

Environmental Impact (Significance Before Mitigation)	Mitigation Measures	Level of Significance After Mitigation	Findings of Fact
5.9-1 Phase 1 of the Re-Use Alternative could cause potentially significant impacts to study intersections. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.9-2 Phase 1 of the Re-Use Alternative could cause potentially significant impacts to study freeway ramps. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.9-3 Phase 1 of the Re-Use Alternative could cause potentially significant impacts to transit. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.9-4 Phase 1 of the Re-Use Alternative could cause potentially significant impacts to pedestrian facilities. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.9-5 Phase 1 of the Re-Use Alternative could cause potentially significant impacts to bicycle facilities. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.9-6 Phase 1 of the Re-Use Alternative could cause potentially significant impacts on parking. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)

No Impact = NI    Less than Significant = LS    Beneficial = B    Significant = S    Cumulative Significant = CS    Significant and Unavoidable = SU    Potentially Significant = PS

Environmental Impact (Significance Before Mitigation)	Mitigation Measures	Level of Significance After Mitigation	Findings of Fact
5.9-7 Phase 1 of the Re-Use Alternative could cause potentially significant impacts due to construction-related activities. (S)	5.9-7 Prior to the beginning of construction, the applicant shall prepare a construction traffic and parking management plan to the satisfaction of City Traffic Engineer and subject to review by all affected agencies. The plan shall ensure that acceptable operating conditions on local roadways and freeway facilities are maintained. At a minimum, the plan shall include: <ul style="list-style-type: none"> <li>• Description of trucks including: number and size of trucks per day, expected arrival/departure times, truck circulation patterns.</li> <li>• Description of staging area including: location, maximum number of trucks simultaneously permitted in staging area, use of traffic control personnel, specific signage.</li> <li>• Description of street closures including: duration, advance warning and posted signage, safe and efficient access routes for emergency vehicles, and use of manual traffic control.</li> <li>• Description of driveway access plan including: provisions for safe vehicular, pedestrian, and bicycle travel, minimum distance from any open trench, special signage, and private vehicle accesses.</li> </ul>	LS	<p><u>Finding:</u> Implementation of Mitigation Measure 5.9-7, which has been required or incorporated into the Re-Use Alternative, will reduce this impact to a less than significant level. The City Council hereby directs that this mitigation measure be adopted. The City Council, therefore, finds that changes or alterations have been required in, or incorporated into, the Re-Use Alternative that avoid this significant environmental effect identified in the Final EIR.</p> <p><u>Explanation:</u> Construction may include disruptions to the transportation network near the site, including the possibility of temporary lane closures, street closures, sidewalk closures, and bikeway closures. Pedestrian, bicycle, and transit access may be disrupted. Heavy vehicles will access the site and may need to be staged for construction. Mitigation Measure 5.9-7 requires the development of a construction traffic and parking management plan to ensure acceptable operating conditions on local roadways and freeway facilities. Implementation of this measure will reduce this significant impact to a less than significant level. (FEIR, vol. 1, p. 5.9-54.)</p>
5.9-8 Re-Use Alternative buildout could cause potentially significant impacts to study intersections. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.9-9 Re-Use Alternative buildout could cause potentially significant impacts	None required.	NA	Under CEQA, no mitigation measures are

No Impact = NI    Less than Significant = LS    Beneficial = B    Significant = S    Cumulative Significant = CS    Significant and Unavoidable = SU    Potentially Significant = PS

Environmental Impact (Significance Before Mitigation)	Mitigation Measures	Level of Significance After Mitigation	Findings of Fact
to study freeway ramps. (LS)			required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.9-10 Re-Use Alternative buildout could cause potentially significant impacts to transit. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.9-11 Re-Use Alternative buildout could cause potentially significant impacts to pedestrian facilities. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.9-12 Re-Use Alternative buildout could cause potentially significant impacts to bicycle facilities. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.9-13 Re-Use Alternative buildout could cause potentially significant impacts on parking. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.9-14 Re-Use Alternative buildout could cause potentially significant impacts due to construction-related activities. (S)	5.9-14 Implement Mitigation Measure 5.9-7.	LS	<u>Finding:</u> Implementation of Mitigation Measure 5.9-14, which has been required or incorporated into the Re-Use Alternative, will reduce this impact to a less than significant level. The City Council hereby directs that this mitigation measure be adopted. The City Council, therefore, finds that changes or alterations have been

No Impact = NI    Less than Significant = LS    Beneficial = B    Significant = S    Cumulative Significant = CS    Significant and Unavoidable = SU    Potentially Significant = PS

Environmental Impact (Significance Before Mitigation)	Mitigation Measures	Level of Significance After Mitigation	Findings of Fact
			<p>required in, or incorporated into, the Re-Use Alternative that avoid this significant environmental effect identified in the Final EIR.</p> <p><u>Explanation:</u> As with Phase I construction, construction of the later project phases may include disruptions to the transportation network near the site, including the possibility of temporary lane closures, street closures, sidewalk closures, and bikeway closures. Pedestrian, bicycle, and transit access may be disrupted. Heavy vehicles will access the site and may need to be staged for construction. Mitigation Measure 5.9-14 requires the development of a construction traffic and parking management plan to ensure acceptable operating conditions on local roadways and freeway facilities. Implementation of this measure will reduce this significant impact to a less than significant level. (FEIR, vol. 1, p. 5.9-54.)</p>
<p><b>5.9-15 The Re-Use Alternative would contribute to unacceptable peak hour operations at the W Street/9th Street intersection. (S)</b></p>	<p>5.9-15 The project applicant shall contribute its fair share toward restriping the southbound approach to the W Street/9<sup>th</sup> Street intersection to add an exclusive right-turn lane while maintaining the two existing through lanes and one existing shared through/right lane.</p>	<p>LS</p>	<p><u>Finding:</u> Implementation of Mitigation Measure 5.9-15, which has been required or incorporated into the Re-Use Alternative, will reduce this impact to a less than significant level. The City Council hereby directs that this mitigation measure be adopted. The City Council, therefore, finds that changes or alterations have been required in, or incorporated into, the Re-Use Alternative that avoid this significant environmental effect identified in the Final EIR.</p> <p><u>Explanation:</u> The Adaptive Re-Use</p>

No Impact = NI    Less than Significant = LS    Beneficial = B    Significant = S    Cumulative Significant = CS    Significant and Unavoidable = SU    Potentially Significant = PS

Environmental Impact (Significance Before Mitigation)	Mitigation Measures	Level of Significance After Mitigation	Findings of Fact
			<p>Alternative would generate approximately 471 fewer total trips than the proposed project; this alternative would also result in fewer AM and PM peak hour trips (28 and 30 fewer peak-hour trips, respectively) than the proposed project. (FEIR, vol. 1, p. 7-9.) Therefore, traffic impacts at the W Street / 9th Street intersection would be less than the proposed project.</p> <p>As discussed in the Final EIR, traffic modeling suggests that after buildout of the project cumulative PM peak hour operations at the signalized intersection of W Street/9th Street would be at LOS F. Since this intersection is in the Core Area as defined by the General Plan Mobility Element, LOS F may be acceptable during peak hours provided that the project provides improvements to other parts of the citywide transportation system within the project site vicinity (or within the area affected by the project's vehicular traffic impacts) to improve transportation-system-wide roadway capacity, to make intersection improvements, or to enhance non-auto travel modes in furtherance of the General Plan goals.</p> <p>Mitigation Measure 5.9-15 would improve PM peak hour operations at the W Street/9th Street intersection to an acceptable LOS C during the PM peak hour. Therefore, implementation of this measure will reduce this significant impact to a less than significant level. (FEIR, vol. 1, p. 5.9-57.)</p>

No Impact = NI    Less than Significant = LS    Beneficial = B    Significant = S    Cumulative Significant = CS    Significant and Unavoidable = SU    Potentially Significant = PS

Environmental Impact (Significance Before Mitigation)	Mitigation Measures	Level of Significance After Mitigation	Findings of Fact
<p>5.9-16 The Re-Use Alternative would contribute to unacceptable peak hour operations at the I-5 NB Off-Ramp/Broadway intersection and vehicular queuing that extends onto the freeway mainline. (S)</p>	<p>5.9-16 The project applicant shall contribute its fair share toward the installation of a traffic signal at the I-5 NB Off-Ramp/Broadway intersection.</p>	<p>LS</p>	<p><u>Finding:</u> Implementation of Mitigation Measure 5.9-16, which has been required or incorporated into the Re-Use Alternative, will reduce this impact to a less than significant level. The City Council hereby directs that this mitigation measure be adopted. The City Council, therefore, finds that changes or alterations have been required in, or incorporated into, the Re-Use Alternative that avoid this significant environmental effect identified in the Final EIR.</p> <p><u>Explanation:</u> The Adaptive Re-Use Alternative would generate approximately 471 fewer total trips than the proposed project; this alternative would also result in fewer AM and PM peak hour trips (28 and 30 fewer peak-hour trips, respectively) than the proposed project. (FEIR, vol. 1, p. 7-9.) Therefore, traffic impacts at the I-5 NB Off-Ramp / Broadway intersection and vehicular queuing extending onto the freeway mainline would be less than the proposed project.</p> <p>As discussed in the Final EIR, traffic modeling suggests cumulative AM peak hour operations at the side-street stop-controlled intersection of I 5 NB Off-Ramp/Broadway would be at LOS F and indicates that the queue from the ramp terminal intersection would exceed the available storage by 250 feet.</p> <p>Cumulative traffic volumes at this intersection would satisfy the peak hour warrant for consideration of a traffic signal.</p>

No Impact = NI    Less than Significant = LS    Beneficial = B    Significant = S    Cumulative Significant = CS    Significant and Unavoidable = SU    Potentially Significant = PS

Environmental Impact (Significance Before Mitigation)	Mitigation Measures	Level of Significance After Mitigation	Findings of Fact
			A traffic signal would improve AM peak hour operations at the I-5 Northbound Ramp/Broadway intersection to an acceptable LOS D during the AM peak hour, and reduce vehicular queuing so that it does not extend beyond the available storage. Therefore, implementation of this measure will reduce this significant impact to a less than significant level. (FEIR, vol. 1, p. 5.9-57.)
5.9-17 The Re-Use Alternative could cause potentially significant impacts to transit. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.9-18 The Re-Use Alternative could cause potentially significant impacts to pedestrian facilities. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.9-19 The Re-Use Alternative could cause potentially significant impacts to bicycle facilities. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.9-20 The Re-Use Alternative could cause potentially significant impacts on parking. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.9-21 The Re-Use Alternative could cause potentially significant impacts due to	5.9-21 Implement Mitigation Measure 5.9-7.	LS	<u>Finding:</u> Implementation of Mitigation Measure 5.9-21, which has been required or

No Impact = NI    Less than Significant = LS    Beneficial = B    Significant = S    Cumulative Significant = CS    Significant and Unavoidable = SU    Potentially Significant = PS

Environmental Impact (Significance Before Mitigation)	Mitigation Measures	Level of Significance After Mitigation	Findings of Fact
construction-related activities. (S)			<p>incorporated into the Re-Use Alternative, will reduce this impact to a less than significant level. The City Council hereby directs that this mitigation measure be adopted. The City Council, therefore, finds that changes or alterations have been required in, or incorporated into, the Re-Use Alternative that avoid this significant environmental effect identified in the Final EIR.</p> <p><u>Explanation:</u> Construction may include disruptions to the transportation network near the site, including the possibility of temporary lane closures, street closures, sidewalk closures, and bikeway closures. Pedestrian, bicycle, and transit access may be disrupted. Heavy vehicles will access the site and may need to be staged for construction. These activities could result in degraded roadway operating conditions.</p> <p>Mitigation Measure 5.9-21 requires the development of a construction traffic and parking management plan to ensure that project construction does not result in degraded roadway operating conditions. Implementation of this measure will reduce this significant impact to a less than significant level. (FEIR, vol. 1, pp. 5.9-58 – 5.9-59.)</p>
<b>URBAN DESIGN AND VISUAL RESOURCES</b>			
5.10-1 Implementation of the Re-Use Alternative could cast glare in such a way as to cause a public hazard or annoyance for a sustained period of	5.10-1 The project shall prohibit new development within the project site from: 1) using reflective glass that exceeds 50 percent of any building surface and on	LS	<u>Finding:</u> Implementation of Mitigation Measure 5.10-1, which has been required or incorporated into the Re-Use Alternative, will reduce this impact to a less than

No Impact = NI    Less than Significant = LS    Beneficial = B    Significant = S    Cumulative Significant = CS    Significant and Unavoidable = SU    Potentially Significant = PS

Environmental Impact (Significance Before Mitigation)	Mitigation Measures	Level of Significance After Mitigation	Findings of Fact
time. (PS)	the ground three floors; 2) using mirrored glass; 3) using black glass that exceeds 25 percent of any surface of a building; and 4) using metal building materials that exceed 50 percent of any street-facing surface of a primarily residential building.		<p>significant level. The City Council hereby directs that this mitigation measure be adopted. The City Council, therefore, finds that changes or alterations have been required in, or incorporated into, the Re-Use Alternative that avoid this potentially significant environmental effect identified in the Final EIR.</p> <p><u>Explanation:</u> Glare results when a light source directly in the field of vision is brighter than the eye can comfortably accept. Glare can be reduced by design features that block direct line of sight to the light source and that direct light downward, with little or no light emitted at high (near horizontal) angles, since this light would travel long distances. Cutoff-type light fixtures minimize glare because they emit relatively low intensity light at these angles.</p> <p>Mitigation Measure 5.10-1 restricts the amount of reflective materials on structures within the project to reduce the potential for glare. Implementation of this mitigation measure will ensure that glare associated with new development within the project site would be reduced to a less than significant level. (FEIR, vol. 1, pp. 5.10-6 – 5.10-7.)</p>
5.10-2 Implementation of the Re-Use Alternative could cast light onto oncoming traffic or residential uses. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.10-3 Implementation of the Re-Use Alternative could result in a substantial adverse change to the	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than

No Impact = NI    Less than Significant = LS    Beneficial = B    Significant = S    Cumulative Significant = CS    Significant and Unavoidable = SU    Potentially Significant = PS

Environmental Impact (Significance Before Mitigation)	Mitigation Measures	Level of Significance After Mitigation	Findings of Fact
existing visual character or quality of the site and its surroundings. (LS)			significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
<b>UTILITIES AND SERVICE SYSTEMS</b>			
5.11-1 Implementation of the Re-Use Alternative could increase demand for potable water. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.11-2 Implementation of the Re-Use Alternative could result in an increase in demand for potable water in excess of the City's existing diversion and treatment capacity, and could require the construction of new water supply facilities. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.11-3 Implementation of the Re-Use Alternative could result in construction activities that could degrade water quality and violate state water quality objectives by increasing sedimentation and other contaminants entering streams and rivers. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.11-4 Implementation of the Re-Use Alternative could generate new sources of polluted runoff that could violate water quality standards. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.11-5 Implementation of the Re-Use Alternative could increase exposure of people and/or property to risk of injury and damage from a localized	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002;

No Impact = NI    Less than Significant = LS    Beneficial = B    Significant = S    Cumulative Significant = CS    Significant and Unavoidable = SU    Potentially Significant = PS

Environmental Impact (Significance Before Mitigation)	Mitigation Measures	Level of Significance After Mitigation	Findings of Fact
100-year flood. (LS)			CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.11-6 Implementation of the Re-Use Alternative could increase exposure of people and/or property to risk of injury and damage from a regional 100-year flood. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.11-7 Implementation of the Re-Use Alternative could generate additional wastewater and stormwater that could require the expansion of existing conveyance and treatment facilities. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.11-8 Implementation of the Re-Use Alternative could require the need for expansion of wastewater treatment facilities, which could cause significant environmental effects. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.11-9 Implementation of the Re-Use Alternative could result in the construction of new solid waste facilities or expansion of existing facilities. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
5.11-10 Implementation of the Re-Use Alternative would not require or result in the construction of new energy production or transmission facilities. (LS)	None required.	NA	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
<b>LAND USE AND PLANNING</b>			

No Impact = NI    Less than Significant = LS    Beneficial = B    Significant = S    Cumulative Significant = CS    Significant and Unavoidable = SU    Potentially Significant = PS

Environmental Impact (Significance Before Mitigation)	Mitigation Measures	Level of Significance After Mitigation	Findings of Fact
<i>The Re-Use Alternative is consistent with the City's 2030 General Plan land use designations for the project site and would not result in any significant impacts related to land use.</i>		NI	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
<b>POPULATION AND HOUSING</b>			
<i>The Re-Use Alternative is consistent with the housing units and population projections for this site as determined by the 2030 General Plan and would not result in any significant impacts related to population or housing impacts.</i>		NI	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
<b>GEOLOGY AND SOILS</b>			
<i>The Re-Use Alternative would not result in any significant impacts related to geology and soils.</i>		NI	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
<b>ODORS</b>			
<i>The Re-Use Alternative would not result in any significant impacts related to odors.</i>		NI	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
<b>AIRPORT RELATED IMPACTS</b>			
<i>The Re-Use Alternative would not result in any significant airport related impacts.</i>		NI	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002;

No Impact = NI    Less than Significant = LS    Beneficial = B    Significant = S    Cumulative Significant = CS    Significant and Unavoidable = SU    Potentially Significant = PS

Environmental Impact (Significance Before Mitigation)	Mitigation Measures	Level of Significance After Mitigation	Findings of Fact
			CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
<b>EMERGENCY RESPONSE AND EVACUATION PLANNING</b>			
<i>The Re-Use Alternative would not result in any significant impacts related to emergency response and evacuation planning.</i>		NI	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)
<b>WILDLAND FIRE HAZARDS</b>			
<i>The Re-Use Alternative would not result in any significant impacts related to wildland fire hazards.</i>		NI	Under CEQA, no mitigation measures are required for impacts that are less than significant. (Pub. Resources Code, § 21002; CEQA Guidelines, §§ 15126.4, subd. (a)(3), 15091.)

No Impact = NI    Less than Significant = LS    Beneficial = B    Significant = S    Cumulative Significant = CS    Significant and Unavoidable = SU    Potentially Significant = PS

## Exhibit C: Mitigation Monitoring and Reporting Program

### 5.0 MITIGATION MONITORING AND REPORTING PROGRAM

---

#### INTRODUCTION

The California Environmental Quality Act (CEQA) requires review of any project that could have significant adverse effects on the environment. In 1988, CEQA was amended to require reporting on and monitoring of mitigation measures adopted as part of the environmental review process. This Mitigation Monitoring Plan (MMP) is designed to aid the City of Sacramento in its implementation and monitoring of measures adopted from the Northwest Land Park Environmental Impact Report (EIR).

#### MITIGATION MEASURES

The mitigation measures are taken from the Northwest Land Park EIR and are assigned the same number they had in the Draft EIR. The MMP describes the actions that must take place to implement each mitigation measure, the timing of those actions, and the entities responsible for implementing and monitoring the actions.

#### MMP COMPONENTS

The components of each monitoring form are addressed briefly, below.

**Impact:** This column summarizes the impact stated in the Draft EIR.

**Mitigation Measure:** All mitigation measures that were identified in the Northwest Land Park EIR are presented, and numbered accordingly.

**Action:** For every mitigation measure, one or more actions are described. These are the center of the MMP, as they delineate the means by which EIR measures will be implemented, and, in some instances, the criteria for determining whether a measure has been successfully implemented. Where mitigation measures are particularly detailed, the action may refer back to the measure.

**Implementing Party:** This item identifies the entity that will undertake the required action.

**Timing:** Each action must take place prior to the time at which a threshold could be exceeded. Implementation of the action must occur prior to or during some part of approval, project design or construction or on an ongoing basis. The timing for each measure is identified.

**Monitoring Party:** The City of Sacramento is responsible for ensuring that mitigation measures are successfully implemented. Within the city, a number of departments and divisions would have responsibility for monitoring some aspect of the overall project.

TABLE 1						
NORTHWEST LAND PARK MITIGATION MONITORING AND REPORTING PROGRAM						
Impact	Mitigation Measure(s)	Action(s)	Implementing Party	Timing	Monitoring and Enforcement	
<b>5.1 Air Quality</b>						
5.1-2 Implementation of the proposed project could result in construction activities that would increase NO <sub>x</sub> levels above 85 pounds per day.	5.1-2 a)	In order to ensure that emissions of NO <sub>x</sub> do not exceed the regulatory threshold of 85 pounds per day, construction of project phases shall not be conducted concurrently nor shall any portion of construction from one phase overlap that of another phase unless the applicant demonstrates to the satisfaction of the City and SMAQMD that the threshold of 85 pounds per day will not be exceeded. Written confirmation to the file from the City's Community Development Department that confirms satisfaction with this mitigation measure and confirms SMAQMD agreement is sufficient.	Prevent concurrent construction of project phases	Project applicant	During construction	Community Development Department
	b)	The following shall be incorporated into all construction plans for projects that estimated construction related NO <sub>x</sub> emissions exceed 85 lbs/day:  If projected construction related emissions for a project are not reduced below the 85 lbs/day by application of MM 5.1-2(a), then an off-site construction mitigation fee shall be applied. The construction mitigation fee shall be calculated based upon the SMAQMD's current construction mitigation fee at the time of project specific evaluation. Verification of payment of the mitigation fee shall be provided to the City prior to the issuance of any grading permit.	Pay off-site mitigation fee if construction emissions exceed 85 lbs/day threshold	Project applicant	During construction	Community Development Department

TABLE 1					
NORTHWEST LAND PARK MITIGATION MONITORING AND REPORTING PROGRAM					
Impact	Mitigation Measure(s)	Action(s)	Implementing Party	Timing	Monitoring and Enforcement
<b>5.3 Cultural Resources</b>					
5.3-2 Implementation of the proposed project could cause a substantial change in the significance of an archaeological resource as defined in CEQA Guidelines section 15064.5.	5.3-2 a) In the event that any prehistoric or historic-era subsurface archaeological features or deposits, including locally darkened soil ("midden"), that could conceal cultural deposits, are discovered during construction-related earth-moving activities, all ground-disturbing activity within 100 feet of the resources shall be halted and the City of Sacramento Community Development Department shall be notified. The City shall consult with a qualified archeologist retained at the applicant's expense to assess the significance of the find. If the find is determined to be significant by the qualified archaeologist (i.e., because the find is determined to constitute either an historical resource or a unique archaeological resource), representatives of the City and the qualified archaeologist shall meet to determine the appropriate course of action, with the City making the final decision. All significant cultural materials recovered shall be subject to scientific analysis, professional museum curation, and a report shall be prepared by the qualified archaeologist according to current professional standards.  If the archaeologist determines that some or all of the affected property qualifies as a Native American Cultural Place, including a Native American sanctified cemetery, place of worship, religious or ceremonial site, or sacred shrine (Public Resources Code §5097.9) or a Native American historic, cultural, or sacred site, that is listed or may be eligible for listing in the California Register of Historical Resources pursuant to Public Resources Code §5024.1, including any historic or prehistoric ruins, any burial ground, any archaeological or historic site (Public Resources Code §5097.993), the archaeologist shall recommend to the City potentially feasible mitigation measures that would preserve the integrity of the site or minimize impacts on it, including any or a combination of the following:	Comply with mitigation requirements regarding consultation and subsequent actions if prehistoric or historic-era subsurface archaeological features are encountered	Construction contractor	During construction	Community Development Department

TABLE 1					
NORTHWEST LAND PARK MITIGATION MONITORING AND REPORTING PROGRAM					
Impact	Mitigation Measure(s)	Action(s)	Implementing Party	Timing	Monitoring and Enforcement
	<ul style="list-style-type: none"> <li>• Avoidance, preservation, and/or enhancement of all or a portion of the Native American Cultural Place as open space or habitat, with a conservation easement dedicated to the most interested and appropriate tribal organization. If such an organization is willing to accept and maintain such an easement, or alternatively, a cultural resource organization that holds conservation easements;</li> <li>• An agreement with any such tribal or cultural resource organization to maintain the confidentiality of the location of the site so as to minimize the danger of vandalism to the site or other damage to its integrity; or</li> <li>• Other measures, short of full or partial avoidance or preservation, intended to minimize impacts on the Native American Cultural Place consistent with land use assumptions and the proposed design and footprint of the development project for which the requested grading permit has been approved.</li> <li>• After receiving such recommendations, the City shall assess the feasibility of the recommendations and impose the most protective mitigation feasible in light of land use assumptions and the proposed design and footprint of the development project. The City shall, in reaching conclusions with respect to these recommendations, consult with both the project applicant and the most appropriate and interested tribal organization.</li> </ul> <p>b) If human remains are discovered at any project construction sites during any phase of construction, all ground-disturbing activity within 50 feet of the remains shall be halted immediately, and the City of Sacramento Community Development Department and the County coroner shall be notified immediately. If the remains are determined by the County coroner to be Native American, the Native American Heritage Commission</p>	<p>If human remains are discovered, stop work and notify City; comply with mitigation requirements</p>	<p>Construction contractor</p>	<p>During construction</p>	<p>Community Development Department</p>

TABLE 1					
NORTHWEST LAND PARK MITIGATION MONITORING AND REPORTING PROGRAM					
Impact	Mitigation Measure(s)	Action(s)	Implementing Party	Timing	Monitoring and Enforcement
	(NAHC) shall be notified within 24 hours, and the guidelines of the NAHC shall be adhered to in the treatment and disposition of the remains. The project applicant shall also retain a professional archaeologist with Native American burial experience to conduct a field investigation of the specific site and consult with the Most Likely Descendant, if any, identified by the NAHC. As necessary, the archaeologist may provide professional assistance to the Most Likely Descendant, including the excavation and removal of the human remains. The City shall be responsible for approval of recommended mitigation as it deems appropriate, taking account of the provisions of state law, as set forth in CEQA Guidelines section 15064.5(e) and Public Resources Code section 5097.98. The project applicant shall implement approved mitigation, to be verified by the City, before the resumption of ground-disturbing activities within 50 feet of where the remains were discovered.				
<b>5.3-3 Implementation of the proposed project could directly or indirectly destroy a unique paleontological resource or site or unique geologic feature.</b>	5.3-3 Should paleontological resources be identified at any project construction sites during any phase of construction, the construction manager shall cease operation at the site of the discovery and immediately notify the City of Sacramento Community Development Department. The project applicant shall retain a qualified paleontologist to provide an evaluation of the find and to prescribe mitigation measures to reduce impacts to a less-than-significant level. In considering any suggested mitigation proposed by the consulting paleontologist, the Community Development Department shall determine whether avoidance is necessary and feasible in light of factors such as the nature of the find, project design, costs, land use assumptions, and other considerations. If avoidance is unnecessary or infeasible, other appropriate measures (e.g., data recovery) shall be instituted. Work may proceed on other parts of the project site while mitigation for paleontological resources is carried out.	Comply with mitigation requirements regarding consultation and subsequent actions if paleontological resources are encountered	Construction contractor	During construction	Community Development Department

TABLE 1					
NORTHWEST LAND PARK MITIGATION MONITORING AND REPORTING PROGRAM					
Impact	Mitigation Measure(s)	Action(s)	Implementing Party	Timing	Monitoring and Enforcement
<b>5.4 Global Climate Change</b>					
5.4-1 Construction and operation of the proposed project would generate greenhouse gas emissions that may have a significant impact on the environment.	5.4-1 The following PUD Guidelines shall be incorporated into project design, as verified by City staff during design review. <ul style="list-style-type: none"> <li>Choice of Mobility – The community shall allow for multiple modes of transportation including private automobiles, bicycles, and pedestrian mobility.</li> <li>Street Connectivity – The community streets shall be designed on a modified grid with multiple connections to the surrounding roadway network.</li> <li>Pedestrian and Bicycle Connectivity – The applicant shall provide sidewalks on both sides along all streets, and a defined multi-use trail network. The applicant shall develop private pathways that provide pedestrian linkages within individual blocks and between community uses.</li> <li>Safe Environment – Streets shall be designed to be safe in terms of traffic mobility, diversity in users, and crime prevention. Climate Appropriate Plants – Trees, shrubs, and grasses shall be conducive to the Northern California environment in terms of water use, drought tolerance, maintenance, and durability. Synthetic Turf should be used for active play areas and small gathering lawns.</li> </ul>	Incorporate into project design greenhouse gas-reducing measures from PUD Guidelines	Project applicant	Prior to Final Map approval	Community Development Department

TABLE 1					
NORTHWEST LAND PARK MITIGATION MONITORING AND REPORTING PROGRAM					
Impact	Mitigation Measure(s)	Action(s)	Implementing Party	Timing	Monitoring and Enforcement
	<ul style="list-style-type: none"> <li>• Low Maintenance &amp; Cost Effectiveness – Landscape material including trees, plants, turf, and hardscape should require minimal maintenance as compared to other varieties and material choices. Synthetic turf shall be used to the extent possible in lieu of natural turf and grasses. Materials should be cost effective to lessen the initial expenditure, periodic replacement, and long-term maintenance. Turf may be synthetic to lessen irrigation demands and long term maintenance.</li> <li>• Standard Streetscape – The plantings along streets and the community trails shall consist mainly of species that at maturity will act as large canopy shade trees and colorful understory plantings. Nothing in this section shall be construed to require an initial planting larger than a 24" box tree.</li> <li>• Alternative Local Streetscape - Landscaping along internal local streets shall be more lush and generous in plant coverage including primarily canopy shade trees to create a dynamic streetscape.</li> <li>• Stormwater Management – The project will redevelop with smaller residential buildings interlaced within green courtyards, large central park and meandering greenbelt, and utilizing decorative permeable materials for private driveways and courts. The pervious to impervious ratio for Phase 1 (40% permeable to 60% Impermeable) will be used as a minimum guideline for the build-out of the entire site through Phase 4.</li> <li>• Water Efficiency – All project landscaping shall be climate appropriate for the area and irrigated with moisture sensor driven systems to provide drought tolerance and maximum efficiency of water use in irrigation. Synthetic turf shall be used, to the greatest extent possible, for private grassed areas within the development.</li> </ul>				

TABLE 1					
NORTHWEST LAND PARK MITIGATION MONITORING AND REPORTING PROGRAM					
Impact	Mitigation Measure(s)	Action(s)	Implementing Party	Timing	Monitoring and Enforcement
	<ul style="list-style-type: none"> <li>Vegetation &amp; Forestation – Vegetation and tree planting plans shall be designed to provide shading for streets, hardscape surfaces, buildings, and recreation areas during summer months. In contrast, said plans shall include landscape varieties that lose their leaves during winter months to promote passive sunlight within the community, thus reducing energy use relating to heating and lighting.</li> <li>Air Quality – The project proposes that all buildings, units, and facilities, indoors and out, are free of devices designated to facilitate the combustion of wood or wood products to eliminate emissions generally associated with traditional fireplaces.</li> <li>Reuse and Recycling - The project shall re-use at least 50% of the salvageable materials in the existing improvements on-site, as measured by weight. This can take the form of re-use of entire structures, re-use or repurposing of significant elements, such as beams or trusses, and recycling materials within the new project such as grinding paving and asphalt for use as base material at the site. These activities will increase the sustainability of the site through reduced waste materials from demolition, reduced need for new materials on-site, and reduction of the ancillary transportation impacts from off-haul and delivery of materials to the site. Additionally, the project will evaluate brick, wood, metal, and masonry materials from the demolition to be re-manufactured into a "heritage" line of finishes to be offered as upgrades to the units. As an example, wood timbers would be converted into flooring material to provide the character and cache of "distressed" lumber underfoot. These efforts will increase the amount of on-site materials reused sustainably within the project.</li> </ul>				
	<ul style="list-style-type: none"> <li>Efficient Floor Plans - The Northwest Land Park</li> </ul>				

TABLE 1					
NORTHWEST LAND PARK MITIGATION MONITORING AND REPORTING PROGRAM					
Impact	Mitigation Measure(s)	Action(s)	Implementing Party	Timing	Monitoring and Enforcement
	<p>community will be developed with compact efficient floor plans. In addition the majority of units will share wall/floor space, and thus thermal mass, with at least one other unit.</p> <ul style="list-style-type: none"> <li>• Insulation – Building shall be designed with a high-efficiency thermal shell for the units with exterior walls at or above R25 for walls and R40 for ceilings.</li> <li>• Climatization – Residential buildings shall use small high efficiency heating and cooling units.</li> <li>• Lighting - Buildings shall use a LED or fluorescent lighting system throughout the units, allowing for energy efficient lighting.</li> <li>• Exterior Lighting – Exterior HOA maintained lighting, including pathway lights, accent/landscaping lights, motor-court lights, and private street lights shall use LED lighting technologies.</li> <li>• Water Heaters - The project shall provide high efficiency tank-less hot water heaters to provide for the most energy efficient delivery of hot water. Nothing in this provision shall preclude installation of high efficiency alternative energy source hot water heating and storage units.</li> <li>• Electrical vehicle accommodations – The project shall incorporate 110v electrical outlets in the garage units such that they are readily accessible for use with electric vehicles.</li> <li>• Renewable Energy Commitment - The project shall incorporate a 400 KW renewable energy system to reduce the amount of energy purchased by the Project. The 400 KW renewable energy will be incorporated over the life of the project such that a minimum of 100 KW will be incorporated into phase 1 with an aggregate total of 100 KWs per phase through the buildout of phase 4. The 400 KW system will result in an annual reduction of 730,000 kWh of</li> </ul>				

TABLE 1					
NORTHWEST LAND PARK MITIGATION MONITORING AND REPORTING PROGRAM					
Impact	Mitigation Measure(s)	Action(s)	Implementing Party	Timing	Monitoring and Enforcement
	<p>purchased electricity at full project buildout. This is equivalent to the emissions from electrical consumption of approximately 188 dwelling units. The renewable energy system may include solar, wind, fuel cells, or other new technology that becomes available over the implementation of the project. The following are the commitments already made by the project to foster this renewable commitment:</p> <ul style="list-style-type: none"> <li>○ Photovoltaic Design - The project shall be planned to orient at least 40% of the roof area of a minimum of 50% of the buildings to the west, south or southwest so that photovoltaic panels and collector systems can provide maximum benefit when installed. The project shall work with the local utility and, through an aggressive sales program, encourage and provide solar systems and/or alternative energy systems as an option.</li> <li>○ Solar Orientation – The majority of the project's buildings shall be designed to orient the roof tops with strong solar capture opportunities for photovoltaic panels throughout the community. The orientation of at least 40% of the roof area of at least 50% of the buildings shall be west, southwest, or south.</li> <li>○ Solar Energy – As indicated in the AQMP (measure M28), the NWLP Project has committed to the implementation of a solar energy system that will offset a minimum of 2.5% of the residential needs of the project.</li> </ul>				

5.0 MITIGATION MONITORING AND REPORTING PROGRAM

5.5 Hazards and Hazardous Materials					
<p>5.5-1 Implementation of the proposed project could result in the exposure of people to hazards and hazardous materials during construction activities.</p>	<p>5.5-1 a) No grading may occur on the parcels within Phases 2, 3 or 4 until SCEMD issues a no further action letter for Phases 2, 3 and 4, respectively. In the event a no further action letter is issued for only certain parcels within a Phase, grading may only occur on the parcels for which a no further action letter was issued. The applicant shall be responsible for providing written confirmation of SCEMD action prior to the issuance of a grading permit for any affected project phase.</p>	<p>Provide SCEMD "no further action letter" to City from SCEMD prior to obtaining grading permit for Phases 2, 3 or 4</p>	<p>Construction contractor</p>	<p>Prior to obtaining grading permit</p>	<p>Building Department</p>
	<p>b) Prior to issuance of a grading permit that would include installation of underground utility trenches, the City shall ensure a groundwater management plan has been prepared by a qualified environmental professional registered in California. The plan shall be submitted to the City for review and approval.</p>	<p>Submit groundwater management plan to City</p>	<p>Construction contractor</p>	<p>Prior to issuance of a grading permit</p>	<p>Building Department</p>
	<p>The groundwater management plan shall identify the locations and depths of underground utility trenches relative to known contaminated groundwater. If it is determined trenches could intercept contaminated groundwater during construction, the plan shall identify measures to be implemented to properly remove and dispose of contaminated groundwater in accordance with best management practices and City requirements. Such measures could include, but not be limited to, the use of a pump to extract the contaminated groundwater out of the trench and then store the water onsite in a sump or storage tank until properly discharged into the City sewer system per City regulations described below.</p>				
	<p>All dewatering activities shall be subject to the requirements of the City's Department of Utilities Engineering Services Policy No. 0001 (adopted as Resolution No. 92-439 by the Sacramento City Council), which protects water quality by monitoring dewatering activities and ensuring that all groundwater discharges are free of contamination.</p>				
	<p>The groundwater management plan shall also identify specific measures (e.g., design features, construction</p>				

5.0 MITIGATION MONITORING AND REPORTING PROGRAM

	<p>methods) to ensure underground utilities do not create a horizontal conduit for contaminant migration. The plan shall include provisions for monitoring the effectiveness of the construction methods in minimizing horizontal contaminated groundwater migration along utility trenches.</p> <p>c) Prior to site preparation (i.e., grading, clearing), the project applicant shall consult with SCEMD to determine whether there are any construction activities that could damage or otherwise interfere with use of on-site monitoring wells, specifically MW-3 for ongoing groundwater monitoring. If SCEMD determines the wells would not be affected by project activities, the project applicant shall obtain written documentation from SCEMD to that effect. If it is determined that well relocation or protective measures are necessary, the project applicant shall coordinate with SCEMD in advance of any site preparation activities during construction to identify the appropriate measures and to obtain regulatory approval of such measures. Site preparation activities that could affect the monitoring wells shall not be implemented until SCEMD has inspected any modifications and provided written notification to the City that it has reviewed and approved the protective measures.</p> <p>The City shall not issue a grading permit to the project applicant until written documentation from SCEMD is provided to the City that determines the groundwater monitoring wells would not be affected by site preparation project activities, or, if it is determined that well relocation or protective measures are necessary, SCEMD has inspected any modifications and provided written notification to the City that it has reviewed and approved the protective measures.</p>	<p>Obtain written documentation from SCEMD that construction activities would not damage on-site monitoring wells</p>	<p>Construction contractor</p>	<p>Prior to issuance of a grading permit</p>	<p>Building Department</p>
<b>5.6 Noise and Vibration</b>					
<p><b>5.6-1 Implementation of the proposed project could result in exterior noise levels at sensitive receptors in the project area (including those on the project site) that are</b></p>	<p>5.6-1 a) Residential structures in the project shall be designed to avoid any exterior communal/recreational areas, excluding balconies, on the third and fourth floors with direct line-of-sight to I-5.</p>	<p>Avoid building design with exterior communal/recreational areas with direct line-of-sight to I-5</p>	<p>Project applicant</p>	<p>Prior to issuance of building permits</p>	<p>Community Development Department</p>



5.0 MITIGATION MONITORING AND REPORTING PROGRAM

	<p>through the use of certain design-specific measures that may include, but are not limited to:</p> <ul style="list-style-type: none"> <li>• The use of triple-paned or no windows for structure walls fronting the existing commercial operations located along 1<sup>st</sup> Avenue;</li> </ul>				
	<ul style="list-style-type: none"> <li>• Not allowing bedrooms along the outermost structure walls of the northern and eastern boundaries of Phase 2 and the eastern boundary of Phase 4;</li> <li>• The use of gypsum board or other sound-insulating building material; and</li> <li>• Providing a uniform wall or line of structures along the boundary of the site where Phase 2 abuts the existing use on the south side of First Avenue and on the eastern boundary of Phase 4 where it abuts the existing use on the north side of First Avenue.</li> </ul> <p>c) The City shall require, through a deed restriction providing notice to purchasers that any future residents of structures adjacent to the existing commercial operations be required to acknowledge ongoing commercial activities that could result in noisy activities at the time of purchase or lease of a residential unit.</p>	Provide notification to future occupants regarding potential noise sources	Project applicant	Prior to issuance of building permits	Building Department
<b>5.9 Transportation and Circulation</b>					
5.9-7 Phase 1 of the proposed project could cause potentially significant impacts due to construction-related activities.	5.9-7 Prior to the beginning of construction, the applicant shall prepare a construction traffic and parking management plan to the satisfaction of City Traffic Engineer and subject to review by all affected agencies. The plan shall ensure that acceptable operating conditions on local roadways and freeway facilities are maintained. At a minimum, the plan shall include: <ul style="list-style-type: none"> <li>• Description of trucks including: number and size of trucks per day, expected arrival/departure times, truck circulation patterns.</li> <li>• Description of staging area including: location, maximum number of trucks simultaneously permitted in staging area, use of traffic control personnel, specific signage.</li> </ul>	Submit construction traffic and parking management plan to City Traffic Engineer	Construction contractor	Prior to issuance of grading or building permits	City Traffic Engineer

5.0 MITIGATION MONITORING AND REPORTING PROGRAM

	<ul style="list-style-type: none"> <li>Description of street closures including: duration, advance warning and posted signage, safe and efficient access routes for emergency vehicles, and use of manual traffic control.</li> <li>Description of driveway access plan including: provisions for safe vehicular, pedestrian, and bicycle travel, minimum distance from any open trench, special signage, and private vehicle accesses.</li> </ul>				
5.9-15 The proposed project would contribute to unacceptable peak hour operations at the W Street/9th Street intersection.	5.9-15 The project applicant shall contribute its fair share toward restriping the southbound approach to the W Street/ 9 <sup>th</sup> Street intersection to add an exclusive right-turn lane while maintaining the two existing through lanes and one existing shared through/right lane.	Pay fair share of W Street/9th Street restriping	Project applicant	Prior to issuance of grading or building permits	Department of Transportation
5.9-16 The proposed project would contribute to unacceptable peak hour operations at the I-5 NB Off-Ramp/Broadway intersection and vehicular queuing that extends onto the freeway mainline.	5.9-16 The project applicant shall contribute its fair share toward the installation of a traffic signal at the I-5 NB Off-Ramp/ Broadway intersection.	Pay fair share of traffic signal at the I-5 NB Off-Ramp/ Broadway intersection	Project applicant	Prior to issuance of grading or building permits	Department of Transportation
<b>5.10 Urban Design and Visual Resources</b>					
5.10-1 Implementation of the proposed project could cast glare in such a way as to cause a public hazard or annoyance for a sustained period of time.	5.10-1 The proposed project shall prohibit new development within the project site from: <ol style="list-style-type: none"> <li>using reflective glass that exceeds 50 percent of any building surface and on the ground three floors;</li> <li>using mirrored glass;</li> <li>using black glass that exceeds 25 percent of any surface of a building; and</li> <li>using metal building materials that exceed 50 percent of any street-facing surface of a primarily residential building.</li> </ol>	Comply with Mitigation Measure 5.10-1 regarding use of glass and reflective building materials	Project applicant	Prior to issuance of grading or building permits	Community Development Department

# Exhibit D: Errata to the Mitigation Monitoring and Reporting Program



COMMUNITY DEVELOPMENT  
DEPARTMENT

CITY OF SACRAMENTO  
CALIFORNIA

300 Richards Boulevard  
3<sup>RD</sup> FLOOR  
SACRAMENTO, CA  
95811

## NORTHWEST LAND PARK PROJECT FINAL ENVIRONMENTAL IMPACT REPORT

SCH No. 2010052011

ERRATA: July 14, 2011

The Northwest Land Park Project Final Environmental Impact Report (Final EIR) has been released for public review. The City has provided copies of responses to written comments on the Draft EIR to persons and agencies submitting such comments.

The following changes are being made to the Final EIR as circulated. The discussion below identifies the changes and the affected sections of the Final EIR.

### Revised Mitigation Measure 5.6-2(b)

So long as existing industrial and commercial uses continue to operate, the project applicant shall design residential structures, immediately adjacent to the existing commercial operations located along 1<sup>st</sup> Avenue in Phases 2 and 4, to achieve ~~up to a 35 dBA~~ a reduction between exterior and interior noise levels in accordance with City standards through the use of certain design-specific measures that may include, but are not limited to:

- The use of triple-paned or no windows for structure walls fronting the existing commercial operations located along 1<sup>st</sup> Avenue;
- Not allowing bedrooms along the outermost structure walls of the northern and eastern boundaries of Phase 2 and the eastern boundary of Phase 4;
- The use of gypsum board or other sound-insulating building material; and
- Providing a uniform wall or line of structures along the boundary of the site where Phase 2 abuts the existing use on the south side of First Avenue and on the eastern boundary of Phase 4 where it abuts the existing use on the north side of First Avenue.



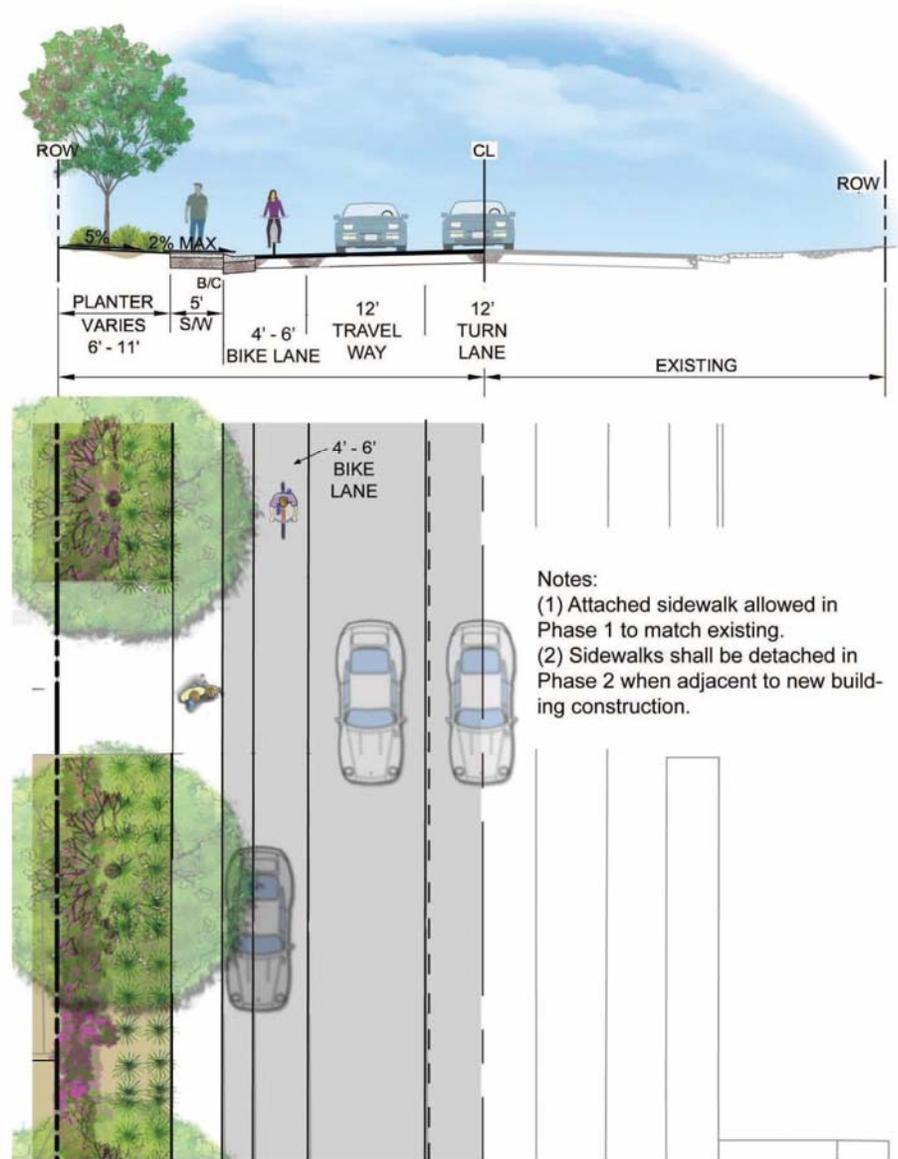


The proposed changes are equivalent or more effective than the mitigation measures that would be revised. None of the changes would result in new significant effects that have not been identified and evaluated in the EIR.



Exhibit E: Figure “Minor Collector (5th Street)-Typical Cross Section and Plan”

Minor Collector (5th Street) - Typical Cross Section and Plan





## **RESOLUTION NO.**

Adopted by the Sacramento City Council

### **AMENDING THE CITY BIKEWAY MASTER PLAN TO INCORPORATE THE BIKEWAY NETWORK IN THE NORTHWEST LAND PARK DEVELOPMENT (P10-039)**

#### **BACKGROUND**

- A. On July 14, 2011, the City Planning Commission conducted a public hearing on the Northwest Land Park project, and forwarded to the City Council a recommendation to approve the project.
- B. On August 23, 2011, the City Council conducted a public hearing, for which notice was given pursuant Sacramento City Code Section 17.200.010(C)(2)(a), (b), and (c) (publication, posting, and mail 500'), and received and considered evidence concerning the Northwest Land Park project.

#### **BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:**

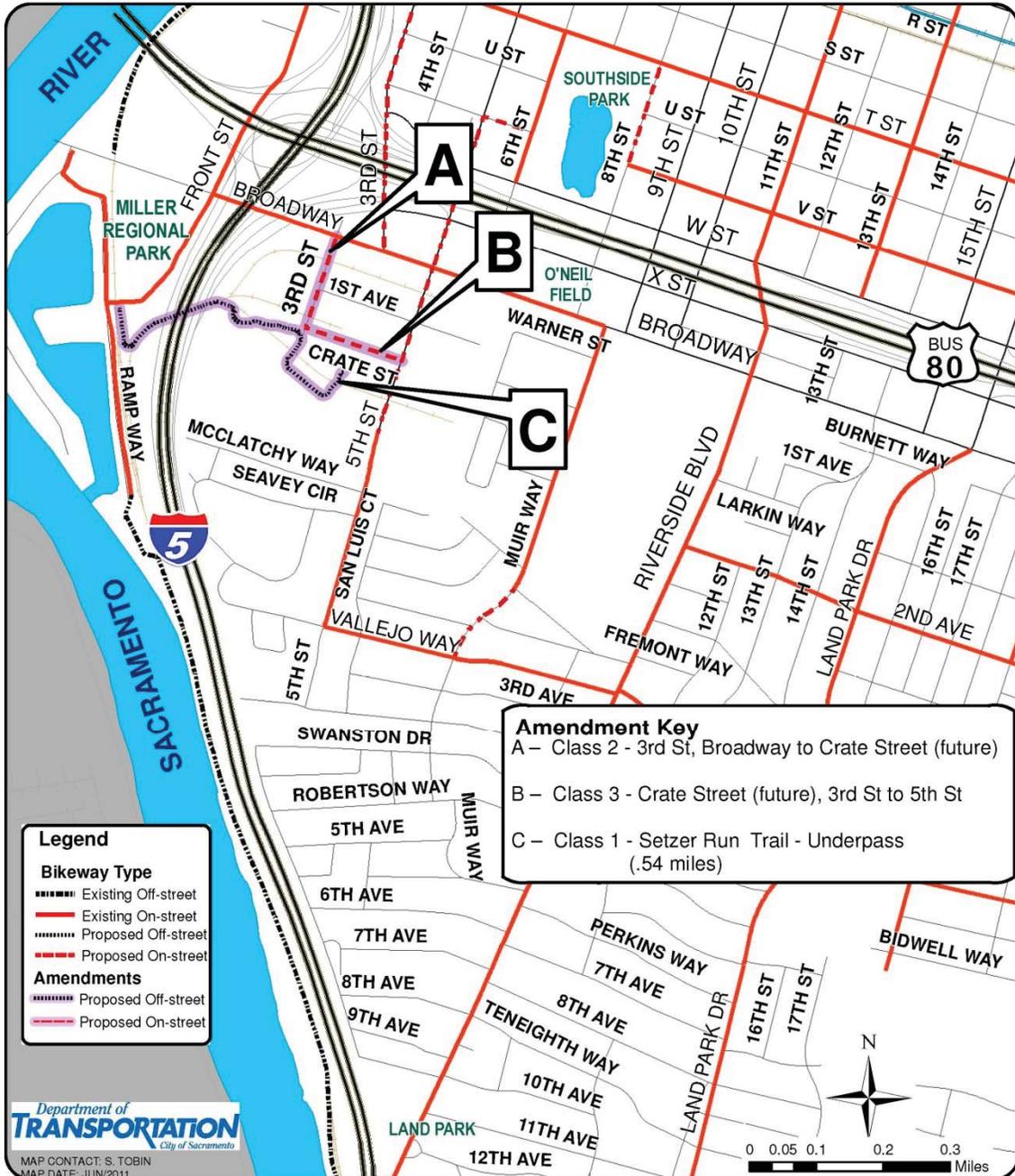
- Section 1. Based on the verbal and documentary evidence received at the hearing on the Northwest Land Park project, the City Council finds that amending the City Bikeway Master Plan to incorporate the bikeway network in the Northwest Land Park development is consistent with the City's General Plan goals to:
  - 1. Develop bicycling as a major transportation and recreational mode.
  - 2. Provide a network of safe and convenient bikeways.
  - 3. Promote bicycling as a feasible transportation alternative which conserves energy, improves air quality, reduces traffic congestion, and improves public health.
- Section 2. City Council hereby amends the City's Bikeway Master Plan to incorporate the Northwest Land Park development as shown in Exhibit A.
- Section 3. Exhibit A is a part of this Resolution.

**Table of Contents:**

Exhibit A: Northwest Land Park Bikeway Plan – 1 page

## CITY OF SACRAMENTO BIKEWAY MASTER PLAN AMENDMENTS

### NORTHWEST LAND PARK DEVELOPMENT





## RESOLUTION NO.

Adopted by the Sacramento City Council

**TO ESTABLISH A PLANNED UNIT DEVELOPMENT, INCLUDING GUIDELINES AND SCHEMATIC PLAN, FOR THE NORTHWEST LAND PARK PLANNED UNIT DEVELOPMENT, FOR AREA BOUNDED BY BROADWAY ON THE NORTH, 5TH STREET ON THE EAST, MCCLATCHY WAY ON THE SOUTH, AND I-5 ON THE WEST. (APN: 009-0030-008, 019, 043, 045; 009-0223-007, 012, 013, 016; 009-0237-018; 009-0270-009, 015, 017, 028, 029, 032, 033; 009-0286-001, 012, 013, 014, 018) (P10-039)**

### BACKGROUND

A. On July 14, 2011, the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve with conditions the Northwest Land Park Project.

B. On August 23, 2011, the City Council conducted a public hearing, for which notice was given pursuant Sacramento City Code Section 17.200.010(C)(1)(a), (b), and (c) (publication, posting, and mail (500 feet) and received and considered evidence concerning the Northwest Land Park Project.

### **BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:**

Section 1. The City Council conducted a public hearing on July 26, 2011, to consider the establishment of the Northwest Land Park Planned Unit Development (PUD) and adopt the Planned Unit Development Guidelines and Schematic Plan for the Northwest Land Park development. Based on documentary and oral evidence submitted at said public hearing, the City Council hereby finds:

- A. The proposed PUD establishment conforms to policies of the General Plan and Land Park Community Plan to promote a variety of housing types within neighborhoods to encourage economic diversity and housing choice; and
- B. The PUD establishment will not be injurious to the public welfare, nor to other properties in the vicinity of the development and will be in harmony with the general purposes and intent of the Zoning Ordinance in that the PUD ensures that development be well-designed, and that the residential uses will not create a negative impact on adjacent uses.

Section 2. The City Council, in accordance with Sacramento City Code Chapter 17.180, establishes the Northwest Land Park Planned Unit Development and approves

the Northwest Land Park Development Guidelines and Schematic Plan (as shown on the attached Exhibits A and B).

