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Paving Off-Gas	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Paving Off Road Diesel	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Paving On Road Diesel	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Paving Worker Trips	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Building 01/11/2008-08/22/2008	5.10	24.06	37.97	0.03	0.12	1.58	1.69	0.04	1.44	1.49
Building Off Road Diesel	4.07	18.22	11.80	0.00	0.00	1.33	1.33	0.00	1.22	1.22
Building Vendor Trips	0.39	4.86	4.43	0.01	0.03	0.20	0.24	0.01	0.19	0.20
Building Worker Trips	0.64	0.98	21.73	0.02	0.09	0.04	0.13	0.03	0.03	0.06
Fine Grading 11/30/2007-01/11/2008	3.35	28.06	14.83	0.00	74.00	1.41	75.42	15.46	1.30	16.76
Fine Grading Dust	0.00	0.00	0.00	0.00	74.00	0.00	74.00	15.45	0.00	15.45
Fine Grading Off Road Diesel	3.31	28.00	13.56	0.00	0.00	1.41	1.41	0.00	1.30	1.30
Fine Grading On Road Diesel	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Fine Grading Worker Trips	0.04	0.06	1.27	0.00	0.00	0.00	0.01	0.00	0.00	0.00
Time Slice 1/14/2008-8/7/2008 Active Days: 149	5.10	24.06	37.97	0.03	0.12	1.58	1.69	0.04	1.44	1.49
Building 01/11/2008-08/22/2008	5.10	24.06	37.97	0.03	0.12	1.58	1.69	0.04	1.44	1.49

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Building Off Road Diesel	4.07	18.22	11.80	0.00	0.00	1.33	1.33	0.00	1.22	1.22
Building Vendor Trips	0.39	4.86	4.43	0.01	0.03	0.20	0.24	0.01	0.19	0.20
Building Worker Trips	0.64	0.98	21.73	0.02	0.09	0.04	0.13	0.03	0.03	0.06
Time Slice 8/8/2008-8/22/2008 Active Days: 11	<b>282.64</b>	24.25	42.09	<b>0.03</b>	0.13	1.58	1.72	0.05	1.45	1.50
Building 01/11/2008-08/22/2008	5.10	24.06	37.97	0.03	0.12	1.58	1.69	0.04	1.44	1.49
Building Off Road Diesel	4.07	18.22	11.80	0.00	0.00	1.33	1.33	0.00	1.22	1.22
Building Vendor Trips	0.39	4.86	4.43	0.01	0.03	0.20	0.24	0.01	0.19	0.20
Building Worker Trips	0.64	0.98	21.73	0.02	0.09	0.04	0.13	0.03	0.03	0.06
Coating 08/08/2008-09/05/2008	277.54	0.19	4.12	0.00	0.02	0.01	0.02	0.01	0.01	0.01
Architectural Coating	277.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Coating Worker Trips	0.12	0.19	4.12	0.00	0.02	0.01	0.02	0.01	0.01	0.01
Time Slice 8/25/2008-9/5/2008 Active Days: 10	277.54	0.19	4.12	0.00	0.02	0.01	0.02	0.01	0.01	0.01
Coating 08/08/2008-09/05/2008	277.54	0.19	4.12	0.00	0.02	0.01	0.02	0.01	0.01	0.01
Architectural Coating	277.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Coating Worker Trips	0.12	0.19	4.12	0.00	0.02	0.01	0.02	0.01	0.01	0.01

Phase Assumptions

Phase: Fine Grading 11/30/2007 - 1/11/2008 - Default Fine Site Grading Description

Total Acres Disturbed: 13.07

Maximum Daily Acreage Disturbed: 3.7

Fugitive Dust Level of Detail: Default

20 lbs per acre-day

On Road Truck Travel (VMT): 0

Off-Road Equipment:

1 Graders (174 hp) operating at a 0.61 load factor for 6 hours per day

1 Rubber Tired Dozers (357 hp) operating at a 0.59 load factor for 6 hours per day

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1 Tractors/Loaders/Backhoes (108 hp) operating at a 0.55 load factor for 7 hours per day

1 Water Trucks (189 hp) operating at a 0.5 load factor for 8 hours per day

Phase: Paving 12/28/2007 - 1/11/2008 - Default Paving Description

Acres to be Paved: 0

Off-Road Equipment:

Phase: Building Construction 1/11/2008 - 8/22/2008 - Default Building Construction Description

Off-Road Equipment:

1 Cranes (399 hp) operating at a 0.43 load factor for 6 hours per day

2 Forklifts (145 hp) operating at a 0.3 load factor for 6 hours per day

1 Generator Sets (49 hp) operating at a 0.74 load factor for 8 hours per day

1 Tractors/Loaders/Backhoes (108 hp) operating at a 0.55 load factor for 8 hours per day

3 Welders (45 hp) operating at a 0.45 load factor for 8 hours per day

Phase: Architectural Coating 8/8/2008 - 9/5/2008 - Default Architectural Coating Description

Rule: Residential Interior Coatings begins 1/1/2005 ends 12/31/2040 specifies a VOC of 250

Rule: Residential Exterior Coatings begins 1/1/2005 ends 12/31/2040 specifies a VOC of 250

Rule: Nonresidential Interior Coatings begins 1/1/2005 ends 12/31/2040 specifies a VOC of 250

Rule: Nonresidential Exterior Coatings begins 1/1/2005 ends 12/31/2040 specifies a VOC of 250

#### Area Source Unmitigated Detail Report:

AREA SOURCE EMISSION ESTIMATES Summer Pounds Per Day, Unmitigated

<u>Source</u>	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
Natural Gas	0.17	2.26	0.96	0.00	0.00	0.00	2,890.21
hearth - No Summer Emissions							
andscape	0.12	0.02	1.55	0.00	0.01	0.01	2.81

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Consumer Products	10.70						
Architectural Coatings	1.59						
<b>TOTALS (lbs/day, unmitigated)</b>	<b>12.58</b>	<b>2.28</b>	<b>2.51</b>	<b>0.00</b>	<b>0.01</b>	<b>0.01</b>	<b>2,893.02</b>

Area Source Changes to Defaults

Percentage of residences with wood stoves changed from 35% to 0%

Percentage of residences with natural gas fireplaces changed from 65% to 100%

Operational Unmitigated Detail Report:

OPERATIONAL EMISSION ESTIMATES Summer Pounds Per Day, Unmitigated

<u>Source</u>	ROG	NOX	CO	SO2	PM10	PM25	CO2
Condo/townhouse general	17.63	16.26	197.93	0.15	24.12	4.67	15,169.08
<b>TOTALS (lbs/day, unmitigated)</b>	<b>17.63</b>	<b>16.26</b>	<b>197.93</b>	<b>0.15</b>	<b>24.12</b>	<b>4.67</b>	<b>15,169.08</b>

Operational Settings:

Does not include correction for passby trips

Does not include double counting adjustment for internal trips

Analysis Year: 2010 Temperature (F): 95 Season: Summer

Emfac: Version : Emfac2007 V2.3 Nov 1 2006

Summary of Land Uses

Land Use Type	Acreage	Trip Rate	Unit Type	No. Units	Total Trips	Total VMT
Condo/townhouse general	14.81	6.90	dwelling units	237.00	1,635.30	13,981.32
					1,635.30	13,981.32

Vehicle Fleet Mix

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Vehicle Type	Percent Type	Non-Catalyst	Catalyst	Diesel
Light Auto	47.7	1.3	98.3	0.4
Light Truck < 3750 lbs	10.0	3.0	90.0	7.0
Light Truck 3751-5750 lbs	22.5	0.9	98.7	0.4
Med Truck 5751-8500 lbs	10.1	1.0	99.0	0.0
Lite-Heavy Truck 8501-10,000 lbs	2.1	0.0	76.2	23.8
Lite-Heavy Truck 10,001-14,000 lbs	0.9	0.0	55.6	44.4
Med-Heavy Truck 14,001-33,000 lbs	1.6	6.2	18.8	75.0
Heavy-Heavy Truck 33,001-60,000 lbs	0.5	0.0	0.0	100.0
Other Bus	0.1	0.0	0.0	100.0
Urban Bus	0.0	0.0	0.0	0.0
Motorcycle	3.5	65.7	34.3	0.0
School Bus	0.1	0.0	0.0	100.0
Motor Home	0.9	0.0	88.9	11.1