Section 3. The City Council approves the Northwest Land Park Planned Unit Development with the Development Guidelines and Schematic Plan subject to the following conditions of approval:

1. Upon issuance of a building permit for residential units along the northern boundary of Phase 2, the project applicant shall construct a ten-foot high uniform wall along the boundary of the project site where Phase 2 abuts the existing uses (currently owned by Sacconi Distributing Company and the Gary and Roland Sacconi Trusts) on the south side of First Avenue, and shall plant fast-growing trees, such as Italian cypress trees, adjacent to the ten-foot high wall on the Project side.
2. Upon issuance of a building permit for residential uses on the eastern boundary of Phase 4, the project applicant shall construct a ten-foot high uniform wall along the eastern boundary of Phase 4 where it abuts the existing use on the north side of First Avenue, and shall plant fast-growing trees, such as Italian cypress trees, adjacent to the ten-foot high wall on the Project side. In the event the project applicant does not seek building permits to construct residential units along the eastern boundary of Phase 4, this condition shall have no effect.
3. The applicant shall explore the feasibility of constructing a traffic circle at the intersection of McClatchy and 5th Streets as part of their public improvements. The traffic circle shall be constructed per City standards and to the satisfaction of the Department of Transportation. If the traffic circle construction is found to be infeasible, the applicant shall contribute the sum of \$50,000 to go towards a Neighborhood Traffic Management Program (NTMP) within the adjacent neighborhood, including 5th Street and Vallejo Street. If the construction of the traffic circle is found to be feasible, the applicant has agreed to construct the traffic circle. Should the total design and construction cost of the traffic circle be less than \$50,000, the remaining balance shall be paid towards the above referenced NTMP. This one time contribution, from the applicant, shall satisfy all requirements towards any neighborhood traffic calming program related to the development of the Northwest Land Park PUD.

Section 4. Exhibits A to G are a part of this Resolution.

**Table of Contents:**

- Exhibit A: Northwest Land Park PUD Guidelines – 78 pages
- Exhibit B: Northwest Land Park PUD Schematic Plan – 1 page
- Exhibit C: Railroad Remnant Parcels – 1 page
- Exhibit D: Land Use Plan Phases 1 through 4 – 1 page
- Exhibit E: Overall Planting Plan – 1 page
- Exhibit F: Overall Circulation Plan – 1 page
- Exhibit G: Site Level Bike Trail Plan – 1 page

# Northwest Land Park

## Planned Unit Development Guidelines

City of Sacramento



Prepared for:  
Northwest Land Park LLC

Prepared by:  
**Cardno WRG**  
201 Creekside Ridge Court, Suite 100  
Roseville, California 95678  
[www.cardnowrg.com](http://www.cardnowrg.com)

In partnership with:  
**Starck Architecture & Planning**  
2045 Kettner Boulevard, Suite 100  
San Diego, California 92101  
[www.starckap.com](http://www.starckap.com)

Mogavero Notestine Associates  
2012 K Street  
Sacramento, California 95814  
[www.magnot.com](http://www.magnot.com)





July 25, 2011

# Northwest Land Park

Planned Unit Development Guidelines

P10-039 Approved (Date)  
Sacramento City Council Resolution No. \_\_\_\_\_



**Table of Contents**

<b>Chapter 1 - Introduction .....</b>	<b>1</b>	<b>Chapter 5 – Landscape Framework.....</b>	<b>62</b>
1.1 - Project Overview .....	1	5.1 – Landscape Framework .....	62
1.2 – Vision and Goals .....	6	5.2 - Landscape Development Guidelines .....	62
1.3 – Community Master Plan .....	7	5.3 – Streetscape Standards.....	62
1.4 – Signature Elements .....	11	5.4 – Lighting and Street Furniture styles..	64
1.5 – Optional Elements .....	16	5.5 – Plant List.....	65
1.6 – Community Phasing Plan .....	18	<b>Chapter 6 – Sustainability &amp; Energy</b>	
1.7 – Framework Plans .....	20	<b>Efficiency Framework.....</b>	<b>68</b>
1.8 – Purpose .....	20	6.1 – Sustainability & Energy Efficiency	
1.9 - Administration .....	20	Framework .....	68
1.10 - Definitions.....	20	6.2 – Sustainability & Energy Site	
<b>Chapter 2 – Land Use Framework.....</b>	<b>21</b>	Development Guidelines .....	68
2.1 – Land Use Framework.....	21	6.3 – Re-use and Recycling On-site	
2.2 – Land Use Districts.....	21	Materials Guidelines .....	69
2.3 – Permitted Land Uses.....	24	6.4 – Dwelling Unit Efficiency Design	
2.4 – Residential Building Types.....	24	Guidelines .....	70
2.5 – Dimensional Standards.....	30	<b>Appendix A – City Council Resolution -</b>	
2.6 – Site Development Guidelines.....	31	<b>Conditions of Approval 1-3 .....</b>	
<b>Chapter 3 – Open Space Framework .....</b>	<b>42</b>		
3.1 – Open Space Framework.....	42		
3.2 – Open Space Development			
Guidelines .....	42		
3.3 – Park.....	45		
3.4 – Setzer Run Greenway Corridor.....	47		
3.5 – Private Courtyards .....	49		
<b>Chapter 4 – Transportation Framework.....</b>	<b>51</b>		
4.1 – Transportation Framework .....	51		
4.2 - Transportation Development			
Guidelines .....	51		
4.3 – Facility Descriptions .....	53		
4.4 – Facility Types .....	54		
4.5 – Facility Design Illustrations .....	54		

## **Chapter 1 - Introduction**

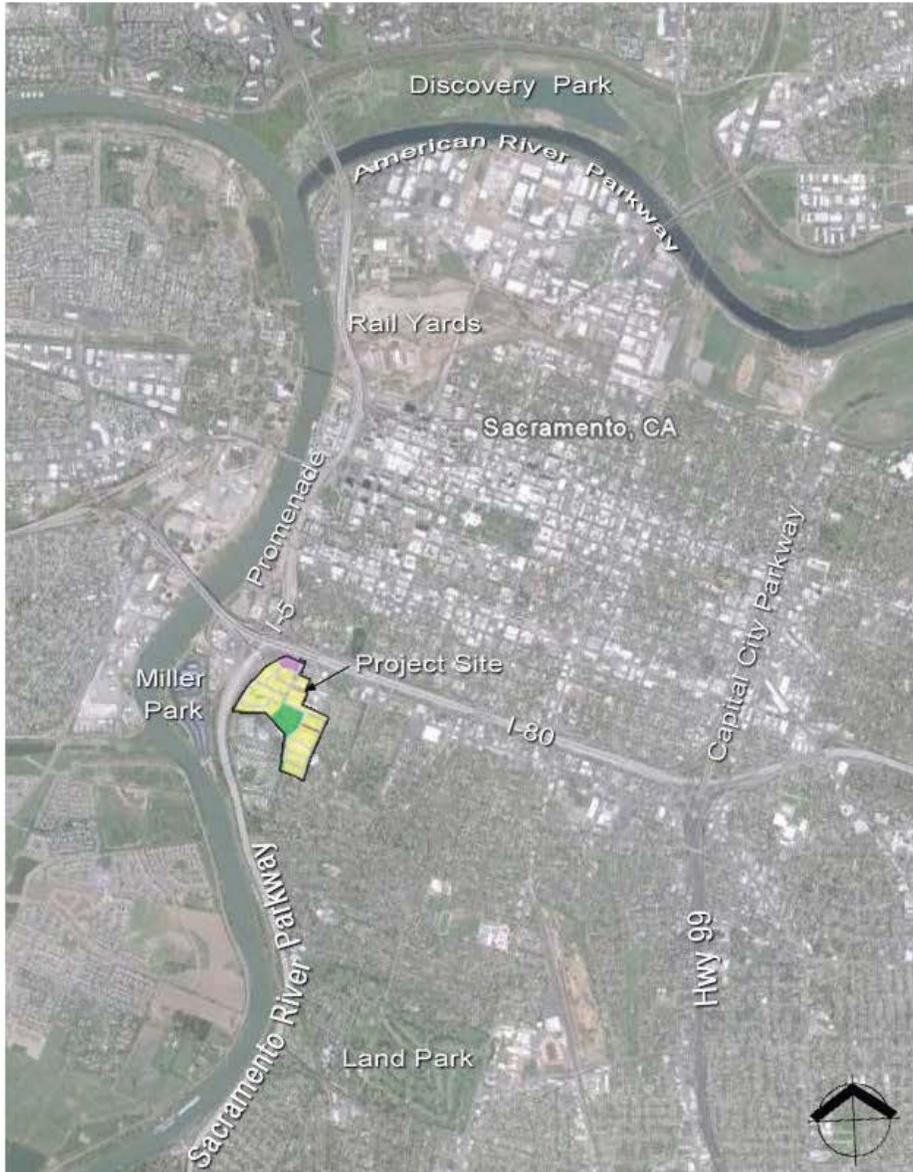
### 1.1 - Project Overview

#### *1.1.1 - Location and Setting*

Northwest Land Park (the "project") is located in the City of Sacramento's Land Park neighborhood. The project site is nestled south of the Central City and located amongst other well-established neighborhoods. The site is bounded by Broadway to the north, 5th Street to the east, McClatchy way to the south and Interstate 5 to the West. The Sacramento River is located one half mile to the west. Across I-5 to the west, Miller Park and the Sacramento Marina provide recreational access to the Sacramento River. Southside Park is located within ¼ mile of the project on the north side of Broadway.

The project is accessed regionally via Interstate 5 and Highway 50. Vehicular and pedestrian access points to the project site are provided by Broadway, 3rd and 5th Streets, 1st Avenue, and McClatchy Way. Sacramento Regional Transit District has both bus and Capitol Shuttle routes on Broadway north of the project site. A bus route is also located to the east of the site, on 5th Street. The nearest light rail station is approximately one mile to the north at "O" Street and 7th Street. An existing rail tunnel under I-5 could potentially connect the property to Front Street and the existing park amenities along the Sacramento River.

The project is planned for redevelopment as a vibrant, urban neighborhood, consistent with the General Plan. Redevelopment along Broadway and other primary roadway connections are planned with pedestrian scale design elements.



Vicinity Map

1.1.2 - Existing Conditions

The site is predominantly covered by commercial and industrial structures and impervious surfaces. Some maintained landscaping surrounds an existing office building at the northeast corner of 3rd Street and 1st Avenue. Existing uses on the project site include light industrial, office, and commercial, including the Setzer Forest Products plant and various produce storage and distribution facilities. (see Map 1.A)

Surrounding land uses include a mix of light industrial, commercial and residential. A mix of commercial and industrial uses is located along Broadway and along the northern project boundaries. The streetscape along 5th Street also projects a mix of commercial and industrial uses. The area transitions to multi-family residential areas and a city cemetery further eastward. To the south of the site are properties owned by the Sacramento Housing and Redevelopment Authority. Jedediah Smith Elementary and Arthur Benjamin High School abut the project to the south.

1.1.3 – Sacramento General Plan

The City of Sacramento 2030 General Plan land use designations for the project site are Urban Neighborhood Medium Density and Urban Corridor Low. The General Plan designation "Urban Corridor Low" applies to the northernmost portion of the site adjacent to Broadway and allows minimum density of 20 dwelling units per net acre and maximum 110 dwelling units per net acre. The project proposes mixed-use development on this portion of the site with 24 to 70 dwelling units on 1.2 net acres. The mixed-use development proposes a density range between 20 and 58 dwelling units per net acre.

The "Urban Neighborhood Medium" designation applies to the majority of the project site and allows for minimum densities of 33 dwelling units per acre and maximum 110 dwelling units per net acre. The project proposes 667 to 801 residential units within 20.2 net acres in this designation. This proposes a density range between of 33 and 40 dwelling units per net acre.



Existing industrial buildings on the project site, June 2010



Left – Existing office building at 3<sup>rd</sup> Street and 1<sup>st</sup> Avenue.  
Right - Existing industrial development along 5<sup>th</sup> Street



Map 1.A - Existing Conditions Map

**This page intentionally left blank**

### 1.2 – Vision and Goals

The project will develop as a vibrant, livable residential infill community reflective of and integrated into the existing Land Park and Southside Park neighborhoods stemming from their roots, history, and character." The overarching goal of the proposed Project is the orderly and systematic development of an integrated, residential and mixed-use community that is consistent with the goals and policies of the City's 2030 General Plan. The project is envisioned to develop as a distinctive neighborhood with a maximum of 825 dwelling units focused around a signature central park. The final unit count may vary but will fall within the permitted density ranges. In support of this overarching goal, the following objectives for the proposed project have been developed:

- To develop a new, medium-density urban residential and mixed-use neighborhood in reasonable proximity to the existing Downtown/Central City urban center consistent with the City's vision for new residential development, as laid out in the 2030 General Plan land use designations.
- To make efficient use of an opportunity for redevelopment of a developed site within the existing Land Park neighborhood and the Downtown/Central City Sacramento urban center.
- To design a development with a physical layout and land use mix that promotes walking to services, biking, and transit use.
- To incorporate public parks and open space into the project design in a manner that provides recreational opportunities for neighborhood residents and is aesthetically pleasing.
- To develop a residential community in proximity to the major employment centers of downtown Sacramento in order to help reduce overall commuter traffic volumes.
- To recycle as much material as possible during the demolition and construction phases of the project.
- To develop a residential neighborhood that will architecturally complement the existing established Land Park, Southside Park, Broadway, Midtown and Central City neighborhoods.



### 1.3 – Community Master Plan

The +/-31-acre Northwest Land Park community is designed as a medium density residential infill project focused around a signature park and the neighborhood's existing school facilities. The community is designed to respond to the area's existing street patterns and redevelop as a logical extension of the urban form. Residential areas are planned with a mix of housing types to create visual interest, housing variety and choice, and to promote a mix of incomes within the community. A mixed-use node is planned at the project's gateway along Broadway to provide neighborhood-oriented retail and civic uses within close proximity. The community is designed to capitalize on existing assets including transportation, pedestrian connections, open space, and civic uses. The Community Master Plan illustrates the project's physical design and spatial arrangement of roads, pedestrian paths, open space & parks, residential areas, and mixed-use districts.

Northwest Land Park provides a medium density housing choice that is proximate to employment, services, and cultural venues within the urban core. The majority of residents could access retail, business, and recreational facilities without driving an automobile. The Applicant is working with the City and studying possible reuse of the railroad undercrossing as a "rails to trails" connection for pedestrian and bicycles to access Front Street, the Sacramento River Parkway, the Promenade, the R Street bicycle bridge, Old Sacramento, West Sacramento via Pioneer Bridge, the future Railyards development and the American River Parkway. This off-street connection, along with direct sidewalk and bicycle lane connections on existing fully improved surface streets such as 3rd through 10th Street, provides an opportunity for an unimpeded pedestrian and bicycle system.

The site abuts Jedediah Smith Elementary School, Arthur Benjamin High School, and is just over 1 mile to the adjoined school campus serving the site. The site is six blocks from Southside Park pool and tennis facilities, 1.5 miles from the vast array of recreational opportunities offered at William Land Park, Miller Park along the Sacramento River and connection to Sacramento River Parkway, O'Neil Park for sports recreation, and just over 2 miles to the main campus for Sacramento City College. These educational and recreational facilities are all within comfortable pedestrian and bicycle distances to the project with direct routes via fully improved residential streets and bike paths.

The community master plan includes several signature elements that will, collectively, contribute to the project's uniqueness and overall sense of place. The plan includes a Festival Street that is designed as both a local street and an outdoor plaza area that can be easily closed to vehicular traffic during civic events. The plan also includes creating an Open Area Market within the existing market club building. Additionally, the plan includes a portion of the existing wholesale produce building to be preserved and improved as a community Adaptive Reuse Building. This facility is located central to the park and can host an array of indoor community events. Finally, the plan includes two optional elements, the Metal Burner Structure and the Tunnel Option. If feasible, the existing metal burner structure may be preserved and restored to serve as a prominent community icon. If implemented, the existing rail tunnel under Interstate-5 may be improved as a designated pedestrian tunnel to link the community to the recreational amenities along the Sacramento River.

Specific site layout calls for several ease of use features that will encourage walking and bicycling within the project. These include prohibiting fence separations and providing direct pathway access from all units to sidewalks, leading to Setzer Run, the central park, an adaptive reuse building, an open air market, and the retail contained within the mixed use portion of the project at 3rd and Broadway. The internal connectivity, the central location, and proposed improvement of the direct off street access under the freeway to the Sacramento River Parkway through the "rails to trails" provides for sustainable living with a reduced need for daily use of a private automobile.



Map 1.B - Land Use Schematic Plan

Land Use/Community Elements	Permitted Development Intensity	
Residential District (RD)	FAR 1.5 - 4.0 / Density 33 - 110 du/a	Residential Buffer
Mixed Use District (MXD)	FAR 0.3 - 3.0 / Density 20 - 110 du/a	Metal Burner Structure Option
Open Space District (OSD)	FAR 1.5 - 4.0	Tunnel Option
Community Trails (approx. alignment)		
Adaptive Reuse Building (approx. location/ see standards)		
Open Air Market (approx. location/ see standards)		

Note: (1) - Total unit count not to exceed 825 dwelling units.  
 (2) - In the Mixed Use District, residential units are only allowed pursuant to the underlying zoning designation.

*1.3.1 – Use Summary*

The project replaces existing light industrial and commercial uses on the project site with up to 825 residential units, commercial-retail uses, parks and open space and opportunities for adaptive reuse of key existing structure. Specifically, the project land uses include:

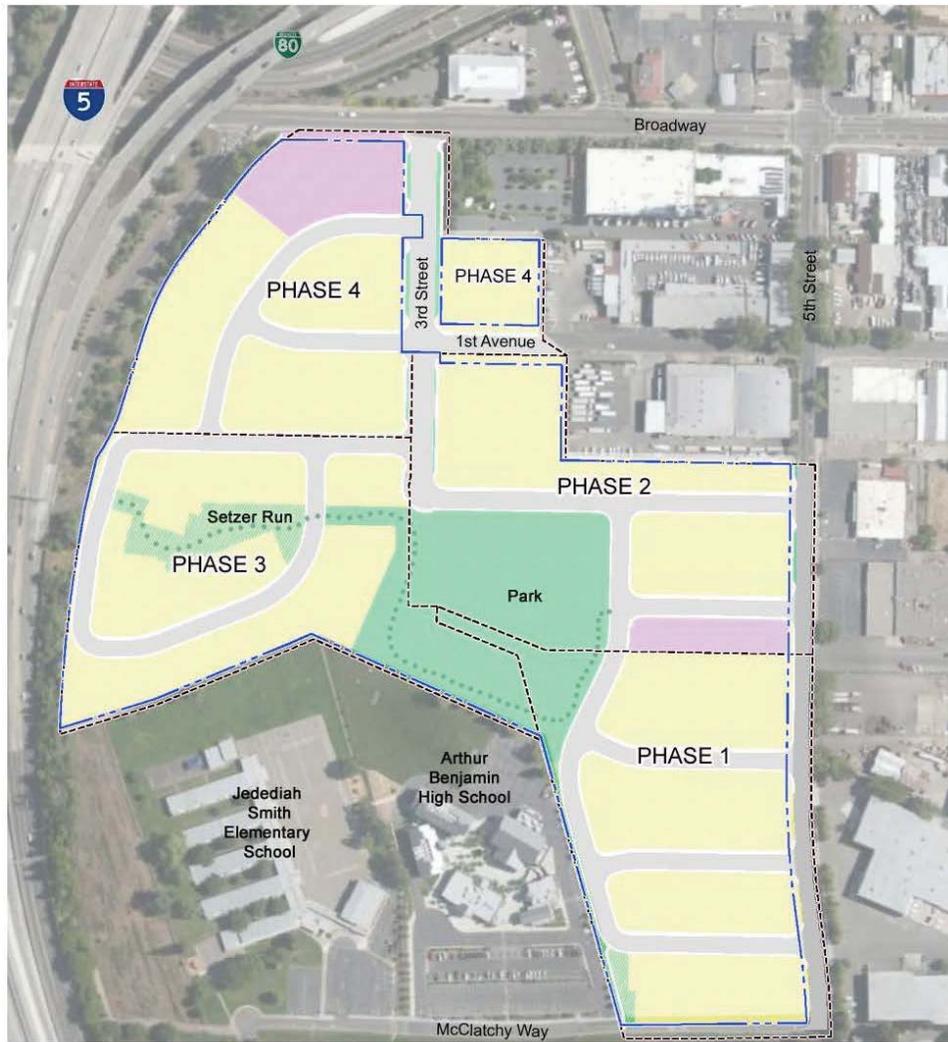
- up to 825 residential units with private open space on approximately 19.6 acres,
- including up to 70 high-density multi-family residences and up to 15,000 square feet of commercial-retail uses on approximately 1.2 acres,
- approximately 16,000 square feet of commercial retail, 2,200 square feet of restaurant uses, and 6,000 square feet of office space within the existing Farmers Market and Market Club building from the Market Club to 5<sup>th</sup> Street on approximately 0.6 acres
- approximately 4.3 acres of public park
- approximately 4.7 acres of private roadway
- approximately 1.2 acres of public rights-of-way.

*1.3.2 - Contextual Theming*

Northwest Land Park proposes to redevelop a long standing forest products industrial use (Setzer Forest Products) and the wholesale produce market. Through street names, adaptive re-use of existing structures, and reuse of selective iconic features, Northwest Land Park proposes to theme the new neighborhood to reflect its heritage. The project proposes to incorporate the iconic and visually-interesting pieces of the site's industrial and warehouse buildings. These features are planned to be incorporated into the new development as thematic sign entrances to adorn Setzer Run (the internal greenway), and to create interesting features and furnishings within the park. These could include recycling brick and large timbers to create raised entrance monument bases and planting beds within the public spaces; the strategic location of sawdust cyclones adorned with directional and monument signage; or even the adaptation of one of the large railcar hoppers into an overhead misting/cooling station along Setzer Run. Finally, the project will meet its Art in Public Spaces requirement by soliciting artists to utilize materials from the site to create industrial based interactive sculptures for placement within the park and/or Setzer Run.



*The site's existing structures and styles will be used to establish the community theming and character*



**Map 1.C - Density Plan**

Phase	Gross Area	Net Area	Resident Units	Proposed Net Density
1	8.1 ac.	5.5 ac.	201 du	38 du/a
2	7.9 ac.	4.0 ac.	132-190 du	33-47 du/a
3	8.5 ac.	5.7 ac.	188-270 du	33-47 du/a
4	6.0 ac (residential) 1.2 ac. (mixed-use)	5.0 ac. 1.2 ac.	165-237 du 24-70 du	33-47 du/a 0-58 du/a
<b>Total Project</b>	<b>31.7 ac.</b>	<b>21.4</b>	<b>691-825</b>	<b>33-40 (residential) 20-58 (mixed-use)</b>

Note: (1) Net acreage excludes areas of proposed streets and public park. Area is subject to change based on proposed street design.  
 (2) Residential units may be transferred amongst phases so long as the overall project density is consistent with the underlying General Plan Designation.  
 (3) Total unit count not to exceed 825 dwelling units.

#### 1.4 – Signature Elements

The Northwest Land Park community is conceptualized with signature elements that will be incorporated into the project by its build-out. These signature elements include a Festival Street designed to host civic events, an Open Air Market intended to re-use the existing market club building as a farmer's market and an Adaptive Reuse Building to preserve the whole sale produce building as a community space within the neighborhood park. The general locations are depicted on the Community Master Plan.

- Festival Street - The Festival Street should be considered as a transportation facility that boasts a second function as a vital civic gathering place. Periodically, to coincide with events and public gatherings, the Festival Street may be closed to vehicular traffic and function as an open outdoor plaza. Plan and design details for the Festival Street are discussed in the Transportation Framework section.
- Open Air Market - A farmer's market, tentatively named "open air market", is planned within the existing structures between the park and 5<sup>th</sup> Street to provide for the sale of local goods and produce. Development of the market would involve enhancing the existing Farmers Market and Market Club building from the Market Club to 5th Street to serve this new use.
- Adaptive Reuse Building – A portion of the existing wholesale produce building is located central to the neighborhood park. This structure will be modified as an adaptive reuse building to serve as a community-oriented facility to complement the neighborhood park. The facility is envisioned to support community gatherings, continued education, public health and safety, and other indoor community events. A park master plan will further explore this option.



Signature Elements Concept Plan Not to Scale  
Adaptive Reuse Building, Festival Street and Open Air Market

1.4.1 – Festival Street

The Festival Street should be considered as a transportation facility that boasts a second function as a vital civic gathering place. The Festival Street is an adaptation of a typical local street; it is designed with decorative pavers and includes perpendicular parking to serve the adjacent market use. The street is also designed as a gateway to the neighborhood, providing a distinctive connection between the market and the neighborhood park. Periodically, to coincide with events and public gatherings, the Festival Street may be closed to vehicular traffic and function as an open outdoor plaza. Plan and design details for the Festival Street are discussed in the following sections.



The image above is a conceptual rendering of a festival street next to an Open Air Market. The illustration depicts a time when the facility is closed to vehicular traffic and functions as a pedestrian plaza.  
Note: Conceptual Design

1.4.2 – Adaptive Reuse Building

The barrel roofed clear-span section of the wholesale produce building located completely within the proposed park and at the termination of Third Street, provides an ideal opportunity to provide an adaptive reuse building to serve Northwest Land Park, Smith Elementary, Benjamin High School, and the surrounding neighborhood. Northwest Land Park envisions the opportunity to adaptively re-use the structure to house classrooms, exercise rooms, computer lab, and other community services facilities. Based on the location within the proposed park and the proximity to the elementary and high school, the likely programming of the neighborhood center would be to enhance and support the education and service needs of the children within the area. The immediate availability of an existing building provides the opportunity to implement the vision of a community-oriented facility. The open nature of the existing structure, including large floor to ceiling doorways and extensive covered elevated outdoor space, lends itself to future opportunities to expand and enhance the neighborhood center over time.

The dock height of the existing structure provides a unique opportunity to reduce run-off and on-site water needs through its existing floor height. By incorporating inexpensive above ground water pillows under the new elevated terraces, the roof run-off from the building can be captured, stored, and utilized for irrigation of the surrounding park landscape. The project will be looking for similar economically viable opportunities to creatively reduce the impact and expand the sustainability of each and every component of the project.



Existing Wholesale Produce Building to be modified as an adaptive reuse building to be used in the neighborhood park.



Conceptual rendering of the adaptive reuse building in the community's central park.  
Note: Conceptual Design.

1.4.3 – Open Air Market

The existing two-story Farmers Market and Market Club building from the park to 5th Street, provide a unique opportunity to convert a wholesale produce area into an "Open Air Market" serving as a year-round produce stand, local and regional gathering space, and food-oriented celebration of the bounty of Northern California. The use is a retail operation with anticipated standard retail hours and a standard retail week. The structures will be adapted to interior semi-permanent retail booths for produce, specialty foods, crafts, and regional and ethnic prepared meals. The large exterior covered docks will house seasonal booths and provide all weather outdoor spaces for gathering and picnicking. The adjoining local roadway will be developed as a Festival Street and will be periodically closed to vehicular traffic. The Festival Street shall be programmed for short term street fairs, art festivals, and other community gatherings with booths and venues taking over the entire block from 5th Street to the park.



(Left) Existing Market Club restaurant. Existing Farmers Market building that may become the Open Air Market



Conceptual rendering of the Open Air Market in the Farmers Market Building.  
Note: Conceptual Design

### 1.5 – Optional Elements

The Northwest Land Park community is conceptualized with two optional elements that may be incorporated into the project by its build-out. These optional elements include preservation and re-use of the existing metal burner structure and the creation of a pedestrian connection under Interstate-5 in an existing rail tunnel. These are optional elements because their establishment depends on additional planning, approvals, and/or potential partnerships with other government agencies. Specifically, the tunnel option requires an intergovernmental partnership to overcome the complexities regarding land ownership, maintenance, and security.

- Metal Burner Structure Option – Metal Burner cylindrical structure located near the railroad undercrossing may be preserved and modified as a distinctive community icon.
- Tunnel Option – The existing rail tunnel under Interstate-5 may be improved to create a pedestrian and bicycle connection between the Northwest Land Park community and Miller Park located along the Sacramento River.

#### 1.5.1 – Metal Burner Structure

Metal Burner cylindrical structure located near the railroad undercrossing may be modestly refurbished to provide a freeway-visible icon for the project. The likely adaptive re-uses for the structure could include housing and screening for both project-serving (such as wireless intranet) and neighborhood-serving telecommunications equipment, security video surveillance equipment, and community lighting fixtures. As part of this program, the project will review the opportunity to re-create the burner dome screen feature to enhance and expand the iconic nature of the structure as a monument.



*Existing Metal Burner Structure and metal work that may be used in project theming and sculptures.*

1.5.2 – Tunnel Option

The Northwest Land Park community is conceptually envisioned to be connected to Miller Park and the Sacramento River Parkway by a pedestrian/bicycle connection under Interstate 5. This connection can be created using the existing rail tunnel under the freeway to create pedestrian linkage for the immediately surrounding neighborhoods. The tunnel is an optional element since it requires coordination and approval from an array of state and local agencies. If developed, the tunnel will be enhanced with paving, lighting, wayfinding, and security enhancements for pedestrian and bicycle use.



**Tunnel Option - Schematic Trail Plan**



The existing rail tunnel is envisioned to be transformed into a pedestrian/bicycle pathway and enhanced with lighting, art, and pavement.

Existing Rail tunnel (left) may be transformed into an active pedestrian and bicycle connection under I-5 (right). Other examples of pedestrian tunnels and passages are illustrated to the right.

#### 1.6 – Community Phasing Plan

The Northwest Land Park community will be developed according to a defined sequencing plan and includes four phases. The phasing plan allows the initial community phases to emerge on vacant land and allows for existing uses to gradually transition out of the community. The initial phase will commence at the southeast corner of the project adjacent to established residential districts, Jedediah Smith Elementary, and the Arthur Benjamin High School. The project will progressively expand northwestward towards its build-out.

The plan includes a park located central to the overall redevelopment project. Open space land will be incrementally dedicated as part of Phases 2-4 to allow existing industrial operations and structures to be gradually removed. This will also allow for comprehensive, cost effective, and thorough site clearing for the project's future uses including residential, recreation, and mixed-use developments.

Utilities and roadway improvements will coincide with the phasing plan. Specific utility and street improvement plans may require minor revisions to future phase lines to achieve cost effective planning and installation of these essential services. Detailed utility planning and street improvements will occur as part of actual site design and as part of the City's development review process. The existing legal nonconforming industrial uses including the Setzer operations may be phased out through the progression of the Northwest Land Park redevelopment.

The Community Phasing Plan (see Map 1.D) illustrates the general phasing and sequencing of project development. Phasing boundaries may vary to respond to infrastructure planning and/or construction.



Map 1.D - Community Phasing Plan

- Residential District (RD)
- Mixed Use District (MXD)
- Open Space District (OSD)
- Private Courtyards (approx. location / see standards)
- Community Trails (approx. alignment)

### 1.7 – Framework Plans

These Planned Unit Development (PUD) Guidelines are presented as a series of Community Design Framework Plans that when combined create the overall project design. These Framework Plans include Land Use, Open Space, Transportation, Landscape, and Sustainability & Energy Efficiency. The subsequent sections of this document will highlight and articulate each of the Framework Plans that establish development guidelines for their inception.

### 1.8 – Purpose

The purpose of these PUD Guidelines (Guidelines) is to define the project vision and ensure quality development within the Northwest Land Park community. These Guidelines establish development styles and development standards specific to the Northwest Land Park community that may not otherwise be possible when applying standard zoning regulations. These Guidelines should be applied to all development within the project to ensure that all development and maintenance within Northwest Land Park produce a unified and integrated community. The PUD Guidelines will be used in the planning and design of all new projects within Northwest Land Park community boundaries and in conjunction with other City standards and reviews. The Guidelines are binding on the Project and will be implemented consistent with the provisions set forth in the City's Municipal Code. The PUD Guidelines include the following components to convey the desired development and design within the community:

- Development Guidelines – Guidelines aim to establish styles and standards unique to the Northwest Land Park community.
- Development Standards – Standards are development requirements that include exact dimensions and limitations. Development standards include, but are not limited to, density, setbacks, building separation, structure height, and parking ratios.
- Development Styles – Styles are design elements and design methods that express the ideal and preferred design. Styles are descriptive in nature and articulate how particular site elements should be designed and developed. Styles include, but are not limited to, building orientation, pedestrian access, color, and preferred architectural elements.
- Objectives – Objectives have been established as goals, consistent with the 2030 General Plan, that are intended to be achieved by the adoption and successful implementation of these Guidelines.
- Written and graphic illustrations of desired development styles – Illustrations convey ideal design elements and are only intended to articulate development styles in a visual form.

### 1.9 - Administration

These Guidelines implement the goals and policies of the City of Sacramento General Plan while creating specific community goals and development standards unique to the project boundaries. These Guidelines support the existing Sacramento Zoning Code. In some cases, these Guidelines create design flexibility for certain community design components and impose more specific design standards unique to this area.

The City of Sacramento will use these PUD Guidelines to review development proposals within the Northwest Land Park community. All development, subsequent tentative and final maps for Phases 2-4, land use activity, and maintenance plans within Northwest Land Park shall comply with these Guidelines. Where these Guidelines are silent or unspecified, the Sacramento Zoning Code shall apply. Amendments to these Guidelines and the standards herein are subject to review and approval as required in the Sacramento Zoning Code.

### 1.10 - Definitions

The terms herein shall be consistent with the Sacramento Zoning Code and as specifically described in these Guidelines.

## Chapter 2 – Land Use Framework

### 2.1 – Land Use Framework

Northwest Land Park - Land Use Framework Plan (See Map 2.A) establishes three specific land use districts within the project boundary. These districts will support a particular development type and project a specific community character. The land use districts are Residential (RD), Mixed-Use (MXD), and Open Space (OSD). The district boundaries coincide with the underlying zoning designation and General Plan designations to ensure the proposed development and uses are consistent with the City-wide goals and allowable development activity. All land use densities and intensities are based on net acres, exclusive of public streets, private streets constructed to City standards, and public parks. These PUD guidelines include permitted land uses and establish unique development standards specific to the Northwest Land Park community and supplant the underlying zoning standards unless the Zoning Code is silent.

### 2.2 – Land Use Districts

#### 2.2.1 – Residential District (RD)

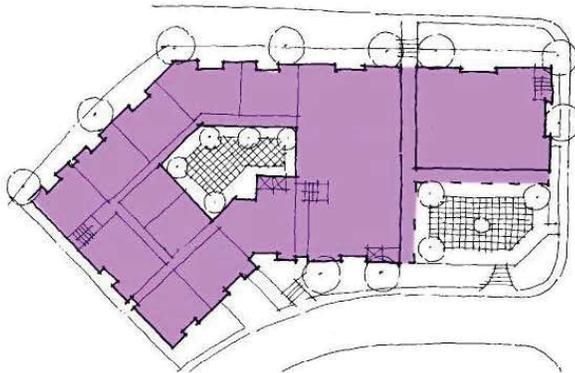
The Residential District supports medium density residential development and a variety of compact, urban-scaled housing types. The district is characterized by multiple family dwellings, duplexes, cluster housing, and single-family detached condominiums. The Residential District supports ownership types including apartment rentals and condominiums. The RD district typically supports densities between 33 and 58 dwelling units per net acre. The RD is implemented with its underlying residential zoning designation and the PUD.

The majority of the district will develop as condominium units arranged on commonly owned parcels. The Residential District allows all the aforementioned housing types and limited community buildings. The RD allows some non-residential uses as provided in the Sacramento Zoning Code and Development Agreement, so long as the site design and architecture are complementary to the primary residential uses and consistent with these Guidelines.

Existing non-residential uses may be established as the *Open Air Market* using the existing Farmers Market and Market Club building to strengthen the project's goal to create a compact mixed-use development that celebrates the site's heritage and past context. The Open Air Market includes retail, restaurant and office uses established in the RD district between the park and 5<sup>th</sup> Street. These non-residential uses within the RD may require additional land use approvals and entitlements consistent with the Sacramento Zoning Code. This use may also require additional agreements and contracts with the City pursuant to an executed Development Agreement (DA).

### 2.2.2 – Mixed-Use District (MXD)

The Mixed-Use District supports high density residential and complementing commercial and service uses that will serve the Northwest Land Park community. The Mixed Use District is characterized by compact, pedestrian-oriented development with buildings oriented close to public streets and creating vibrant street activity. The MXD district is planned to act as a transitional area between other intensive, non-residential uses along Broadway and the planned residential and park areas to the south. The Mixed-Use District is planned with a Floor Area Ratio (FAR) ranging between 0.3 and 3.0 in order to promote the distinctively-urban scale envisioned for the community. The MXD district supports densities between 20 and 110 dwelling units per net acre. The MXD is implemented with its underlying C-2 zoning designation and the PUD. Buildings shall be designed in accordance with the Central City development guidelines.



Potential building plan for the Mixed-Use District.  
Note: Conceptual Design

### 2.2.3– Open Space District (OSD)

The Open Space District supports parks, recreational areas, trails, and civic facilities for the Northwest Land Park Community. The Open Space District is characterized by special areas for community outdoor recreation and is planned as a public space for City residents. The OSD is envisioned to support other uses that complement and are accessory to the recreational intent. The neighborhood park will be further designed as part of a City of Sacramento park master plan. Significant privately own recreational tracts also occupy the OSD. The OSD is implemented with its underlying residential zoning designation and the PUD.



Map 2.A - Land Use Framework Plan

Land Use/Community Elements	Permitted Development Intensity	
Residential District (RD)	FAR 1.5 - 4.0 / Density 33 - 110 du/a	
Mixed Use District (MXD)	FAR 0.3 - 3.0 / Density 20 - 110 du/a	
Open Space District (OSD)	FAR 1.5 - 4.0	
Community Trails (approx. alignment)		
Adaptive Reuse Building (approx. location/ see standards)		Metal Burner Structure Option
Open Air Market (approx. location/ see standards)		Tunnel Option

Note: In the Mixed Use District, residential units are only allowed pursuant to the underlying zoning designation.

2.3 – Permitted Land Uses

<b>Table 2.3: Permitted Uses/Intensity</b>		
<i>Land Use District</i>	<i>Permitted Uses / Building Types</i>	<i>Intensity(FAR / Density)</i>
Residential (RD)	Land uses listed for underlying zone in the Sacramento Zoning Code, Non-residential uses per the Development Agreement.	(All Underlying Zones) FAR 1.5 – 4.0 / Density 33-110 du/ac
Mixed Use (MXD) <sup>(2)</sup>	Land uses listed for the underlying zone in the Sacramento Zoning Code	(C-1 Underlying Zone) FAR 1.5 – 4.0 / Density 33-110 du/ac AND (All other Zones) FAR 0.3– 3.0 / Density 20-110 du/ac
Open Space (OSD)	Recreational and Institutional uses listed for the underlying zone in the Sacramento Zoning Code	FAR 1.5 – 4.0
Notes: (1) Land uses within the Northwest Land Park land use districts shall be permitted, reviewed, and approved pursuant to the City of Sacramento Zoning Code. Allowable land uses are based on the underlying zoning district and Development Agreement. (2) The building intensity are FAR and density ranges as stated in the General Plan for the underlying land use designations		

2.4 – Residential Building Types

The Northwest Land Park community includes a variety of medium and high density residential building types. The community goal is to achieve an eclectic and diverse housing mix. Building types make no distinction between ownership types. The community may include the residential housing types listed in Table 2.3 of these PUD Guidelines and those permitted by the underlying zoning designation. The following development standards define predominate residential buildings for the Northwest Land Park community and amend the definitions for said housing types stated in the Sacramento Zoning Code:

*Cluster Housing*

Cluster housing is described as a grouping of two or more single family dwelling units connected with at least one common exterior wall or significant architectural element such as porches, verandas, and balconies, but not under the same roof. Units within a single cluster are arranged side by side along a parcel or tract. Dwelling units front on a street, open space tract, or courtyard. Rowhouses and townhouse buildings are common examples of Cluster Houses as applied to the Northwest Land Park community.

*Duplex*

A duplex building is described as two individual dwelling units located in a single building. Dwelling units may be arranged either side by side or atop one another. A duplex is built as a single structure but may be connected to neighboring buildings by secondary architectural elements such as porches, verandas, and balconies.

*Multiple-Family Dwellings/Apartments*

A multiple-family dwelling/apartment building is described as three or more individual dwelling units located in a single building. Dwelling units may be arranged either side by side or atop one another. The front façade and primary entrance front on a street, open space tract, or courtyard.

*Single-Family Detached Condominiums*

A single-family detached condominium is described as a stand-alone building containing one dwelling unit. These units are situated on common lots shared and maintained by a property owners association.



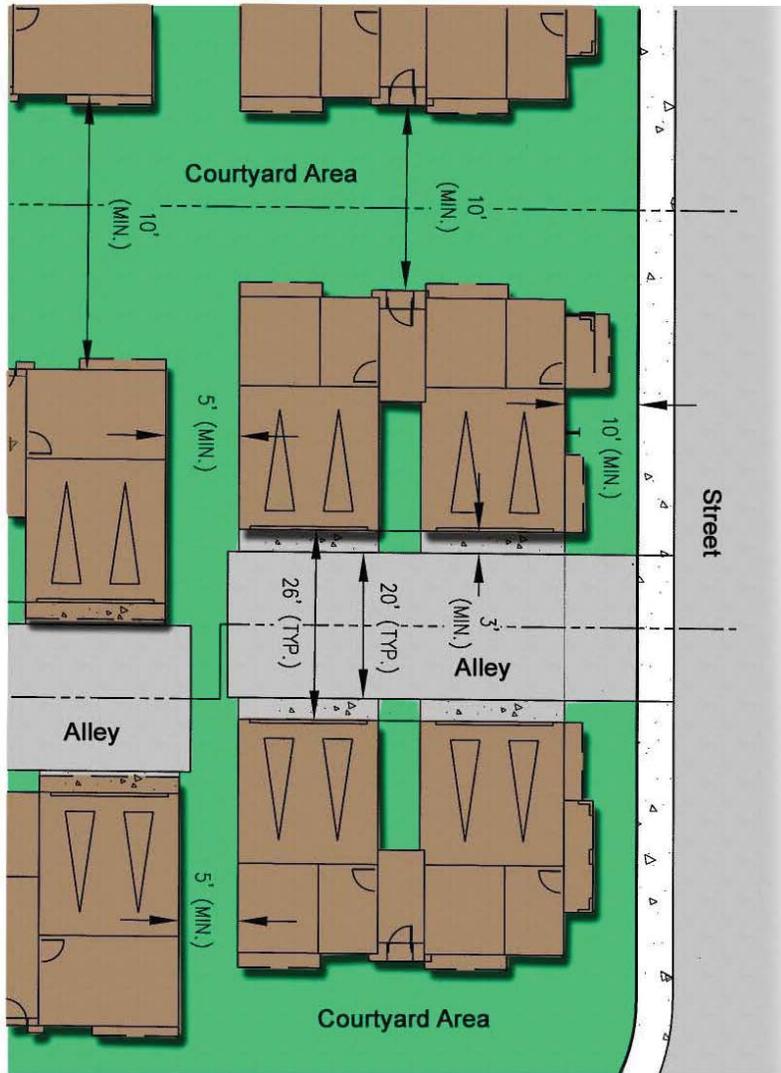
*Cluster Housing Example*



*Duplex Example*



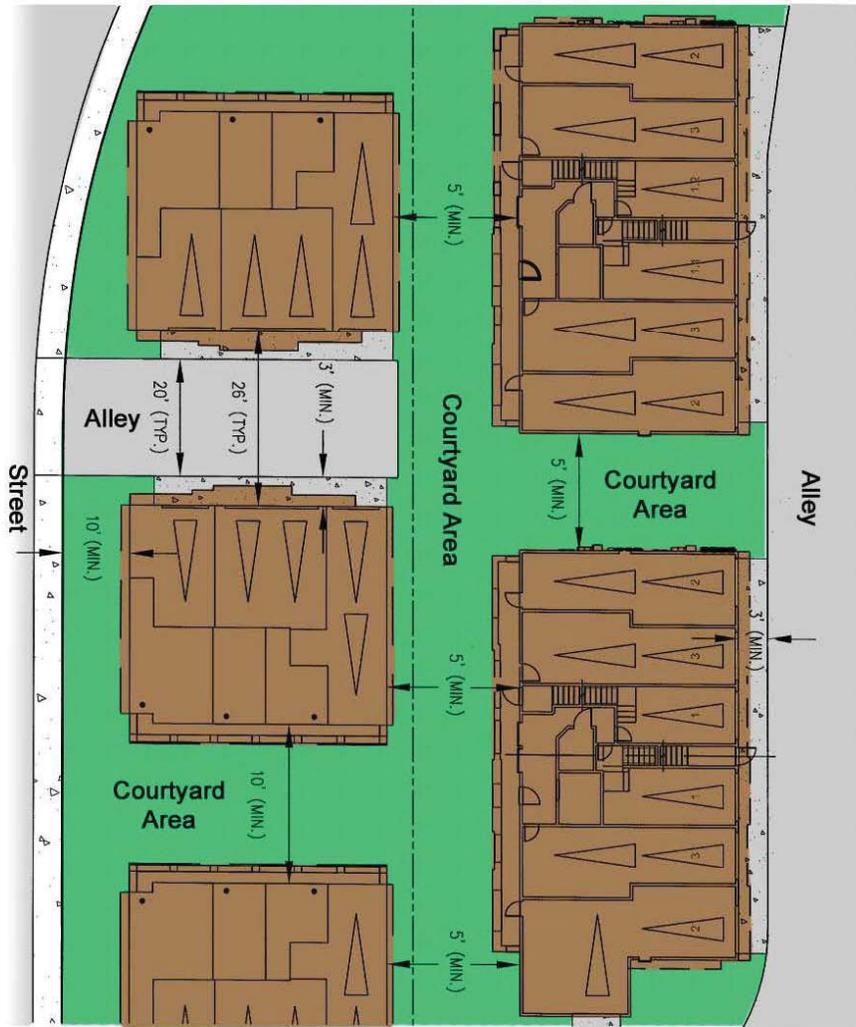
*Multiple-Family Dwelling/Apartments example*



**Plot Plan - Cluster Housing**

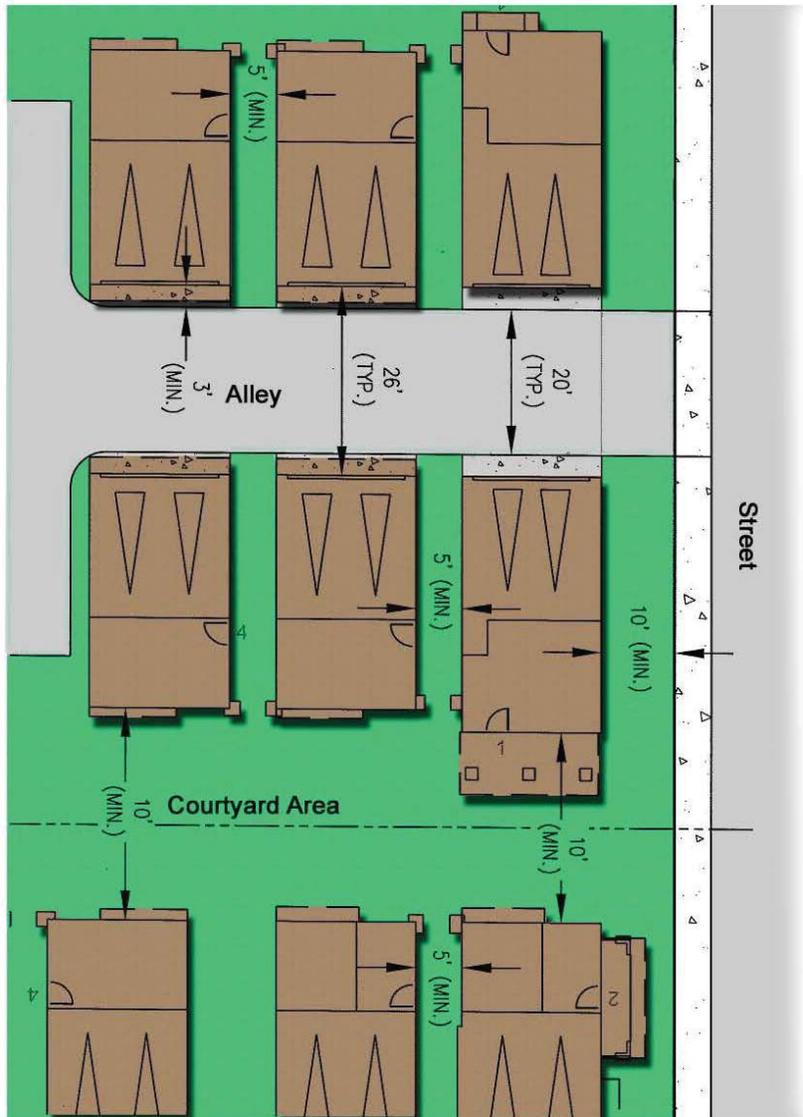
Note: Dimensions are typical setbacks and building separation.





**Plot Plan - Multiple Family**

Note: Dimensions are typical setbacks and building separation.



**Plot Plan - Single Family Condos**

Note: Dimensions are typical setbacks and building separation.

2.5 – Dimensional Standards

<b>Table 2.5: Dimensional Standards</b>									
Building Type	Max. Lot Coverage	Min. Lot Area/ Lot Width (8)	Primary Building Setbacks (1)(2)(3)(4)		Garage Face Setbacks (5)(11)		Building Separation (1)(3)(4)(8)		Height (6)(9)(7)
			Min./ Max. Street	Min. Side/ Rear	Min. Street Loading	Min. Private Alley/ Access -way Loading	Minimum Front Façade to other façade	Minimum Side/Rear Facades to other facades	
<b>Residential District (RD)</b>									
Non-Residential	90%	N/A	10'/20'	5'	5'	3'	15'	10'	45-ft
Residential	90%	N/A/ 1,200 sf/ 20'(8)	10'/20'	5'	5'	3'	10'	5'	40-ft / 55-ft (7)
<b>Open Space District (OSD)</b>									
All Buildings	N/A	N/A	0'/25'	5'	0'	0'	10'	6'	35-ft
<b>Mixed Use District (MXD)</b>									
All Buildings	N/A	N/A	0'/25'	5'	0'	0'	10'	0'	75-ft
<p>Notes:</p> <ol style="list-style-type: none"> <li>No building setback is required in the RD between a new residential structure and an existing non-residential building, aside from what might be required by the Building Code.</li> <li>Architectural features such as chimneys, clock towers, and photovoltaics, etc. may extend beyond the maximum structural height.</li> <li>Upper story portions of a building may project into the setback and building separation areas according to the following: (a) Side setback/separation area: one and a half (1.5) feet, (b) Front setback/separation area: three (3) feet (c) Rear setback/separation area: two (2) feet OR five (5) feet when an alley or accessway is present. This standard is subject to the building code.</li> <li>These PUD guidelines allow for certain exceptions to the building setbacks where a structure abuts or extends into the required Landscape Setback as described in Section 5.3.2. Setback standards stated in Section 5.3.2. may supersede setback and building separation standards herein. In some cases, Section 5.3.2 may allow for lesser setback standards.</li> <li>Garage setbacks shall be measured from the garage door to the right-of-way edge for front loading designs and to the paving edges of alleys or accessways for rear loading designs.</li> <li>Building height is measured as the vertical dimension from the average elevation of the finished lot grade at the front of the building to the plate line, where the roof meets the wall.</li> <li>Residential buildings shall be limited to 40-feet of structure height. Alternatively, buildings with four or more units may be permitted a maximum structure height of 55-feet.</li> <li>Individual, fee simple lots shall be applicable to the lot dimensional standards. Residential units developed as condominiums and located on a common/shared lot shall be exempt from the lot dimensional requirements.</li> <li>Architectural projections attached to the ground such as chimneys, bay windows, stairs etc. may project into any setback by two (2) feet for a maximum width of ten (10) feet. Porches, patios, and balconies are permitted a 3 (three) foot street setback.</li> <li>The front façade shall be the principal face of any building generally hosting the majority of the openings and/or projecting the most prominent façade treatments. The front façade may be designated by the developer at the time of development. The front façade may either face a street, open space tract, and/or courtyard. The rear façade shall be the building face on the opposite end of the structure as the front façade.</li> <li>An accessway is defined as a private, shared driveway or motor court serving one or more buildings and is usually located behind the rear façade.</li> <li>No building separation shall be required where a structure is connected to a neighboring building by an architectural feature such as a shared exterior wall, porch, or balcony. (Cluster Housing - Bungalows)</li> <li>Table 2.3 establishes the FAR and density ranges for each land use district.</li> <li>Residential development that is part of a mixed-use building shall comply with the allowed floor area ratio (FAR) range and is not subject to the allowed density range. Stand-alone residential development shall comply with the allowed density range.</li> </ol>									

## 2.6 – Site Development Guidelines

### 2.6.1 – Building Orientation

The community shall be developed as an inviting, pedestrian-oriented neighborhood that encourages human interaction. The community will be both compatible with and reflective of Land Park and the adjoining Southside Park neighborhoods. Buildings shall be designed and oriented directly to public and semi-public spaces. In addition to the dimensional standards listed in table 2.4, buildings shall be planned and sited with a focus on the following development styles:

- **Front Façades** - Front Façades shall be oriented to a street, public open space, and/or courtyards.
- **Façades Treatments** – All façades facing a street, public open space, courtyard, alley, or accessway shall include windows and/or distinctive architectural elements or materials to avoid large monotonous wall planes.
- **Garage Doors** – Garage doors should be oriented to a rear or side alley or accessway. Garages may face a street so long as they are set back two (2) feet from the primary exterior wall and architectural treatments downplay their prominence on the building façade.
- **Building Entrances** – Buildings shall be designed to create a clear distinction of the main entrance(s). Buildings shall provide a clear pedestrian connection to a nearby street.
- **Streetscape Diversity** – Blocks will provide architectural diversity among neighboring buildings. Neighboring buildings should be distinctively different in terms of elevation design, exterior materials, color, and/or height. No more than two individual buildings with the same elevation shall be located side by side on a given block.
- **Service Areas** – Service and loading areas should be oriented away from an adjacent street. Where a site abuts multiple streets, a service and loading area may be developed near a street so long as a continuous wall screens the area from off-site view to the greatest extent practicable.



*Building facades oriented to streets, parks and courtyards*

### 2.6.2 – Parking

Parking areas serving private development within the community should be secondary and less prominent than the primary uses and open space areas of the neighborhood. Additionally, parking demand will be reduced due to the community's distinct urban environment which provides on-street parking and creates a highly pedestrian-oriented atmosphere. Parking design serving private development shall focus on the following development standards:

- Private Parking Lots – Surface parking lots should be sited and planned to be less prominent than the building and use they serve. Parking lots shall be buffered from the public right-of-way with a continuous hedge or non-opaque wall/fencing.
- On-site Parking Location - Parking lots should be located behind or to the side of adjacent buildings. A surface parking lot should not be located between a public roadway and the street-facing exterior wall of a building; for parcels that front on multiple streets, this standard shall only apply to one of the streets. Parking areas should be adequately screened either through landscaping or other screening feature.
- Private Garage Parking – Parking for private development may be provided in individual garages or within parking structures. When parking for private residential units is provided within a garage or structure, said garage or structure shall be located within 300 feet of the unit it is intended to serve. The minimum dimensions for a private garage is 9-ft x 18-ft. Driveway lengths shall be consistent with the building setbacks/building separation standards listed in Table 2.4.
- Required Parking Quantity – Parking shall be provided for all uses according to the table below. Private residential garages, parking structures, and surface lots shall be counted towards this parking requirement.
- Street Parking – Street parking shall be provided along all internal local roadways to serve guests and business patrons of the community.
- Parking Exemption – The Open Air Market and the Adaptive Reuse Building shall be exempt from any parking standards. These buildings are intended to complement the Northwest Land Park community as a vibrant civic gathering place. The buildings are envisioned to attract many of its patrons from the immediately surrounding area and contribute to a highly pedestrian-oriented and bicycle-friendly community. The site shall provide direct linkages to the multi-use trail, which runs through the project and existing City bikeways along 5<sup>th</sup> Street, further promoting pedestrian and bicycle use.

Table 2.6.2: Required Parking		
Building Type	Required Motor Vehicle Parking Ratio	Required Bicycle Parking Ratio
<b>Residential</b>		
Cluster Housing	1 per unit	0.50 per unit OR 2 per building whichever is greater
Duplex	1 per unit	None
Multiple-Family Dwellings/ Apartments	1 per unit	0.50 per unit OR 2 per building whichever is greater
Single-Family Detached Condominiums	1 per unit	None
<b>Other Uses</b>		
Non-residential	25% less than City Standard	Per City Code
Notes:		
<ul style="list-style-type: none"> <li>(1) Required parking ratio is based on average stalls per unit within a shared building.</li> <li>(2) Private motor vehicle parking may be provided in private residential garages, parking structures, and surface lots. Tandem parking in private garages and private driveways is allowed.</li> <li>(3) Bicycle parking may be provided within private garages, parking structures, surface lots, and/or bicycle racks within adjacent courtyards.</li> <li>(4) Parking ratios are inclusive of resident and guest parking. No separate guest parking ratio shall apply.</li> <li>(5) For the purposes of these PUD guidelines, commercial athletic clubs, fitness centers, yoga studios and similar businesses shall be considered as <i>retail uses</i> for the purposes of calculating parking ratios.</li> <li>(6) The Open Air Market and Adaptive Reuse Building shall be exempt from any on-site parking requirement. Parking on surrounding streets may be used to satisfy the parking demand.</li> </ul>		

### 2.6.3 – Colors

A coordinated palette of colors shall be applied to all buildings, concrete and stucco walls, and architectural elements within the community. Color application shall be focused on the following development styles:

- Color Selection – Colors should be responsive to the natural environment and project low reflectivity in radiant sunlight. Color selections should include earth tones and rich tones. Stark whites, pure blacks, and florescent hues shall be avoided as primary façade colors. Wood stains shall be considered appropriate color applications.
- Color Palette - Buildings shall have a minimum of three colors in a coordinated and complementary palette. Trim and Fascia elements may be included in the minimum color requirements. Trim and Fascia elements shall be different from the primary exterior wall color.
- Color Variety - Buildings shall be painted with a variety of color palettes to provide visual diversity among individual structures. Residential buildings shall specifically have a different primary color than the neighboring structure.
- Garage Doors - Garage doors shall be painted to understate their prominence to the façade in which they are attached.
- Masonry Walls/Fencing – Masonry walls/fencing shall be painted complementing colors to nearby buildings within the Northwest Land Park Community. Color styles are only intended for concrete and stucco finished walls. Masonry walls/fencing may be designed with stone or finishes that resemble stone.



*Buildings with a variety of color palette*

2.6.4 – Materials List

A mix of coordinated materials may be used on any building within the community. The exterior building materials shall focus on the following development styles:

**Table 2.6.4: Exterior Materials**

**Exterior Wall Treatments**



Board & Batten



Horizontal Siding



Hardboard Shingle Siding



Plaster



Plaster – Heavy Texture



Brick



Stone – River Rock



Stone – Rustic Ledge



Photovoltaic Cells

**Roof Treatments (Sloped only)**



Roof Tile – Shingle



Roof Tile – Flat



Roof Tile – “S”

Note: Exterior walls and roofs should be focused on the appearance and texture of the above examples but not subject to specific products or natural materials. Alternative materials and concrete treatments that resemble the above examples are permitted on any building exterior. Flat roofs are not subject to the aforementioned material standards.

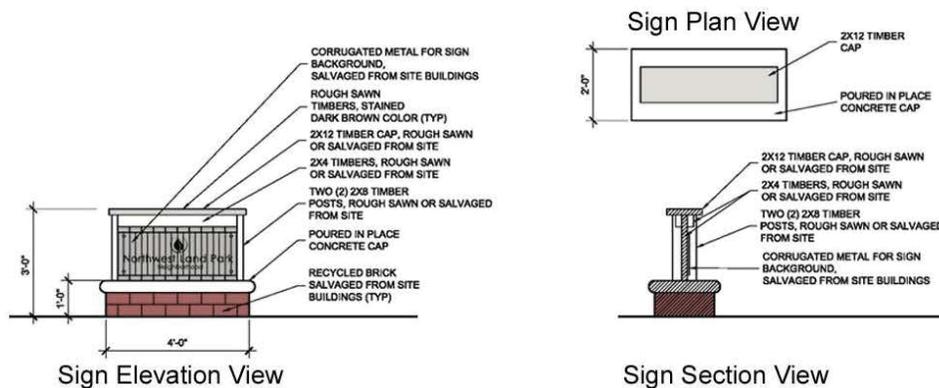
2.6.5 – Signage

Signage within the Northwest Land Park community shall project a consistent theme and style throughout the development. In doing so, a comprehensive signage design package shall be approved by the Planning Director.

The allowable signage in the Northwest Land Park community includes *Project Identification Signage*, *Neighborhood Identification Signage*, *street signage*, and *Directional Signage*. Project signage for community facilities and major destination within the neighborhood are also permitted. Street entry signage is envisioned at all neighborhood entryways from Broadway and 5<sup>th</sup> Street. Signage shall include complementary materials as found on community buildings and major architectural elements. Signage may also incorporate reuse material from the site's original buildings and structures.

Signage area and locational standards shall be consistent with the Sacramento Sign Code based on the underlying zone. Notwithstanding, a sign program, subject to approval by the Planning Director shall be submitted for any signage for the Open Air Market.

*Typical Neighborhood Identification Signage*



2.6.6 - Buffering

The Northwest Land Park community shall be planned and developed to buffer greatly differing land uses to ensure compatibility and livability. Buffering shall be focused on areas within the community that propose new residential buildings adjacent to existing industrial uses. Buffers shall be planned and constructed according to the following:

- Phase 2 Residential Buffer - A continuous buffer shall be constructed along the RD portions of the project boundaries that abut industrial uses and industrially zoned land. The buffer shall include, at a minimum, a ten (10) foot high uniform wall. The buffer wall shall be landscaped with fast-growing trees planted on the project side (see Appendix A, Condition 1).
- Phase 4 Residential Buffer - A continuous buffer shall be constructed within the RD portions of the community along the eastern project boundary that abuts industrial uses and industrially zoned land. The buffer shall include, at a minimum, a ten (10) foot high uniform wall. The buffer wall shall be landscaped with fast-growing trees planted on the project side. This buffer shall only be required to be constructed where residential

development will occur near or adjacent to the eastern boundary. A buffer is not required to be constructed directly along 3rd Street (see Appendix A, Condition 2).

## 2.7 – Architectural Styles

Northwest Land Park will include a variety of architectural styles and an eclectic mix of building styles to create a distinct neighborhood district. The proposed architectural styles at Land Park are historically based, drawing inspiration from the neighboring residential areas. The following architectural development styles represent the most prominent elevation and building design for the community.

### 2.7.1 – Spanish Style

#### Spanish Style Architectural Elements

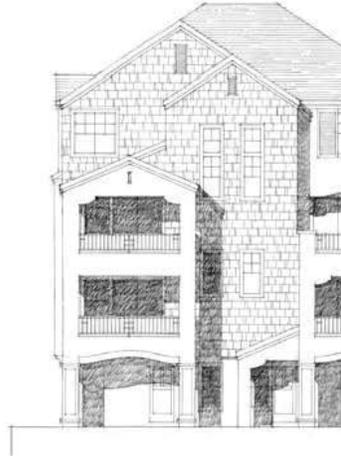
- Stucco exterior
- Square, arched and corbelled openings
- ‘S’ tile roof
- 4/12 roof slope
- Mostly gable roofs
- Decorative gable vent tiles
- Decorative horizontal trim
- Board shutters
- Corbels
- Wainscot
- Decorative plaster projections



2.7.2– Shingle Style

**Shingle Style Architectural Elements**

- Stucco and shingle siding
- 6/12 to 8/12 roof slope
- Gables and hips
- Flat roof tiles
- Openings with square or double curve tops
- Trim at top of openings
- Decorative gable vents
- Corbels
- Plaster columns with panels and plaster trim
- Louvered shutters
- Decorative plaster projections



2.7.3 – Craftsman Style

**Craftsman Style Architectural Elements**

- 4/12 roof slope
- Mostly gable roofs
- Flat concrete tile
- Projecting rakes and beam ends
- Projecting eaves and rake boards
- Decorative plaster projections
- Stucco, shingle siding, board and batten siding and brick pattern veneer
- Board shutters
- Square and arched openings
- Horizontal trim
- Decorative plaster projections
- Corbels



2.7.4 – Traditional Style

**Traditional Style Architectural Elements**

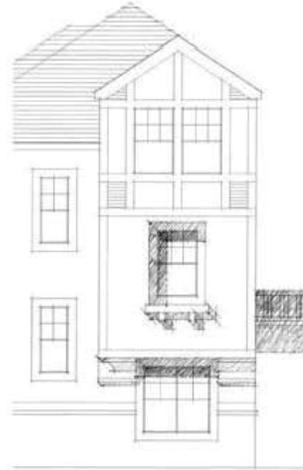
- Dutch gables with horizontal siding
- 6/12 to 8/12 roof slope
- Gables and hips
- Flat roof tile
- Porches with regularly spaced columns
- Columns with trim at top
- Open rails at porches
- Stucco and horizontal siding
- Louvered shutters
- Decorative gable vents
- Decorative plaster projections
- Corbels



2.7.5 – European Style

**European Style Architectural Elements**

- Gables with horizontal siding
- 6/12 to 8/12 roof slope
- Gables and hips
- Flat roof tile
- Columns with trim at mid-height
- Decorative rails with horizontal, vertical and/or diagonal patterns
- Stucco, horizontal siding and brick pattern veneer
- Trim in horizontal, vertical and/or diagonal patterns on walls
- Board shutters
- Corbels
- Square and round head openings



### Chapter 3 – Open Space Framework

#### 3.1 – Open Space Framework

The Northwest Land Park community is designed with a variety of open space and park areas including a central public park and a linear green space corridor spanning the community. The Northwest Land Park community includes a 4.3-acre public park located central to the neighborhood. (see Map 3.A)

The City has a service level goal to provide 5-acres of neighborhood and community parkland for every 1,000 residents. This goal is partially implemented by requiring 0.0088 acres of parkland for each multifamily unit within a new development. The target maximum residential count is 825 dwelling units; this target development intensity will require 8.64 acres of parkland to be provided by full build out. However, the final unit count at build-out may vary but will be within the allowable density ranges established by the General Plan; any park shortfall due to additional densities and changes in unit type will be mitigated with additional park land or fee. The Northwest Land Park community will achieve this requirement with a combination of park dedication and a payment in lieu as the community is built out, thus implementing the City's goal and commitment to providing quality parks and recreation to the community as a whole.

The Open Space Framework also includes semi private courtyards that are planned to provide passive outdoor gathering space for attached and multi-family residential buildings. These courtyards will be planned, located and designed as part of individual neighborhoods and will provide common areas for those specific residents.

#### 3.2 – Open Space Development Guidelines

The Northwest Land Park community shall be designed with an integrated open space network that will contribute to the overall community vision. Open space districts and amenities shall focus on the following development standards:

- Hierarchy of Open Space – The community shall be designed with a clear hierarchy of parks and open space amenities that will serve a variety of community recreational needs.
- Park – The community shall be designed around a significant, yet distinctive central park to serve as a civic node and neighborhood focus. The park is located within two blocks of the majority of the neighborhood residents.
- Neighborhood Trail Network – The community shall be designed with an integrated trail network to provide comfortable pedestrian and bicycle linkages to the central park and districts within the community.
- Private Courtyards – The community will provide semi-private open space for all cluster, duplex, and multiple-family units within the neighborhood that otherwise do not front on streets, parks and/or greenway corridors. Courtyards will be provided to ensure all residents have outdoor areas for gathering and enjoyment within close proximity to their individual dwellings.



*The neighborhood park shall be a prominent community focal point with buildings and roadways oriented towards the public space.*



Map 3.A - Open Space Framework Plan

-  Open Space District (OSD)
-  Private Courtyards (approx. location/ see standards)
-  Community Trails (approx. alignment)
-  Tunnel Option
-  Metal Burner Structure Option

<b>Table 3.2: Northwest Land Park Quimby Calculations</b>				
<b>Phase 1 = 201</b>				
Unit Classification	Dwelling Units	Factor	(DU x Factor) Park Acreage	Ratio of Phase
Single Family Unit	29	0.0149	0.4321	14%
Multifamily Dwelling Units	60	0.0088	0.5280	30%
Two Family Units / Halfplex	112	0.0112	1.2544	56%
<b>Totals:</b>	<b>201</b>		<b>2.2145</b>	
<b>Phase 2 - Total Units = 132-190</b>				
Unit Classification	Dwelling Units (using Ratio from Ph I)	Factor	(DU x Factor) Park Acreage	
Single Family Unit	0	0.0149	0.0000	0%
Multifamily Dwelling Units	35	0.0088	0.3080	23%
Two Family Units / Halfplex	120	0.0112	1.3440	77%
<b>Totals:</b>	<b>155</b>		<b>1.6520</b>	
<b>Phase 3- Total Units = 188 – 270</b>				
Unit Classification	Dwelling Units (using Ratio from Ph I)	Factor	(DU x Factor) Park Acreage	
Single Family Unit	31	0.0149	0.4619	14%
Multifamily Dwelling Units	113	0.0088	0.9944	52%
Two Family Units / Halfplex	75	0.0112	0.8400	34%
<b>Totals:</b>	<b>219</b>		<b>2.2963</b>	
<b>Phase 4- Total Units = 189 -307 (2)</b>				
Unit Classification	Dwelling Units (using Ratio from Ph I)	Factor	(DU x Factor) Park Acreage	
Single Family Unit	0	0.0149	0.0000	0%
Multifamily Dwelling Units	136	0.0088	1.1968	54%
Two Family Units / Halfplex	114	0.0112	1.2768	46%
<b>Totals:</b>	<b>250</b>		<b>2.4736</b>	
<b>Totals and Summary</b>				
Area / Phase	Total Units	Park Acreage	Land Value / Acre	Quimby Fee
Phase 1	201	2.2145	\$ 250,000.00	\$664,350.00
Phase 2-	155	1.6520	\$ 250,000.00	\$495,600.00
Phase 3	219	2.2963	\$ 250,000.00	\$688,890.00
Phase 4	250	2.4736	\$ 250,000.00	\$742,080.00
<b>Totals:</b>	<b>825</b>	<b>8.6364</b>		<b>\$2,590,920.00</b>
	<b>Half as Dedication =</b>	<b>4.3182</b>	<b>Half as Fee =</b>	<b>\$1,295,460.00</b>
<b>Note:</b> (1) Any park shortfall due to additional densities & changes in unit type will be mitigated with additional park land or fee. (2) Total unit count not to exceed 825 dwelling units.				

3.3 – Park

A 4.3-acre park is located central to the project and serves a one mile radius to provide recreational opportunities for the Northwest Land Park community and the immediately surrounding neighborhoods. The park is located adjacent to the Jedediah Smith Elementary School and Arthur Benjamin High School to create a large civic node central to the neighborhood. The park will be dedicated to the Sacramento Department of Parks and Recreation for planning, programming, development, and maintenance.

The park will accommodate a variety of civic gathering and recreational uses. Specifically, the Park will provide less intensive recreational activities including playgrounds, small play fields, sports courts, trails, and picnic areas.



*Park arranged as a central focus*



*Picnic and gathering shelters*

The Park will serve as a hub for the neighborhood's trail network. Specifically, the neighborhood trails radiate outward from the park and connect residential areas, civic uses, and neighboring uses to a comprehensive open space network.

Park Development Guidelines

- Park Orientation - The park shall be designed to be oriented to the pedestrians, bicyclists and the surrounding residential dwellings.
- Park Programming - The park's programming includes playfields, sport courts and recreation facilities shall be sized appropriately, in scale with, and complementary to the surrounding residential uses. Recreational activities shall not interfere with, or impose on the adjacent residential uses in terms of noise, light, and view obstruction.
- Recreational Hierarchy - The park shall be designed to include a hierarchy of recreational uses that range from passive outdoor space to active play areas. Use areas shall be located and planned according to intensity and compatibility with adjacent uses.
- Compatibility - Utilities and waste receptacles shall be planned and located away from adjacent residential uses and screened from rights-of-way. Lighting shall be designed not to project a nuisance onto adjacent residential uses.
- Safety - The park shall be designed to provide a safe outdoor environment by providing high visibility to all areas of the park as seen from the adjacent rights-of-way and neighboring uses. Fencing - Fencing shall be limited to non-opaque, decorative material along the park's perimeter. Chainlink and opaque fencing shall not be installed along park boundaries that will otherwise block views from surrounding residential uses.
- Landscaping - Landscape material shall be consistent with the plant list and hardscape elements listed in the Landscape Framework of these Guidelines.
- Adaptive Reuse Building – A portion of the existing wholesale produce building will be modified as an adaptive reuse building to complement the neighborhood park and provide indoor community space. The building entrance shall be oriented to an adjacent right-of-way with a designated pathway linking pedestrians to public sidewalks.
- Master Plan: The neighborhood park design and facility programming may be more defined as part of the City of Sacramento park master plan process.

### 3.4 – Setzer Run Greenway Corridor

The Setzer Run Greenway Corridor is composed of linear parklands planned to radiate outward from the central park. Setzer Run contains a continuous multi-use trail that is interconnected with the community's open space network and links recreational areas to the park and the adjacent schools to the south. The Setzer Run Greenway Corridor is private and will be dedicated to the property owners association for ownership and maintenance.

Setzer Run's east and west sections are aligned and integrated amongst the blocks to weave the open space framework into the residential areas. This design allows residential units to front directly onto open space corridors and provides for pedestrian linkages to the central park. The south section creates an open space and pedestrian linkage to the neighboring schools and existing residential uses to the south.

Setzer Run is a passive recreation amenity with a heavily landscaped corridor that complements the style and theme found in the park. Recreational uses are generally limited to a continuous multi-use trail with a few gathering spaces.

If developed, the tunnel option will allow for a pedestrian and bicycle connection under Interstate-5 between the Northwest Land Park community and Miller Park along the Sacramento River. The tunnel option design will incorporate paving, lighting, wayfinding, and security enhancements for pedestrian and bicycle use. Finally, if the tunnel option is implemented, Setzer Run may be dedicated to the public for overall community use. In this situation, the Northwest Land Park homeowners association will maintain Setzer Run since it serves as the "front door" to many of the residences and to ensure routine and quality maintenance scheduling.



*Setzer Run is a continuous greenway. Some residential units will front directly on Setzer Run.*

Setzer Run Greenway Corridor Development Standards

- Complementing Style - Setzer Run shall complement the abutting residential uses. Landscape plantings, lighting, paving, and furniture shall be complementary to similar design elements in the adjacent residential community.
- Pathway - The corridor shall include a paved multi-use trail designed to radiate outward from the park. The trail shall be a minimum of 12-ft in width in accordance with City requirements and shall be lined with shade trees and shrubs.
- Safety - Setzer Run shall be designed to provide a safe outdoor environment by providing high visibility to all areas as seen from the adjacent roadways and/or neighboring uses. Landscaping and fencing shall be planned at an appropriate scale to define the corridor edges while not eliminating views as seen from adjacent uses.
- Fencing - Fencing shall be limited to non-opaque, decorative material along the corridor's perimeter. Fencing shall be limited to four (4) feet in height. Chainlink and opaque fencing shall not be installed along corridor boundaries that will otherwise block views from adjacent residential uses.
- Boundaries - Setzer Run shall be planned and developed to provide a clear delineation of the park's edges and property boundaries.
- Compatibility - Utilities shall be planned and located away from adjacent residential uses and screened from roadways. Lighting shall be designed to not project a nuisance on to adjacent residential uses.
- Landscaping - Landscape material shall be consistent with the plant list and hardscape elements listed in the Landscape Framework of these Guidelines.
- Shade - Setzer Run shall include trees and other landscape material that achieves 70-percent shade coverage at maturity.

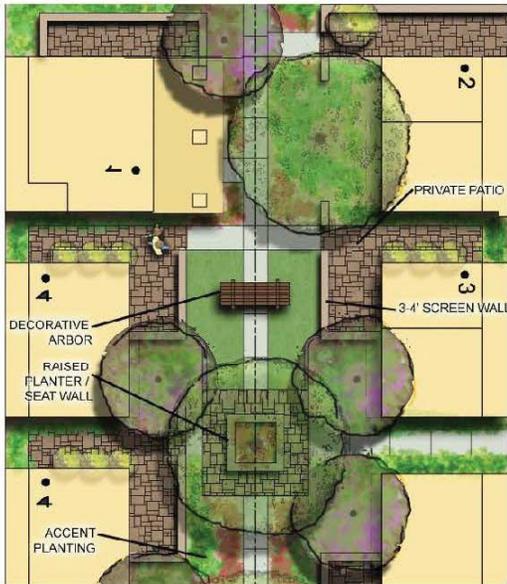
### 3.5 – Private Courtyards

Private Courtyards will be planned and integrated as part of all attached and multi-family residential developments that do not have individual private lots. Courtyards are common outdoor gathering spaces for the residents of the residential units in which they are a part. Specifically, courtyards provide outdoor spaces for residential units that have limited or no private outdoor yards. Courtyards also act as the front yard and primary pedestrian access way to individual residential units that do not front directly onto a street or a public park. Courts are planned to provide the access linkage from the public rights-of-way and private streets and define the transition between the public and private realms. In most cases, courtyards should be planned to interconnect with other courtyards via sidewalks to provide continuous pedestrian linkages amongst uses within the same block.

The Courtyards within the Northwest Land Park community shall be passive outdoor environments. Their design shall be responsive to the building in which they serve and shall include complementary building elements including hardscape, ornamentation, and furniture. Open space elements should be limited to seating areas and gathering spaces. Swimming pools, community rooms, gazebos, and small tot lots may be developed in larger courtyards. Courtyards are developed as part of the building in which they serve. They will be maintained by the associated homeowner's association.

#### Courtyard Development Standards

- Required Courtyards - Courtyards shall be provided as part of cluster housing and multi-family developments. Courtyards shall also be required where a residential dwelling does not front on a street, open space, or park. A pedestrian pathway may be provided in lieu of a courtyard where a residential building is adjacent to an existing non-residential building intended for preservation.
- Complementary Style - Courtyards shall complement the building they serve in terms of size and scale. Landscape plantings, lighting, paving, and furniture shall reflect similar elements used in the adjoining building(s).
- Accessibility and Use - Courtyards shall be accessible and useable to the residents they serve. Courtyards shall include a paved sidewalk/pathway and provide a linkage to primary building entrances and to a nearby roadway.
- Safety - Courtyards shall be designed as safe outdoor environments by providing high visibility as seen from the building they serve. Landscaping and fencing shall be planned at an appropriate scale and shall not impede views as seen from the buildings' windows.
- Fencing - Fencing shall be limited to decorative material that complements the adjacent buildings' design.
- Boundaries - Courtyards shall be planned and developed to provide a clear delineation of the space boundaries and make a clear distinction between the public and private realms.
- Comfort - All courtyards shall be designed as outdoor gathering spaces with seating and shaded landscaping.
- Landscaping - Landscape material shall be consistent with the plant list and hardscape elements listed in the Landscape Framework of these Guidelines.
- Dimensions – Required courtyards shall be established at an appropriate width and dimension to support passive outdoor recreation opportunities for the adjacent residential that they are intended to serve. At least 75 percent of the required courtyards shall be a minimum of 20 feet in width as measured between two opposite building exterior walls. The residual courtyards shall be at least 10 feet in width. Porches, patios, balconies, and architectural elements may encroach into the minimum courtyard width.



*Courtyards are common outdoor gathering spaces for a collection of attached dwellings. Courtyards shall vary in design throughout the community.*

## Chapter 4 – Transportation Framework

### 4.1 – Transportation Framework

The Transportation Framework establishes the general design and character for the streets and trail facilities within the community. The Northwest Land Park community will provide an intimate, urban neighborhood that is compact in design with a vibrant public realm. In doing so, its streets and transportation network are planned to become a distinctive urban design component of the community. Generally, the streets should be laid out as a modified grid in keeping with an urban character and continuing the pattern of the Central City. Streets should be designed to project a strong pedestrian environment and the streetscape elements should aim to create a distinctive sense of place similar to Land Park. (See Map 4.A)

### 4.2 - Transportation Development Guidelines

The Northwest Land Park community will be designed with distinctive streetscapes, roadways, and multi-use trails that will contribute to the overall community vision. Community streets and trails shall focus on the following development standards:

- Distinctive Urban Neighborhood – The community will create a unique and distinct neighborhood in terms of design and amenities. The neighborhood streets shall project a strong sense of place while complementing its surroundings.
- Choice of Mobility – The community shall allow for multiple modes of transportation including private automobiles, bicycles, and pedestrian mobility.
- Street Connectivity – The community streets shall be designed on a modified grid with multiple connections to the surrounding roadway network.
- Pedestrian and Bicycle Connectivity – The community shall provide sidewalks on both sides along all streets, and a defined multi-use trail network. The community shall develop private pathways that provide pedestrian linkages within individual blocks and between community uses.
- Traffic Calming - Internal community streets and appropriate roadways in the immediately abutting neighborhoods shall be designed, constructed, or improved to achieve an environment that is safe and inviting to pedestrians. If feasible, a traffic circle shall be constructed at the intersection of McClatchy and 5th Street. Traffic calming measures on adjacent streets shall be focused on 5th Street and Vallejo Street. Traffic calming measures shall be explored at the sole discretion of the developer and shall be consistent with any approved in the Development Agreement (see Appendix A, Condition 3).
- Attractive Environment – The community streets shall provide enhanced streetscape elements such as landscaping, street lights, and street furniture that create an attractive street environment. Streets shall coordinate tree and plant palettes to provide a thematic and distinctive character. Landscaping along internal local streets shall be created within buffer yards on private property to provide generous plantings and to ensure long term maintenance.
- Safe Environment – Streets shall be designed to be safe in terms of traffic mobility, diversity in users, and crime prevention.



**Map 4.A - Transportation Framework Plan**

- Minor Arterial 80' R.O.W.
- Minor Collector 71' to 80' R.O.W.
- Local Street 41' R.O.W. / Facility Width
- Festival Street 68' R.O.W. / Facility Width
- ⋯ Community Trails (approx. alignment)
- ⊙ Tunnel Option

#### 4.3 – Facility Descriptions

- Minor Arterials - Broadway is an arterial roadway that abuts the community along its northern boundaries. The portions of Broadway that abut the community shall be designed to create a comfortable urban pedestrian environment.
- Minor Collectors - 3rd and 5th Streets are existing collector roadways within and adjacent to the Northwest Land Park community. These minor collectors serve as gateways into the neighborhood and provide connections to surrounding neighborhoods. These streetscapes shall be enhanced for the portions that abut or are within the Northwest Land Park community. Specifically, 3rd Street shall be designed as an urban cross section with general hardscape and pedestrian amenities for portions that abut mixed-use development. 5th Street shall be designed as a residential cross section with sidewalks and landscape planters.
- Local Streets - The internal local streets within the Northwest Land Park community shall be designed as *local residential roadways without planters* to create a distinctively urban residential community. Streets should be designed with rolled curb and gutter with integral sidewalks. Landscaping shall be provided within landscape planters behind the sidewalks to allow for generous planting of canopy shade trees and to ensure the streetscape is maintained by private property owners associations.
- Festival Street – A Festival Street is an adaptation of local streets. This roadway section is designed so it can be closed to vehicular traffic and used as an open outdoor pedestrian environment and event area. A Festival Street provides standard curbs and perpendicular parking on both sides of the street. Pedestrian paths of travel can be integrated into the outdoor spaces of the existing buildings, alternatively, traditional curb tight sidewalks may be provided where space allows. Individual travel areas are designated with treatment elements such as stamped concrete, coloration, and texture. Streetscape plantings for a Festival Street may be located within the public right-of-way and/or within landscape setbacks outside the paved area and on adjacent property lining the space. Given the intended character and dimensional arrangements, a Festival Street shall be exempt from specific shading standards.
- Community Trails/Setzer Run - The Northwest Land Park community shall include an interconnected multi-use trail network that links the central park to community uses, neighboring uses, and the greenway corridors. Trails shall be designed with 12-feet of pavement, allowing two-way pedestrian and bicycle travel. Landscaping with shade trees and shrubs will be provided along pavement edges to contribute to a comfortable and aesthetically-pleasing environment for trail users.

4.4 – Facility Types

The Transportation Framework plan is composed of a specific facility hierarchy arranged by intended user and intensity. Specific street cross sections are established for the Northwest Land Park community which, collectively contribute to the community vision and intended character. The community streets are designed per City of Sacramento roadway infill standards in order to create a desired community character and achieve a compact urban environment.

The following table lists the development standards for streets and trails in and abutting the Northwest Land Park community. The subsequent subsection illustrates the typical facility designs for the community.

**Table 4.4: Transportation Facility Development Standards**

Facility Type	Right-of-way Width	Travel Lanes	Pavement Width	Sidewalk Width <sup>(2)</sup>	Landscape Setback Width <sup>(2)(4)</sup>	Street Parking	Bicycle Lanes
Minor Arterial	80'	4	44' - 52'	8'-12'	0'	No	Yes
Minor Collector	71' to 80'	2	40' - 48'	5'	6 - 10' <sup>(3)</sup>	Yes	Yes
Local Street	41'	2	30'	5'	6' - 10' <sup>(1)</sup>	Yes	No
Community Trail	20'	N/A	12'	N/A	0'-12'	N/A	N/A
Alleys/ Accessways	<i>Not Applicable</i>	2	20'	None	None	No	No
Festival Street (private)	68'	2	26'	10' min. (one side)	-	Yes	No

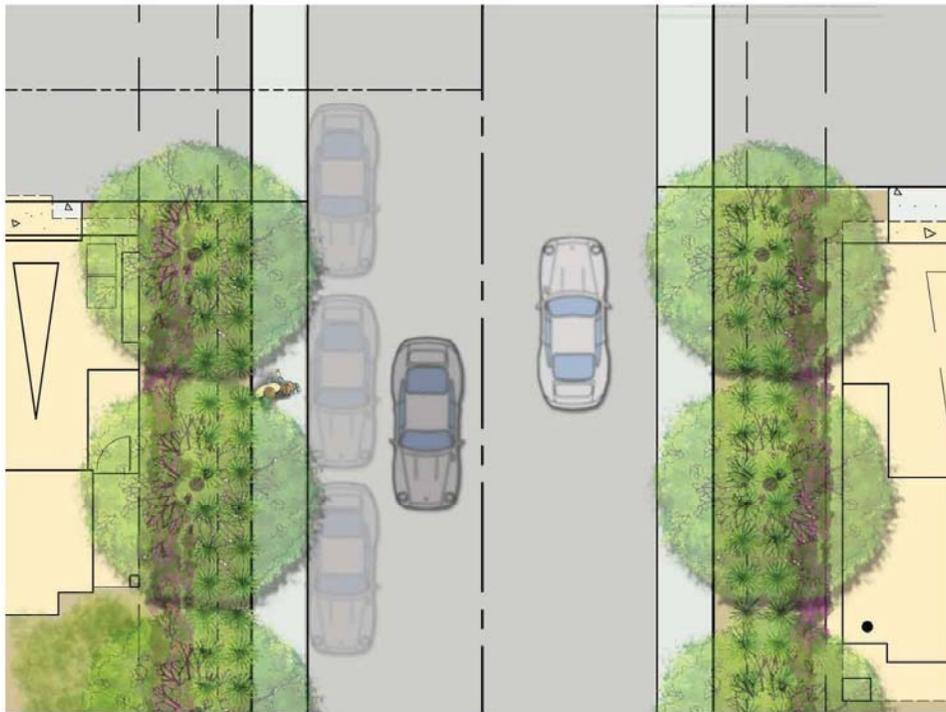
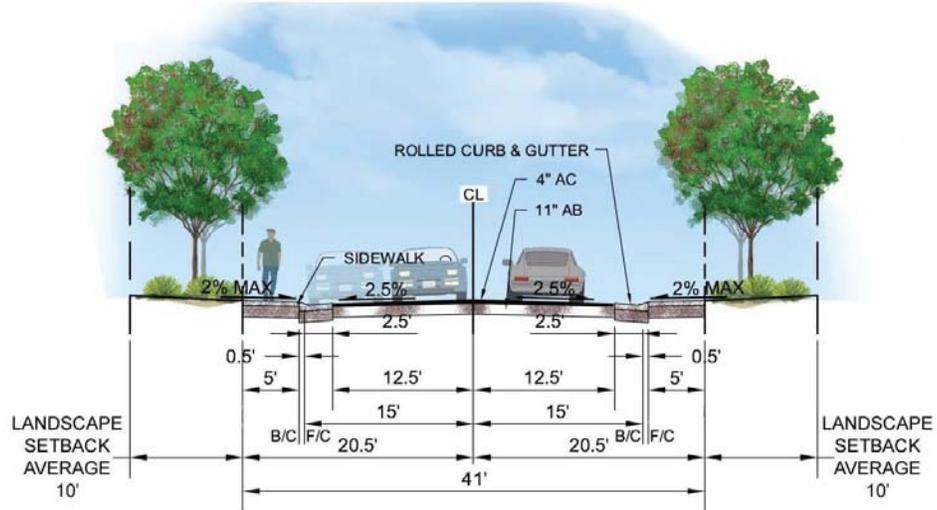
Notes:

- (1) Landscaping and plantings for Local Streets are provided within a landscape setback on private property behind the sidewalks. Mailboxes, lighting, private utilities and other street furnishings may be permitted within the landscape setback when approved by the developer. Refer to 5.3.2 for allowable encroachments.
- (2) The sidewalk and landscape setback may be combined to create a continuous hardscape area for sections that abut mixed-use.
- (3) The plant list for street trees, shrubs and vegetative material is stated and described in the Landscape Framework section of these Guidelines.
- (4) Community Trails shall have a landscape planter along at least one side of the sidewalk/pathway.
- (5) On-street parking shall be permitted on all internal local streets.
- (6) Landscaping and plantings for a Festival Street are provided within a landscape setback on private property behind the sidewalks or within landscape islands between parking spaces.
- (7) Festival Streets are exempt from any shade cover standards.

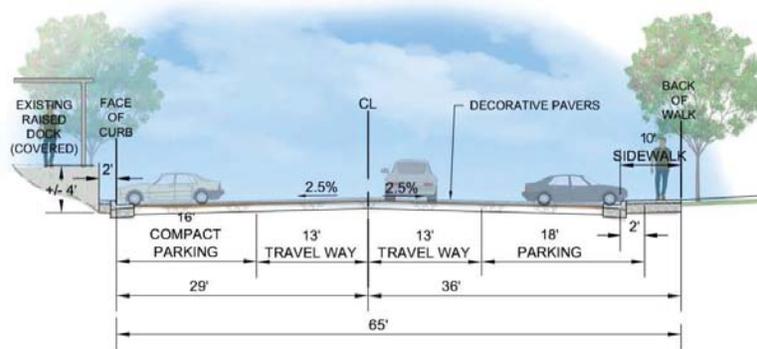
4.5 – Facility Design Illustrations

The following figures serve as the typical cross section and plan for each transportation facility. These illustrations shall serve as the general facility design for specific streets and trails within the Northwest Land Park community and may reflect minor adjustments to standard City of Sacramento typical sections based on existing right-of-way conditions.

Local Street - Typical Cross Section and Plan with On-street Parking

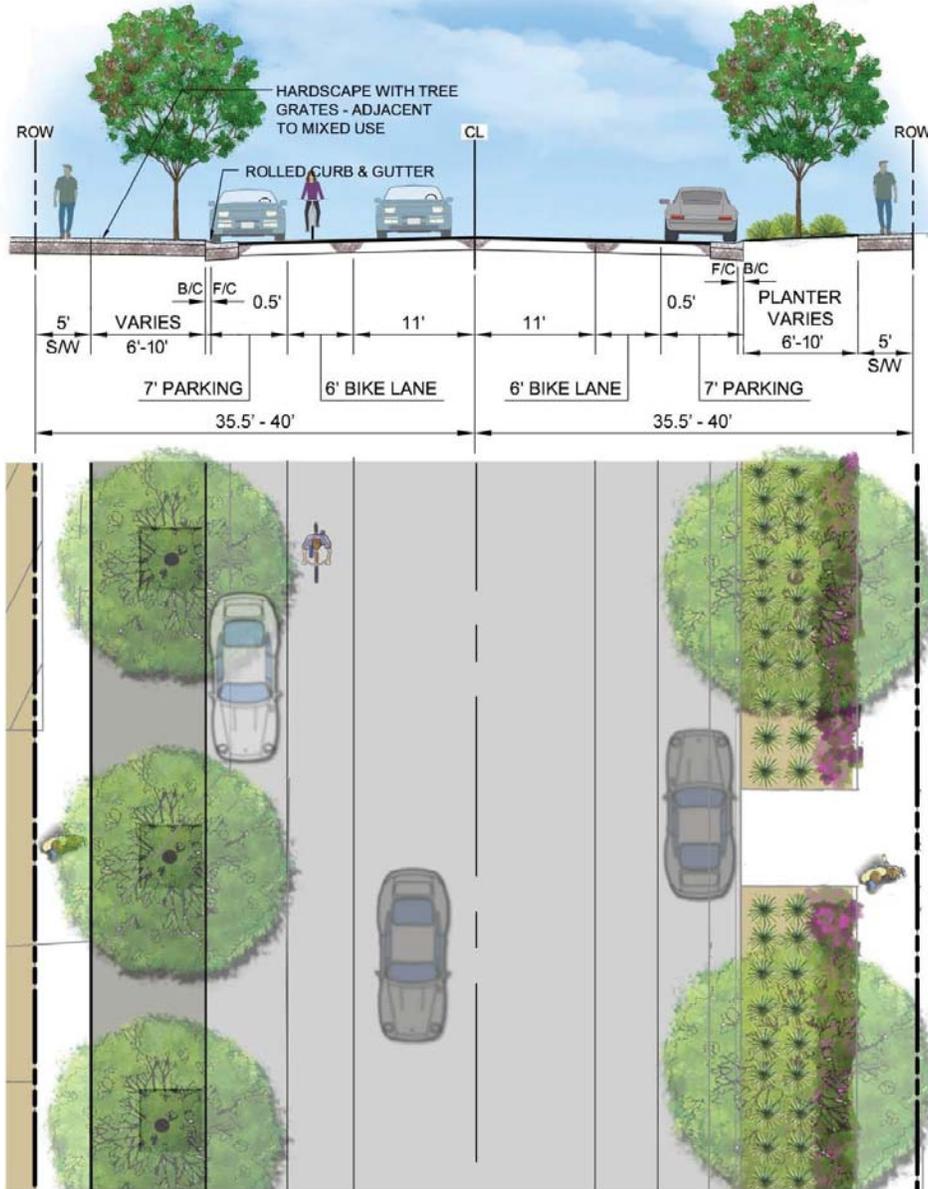


Festival Street - Typical Cross Section and Plan

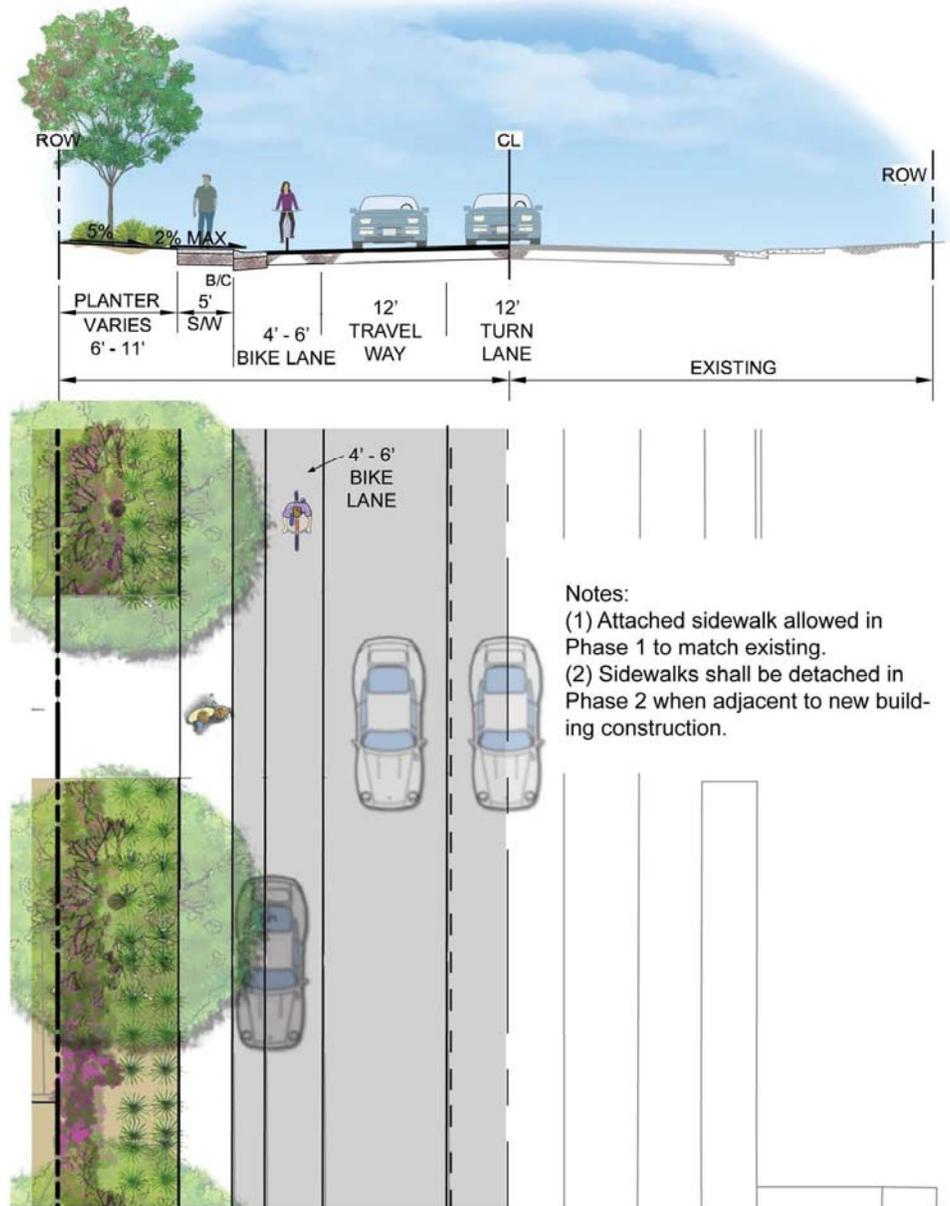


Note (1) Layout shown reflects street condition when open to vehicular traffic. Periodic closure for special events would eliminate vehicular traffic but could allow for some parked vehicles to remain throughout the street.

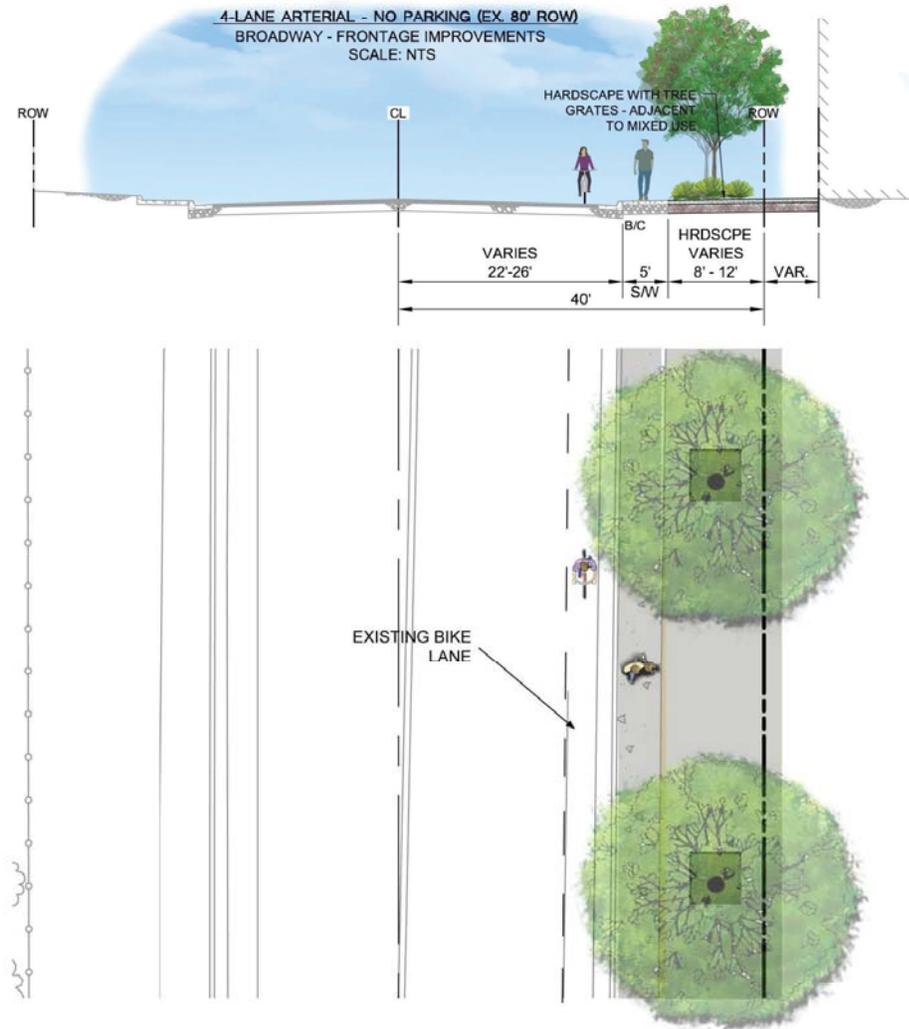
Minor Collector (3<sup>rd</sup> Street) - Typical Cross Section and Plan



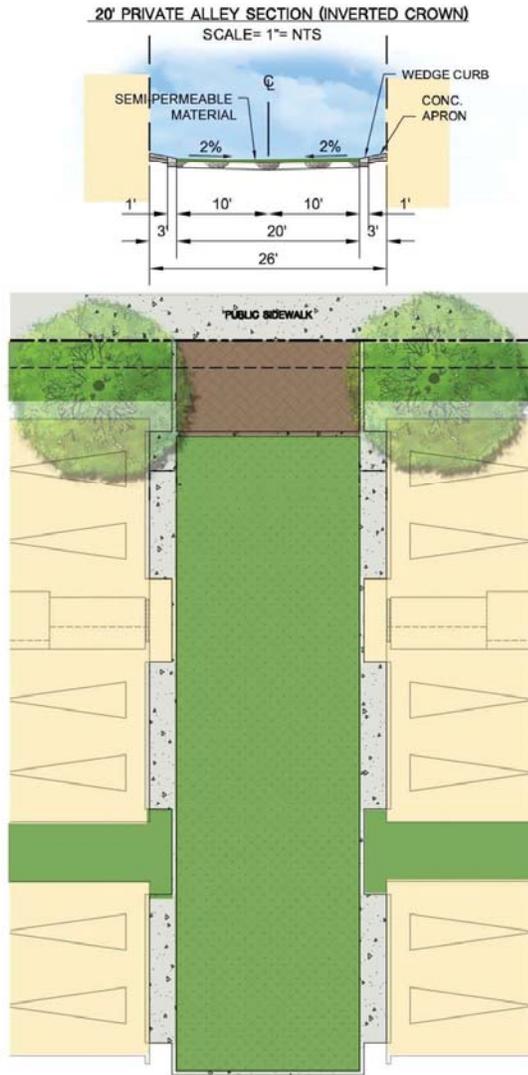
Minor Collector (5th Street) - Typical Cross Section and Plan



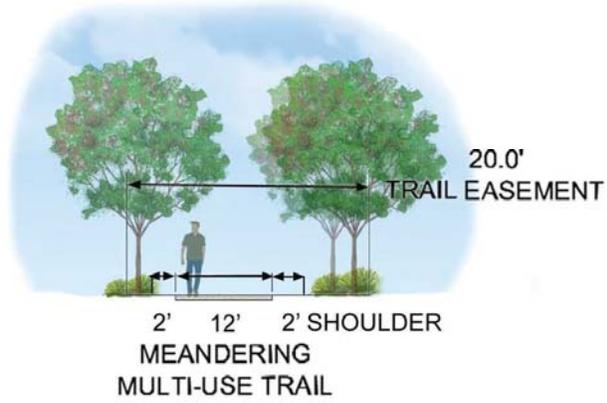
Arterial (Broadway) - Typical Cross Section and Plan



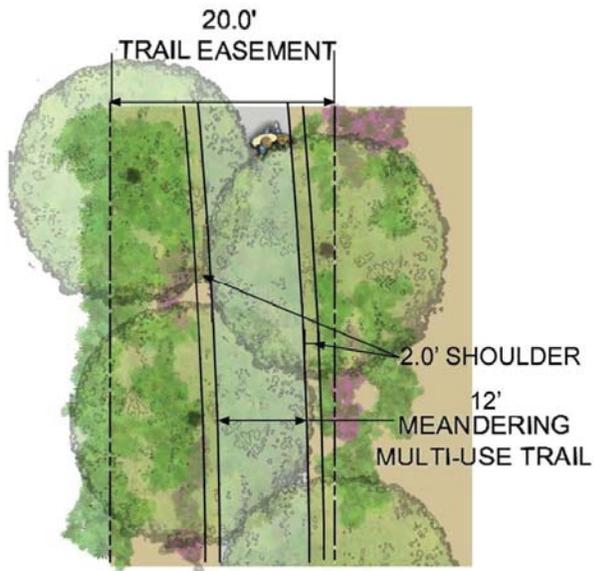
Accessway/Private Alley - Typical Cross Section and Plan



Community Trail - Typical Cross Section and Plan



6 TYPICAL MULTI-USE TRAIL SECTION  
SCALE: NTS



## Chapter 5 – Landscape Framework

### 5.1 – Landscape Framework

The Landscape Framework establishes the general landscape character and design principles for the Northwest Land Park community. This framework provides the general vision and material palette for enhancing the public areas and semi-public realms within the community. The landscape framework shall achieve a cohesive and harmonious sense of place across the community while also allowing individual neighborhood subareas to be distinctive.

### 5.2 - Landscape Development Guidelines

The Northwest Land Park community shall use landscape design to create a strong sense of place and a unique neighborhood within the City. Landscape design shall include plant and hardscape material appropriate for the setting and shall lessen long-term maintenance for the community. Landscape design shall focus on the following development standards:

- Landscape & Placemaking – Landscaping and hardscape design should be used to reinforce the community character. Designs should invoke a strong sense of place that is unique to the Northwest Land Park community.
- Climate Appropriate Plants – Trees, shrubs, and grasses shall be conducive to the Northern California environment in terms of water use, drought tolerance, maintenance, and durability. Materials such as synthetic Turf may be used for active play areas and small gathering lawns. (examples maybe found at [www.fieldturf.com](http://www.fieldturf.com))
- Continual Aesthetics – Landscape material shall be selected to provide year-round interest in terms of changing foliage, flowers, and scent.
- Low Maintenance & Cost Effectiveness – Landscape material including trees, plants, turf, and hardscape should require minimal maintenance as compared to other varieties and material choices. Synthetic turf shall be used to the extent possible in lieu of natural turf and grasses. Materials should be cost effective to lessen the initial expenditure, periodic replacement, and long-term maintenance. Turf may be synthetic to lessen irrigation demands and long term maintenance.

### 5.3 – Streetscape Standards

#### 5.3.1. - *Standard Streetscape Landscaping standards*

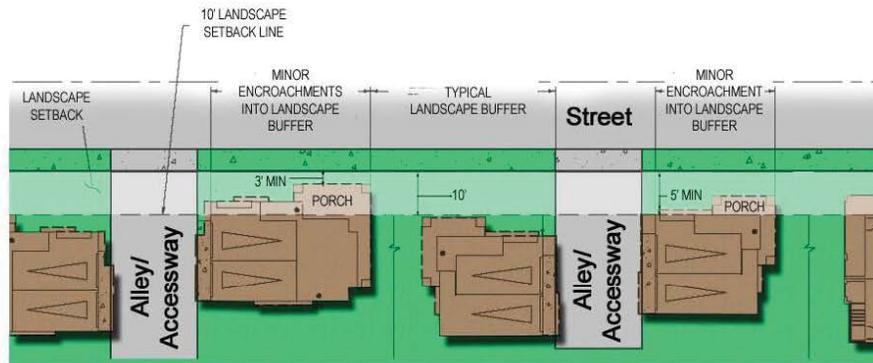
The plantings along streets and the community trails shall consist mainly of large canopy shade trees and colorful understory plantings. Accent trees and plants shall be clustered in key locations to add variety and interest. Turf or synthetic turf shall be utilized in larger areas to create a usable and inviting open space for residents as well as define view corridors into the project for safety. The plant species used in landscape setbacks shall be carefully chosen to give the project year-round color and interest. For a detailed illustration, refer to the cross sections and street plans in the Transportation Framework section of these Guidelines.

5.3.2. - Local Streetscape Landscaping standards

Streetscape plantings for internal local streets shall be located within landscape setback areas on private property situated behind sidewalks. Landscaping along internal local streets shall be more lush and generous in plant coverage including primarily canopy shade trees to create a dynamic streetscape. By creating intimate setbacks while still accommodating significant canopy trees, the plan will achieve the true character of compact urban development. Mailboxes, lighting, and street furniture may be located within landscape setbacks. These landscape setback areas will be maintained by the property owners association.

Landscape setback areas shall average 10-feet in width extending from the right-of-way edge into the adjacent property. The following lists specific design situations that allow for setback exceptions:

- Sidewalks and driveways that provide access between the street and a building entrance may be located within the landscape setback area.
- Ground floor covered porches, balconies, stairs, and non-habitable architectural elements may encroach into the landscape setback areas by up to five (5) feet.
- Structural areas, including habitable building areas and garages, may encroach into the landscape setback areas up to seven (7) feet.
- The maximum aggregate length of street frontage with encroachments shall not exceed 40% of the total length of street frontage on any given street.
- Adequate planting space shall be provided along all street frontages to provide a minimum of three (3) trees in every 120 linear feet of frontage, or roughly 40' spacing on center. Thirty percent (30%) of the minimum required trees shall be classified as large or medium canopy species.



**Landscape Setback Detail**

5.3.3 – Festival Street Streetscape Landscaping

The Festival Street shall be designed as both a transportation facility with a dual function as an open plaza area to support community gatherings and routine events. In doing so, the space shall be designed as an open area with decorative hardscape to support its intended use. Streetscape plantings for the Festival Street may be located within landscape setbacks outside the paved area and on adjacent property lining the space. Given the intended character and dimensional arrangements, the Festival Street shall be exempt from specific shading standards.

5.3.4 – Hardscape Design standards

Hardscape areas within the Northwest Land Park community shall include a variety of colors, styles, textures, and materials to create a strong sense of place. Hardscape design shall include a variety of patterns and materials particularly in public gathering spaces, community entries, designated pedestrian crossings, and within private courtyards. Diverse paving treatments shall be encouraged for these areas over monotonous concrete and asphalt paving. Hardscape treatments within the Northwest Land Park community shall be focused primarily on attractive appearance and long-term durability in lieu of a specific material type. To the extent feasible, project will use synthetic turf to create a softscape look and feel with hardscape functionality.

5.4 – Lighting and Street Furniture styles

Decorative street lighting and furniture will reinforce the character for Northwest Land Park. Specifically, these elements will create interesting public realms along streets, within parks and public areas. Street lighting and furniture located in the public right-of-way and within the community's central park shall be consistent with the City of Sacramento approved materials list. Specific styles and design will be proposed at the time construction plan approval.



*Community mailbox design; clustered and housed in a distinctive architectural enclosure*

*Street Lamp style; decorative light pole and acorn-shaped fixture*

5.5 – Plant List

The plant list for Northwest Land Park is a mix of native and regionally appropriate species that have been selected for proven durability, historical context, efficient water use, and low maintenance. Plant types shall be clustered in zones to maximize water efficiency with low water use species in the larger open areas and higher water use species clustered in key locations and semi-private gardens. In some locations synthetic turf may be used in place of lawn to further reduce water demands and minimize maintenance. The plant list consists of a large variety of plant species to give the project year-round interest, form, texture and color.