Travel Conditions

	Residential			Commercial		
	Home-Work	Home-Shop	Home-Other	Commute	Non-Work	Customer
Urban Trip Length (miles)	10.8	7.3	7.5	10.8	7.3	7.3
Rural Trip Length (miles)	15.0	10.0	10.0	15.0	10.0	10.0
Trip speeds (mph)	35.0	35.0	35.0	35.0	35.0	35.0
% of Trips - Residential	32.9	18.0	49.1			

% of Trips - Commercial (by land use)

CO2

2,358.81

6,777.43

CO2

2,358.81

2,358.81

0.00

2,247.32

0.00

111.49

2,358.81

0.00

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0.00  
0.00  
0.00  
4,418.62  
1,621.20  
894.96  
1,902.46  
2,358.81  
0.00  
2,247.32  
0.00  
111.49  
4,418.62  
4,418.62

1,621.20  
894.96  
1,902.46  
4,779.58  
4,418.62  
1,621.20  
894.96  
1,902.46  
360.96  
0.00  
360.96  
360.96  
360.96  
360.96  
0.00  
360.96

Urbemis 2007 Version 9.2.4  
 Combined Winter Emissions Reports (Pounds/Day)

File Name:

Project Name: Provance

Project Location: Sacramento County AQMD

On-Road Vehicle Emissions Based on: Version : Emfac2007 V2.3 Nov 1 2006

Off-Road Vehicle Emissions Based on: OFFROAD2007

Summary Report:

CONSTRUCTION EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10 Dust</u>	<u>PM10 Exhaust</u>	<u>PM10</u>	<u>PM2.5 Dust</u>	<u>PM2.5</u>	<u>PM2.5</u>
2007 TOTALS (lbs/day unmitigated)	3.56	29.70	15.53	0.00	74.00	1.49	75.50	15.46	1.37	16.83
2008 TOTALS (lbs/day unmitigated)	282.64	52.12	52.80	0.03	74.12	2.99	77.11	15.50	2.75	18.24

AREA SOURCE EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
TOTALS (lbs/day, unmitigated)	12.54	3.57	1.52	0.01	0.11	0.10	4,563.15

OPERATIONAL (VEHICLE) EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
TOTALS (lbs/day, unmitigated)	15.00	24.35	168.98	0.12	24.12	4.67	12,264.68

SUM OF AREA SOURCE AND OPERATIONAL EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
TOTALS (lbs/day, unmitigated)	27.54	27.92	170.50	0.13	24.23	4.77	16,827.83

CO2

2,358.81

6,777.43

CO2

2,358.81

6,777.43



## Description/Analysis

**Issue:** Entitlements were previously approved to develop a 187 unit townhouse complex at the subject site (P06-194) within the Natomas Crossing – Area #3 Planned Unit Development (PUD). The site is currently developed with five of the 187 units, along with interior streets and the pool and pool house. The current application request is for the necessary entitlements to develop 233 condominium units within the undeveloped areas of the site on approximately 13.9 gross acres (10.8 net acres) in the Employment Center 50 Planned Unit Development (EC-50-PUD) zone. The project requires a PUD Schematic Plan Amendment, a Tentative Map, a Special Permit for condominiums and a Special Permit Major Modification to amend the previous approval. Since the proposal reflects an increase in density of more than 10% from the previous approval, the project requires approval of the City Council.

### Policy Considerations:

General Plan: The 2030 General Plan Update was adopted by City Council on March 3, 2009. The 2030 General Plan's goals, policies, and implementation programs define a roadmap to achieving Sacramento's vision to be the most livable city in America. The 2030 General Plan Update designation of the subject site is Employment Center Mid Rise which provides for large mixed-use office/employment centers that includes residential uses as a supportive mixed use or adjacent to large employment center; this designation allows a minimum density of 18 units per net acre to a maximum of 60 units per net acre. The 2030 General Plan has identified goals and policies under the Land Use and Urban Design Element as well as the Housing Element. Some of the goals and policies supported by this project are:

- *Citywide Land Use and Urban Design. (Policy LU 7.1.2) Housing in Employment Centers.* The City shall require compatible integration of housing in existing and proposed employment centers to help meet housing needs and reduce vehicle trips and commute times, where such development will not compromise the City's ability to attract and maintain employment-generating uses.
- *Housing Element. Housing Diversity (Goal H-1.2)* Provide a variety of quality housing types to encourage neighborhood stability.
- *Housing Element. Balanced Communities (Goal H-1.3)* Promote racial, economic, and demographic integration in new and existing neighborhoods

The project, with a density of 21 units per net acre, is within the range of 18 to 60 units per net acre of the General Plan land use designation. The proposed project meets the 2030 General Plan goals and policies related to Citywide Land Use and Urban Design and the development of Employment Center Mid Rise.