The following plant list includes the desired species for Northwest Land Park that meet the development styles and community vision. Alternative plant choices may be approved by the City.

**Table 5.5.1 – Street and Park Trees List**

Botanical Name	Common Name
<b>Deciduous Trees</b>	
Acer rubrum	Red Maple
Carpinus Betulus	European Hornbeam
Fraxinus uhdei	Shamel Ash
Ginkgo Biloba	Autumn Gold Maidenhair Tree
Pistacia chinensis	Chinese Pistache
Platanus acerifolia	Plane Tree
Platanus racemosa	California Sycamore
Quercus lobata	Valley Oak
Quercus phellos	Willow Oak
Quercus Shumardii	Shmard Red Oak
Ulmus parvifolia	Allee Chinese Elm
Zelkova serrata	Saw-leaf Zelkova
<b>Evergreen Trees</b>	
Cedrus deodara	Deodar cedar
Podocarpus gracillior	Fern Pine
Quercus agrifolia	Coast Live Oak



Acer rubrum      Pistacia chinensis      Platanus acerifolia



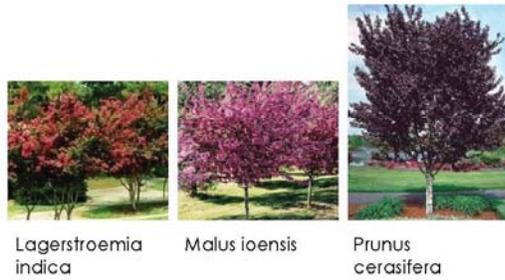
Quercus lobata      Quercus phellos      Zelkova serrata



Cedrus deodara      Evergreen Ash      Podocarpus gracillior

**Table 5.5.2 – Accent and Courtyard Trees**

<i>Botanical Name</i>	<i>Common Name</i>
<b>Deciduous Trees</b>	
<i>Acer palmatum</i>	Japanese Maple
<i>Lagerstroemia indica</i>	Crepe Myrtle
<i>Malus ioensis</i>	Bechtel Crabapple
<i>Osmanthus fragrans</i>	Sweet Olive
<i>Prunus cerasifera</i>	Purple-Leaf Plum
<i>Pyrus kawakamii</i>	Western Redbud
<b>Evergreen Trees</b>	
<i>Cupressus sempervirens</i>	Italian Cypress
<i>Olea europaea</i>	Olive
<i>Xylosma congestum</i>	Shiny Xylosma



**Table 5.5.3 – Vines**

<i>Botanical Name</i>	<i>Common Name</i>
<i>Ficus pumila</i>	Creeping Fig
<i>Hardenbergia violacea</i>	Purple Lilac Vine
<i>Wisteria floribunda</i>	Japanese Wisteria



**Table 5.5.4 – Shrubs**

<i>Botanical Name</i>	<i>Common Name</i>
<i>Agapanthus africanus</i>	Lily of the Nile
<i>Amelanchier alnifolia</i>	Western serviceberry
<i>Arbutus unedo</i>	Strawberry Tree
<i>Azalea</i> sp.	Azalea
<i>Buxus japonica</i>	Japanese Boxwood
<i>Carpenteria californica</i>	Carpenteria
<i>Ceanothus</i> sp.	Wild Lilac
<i>Cistus</i> sp.	Rock Rose
<i>Cornus sericea</i>	Redtwig Dogwood
<i>Cotoneaster</i> sp.	Cotoneaster
<i>Dietes vegeta</i>	Fortnight Lily
<i>Escallonia rubra</i>	Red Escallonia
<i>Euonymus japonica</i>	Euonymus
<i>Heteromeles arbutifolia</i>	Toyon
<i>Lavendula</i> sp.	Lavender
<i>Ligustrum japonica</i>	Privet
<i>Muhlenbergia rigens</i>	Deer Grass
<i>Nandina domestica</i>	Heavenly Bamboo
<i>Phormium tenax</i>	Flax
<i>Photinia fraseri</i>	Photinia
<i>Pittosporum</i>	Japanese Pittosporum
<i>Rhamnus californica</i>	Coffeeberry
<i>Rhapiolepis indica</i>	Indian Hawthorn
<i>Rosa gymnocarpa</i>	Bald Hip Rose
<i>Rose woodsii</i>	Wood's Rose
<i>Rosmarinus officinalis</i>	Rosemary
<i>Spiraea douglasii</i>	Western spiraea
<i>Westringia fruticosa</i>	Coast Rosemary
<i>Xylosma congestum</i>	Compact Xylosma



*Agapanthus africanus*



*Arbutus unedo*



*Buxus japonica*



*Carpenteria californica*



*Ceanothus* sp.



*Cistus* sp.



*Dietes vegeta*



*Escallonia rubra*



*Lavendula* sp.



*Muhlenbergia rigens*



*Phormium tenax*



*Rosmarinus officinalis*



*Rhapiolepis indica*



*Rose woodsii*



*Spiraea douglasii*

## Chapter 6 – Sustainability & Energy Efficiency Framework

### 6.1 – Sustainability & Energy Efficiency Framework

Northwest Land Park redevelops centrally located in-fill land to provide an alternative to suburban sprawl, bringing residential growth back to the community job and cultural core. Northwest Land Park will build on this foundation through innovative and resource conscious design to create one of the most sustainable and energy efficient communities in the region. This will include sustainable land planning and site development designs and uses; adaptive re-use of existing buildings and recycling of on-site materials; and innovative design and use of materials within each building. These sustainability and energy efficiency goals are targeted towards new buildings and construction. Any existing buildings or structures that are preserved or reused within the project are not subject to these goals in order to sustain their character and traditional style.

Proposed commercial and retail uses will include lighting conservation elements and other energy conservation measures. Lighting conservation elements would include occupancy sensors to automatically turn off lights when not in use, lighting reflectors, electrical ballasts, and energy-efficient lamps. Conservation elements are expected to include improved HVAC systems with microprocessor-controlled energy-management systems.

### 6.2 – Sustainability & Energy Site Development Guidelines

Development guidelines will increase the sustainability and energy efficiency opportunities of the project in the areas of storm water retention, water efficiency, energy production and conservation. Sustainability and energy will be achieved by focusing site design on the following development standards:

- Stormwater Management – The project will redevelop with smaller residential buildings interlaced within green courtyards, large central park and meandering greenbelt, and utilizing decorative semi-permeable materials for private driveways and courts. The pervious to impervious ratio for Phase 1 (40% permeable to 60% Impermeable) will be used as a minimum guideline for the build-out of the entire site through Phase 4.
- Water Efficiency – All project landscaping shall be climate appropriate for the area and irrigated with moisture sensor driven systems to provide drought tolerance and maximum efficiency of water use in irrigation. Synthetic turf shall be used, to the greatest extent possible, for private grassed areas within the development.
- Vegetation & Forestation –Vegetation and tree planting plans shall be designed to provide shading for streets, hardscape surfaces, buildings, and recreation areas during summer months. In contrast, said plans shall include landscape varieties that lose their leaves during winter months to promote passive sunlight within the community, thus reducing energy use relating to heating and lighting.
- Air Quality – The project proposes that all buildings, units, and facilities, indoors and out, are free of devices designated to facilitate the combustion of wood or wood products to eliminate emissions generally associated with traditional fireplaces.
- On-site Alternative Energy Production - The project will provide a minimum of 100 KW of alternative energy (i.e. solar, wind, fuel cell) production in Phase 1, with aggregate commitments of 200 KW through Phase 2, 300 KW through Phase 3 and 400 KW for all 4 phases at build-out. The on-site production (using industry standard yield calculations) equates to 182,500 kWhs annually of alternative energy production in Phase 1, with aggregate annual production of 365,000 kWhs through Phase 2, 547,500 kWhs through Phase 3 and 730,000 kWhs for all 4 phases at build-out.

### 6.3 – Re-use and Recycling On-site Materials Guidelines

A re-use and recycling initiative is introduced to retain and celebrate the long standing character of the site as a “place making” context for the Northwest Land Park community. This goal will: (a) complement and enhance the sustainability of the project by seeking active reuse and recycle opportunities on-site; (b) provide contextual materials for theming the site for use in markers, signage, auxiliary structures, and public art; and, (c) adaptively re-use select structures in their entirety for community gathering spaces including a public community building within the park setting and a festival market to provide a focal center for the neighborhood and the surrounding community.

The project shall re-use at least 50 percent of salvageable materials in the existing improvements on-site, as measured by weight. This can take the form of re-use of entire structures, re-use or repurposing of significant elements, such as beams or trusses, and recycling materials within the new project such as grinding paving and asphalt for use as base material at the site. These activities will increase the sustainability of the site through reduced waste materials from demolition, reduced need for new materials on-site, and reduction of the ancillary transportation impacts from off-haul and delivery of materials to the site. Additionally, the project will evaluate brick, wood, metal, and masonry materials from the demolition to be re-manufactured into a “heritage” line of finishes to be offered as upgrades to the units. As an example, wood timbers would be converted into flooring material to provide the character and cache of “distressed” lumber underfoot. These efforts will increase the amount of on-site materials reused sustainably within the project.

To the greatest extent feasible, the project shall reduce waste and recycle non-reusable materials during demolition, site development, and unit construction using the following hierarchy of procedures:

1. Recondition and re-use on site. For example; crushing old concrete, asphalt and paving to be reused as base rock within the project grading activities.
2. Seek dealers and other re-use distributors to harvest intact components and materials for re-sale. As an example; large pole barn supports sold intact to telephone pole providers.
3. Separate bulk recyclable items, such as wood and metal pieces, for reprocessing at standard recycling facilities.

Following this process the project will meet stated development standard of re-using at least 50% of the existing materials by volume on site, and will seek to approach zero non-recycle waste leaving the project site throughout all development activities.

#### 6.3.1 – Adaptive Reuse of Structures

The project will pursue adaptive reuse on three existing structures within the site; the Metal Burner Structure, Wholesale Produce Building, and the Farmers Market and Market Club building. In order to preserve their original architecture, style, and iconic character, these buildings may not achieve the same energy efficiency goals as new buildings within the project. To the extent economically and structurally feasible *and* approved by the City, the aforementioned unique structures will be considered for adaptive reuse in whole or part.

### 6.3.2 - Reuse for Sustainability

The existing site contains various industrial and commercial structures and improvements which will be removed as part of the build-out of Northwest Land Park. The project will carry out normal and customary material recycling programs, such as grinding and recycling pavement sections removed during grading and demolition. In addition, the development will seek opportunities to re-use brick, structural and industrial pieces for markers, signage, auxiliary structures, and public art. Examples of this may include building picnic areas and public seating (trail side benches) covers re-utilizing poles and I-beams for supports, trusses for roof structures, and refurbished corrugated sheets for roofing. Additionally, appropriate distressed materials could be re-manufactured for use as finish materials in the homes (beams and boards converted to "heritage" plank flooring for example). Monument signage could be designed to incorporate iconic industrial pieces, such as the saw dust system cyclones, to theme the project at key corners and focal points. These and other landscape areas would also be ideal locations for re-use of brick and large wooden beams to create low wall structures for retaining and plant bedding purposes. The direct re-use of these materials on site will improve the sustainability of the project by avoiding transportation and processing of the materials for standard recycling off-site and by reducing the amount of new materials required to build out the project.

### 6.4 – Dwelling Unit Efficiency Design Guidelines

Northwest Land Park will be one of the most energy efficient projects in the Central Valley. The units will be designed using a "whole system" model where each unit is viewed as a complete system and all of the components are formatted and integrated to support the sustainability and efficiency of the entire home. The buildings will have a vastly superior thermal shell than required by code, will utilize efficient ductless heating and cooling systems, will incorporate low voltage hardwired LED lights, Energy Star rated appliances, high efficiency natural gas tank-less water heaters, photovoltaic panels, or rooftop collector systems offered, and, most importantly, modest living areas to be lit, heated, and cooled. Individual dwelling units shall include efficiency design consistent with the following development standards:

- Efficient Floor Plans - The Northwest Land Park community will be developed with compact efficient floor plans. In addition the majority of units will share wall/floor space, and thus thermal mass, with at least one other unit.
- Insulation – Building shall be designed with a high-efficiency thermal shell for the units with exterior walls at or above R25 for walls and R40 for ceilings.
- Climatization – Residential buildings shall use small high efficiency heating and cooling units.
- Lighting - Buildings shall use LED or fluorescent lighting system throughout the units, allowing for energy efficient lighting.
- Exterior Lighting - Exterior HOA maintained lighting, including pathway lights, accent/landscaping lights, motor-court lights, and private street lights shall use LED lighting technologies.
- Water Heaters - The project shall provide high efficiency tank-less hot water heaters to provide for the most energy efficient delivery of hot water. Nothing in this provision shall preclude installation of high efficiency alternative energy source hot water heating and storage units.
- Electrical vehicle accommodations - The project shall incorporate 110v electrical outlets in the garage units such that they are readily accessible for use with electric vehicles.
- Renewable Energy Commitment - The project shall incorporate a 400 KW renewable energy system to reduce the amount of energy purchased by the Project. The renewable energy will be incorporated over the life of the project such that a minimum of 100 KW will be incorporated into phase 1 with an aggregate total of 100 KWs per

phase through the buildout of phase 4. The 400 KW system will result in an annual reduction of 730,000 kWh of purchased electricity at full project buildout. This is equivalent to the emissions from electrical consumption of approximately 188 dwelling units. The renewable energy system may include solar, wind, fuel cells, or other new technology that becomes available over the implementation of the project. The following are the commitments already made by the project to foster this renewable commitment:

- ▶ Photovoltaic Design - The project shall be planned to orient at least 40% of the roof area of a minimum of 50% of the buildings to the west, south or southwest so that photovoltaic panels and collector systems can provide maximum benefit when installed. The project shall work with the local utility and, through an aggressive sales program, encourage and provide solar systems and/or alternative energy systems as an option.
- ▶ The orientation of at least 40% of the roof area of at least 50% of the buildings shall be west, southwest, or south.
- ▶ Solar Energy – As indicated in the AQMP (measure M28), the NWLP Project has committed to the implementation of a solar energy system that will offset a minimum of 2.5% of the residential needs of the project.

**Appendix A – City Council Resolution - Conditions of Approval 1-3**

1. Upon issuance of a building permit for residential units along the northern boundary of Phase 2, the project applicant shall construct a ten-foot high uniform wall along the boundary of the project site where Phase 2 abuts the existing uses (currently owned by Saccani Distributing Company and the Gary and Roland Saccani Trusts) on the south side of First Avenue, and shall plant fast-growing trees, such as Italian cypress trees, adjacent to the ten-foot high wall on the Project side.
2. Upon issuance of a building permit for residential uses on the eastern boundary of Phase 4, the project applicant shall construct a ten-foot high uniform wall along the eastern boundary of Phase 4 where it abuts the existing use on the north side of First Avenue, and shall plant fast-growing trees, such as Italian cypress trees, adjacent to the ten-foot high wall on the Project side. In the event the project applicant does not seek building permits to construct residential units along the eastern boundary of Phase 4, this condition shall have no effect.
3. The applicant shall explore the feasibility of constructing a traffic circle at the intersection of McClatchy and 5th Streets as part of their public improvements. The traffic circle shall be constructed per City standards and to the satisfaction of the Department of Transportation. If the traffic circle construction is found to be infeasible, the applicant shall contribute the sum of \$50,000 to go towards a Neighborhood Traffic Management Program (NTMP) within the adjacent neighborhood, including 5th Street and Vallejo Street. If the construction of the traffic circle is found to be feasible, the applicant has agreed to construct the traffic circle. Should the total design and construction cost of the traffic circle be less than \$50,000, the remaining balance shall be paid towards the above referenced NTMP. This one time contribution, from the applicant, shall satisfy all requirements towards any neighborhood traffic calming program related to the development of the Northwest Land Park PUD.



Map 1.B - Land Use Schematic Plan

Land Use/Community Elements	Permitted Development Intensity
Residential District (RD)	FAR 1.5 - 4.0 / Density 33 - 110 du/a
Mixed Use District (MXD)	FAR 0.3 - 3.0 / Density 20 - 110 du/a
Open Space District (OSD)	FAR 1.5 - 4.0
Community Trails (approx. alignment)	
Adaptive Reuse Building (approx. location/ see standards)	
Open Air Market (approx. location/ see standards)	
Residential Buffer	
Metal Burner Structure Option	
Tunnel Option	

Note: (1) - Total unit count not to exceed 825 dwelling units.  
 (2) - In the Mixed Use District, residential units are only allowed pursuant to the underlying zoning designation.

# Exhibit B: Northwest Land Park PUD Schematic Plan

Northwest Land Park PUD Guidelines



**Map 1.B - Land Use Schematic Plan**

Land Use/Community Elements	Permitted Development Intensity
 Residential District (RD)	FAR 1.5 - 4.0 / Density 33 - 110 du/a
 Mixed Use District (MXD)	FAR 0.3 - 3.0 / Density 20 - 110 du/a
 Open Space District (OSD)	FAR 1.5 - 4.0
 Community Trails	(approx. alignment)
 Adaptive Reuse Building	(approx. location/ see standards)
 Open Air Market	(approx. location/ see standards)
 Metal Burner Structure Option	
 Tunnel Option	

Note: (1) - Total unit count not to exceed 825 dwelling units.  
 (2) - In the Mixed Use District, residential units are only allowed pursuant to the underlying zoning designation.

Exhibit C: Railroad Remnant Parcels

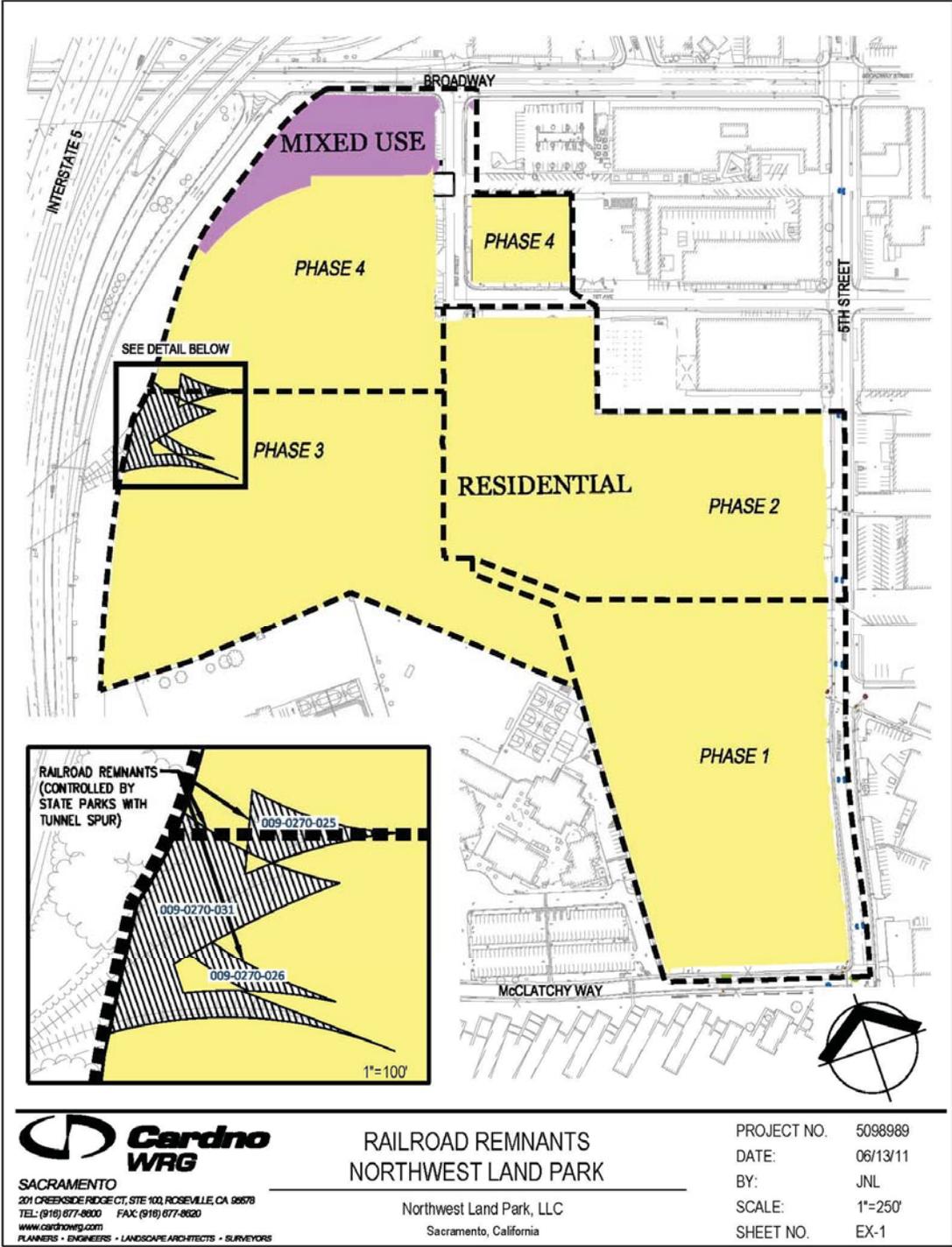


Exhibit D: Land Use Plan Phases 1 through 4



LAND USE PLAN PHASES 1 - 4  
NORTHWEST LAND PARK  
NORTHWEST LAND PARK, LLC  
SACRAMENTO, CALIFORNIA

PROJECT NO: 08866003  
DATE: 08/02/11  
DESIGNED BY: W001/JAS  
DRAWN BY: 8008  
CHECKED BY: JAB

LAND USE  
PHASE 1-4

# Exhibit E: Overall Planting Plan



**STREET TREES**

Botanical Name	Common Name
<i>Acer rubrum</i>	Redbark Maple
<i>Carpinus bellota</i>	Yucca
<i>Celtis occidentalis</i>	Golden Sycamore
<i>Quercus agrifolia</i>	Live Oak
<i>Platanus acerifolia</i>	Plane Tree
<i>Platanus racemosa</i>	California Sycamore
<i>Quercus macrocarpa</i>	Heritage Oak
<i>Quercus lobata</i>	Valley Oak
<i>Quercus shumardii</i>	Shumard Oak
<i>Ulmus parvifolia</i>	European Elm
<i>Ulmus japonicus</i>	Japanese Elm
<i>Zelkova serrata</i>	Green Vase Zelkova
<i>Cedrus deodara</i>	Deodar Cedar
<i>Pinus torreyana</i>	Torrey Pine
<i>Quercus agrifolia</i>	Coast Live Oak
<i>Sonchus oleraceus</i>	Coast Redwood

**ACCENT & COURTYARD TREES**

Botanical Name	Common Name
<i>Acer palmatum</i>	Japanese Maple
<i>Lagerstroemia indica</i>	Crape Myrtle
<i>Malus domestica</i>	Braeburn Crabapple
<i>Quercus agrifolia</i>	Live Oak
<i>Prunus serrulata</i>	Purple Leaf Plum
<i>Pyrus kawakami</i>	Western Redbud
<i>Cupressus sempervirens</i>	Italian Cypress
<i>Olea europaea</i>	European Olive
<i>Xylocopa congestum</i>	Shiny Xylocopa

**SHRUBS**

Botanical Name	Common Name
<i>Agave attenuata</i>	Leaf of the Nile
<i>Amelanchier alnifolia</i>	Western serviceberry
<i>Arbutus unedo</i>	Strawberry Tree
<i>Azalea sp.</i>	Azalea
<i>Buxus japonica</i>	Japanese Boxwood
<i>Carpenteria californica</i>	Carpenteria
<i>Ceanothus sp.</i>	Wax Linc.
<i>Celastrus sp.</i>	Rack Rose
<i>Cornus sericea</i>	Redtwig Dogwood
<i>Cotoneaster sp.</i>	Cotoneaster
<i>Dalrymplea sp.</i>	Fernside Lily
<i>Erica verticillata</i>	Red Escallonia
<i>Eucalyptus japonica</i>	Eucalyptus
<i>Heteromeles arbutifolia</i>	Toyon
<i>Lavandula sp.</i>	Lavender
<i>Ligularia japonica</i>	Priest
<i>Muhlenbergia rigens</i>	Deer Grass
<i>Nandina domestica</i>	Heavenly Bamboo
<i>Phlox paniculata</i>	Flax
<i>Phlox subulata</i>	Phloxia
<i>Pittosporum</i>	Japanese Pittosporum
<i>Rhamnus californica</i>	Coffeetree
<i>Rhapidois indica</i>	Indian Hawthorn
<i>Rosa gymnocarpa</i>	Bald Hip Rose
<i>Rosa woodii</i>	Wood's Rose
<i>Rosemaria officinalis</i>	Rosemary
<i>Spiraea douglasii</i>	Western spiraea
<i>Wisteria floribunda</i>	Coast Rosemary
<i>Xylocopa congestum</i>	Compact Xylocopa

**VINES**

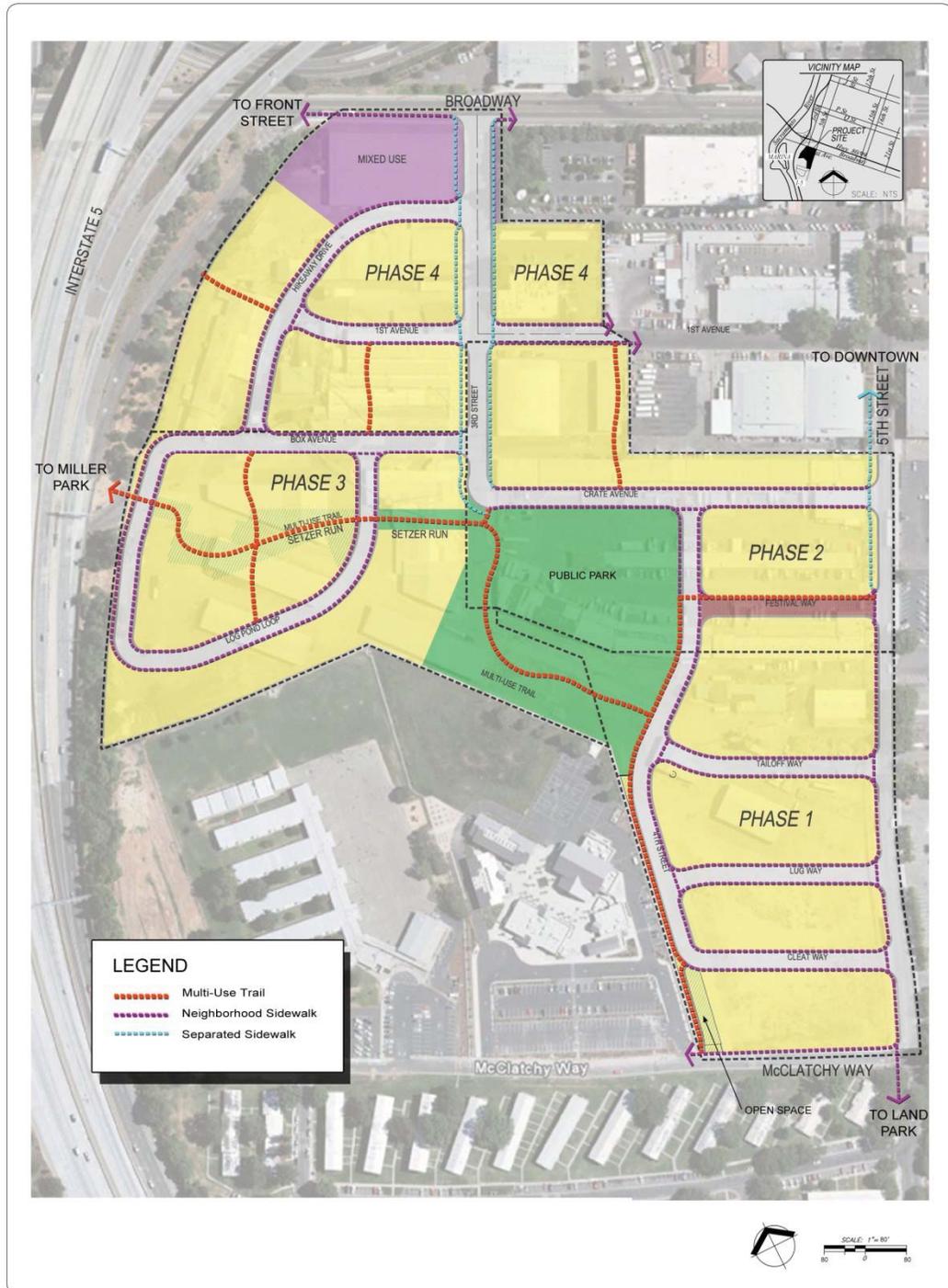
Botanical Name	Common Name
<i>Ficus pumila</i>	Creeping Fig
<i>Hardenbergia violacea</i>	Purple Linc. Vine
<i>Wisteria floribunda</i>	Japanese Wisteria

**Cardno WRG**  
SACRAMENTO  
211 O'NEILL WALKER DRIVE, SUITE 100, SACRAMENTO, CA 95811  
TEL: (916) 677-8800 FAX: (916) 677-8800  
www.cardnowrg.com  
PLANNERS • ENGINEERS • LANDSCAPE ARCHITECTS • SURVEYORS

**OVERALL PLANTING PLAN**  
NORTHWEST LAND PARK, LLC  
SACRAMENTO, CALIFORNIA

PROJECT NO.	50899000
DATE	04-15-11
DESIGNED BY	AKS/JMS
DRAWN BY	HLS
CHECKED BY	JMB

Exhibit F: Overall Circulation Plan

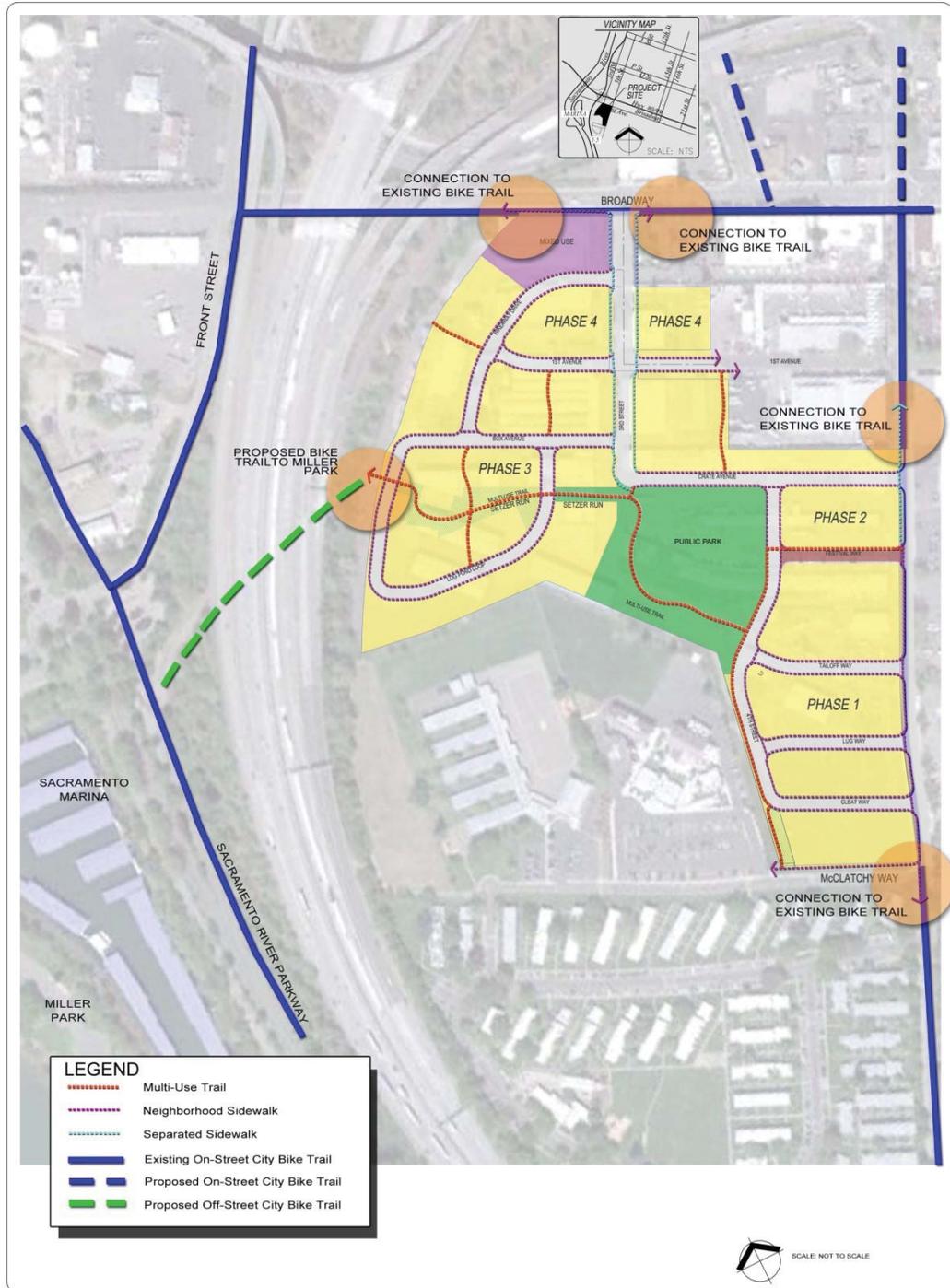


**Cardno**  
**WRG**  
SACRAMENTO  
301 CRENSHAW BLVD, SUITE 100, SACRAMENTO, CA 95811  
TEL: 916.421.7800 FAX: 916.421.7802  
www.cardnowrg.com  
PLANNING • ENGINEERING • LANDSCAPE ARCHITECTURE • SURVEYING

OVERALL CIRCULATION PLAN  
NORTHWEST LAND PARK, LLC  
SACRAMENTO, CALIFORNIA

PROJECT NO.:	030900000
DATE:	04-15-11
DESIGNED BY:	WJW/JNB
DRAWN BY:	PLB
CHECKED BY:	JMB

# Exhibit G: Site Level Bike Trail Plan



**Cardno WRG**  
 SACRAMENTO  
 201 ONEISQUE HEDGE CT, STE 100, ROSELILLE, CA 95678  
 TEL: (916) 477-9600 FAX: (916) 477-9600  
 WWW.CARDNOWRG.COM  
 PLANNERS • ENGINEERS • LANDSCAPE ARCHITECTS • SURVEYORS

**SITE LEVEL BIKE TRAIL PLAN**  
 NORTHWEST LAND PARK, LLC  
 SACRAMENTO, CALIFORNIA

PROJECT NO.: 00000000  
 DATE: 04/15/17  
 DESIGNED BY: MCKINLEY  
 DRAWN BY: KLS  
 CHECKED BY: JNE



## RESOLUTION NO.

Adopted by the Sacramento City Council

**ADOPTING FINDINGS OF FACT AND APPROVING THE NORTHWEST LAND PARK PROJECT'S PHASE 1 TENTATIVE MAP, SUBDIVISION MODIFICATION AND SPECIAL PERMIT TO DEVELOP 201 CONDOMINIUM UNITS IN THE R-4-PUD ZONE FOR AREA BOUNDED BY BROADWAY ON THE NORTH, 5TH STREET ON THE EAST, MCCLATCHY WAY ON THE SOUTH, AND I-5 ON THE WEST. (APN: 009-0030-008, 019, 043, 045; 009-0223-007, 012, 013, 016; 009-0237-018; 009-0270-009, 015, 017, 028, 029, 032, 033; 009-0286-001, 012, 013, 014, 018) (P10-039)**

### BACKGROUND

- A. On July 14, 2011, the City Planning Commission conducted a public hearing on the Northwest Land Park project, and forwarded to the City Council a recommendation to approve the project.
- B. On August 23, 2011, the City Council conducted a public hearing, for which notice was given pursuant Sacramento City Code Section 17.200.010(C)(2)(a), (b), and (c) (publication, posting, and mail 500'), and received and considered evidence concerning the Northwest Land Park project.

### **BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:**

Section 1. Based on the verbal and documentary evidence received at the hearing on the Northwest Land Park project, the City Council approves the project based on the findings of fact and subject to the conditions of approval as set forth below.

Section 2. The City Council approves the Project entitlements based on the following findings of fact:

- A. Tentative Map:** The Tentative Map to subdivide approximately 8.1 gross acres into 17 condominium lots, three private street lots and one temporary detention basin lot is **approved** based on the following findings of fact:

1. None of the conditions described in Government Code Section 66474, subsection (a) through (g), inclusive, exist with respect to the proposed subdivision as follows:
  - a. The proposed subdivision, together with the provisions for its design and improvement, is consistent with the City's General Plan, all applicable community and specific plans, and Title 16 of the City Code, which is a specific plan of the City;
  - b. The site is physically suitable for the type of development proposed and suited for the proposed density;
  - c. The design of the subdivision and the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife their habitat;
  - d. The design of the subdivision and the type of improvements are not likely to cause serious public health problems;
  - e. The design of the subdivision and the type of improvements will not conflict with easements, acquired by the public at large, for access through or use, of, property within the proposed subdivision.
2. The proposed subdivision, together with the provisions for its design and improvement, is consistent with the City General Plan and Title 16 Subdivisions of the City Code, which is a specific plan of the City (Gov. Code §66473.5);
3. The discharge of waste from the proposed subdivision into the existing community sewer system will not result in a violation of the applicable waste discharge requirements prescribed by the California Regional Water Quality Board, Central Valley Region, in that existing treatment plants have a design capacity adequate to service the proposed subdivision (Gov. code §66474.6);
4. The design of the proposed subdivision provides, to the extent feasible, for future passive or natural heating and cooling opportunities (Gov. Code §66473.1);
5. The City Council has considered the effect of the approval of this tentative subdivision map on the housing needs of the region and has balanced these needs against the public service needs of its residents and available fiscal and environmental resources (Gov. Code §66412.3).

**B. Subdivision Modification:** The Subdivision Modification to allow non-standard elbow is **approved** based on the following findings of fact:

1. That the property to be divided is of such size or shape, or is affected by such topographic conditions, or that there are such special circumstances or

conditions affecting the property that it is impossible, impractical, or undesirable in the particular case to conform to the strict application of these regulations;

Due to the shape of the parcel and site constraints, it is impractical or undesirable in this particular case to conform to the strict application of these regulations.

2. That the cost to the subdivider of strict or literal compliance with the regulation is not the sole reason for granting the modification;

The cost to the subdivider of strict or literal compliance with the regulation is not the sole reason for granting the modification.

3. That the modification will not be detrimental to the public health, safety or welfare or be injurious to other properties in the vicinity;

The modification will not be detrimental to the public health, safety or welfare or be injurious to other properties in the vicinity in that the elbow does not abut residential driveways and adequate turning movements by fire trucks can be achieved.

4. That granting the modification is in accord with the intent and purposes of these regulations and is consistent with the general plan and with all other applicable specific plans of the city.

The density and the land use are consistent with general plan goals and policies for Urban Neighborhood Medium designation.

**C. Special Permit:** The Special Permit to construct 201 condominiums in the proposed Multi-Family Planned Unit Development (R-4-PUD) zone is **approved** based on the following findings of fact:

1. A special permit shall be granted upon sound principles of land use.  
Staff finds that the proposed residential development is an appropriate land use that will have positive contribution to the surrounding area, in that the project site is in close proximity to future commercial and open space uses and that the site will be well served by auto, bicycle, and pedestrian linkages.
2. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.  
Staff finds that the proposed condominium development site and building design are consistent with the Northwest Land Park PUD Guidelines and the Multi-Family Residential Design Principles and will not be detrimental to public health, safety or welfare.

3. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.

The proposed project is consistent with the goals and policies of the Urban Neighborhood Medium designation in the General Plan and the Land Park Community Plan.

Section 3. The City Council approves the Project entitlements subject to the following conditions of approval:

### **Conditions of Approval**

- A. The **Tentative Map** to subdivide approximately 8.1 gross acres into 17 condominium lots, three private street lots and one temporary detention basin lot is hereby approved subject to the following conditions:

**NOTE: These conditions shall supersede any contradictory information shown on the Tentative Map or any contradictory provisions in the PUD guidelines approved for this project (P10-039). The design of any improvement not covered by these conditions or the PUD Guidelines shall be to City standard.**

The applicant shall satisfy each of the following conditions prior to filing the Final Map unless a different time for compliance is specifically stated in these conditions. Any condition requiring an improvement that has already been designed and secured under a City Approved improvement agreement may be considered satisfied at the discretion of the Department of Transportation.

### **GENERAL:** All Projects

- A1. Pay off existing assessments, or file the necessary segregation requests and fees to segregate existing assessments.
- A2. Pursuant to City Code Section 16.40.190, indicate easements on the Final Map to allow for the placement of centralized mail delivery units. The specific locations for such easements shall be subject to review and approval of the Department of Transportation after consultation with the U.S. Postal Service.
- A3. Private reciprocal ingress, egress, maneuvering and parking easements are required for future development of the area covered by this Tentative Map. The applicant shall enter into and record an Agreement for Conveyance of Easements with the City stating that a private reciprocal ingress/egress, maneuvering, and parking easement shall be conveyed to and reserved from all appropriate parcels, at no cost, at the time of sale or other conveyance of either

parcel.

- A4. Comply with requirements included in the Mitigation Monitoring and Reporting Program developed by, and kept on file in, the Planning Division Office (P10-039).
- A5. Show all continuing and proposed/required easements on the Final Map.
- A6. Multiple Final Maps may be recorded. Prior to recordation of any Final Map all infrastructure/improvements necessary for the respective Final Map must be in place to the satisfaction of the Departments of Utilities, and Department of Transportation.

**Department of Transportation: Streets**

- A7. Submit a Geotechnical Analysis prepared by a registered engineer to be used in street design. The analysis shall identify and recommend solutions for groundwater related problems, which may occur within both the subdivision lots and public right-of-way. Construct appropriate facilities to alleviate those problems. As a result of the analysis street sections shall be designed to provide for stabilized subgrades and pavement sections under high groundwater conditions.
- A8. Construct standard subdivision improvements as noted in these conditions pursuant to section 16.48.110 of the City Code. All improvements shall be designed and constructed to the satisfaction of the Department of Transportation. Improvements required shall be determined by the city. The City shall determine improvements required for each phase prior to recordation of each phase. Any public improvement not specifically noted in these conditions or on the Tentative Map shall be designed and constructed to City standards. This shall include street lighting and the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk fronting the property (5<sup>th</sup> street, McClatchy Way) per City standards to the satisfaction of the Department of Transportation.
- A9. The applicant shall dedicate a public easement and construct streets (Tailoff way, Lug Way, Cleat way and 4<sup>th</sup> street) within Phase 1 as an infill street section of 41-feet with attached sidewalks to the satisfaction of the Department of Transportation.
- A10. The applicant shall provide additional roadway width (approximately 2-feet) along the west side of 4<sup>th</sup> Street (Phase 1) and along the detention basin's frontage only and construct a modified 43-foot section with a raised curb to accommodate on-street parking to the satisfaction of the Department of Transportation.

- A11. The applicant shall dedicate sufficient right-of-way (If needed) and restripe 5<sup>th</sup> street to include one travel lane in each direction and bike lanes for each direction separated by a continuous two-way left turn lane along the project's frontage in phase 1 to the satisfaction of the Department of Transportation.
- A12. At its discretion, the City may require the inclusion of traffic calming devices along residential streets, to be constructed as part of the public improvements. These devices may include, but are not limited to, chicanes, undulations, additional 4-way intersections, etc. Undulations will be required on certain streets adjacent to school/park combinations, as determined by the Department of Transportation.
- A13. The design and placement of walls, fences, signs and Landscaping near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height. The area of exclusion shall be determined by the Department of Transportation.
- A14. All right-of-way and street improvement transitions that result from changing the right-of-way of any street shall be located, designed and constructed to the satisfaction of the Department of Transportation. The center lines of such streets shall be aligned.
- A15. Provide a temporary traffic turnaround at the end of 4<sup>th</sup> Street in the form of a hammer head to the satisfaction of the Department of Transportation.
- A16. The applicant shall provide for a temporary barricade at the end of 4<sup>th</sup> Street for phase 1 and shall provide all the appropriate signage and markings to the satisfaction of the Department of Transportation.
- A17. The applicant shall make provisions for bus stops, shelters, etc. to the satisfaction of Regional Transit.
- A18. The applicant shall explore the feasibility of constructing a traffic circle at the intersection of McClatchy and 5th Streets as part of their public improvements. The traffic circle shall be constructed per City standards and to the satisfaction of the Department of Transportation. If the traffic circle construction is found to be infeasible, the applicant shall contribute the sum of \$50,000 to go towards a Neighborhood Traffic Management Program (NTMP) within the adjacent neighborhood, including 5th Street and Vallejo Street. If the construction of the traffic circle is found to be feasible, the applicant has agreed to construct the traffic circle. Should the total design and construction cost of the traffic circle be less than \$50,000, the remaining balance shall be paid towards the above referenced NTMP. This one time contribution, from the applicant, shall satisfy

all requirements towards any neighborhood traffic calming program related to the development of the Northwest Land Park PUD.

**Department of Transportation: Private Streets (DOT)**

- A19. Design private drives to meet the City standards regarding structural section. Private drives shall be inspected to the satisfaction of the Department of Transportation.
- A20. The applicant shall delineate the private streets by constructing decorative paving at the entrance to the private streets from 5<sup>th</sup> Street and using appropriate streets signs to the satisfaction of the Department of Transportation.
- A21. The applicant may use a permeable material such as asphaltic concrete for the construction of all private drives to the satisfaction of the Department of Transportation. All private drives shall be maintained by the Home Owners Association. Any other material shall require the approval of the Department of Transportation and the Community Development Department.

**PUBLIC/PRIVATE UTILITIES**

- A22. An easement shall be granted on, over, under and across residential lots 1-17, as shown here on, as a blanket "Public Utility Easement" for dry utilities, excepting there from future building footprints.

**FIRE (King Tunson, Fire Department, 808-1358)**

- A23. All turning radii for fire access shall be designed as 35' inside and 55' outside. Shall apply to private streets and alleys designed for fire access.
- A24. Roads used for Fire Department access shall have an unobstructed width of not less than 20' and unobstructed vertical clearance of 13'6" or more. *Shall apply to private streets and alleys designed for fire access.*
- A25. Fire Apparatus access roads shall be designed and maintained to support the imposed loads of fire apparatus and shall be surfaced so as to provide all-weather driving capabilities. CFC 503.2.3 *The proposed usage of synthetic turf within the alley shall be submitted for review and approval by the Fire Marshal. The aggregate base sections shall be to the manufacture's specifications or should be 12 inches thick. Compaction shall be verified in writing to the Fire Marshal by a licensed engineer.*
- A26. Provide the required fire hydrants in accordance with CFC 508 and Appendix C, Section C105.
- A27. Roads used for Fire Department access that are less than 28 feet in width shall

be marked "No Parking Fire Lane" on both sides; roads less than 36 feet in width shall be marked on one side. *Shall apply to private streets and alleys designed for fire access.*

- A28. Maintenance agreements shall be provided for the interior roadways of the proposed complex and for the fire protection systems. The agreement shall be recorded in the Office of the Sacramento County Recorder and shall provide for the following:
- 1 Provisions for the necessary repair and maintenance of the roadway surface.
  - 2 Removal of vegetation overgrowing the roadway and infringing on the roadway clear vertical height of thirteen feet six inches (13'6") and/or width of twenty feet (20').
  - 3 Provisions for the maintenance, repair, and/or replacement of NO PARKING-FIRE LANE signage or striping.
  - 4 Provisions for the necessary repair and maintenance of vehicle and pedestrian access gates and opening systems.
  - 5 Unrestricted use of and access to the roadways covered by the agreements.
  - 6 Provisions for the control of vehicle parking in prohibited areas and a mechanism for the removal of vehicles illegally parked.
  - 7 Maintenance and timely repair of all fire protection systems, including but not limited to hydrants, fire alarm systems and fire sprinklers.

**CITY UTILITIES** (Inthira Mendoza, Utilities Department, 808-1473)

- A29. The onsite water, sewer and storm drain systems on Lots 1-17 shall be private systems maintained by the association. Prior to the initiation of any water, sanitary sewer or storm drainage services to the condominium project, an ownership association shall be formed and C.C. & R.s shall be approved by the City and recorded assuring maintenance of sanitary sewer, water and storm drainage facilities within the condominium project. The C.C. & R.s shall authorize the association to contract on behalf of all owners within the condominium project for sanitary sewer, water and storm drainage services for the condominium units, common areas and all other areas within the condominium project. The association shall enter a Utility Service Agreement with the DOU for sanitary Sewer, Water and storm drainage services.
- A30. A water study for this project shall be completed by the applicant and shall be approved by the DOU. The water distribution system shall be designed to satisfy the more critical of the two following conditions: (1) at maximum day peak hour demand, the operating or "residual" pressure at all water service connections shall be a least 30 pounds per square inch and (2) at average maximum day demand plus fire flow, the operating or "residual" pressure in the area of the fire shall not be less than 20 pounds per square inch. The water study shall determine if the existing and proposed water distribution system is

adequate to supply fire flow demands for the project. A water supply test may be required for this project. Contact the Department of Utilities for the pressure boundary conditions to be used in the water study.

- A31. All water connections shall comply with the City of Sacramento's Cross Connection Control Policy. Any new domestic water services shall be metered.
- A32. Two points of connection for the water distribution system for this subdivision or any phase of this subdivision are required.
- A33. If required by the DOU, the applicant shall enter into and record a Hold Harmless Agreement, in a form acceptable to the City Attorney, for the placement of water meters within driveways, walkways, hardscape and concrete or asphalt concrete (AC) flat work.
- A34. Decorative paving which is removed by the City while repairing, maintaining and/or replacing surface and subsurface water, drainage and sanitary sewer facilities will be repaved with asphalt concrete (AC). A business association and/or homeowners association shall be responsible for replacing the decorative paving at no cost to the City. The business and/or homeowners associations shall enter into and record a hold harmless agreement, in a form acceptable to the City Attorney, regarding the removal and replacement of decorative paving by the City.
- A35. Dedicate all private streets and a minimum of 3 feet adjacent to private streets as public easement for water, sewer, drainage facilities and dry utilities. This easement shall be reviewed and approved by the Department of Utilities prior to recordation of the Final Map.
- A36. Public and private streets with City maintained water, sewer and drainage facilities shall have a minimum paved AC (asphalt concrete) width of 25-feet from the lip of curb to the lip of curb (i.e. water, sewer and drainage). Drain inlets, curb and gutter shall be constructed to City Standards for residential streets.
- A37. Public sewer and drainage mains shall be separate systems.
- A38. The water, sewer and drainage system within lots B, C, and D shall be public utilities. Public storm drain, water and sanitary sewer mains shall be designed and constructed within the asphalt section of the private street as per the City's Design and Procedures Manual, unless otherwise approved by the DOU.
- A39. This project is served by the Combined Sewer System (CSS). Therefore, the developer/property owner will be required to pay the Combined Sewer System Development Fee prior to the issuance of building permit. The Combined Sewer System fee at time of building permit is estimated to be **\$358,978.31** plus

any increases to the fee due to inflation. The fee will be used for improvements to the CSS.

- A40. Drain inlets shall be 6 inches above the 10-year HGL. Finished lot pad elevations shall be a minimum of 1.2 feet above the 100-year HGL and a minimum of 1.5 feet above the local controlling overland flow release elevation, whichever is higher or as approved by DOU.

Prior to or concurrent with the submittal of improvement plans, a combined sanitary sewer study is required for the entire project and shall be approved by the DOU. The study shall provide an analysis of the pre and post development condition of both the sewer and drainage flow that is contributing to the combined system. The drainage study shall include an overland flow release map for the proposed project. If it is shown that the post development conditions cause an increase in flow to the combined system, either onsite storage and/or improvements to existing combined sanitary system will be required to the satisfaction of the DOU.

- A41. Per City Code, the Subdivider may not develop the project in any way that obstructs, impedes, or interferes with the natural flow of existing off-site drainage that crosses the property. The project shall construct the required public and/or private infrastructure to handle off-site runoff to the satisfaction of the DOU. If private infrastructure is constructed to handle off-site runoff, the applicant shall dedicate the required private easements and/or, at the discretion of the DOU, the applicant shall enter into and record an Agreement for Maintenance of Drainage with the City, in a form acceptable to the City Attorney. Sufficient off-site and on-site spot elevations shall be provided in the drainage study to determine the direction of storm drain runoff.
- A42. The applicant shall design and construct the private detention basin on Lot A to the satisfaction of the DOU and Parks and Recreation Department pursuant to City Standards. A separate set of improvement plans shall be prepared for the detention basin.
- A43. An as-built survey of the detention basin is required prior to issuance of a notice of completion for the subdivision.
- A44. The applicant or HOA shall properly maintain the temporary detention basin to City standards. To ensure proper maintenance, the applicant shall enter into a maintenance agreement with the City which may require posting or depositing with the City of unconditional security for performance of the applicant's obligations, which security is adequate in the sole and exclusive discretion of the City, and which is in a form acceptable to the City Attorney.
- A45. A note stating the following shall be placed on the Final Map: "Where necessary private reciprocal easement for ingress/egress, utilities, drainage,

water and sanitary sewer facilities, and surface storm drainage, shall be granted and reserved, as necessary and at no cost, at or before the time of sale or conveyance of any parcel shown in this map.

- A46. A grading plan showing existing and proposed elevations is required. Adjacent off-site topography shall also be shown to the extent necessary to determine impacts to existing surface drainage paths. No grading shall occur until the grading plan has been reviewed and approved by the Department of Utilities.
- A47. The applicant must comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinance. This ordinance requires the applicant to show erosion and sediment control methods on the subdivision improvement plans. These plans shall also show the methods to control urban runoff pollution from the project site during construction.
- A48. Stormwater quality control measures shall be incorporated into the development to minimize the increase of urban runoff pollution caused by development in the area. Since the project is in the combined sewer area, only source controls are required. Storm drain public notice message is required at all drain inlets. Improvement plans must include the source controls measures selected for the site. Refer to the "Stormwater Quality Design Manual for the Sacramento and South Placer Regions", dated May 2007 for appropriate source controls measures.
- A49. Each block shall have a common metered irrigation service serving all parcels within the block. The applicant shall enter into a Utility Service Agreement for Common Irrigation with the DOU for a common irrigation service, on such terms and conditions as may be determined by the DOU. The applicant shall grant an easement or other property right to a HOA or an entity authorizing a common irrigation service for multiple parcels within each block.

**PPDS:** Parks (Mary de Beauvieres, Parks Department, 808-8722)

- A50. **Payment of In-lieu Park Fee:** Pursuant to Sacramento City Code Chapter 16.64 (Parkland Dedication) the applicant shall pay to City an in-lieu park fee in the amount determined under SCC §§16.64.040 and 16.64.050 equal to the value of land prescribed for dedication under 16.64.030 and not satisfied by dedication. (See Advisory Note).
- A51. **Maintenance District:** The applicant shall initiate and complete the formation of a parks maintenance district (assessment or Mello-Roos special tax district), or annex the project into an existing parks maintenance district. The applicant shall pay all city fees for formation of or annexation to a parks maintenance district (contact Public Improvement Financing, Special Districts Project Manager.) In assessment districts, the cost of neighborhood park maintenance is equitably spread on the basis of special benefit. In special tax districts, the

cost of neighborhood park maintenance is spread based upon the hearing report, which specifies the tax rate and method of apportionment.).

- A52. **Design Coordination for PUE's and Facilities:** If a public utility easement (PUE) for underground facilities and appurtenances currently exists or is required to be dedicated adjacent to and contiguous to Lot A, the applicant shall coordinate with PPDS and SMUD regarding the location of appurtenances within the PUE to minimize visual obstruction in relation to the park and to best accommodate future park improvements. The applicant shall facilitate a meeting with SMUD and PPDS prior to SMUD's facilities coordinating meeting for the project.

## **MISCELLANEOUS**

- A53. Meet all applicable terms and conditions of the development agreement.
- A54. Title to any property required to be dedicated to the City in fee shall be conveyed free and clear of all rights, restrictions, easements, impediments, encumbrances, liens, taxes, assessments or other security interests of any kind (hereafter collectively referred to as "Encumbrances"), except as provided herein. The applicant shall take all actions necessary to remove any and all Encumbrances prior to approval of the Final Map and acceptance of the dedication by City, except that the applicant shall not be required to remove Encumbrances of record, including but not limited to easements or rights-of-way for public roads or public utilities, which, in the sole and exclusive judgment of the City, cannot be removed and/or would not interfere with the City's future use of the property. The applicant shall provide title insurance with the City as the named beneficiary assuring the conveyance of such title to City. (DOT)
- A55. Form a Homeowner's Association. CC&R's shall be approved by the City and recorded assuring maintenance of all private streets (Tailoff way, Lug Way, Cleat way and 4<sup>th</sup> street) and all private drives. The Homeowner's Association shall maintain all private streets, private drives, lights, private utilities, common landscaping/open space (Lot A), common areas and the landscaping sliver within parcel 4 along the west side of 4<sup>th</sup> street. (DOT)

## **ADVISORY NOTES:**

The following advisory notes are informational in nature and are not a requirement of this Tentative Map:

- A56. If unusual amounts of bone, stone, or artifacts are uncovered, work within 50 meters of the area will cease immediately and a qualified archaeologist shall be consulted to develop, if necessary, further mitigation measures to reduce any archaeological impact to a less than significant effect before construction

resumes. A note shall be placed on the final improvement plans referencing this condition. (DOT)

- A57. Prior to the initiation of any water or storm drainage services to the condominium project, the owner(s) and ownership association shall enter into a utility service agreement with the City to receive such utility services at points of service designated by the DOU. Such agreement shall provide, among other requirements, for payment of all charges for the condominium project's water and storm drainage services, shall authorize discontinuance of utility services at the City's points of service in the event that all or any portion of such charges are not paid when and as required, shall require compliance with all relevant utility billing and maintenance requirements of the City, shall require sub-metering of water service to the condominium units if requested by the DOU or required by any other government agency, and shall be in a form approved by the City Attorney. (DOU)
- A58. Many projects within the City of Sacramento require on-site booster pumps for fire suppression and domestic water system. Prior to design of the subject project, the Department of Utilities suggests that the applicant request a water supply test to determine what pressure and flows the surrounding public water distribution system can provide to the site. This information can then be used to assist the applicant's engineers in the design of the on-site domestic, irrigation and fire suppression systems. (DOU)
- A59. Location of the wet utilities in the street without center medians shall be as follows, drainage mains shall be placed in the centerline of the street, water mains shall be placed north and west of the centerline and the sewer main shall be placed south and east of the centerline. The location of wet utilities in streets with medians shall be approved by DOU pursuant to City Standards prior to design. (DOU)
- A60. Prior to issuance of any building permits within any phase, all sanitary sewer, storm drainage, water, and flood control improvements shall be in place and fully functioning as determined by DOU unless otherwise approved by the Department of Utilities. (DOU)
- A61. The proposed project is located in the Flood zone designated as **Shaded X** zone on the Federal Emergency Management Agency (FEMA) Federal Insurance Rate Maps (FIRMs) that have been revised by a Letter of Map Revision effective February 18, 2005. Within the Shaded X zone, there are no requirements to elevate or flood proof. (DOU)
- A62. The applicant is responsible for obtaining all necessary permits, easements and approvals from federal, state and local agencies, and private landowners for the construction of this project. (DOU)

- A63. Developing this property will require the payment of SRCSD Sewer Impact fees. Impact fees shall be paid prior to issuance of Building Permits. Applicant should contact the Fee Quote Desk at (916) 876-6100 for sewer impact fee information. (SRCSD)
- A64. **Improvements**: The applicant shall construct the following public improvements prior to and as a condition of City's eventual acceptance of Lot A as the park site. Even though Lot A will be dedicated as parkland in a subsequent phase, the frontage improvements should occur during Phase 1 as follows:
- 1 Full street improvements for Lot A, including but not limited to curbs, gutters, accessible ramps, street paving, streetlights, and sidewalks.
  - 2 A concrete sidewalk and vertical curb along all street frontages that open onto "Lot A". The sidewalk shall be contiguous to the curb (attached) for neighborhood parks.
- A65. As per City Code, acreage within an existing or proposed drainage area, easement, public right-of-way, or areas with 10% and greater slopes shall not receive parkland dedication credit. Quimby parkland credit can be granted only to "buildable acres". (PARKS)
- A66. As per City Code, the applicant will be responsible to meet his/her obligations regarding: (PARKS)
- 1 Title 16, 16.64 Park Dedication / In Lieu (Quimby) Fees, due prior to recordation of the final map. The Quimby fee due for this project is estimated at \$664,350. This is based on 29 single family, 112 duplex / halfplex, and 60 multi-family residential units and an average land value of \$250,000 per acre for the Land Park Community Planning Area, plus an additional 20% for off-site park infrastructure improvements. Any change in these factors will change the amount of the Quimby fee due. The final fee is calculated using factors at the time of payment.
  - 2 Title 18, 18.44 Park Development Impact Fee, due at the time of issuance of building permit. The Park Development Impact Fee due for this project is estimated at \$771,939. This is based on 29 single family residential units at \$5,191 each, 112 duplex / halfplex residential units at \$3,910 each, and 60 multi-family residential units at \$3,058 each. Any change in these factors will change the amount of the PIF due. The fee is calculated using factors at the time that the project is submitted for building permit. The fee adjusts for inflation on July 1 of each year.
  - 3 Community Facilities District 2002-02, Neighborhood Park Maintenance CFD Annexation.

B. The **Special Permit** to construct 201 condominiums in the proposed Multi-Family Planned Unit Development (R-4-PUD) zone is hereby approved subject to the following conditions:

### Planning

- B1. This approval is for the construction of 201 condominium units per attached exhibits. Any change in the design, materials, or colors from this approval shall be submitted to the Planning Division for review and determination for further actions.
- B2. Final landscape and circulation plans shall be submitted to Planning Division for review and comment prior to submittal for building permits.
- B3. The applicant shall obtain all necessary building permits prior to commencement of construction.
- B4. Building plans and elevations shall be plotted such that no two same elevations and no two same color schemes are adjacent to one another. All building side elevations facing a street shall be enhanced with window openings, trims and decorative elements.
- B5. Provide an ownership association responsible for the care and maintenance of all common areas and common improvements and any other interest common to the condominium owners. Complete and true copies of all covenants, conditions and restrictions, articles of incorporation and by-laws shall be subject to review and approval by the city prior to occupancy as a condominium unit.
- B6. The homeowner's association shall conduct periodic inspections, not less than monthly, of the exterior of all buildings, trash enclosures and recreation facilities.
- B7. The homeowner's association shall establish and conduct a regular program of routine maintenance for the property. Such a program shall include common areas and scheduled repainting, replanting and other similar activities that typically require attention at periodic intervals but not necessarily continuous. Owner/Operator shall repaint or retreat all painted or treated areas at least once every 8 years; provided that the Planning Director may approve less frequent painting or re-treatment upon a determination that less frequent repainting or re-treatment is appropriate, given the nature of the materials used or other factors. The program shall be subject to review and approval by the Planning Director.
- B8. The homeowner's association shall maintain landscaping and irrigation in a healthy and serviceable condition.
- B9. The homeowner's association shall indicate and maintain all locations of parking stalls for handicapped/disabled access and strictly enforce rules related thereto.

- B10. Each condominium unit shall comply with the state of California's Noise Insulation Standards (California Amended Code Section 1092).
- B11. Each condominium unit shall provide the following:
- a. A separate sewer service hookup; provided, that the planning commission may permit the use of common sewer lines that are oversized by one size or more, or which are hydraulically designed with the concurrence of the city engineer, finds the common sewer lines can adequately service the condominiums and that separate service hookups would not be feasible. For this provision, the Planning Commission is delegating the approval to the Planning Director subject to concurrence with the Public Works Director and the Utilities Department.
  - b. A separate water service hookup or shutoff; provided, that the planning commission may permit a single water system to service more than one condominium unit where shutoffs are provided wherever practicable and where the planning commission, with the concurrence of the city engineer, finds that the single water system can adequately service the condominiums and separate service hookups or shutoffs are not feasible. For this provision, the Planning Commission is delegating the approval to the Planning Director subject to concurrence with the Public Works Director and the Utilities Department.
  - c. A separate gas service where gas is a necessary utility.
  - d. A separate electrical service, with separate meters and disconnects and ground fault interrupters where and as required by Building Code.
- B12. Each unit of a condominium project, and all commonly owned portions of a condominium building shall comply with all applicable building code standards. Nothing herein shall be construed to prevent or prohibit the applicant or the city from providing or requiring building standards greater than those set forth in the Building Code where the greater standards are found to be necessary to carry out the purposes and objectives of this chapter. (Ord. 99-015 § 6-3-D)
- B13. Landscaping & Walls and Fencing:
- a. All landscaping and planting shall conform to City standards for sight line requirements at intersections and driveways.
  - b. All mechanical equipment shall be located within enclosed cabinets or screened by landscaping and/or screening/fencing.

- c. In order to provide adequate surveillance opportunities, all *plants and shrubs* are to be maintained at maximum height of thirty inches (30"); the lowest branch height shall be at least six feet (6'). Decorative planting shall be maintained so as not to obstruct or diminish lighting level throughout the project.
- d. Walls and fences shall conform to City standards for sight line requirements at intersections and driveways.

B14. The applicant shall comply with the City's Recycling Ordinance (Section 17.72).

B15. Signage within Phase 1:

- a. All detached signs shall be monument-type, constructed of masonry with finish materials and colors, which are consistent with building architecture.
- b. The project identification sign(s) may be placed in the landscape setback area; however, it must be located farther than ten (10) feet from the public right-of-way.
- c. Attached signage shall consist of address numbers only.
- d. All signage shall comply with the Northwest Land Park PUD signage criteria; a sign permit shall be obtained prior to construction of any sign.

B16. Lighting:

- a. The type and location of the outdoor lighting (building, parking lot, walkway, etc.) must be approved by the Planning Director prior to issuance of a building permit. Lighting shall be provided in garage areas and each building address number shall be illuminated.
- b. Project lighting shall be provided as follows: one foot-candle of minimum maintained illumination per square foot of parking space and exterior walkways/sidewalks during hours of darkness and 0.25 foot-candle of minimum maintained illumination per square foot of surface on any interior walkway, alcove, passageway, etc., from one-half hour before dusk to one-half hour after dawn. All light fixtures are to be vandal-resistant.
- c. Per Section 17.68.030(B), exterior lighting, if provided, shall reflect away from residential areas and public streets.
- d. Fixtures shall be unobtrusive and complementary to the architectural design of the building. Lighting shall be designed so as not to produce hazardous and annoying glare to motorists and building occupants, adjacent residents, or the general public.

- B17. The applicant shall comply with the approved Mitigation Monitoring and Reporting Program (P10-039) on file at the Planning Division.
- B18. The final building color palette shall be submitted for review and approval by the Planning Director prior to the issuance of building permit.
- B19. The applicant shall paint electrical meters/cabinets, telephone connection boxes and other utility appurtenances to match the building to which they are attached.
- B20. *If security becomes a concern and if deemed necessary by the Police Department*, the applicant shall contract with a reputable security firm to provide on-site security, night and day.
- B21. Prior to the issuance of any building permits, the applicant shall provide the City with a copy of the certificate of payment of school fees for the applicable school district(s).
- B22. This approval does not include any vehicular gates. Any proposal for vehicular gates will require approval of an additional entitlement.
- B23. Condominiums addressing shall be to City standards.
- B24. Auto repairs on the street or within the private driveways are not permitted at any time, except in emergencies, such as flat tire or dead battery.

#### **Department of Transportation**

- B25. Construct standard improvements as noted in these conditions pursuant to section 18 of the City Code. All improvements shall be designed and constructed to the satisfaction of the Department of Transportation. Improvements required shall be determined by the city. The City shall determine improvements required for each phase prior to recordation of each phase. Any public improvement not specifically noted in these conditions or on the Tentative Map shall be designed and constructed to City standards. This shall include street lighting and the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk fronting the property (5th street, McClatchy Way) per City standards to the satisfaction of the Department of Transportation.
- B26. The applicant shall dedicate a public easement and construct streets (Tailoff way, Lug Way, Cleat way and 4th street) within Phase 1 as an infill street section of 41-feet with attached sidewalks to the satisfaction of the Department of Transportation.
- B27. The applicant shall provide additional roadway width (approximately 2-feet) along the west side of 4th Street (Phase 1) and along the detention basin's frontage

only and construct a modified 43-foot section with a raised curb to accommodate on-street parking to the satisfaction of the Department of Transportation.

- B28. The applicant shall dedicate sufficient right-of-way (If needed) and restripe 5th street to include one travel lane in each direction and bike lanes for each direction separated by a continuous two-way left turn lane along the project's frontage in phase 1 to the satisfaction of the Department of Transportation.
- B29. The design and placement of walls, fences, signs and Landscaping near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height. The area of exclusion shall be determined by the Department of Transportation.
- B30. All right-of-way and street improvement transitions that result from changing the right-of-way of any street shall be located, designed and constructed to the satisfaction of the Department of Transportation. The center lines of such streets shall be aligned.
- B31. Provide a temporary traffic turnaround at the end of 4th Street in the form of a hammer head to the satisfaction of the Department of Transportation.
- B32. The applicant shall provide for a temporary barricade at the end of 4th Street for phase 1 and shall provide all the appropriate signage and markings to the satisfaction of the Department of Transportation.
- B33. Design private drives to meet the City standards regarding structural section. Private drives shall be inspected to the satisfaction of the Department of Transportation.
- B34. The applicant shall delineate the private streets by constructing decorative paving at the entrance to the private streets from 5th Street and using appropriate streets signs to the satisfaction of the Department of Transportation.
- B35. The applicant may use a permeable material such as asphaltic concrete for the construction of all private drives to the satisfaction of the Department of Transportation. All private drives shall be maintained by the Home Owners Association. Any other material shall require the approval of the Department of Transportation and the Community Development Department.
- B36. The applicant shall record the Final Map, which creates the lot pattern shown on the proposed site plan prior to obtaining any Building Permits.

- B37. The site plan shall conform to A.D.A. requirements in all respects. This shall include the replacement of any curb ramp that does not meet current A.D.A. standards.
- B38. Form a Homeowner's Association. CC&R's shall be approved by the City and recorded assuring maintenance of all private streets (Tailoff way, Lug Way, Cleat way and 4th street) and all private drives. The Homeowner's Association shall maintain all private streets, private drives, lights, private utilities, common landscaping/open space (Lot A), common areas and the landscaping sliver within parcel 4 along the west side of 4th street.

### **Fire Department**

- B39. Timing and Installation. When fire protection, including fire apparatus access roads and water supplies for fire protection, is required to be installed, such protection shall be installed and made serviceable prior to and during the time of construction.
- B40. Provide a water flow test. (Make arrangements at the Permit Center walk-in counter: 300 Richards Blvd, Sacramento, CA 95814). CFC 508.4
- B41. The furthest projection of the exterior wall of a building shall be accessible from within 150 ft of an approved Fire Department access road and water supply as measured by an unobstructed route around the exterior of the building. (CFC 503.1.1)
- B42. Provide appropriate Knox access for site- *Shall apply to all proposed R-2 occupancies.*
- B43. An automatic fire sprinkler system shall be installed in any portion of a building when the floor area of the building exceeds 3,599 square feet. *This shall apply to all proposed R-2 occupancies.*
- B44. Locate and identify Fire Department Connections (FDCs) on address side of building no further than 50 feet and no closer than 15 feet from a fire hydrant. *Shall apply to all proposed R-2 occupancies.*
- B45. An approved fire control room shall be provided for all buildings protected by an automatic fire extinguishing system. Fire control rooms shall be located within the building at a location approved by the Chief, and shall be provided with a means to access the room directly from the exterior. Durable signage shall be provided on the exterior side of the access door to identify the fire control room. CFC 903.8 *Shall apply to all proposed R-2 occupancies.*
- B46. Provide at least 5' setback for second story and 8' for third story bedroom windows to allow for fire ladder rescue operations. Provide clear access to

buildings openings, free to landscaping and other obstructions. Exterior doors and openings required by this code or the Building Code shall be maintained readily accessible for emergency access by the Fire Department. CFC 504.1

- B47. Applicant must explain the potential use of tunnel and provide access for rescue operations.

### **Utilities Department**

- B48. The onsite water, sewer and storm drain systems shall be private systems maintained by the association. Prior to the initiation of any water, sanitary sewer or storm drainage services to the condominium project, an ownership association shall be formed and C.C. & R.s shall be approved by the City and recorded assuring maintenance of sanitary sewer, water and storm drainage facilities within the condominium project. The C.C. & R.s shall authorize the association to contract on behalf of all owners within the condominium project for sanitary sewer, water and storm drainage services for the condominium units, common areas and all other areas within the condominium project.
- B49. Prior to the initiation of any water or storm drainage services to the condominium project, the owner(s) and ownership association shall enter into a utility service agreement with the City to receive such utility services at points of service designated by the DOU. Such agreement shall provide, among other requirements, for payment of all charges for the condominium project's water and storm drainage services, shall authorize discontinuance of utility services at the City's points of service in the event that all or any portion of such charges are not paid when and as required, shall require compliance with all relevant utility billing and maintenance requirements of the City, shall require sub-metering of water service to the condominium units if requested by the DOU or required by any other government agency, and shall be in a form approved by the City Attorney.
- B50. A water study for this project shall be completed by the applicant and shall be approved by the DOU. The water distribution system shall be designed to satisfy the more critical of the two following conditions: (1) at maximum day peak hour demand, the operating or "residual" pressure at all water service connections shall be a least 30 pounds per square inch and (2) at average maximum day demand plus fire flow, the operating or "residual" pressure in the area of the fire shall not be less than 20 pounds per square inch. The water study shall determine if the existing and proposed water distribution system is adequate to supply fire flow demands for the project. A water supply test may be required for this project. Contact the Department of Utilities for the pressure boundary conditions to be used in the water study.
- B51. All water connections shall comply with the City of Sacramento's Cross Connection Control Policy. Any new domestic water services shall be metered

- B52. Two points of connection for the water distribution system for this subdivision or any phase of this subdivision are required.
- B53. If required by the DOU, the applicant shall enter into and record a Hold Harmless Agreement, in a form acceptable to the City Attorney, for all lots within the subdivision regarding the placement of water meters within driveways, walkways, hardscape and concrete or asphalt concrete (AC) flat work.
- B54. Decorative paving which is removed by the City while repairing, maintaining and/or replacing surface and subsurface water, drainage and sanitary sewer facilities will be repaved with asphalt concrete (AC). A business association and/or homeowners association shall be responsible for replacing the decorative paving at no cost to the City. The business and/or homeowners associations shall enter into and record a hold harmless agreement, in a form acceptable to the City Attorney, regarding the removal and replacement of decorative paving by the City.
- B55. Dedicate all private streets and a minimum of 3 feet adjacent to private streets as public easement for water, sewer, drainage facilities and dry utilities. This easement shall be reviewed and approved by the Department of Utilities prior to recordation of the Final Map.
- B56. Public and private streets with City maintained water, sewer and drainage facilities shall have a minimum paved AC (asphalt concrete) width of 25-feet from the lip of curb to the lip of curb (i.e. water, sewer and drainage). Drain inlets, curb and gutter shall be constructed to City Standards for residential streets.
- B57. No public water, sewer or drainage mains shall be placed under the planter area.
- B58. Public sewer and drainage mains shall be separate systems.
- B59. Public storm drain, water and sanitary sewer mains shall be designed and constructed within the asphalt section of public street right-of-ways as per the City's Design and Procedures Manual, unless otherwise approved by the DOU.
- B60. This project is served by the Combined Sewer System (CSS). Therefore, the developer/property owner will be required to pay the Combined Sewer System Development Fee prior to the issuance of building permit. The Combined Sewer System fee at time of building permit is estimated to be \$358,978.31 plus any increases to the fee due to inflation. The fee will be used for improvements to the CSS.
- B61. Drain inlets shall be 6 inches above the 10-year HGL. Finished lot pad elevations shall be a minimum of 1.2 feet above the 100-year HGL and a minimum of 1.5 feet above the local controlling overland flow release elevation, whichever is higher or as approved by DOU.

- B62. Prior to or concurrent with the submittal of improvement plans, a combined sanitary sewer study is required for the entire project and shall be approved by the DOU. The study shall provide an analysis of the pre and post development condition of both the sewer and drainage flow that is contributing to the combined system. The drainage study shall include an overland flow release map for the proposed project. If it is shown that the post development conditions cause an increase in flow to the combined system, either onsite storage and/or improvements to existing combined sanitary system will be required to the satisfaction of the DOU.
- B63. Per City Code, the Subdivider may not develop the project in any way that obstructs, impedes, or interferes with the natural flow of existing off-site drainage that crosses the property. The project shall construct the required public and/or private infrastructure to handle off-site runoff to the satisfaction of the DOU. If private infrastructure is constructed to handle off-site runoff, the applicant shall dedicate the required private easements and/or, at the discretion of the DOU, the applicant shall enter into and record an Agreement for Maintenance of Drainage with the City, in a form acceptable to the City Attorney. Sufficient off-site and on-site spot elevations shall be provided in the drainage study to determine the direction of storm drain runoff.
- B64. The applicant shall design and construct the private detention basin on Lot A to the satisfaction of the DOU and Parks and Recreation Department pursuant to City Standards. A separate set of improvement plans shall be prepared for the detention basin.
- B65. An as-built survey of the drainage basin is required prior to issuance of a notice of completion for the subdivision.
- B66. The applicant or HOA shall properly maintain the temporary detention basin to City standards. To ensure proper maintenance, the applicant shall enter into a maintenance agreement with the City which may require posting or depositing with the City of unconditional security for performance of the applicant's obligations, which security is adequate in the sole and exclusive discretion of the City, and which is in a form acceptable to the City Attorney.
- B67. A note stating the following shall be placed on the Final Map: "Where necessary private reciprocal easement for ingress/egress, utilities, drainage, water and sanitary sewer facilities, and surface storm drainage, shall be granted and reserved, as necessary and at no cost, at or before the time of sale or conveyance of any parcel shown in this map."
- B68. A grading plan showing existing and proposed elevations is required. Adjacent off-site topography shall also be shown to the extent necessary to determine

impacts to existing surface drainage paths. No grading shall occur until the grading plan has been reviewed and approved by the Department of Utilities.

- B69. The applicant must comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinance. This ordinance requires the applicant to show erosion and sediment control methods on the subdivision improvement plans. These plans shall also show the methods to control urban runoff pollution from the project site during construction.
- B70. Stormwater quality control measures shall be incorporated into the development to minimize the increase of urban runoff pollution caused by development in the area. Since the project is in the combined sewer area, only source controls are required. Storm drain public notice message is required at all drain inlets. Improvement plans must include the source controls measures selected for the site. Refer to the "Stormwater Quality Design Manual for the Sacramento and South Placer Regions".
- B71. All existing easements and all existing right-of-ways shall be shown on the Final Map.

### **Solid Waste Division**

- B72. Project must meet the requirements outlined in Sacramento City Code Chapter 17.72.
- B73. Solid waste trucks must be able to safely move about the property, with minimum backing, and be able to empty the bins and cans safely. When a solid waste truck has to make a turn, such as in and out of an alley, the truck must be able to make the turn safely around obstacles, such as tree wells and other planters. Please see the file "Truck, bin, can dimensions.xlsx" for the dimensions of a City solid waste truck, including turning radius, to confirm the truck can safely make all necessary turns.
- B74. All residences in this project are single family or condominium units, which are required by City Code Chapter 13.10 to be served by City of Sacramento Solid Waste Services.
- B75. Residents must place solid waste cans curbside on closest street. Cans cannot be placed in alleys.
- B76. All streets must be of standard public street construction, or a waiver must be submitted releasing the City of Sacramento of liability for normal wear-and-tear damage due to standard collection operations. This does not apply to alleys, as the City's solid waste vehicles will not be collected from alleys.

- B77. Homes facing a private alley or street are able to waive yard waste service, if desired. Homes facing a public street, however, may be required to have yard waste service. Or, the HOA must agree to maintain yard waste removal for all homes within the project.
- B78. HOA will provide a landscaping service for all common area greenery. Landscaper will then haul away yard waste.

### **Regional Transit**

- B79. The project shall include a bus stop on 5<sup>th</sup> Street, somewhere between Crate Avenue and Cleat Way, at the developer's discretion. The bus stop shall be on the far side (south side) of an intersection and requires a vertical curb and a reinforced concrete pad where the bus will be stopping. Please contact Robert Hendrix at (916) 649-2759 for design specifics and final approval on bus stop location.
- B80. When Phase 4 is under way, the sidewalk on Broadway shall be built to accommodate a bus stop, with a vertical curb and reinforced concrete pad.
- B81. Project construction shall not disrupt transit service or pedestrian access to transit stops/stations.
- B82. Pedestrian access to 5<sup>th</sup> Street shall not be obstructed by any fences, walls, or other barriers.
- B83. Transit information shall be displayed in a prominent location in the residential sales/rental office, through a homeowner's association, or with real estate transactions.

### **Advisory notes for the Special Permit:**

#### **Planning**

- 1. Final approved conditions of approval and project exhibits shall be included on full-size sheets as part of the building permit submittal. Additionally, an affidavit signed by the applicant that affirms the plans submitted for building permit comply with all conditions of approval and approved exhibits shall also be included.

#### **Fire Department**

- 2. An automatic fire sprinkler system shall be installed in any portion of a building when the floor area of the building exceeds 3,599 square feet. *Per the newly adopted 2010 California Residential Code, all new residential construction*

*including 1 and 2 family dwellings and townhouses will be required to be sprinklered effective January 1, 2011.*

#### Utilities Department

3. Many projects within the City of Sacramento require on-site booster pumps for fire suppression and domestic water system. Prior to design of the subject project, the Department of Utilities suggests that the applicant request a water supply test to determine what pressure and flows the surrounding public water distribution system can provide to the site. This information can then be used to assist the applicant's engineers in the design of the on-site domestic, irrigation and fire suppression systems.
4. Location of the wet utilities in the street without center medians shall be as follows, drainage mains shall be placed in the centerline of the street, water mains shall be placed north and west of the centerline and the sewer main shall be placed south and east of the centerline. The location of wet utilities in streets with medians shall be approved by DOU pursuant to City Standards prior to design.
5. The proposed project is located in the Flood zone designated as **Shaded X** zone on the Federal Emergency Management Agency (FEMA) Federal Insurance Rate Maps (FIRMs) that have been revised by a Letter of Map Revision effective February 18, 2005. Within the Shaded X zone, there are no requirements to elevate or flood proof.
6. The applicant is responsible for obtaining all necessary permits, easements and approvals from federal, state and local agencies, and private landowners for the construction of this project.

#### Regional Transit

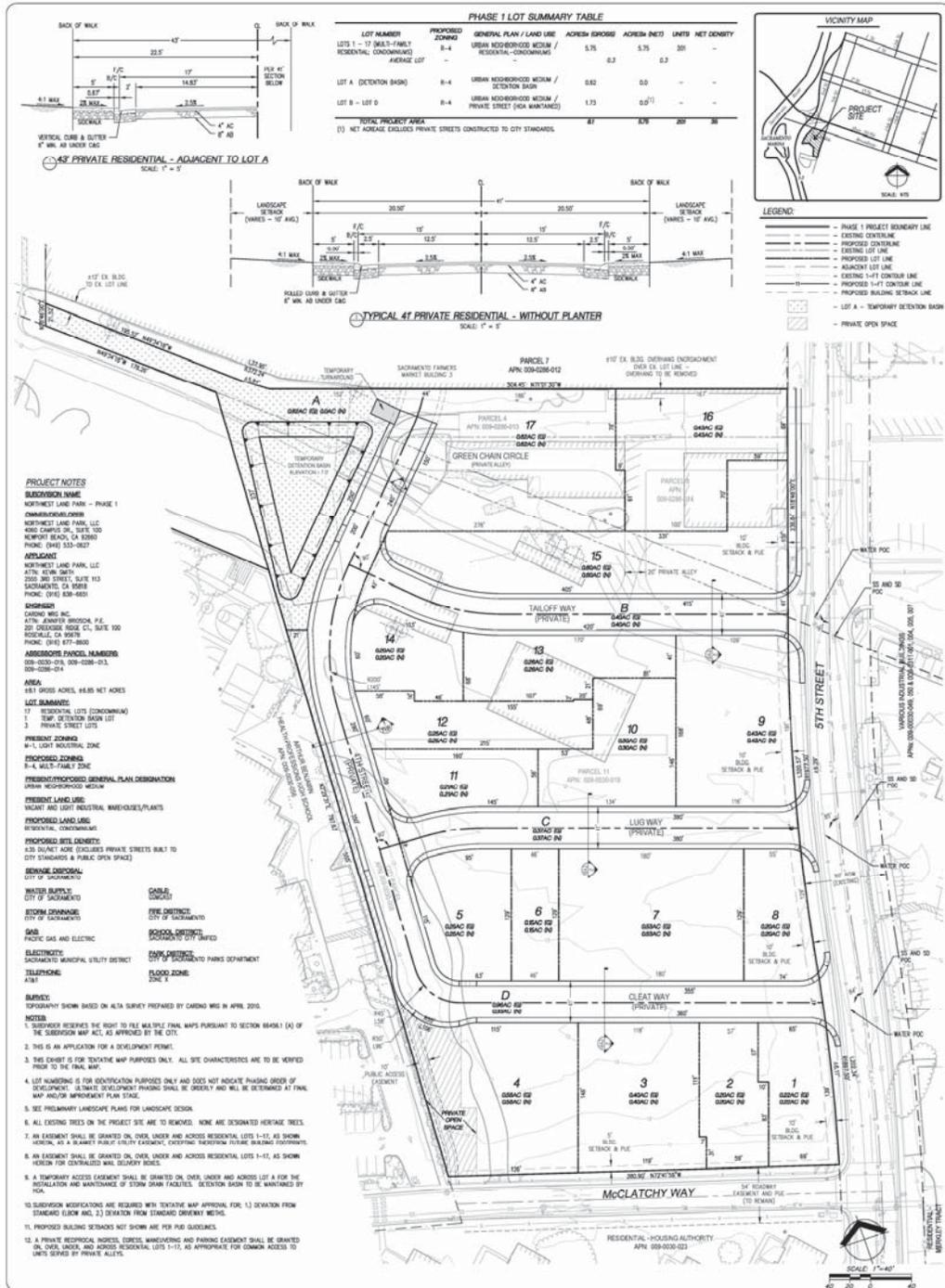
7. Develop a program to offer transit passes at a 50% or greater discount to new homeowners for a period of six months or more. Program shall be reviewed and approved by RT prior to approval of any special permit for the project.

#### **Table of Contents:**

Exhibit A: Phase 1 Tentative Map – 1 page  
Exhibit B: Phase 1 Existing Conditions Plan – 1 page  
Exhibit C: Phase 1 Special Permit Site Plan – 1 page  
Exhibit D: Phase 1 Planting Plan – 1 page  
Exhibit E: Phase 1 Product Type Key Map – 1 page  
Exhibit F: Phase 1 Circulation Plan – 1 page

Exhibit G: Interior Circulation Plan – Brownstones and City Homes – 1 page  
Exhibit H: Interior Circulation Plan – Urban Courts and Towns – 1 page  
Exhibit I: Interior Circulation Plan – Cottages and Towns – 1 page  
Exhibit J: Interior Circulation Plan – Bungalows – 1 page  
Exhibit K: Brownstones – 18 pages  
Exhibit L: City Homes – 11 pages  
Exhibit M: The Towns – 11 pages  
Exhibit N: Urban Courts – 25 pages  
Exhibit O: Cottages – 6 pages  
Exhibit P: Cottage Townhomes – 9 pages  
Exhibit Q: Bungalows – 13 pages

# Exhibit A: Phase 1 Tentative Map



**PROJECT NOTES**

**SUBDIVISION NAME**  
NORTHWEST LAND PARK - PHASE 1

**CONDOMINIUMS**  
NORTHWEST LAND PARK, LLC  
1000 COLLEGE BLVD., SUITE 100  
NORTH BEACH, CA 94948  
PHONE: (415) 333-0227

**ADJACENT**  
NORTHWEST LAND PARK, LLC  
4776 KENNEDY BLVD.  
2100 3RD STREET, SUITE 113  
SACRAMENTO, CA 95811  
PHONE: (916) 838-8851

**DESIGNER**  
CARDNO WRG INC.  
4176 JENNIFER WOODS PL.  
250 COLLEGE BLVD., SUITE 100  
NORTH BEACH, CA 94948  
PHONE: (415) 877-8800

**ADJACENT PARCEL NUMBERS**  
009-0038-016, 009-0038-013,  
009-0038-014

**AREA**  
48.5 ACRES, 48.5 NET ACRES

**LOT SUMMARY:**  
17 RESIDENTIAL LOTS (CONDOMINIUMS)  
1 TEMP. DETENTION BASIN LOT  
3 PRIVATE STREET LOTS

**PRESENT ZONING**  
R-4, L-1, L-2, L-3

**PROPOSED ZONING**  
R-4, M-1, L-1, L-2, L-3

**PROPOSED GENERAL PLAN DESIGNATION**  
URBAN NEIGHBORHOOD MEDIUM

**PRESENT LAND USE**  
MIXED USE RESIDENTIAL, WAREHOUSES, PLANTS

**PROPOSED LAND USE**  
RESIDENTIAL, CONDOMINIUMS

**PROPOSED SETBACKS**  
330' FRONT YARD (EXCLUDES PRIVATE STREETS BUILT TO CITY STANDARDS & PRIVATE OPEN SPACE)

**SEWERAGE/SANITATION**  
CITY OF SACRAMENTO

**WATER SUPPLY**  
CITY OF SACRAMENTO

**STORM DRAINAGE**  
CITY OF SACRAMENTO

**GAZ**  
PUEBLO GAS AND ELECTRIC

**ELECTRICITY**  
SACRAMENTO MUNICIPAL UTILITY DISTRICT

**TELEPHONE**  
AT&T

**BURNING**  
TOPOGRAPHY SHOWN BASED ON ALTA SURVEY PREPARED BY CARDNO WRG IN APRIL 2005.

**NOTES**

- SUBDIVISION RESERVES THE RIGHT TO FILE MAJOR FINAL MAPS PURSUANT TO SECTION 66461(A) OF THE SUBDIVISION MAP ACT, AS APPROVED BY THE CITY.
- THIS IS AN APPLICATION FOR A DEVELOPMENT PERMIT.
- THIS EXHIBIT IS FOR TENTATIVE MAP PURPOSES ONLY. ALL SITE CHARACTERISTICS ARE TO BE VERIFIED PRIOR TO THE FINAL MAP.
- LOT NUMBERING IS FOR IDENTIFICATION PURPOSES ONLY AND DOES NOT INDICATE PRIORITY ORDER OF DEVELOPMENT. SEPARATE DEVELOPMENT PERMITS SHALL BE OBTAINED AND WILL BE OBTAINED AT FINAL MAP AND/OR IMPROVEMENT PLAN STAGE.
- SEE PRELIMINARY LANDSCAPE PLANS FOR LANDSCAPE DESIGN.
- ALL EXISTING TREES ON THE PROJECT SITE ARE TO REMAIN. NONE ARE DESIGNATED HERITAGE TREES.
- AN EASEMENT SHALL BE GRANTED ON OVER, UNDER AND ACROSS RESIDENTIAL LOTS 1-17, AS SHOWN HEREON, AS A NEARBY PUBLIC UTILITY EASEMENT, EXCEPTING THEREFROM PUBLIC SEWERAGE TREATMENT PLANTS.
- AN EASEMENT SHALL BE GRANTED ON OVER, UNDER AND ACROSS RESIDENTIAL LOTS 1-17, AS SHOWN HEREON FOR CONVEYANCE AND UTILITY PURPOSES.
- A TEMPORARY ACCESS EASEMENT SHALL BE GRANTED ON OVER, UNDER AND ACROSS LOT A FOR THE INSTALLATION AND MAINTENANCE OF STORM DRAIN FACILITIES, DETENTION BASIN TO BE MAINTAINED BY PWA.
- SUBDIVISION MODIFICATIONS ARE REQUIRED WITH TENTATIVE MAP APPROVAL, FOR: 1.) DEVIATION FROM STANDARD (SUDM) AND 2.) DEVIATION FROM STANDARD DRIVEWAY WIDTHS.
- PROPOSED BUILDING SETBACKS NOT SHOWN ARE PER PWS GUIDELINES.
- A PRIVATE RESIDENTIAL, ACCESS, EGRESS, AMENITY AND PARKING EASEMENT SHALL BE GRANTED ON OVER, UNDER, AND ACROSS RESIDENTIAL LOTS 1-17, AS APPROPRIATE FOR COMMON ACCESS TO UNITS SERVED BY PRIVATE ALLEYS.

**Cardno WRG**  
SACRAMENTO  
300 OREGON STREET, SUITE 100, SACRAMENTO, CA 95811  
TEL: (916) 877-8800 FAX: (916) 877-8800  
WWW.CARDNOWRG.COM  
PLANNING + DESIGN + LANDSCAPE ARCHITECTURE + SURVEYING

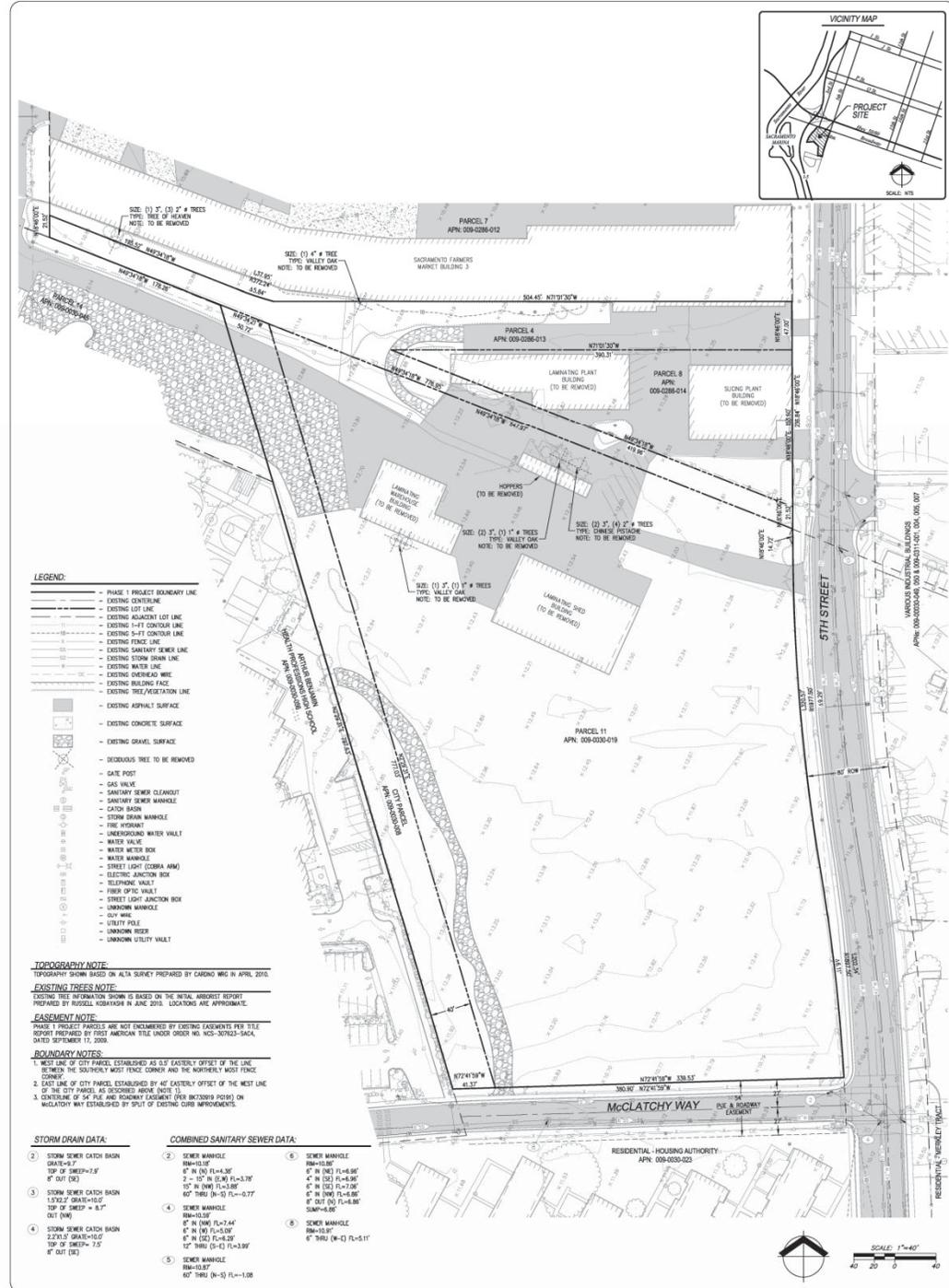
TENTATIVE SUBDIVISION MAP FOR CONDOMINIUM PURPOSES - PHASE 1  
**NORTHWEST LAND PARK**  
NORTHWEST LAND PARK, LLC  
SACRAMENTO, CALIFORNIA



PROJECT NO. 0400201  
DATE: 04/20/11  
DESIGNED BY: MHW/JP  
DRAWN BY: MHW  
CHECKED BY: JAW

TENTATIVE MAP  
PHASE 1

# Exhibit B: Phase 1 Existing Conditions Plan



**Cardno WRG**  
 SACRAMENTO  
 201 DUNDON HERBERTS TRL, SACRAMENTO, CA 95833  
 TEL: (916) 677-4800 FAX: (916) 677-4800  
 WWW.CARDNOWRG.COM  
 PLANNERS • ENGINEERS • LANDSCAPE ARCHITECTS • SURVEYORS

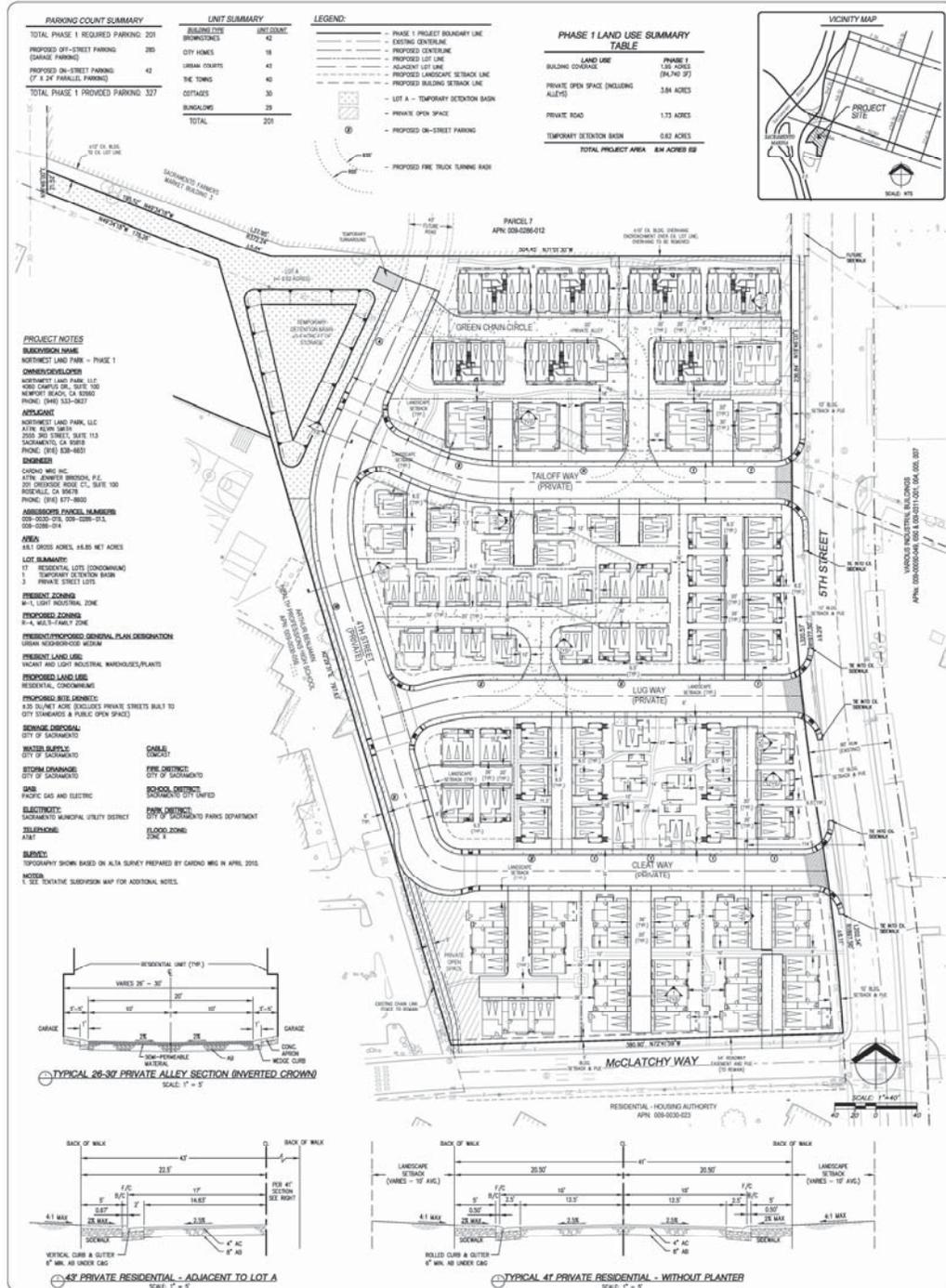
EXISTING CONDITIONS PLAN - PHASE 1  
**NORTHWEST LAND PARK**  
 NORTHWEST LAND PARK, LLC  
 SACRAMENTO, CALIFORNIA



PROJECT NO: 00000002  
 DATE: 08/12/18  
 DESIGNED BY: MCK/DMC  
 DRAWN BY: MCK  
 CHECKED BY: JMB

EXISTING CONDITIONS  
 PHASE 1

# Exhibit C: Phase 1 Special Permit Site Plan



**Cardno WRG**  
SACRAMENTO  
201 UNIVERSITY AVENUE, SUITE 100, SACRAMENTO, CA 95817  
TEL: (916) 577-8000 FAX: (916) 577-8000  
WWW.CARDNOWRG.COM  
PLANNERS - ENGINEERS - LANDSCAPE ARCHITECTS - SURVEYORS

SPECIAL PERMIT SITE PLAN - PHASE 1  
NORTHWEST LAND PARK  
NORTHWEST LAND PARK, LLC  
SACRAMENTO, CALIFORNIA

PROJECT NO: 00000001  
DATE: 08/20/21  
DESIGNED BY: JMS  
DRAWN BY: JMS  
CHECKED BY: JMS

SITE PLAN  
PHASE 1

# Exhibit D: Phase 1 Planting Plan



**Cardno WRG**  
SACRAMENTO  
201 DRENSIDE DRIVE, SUITE 100, ROSELAND, CA 95678  
TEL: (916) 877-4900 FAX: (916) 877-4903  
www.cardnowrg.com  
PLANNING - ENGINEERING - LANDSCAPE ARCHITECTURE - SURVEYING

**PHASE 1 PLANTING PLAN**  
NORTHWEST LAND PARK, LLC  
SACRAMENTO, CALIFORNIA

PROJECT NO: 00969002  
DATE: 06-16-11  
DESIGNED BY: WAW/JRS  
DRAWN BY: KLR  
CHECKED BY: JRS

# Exhibit E: Phase 1 Product Type Key Map



**Cardno**  
**WRG**  
SACRAMENTO  
201 CRENSHAW BLVD. SUITE 100, ROSSELLE, CA 95678  
TEL: (916) 677-8600 FAX: (916) 677-8607  
www.cardnowrg.com  
PLANNERS • DESIGNERS • LANDSCAPE ARCHITECTS • SURVEYORS

**PRODUCT TYPE KEY MAP**  
NORTHWEST LAND PARK, LLC  
SACRAMENTO, CALIFORNIA

PROJECT NO.: 538990000  
DATE: 04-15-11  
DESIGNED BY: 9090-JAB  
DRAWN BY: 11.02  
CHECKED BY: JAB

# Exhibit F: Phase 1 Circulation Plan



**Cardno**  
WRG  
SACRAMENTO  
301 O'REILLY BLVD, STE 100 SACRAMENTO, CA 95811  
TEL: (916) 677-2600 FAX: (916) 677-4600  
WWW.CARDNO.COM  
PLANNERS • ENGINEERS • LANDSCAPE ARCHITECTS • SURVEYORS

PHASE 1 CIRCULATION PLAN  
NORTHWEST LAND PARK, LLC  
SACRAMENTO, CALIFORNIA

PROJECT NO: 00000000  
DATE: 04-15-11  
DESIGNED BY: WORKLAB  
DRAWN BY: KLB  
CHECKED BY: JMB

# Exhibit G: Interior Circulation Plan – Brownstones and City Homes



  
**Cardno**  
**WRG**  
 SACRAMENTO  
 201 CRENSHAW AVENUE, STE. 100, ROSELAND, CA 95678  
 TEL: (916) 877-8800 FAX: (916) 877-8802  
 www.cardnowrg.com  
 PLANNERS • ENGINEERS • LANDSCAPE ARCHITECTS • SURVEYORS

**INTERIOR CIRCULATION PLAN -**  
**BROWNSTONES AND CITY HOMES**  
 NORTHWEST LAND PARK, LLC  
 SACRAMENTO, CALIFORNIA

PROJECT NO.: 008889002  
 DATE: 04-15-11  
 DESIGNED BY: MOK/AB  
 DRAWN BY: KLB  
 CHECKED BY: JIB

Exhibit H: Interior Circulation Plan – Urban Courts and Towns



**Cardno**  
**WRG**  
SACRAMENTO  
201 CRENSHAW HERBERT CT. SUITE 100 ROSELLE CA 95829  
TEL: (916) 677-8600 FAX: (916) 677-8602  
WWW.CARDNOWRG.COM  
PLANNERS • ENGINEERS • LANDSCAPE ARCHITECTS • SURVEYORS

INTERIOR CIRCULATION PLAN -  
URBAN COURTS AND TOWNS  
NORTHWEST LAND PARK, LLC  
SACRAMENTO, CALIFORNIA

PROJECT NO: 030808000  
DATE: 04-15-11  
DESIGNED BY: KJW/JJB  
DRAWN BY: KLS  
CHECKED BY: JJB

# Exhibit I: Interior Circulation Plan – Cottages and Towns



**Cardno**  
**WRG**  
SACRAMENTO  
201 CRENSHAW BLVD, SUITE 100, ROSELILLE, CA 95678  
TEL: (916) 677-8800 FAX: (916) 677-8820  
WWW.CARDNOWRG.COM  
PLANNERS • ENGINEERS • LANDSCAPE ARCHITECTS • SURVEYORS

INTERIOR CIRCULATION PLAN -  
COTTAGES & TOWNS  
NORTHWEST LAND PARK, LLC  
SACRAMENTO, CALIFORNIA

PROJECT NO.: 508993000  
DATE: 04-10-11  
DESIGNED BY: KRS/MLB  
DRAWN BY: KLS  
CHECKED BY: JMB

# Exhibit J: Interior Circulation Plan – Bungalows



  
**Cardno  
WRG**  
 SACRAMENTO  
 201 ORENDALE AVENUE, SUITE 100, SACRAMENTO, CA 95829  
 TEL: (916) 487-7400 FAX: (916) 487-7400  
 www.cardnowrg.com  
 PLANNING • ENGINEERING • LANDSCAPE ARCHITECTURE • SURVEYING

**INTERIOR CIRCULATION PLAN -  
 BUNGALOWS**  
 NORTHWEST LAND PARK, LLC  
 SACRAMENTO, CALIFORNIA

PROJECT NO.	200808000
DATE	04-15-11
DESIGNED BY	MMWJMB
DRAWN BY	FLB
CHECKED BY	JMB

- Date: July 12, 2010  
 Preliminary 2007 CBC Review  
 R-2 residential, Type V-A (one-hour) construction, NFPA 13 sprinklers
1. Occupancy
    - a. Residential R-2 (Section 310)
    - b. Garages (U) private garages grouped together (113 and 1103A.3.1)
  2. Maximum Stories/Height
    - a. Maximum height of a building will be 50'-20" (sprinkler increase, but not over 60' (Table 603 and 504.2).
    - b. Max. stories (Table 503 and 504.2): 4 stories
      - i. Mezzanines per 505.2 are part of story below
        1. Aggregate area does not exceed 1/3 of the floor area of that room in which it is located (505.2).
  3. Building Area
    - a. Maximum building area per floor:
      - i. A1 = 12,000 sf (Table 503)
      - ii. Assume no footage increase, if = 0
      - iii. Is = 2 in addition to story & height increase (506.3)
      - iv. Maximum area per floor:  $A_1 + Is = 12,000 + (12,000 \times 2) = 36,000$  sf
    - b. Maximum Building Area (506.4) =  $A_1 \times 2 = 24,000$  sf
  4. Buildings on the Same Lot
    - a. Two or more buildings on the same lot shall be considered as portions of the same building if the aggregate area of such buildings is within the limits specified in Chapter 5 for a single building (705.5, exception).
    - i. Total area building =  $\approx 2,000$  sf
    - ii. 12,000 of area = 12,000 building = 6 bldgs maximum on one lot without front setbacks (503.1.2)
    - iii.  $\approx 6$  bldgs maximum between imaginary lines
  5. Fire Walls
    - a. Since the actual area of the building per floor is less than 36,000 s.f. and the total building area is less than 72,000 s.f., there is NO requirement for fire walls (503.1).
  6. Fire Resistance of Building Elements (Table 601)
    - a. Structural Frame: 1 Hour
    - b. Bearing Walls, Part I & Ex 1
    - c. Non-bearing walls and partitions (Table 601 & 602):
      - i. Fire Separation X-30' 1
      - ii. Fire Separation X-30' 0
    - d. Floor: 1
    - e. Roof: 1 or heavy timber
  7. Fire Resistance Rating Requirements for Exterior Walls based on Fire Separation Distance (Table 602)
    - a. X-30' 1 Hour
    - b. X-30' 0

8. Projections (Including Balconies)
  - a. Maximum Projection: Max. 12 inches where openings prohibited (704.2.2)
  - b. Openings prohibited where X-0" (Table 704.8)
  - c. Projections allowed where X-2"
  - d. Combustible projections where openings are not permitted shall be 1-hour min (704.2.3)
9. Maximum Area of Exterior Openings (Table 704.8 & 704.8.1):
  - a. X-fire separation distance
 

1. 0'-0" X 3'	Not Permitted
2. 3'-0" X 6'	15%
3. 6'-0" X 10'	25%
4. 10'-0" X 16'	45%
10. Elevators
  - a. Enclosed lobby not required in non-high-rise with automatic sprinkler system (707.14.1, exception 4)
11. Unit/Occupancy Separations
  - a. Between Units
    - i. Walls and Supporting Structure: 1 hour fire partitions (703.3 and 708.4)
    - ii. Floors and Supporting Structures: 1 hour (711.3 and 711.4)
  - b. Residential to Private Garages per 406.1.4; (Table 508.3.3.d and 406.1.4)
12. Sprinklers: NFPA 13 per 503.3.1.1
13. Egress
  - a. Occupant Load:  $(4^{\text{th}} \text{ fl} \times 60) (200 = 1,767/200) \text{ sf} \text{ occupant} = 8.8$  occupant max
    - i. Decks not included per 1004.6, not used by persons in addition to occupants.
  - b. Egress Width: Table 1005.1
    - i. Stairways: 200 occupants (2 units x 5/2 stairs) = 1.76'
    - ii. 36" min (1008.1, exception 1)
    - iii. Corridors: 36" min (1017.2, exception 2)
      1. Occupant Load = 8.8
  - c. Accessible Means of Egress (1007.1)
    - i. This building includes covered units, see 14 below
    - ii. Accessible spaces shall be provided with not less than one accessible means of egress. Where more than one means of egress is required by 1015.1 or 1018.1 from any accessible space, each accessible portion of the space shall be served by accessible means of egress in at least the same number as required by 1015.1 or 1018.1.
      1. Each unit is an accessible space
      2. Maximum Occupant Load/Unit = 6.485
3. One exit is required from a space with occupant load < 10 (1015.1, Table 1015.1)
  4. One accessible means of egress required
  5. One accessible elevator (1007.2.1) and one accessible stair provided
- d. Stairs
  - i. Within Units
    1. Max. riser (1009.3.4) = 7.75"
    2. Min. tread = 9"
    3. Min. window tread width = 6"
  - ii. Outside units
    1. Max. riser (1009.3) = 7"
    2. Min. tread = 11"
  - iii. Accessible Stair
    1. Min. (1007.3.3 and 1005.11 and 1008.1, exception 1): 36" minimum
    2. Areas of Refuge: Enforcing Agency may reduce requirement to one area where occupant load on floor = 500 (1007.2.8 and 1007.5.1, exception)
    3. Max. Occupant Load/floor = 12,565
  - iv. Landings
    1. Doors opening onto landing shall not reduce landing to less than one-half required width (1009.4.2)
- e. Common Path of Egress Travel: 125' max with NFPA 13 fire sprinklers (1014.3 exception 4)
14. Accessibility within Building (site access, guest parking not in this review):
  - a. Units
    - i. All units are covered units (1102A.1.2 & 1102A.3.2)
    - ii. Flats, plans 1.1, 1.2, 2. Shall be accessible (1103A.1, 2)
    - iii. Townhouses, plans 3 & 4: Story served by elevator shall be accessible (1103A.3.2)
  - b. Garages: U-Private garages grouped together
  - c. Building Entry: Primary entrances and exterior exit doors on accessible routes shall be accessible (1117A.2)
    - i. Exit from non-accessible exit stair not on an accessible route is not required to be accessible
  - d. Interior Routes of Travel: Accessible route provided to all portions of building, coincide with route for public, connect building entrance to each unit, accessible elevator (1119A.1)
    - e. Elevators shall comply with 1124A.
    - f. Units shall comply with 1126A-1130A.
  15. Yards or Courts (1206)
    - a. 1206 applies only to coverings that provide required natural light or ventilation (1206.1 and IBC Commentary)
    - b. Yards or courts with openings that do not provide required natural light or ventilation do not need to comply with 1206 requirements
    - c. Space bounded on only 2 sides is a yard (202)
    - d. A story yard with required openings shall be 5' minimum width (3' min x 1-story over 2 = 3' + 2' x 7) (1206.2)
    - e. Yard minimum width shall be unobstructed from the ground to the sky (502)
    - f. Note: no required openings are located on sides of buildings.