Smart Growth Principles: City Council adopted a set of Smart Growth Principles in

December 2001 to encourage development patterns that are sustainable and balanced in terms of economic objectives, social goals, and use of environmental/natural resources. The project, which is 233 condominium units in the Employment Center land use designation, helps create a range of housing opportunities and choices, fosters walkable, close-knit neighborhoods and promotes distinctive, attractive communities with a strong sense of place.

Strategic Plan Implementation: The recommended action conforms with the City of Sacramento's Strategic Plan, specifically by adhering to goals that achieve sustainability, enhance livability, and expand economic development throughout the City.

### **Environmental Considerations:**

**California Environmental Quality Act (CEQA):** The City of Sacramento prepared a Mitigated Negative Declaration (MND) for the Provence project. In accordance with the California Environmental Quality Act (CEQA), the MND was submitted to a 20-day public review period from May 8, 2009 through May 28, 2009. The Notice of Availability was advertised in a newspaper of general circulation, posted with the Sacramento County Clerk and sent to stakeholders in the project area. No comments were received during circulation for public comment.

The following sections were identified to have potentially significant impacts: Hydrology and Noise. Mitigation measures were incorporated into the project description to reduce these impacts to a less-than-significant level, consistent with CEQA Guidelines Section 15070.

Following preparation of the MND and circulation for public comment, the project was revised to reduce the dwelling unit count from 237 to 233, along with some minor alterations in layout. The reduction in dwelling unit count would have no effects that were not identified and evaluated in the MND, and no changes to the environmental document are required. Recirculation of the MND is not required pursuant to CEQA Guidelines Section 15073.5.

The MND is currently posted on the Community Development Department's web site at: <http://www.cityofsacramento.org/dsd/planning/environmental-review/eirs/>

**Sustainability Considerations:** The City has adopted a Sustainability Master Plan to complement the City's General Plan. This was done to ensure that the City set the standard for the practices of sustainability within its own organization as well as becoming a model for any construction projects within the City. Projects should consider the following goals adopted by the City as projects are proposed within the City: 1) Reduce consumption of material and encourage the reuse and local recycling of materials; 2) Reduce the use of toxic materials; 3) Establish and continuously improve "green" building standards for both

residential and commercial development--new and remodeled; 4) Reduce dependence on the private automobile by working with community partners to provide efficient and accessible public transit and transit supportive land uses; 5) Reduce long commutes by providing a wide array of transportation and housing choices near jobs for a balanced, healthy city; 6) Improve the health of residents through access to a diverse mix of wellness activities and locally produced food, promote “greening” and “gardening” within the City; 7) Create “Healthy Urban Environments” through Restorative Redevelopment, and 8) Maintain and expand the urban forest.

The proposed project is consistent with the Sustainability Master Plan goals to reduce dependence on the private automobile due to its proximity to retail uses, reduce long commutes due to its proximity to downtown and nearby employment centers, and improve energy efficiency by adhering to “green” building standards.

**Commission/Committee Action:** On March 24, 2010, the Planning Commission forwarded to the City Council the recommendation for approval by a vote of nine ayes and zero noes (one abstention and one vacancy).

**Rationale for Recommendation:** Staff finds: 1) The project is consistent with the goals and policies of the 2030 General Plan, North Natomas Community Plan, and the Natomas Crossing – Area #3 Planned Unit Development (PUD) Guidelines and Schematic Plan; 2) The proposed use is consistent with the Employment Center zoning designation; and 3) The project contributes positively to the surrounding area.

**Financial Considerations:** This project has no fiscal considerations.

**Emerging Small Business Development (ESBD):** No goods or services are being purchased under this report.