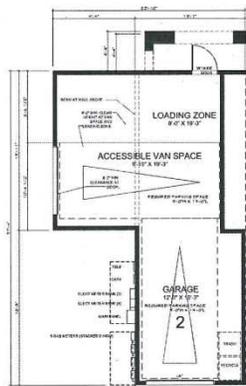


COMMON PATH OF EGRESS TRAVEL (CPET) DIAGRAMS

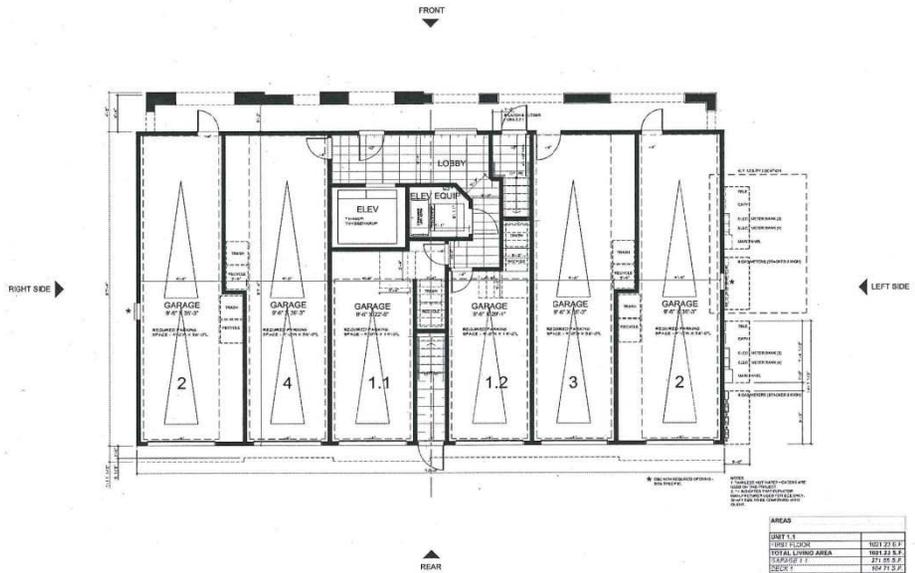
CODE ANALYSIS  
 NTS  
 NORTHWEST LAND PARK - BROWNSTONES  
 NORTHWEST LAND PARK, LLC  
 20100720



Exhibit K: Brownstones



PARTIAL PLAN AT  
ACCESSIBLE GARAGE: SITE SPECIFIC  
(SEE ADDITIONAL GARAGE S.F.)

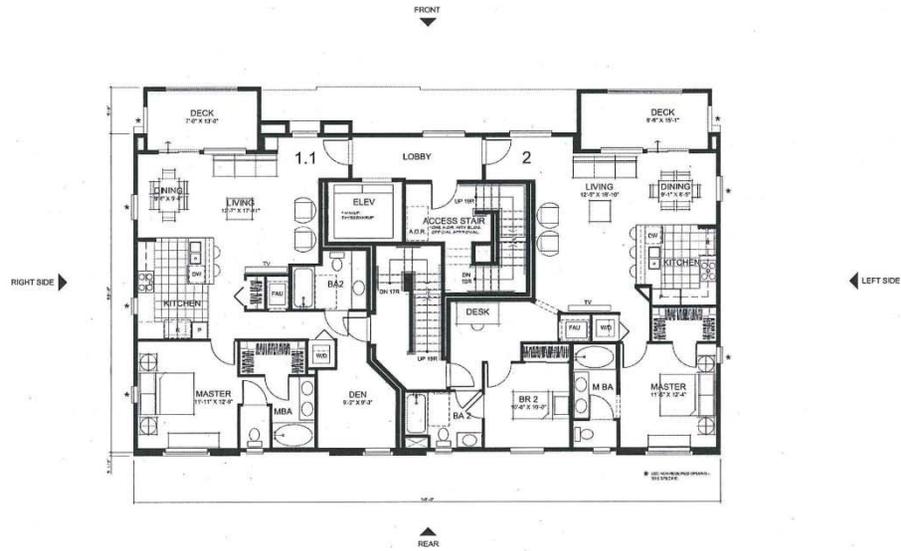


6-PLEX  
FIRST FLOOR BUILDING PLAN  
ELEVATION STYLE - 'A'

NORTHWEST LAND PARK - BROWNSTONES  
NORTHWEST LAND PARK, LLC



AREAS	
UNIT 1.1	1061.21 S.F.
FIRST FLOOR	1061.21 S.F.
TOTAL LIVING AREA	211.26 S.F.
CLIPPING F.	850.95 S.F.
DECK F.	3.00 S.F.
STAIR AREA	3.00 S.F.
UNIT 1.2	1061.21 S.F.
FIRST FLOOR	1061.21 S.F.
TOTAL LIVING AREA	211.26 S.F.
CLIPPING F.	208.95 S.F.
DECK F.	3.00 S.F.
STAIR AREA	3.00 S.F.
PLAN 2	1068.48 S.F.
FIRST LEVEL	1068.48 S.F.
TOTAL LIVING AREA	425.13 S.F.
CLIPPING F.	714.35 S.F.
DECK AREA	3.00 S.F.
PLAN 3	1041.76 S.F.
FIRST LEVEL	1041.76 S.F.
TOTAL LIVING AREA	425.13 S.F.
CLIPPING F.	477.63 S.F.
DECK F.	3.00 S.F.
STAIR AREA	3.00 S.F.
PLAN 4	1139.21 S.F.
FIRST LEVEL	1139.21 S.F.
TOTAL LIVING AREA	387.21 S.F.
CLIPPING F.	425.00 S.F.
DECK F.	3.00 S.F.
STAIR AREA	3.00 S.F.
ELEV. LOBBY STAIRS	409.27 S.F.
FIRST FLOOR FRONT	349.22 S.F.
FIRST FLOOR REAR	34.94 S.F.
SECOND FLOOR	626.08 S.F.
THIRD FLOOR	425.00 S.F.
FOURTH FLOOR	304.90 S.F.
FIFTH FLOOR	1.00 S.F.
TOTAL ELEV. LOBBY STAIRS	1044.17 S.F.



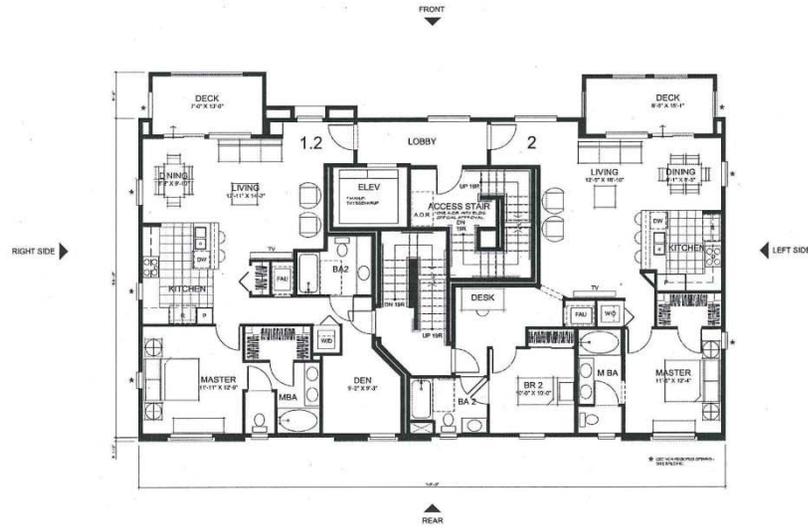
6-PLEX  
 SECOND FLOOR BUILDING PLAN  
 ELEVATION STYLE - 'A'

19'-0" x 30'-0"

**NORTHWEST LAND PARK - BROWNSTONES**  
 NORTHWEST LAND PARK, LLC



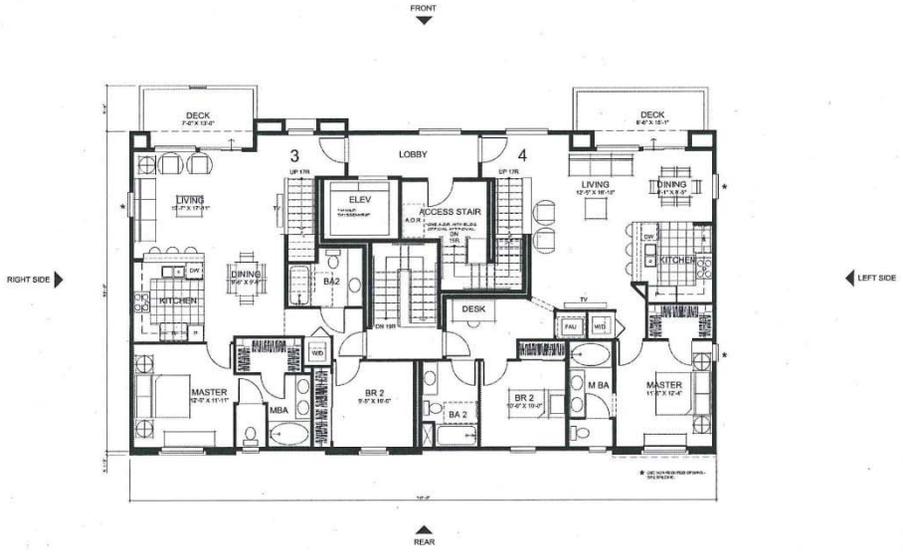
AREAS	
UNIT 1.1	
FIRST FLOOR	1011.21 S.F.
TOTAL LIVING AREA	1011.21 S.F.
CORRIDOR	271.15 S.F.
DECK	104.71 S.F.
Other area	1397.07 S.F.
UNIT 1.2	
FIRST FLOOR	1011.21 S.F.
TOTAL LIVING AREA	1011.21 S.F.
CORRIDOR	209.27 S.F.
DECK	104.71 S.F.
Other area	1284.99 S.F.
PLANT	
FIRST LEVEL	1008.42 S.F.
TOTAL LIVING AREA	1008.42 S.F.
CORRIDOR	435.17 S.F.
DECK	114.84 S.F.
Other area	1653.43 S.F.
PLANT	
FIRST LEVEL	1011.76 S.F.
TOTAL LIVING AREA	1011.76 S.F.
CORRIDOR	477.14 S.F.
DECK	104.42 S.F.
Other area	1793.32 S.F.
PLANT	
FIRST LEVEL	1133.51 S.F.
TOTAL LIVING AREA	1133.51 S.F.
CORRIDOR	188.56 S.F.
DECK	41.53 S.F.
Other area	1363.57 S.F.
ELEV. LOBBY STAIRS	
FIRST FLOOR FRONT	345.22 S.F.
FIRST FLOOR REAR	78.89 S.F.
SECOND FLOOR	430.89 S.F.
THIRD FLOOR	430.89 S.F.
FOURTH FLOOR	350.00 S.F.
FIFTH FLOOR	350.00 S.F.
TOTAL ELEV. LOBBY STAIRS	1544.17 S.F.



6-PLEX  
 THIRD FLOOR BUILDING PLAN  
 ELEVATION STYLE - 'A'  
 1/8"=1'-0"  
**NORTHWEST LAND PARK - BROWNSTONES**  
 NORTHWEST LAND PARK, LLC



AREAS	
<b>UNIT 1.2</b>	
FIRST FLOOR	1001.21 S.F.
TOTAL LIVING AREA	1001.21 S.F.
LARGEST F.F.	711.05 S.F.
DECK F.F.	854.71 S.F.
TOTAL AREA	1855.92 S.F.
<b>UNIT 2</b>	
FIRST FLOOR	1001.21 S.F.
TOTAL LIVING AREA	1001.21 S.F.
LARGEST F.F.	711.05 S.F.
DECK F.F.	854.71 S.F.
TOTAL AREA	1855.92 S.F.
<b>PLAN 1</b>	
FIRST LEVEL	1008.06 S.F.
TOTAL LIVING AREA	1008.06 S.F.
LARGEST F.F.	430.13 S.F.
DECK F.F.	114.84 S.F.
TOTAL AREA	1122.90 S.F.
<b>PLAN 2</b>	
FIRST LEVEL	1001.21 S.F.
TOTAL LIVING AREA	1001.21 S.F.
LARGEST F.F.	477.14 S.F.
DECK F.F.	403.07 S.F.
TOTAL AREA	1404.28 S.F.
<b>PLAN 3</b>	
FIRST LEVEL	1113.21 S.F.
TOTAL LIVING AREA	1113.21 S.F.
LARGEST F.F.	444.90 S.F.
DECK F.F.	587.31 S.F.
TOTAL AREA	1700.52 S.F.
<b>ELEV. LOBBY STAIRS</b>	
FIRST FLOOR	949.32 S.F.
FIRST FLOOR REAR	13.41 S.F.
SECOND FLOOR	428.88 S.F.
THIRD FLOOR	400.00 S.F.
FOURTH FLOOR	36.10 S.F.
FIFTH FLOOR	0.00 S.F.
TOTAL FLOOR, LOBBY STAIRS	1827.71 S.F.



6-PLEX  
 FOURTH FLOOR BUILDING PLAN  
 ELEVATION STYLE - 'A'

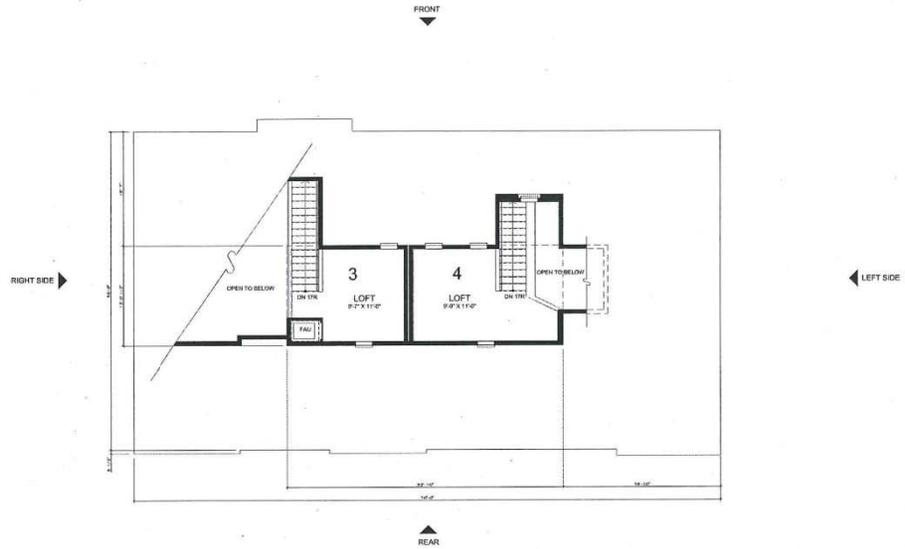
130'-0" W  
 130'-0" D

**NORTHWEST LAND PARK - BROWNSTONES**  
 NORTHWEST LAND PARK, LLC

2020-07-29



AREAS	
UNIT 1.1	
FIRST FLOOR	1001.23 S.F.
TOTAL LIVING AREA	1001.23 S.F.
DECK	211.00 S.F.
DECK 1	80.00 S.F.
DECK 2	131.00 S.F.
UNIT 1.2	
FIRST FLOOR	1001.23 S.F.
TOTAL LIVING AREA	1001.23 S.F.
DECK	211.00 S.F.
DECK 1	80.00 S.F.
DECK 2	131.00 S.F.
PLAN 2	
FIRST LEVEL	1001.23 S.F.
TOTAL LIVING AREA	1001.23 S.F.
DECK	211.00 S.F.
DECK 1	80.00 S.F.
DECK 2	131.00 S.F.
PLAN 3	
FIRST LEVEL	1001.23 S.F.
TOTAL LIVING AREA	1001.23 S.F.
DECK	211.00 S.F.
DECK 1	80.00 S.F.
DECK 2	131.00 S.F.
PLAN 4	
FIRST LEVEL	1117.71 S.F.
DECK	188.00 S.F.
TOTAL LIVING AREA	1305.71 S.F.
DECK	42.00 S.F.
DECK 1	80.00 S.F.
DECK 2	138.00 S.F.
PLAN 5	
FIRST LEVEL	1117.71 S.F.
DECK	188.00 S.F.
TOTAL LIVING AREA	1305.71 S.F.
DECK	42.00 S.F.
DECK 1	80.00 S.F.
DECK 2	138.00 S.F.
ELEV. LOBBY STAIRS	
FIRST FLOOR FRONT	242.22 S.F.
FIRST FLOOR REAR	75.88 S.F.
SECOND FLOOR	420.88 S.F.
THIRD FLOOR	420.88 S.F.
FOURTH FLOOR	1305.71 S.F.
FIFTH FLOOR	0.00 S.F.
TOTAL ELEV. LOBBY STAIRS	1544.17 S.F.

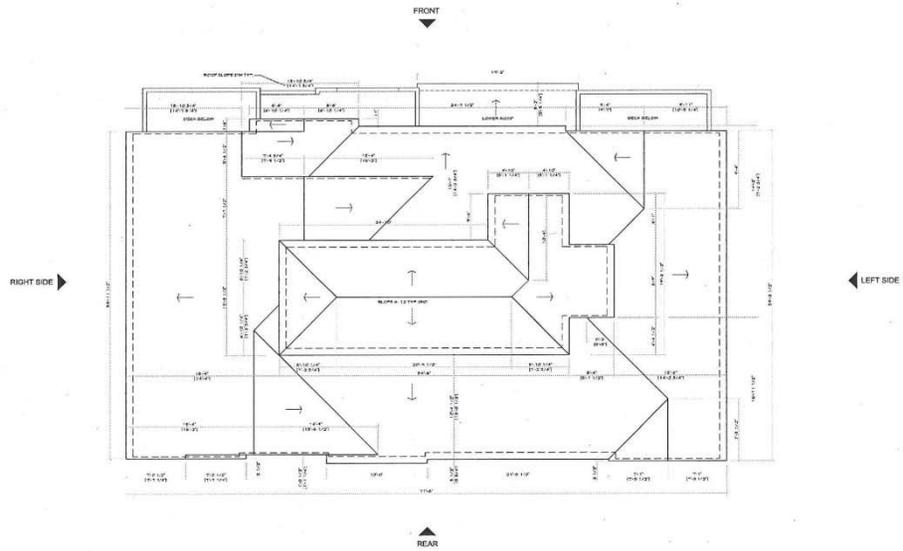


6-PLEX  
LOFT LEVEL BUILDING PLAN  
ELEVATION STYLE - 'A'

1/4"=1'-0"  
NORTHWEST LAND PARK - BROWNSTONES  
NORTHWEST LAND PARK, LLC



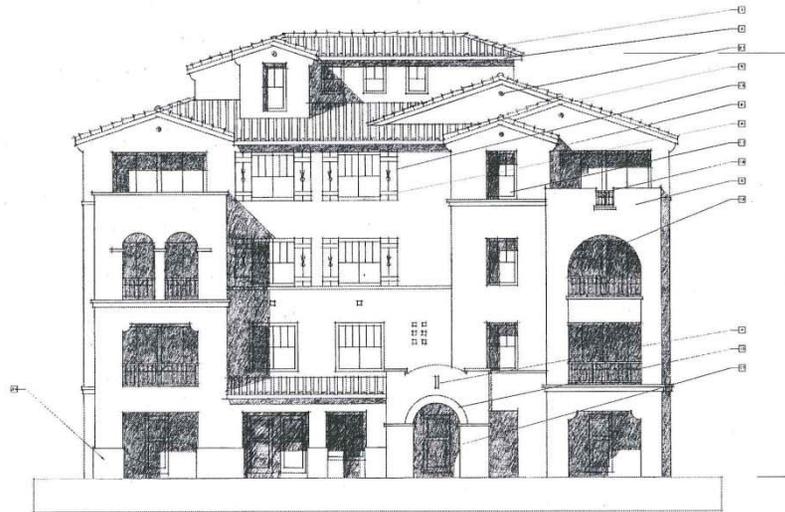
AREAS	
UNIT 1.1	
FIRST FLOOR	1021.23 S.F.
TOTAL LIVING AREA	1021.23 S.F.
CLIMATE S.F.	274.65 S.F.
DECK S.F.	0.00 S.F.
LOFT AREA	1337.48 S.F.
UNIT 1.2	
FIRST FLOOR	1021.23 S.F.
TOTAL LIVING AREA	1021.23 S.F.
CLIMATE S.F.	274.65 S.F.
DECK S.F.	0.00 S.F.
LOFT AREA	1337.48 S.F.
PLAN 2	
FIRST LEVEL	1021.23 S.F.
TOTAL LIVING AREA	1021.23 S.F.
CLIMATE S.F.	274.65 S.F.
DECK S.F.	0.00 S.F.
LOFT AREA	1337.48 S.F.
PLAN 3	
FIRST LEVEL	1021.23 S.F.
TOTAL LIVING AREA	1021.23 S.F.
CLIMATE S.F.	274.65 S.F.
DECK S.F.	0.00 S.F.
LOFT AREA	1337.48 S.F.
PLAN 4	
FIRST LEVEL	1021.23 S.F.
TOTAL LIVING AREA	1021.23 S.F.
CLIMATE S.F.	274.65 S.F.
DECK S.F.	0.00 S.F.
LOFT AREA	1337.48 S.F.
ELEV. LOBBY STAIRS	
FIRST FLOOR FRONT	340.22 S.F.
FIRST FLOOR REAR	32.81 S.F.
SECOND FLOOR	420.88 S.F.
THIRD FLOOR	420.88 S.F.
FOURTH FLOOR	330.88 S.F.
FIFTH FLOOR	330.88 S.F.
TOTAL ELEV. LOBBY STAIRS	1444.11 S.F.



6-PLEX  
 ROOF PLAN  
 ELEVATION STYLE - 'A'

1/4" = 1'-0"  
 NORTHWEST LAND PARK - BROWNSTONES  
 NORTHWEST LAND PARK, LLC





FRONT ELEVATION

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

10'-0"

NORTHWEST LAND PARK - BROWNSTONES  
NORTHWEST LAND PARK, LLC

2006/07/20



01.31.11

TYPICAL BUILDING MATERIALS

- INTERNAL COLOR 9" TILE ROOF
  - INTERNAL COLOR FLAT TILE ROOF
  - INTERNAL COLOR PLASTER
  - 2 X 4 FASCIA
  - 2 X 4 BRACKETS
  - PLASTER CORNICE
  - PLASTER COLUMN
  - PLASTER WINDOW TRIM
  - DECORATIVE PLASTER PROJECTION
  - PLASTER TRIM
  - VINYL WINDOW
  - HORIZONTAL SIDING
  - DECORATIVE SHUTTER
  - BRICK PATTERN VENEER
  - RAIL / GUARDRAIL
  - METAL RAIL / GUARDRAIL
  - FRIGGLASS ENTRY DOOR
  - SLIDING GLASS DOOR
  - METAL SECTIONAL GARAGE DOOR
  - STORAGE CLOSET
  - DECORATIVE ACCENT TILE
  - DECORATIVE VENT
  - HEAVY TEXTURE PLASTER
  - PLASTER FINISHT
  - EXPANSION JOINT
- \* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



LEFT SIDE ELEVATION  
SIMILAR FOR STYLES 'D' AND 'E'

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

1/8"=1'-0"

NORTHWEST LAND PARK - BROWNSTONES  
NORTHWEST LAND PARK, LLC

2020/07/28



00 01 11

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR G/TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR FLAT TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	3 X PASCIA
<input type="checkbox"/>	J/FRAME BOARD
<input type="checkbox"/>	PLASTER CORBEL
<input type="checkbox"/>	PLASTER COLUMN
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	HORIZONTAL SIDING
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	BRICK PATTERN VENER
<input type="checkbox"/>	RAIL/GUARDRAIL
<input type="checkbox"/>	METAL RAIL/GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	METAL REGIONAL GARAGE DOOR
<input type="checkbox"/>	STORAGE CLOSET
<input type="checkbox"/>	DECORATIVE ACCENT TILE
<input type="checkbox"/>	DECORATIVE VENT
<input type="checkbox"/>	HEAVY TEXTURE PLASTER
<input type="checkbox"/>	PLASTER HANDICUT
<input type="checkbox"/>	EXPANSION JOINT

\* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



REAR ELEVATION

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

1/8"=1'-0"

NORTHWEST LAND PARK - BROWNSTONES  
NORTHWEST LAND PARK, LLC

2018/07/20

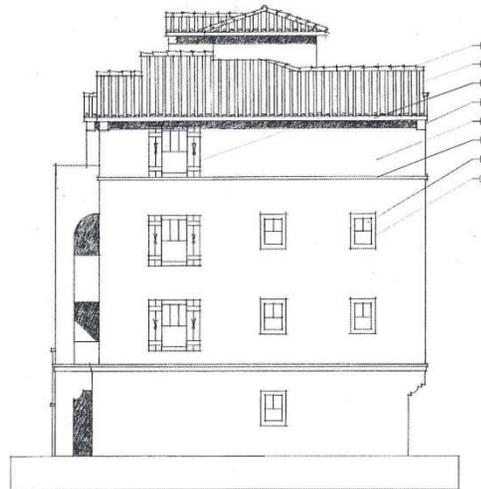


01.21.18

TYPICAL BUILDING MATERIALS

- ☐ INTERNAL COLOR 9" TILE ROOF
- ☐ INTERNAL COLOR FLAT TILE ROOF
- ☐ INTERNAL COLOR PLASTER
- ☐ 2" X FASCIA
- ☐ 2" X RAFTER BOARD
- ☐ PLASTER CORBEL
- ☐ PLASTER COLUMN
- ☐ PLASTER WINDOW TRIM
- ☐ DECORATIVE PLASTER PROJECTION
- ☐ PLASTER TRIM
- ☐ VINYL WINDOW
- ☐ HORIZONTAL SIDING
- ☐ DECORATIVE SHUTTER
- ☐ BRICK PATTERN VENEER
- ☐ RAIL / GUARDRAIL
- ☐ METAL RAIL / GUARDRAIL
- ☐ FIBERGLASS ENTRY DOOR
- ☐ SLIDING GLASS DOOR
- ☐ METAL SECTIONAL GARAGE DOOR
- ☐ STORAGE CLOSET
- ☐ DECORATIVE ACCENT TILE
- ☐ DECORATIVE VENT
- ☐ HEAVY TEXTURE PLASTER
- ☐ PLASTER HANGING
- ☐ EXPANSION JOINT

\* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



RIGHT SIDE ELEVATION  
SIMILAR FOR STYLES 'D' AND 'E'

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

1/8"=1'-0"

NORTHWEST LAND PARK - BROWNSTONES  
NORTHWEST LAND PARK, LLC

2016/04/20

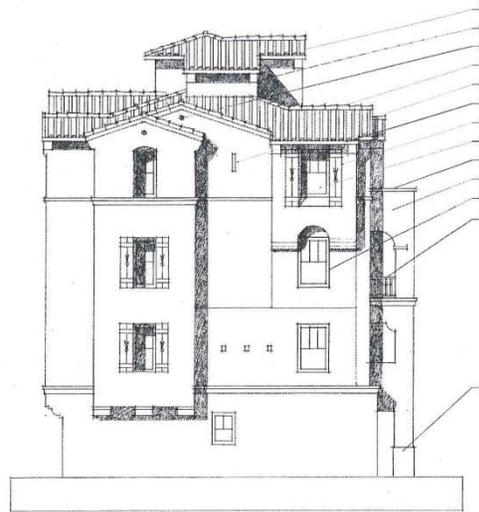


03.21.13

TYPICAL BUILDING MATERIALS

- INTERNAL COLOR 9" TILE ROOF
- INTERNAL COLOR FLAT TILE ROOF
- INTERNAL COLOR PLASTER
- 3" X 6" FASCIA
- 3/4" BARE BOARD
- PLASTER CORNELL
- PLASTER GOURN
- PLASTER WINDOW TRIM
- DECORATIVE PLASTER PROJECTION
- PLASTER TRIM
- VINYL WINDOW
- HORIZONTAL SIBING
- DECORATIVE SHUTTER
- BRICK PATTERN VENERA
- RAIL / GUARDRAIL
- METAL RAIL / GUARDRAIL
- FIBERGLASS ENTRY DOOR
- SLIDING GLASS DOOR
- METAL SECTIONAL GARAGE DOOR
- STORAGE CLOSET
- DECORATIVE ACCENT TILE
- DECORATIVE VENT
- HEAVY TEXTURE PLASTER
- PLASTER FINISHT
- EXPANSION JOINT

\* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



ENHANCED LEFT SIDE ELEVATION

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

1/4" = 1'-0"

NORTHWEST LAND PARK - BROWNSTONES  
NORTHWEST LAND PARK, LLC

2/28/2023



03.21.23

TYPICAL BUILDING MATERIALS

- ☐ INTERNAL COLOR S' TILE ROOF
  - ☐ INTERNAL COLOR FLAT TILE ROOF
  - ☐ INTERNAL COLOR PLASTER
  - ☐ 3 X FASCIA
  - ☐ JEWELER BOARD
  - ☐ PLASTER CORBEL
  - ☐ PLASTER COLUMN
  - ☐ PLASTER WINDOW TRIM
  - ☐ DECORATIVE PLASTER PROJECTION
  - ☐ PLASTER TRIM
  - ☐ VINYL WINDOW
  - ☐ HORIZONTAL SIDING
  - ☐ DECORATIVE SHUTTER
  - ☐ BRICK PATTERN VENEER
  - ☐ RAIL / GUARDRAIL
  - ☐ METAL RAIL / GUARDRAIL
  - ☐ FIBERGLASS ENTRY DOOR
  - ☐ SLIDING GLASS DOOR
  - ☐ METAL REGIONAL GARAGE DOOR
  - ☐ STORAGE CLOSET
  - ☐ DECORATIVE ACCENT TILE
  - ☐ DECORATIVE VENT
  - ☐ HEAVY TEXTURE PLASTER
  - ☐ PLASTER HANGOUT
  - ☐ EXPANSION JOINT
- \* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



FRONT ELEVATION  
SEE STYLE 'A' FOR LEFT AND RIGHT ELEVATIONS

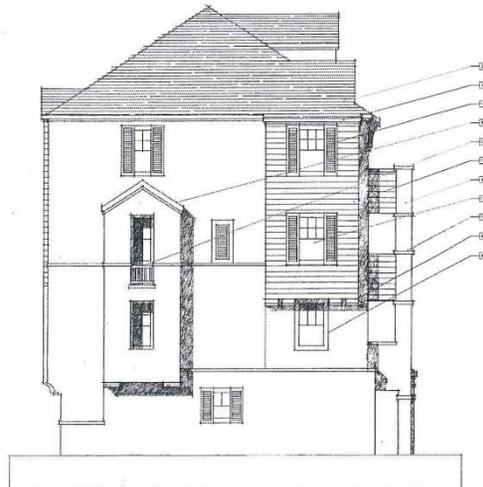
ALL FINISHES TO BE PAINTED  
UNLESS OTHERWISE NOTED  
ALL METAL AND JOIST ELEMENTS TO BE PAINTED

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'D'

1/8"=1'-0"  
NORTHWEST LAND PARK - BROWNSTONES  
NORTHWEST LAND PARK, LLC



TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR 8" TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR FLAT TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	3" FASCIA
<input type="checkbox"/>	SHAKE BOARD
<input type="checkbox"/>	PLASTER GORBEL
<input type="checkbox"/>	PLASTER GORBEL
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	HORIZONTAL SIDING
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	BRICK PATTERN VENER
<input type="checkbox"/>	RAIL / GUARDRAIL
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FRIGGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	METAL REGIONAL GARAGE DOOR
<input type="checkbox"/>	SPRINKLE CLOSET
<input type="checkbox"/>	DECORATIVE ACCENT TILE
<input type="checkbox"/>	DECORATIVE VENT
<input type="checkbox"/>	HEAVY TEXTURE PLASTER
<input type="checkbox"/>	PLASTER HANDCOT
<input type="checkbox"/>	EXPANSION JOINT



ENHANCED LEFT SIDE ELEVATION

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'D'

1/8"=1'-0"

NORTHWEST LAND PARK - BROWNSTONES  
NORTHWEST LAND PARK, LLC

202006030



02.11.17

TYPICAL BUILDING MATERIALS

- INTERNAL COLOR 9 TILE ROOF
- INTERNAL COLOR FLAT TILE ROOF
- INTERNAL COLOR PLASTER
- 3" FASCIA
- GARAGE BOARD
- PLASTER CORNICE
- PLASTER COLUMN
- PLASTER WINDOW TRIM
- DECORATIVE PLASTER PROJECTION
- PLASTER TRIM
- VINYL WINDOW
- HORIZONTAL BOND
- DECORATIVE SHUTTER
- BRICK PATTERN VENER
- RAIL / GUARDRAIL
- METAL RAIL / GUARDRAIL
- FIBROGLASS ENTRY DOOR
- BLEND GLASS DOOR
- METAL SECTIONAL GARAGE DOOR
- UPHOLSTER CLOSET
- DECORATIVE ACCENT TILE
- DECORATIVE VENT
- HEAVY TEXTURE PLASTER
- PLASTER HANDICUT
- EXPANSION JOINT

\* ALL METAL AND JOINT ELEMENTS TO BE PAINTED



REAR ELEVATION

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 17

18'-0" P  
NORTHWEST LAND PARK - BROWNSTONES  
NORTHWEST LAND PARK, LLC



01.23.21

TYPICAL BUILDING MATERIALS

- INTERNAL COLOR 9' FLE ROOF
- INTERNAL COLOR FLAT TILE ROOF
- INTERNAL COLOR PLASTER
- 2 # FASGA
- 2x4x8x8 BOARD
- PLASTER CORNER
- PLASTER CORNER
- PLASTER WINDOW TRIM
- DECORATIVE PLASTER PROJECTION
- PLASTER TRIM
- VINYL WINDOW
- HORIZONTAL Siding
- DECORATIVE SHUTTER
- BRICK PATTERN VENEER
- WALK / SIDEWALK
- METAL RAIL / GUARDRAIL
- FIBERGLASS ENTRY DOOR
- SLIDING GLASS DOOR
- METAL REGIONAL GARAGE DOOR
- STORAGE CLOSET
- DECORATIVE ACCENT TILE
- DECORATIVE VENT
- HEAVY TEXTURE PLASTER
- PLASTER PARADEOT
- EXPANSION JOINT

\* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



FRONT ELEVATION  
SEE STYLE 'A' FOR LEFT AND RIGHT ELEVATIONS

ALL METAL AND WOOD ELEMENTS TO BE PAINTED

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'E'

NORTHWEST LAND PARK - BROWNSTONES  
NORTHWEST LAND PARK, LLC



TYPICAL BUILDING MATERIALS

- ☐ INTEGRAL COLOR 9' TILE ROOF
- ☐ INTEGRAL COLOR FLAT TILE ROOF
- ☐ INTEGRAL COLOR PLASTER
- ☐ 2 # FASCIA
- ☐ 2x4 FRAME BOARD
- ☐ PLASTER CORNICE
- ☐ PLASTER COLUMN
- ☐ PLASTER WINDOW TRIM
- ☐ DECORATIVE PLASTER PROJECTION
- ☐ PLASTER TRIM
- ☐ VINYL WINDOW
- ☐ HORIZONTAL SIDING
- ☐ DECORATIVE SHUTTER
- ☐ BRICK PATTERN VENEER
- ☐ RAIL / SHINGLES
- ☐ METAL RAIL / SHINGLES
- ☐ FIBERGLASS ENTRY DOOR
- ☐ SECOND GLASS DOOR
- ☐ METAL REGIONAL GARAGE DOOR
- ☐ STORAGE CLOSET
- ☐ DECORATIVE ACCENT TILE
- ☐ DECORATIVE VENT
- ☐ HEAVY TEXTURE PLASTER
- ☐ PLASTER PARADIGM
- ☐ EXPANSION JOINT

\* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



ENHANCED RIGHT SIDE ELEVATION

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'E'

1/4"=1'-0"  
NORTHWEST LAND PARK - BROWNSTONES  
NORTHWEST LAND PARK, LLC



02.10.11

TYPICAL BUILDING MATERIALS

- INTERNAL COLOR 9" TILE ROOF
- INTERNAL COLOR FLAT TILE ROOF
- INTERNAL COLOR PLASTER
- 2 X FASCIA
- JOIST END BOARD
- PLASTER CORBEL
- PLASTER COLUMN
- PLASTER WINDOW TRIM
- DECORATIVE PLASTER PROJECTION
- PLASTER TRIM
- VINYL WINDOW
- HORIZONTAL Siding
- DECORATIVE SHUTTER
- BRICK PATTERN VENER
- RAIL / GUARDRAIL
- METAL RAIL / GUARDRAIL
- FIBERGLASS ENTRY DOOR
- SLIDING GLASS DOOR
- METAL REGIONAL GARAGE DOOR
- STORAGE CLOSET
- DECORATIVE AGENT TILE
- DECORATIVE VENT
- HEAVY TEXTURE PLASTER
- PLASTER FINISHER
- EXPANSION JOINT

\* ALL METAL AND JOIST ELEMENTS TO BE PAINTED



REAR ELEVATION

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'E'

1/2" = 1'-0"

NORTHWEST LAND PARK - BROWNSTONES  
NORTHWEST LAND PARK, LLC

20080923



01.31.11

TYPICAL BUILDING MATERIALS

- ☐ INTERNAL COLOR V-TILE ROOF
- ☐ INTERNAL COLOR FLAT TILE ROOF
- ☐ INTERNAL COLOR PLASTER
- ☐ 2 X FASCIA
- ☐ 2X SHIM BOARD
- ☐ PLASTER CORNICE
- ☐ PLASTER GOURN
- ☐ PLASTER RIBBON TRIM
- ☐ DECORATIVE PLASTER PROJECTION
- ☐ PLASTER TRIM
- ☐ VENT, WINDOW
- ☐ HORIZONTAL BOARD
- ☐ DECORATIVE SHUTTER
- ☐ BRICK PATTERN VENEER
- ☐ RAIL/GUARDRAIL
- ☐ METAL RAIL/GUARDRAIL
- ☐ FIBERGLASS ENTRY DOOR
- ☐ SLIDING GLASS DOOR
- ☐ METAL SECTIONAL GARAGE DOOR
- ☐ STORAGE CLOSET
- ☐ DECORATIVE ACCENT TILE
- ☐ DECORATIVE VENT
- ☐ HEAVY TEXTURE PLASTER
- ☐ PLASTER HANGCOF
- ☐ EXPANSION JOINT

\* ALL METAL AND WOOD ELEMENTS TO BE PAINTED

Date: July 12, 2010

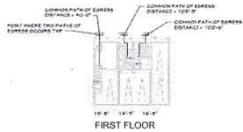
Preliminary 2007 CBC Review  
R-2 residential, type V-A (one-hour) construction, NFPA 13 sprinklers

1. Occupancy
  - a. Residential R-2 (Section 310)
  - b. Garages, L1 private garages grouped together (312 and 1102A.2.1)
2. Maximum Storey Height
  - a. Maximum height of the building will be 60'-0" sprinkler increase, but not over 60' (Table 502 and 504.2)
  - b. Max stories (Table 502 and 504.2): 4 stories
3. Building Area
  - a. Maximum building area per
    - i.  $A_1 = 12,000$  sf (Table 503)
    - ii. Assume no heritage structures,  $R = 0$
    - iii.  $A_2 = 2$  in addition to story & height increase (506.3)
    - iv. Maximum area per floor =  $12,000 \times (2 \times 2) = 36,000$  sf
  - b. Maximum Building Area (506.4) -  $A_1 \times 2 = 72,000$  sf
4. Buildings on the Same Lot
  - a. Two or more buildings on the same lot shall be considered as portions of the same building if the aggregate area of such buildings is within the limits specified in Chapter A for a single building (104.3, exception).
  - i. Total area building =  $4 \times 72,000$  sf
  - ii. 72,000 sf max = 72,000 sf (104.3, exception) - 10 bldgs maximum on one lot without firewalls (Section 603.1.2)
  - iii. = 30 bldgs maximum between contiguous lines (702)
5. Fire Walls
  - a. Since the actual area of the building per floor is less than 36,000 sf, and the total building area is less than 72,000 sf., there is NO requirement for fire walls (603.1).
6. Fire Resistance of Building Elements (Table 601)
  - a. Structural Frame: 1
  - b. Bearing Walls, Int. & Ext.: 1
  - c. Non-bearing walls and partitions (Table 601 & 602):
    - i. Fire Separation X=0: 1

- ii. Fire Separation X=30': 0
- d. Floors: 1
- e. Roof: 1 or heavy timber
5. Fire Resistance Rating Requirements for Exterior Walls based on Fire Separation Distance (Table 602)
  - a. X=0: 1 hour
  - b. X=30': 0
7. Projections
  - a. Maximum Projection: Max 12 inches where openings prohibited (704.2.2)
  - b. Openings prohibited where X=0' (Table 704.8)
  - c. Projections allowed where X=2'
  - d. Combustible projections where openings are not permitted shall be 1-hour riser (704.2.3)
8. Maximum Area of Exterior Openings (Table 704.8 & 704.8.1):
  - a. X-fire separation distance:
 

1. $0 < X <= 3'$	Not Permitted
2. $3' < X <= 6'$	15%
3. $6' < X <= 10'$	25%
4. $10' < X <= 15'$	45%
5. $15' < X <= 20'$	75%
9. Unit/Occupancy Separations
  - a. Between Units
    - i. Walls and Supporting Structure: 1 hour fire partitions (708.3 and 708.4)
    - ii. Floors and Supporting Structures: 1 hour (711.3 and 711.4)
  - b. Residential to Private Garages per 406.1.4; (Table 508.3.3.d and 406.1.4)
10. Sprinklers: NFPA 13 per 903.3.1.1
11. Egress
  - a. Each unit exits down private stair, to ground level and out its own private front door, per 1015.1, when:
    - i. Occupant Load = 10
    1. Occupant load factor = 200 sf (Table 1004.1.1)
    2. No unit >200 sf, no unit occupant load >10
  - ii. Common Path of Egress Travel: 120' (1014.3, Exception 4)
  1. see Common Path of Egress Travel diagrams
- b. Egress Width: Table 1005.1
  - i. Stairways: 2/occupants & 9 occupants max = 2'
    1. 36" min (Section 1009.1, exception 1)
  - ii. Corridors: 36" min (1017.2, exception 2)
    1. Occupant Load <50
- c. Accessible Means of Egress (1007.1)
  - i. Not required, no covered units. See 13 below

- d. Stairs:
  - i. Within units
    1. Max. riser (1009.3.4) = 7.75"
    2. Min. tread=10"
    3. Min. winder tread width=6"
  - ii. Landings
    1. Doors opening onto landing shall not reduce landing to less than one-half required width (1009.4.2).
- a. Common Path of Egress Travel: 125' max with NFPA 13 fire sprinklers (1014.3, exception 4).
13. Access within Building (site access, guest parking not in this review)
  - a. Exempt
    - i. All units are multi-story units in a non-stlevator building, which are exempt (1102A.3.1)
    - ii. Requirement in multi-story dwellings for 10% "visible" units is provided in Plan 1 visible unit alternate-see floor plans (1102A.3.1)
    - iii. 10% visible units per 1102A.3.1 are not covered units, no accessible guest or resident parking is required (1109A.1)
14. Yards or Courts 1206
  - a. 1206 applies only to openings that provide required natural light or ventilation 1206.1 and IBC Commentary)
  - b. Yards with openings that do not provide required natural light or ventilation do not need to comply with 1206 requirements
  - c. Space bounded on only 2 sides is a yard (202)
  - d. A story yards with required openings shall be 6' minimum width (5' min = 1 story over 2 = 3' + 2' x 1) (1005.2)
  - e. Yard minimum width shall be unobstructed from the ground to the sky (202)
  - f. Note no required openings are located on sides of buildings.



COMMON PATH OF EGRESS TRAVEL (CPET) DIAGRAMS

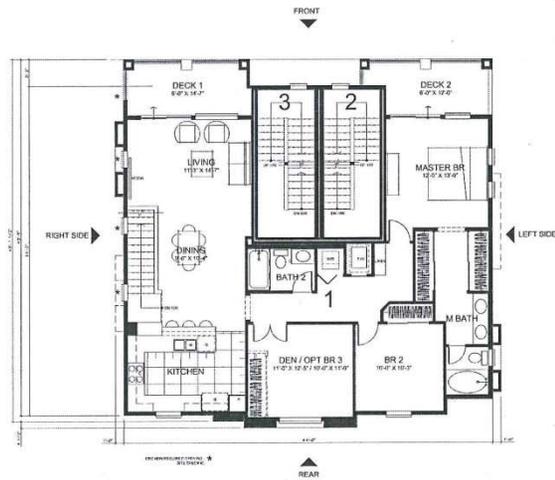
CODE ANALYSIS

NYS

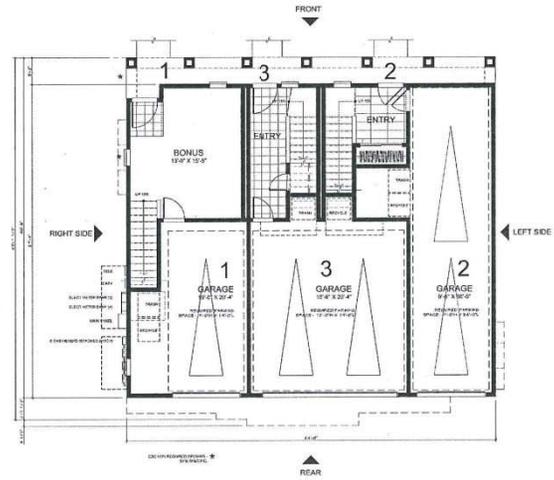
NORTHWEST LAND PARK - CITY HOMES  
NORTHWEST LAND PARK, LLC

20100909





SECOND FLOOR PLAN



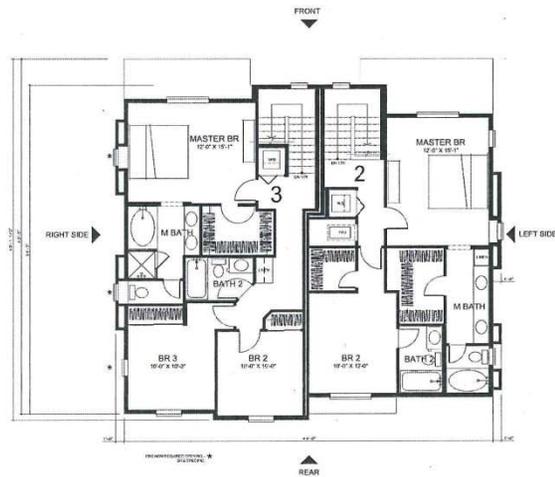
FIRST FLOOR PLAN

3-PLEX  
FIRST / SECOND FLOOR BUILDING PLAN  
ELEVATION STYLE - 'D'

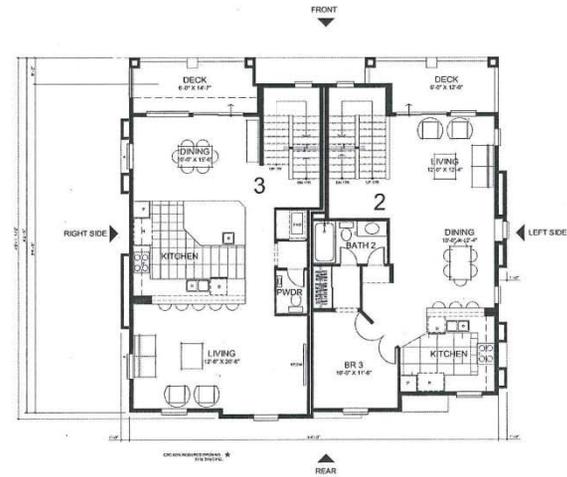
NORTHWEST LAND PARK - CITY HOMES  
NORTHWEST LAND PARK, LLC



3-PLEX AREAS	
PLAN 1	250 US SF
FIRST FLOOR	1315 US SF
SECOND FLOOR	1315 US SF
TOTAL LIVING AREA	2630 US SF
GARAGE 1	200 US SF
GARAGE 2	180 US SF
GARAGE 3	180 US SF
DECK 1	112 US SF
DECK 2	96 US SF
DECK 3	96 US SF
DECK 4	96 US SF
DECK 5	96 US SF
DECK 6	96 US SF
DECK 7	96 US SF
DECK 8	96 US SF
DECK 9	96 US SF
DECK 10	96 US SF
DECK 11	96 US SF
DECK 12	96 US SF
DECK 13	96 US SF
DECK 14	96 US SF
DECK 15	96 US SF
DECK 16	96 US SF
DECK 17	96 US SF
DECK 18	96 US SF
DECK 19	96 US SF
DECK 20	96 US SF
DECK 21	96 US SF
DECK 22	96 US SF
DECK 23	96 US SF
DECK 24	96 US SF
DECK 25	96 US SF
DECK 26	96 US SF
DECK 27	96 US SF
DECK 28	96 US SF
DECK 29	96 US SF
DECK 30	96 US SF
DECK 31	96 US SF
DECK 32	96 US SF
DECK 33	96 US SF
DECK 34	96 US SF
DECK 35	96 US SF
DECK 36	96 US SF
DECK 37	96 US SF
DECK 38	96 US SF
DECK 39	96 US SF
DECK 40	96 US SF
DECK 41	96 US SF
DECK 42	96 US SF
DECK 43	96 US SF
DECK 44	96 US SF
DECK 45	96 US SF
DECK 46	96 US SF
DECK 47	96 US SF
DECK 48	96 US SF
DECK 49	96 US SF
DECK 50	96 US SF
DECK 51	96 US SF
DECK 52	96 US SF
DECK 53	96 US SF
DECK 54	96 US SF
DECK 55	96 US SF
DECK 56	96 US SF
DECK 57	96 US SF
DECK 58	96 US SF
DECK 59	96 US SF
DECK 60	96 US SF
DECK 61	96 US SF
DECK 62	96 US SF
DECK 63	96 US SF
DECK 64	96 US SF
DECK 65	96 US SF
DECK 66	96 US SF
DECK 67	96 US SF
DECK 68	96 US SF
DECK 69	96 US SF
DECK 70	96 US SF
DECK 71	96 US SF
DECK 72	96 US SF
DECK 73	96 US SF
DECK 74	96 US SF
DECK 75	96 US SF
DECK 76	96 US SF
DECK 77	96 US SF
DECK 78	96 US SF
DECK 79	96 US SF
DECK 80	96 US SF
DECK 81	96 US SF
DECK 82	96 US SF
DECK 83	96 US SF
DECK 84	96 US SF
DECK 85	96 US SF
DECK 86	96 US SF
DECK 87	96 US SF
DECK 88	96 US SF
DECK 89	96 US SF
DECK 90	96 US SF
DECK 91	96 US SF
DECK 92	96 US SF
DECK 93	96 US SF
DECK 94	96 US SF
DECK 95	96 US SF
DECK 96	96 US SF
DECK 97	96 US SF
DECK 98	96 US SF
DECK 99	96 US SF
DECK 100	96 US SF
DECK 101	96 US SF
DECK 102	96 US SF
DECK 103	96 US SF
DECK 104	96 US SF
DECK 105	96 US SF
DECK 106	96 US SF
DECK 107	96 US SF
DECK 108	96 US SF
DECK 109	96 US SF
DECK 110	96 US SF
DECK 111	96 US SF
DECK 112	96 US SF
DECK 113	96 US SF
DECK 114	96 US SF
DECK 115	96 US SF
DECK 116	96 US SF
DECK 117	96 US SF
DECK 118	96 US SF
DECK 119	96 US SF
DECK 120	96 US SF
DECK 121	96 US SF
DECK 122	96 US SF
DECK 123	96 US SF
DECK 124	96 US SF
DECK 125	96 US SF
DECK 126	96 US SF
DECK 127	96 US SF
DECK 128	96 US SF
DECK 129	96 US SF
DECK 130	96 US SF
DECK 131	96 US SF
DECK 132	96 US SF
DECK 133	96 US SF
DECK 134	96 US SF
DECK 135	96 US SF
DECK 136	96 US SF
DECK 137	96 US SF
DECK 138	96 US SF
DECK 139	96 US SF
DECK 140	96 US SF
DECK 141	96 US SF
DECK 142	96 US SF
DECK 143	96 US SF
DECK 144	96 US SF
DECK 145	96 US SF
DECK 146	96 US SF
DECK 147	96 US SF
DECK 148	96 US SF
DECK 149	96 US SF
DECK 150	96 US SF
DECK 151	96 US SF
DECK 152	96 US SF
DECK 153	96 US SF
DECK 154	96 US SF
DECK 155	96 US SF
DECK 156	96 US SF
DECK 157	96 US SF
DECK 158	96 US SF
DECK 159	96 US SF
DECK 160	96 US SF
DECK 161	96 US SF
DECK 162	96 US SF
DECK 163	96 US SF
DECK 164	96 US SF
DECK 165	96 US SF
DECK 166	96 US SF
DECK 167	96 US SF
DECK 168	96 US SF
DECK 169	96 US SF
DECK 170	96 US SF
DECK 171	96 US SF
DECK 172	96 US SF
DECK 173	96 US SF
DECK 174	96 US SF
DECK 175	96 US SF
DECK 176	96 US SF
DECK 177	96 US SF
DECK 178	96 US SF
DECK 179	96 US SF
DECK 180	96 US SF
DECK 181	96 US SF
DECK 182	96 US SF
DECK 183	96 US SF
DECK 184	96 US SF
DECK 185	96 US SF
DECK 186	96 US SF
DECK 187	96 US SF
DECK 188	96 US SF
DECK 189	96 US SF
DECK 190	96 US SF
DECK 191	96 US SF
DECK 192	96 US SF
DECK 193	96 US SF
DECK 194	96 US SF
DECK 195	96 US SF
DECK 196	96 US SF
DECK 197	96 US SF
DECK 198	96 US SF
DECK 199	96 US SF
DECK 200	96 US SF
DECK 201	96 US SF
DECK 202	96 US SF
DECK 203	96 US SF
DECK 204	96 US SF
DECK 205	96 US SF
DECK 206	96 US SF
DECK 207	96 US SF
DECK 208	96 US SF
DECK 209	96 US SF
DECK 210	96 US SF
DECK 211	96 US SF
DECK 212	96 US SF
DECK 213	96 US SF
DECK 214	96 US SF
DECK 215	96 US SF
DECK 216	96 US SF
DECK 217	96 US SF
DECK 218	96 US SF
DECK 219	96 US SF
DECK 220	96 US SF
DECK 221	96 US SF
DECK 222	96 US SF
DECK 223	96 US SF
DECK 224	96 US SF
DECK 225	96 US SF
DECK 226	96 US SF
DECK 227	96 US SF
DECK 228	96 US SF
DECK 229	96 US SF
DECK 230	96 US SF
DECK 231	96 US SF
DECK 232	96 US SF
DECK 233	96 US SF
DECK 234	96 US SF
DECK 235	96 US SF
DECK 236	96 US SF
DECK 237	96 US SF
DECK 238	96 US SF
DECK 239	96 US SF
DECK 240	96 US SF
DECK 241	96 US SF
DECK 242	96 US SF
DECK 243	96 US SF
DECK 244	96 US SF
DECK 245	96 US SF
DECK 246	96 US SF
DECK 247	96 US SF
DECK 248	96 US SF
DECK 249	96 US SF
DECK 250	96 US SF
DECK 251	96 US SF
DECK 252	96 US SF
DECK 253	96 US SF
DECK 254	96 US SF
DECK 255	96 US SF
DECK 256	96 US SF
DECK 257	96 US SF
DECK 258	96 US SF
DECK 259	96 US SF
DECK 260	96 US SF
DECK 261	96 US SF
DECK 262	96 US SF
DECK 263	96 US SF
DECK 264	96 US SF
DECK 265	96 US SF
DECK 266	96 US SF
DECK 267	96 US SF
DECK 268	96 US SF
DECK 269	96 US SF
DECK 270	96 US SF
DECK 271	96 US SF
DECK 272	96 US SF
DECK 273	96 US SF
DECK 274	96 US SF
DECK 275	96 US SF
DECK 276	96 US SF
DECK 277	96 US SF
DECK 278	96 US SF
DECK 279	96 US SF
DECK 280	96 US SF
DECK 281	96 US SF
DECK 282	96 US SF
DECK 283	96 US SF
DECK 284	96 US SF
DECK 285	96 US SF
DECK 286	96 US SF
DECK 287	96 US SF
DECK 288	96 US SF
DECK 289	96 US SF
DECK 290	96 US SF
DECK 291	96 US SF
DECK 292	96 US SF
DECK 293	96 US SF
DECK 294	96 US SF
DECK 295	96 US SF
DECK 296	96 US SF
DECK 297	96 US SF
DECK 298	96 US SF
DECK 299	96 US SF
DECK 300	96 US SF
DECK 301	96 US SF
DECK 302	96 US SF
DECK 303	96 US SF
DECK 304	96 US SF
DECK 305	96 US SF
DECK 306	96 US SF
DECK 307	96 US SF
DECK 308	96 US SF
DECK 309	96 US SF
DECK 310	96 US SF
DECK 311	96 US SF
DECK 312	96 US SF
DECK 313	96 US SF
DECK 314	96 US SF
DECK 315	96 US SF
DECK 316	96 US SF
DECK 317	96 US SF
DECK 318	96 US SF
DECK 319	96 US SF
DECK 320	96 US SF
DECK 321	96 US SF
DECK 322	96 US SF
DECK 323	96 US SF
DECK 324	96 US SF
DECK 325	96 US SF
DECK 326	96 US SF
DECK 327	96 US SF
DECK 328	96 US SF
DECK 329	96 US SF
DECK 330	96 US SF
DECK 331	96 US SF
DECK 332	96 US SF
DECK 333	96 US SF
DECK 334	96 US SF
DECK 335	96 US SF
DECK 336	96 US SF
DECK 337	96 US SF
DECK 338	96 US SF
DECK 339	96 US SF
DECK 340	96 US SF
DECK 341	96 US SF
DECK 342	96 US SF
DECK 343	96 US SF
DECK 344	96 US SF
DECK 345	96 US SF
DECK 346	96 US SF
DECK 347	96 US SF
DECK 348	96 US SF
DECK 349	96 US SF
DECK 350	96 US SF
DECK 351	96 US SF
DECK 352	96 US SF
DECK 353	96 US SF
DECK 354	96 US SF
DECK 355	96 US SF
DECK 356	96 US SF
DECK 357	96 US SF
DECK 358	96 US SF
DECK 359	96 US SF
DECK 360	96 US SF
DECK 361	96 US SF
DECK 362	96 US SF
DECK 363	96 US SF
DECK 364	96 US SF
DECK 365	96 US SF
DECK 366	96 US SF
DECK 367	96 US SF
DECK 368	96 US SF
DECK 369	96 US SF
DECK 370	96 US SF
DECK 371	96 US SF
DECK 372	96 US SF
DECK 373	96 US SF
DECK 374	96 US SF
DECK 375	96 US SF
DECK 376	96 US SF
DECK 377	96 US SF
DECK 378	96 US SF
DECK 379	96 US SF
DECK 380	96 US SF
DECK 381	96 US SF
DECK 382	96 US SF
DECK 383	96 US SF
DECK 384	96 US SF
DECK 385	96 US SF
DECK 386	96 US SF
DECK 387	96 US SF
DECK 388	96 US SF
DECK 389	96 US SF
DECK 390	96 US SF
DECK 391	96 US SF
DECK 392	96 US SF
DECK 3	



FOURTH FLOOR PLAN



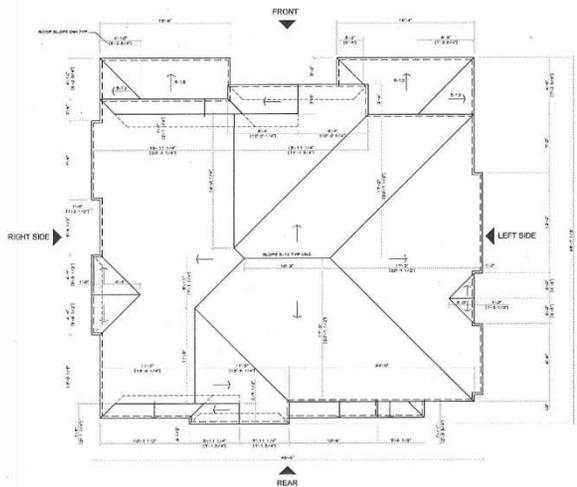
THIRD FLOOR PLAN

3-PLEX  
THIRD / FOURTH FLOOR BUILDING PLAN  
ELEVATION STYLE - D7

NORTHWEST LAND PARK - CITY HOMES  
NORTHWEST LAND PARK, LLC



3-PLEX AREAS	
PLAN 1	
FIRST FLOOR	255 18 5 FT
SECOND FLOOR	1712 53 5 FT
TOTAL LIVING AREA	1968 18 5 FT
GARAGE 1	789 58 5 FT
DECK 1	187 78 5 FT
DECK 2	185 58 5 FT
DECK 3	301 58 5 FT
PLAN 2	
FIRST FLOOR	165 47 5 FT
SECOND FLOOR	119 41 5 FT
THIRD FLOOR	775 32 5 FT
FOURTH FLOOR	737 33 5 FT
TOTAL LIVING AREA	1105 54 5 FT
GARAGE 2	422 42 5 FT
DECK 4	74 33 5 FT
DECK 5	287 17 5 FT
PLAN 3	
FIRST FLOOR	119 51 5 FT
SECOND FLOOR	119 43 5 FT
THIRD FLOOR	878 41 5 FT
FOURTH FLOOR	828 38 5 FT
TOTAL LIVING AREA	1845 13 5 FT
GARAGE 3	422 42 5 FT
DECK 6	187 78 5 FT
DECK 7	267 38 5 FT



3-PLEX  
 ROOF PLAN  
 ELEVATION STYLE - 'D'

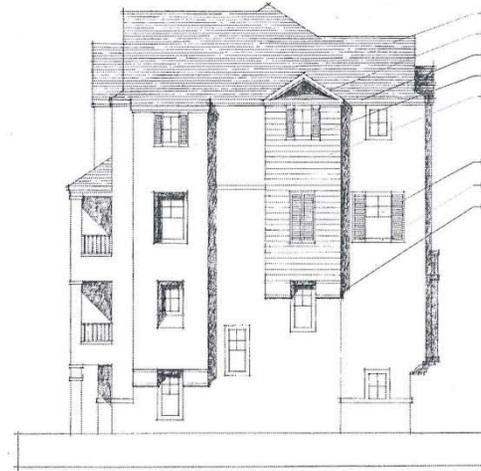
100'-0" 0"  
 NORTHWEST LAND PARK - CITY HOMES  
 NORTHWEST LAND PARK, LLC



01.21.11



FRONT ELEVATION



RIGHT SIDE ELEVATION

1'-0" = 1/8" = 1/16" = 1/32" = 1/64" = 1/128" = 1/256" = 1/512" = 1/1024" = 1/2048" = 1/4096" = 1/8192" = 1/16384" = 1/32768" = 1/65536" = 1/131072" = 1/262144" = 1/524288" = 1/1048576" = 1/2097152" = 1/4194304" = 1/8388608" = 1/16777216" = 1/33554432" = 1/67108864" = 1/134217728" = 1/268435456" = 1/536870912" = 1/1073741824" = 1/2147483648" = 1/4294967296" = 1/8589934592" = 1/17179869184" = 1/34359738368" = 1/68719476736" = 1/137438953472" = 1/274877906944" = 1/549755813888" = 1/1099511627776" = 1/2199023255552" = 1/4398046511104" = 1/8796093022208" = 1/17592186044416" = 1/35184372088832" = 1/70368744177664" = 1/140737488355328" = 1/281474976710656" = 1/562949953421312" = 1/1125899906842624" = 1/2251799813685248" = 1/4503599627370496" = 1/9007199254740992" = 1/18014398509481984" = 1/36028797018963968" = 1/72057594037927936" = 1/144115188075855872" = 1/288230376151711744" = 1/576460752303423488" = 1/1152921504606846976" = 1/2305843009213693952" = 1/4611686018427387904" = 1/9223372036854775808" = 1/18446744073709551616" = 1/36893488147419103232" = 1/73786976294838206464" = 1/147573952589676412928" = 1/295147905179352825856" = 1/590295810358705651712" = 1/1180591620717411303424" = 1/2361183241434822606848" = 1/4722366482869645213696" = 1/9444732965739290427392" = 1/18889465931478580854784" = 1/37778931862957161709568" = 1/75557863725914323419136" = 1/151115727451828646838272" = 1/302231454903657293676544" = 1/604462909807314587353088" = 1/1208925819614629174706176" = 1/2417851639229258349412352" = 1/4835703278458516698824704" = 1/9671406556917033397649408" = 1/19342813113834066795298816" = 1/38685626227668133590597632" = 1/77371252455336267181195264" = 1/154742504910672534362390528" = 1/309485009821345068724781056" = 1/618970019642690137449562112" = 1/1237940039285380274899124224" = 1/2475880078570760549798248448" = 1/4951760157141521099596496896" = 1/9903520314283042199192993792" = 1/19807040628566084398385987584" = 1/39614081257132168796771975168" = 1/79228162514264337593543950336" = 1/158456325028528675187087900672" = 1/316912650057057350374175801344" = 1/633825300114114700748351602688" = 1/1267650600228229401496703205376" = 1/2535301200456458802993406410752" = 1/5070602400912917605986812821504" = 1/10141204801825835211973625643008" = 1/20282409603651670423947251286016" = 1/40564819207303340847894502572032" = 1/81129638414606681695789005144064" = 1/162259276832213363391578010288128" = 1/324518553664426726783156020576256" = 1/649037107328853453566312041152512" = 1/1298074214657706907132624082305024" = 1/2596148429315413814265248164610048" = 1/5192296858630827628530496329220096" = 1/10384593717261655257060992658440192" = 1/20769187434523310514121985316880384" = 1/41538374869046621028243970633760768" = 1/83076749738093242056487941267521536" = 1/166153499476186484112975882535043072" = 1/332306998952372968225951765070086144" = 1/664613997904745936451903530140172288" = 1/1329227995809491872903807060280344576" = 1/26584559916189837458076141205606891536" = 1/53169119832379674916152282411213783072" = 1/106338239664759349832304564822427566144" = 1/212676479329518699664609129644855132288" = 1/425352958659037399329218259289710264576" = 1/850705917318074798658436518579420529152" = 1/1701411834636149597316873037158841058304" = 1/3402823669272299194633746074317682116608" = 1/6805647338544598389267492148635364233216" = 1/13611294677089196778534984297270728466432" = 1/27222589354178393557069968594541457332672" = 1/54445178708356787114139937189082914665344" = 1/108890357416713574228279874378165829330688" = 1/217780714833427148456559748756331658661376" = 1/435561429666854296913119497512663317322752" = 1/871122859333708593826238995025326634645504" = 1/1742245718667417187652477990050653273291008" = 1/3484491437334834375304955980101306546582016" = 1/6968982874669668750609911960202613093164032" = 1/13937965749339337501219823920405226186328064" = 1/27875931498678675002439647840810452372656128" = 1/55751862997357350004879295681620904745312256" = 1/11150372599471470000975859136324180949062512" = 1/22300745198942940001951718272648361898125024" = 1/44601490397885880003903436545296723796250048" = 1/89202980795771760007806873090593447592500096" = 1/178405961591543520015613746181186895185000192" = 1/356811923183087040031227492362373790370000384" = 1/713623846366174080062454984724747580740000768" = 1/1427247692732348160124909969449495161480001536" = 1/2854495385464696320249819938898990322960003072" = 1/5708990770929392640499639877797980645920006144" = 1/11417981541858785280999279755595961291840012288" = 1/22835963083717570561998559511191922583680024576" = 1/45671926167435141123997119022383845167360049152" = 1/91343852334870282247994238044767690334720098304" = 1/182687704669740564495988476089535380669440196608" = 1/365375409339481128991976952179070761338880393216" = 1/730750818678962257983953904358141522677760786432" = 1/1461501637357924515967907808716283045355521568864" = 1/2923003274715849031935815617432566090711043137728" = 1/5846006549431698063871631234865132181422086275456" = 1/11692013098863396127743262469730264362844172550912" = 1/23384026197726792255486524939460528725688345101824" = 1/46768052395453584510973049878921057451376690203648" = 1/93536104790907169021946099757842114902753380407296" = 1/187072209581814338043892199515684229805506760814592" = 1/374144419163628676087784399031368459611013521629184" = 1/748288838327257352175568798062737319222027043258368" = 1/1496577676654514704351137596125474638444054086516736" = 1/2993155353309029408702275192250949276888108173033472" = 1/5986310706618058817404550384501898553776216346066944" = 1/11972621413236117634809100769003797107552332692133888" = 1/23945242826472235269618201538007594215104665384267776" = 1/47890485652944470539236403076015188430209330768535552" = 1/95780971305888941078472806152030376860418661537071104" = 1/191561942611777882156945612304060753720837323074142208" = 1/383123885223555764313891224608121507441674646148284416" = 1/766247770447111528627782449216243014883349292296568832" = 1/1532495540894223057255564898432486029766698584593137664" = 1/3064991081788446114511129796864972059533397169186275328" = 1/6129982163576892229022259533729844119066794338372550656" = 1/1225996432715378445804451906745968823813348867674510112" = 1/2451992865430756891608903813491937647626697735349020224" = 1/4903985730861513783217807626983875295253395470698040448" = 1/9807971461723027566435615253967750590506790941396080896" = 1/19615942923446055132871230507935501181013581882792161792" = 1/39231885846892110265742461015871002362027163765584383584" = 1/78463771693784220531484922031742004724054327531168767168" = 1/156927543387568441062969844063484009448108655062336336" = 1/313855086775136882125939688126968018896217310124672672" = 1/627710173550273764251879376253936037792434620249345344" = 1/1255420347100547528503758752507872075584869240498690688" = 1/2510840694201095057007517505015744151169738480997381376" = 1/5021681388402190114015035010031488302339476961994762752" = 1/1004336277680438022803007002006297660467893992398952544" = 1/2008672555360876045606014004012595320935787984797905088" = 1/4017345110721752091212028008025190641871575969595810176" = 1/8034690221443504182424056016050381283743151939191620352" = 1/1606938044288700836484811203210076256748630387838324064" = 1/3213876088577401672969622406420152513497260775676648128" = 1/6427752177154803345939244812840285026994521551353296256" = 1/12855504354309606691878489625680570053989043102706592512" = 1/25711008708619213383756979251361140107978086205413185024" = 1/51422017417238426767513958502722280215956172410826370048" = 1/102844034834476853535027917005444560431912344821652740096" = 1/205688069668953707070055834010889120863824689643305480192" = 1/411376139337907414140111668021778241727649379286610960384" = 1/822752278675814828280223336043556483455298758573221920768" = 1/1645504557351629656560446672087112966910597117146443841536" = 1/3291009114703259313120893344174225933821194234292887683072" = 1/6582018229406518626241786688348451867642388468585775366144" = 1/13164036458813037252483573376696903735284777337171550722288" = 1/26328072917626074504967146753393807470569554674343101454576" = 1/52656145835252149009934293506787614941139109348686202909152" = 1/105312291670504298119868587013575229882278218697372405818304" = 1/210624583341008596239737174027150459764556437394744811636608" = 1/421249166682017192479474348054300919529112874789489623273216" = 1/842498333364034384958948696108601839058225749578979246546432" = 1/1684996667128068769917897392217203678116451499157958493092864" = 1/3369993334256137539835794784434407356232902998315916986185728" = 1/6739986668512275079671589568868814712465805996631833972371456" = 1/13479973337024550159343179137737294424931611993263667944742912" = 1/26959946674049100318686358275474588849863223986527335889485824" = 1/53919893348098200637372716550949177699726447973054671778971648" = 1/107839786696196401274745433101898355399452895946109343557943296" = 1/215679573392392802549490866203796710798905791892218687115886592" = 1/431359146784785605098981732407593421597811583784437374231773184" = 1/862718293569571210197963464815186843195623167568874748463462768" = 1/1725436587139142420395926929630373686391246335137749496926925536" = 1/3450873174278284840791853859260747372782492670275498993853851072" = 1/6901746348556569681583707718521494745564985340550997987707702144" = 1/13803492697113139363167415437042989491129970681101995975415404288" = 1/27606985394226278726334830874085978982259941362203991950830808576" = 1/55213970788452557452669661748171957964519882724407983901661617152" = 1/11042794157690511490533932349634391592903976544881596780332323424" = 1/22085588315381022981067864699268783185807953089763193560664646848" = 1/44171176630762045962134729398537566371615906179526387121329293696" = 1/88342353261524091924269458797075132743231812359052774242658587392" = 1/176684706523048183848538917594150265486463624718105548485317174784" = 1/353369413046096367697077835188300530972927249436211096970634349568" = 1/706738826092192735394155670376601061945854498872422193941268699136" = 1/1413477652184385470788311340753202123891709997744844387882537398272" = 1/2826955304368770941576622681506404247783419995489688775765074796544" = 1/5653910608737541883153245363012808495566839990979377551530149593088" = 1/11307821217475083766306490726025616911333679981958755103060299186176" = 1/22615642434950167532612981452051233822667359963917510206120598372352" = 1/45231284869900335065225962904102467645334719927835020412241196744704" = 1/90462569739800670130451925808204935290669439855670040824482393489408" = 1/180925139479601340260903851616409870581338879711340081648964786978816" = 1/361850278959202680521807703232819741162677759422680162979291573957632" = 1/723700557918405361043615406465639482325355518845360325958583478915264" = 1/1447401115836810722087230812931278964650711037690720658171166957830528" = 1/2894802231673621444174461625862557929301422075381441316342333915661056" = 1/5789604463347242888348923251725115858602844150762882632684667831322112" = 1/11579208926694485776697846503450231717205688301525765265369335662644224" = 1/23158417853388971553395693006900463434411376603051530530738671325288448" = 1/46316835706777943106791386013800926868822753206103061061477342650576896" = 1/92633671413555886213582772027601853737645506412206122122954685301153792" = 1/185267342827111772427165544055203707475291012824412244257909370602307584" = 1/370534685654223544854331088110407414950582025648824488515818741204615168" = 1/741069371308447089708662176220814829901164051297648977031637482409230336" = 1/1482138742616894179417324352441629659802328102595297954063274964818460672" = 1/2964277485233788358834648704883259319604656205190595908126549929636921344" = 1/5928554970467576717669297409766518392209312410381191816253099859273842688" = 1/11857109940935153435338594819533036784418624220762383632506199718547685376" = 1/23714219881870306870677189639066073568837248441524767265012399437095370752" = 1/47428439763740613741354379278132147137674496883049534530024798874190741504" = 1/94856879527481227482708758556264294275348993766099069060049597748381483008" = 1/189713759054962454965417517112528588550697987532198138120099195496762966016" = 1/379427518109924909930835034225057177101395975064396276240198390993525932032" = 1/758855036219



REAR ELEVATION



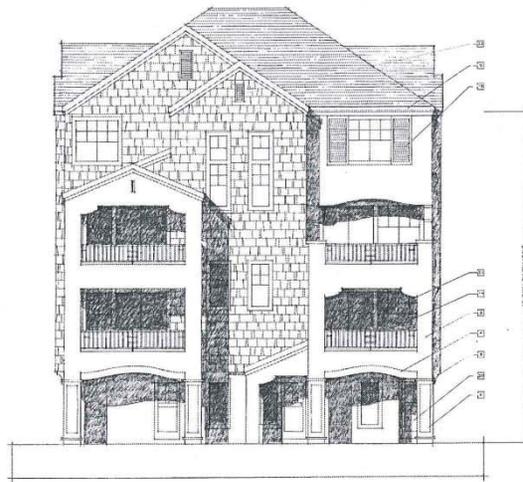
LEFT SIDE ELEVATION

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'D'

1/8"=1'-0"  
NORTHWEST LAND PARK - CITY HOMES  
NORTHWEST LAND PARK, LLC



TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR FLAT TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	3 X 6 T&G
<input type="checkbox"/>	2 X RAKE BOARD
<input type="checkbox"/>	PLASTER CORNELL
<input type="checkbox"/>	PLASTER COLUMN
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	DEGRATIVE PLASTER PRODUCTION
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	WIND SHIELD
<input type="checkbox"/>	PLASTER BATTERED WALL
<input type="checkbox"/>	HORIZONTAL SHINGLE SIDING
<input type="checkbox"/>	HORIZONTAL SIDING
<input type="checkbox"/>	BOARD AND BATTEN
<input type="checkbox"/>	DEGRATIVE SHUTTER
<input type="checkbox"/>	BEAM / BEAM ENDS
<input type="checkbox"/>	BACK PATTERNS VENEER
<input type="checkbox"/>	RAIL GUARDRAIL
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	ALUMINUM SLIDING DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR
<input type="checkbox"/>	DEGRATIVE VENT
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



FRONT ELEVATION



RIGHT SIDE ELEVATION

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'

1/8"=1'-0"

NORTHWEST LAND PARK - CITY HOMES  
NORTHWEST LAND PARK, LLC

201804720



01.21.11

TYPICAL BUILDING MATERIALS

- INTEGRAL COLOR FLAT TILE ROOF
- INTEGRAL COLOR PLASTER
- SHAKES
- SHAKE BOARD
- PLASTER CORBEL
- PLASTER COLUMN
- PLASTER FINISH TRIM
- DECORATIVE PLASTER PROJECTION
- PLASTER TRIM
- CIVIL FINISH
- PLASTER BATTERED HALL
- HARDBOARD SINGLE BEAM
- HORIZONTAL BEAM
- BOARD AND BATTEN
- DECORATIVE SHUTTER
- BEAM / BOARD DOOR
- BRICK PATTERN VENEER
- 1/2" BOARD
- METAL HALF GROUND
- FIBERGLASS ENTRY DOOR
- SLIDING GLASS DOOR
- METAL SECTIONAL GARAGE DOOR
- DECORATIVE VENT

\* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



REAR ELEVATION



LEFT SIDE ELEVATION

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'

1/4"=1'-0"

NORTHWEST LAND PARK - CITY HOMES  
NORTHWEST LAND PARK, LLC

201808020



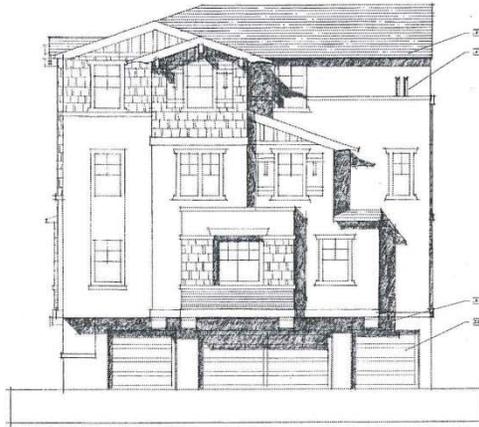
REAR ELEVATION

TYPICAL BUILDING MATERIALS

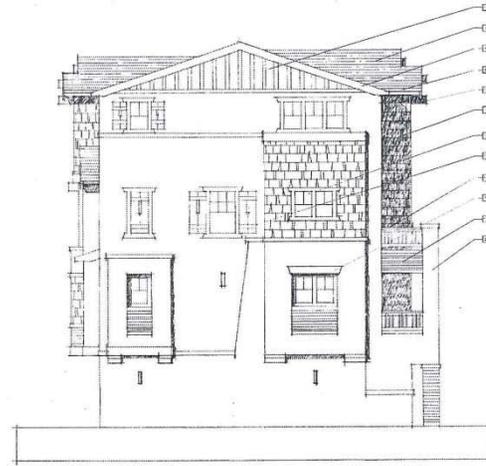
- ☐ INTEGRAL COLOR FLAT TILE ROOF
- ☐ INTEGRAL COLOR PLASTER
- ☐ 2x4 FASCIA
- ☐ 2x4 RAKE BOARD
- ☐ PLASTER CORBEL
- ☐ PLASTER COLUMN
- ☐ PLASTER FINISH TRIM
- ☐ DECORATIVE PLASTER PROJECTION
- ☐ PLASTER TRIM
- ☐ VINYL FINISH
- ☐ PLASTER BATTERED PILL
- ☐ HARDBOARD SINGLE SIDING
- ☐ HORIZONTAL SIDING
- ☐ BOARD AND BATTEN
- ☐ DECORATIVE SHUTTER
- ☐ BEAM / BRAM BOSS
- ☐ BRICK PATTERN VENEER
- ☐ RAL / GUARDRAIL
- ☐ METAL RAIL / GUARDRAIL
- ☐ FIBERGLASS ENTRY DOOR
- ☐ SLIPING GLASS DOOR
- ☐ METAL SECTIONAL GARAGE DOOR
- ☐ DECORATIVE VENT

\* ALL METAL AND WOOD ELEMENTS TO BE PAINTED





REAR ELEVATION



LEFT SIDE ELEVATION - ENHANCED

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

1/8"=1'-0"

NORTHWEST LAND PARK - CITY HOMES  
NORTHWEST LAND PARK, LLC

2010080720



BL 21 11

TYPICAL BUILDING MATERIALS

- INTEGRAL COLOR FLAT TILE ROOF
- INTEGRAL COLOR PLASTER
- S/S RAFTER
- S/S RAKE BOARD
- PLASTER CORBEL
- PLASTER COLONN
- PLASTER ARCHON TRIM
- DEGRATIVE PLASTER PROJECTION
- PLASTER TRIM
- CIVIL FINISH
- PLASTER BATTERED HILL
- HARDWOOD SHINGLE SIDING
- HORIZONTAL SIDING
- BOARD AND BATTEN
- BOARD AND BATTEN
- DEGRATIVE SHUTTER
- BEAM / BEAM END
- BRICK PATTERN VENEER
- RAIL / GARDENAL
- METAL RAIL / GARDENAL
- FIBERGLASS ENTRY DOOR
- SLIDING GLASS DOOR
- METAL SECTIONAL GARAGE DOOR
- DEGRATIVE VENT

\* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



LEFT SIDE ELEVATION

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

12/14/19

**NORTHWEST LAND PARK - CITY HOMES**  
NORTHWEST LAND PARK, LLC

12/14/19



12.14.19

TYPICAL BUILDING MATERIALS	
☐	INTERNAL COLOR FLAT TILE ROOF
☐	INTERNAL COLOR PLASTER
☐	2 X 4 PAPER
☐	EXTERIOR BOARD
☐	PLASTER CORNELL
☐	PLASTER COLUMN
☐	PLASTER WINDOW TRIM
☐	DECORATIVE PLASTER PROJECTION
☐	PLASTER TRIM
☐	VINYL WINDOW
☐	PLASTER BATTERED PILL
☐	HARDBOARD SHINGLE SIDING
☐	HORIZONTAL SIDING
☐	BOARD AND BATTEN
☐	DECORATIVE SHUTTER
☐	BEAM / BEAM ENDS
☐	BEIG PATTERN VENEER
☐	RAIL / GUARDRAIL
☐	METAL RAIL / GUARDRAIL
☐	FIBERGLASS ENTRY DOOR
☐	SLIDING GLASS DOOR
☐	METAL SECTIONAL GARAGE DOOR
☐	DECORATIVE VENT

\* ALL METAL AND ACCENT ELEMENTS TO BE PAINTED

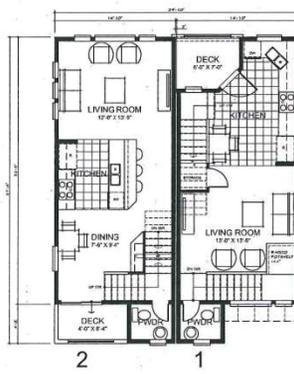
<p>Date: July 12, 2010</p> <p>Preliminary 2007 CBC Review</p> <p>R-3 residential, type V-B (non-rated) construction, no sprinklers</p> <p>1. Occupancy</p> <p>a. Residential: R-3 Duplex and Single Family (Section 310.1)</p> <p>b. Private Garages (406.1); Group U accessory to R-3 occupancy</p> <p>2. Maximum Stories/Height</p> <p>a. Measured to average height of highest roof surface (502)</p> <p>b. Maximum height: 3 stories and 40' (Table 503)</p> <p>3. Maximum Building Area (Table 503)</p> <p>a. Unlimited</p> <p>4. Buildings on the Same Lot: (704.3, Exception)</p> <p>a. Imaginary line assumed between duplex buildings and between single family</p> <p>b. R-3 area unlimited (Table 503)</p> <p>5. Fire Resistance of Building Elements (Table 601)</p> <p>a. Structural Frame: 0 Hr</p> <p>b. Bearing Walls, Int. &amp; Ex: 0</p> <p>c. Non-bearing walls and partitions (Table 601): 0</p> <p>d. Floors: 0</p> <p>e. Roof: 0</p> <p>6. Fire Resistance Rating Requirements for Exterior Walls based on Fire Separation Distance (Table 602)</p> <p>a. <math>0 &lt; X \leq 5'</math>: 1 Hr</p> <p>b. <math>X &gt; 5'</math>: 0</p> <p>7. Projections (Including Balconies)</p> <p>a. Maximum Projection: Max 12 inches where openings prohibited (704.2.2)</p> <p>b. Openings prohibited where <math>X &gt; 5'</math> (Table 704.8)</p> <p>c. Projections allowed where <math>X &lt; 5'</math></p> <p>d. Combustible projections where openings are not permitted shall be 1-hour min (704.2.2)</p> <p>8. Maximum Area of Exterior Openings (Table 704.8.c.8.g):</p> <p>a. X-fire separation distance</p> <table border="1"> <tr> <td>1. <math>0 &lt; X &lt; 5'</math></td> <td>Not Permitted</td> </tr> <tr> <td>2. <math>5' &lt; X &lt; 5'</math></td> <td>25%</td> </tr> </table>	1. $0 < X < 5'$	Not Permitted	2. $5' < X < 5'$	25%	<p>b. Sixx Unlimited</p> <p>9. Unit/Occupancy Separations</p> <p>a. Between Units:</p> <p>i. Walls and Supporting Structure: 1 hour fire partitions (708.3, 708.4)</p> <p>ii. Floors and Supporting Structure: 1 hour (711.3, 711.4)</p> <p>b. Residence to Private Garage per 406.1.4</p> <p>10. Sprinklers</p> <p>a. Not required per 2007 CBC</p> <p>11. Ealing</p> <p>a. Stairs within individual units:</p> <p>i. Min Width (1008.1, exception 1): 36", occupant load &lt; 50</p> <p>ii. Max. Rise (1009.3.4) - 7'3"</p> <p>iii. Min. Treads: 10"</p> <p>iv. Min. Riser: 4"</p> <p>b. Landings</p> <p>i. Doors opening onto landing shall not reduce landing to less than one-half required width (1009.4.2)</p> <p>ii. In R-3, a floor or landing is not required at the top of an interior flight of stairs provided door does not swing over stairs (1009.4.3)</p> <p>c. Egress</p> <p>i. One exit required from each unit, Common Path of Egress Travel lines do not apply in R-3. (1019.2.2)</p> <p>d. Egress Court</p> <p>i. Width: 36" minimum (1024.5.1)</p> <p>12. Accessibility</p> <p>a. Duplexes and single family are exempt (1102A.1.2)</p> <p>b. Duplexes and single family are exempt from Multi-story Dwelling requirements (1102A.3.1)</p> <p>13. Yards or Courts (1206)</p> <p>a. 1206 applies only to openings that provide required natural light or ventilation.</p> <p>(1206.1 and IBC Commentary)</p> <p>b. Yards or courts with openings that do not provide required natural light or ventilation do not need to comply with 1206 requirements</p> <p>c. Spaces bounded on only 2 sides in a yard (1206.2)</p> <p>d. 3 story yards with required openings shall be 4' minimum width (2' min + 1' over zone 2 - 2' + 1' x 1') (1206.2)</p> <p>e. Yard minimum width shall be unobstructed from the ground to the sky (1206.2)</p> <p>f. Spaces bounded on 3 sides in a court (1206)</p> <p>g. Note: There are no openings that provide required natural light or ventilation at roof.</p>	
1. $0 < X < 5'$	Not Permitted					
2. $5' < X < 5'$	25%					

CODE ANALYSIS

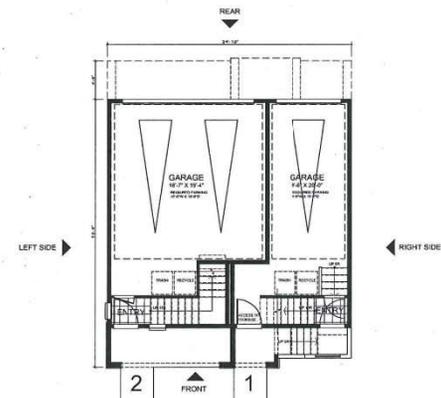
N.T.E.

NORTHWEST LAND PARK - THE TOWNS  
NORTHWEST LAND PARK, LLC





SECOND FLOOR PLAN



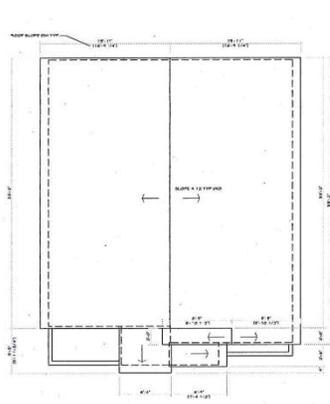
FIRST FLOOR PLAN

2-PLEX  
FIRST / SECOND FLOOR PLAN  
ELEVATION STYLE - 'A'

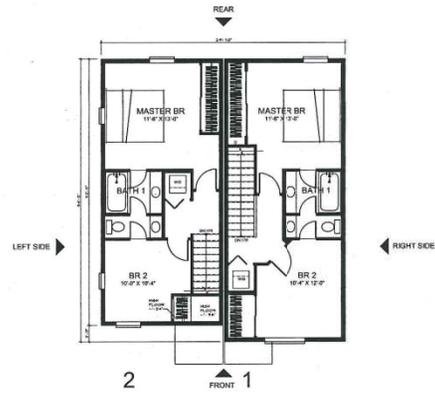
10'0" x 10'0"  
NORTHWEST LAND PARK - THE TOWNS  
NORTHWEST LAND PARK, LLC



UNIT 1	
FIRST FLOOR	40.79 S.F.
SECOND FLOOR	439.31 S.F.
TOTAL FLOOR	480.10 S.F.
TOTAL LIVING AREA	364.34 S.F.
GARAGE	202.37 S.F.
DECK	48.45 S.F.
total area	1274.37 S.F.
UNIT 2	
FIRST FLOOR	35.74 S.F.
SECOND FLOOR	517.26 S.F.
TOTAL FLOOR	553.00 S.F.
TOTAL LIVING AREA	1145.89 S.F.
GARAGE	438.82 S.F.
DECK	48.00 S.F.
total area	2132.71 S.F.



ROOF PLAN



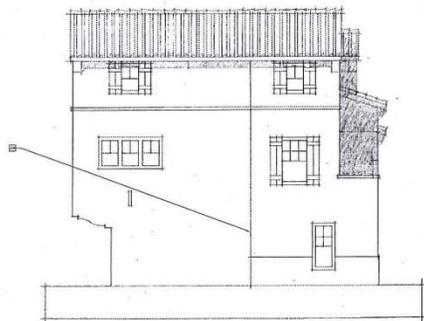
THIRD FLOOR PLAN

2-PLEX  
 THIRD FLOOR PLAN / ROOF PLAN  
 ELEVATION STYLE - 'A'

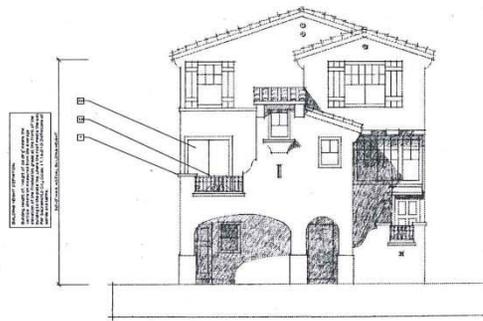
NORTHWEST LAND PARK - THE TOWNS  
 NORTHWEST LAND PARK, LLC



UNIT 1	
FIRST FLOOR	42.32 S.F.
SECOND FLOOR	438.92 S.F.
THIRD FLOOR	524.88 S.F.
TOTAL LIVING AREA	986.02 S.F.
GARAGE	20.27 S.F.
DECK	44.47 S.F.
ROOF AREA	1274.87 S.F.
UNIT 2	
FIRST FLOOR	55.74 S.F.
SECOND FLOOR	317.06 S.F.
THIRD FLOOR	451.88 S.F.
TOTAL LIVING AREA	824.68 S.F.
GARAGE	43.88 S.F.
DECK	42.00 S.F.
ROOF AREA	1118.24 S.F.



LEFT SIDE ELEVATION (B, C, D SIMILAR)



FRONT ELEVATION

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

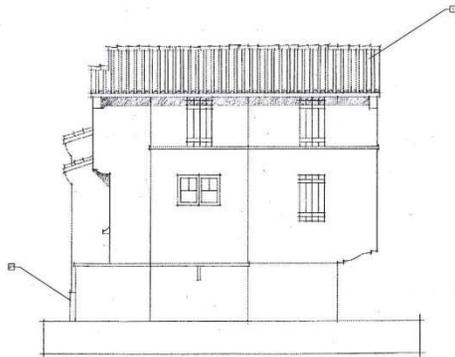
NORTHWEST LAND PARK - THE TOWNS  
NORTHWEST LAND PARK, LLC



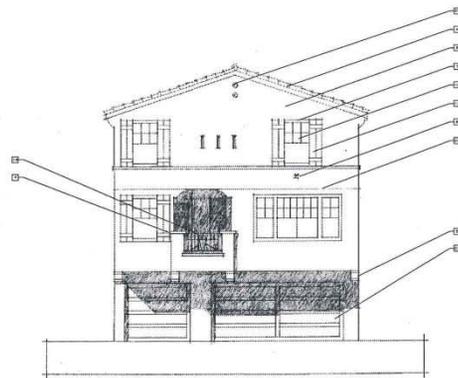
TYPICAL BUILDING MATERIALS

- INTEGRAL COLOR 12" TILE ROOF
- INTEGRAL COLOR FLAT TILE ROOF
- INTEGRAL COLOR PLASTER
- 2X FRAMING
- CORBEL
- COLUMN
- PLASTER WOODY TRIM
- DECORATIVE PLASTER PROJECTION
- PLASTER TRIM
- VINYL WINDOWS
- HARDBOARD SHINGLE SIDING
- HORIZONTAL SIDING
- DECORATIVE SHUTTER
- TRELLIS
- BRUSH
- RAFTER TAIL
- RAIL / SHARDORAIL
- METAL RAIL / SHARDORAIL
- FIBERGLASS SHINY DOOR
- SLIP-ON SLIDE DOOR
- FRENCH DOOR
- METAL SECTIONAL GARAGE DOOR
- STORAGE CLOSET
- DECORATIVE POSSUM
- DECORATIVE ACCENT TILE
- DECORATIVE VENT
- PLASTER PARQUET
- EXPANSION JOINT
- 2X RAFTER BOARD
- BRICK PATTERNS VENEER

\* ALL METAL AND FOOD ELEMENTS TO BE PAINTED



RIGHT SIDE ELEVATION (B, C, D SIMILAR)



REAR ELEVATION

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

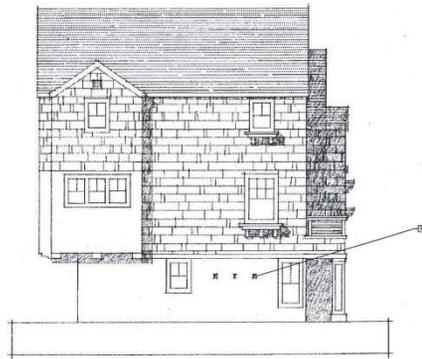
NORTHWEST LAND PARK - THE TOWNS  
NORTHWEST LAND PARK, LLC



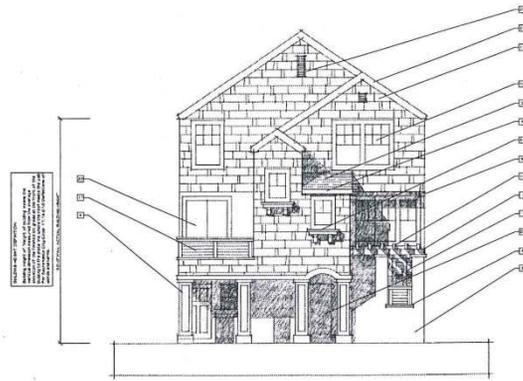
TYPICAL BUILDING MATERIALS

- INTEGRAL COLOR W/TILE ROOF
- INTEGRAL COLOR FLAT TILE ROOF
- INTEGRAL COLOR PLASTER
- 3X FASCIA
- CORNICE
- COLUMN
- PLASTER WINDOW TRIM
- DECORATIVE PLASTER PROJECTION
- PLASTER TRIM
- VINYL WINDOW
- HORIZONTAL SHINGLE SIDING
- HORIZONTAL SIDING
- DECORATIVE SHUTTER
- TRELLIS
- BRAM
- RAFTER TAIL
- RAIL / SHARDING
- METAL RAIL / SHARDING
- FIBERGLASS ENTRY DOOR
- BLEND GLASS DOOR
- FRENCH DOOR
- METAL SECTIONAL GARAGE DOOR
- SHOWER CLOSET
- DECORATIVE POSTMOUNT
- DECORATIVE ACCENT TILE
- DECORATIVE VENT
- PLASTER PARAPET
- EXPANSION JOINT
- 2X NAME BOARD
- BRICK PATTERNS - VENER

\* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



LEFT SIDE ELEVATION - ENHANCED



FRONT ELEVATION

ALL METAL AND WOOD ELEMENTS TO BE PAINTED  
 SEE FINISH SCHEDULE FOR PAINT TYPE AND COLOR  
 SEE FINISH SCHEDULE FOR PAINT TYPE AND COLOR  
 SEE FINISH SCHEDULE FOR PAINT TYPE AND COLOR

EXTERIOR ELEVATIONS  
 ELEVATION STYLE - 'B'

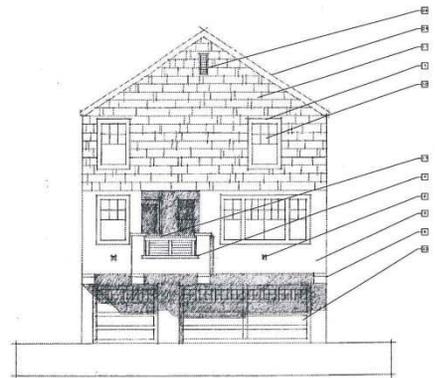
NORTHWEST LAND PARK - THE TOWNS  
 NORTHWEST LAND PARK, LLC



TYPICAL BUILDING MATERIALS

- INTEGRAL COLOR 1/2" TILE ROOF
- INTEGRAL COLOR FLAT TILE ROOF
- INTEGRAL COLOR PLASTER
- 2X FASCIA
- CORBEL
- GORBEL
- PLASTER FINISH
- PLASTER FINISH PROJECTION
- PLASTER FINISH
- VINYL FINISH
- HANDICAPped SINGLE BOND
- HORIZONTAL BOND
- DECORATIVE SHUTTER
- TRELLIS
- BRICK
- RAFTER TAIL
- RAK / GUARDRAIL
- METAL RAIL / GUARDRAIL
- FIBERGLASS ENTRY DOOR
- SLIP RESISTANT DOOR
- FRENCH DOOR
- METAL SECTIONAL GARAGE DOOR
- STORAGE CLOSET
- DECORATIVE POTTERY
- DECORATIVE ACCENT TILE
- DECORATIVE VENT
- PLASTER FINISH
- EXPANSION JOINT
- 2X RAFTER BOARD
- BRICK PATTERNS VENEER

\* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



REAR ELEVATION

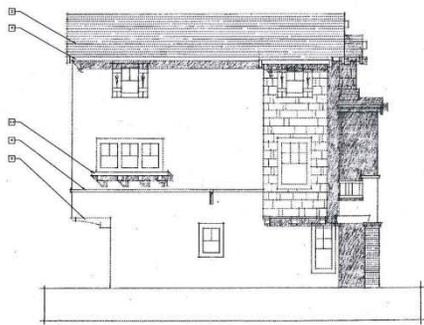
TYPICAL BUILDING MATERIALS	
☐	INTEGRAL COLOR STEEL ROOF
☐	INTEGRAL COLOR FLAT TILE ROOF
☐	INTEGRAL COLOR PLASTER
☐	JK PANDA
☐	SONBEL
☐	COLUMB
☐	PLASTER WINDOW TRIM
☐	DECORATIVE PLASTER PROJECTION
☐	PLASTER TRIM
☐	VINYL WINDOW
☐	HANDICAPPED SHINGLE BOND
☐	HORIZONTAL BOND
☐	DECORATIVE SHUTTER
☐	TRELIS
☐	BEAM
☐	RAFTER TAIL
☐	RAIL / BARRICADE
☐	METAL WALL / BARRICADE
☐	FIBERGLASS SHUTTER DOOR
☐	SLIDING GLASS DOOR
☐	FRANCH DOOR
☐	METAL SECTIONAL GARAGE DOOR
☐	STORAGE CLOSET
☐	DECORATIVE POTSHIELD
☐	DECORATIVE ACCENT TILE
☐	DECORATIVE VENT
☐	PLASTER HANDICUT
☐	EXPANSION JOINT
☐	JK RANGE BOARD
☐	BRICK PATTERNS VENEER

\* ALL METAL SHEET ROOF ELEMENTS TO BE PAINTED

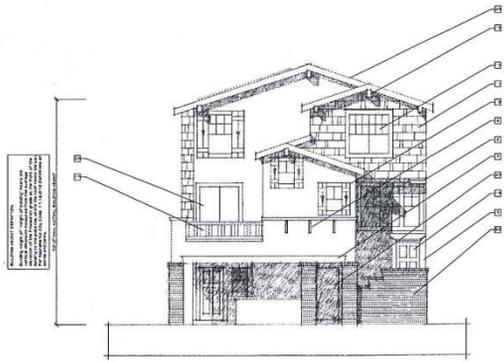
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'

1/4"=1'-0"  
NORTHWEST LAND PARK - THE TOWNS  
NORTHWEST LAND PARK, LLC





LEFT SIDE ELEVATION - ENHANCED



FRONT ELEVATION

THIS ELEVATION IS FOR INFORMATION ONLY. IT IS NOT TO BE USED FOR CONSTRUCTION. THE ARCHITECT ASSUMES NO LIABILITY FOR ANY ERRORS OR OMISSIONS. THE CLIENT ASSUMES ALL LIABILITY FOR ANY ERRORS OR OMISSIONS.

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

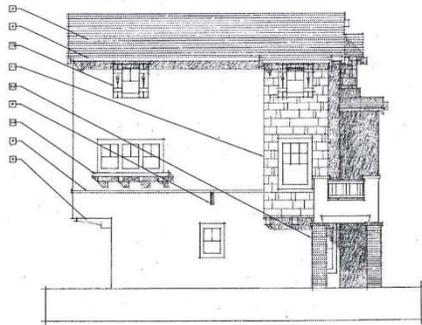
1/16"=1'-0"

NORTHWEST LAND PARK - THE TOWNS  
NORTHWEST LAND PARK, LLC

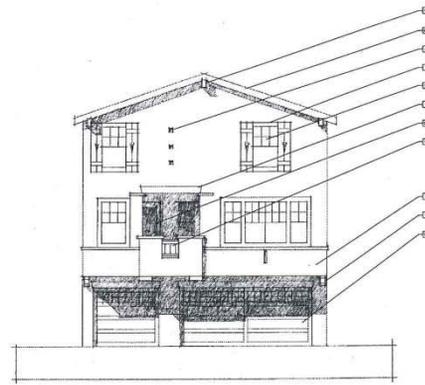


TYPICAL BUILDING MATERIALS

- ☐ INTEGRAL COLOR W/ TILE ROOF
  - ☐ INTEGRAL COLOR FLAT TILE ROOF
  - ☐ INTEGRAL COLOR PLASTER
  - ☐ 2X FINISH
  - ☐ CORBEL
  - ☐ GROUND
  - ☐ PLASTER HOODHOLE TRIM
  - ☐ DECORATIVE PLASTER PROJECTION
  - ☐ PLASTER TRIM
  - ☐ VINYL WINDOW
  - ☐ HANDSCAPED SHINGLE SIDING
  - ☐ HORIZONTAL SIDING
  - ☐ DECORATIVE SHUTTER
  - ☐ TRUSS
  - ☐ BRAM
  - ☐ PLASTER TAIL
  - ☐ RAIL / BEARDTAIL
  - ☐ METAL RAIL / BEARDTAIL
  - ☐ FIBERGLASS ENTRY DOOR
  - ☐ SLIDING GLASS DOOR
  - ☐ FRENCH DOOR
  - ☐ METAL SECTIONAL GARAGE DOOR
  - ☐ STORAGE CLOSET
  - ☐ DECORATIVE POST/SELF
  - ☐ DECORATIVE ACCENT TILE
  - ☐ DECORATIVE VENT
  - ☐ PLASTER PARAPET
  - ☐ EXPANSION JOINT
  - ☐ 2X RAFTER BOARD
  - ☐ BRICK PLASTER VENEER
- \* ALL METAL AND HOOD ELEMENTS TO BE PAINTED



LEFT SIDE ELEVATION - ENHANCED WITH DECK



REAR ELEVATION

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

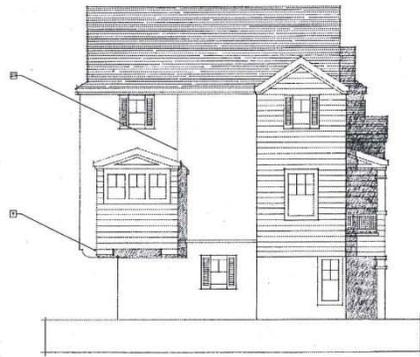
10749P  
NORTHWEST LAND PARK - THE TOWNS  
NORTHWEST LAND PARK, LLC  
2008009



01.31.11

TYPICAL BUILDING MATERIALS

- INTEGRAL COLOR "V" TILE ROOF
  - INTEGRAL COLOR FLAT TILE ROOF
  - INTEGRAL COLOR PLASTER
  - 3X FASCIA
  - CORBEL
  - COLUMN
  - PLASTER WINDSHIELD
  - DECORATIVE PLASTER PROJECTION
  - PLASTER TRIM
  - VINYL WINDOCH
  - HANDICAPped SHIMBLE BOND
  - HORIZONTAL SIDING
  - DECORATIVE SHUTTER
  - TRELLIS
  - BRIM
  - RAFTER TAIL
  - RAIL / GUARDRAIL
  - METAL RAIL / GUARDRAIL
  - FIBERGLASS ENTRY DOOR
  - SLIP-RESISTANT GLASS DOOR
  - FRENCH DOOR
  - METAL SECTIONAL GARAGE DOOR
  - STORAGE CLOSET
  - DECORATIVE FOTHELF
  - DECORATIVE ACCENT TILE
  - DECORATIVE VENT
  - PLASTER HANGBOY
  - EXPANSION JOINT
  - ZIRKON BOARD
  - BRICK PATTERN VENER
- \* ALL METAL AND FIBER DOOR ELEMENTS TO BE PAINTED



LEFT SIDE ELEVATION - ENHANCED



FRONT ELEVATION

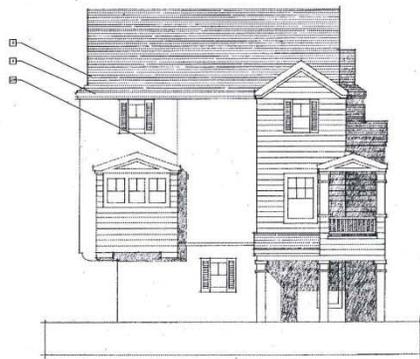
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'D'

117'-4 1/2"  
NORTHWEST LAND PARK - THE TOWNS  
NORTHWEST LAND PARK, LLC

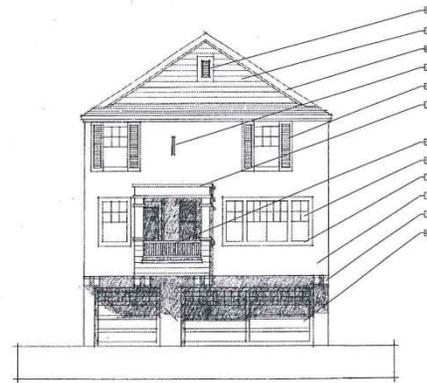


TYPICAL BUILDING MATERIALS

- INTEGRAL COLOR 12" TILE ROOF
  - INTEGRAL COLOR FLAT TILE ROOF
  - INTEGRAL COLOR PLASTER
  - 3X PANEL
  - CORBEL
  - GROUND
  - PLASTER WOODY TRIM
  - DECORATIVE PLASTER PROJECTION
  - PLASTER TRIM
  - VINYL WINDOWS
  - HANDICAPPED SINGLE BOND
  - HORIZONTAL LINE
  - DECORATIVE BUTTER
  - TRELLIS
  - BRASS
  - PLASTER TAIL
  - RAIL / GUARDRAIL
  - METAL RAIL / GUARDRAIL
  - FERRIS / BICYCLE DOOR
  - SLIDING GLASS DOOR
  - FRENCH DOOR
  - METAL SECTIONAL GARAGE DOOR
  - STORAGE CLOSET
  - DECORATIVE NOTICELF
  - DECORATIVE ACCENT TILE
  - DECORATIVE VENT
  - PLASTER PARACET
  - EXPANSION JOINT
  - 3X RAKE BOARD
  - BRICK PATTERNS VENEER
- \* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



LEFT SIDE ELEVATION - ENHANCED WITH DECK



REAR ELEVATION

EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'D'

1/4"=1'-0"

NORTHWEST LAND PARK - THE TOWNS  
NORTHWEST LAND PARK, LLC



TYPICAL BUILDING MATERIALS

- INTEGRAL COLOR "G" TILE ROOF
  - INTEGRAL COLOR FLAT TILE ROOF
  - INTEGRAL COLOR PLASTER
  - 2X FASCIA
  - CORBEL
  - COLUMN
  - PLASTER FINISH TRIM
  - DECORATIVE PLASTER PROJECTION
  - PLASTER TRIM
  - VINYL FINISH
  - HANDS-CARD SHINGLE SIDING
  - HORIZONTAL SIDING
  - DECORATIVE SHUTTER
  - TRUSS
  - BEAM
  - RAFTER TAIL
  - RAIL / GUARDRAIL
  - METAL RAIL / GUARDRAIL
  - FIBERGLASS ENTRY DOOR
  - SLIP-RESIST GLASS DOOR
  - FRENCH DOOR
  - METAL SECTIONAL GARAGE DOOR
  - STORAGE CLOSET
  - DECORATIVE PLYSHELF
  - DECORATIVE ACCENT TILE
  - DECORATIVE VENT
  - PLASTER PANSCOT
  - EXPANSION JOINT
  - 2X RAFTER BOARD
  - BRICK PATTERN VENEER
- \* ALL METAL AND HOOD ELEMENTS TO BE PAINTED

<p>Date: July 12, 2010</p> <p>Preliminary 2007 CBC Review</p> <p>R-3 residential, type V-B (non-rated) construction, no sprinklers</p> <p>1 Occupancy</p> <p>a. Residential: R-3 Duplex and Single Family (Section 310.1)</p> <p>b. Private Garages (405.1), Group U accessory to R-3 occupancy</p> <p>2 Maximum Story Height</p> <p>a. Measured to average height of highest roof surface (502)</p> <p>b. Maximum height: 3 stories and 40' (Table 503)</p> <p>3 Maximum Building Area (Table 503)</p> <p>a. Unlimited</p> <p>4 Buildings on the Same Lot (704.3, Exception)</p> <p>a. Imaginary line assumed between duplex buildings and between single family R-3 areas unlimited (Table 503)</p> <p>5 Fire Resistance of Building Elements (Table 601)</p> <p>a. Structural Frame: 0 Hr</p> <p>b. Bearing Walls, Int &amp; Ext: 0</p> <p>c. Non-bearing walls and partitions (Table 601): 0</p> <p>d. Floor: 0</p> <p>e. Roof: 0</p> <p>6 Fire Resistance Rating Requirements for Exterior Walls based on Fire Separation Distance (Table 602)</p> <p>a. 0-x&lt;45' 1 hr</p> <p>b. x&lt;45' 0</p> <p>7 Projections (including Balconies)</p> <p>a. Maximum Projection: Max 12 inches where openings prohibited (704.2.2)</p> <p>b. Openings prohibited where x&lt;45' (Table 704.8)</p> <p>c. Projections allowed where x&lt;45'</p> <p>d. Combustible projections where openings are not permitted shall be 1-hour min (704.2.3)</p> <p>8 Maximum Area of Exterior Openings (Table 704.6, c&amp;g)</p> <p>a. X:fire separation distance</p> <table border="0"> <tr> <td>1. 0-x&lt;45'</td> <td>Not Permitted</td> </tr> <tr> <td>2. 3'-x&lt;45'</td> <td>25%</td> </tr> </table>	1. 0-x<45'	Not Permitted	2. 3'-x<45'	25%	<p>3. 3'-x&lt;45' Unlimited</p> <p>B. Unit/Occupancy Separations</p> <p>a. Bearing Walls</p> <p>i. Walls and Supporting Structure: 1 hour fire partitions (706.3, 706.4)</p> <p>ii. Floors and Supporting Structures: 1 hour (711.3, 711.4)</p> <p>b. Residential to Private Garage: per 405.1.4</p> <p>10 Egress</p> <p>a. Not required per 2007 CBC</p> <p>11 Stairs</p> <p>a. Stairs within individual units</p> <p>i. Min. Width (1009.1, exception 1): 36", occupant load &gt;=50</p> <p>ii. Max. Rise (1009.3.4) + 7.75"</p> <p>iii. Min. tread: 10"</p> <p>iv. Min. nosing tread width: 6"</p> <p>b. Landings</p> <p>i. Doors opening onto landing shall not reduce landing to less than one-half required width (1009.4.2)</p> <p>ii. In R-3, a floor or landing is not required at the top of an interior flight of stairs provided door does not swing over stairs (1009.4.3)</p> <p>c. Exit</p> <p>i. One exit required from each unit, Common Path of Egress Travel levels do not apply in R-3 (1019.2)</p> <p>d. Egress Court</p> <p>i. Width: 30' minimum (1024.5.1)</p> <p>12 Accessibility</p> <p>a. Duplexes and single family are exempt (1102A.1.2)</p> <p>b. Duplexes and single family are exempt from Multi-story Dwelling requirements (1102A.3.1)</p> <p>13 Yards or Courts (206)</p> <p>a. ISGE applies only to openings that provide required natural light or ventilation (206.1 and IBC Commentary)</p> <p>b. Yards or courts with openings that do not provide required natural light or ventilation do not need to comply with 1206 requirements</p> <p>c. Openings based on only 5 sides in a yard (202)</p> <p>d. 3 story yards with required openings shall be 4' minimum width (3 min + 1 story over 2'-3' + 1 x 13) (202.3)</p> <p>e. Yard minimum width shall be unobstructed from the ground to the sky (202)</p> <p>f. Openings based on 5 sides in a court (202)</p> <p>g. Note: There are no openings that provide required natural light or ventilation at courts</p>	
1. 0-x<45'	Not Permitted					
2. 3'-x<45'	25%					

CODE ANALYSIS

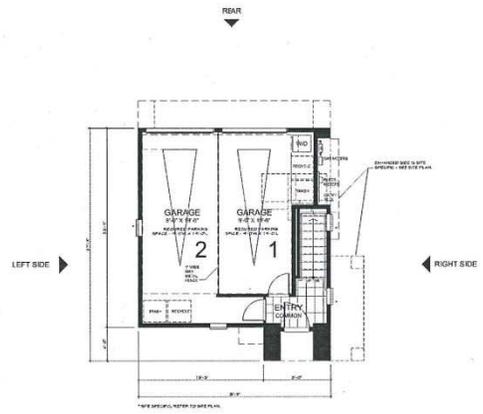
NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC



01/17/11



SECOND FLOOR



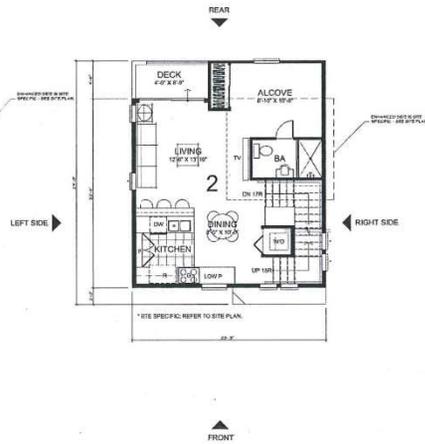
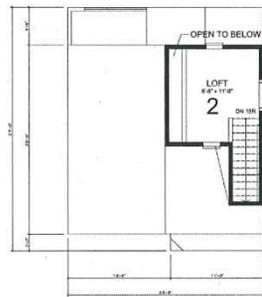
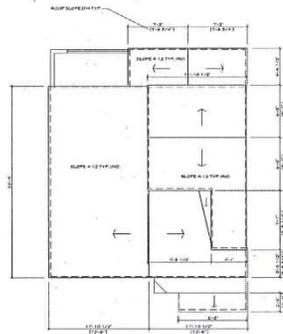
FIRST FLOOR

UNIT 1/2  
FIRST / SECOND FLOOR BUILDING PLANS  
ELEVATION STYLE - 'A'

NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC



AREAS	
UNIT 1	
SECOND FLOOR	420.00 S.F.
TOTAL LIVING AREA	420.00 S.F.
GARAGE 1	210.00 S.F.
DECK	42.00 S.F.
Unit Area	672.00 S.F.
UNIT 2	
SECOND FLOOR	571.11 S.F.
THIRD FLOOR	570.04 S.F.
LOFT	120.00 S.F.
TOTAL LIVING AREA	781.15 S.F.
Garage 2	210.00 S.F.
DECK	42.00 S.F.
Unit Area	1033.15 S.F.
COMMON AREA	
FIRST FLOOR	65.34 S.F.
SECOND FLOOR	46.08 S.F.
Off Common Area	118.37 S.F.
UNIT 3	
FIRST FLOOR	270.41 S.F.
SECOND FLOOR	562.02 S.F.
TOTAL LIVING AREA	832.43 S.F.
GARAGE	189.24 S.F.
DECK	77.00 S.F.
Unit Area	1178.74 S.F.
UNIT 4	
FIRST FLOOR	35.97 S.F.
SECOND FLOOR	772.22 S.F.
LOFT	120.00 S.F.
TOTAL LIVING AREA	898.19 S.F.
GARAGE	210.00 S.F.
DECK	42.00 S.F.
Unit Area	1380.39 S.F.



ROOF PLAN

LOFT LEVEL

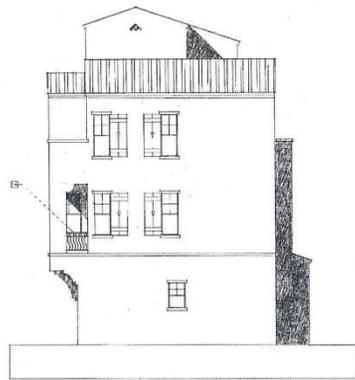
THIRD FLOOR

UNIT 1/2  
FIRST / THIRD FLOOR / LOFT LEVEL / ROOF PLAN  
ELEVATION STYLE - 'A'

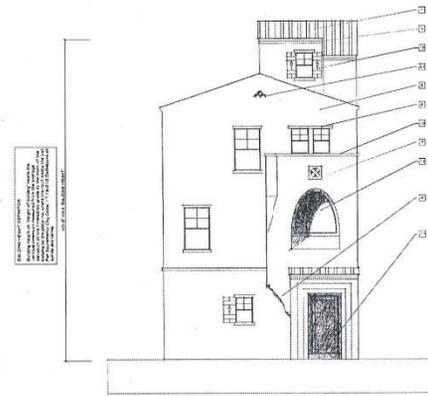
NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC



AREAS	
<b>UNIT 1</b>	
SECOND FLOOR	435.03 S.F.
TOTAL LIVING AREA	435.03 S.F.
STORAGE 1	218.42 S.F.
DECK	42.39 S.F.
<b>Total area</b>	<b>695.84 S.F.</b>
<b>UNIT 2</b>	
SECOND FLOOR	58.71 S.F.
THIRD FLOOR	870.84 S.F.
LOFT	100.00 S.F.
TOTAL LIVING AREA	1029.55 S.F.
STORAGE 2	249.97 S.F.
DECK	47.37 S.F.
<b>Total area</b>	<b>1527.47 S.F.</b>
<b>COMMON AREA</b>	
FIRST FLOOR	60.34 S.F.
SECOND FLOOR	48.88 S.F.
<b>Total common area</b>	<b>109.22 S.F.</b>
<b>UNIT 3</b>	
FIRST FLOOR	210.41 S.F.
SECOND FLOOR	960.03 S.F.
TOTAL LIVING AREA	1170.44 S.F.
STORAGE 3	189.24 S.F.
DECK	71.68 S.F.
<b>Total area</b>	<b>1731.74 S.F.</b>
<b>UNIT 4</b>	
FIRST FLOOR	36.87 S.F.
THIRD FLOOR	772.22 S.F.
LOFT	100.00 S.F.
TOTAL LIVING AREA	909.09 S.F.
STORAGE 4	279.79 S.F.
DECK	177.68 S.F.
<b>Total area</b>	<b>1366.56 S.F.</b>



LEFT SIDE ELEVATION



FRONT ELEVATION

UNIT 1/2  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

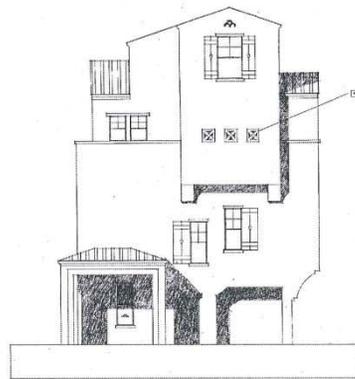
1/4"=1'-0"  
NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC



11.23.11

TYPICAL BUILDING MATERIALS

- INTERNAL COLOR 3" TILE ROOF
  - INTERNAL COLOR FLAT TILE ROOF
  - INTERNAL COLOR PLASTER
  - 2X FASCIA
  - 2X RAFTER BOARD
  - PLASTER CORBEL
  - PLASTER COLUMN
  - PLASTER WINDOW TRIM
  - DECORATIVE PLASTER PROJECTION
  - PLASTER TRIM
  - VINYL WINDOW
  - HORIZONTAL SINGLE SIDING
  - HORIZONTAL SIDING
  - BRICK PATTERN VENEER
  - DECORATIVE SHUTTER
  - METAL RAIL / GUARDRAIL
  - FIBERGLASS ENTRY DOOR
  - SLURRY GLASS DOOR
  - METAL SECTIONAL GARAGE DOOR
  - DECORATIVE FODDSHEL
  - DECORATIVE SAGEANT TILE
  - DECORATIVE VENT
  - HEAVY PLASTER TEXTURE
  - PLASTER PARASIT
  - BOARD AND BATTEN
- \* ALL METAL AND PIGGID ELEMENTS TO BE PAINTED



ENHANCED RIGHT SIDE ELEVATION



REAR ELEVATION

UNIT 1/2  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

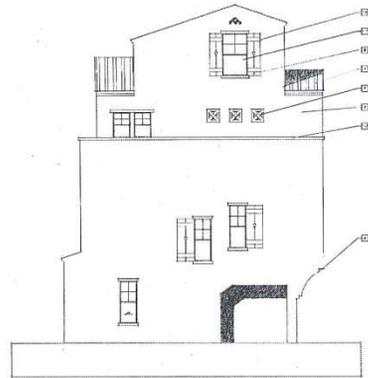
NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC



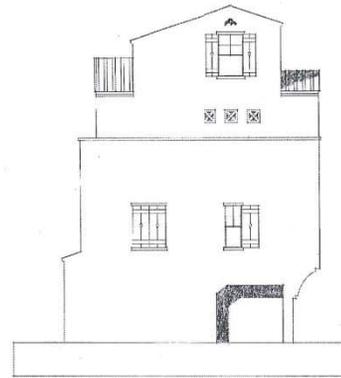
11.11.11

TYPICAL BUILDING MATERIALS

- INTEGRAL COLOR 'V' TILE ROOF
  - INTEGRAL COLOR FLAT TILE ROOF
  - INTEGRAL COLOR PLASTER
  - BK FASCHA
  - 3/4 INCH BOARD
  - PLASTER CORBEL
  - PLASTER COLUMN
  - PLASTER WINDOW TRIM
  - DECORATIVE PLASTER PROJECTION
  - PLASTER TRIM
  - WHITE WINDOW
  - HARDWOOD SINGLE SIDING
  - HORIZONTAL SIDING
  - BRICK PATTERN VENEER
  - DECORATIVE SHUTTER
  - METAL RAIL / GUARDRAIL
  - FIBERGLASS ENTRY DOOR
  - SLIP-RESISTANT DOOR
  - METAL SECTIONAL GARAGE DOOR
  - DECORATIVE FOOTWALL
  - DECORATIVE ACCENT TILE
  - DECORATIVE VENT
  - HEAVY PLASTER TEXTURE
  - PLASTER FINISHT
  - BOARD AND BATTEN
- \* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



RIGHT SIDE ELEVATION



RIGHT SIDE ELEVATION (SITE SPECIFIC)

UNIT 1/2  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

1/4" = 1'-0"

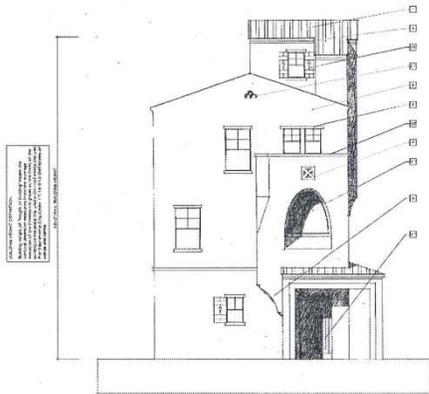
NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC



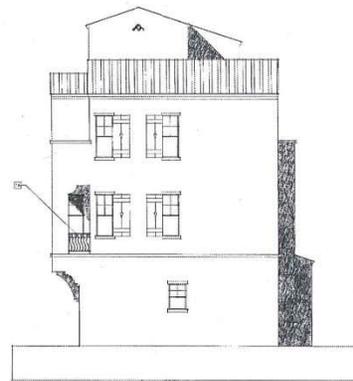
4.18.11

TYPICAL BUILDING MATERIALS

- INTERNAL COLOR 3" TILE ROOF
  - INTERNAL COLOR FLAT TILE ROOF
  - INTERNAL COLOR PLASTER
  - 2X FASGA
  - 2X WADE BOARD
  - PLASTER CORBEL
  - PLASTER COLUMN
  - PLASTER WINDOW TRIM
  - DECORATIVE PLASTER PROJECTION
  - PLASTER TRIM
  - VINYL WINDOW
  - HORIZONTAL SHINGLE SIDING
  - HORIZONTAL SIDING
  - BRICK PATTERN VENEER
  - DECORATIVE SHUTTER
  - METAL SAIL / GUARDRAIL
  - FIBERGLASS ENTRY DOOR
  - SLIDING GLASS DOOR
  - METAL SECTIONAL GARAGE DOOR
  - DECORATIVE POTSHIELD
  - DECORATIVE AGENT TILE
  - DECORATIVE VENT
  - HEAVY PLASTER TEXTURE
  - PLASTER PARASCOPY
  - BOARD AND BATTEN
- \* ALL METAL AND ROOF ELEMENTS TO BE PAINTED



FRONT ELEVATION WITH ENH. RIGHT SIDE



LEFT SIDE ELEVATION (SITE SPECIFIC)

UNIT 1/2  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

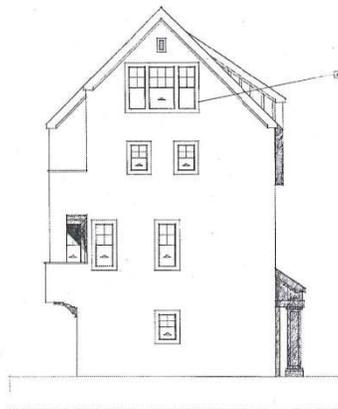
14'-0" 0"  
NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC



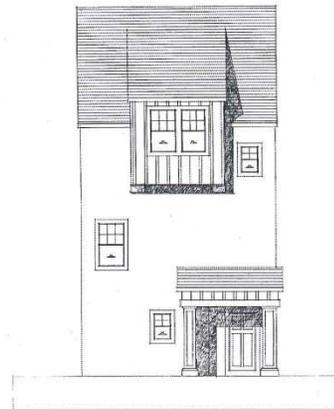
RL11

TYPICAL BUILDING MATERIALS	
1	INTERIOR COLOR 3" TILE ROOF
2	INTERIOR COLOR FLAT TILE ROOF
3	INTERIOR COLOR PLASTER
4	2" FASCIA
5	2" RAKE BOARD
6	PLASTER CORBEL
7	PLASTER COLUMN
8	PLASTER WINDOW TRIM
9	DECORATIVE PLASTER PROJECTION
10	PLASTER FIN
11	VINYL WINDOW
12	HANDSHOWN SINGLE SOUND
13	HORIZONTAL SIDING
14	BRICK PATTERN VENEER
15	DECORATIVE BUTTER
16	METAL RAIL / GUARDRAIL
17	FIBERGLASS ENTRY DOOR
18	SLURRY GLASS DOOR
19	METAL SECTIONAL GARAGE DOOR
20	DECORATIVE FOOTRAMP
21	DECORATIVE ACCENT TILE
22	DECORATIVE VENT
23	HEAVY PLASTER TEXTURE
24	PLASTER MANSOFT
25	BOARD AND BATTEN

\* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



LEFT SIDE ELEVATION



FRONT ELEVATION

NOTES:  
1. SEE ARCHITECTURAL SPECIFICATIONS FOR MATERIALS AND FINISHES.  
2. SEE ARCHITECTURAL SPECIFICATIONS FOR WINDOW AND DOOR SCHEDULES.  
3. SEE ARCHITECTURAL SPECIFICATIONS FOR ROOFING AND GUTTER SCHEDULES.  
4. SEE ARCHITECTURAL SPECIFICATIONS FOR PAINT SCHEDULES.  
5. SEE ARCHITECTURAL SPECIFICATIONS FOR LIGHTING SCHEDULES.  
6. SEE ARCHITECTURAL SPECIFICATIONS FOR MECHANICAL SCHEDULES.  
7. SEE ARCHITECTURAL SPECIFICATIONS FOR ELECTRICAL SCHEDULES.  
8. SEE ARCHITECTURAL SPECIFICATIONS FOR PLUMBING SCHEDULES.  
9. SEE ARCHITECTURAL SPECIFICATIONS FOR HEATING SCHEDULES.  
10. SEE ARCHITECTURAL SPECIFICATIONS FOR FINISHES SCHEDULES.

UNIT 1/2  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'

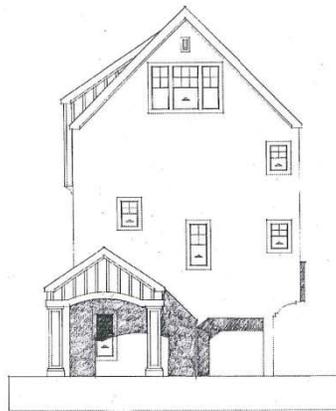
NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC



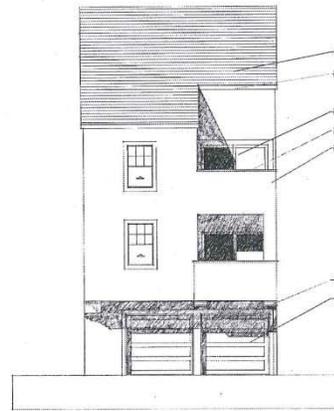
4/18/11

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERIOR COLOR 3" TILE ROOF
<input type="checkbox"/>	INTERIOR COLOR FLAT TILE ROOF
<input type="checkbox"/>	INTERIOR COLOR PLASTER
<input type="checkbox"/>	2X FASCIA
<input type="checkbox"/>	2X RAISE BOARD
<input type="checkbox"/>	PLASTER CORBEL
<input type="checkbox"/>	PLASTER COLUMN
<input type="checkbox"/>	PLASTER HINGED TRIM
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	HANDSHED SHINGLE SIDING
<input type="checkbox"/>	HORIZONTAL SIDING
<input type="checkbox"/>	BRICK PATTERN VENER
<input type="checkbox"/>	DECORATIVE ENTICE
<input type="checkbox"/>	METAL SAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIP-RESIST DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR
<input type="checkbox"/>	DECORATIVE POTSHIELD
<input type="checkbox"/>	DECORATIVE ACCENT TILE
<input type="checkbox"/>	DECORATIVE VENT
<input type="checkbox"/>	HEAVY PLASTER TEXTURE
<input type="checkbox"/>	PLASTER HANGBOT
<input type="checkbox"/>	BOARD AND BATTEN

\* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



ENHANCED RIGHT SIDE ELEVATION



REAR ELEVATION

UNIT 1/2  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'

1/4"=1'-0"

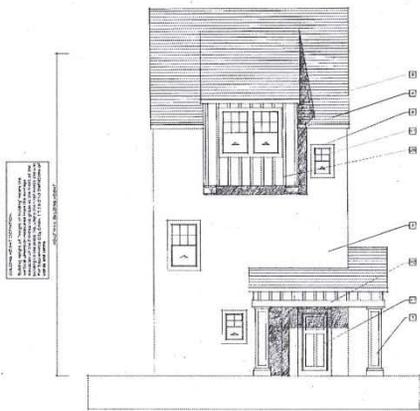
NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC

2018/07/29

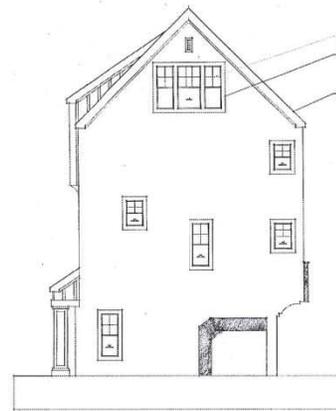


10.11.11

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR 3" TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR FLAT TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	2X FASGA
<input type="checkbox"/>	2X K&K BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER COLUMN
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	HANDSHOWN SHINGLE SIDING
<input type="checkbox"/>	HORIZONTAL SIDING
<input type="checkbox"/>	BRICK PATTERN VENEER
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR
<input type="checkbox"/>	DECORATIVE POTSHIELD
<input type="checkbox"/>	DECORATIVE ACCENT TILE
<input type="checkbox"/>	DECORATIVE VENT
<input type="checkbox"/>	HEAVY PLASTER TEXTURE
<input type="checkbox"/>	PLASTER PARASOFT
<input type="checkbox"/>	BOARD AND BATTEN
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



FRONT ELEVATION WITH ENH. RIGHT SIDE



RIGHT SIDE ELEVATION

1. INTEGRAL COLOR 3" TILE ROOF  
 2. INTEGRAL COLOR FLAT TILE ROOF  
 3. INTEGRAL COLOR PLASTER  
 4. 2X FASGA  
 5. 2X FASG BOARD  
 6. PLASTER CORNELL  
 7. PLASTER COLUMN  
 8. PLASTER WINDOW TRIM  
 9. DECORATIVE PLASTER PROJECTION  
 10. PLASTER TRIM  
 11. VINYL WINDOW  
 12. HARDWOOD SINGLE SING  
 HORIZONTAL SING

UNIT 1/2  
 EXTERIOR ELEVATIONS  
 ELEVATION STYLE - 'B'

1/4" = 1'-0"

NORTHWEST LAND PARK - URBAN COURTS  
 NORTHWEST LAND PARK, LLC

2010/07/20



10.21.11

TYPICAL BUILDING MATERIALS	
☐	INTEGRAL COLOR 3" TILE ROOF
☐	INTEGRAL COLOR FLAT TILE ROOF
☐	INTEGRAL COLOR PLASTER
☐	2X FASGA
☐	2X FASG BOARD
☐	PLASTER CORNELL
☐	PLASTER COLUMN
☐	PLASTER WINDOW TRIM
☐	DECORATIVE PLASTER PROJECTION
☐	PLASTER TRIM
☐	VINYL WINDOW
☐	HARDWOOD SINGLE SING
☐	HORIZONTAL SING
☐	BRCK. PATTERN VENER
☐	DECORATIVE BUTTER
☐	METAL RAIL / GUARDRA
☐	FIBERGLASS ENTRY DOOR
☐	SLURRING GLASS DOOR
☐	METAL SECTIONAL GARAGE DOOR
☐	DECORATIVE POTSHIELD
☐	DECORATIVE SILENT TILE
☐	DECORATIVE VENT
☐	HEAVY PLASTER TEXTURE
☐	PLASTER HANDSLOT
☐	BOARD AND BATTEN
*ALL METAL AND WOOD ELEMENTS TO BE PAINTED	





ENHANCED RIGHT SIDE ELEVATION



REAR ELEVATION

UNIT 1/2  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

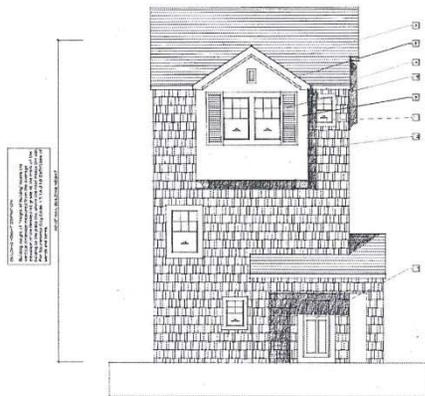
NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC



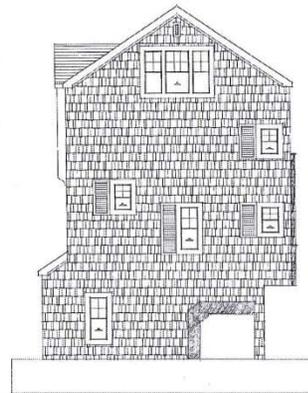
11.21.11

TYPICAL BUILDING MATERIALS

- INTERNAL COLOR 'W' TILE ROOF
  - INTERNAL COLOR FLAT TILE ROOF
  - INTERNAL COLOR PLASTER
  - 2X FASION
  - 2X SHIMS BOARD
  - PLASTER CORNICE
  - PLASTER COLUMN
  - PLASTER HINGED TRIM
  - DECORATIVE PLASTER PROJECTION
  - PLASTER TRIM
  - VINYL WINDOW
  - HANDSCRAVED SHINGLE SIDING
  - HORIZONTAL SIDING
  - BRICK PATTERN VENEER
  - DECORATIVE SHUTTER
  - METAL RAIL / GUARDRAIL
  - FIBERGLASS ENTRY DOOR
  - SLIP-RESISTANT ROOF
  - METAL SECTIONAL GARAGE DOOR
  - DECORATIVE POT/HELF
  - DECORATIVE ACCENT TILE
  - DECORATIVE VENT
  - HEAVY PLASTER TEXTURE
  - PLASTER HANDSCOT
  - BOARD AND BATTEN
- \* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



FRONT ELEVATION WITH ENH. RIGHT SIDE



RIGHT SIDE ELEVATION

UNIT 1/2  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC

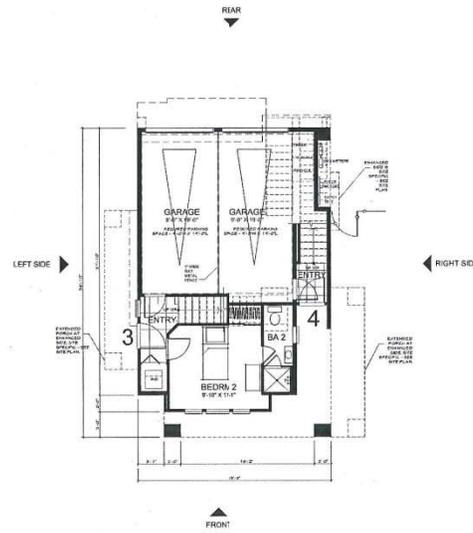


01.20.11

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERIAL COLOR 3" TILE ROOF
<input type="checkbox"/>	INTERIAL COLOR FLAT TILE ROOF
<input type="checkbox"/>	INTERIAL COLOR PLASTER
<input type="checkbox"/>	2X FASGA
<input type="checkbox"/>	2X K RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER COLUMN
<input type="checkbox"/>	PLASTER FINISH TRIM
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	VINYL FINISH
<input type="checkbox"/>	HANDSCRAPE SHINGLE SIDING
<input type="checkbox"/>	HORIZONTAL SIDING
<input type="checkbox"/>	BRICK PATTERN VENEER
<input type="checkbox"/>	DECORATIVE BUTTER
<input type="checkbox"/>	METAL RAIL GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR
<input type="checkbox"/>	DECORATIVE POTSHIELD
<input type="checkbox"/>	DECORATIVE AGENT TILE
<input type="checkbox"/>	DECORATIVE VENT
<input type="checkbox"/>	HEAVY PLASTER TEXTURE
<input type="checkbox"/>	PLASTER HANDSOT
<input type="checkbox"/>	BOARD AND BATTEN
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



SECOND FLOOR



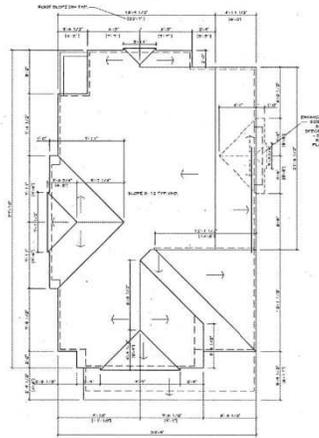
FIRST FLOOR

UNIT 3/4  
FIRST / SECOND FLOOR BUILDING PLANS  
ELEVATION STYLE - 'D'

NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC



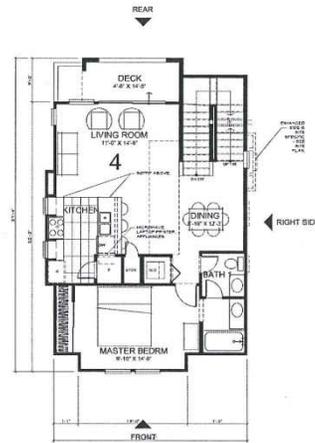
AREAS	
<b>UNIT 1</b>	
SECOND FLOOR	430.88 S.F.
TOTAL LIVING AREA	430.88 S.F.
GARAGE 1	72.42 S.F.
DECK	42.36 S.F.
<b>total area</b>	<b>545.66 S.F.</b>
<b>UNIT 2</b>	
SECOND FLOOR	807.8 S.F.
THIRD FLOOR	576.04 S.F.
LOFT	300.00 S.F.
TOTAL LIVING AREA	1683.84 S.F.
DECK	242.87 S.F.
DECK	27.37 S.F.
<b>total area</b>	<b>2554.08 S.F.</b>
<b>COMMON AREA</b>	
FIRST FLOOR	95.24 S.F.
SECOND FLOOR	48.88 S.F.
<b>total common area</b>	<b>144.12 S.F.</b>
<b>UNIT 3</b>	
FIRST FLOOR	370.47 S.F.
SECOND FLOOR	567.55 S.F.
TOTAL LIVING AREA	938.02 S.F.
DECK	72.24 S.F.
DECK	77.68 S.F.
<b>total area</b>	<b>1188.94 S.F.</b>
<b>UNIT 4</b>	
FIRST FLOOR	78.87 S.F.
THIRD FLOOR	722.22 S.F.
LOFT	166.44 S.F.
TOTAL LIVING AREA	967.53 S.F.
DECK	239.74 S.F.
DECK	177.77 S.F.
<b>total area</b>	<b>1385.04 S.F.</b>



ROOF PLAN



LOFT LEVEL



THIRD FLOOR

UNIT 3/4  
FIRST / THIRD FLOOR / LOFT LEVEL / ROOF PLAN  
ELEVATION STYLE - 'D'

NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC



AREAS	
<b>UNIT 1</b>	
SECOND FLOOR	432.83 S.F.
TOTAL LIVING AREA	438.85 S.F.
GARAGE	274.43 S.F.
DECK	42.38 S.F.
Other area	898.44 S.F.
<b>UNIT 2</b>	
SECOND FLOOR	55.71 S.F.
THIRD FLOOR	576.04 S.F.
LOFT	120.38 S.F.
TOTAL LIVING AREA	756.13 S.F.
GARAGE	249.87 S.F.
DECK	42.37 S.F.
Other area	1027.27 S.F.
<b>COMMON AREA</b>	
FIRST FLOOR	66.34 S.F.
SECOND FLOOR	45.09 S.F.
Other common area	778.32 S.F.
<b>UNIT 3</b>	
FIRST FLOOR	226.41 S.F.
SECOND FLOOR	441.01 S.F.
TOTAL LIVING AREA	667.42 S.F.
GARAGE	181.81 S.F.
DECK	27.52 S.F.
Other area	1710.74 S.F.
<b>UNIT 4</b>	
FIRST FLOOR	26.87 S.F.
THIRD FLOOR	775.21 S.F.
LOFT	150.49 S.F.
TOTAL LIVING AREA	952.57 S.F.
GARAGE	229.75 S.F.
DECK	77.68 S.F.
Other area	1500.38 S.F.



ENHANCED LEFT SIDE ELEVATION



FRONT ELEVATION

SEE ELEVATION NOTES FOR MATERIALS AND FINISHES. ALL MATERIALS AND FINISHES TO BE PAINTED UNLESS OTHERWISE NOTED.

UNIT 3/4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'D'

1/4" = 1'-0"

NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC

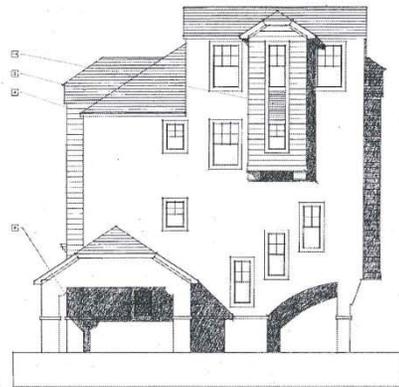
2018081702



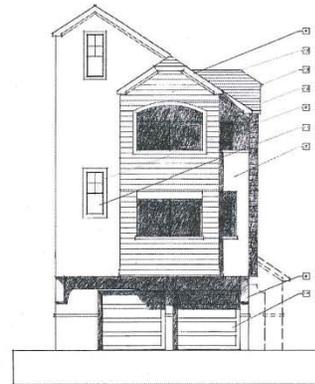
02.21.11

TYPICAL BUILDING MATERIALS

- INTERIOR COLOR TILE ROOF
  - INTERIOR COLOR FLAT TILE ROOF
  - INTERIOR COLOR PLASTER
  - SF FASGA
  - 2X RAWE BOARD
  - PLASTER CORBEL
  - PLASTER COLUMN
  - PLASTER WINDOW TRIM
  - DECORATIVE PLASTER PROJECTION
  - PLASTER TRIM
  - VINYL WINDOW
  - HANGBOARD SINGLE SIDING
  - HORIZONTAL SIDING
  - BRICK PATTERN VENEER
  - DECORATIVE BUTTER
  - METAL RAIL / GUARDRAIL
  - FIBERGLASS ENTRY DOOR
  - SLIDING GLASS DOOR
  - METAL SECTIONAL GARAGE DOOR
  - DECORATIVE POTSHELF
  - DECORATIVE ACCENT TILE
  - DECORATIVE VENT
  - HEAVY PLASTER TEXTURE
  - PLASTER HANGSOFF
  - BOARD AND BATTEN
- \*ALL METAL AND FOOD ELEMENTS TO BE PAINTED



ENHANCED RIGHT SIDE ELEVATION



REAR ELEVATION

UNIT 3/4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'D'

1/4" = 1'-0"  
NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC



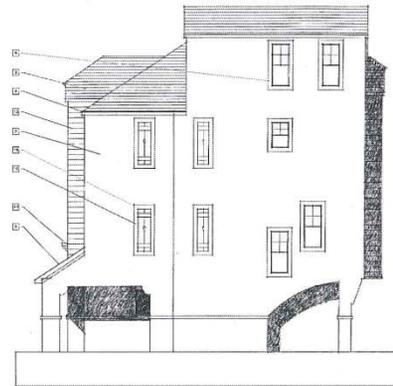
HLN.11

TYPICAL BUILDING MATERIALS

- 1 INTEGRAL COLOR 3" TILE ROOF
  - 2 INTEGRAL COLOR FLAT TILE ROOF
  - 3 INTEGRAL COLOR PLASTER
  - 4 2" FASCIA
  - 5 1/2" RAISE BOUND
  - 6 PLASTER CORREL
  - 7 PLASTER COLVIN
  - 8 PLASTER HINDOR TANK
  - 9 DECORATIVE PLASTER PROJECTION
  - 10 PLASTER TANK
  - 11 VINYL WINDOW
  - 12 HARDBOARD SHINGLE SIDING
  - 13 HORIZONTAL SIDING
  - 14 BRICK PATTERNS VENEER
  - 15 DECORATIVE SHUTTER
  - 16 METAL RAIL / GUARDRAIL
  - 17 FIBERGLASS ENTRY DOOR
  - 18 GLASS-GLASS DOOR
  - 19 METAL SECTIONAL GARAGE DOOR
  - 20 DECORATIVE POTTSHELF
  - 21 DECORATIVE AGENT TILE
  - 22 DECORATIVE VENT
  - 23 HEAVY PLASTER TEXTURE
  - 24 PLASTER MANEGOT
  - 25 BOARD AND BATTEN
- \* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



LEFT SIDE ELEVATION



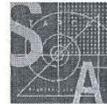
RIGHT SIDE ELEVATION

UNIT 3/4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'D'

2014-10-01

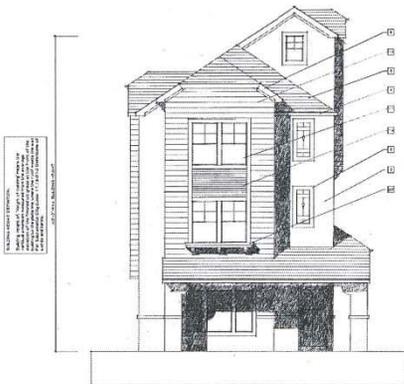
NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC

2014-10-01



10.11.14

TYPICAL BUILDING MATERIALS	
☐	INTERIOR COLOR TILE ROOF
☐	INTERIOR COLOR FLAT TILE ROOF
☐	INTERIOR COLOR PLASTER
☐	2X FASGA
☐	2X RAKE BOARD
☐	PLASTER CORNICE
☐	PLASTER COLUMN
☐	PLASTER RIBBON TRIM
☐	DECORATIVE PLASTER PROJECTION
☐	PLASTER TRIM
☐	VINYL RIBBON
☐	HANDSHAPED SHINGLE SIDING
☐	HORIZONTAL SIDING
☐	BRICK PATTERN VENEER
☐	DECORATIVE BUTTER
☐	METAL RAIL / GUARDRAIL
☐	FIBERGLASS ENTRY DOOR
☐	SLIDING GLASS DOOR
☐	METAL SECTIONAL GARAGE DOOR
☐	DECORATIVE POTSWELL
☐	DECORATIVE ACCENT TILE
☐	DECORATIVE VENT
☐	HEAVY PLASTER TEXTURE
☐	PLASTER HANDSOFF
☐	BOARD AND BATTEN
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



1. ALL METAL AND WOOD ELEMENTS TO BE PAINTED  
 2. ALL METAL AND WOOD ELEMENTS TO BE PAINTED  
 3. ALL METAL AND WOOD ELEMENTS TO BE PAINTED  
 4. ALL METAL AND WOOD ELEMENTS TO BE PAINTED  
 5. ALL METAL AND WOOD ELEMENTS TO BE PAINTED  
 6. ALL METAL AND WOOD ELEMENTS TO BE PAINTED  
 7. ALL METAL AND WOOD ELEMENTS TO BE PAINTED  
 8. ALL METAL AND WOOD ELEMENTS TO BE PAINTED

FRONT ELEVATION WITH ENH. RIGHT SIDE

UNIT 3/4  
 EXTERIOR ELEVATIONS  
 ELEVATION STYLE - "D"

NORTHWEST LAND PARK - URBAN COURTS  
 NORTHWEST LAND PARK, LLC



01.21.11

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR "T" TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR FLAT TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	2X FASGA
<input type="checkbox"/>	3/4 INCH BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER COLUMN
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	HANDSHAPED SINGLE SIDING
<input type="checkbox"/>	HORIZONTAL SIDING
<input type="checkbox"/>	BIRCH PATTERN VENEER
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	METAL RAIL / RUMORAL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLURRY GLASS DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR
<input type="checkbox"/>	DECORATIVE POST/HELP
<input type="checkbox"/>	DECORATIVE AGENT TILE
<input type="checkbox"/>	DECORATIVE VENT
<input type="checkbox"/>	HEAVY PLASTER TEXTURE
<input type="checkbox"/>	PLASTER HANDSOT
<input type="checkbox"/>	BOARD AND BATTEN

\*ALL METAL AND WOOD ELEMENTS TO BE PAINTED



LEFT SIDE ELEVATION



FRONT ELEVATION

NOTES:  
1. SEE PLAN FOR WINDOW AND DOOR SIZES.  
2. SEE PLAN FOR ROOF PITCH.  
3. SEE PLAN FOR ROOF MATERIAL.  
4. SEE PLAN FOR EXTERIOR FINISHES.  
5. SEE PLAN FOR INTERIOR FINISHES.  
6. SEE PLAN FOR LIGHTING FIXTURES.  
7. SEE PLAN FOR FLOORING MATERIALS.  
8. SEE PLAN FOR CEILING MATERIALS.  
9. SEE PLAN FOR WALL MATERIALS.  
10. SEE PLAN FOR TRIM MATERIALS.  
11. SEE PLAN FOR PAINT SCHEDULE.

UNIT 3/4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

1st of 1st  
**NORTHWEST LAND PARK - URBAN COURTS**  
NORTHWEST LAND PARK, LLC

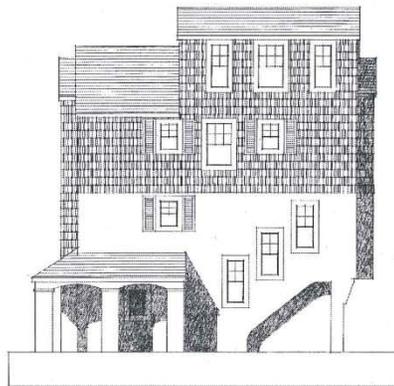


03.11

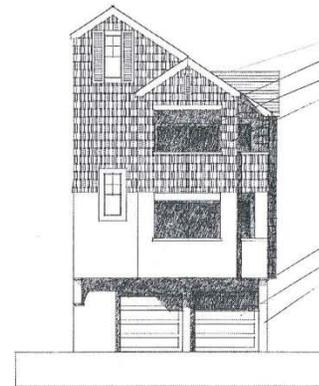
TYPICAL BUILDING MATERIALS

- INTERIOR COLOR TILE ROOF
- INTERIOR COLOR PLASTER
- 2X FASGA
- 2X RAKE BOARD
- PLASTER CORBEL
- PLASTER COLUMN
- PLASTER WINDOW TRIM
- DECORATIVE PLASTER PROJECTION
- PLASTER TANK
- VINYL SIDING
- HARBORNEED SHINGLE SIDING
- HORIZONTAL SIDING
- BRICK PATTERN VENEER
- DECORATIVE SHUTTER
- METAL RAIL / GUARDRAIL
- FIBERGLASS ENTRY DOOR
- SLIP-RESISTANT DOOR
- METAL SECTIONAL GARAGE DOOR
- DECORATIVE POTTSHELF
- DECORATIVE ACCENT TILE
- DECORATIVE LENT
- HEAVY PLASTER TEXTURE
- PLASTER HANDSOT
- BOARD AND BATTEN

\* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



ENHANCED RIGHT SIDE ELEVATION



REAR ELEVATION

UNIT 3/4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC



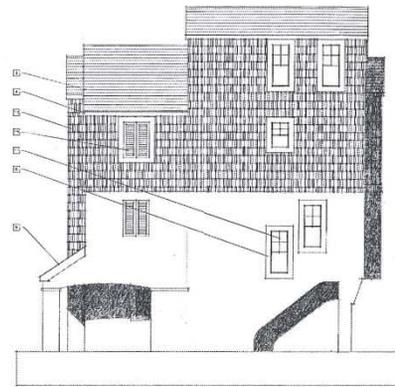
4/28/11

TYPICAL BUILDING MATERIALS

- INTEGRAL COLOR TILE ROOF
  - INTEGRAL COLOR FLAT TILE ROOF
  - INTEGRAL COLOR PLASTER
  - 2X FASGA
  - 2X PLANK BOARD
  - PLASTER CORSEL
  - PLASTER COLUMN
  - PLASTER WINDOW TRIM
  - DECORATIVE PLASTER PROJECTION
  - PLASTER TRIM
  - VINYL WINDOW
  - HANDICAPPED SINGLE SLOPE
  - HORIZONTAL SIDING
  - BRICK PATTERN VENEER
  - DECORATIVE BUTTER
  - METAL RAIL / GUARDRAIL
  - FIBERGLASS ENTRY DOOR
  - SLIDING GLASS DOOR
  - METAL SECTIONAL GARAGE DOOR
  - DECORATIVE POTSHIELD
  - DECORATIVE SAGEANT TILE
  - DECORATIVE VENT
  - HEAVY PLASTER TEXTURE
  - PLASTER HANGSOFF
  - BOARD AND BATTEN
- \* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



FRONT ELEVATION WITH ENH. RIGHT SIDE



RIGHT SIDE ELEVATION

UNIT 3/4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

DATE: 06/10/10  
**NORTHWEST LAND PARK - URBAN COURTS**  
NORTHWEST LAND PARK, LLC

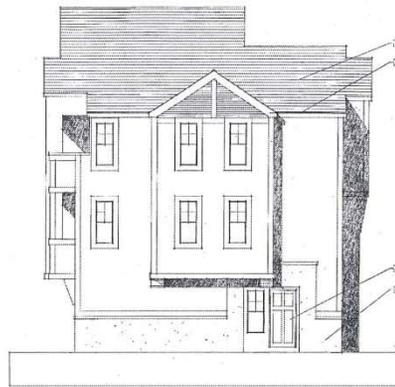


06.10.10

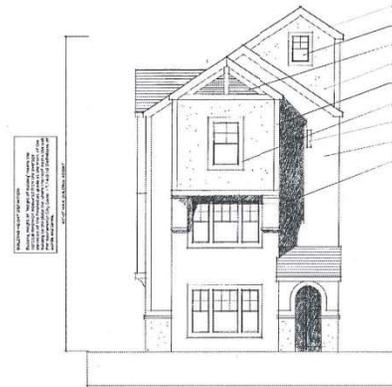
TYPICAL BUILDING MATERIALS

- INTEGRAL COLOR 3" TILE ROOF
- INTEGRAL COLOR FLAT TILE ROOF
- INTEGRAL COLOR PLASTER
- 2X PASCAL
- 2X WADE BOARD
- PLASTER CORBEL
- PLASTER COLUMN
- PLASTER WINDOW TRIM
- DECORATIVE PLASTER PROJECTION
- PLASTER TRIM
- VINYL WINDOW
- HORIZONTAL SINGLE SIDING
- HORIZONTAL SIDING
- BRICK PATTERN VENEER
- DECORATIVE SHUTTER
- METAL SAIL / GUARDRAIL
- SLIDING GLASS DOOR
- METAL SECTIONAL GARAGE DOOR
- DECORATIVE POTSHIELD
- DECORATIVE ACCENT TILE
- DECORATIVE VENT
- HEAVY PLASTER TEXTURE
- PLASTER PARAPET
- BOARD AND BATTEN

\* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



LEFT SIDE ELEVATION



FRONT ELEVATION

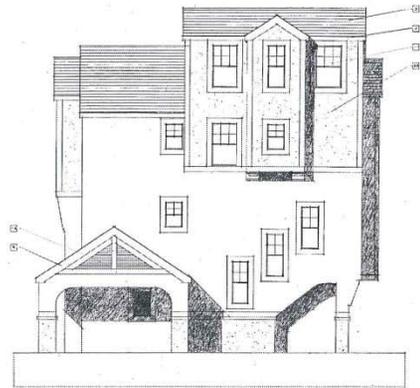
NOT TO SCALE  
 ALL DIMENSIONS ARE TO FACE UNLESS NOTED OTHERWISE  
 ALL MATERIALS AND FINISHES TO BE DETERMINED BY THE ARCHITECT  
 ALL MATERIALS AND FINISHES TO BE DETERMINED BY THE ARCHITECT  
 ALL MATERIALS AND FINISHES TO BE DETERMINED BY THE ARCHITECT

UNIT 3/4  
 EXTERIOR ELEVATIONS  
 ELEVATION STYLE - 'E'  
 1/8" = 1'-0"  
**NORTHWEST LAND PARK - URBAN COURTS**  
 NORTHWEST LAND PARK, LLC

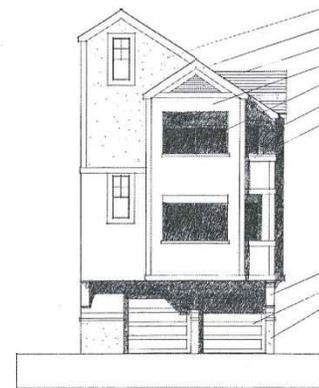


08.21.11

TYPICAL BUILDING MATERIALS	
1	INTERNAL COLOR TILE ROOF
2	INTERNAL COLOR FLAT TILE ROOF
3	INTERNAL COLOR PLASTER
4	2X PANDA
5	2X RAKE BOARD
6	PLASTER CORBEL
7	PLASTER COLUMN
8	PLASTER WINDOW TRIM
9	DECORATIVE PLASTER PROJECTION
10	PLASTER TRIM
11	WYLLI WINDOW
12	HARDBOARD SINGLE SING
13	HORIZONTAL SING
14	BRICK PATTER VENEER
15	DECORATIVE SHUTTER
16	METAL RAIL / GUARDRAIL
17	FIBERGLASS ENTRY DOOR
18	SLIDING GLASS DOOR
19	METAL SECTIONAL GARAGE DOOR
20	DECORATIVE POTTERY
21	DECORATIVE ACCENT TILE
22	DECORATIVE VENT
23	HEAVY PLASTER TEXTURE
24	PLASTER MANSCOT
25	BOARD AND BATTEN
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



ENHANCED RIGHT SIDE ELEVATION



REAR ELEVATION

UNIT 3/4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - "E"

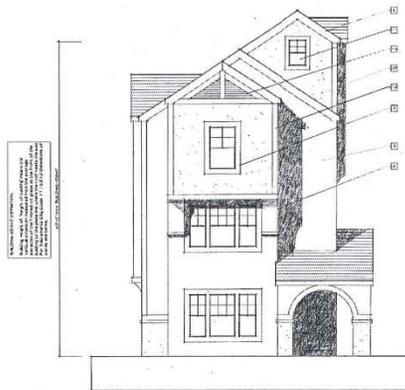
NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC



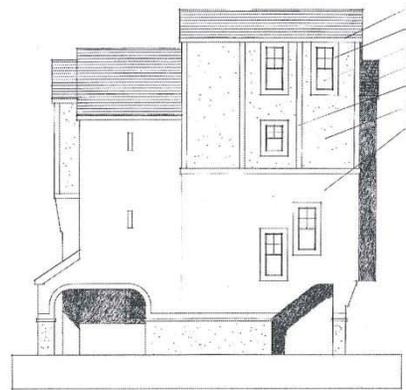
01.24.11

TYPICAL BUILDING MATERIALS

- 1 INTERIOR COLOR 3" TILE ROOF
  - 2 INTERIOR COLOR FLAT TILE ROOF
  - 3 INTERIOR COLOR PLASTER
  - 4 2X FASCIA
  - 5 2X WADE BOARD
  - 6 PLASTER CORBEL
  - 7 PLASTER COLUMN
  - 8 PLASTER WINDOW TRIM
  - 9 DECORATIVE PLASTER PROJECTION
  - 10 PLASTER TRIM
  - 11 VINYL WINDOW
  - 12 HANGING/SHINGLE SIDING
  - 13 HORIZONTAL SIDING
  - 14 BRICK PATTERN VENEER
  - 15 DECORATIVE SHUTTER
  - 16 METAL RAIL / GUARDRAIL
  - 17 SLURRY GLASS DOOR
  - 18 METAL SECTIONAL GARAGE DOOR
  - 19 DECORATIVE POTSHELF
  - 20 DECORATIVE SILLING TILE
  - 21 DECORATIVE VENT
  - 22 HEAVY PLASTER TEXTURE
  - 23 PLASTER MANHOLE
  - 24 BOARD AND BATTEN
- \*ALL METAL AND WOOD ELEMENTS TO BE PAINTED



FRONT ELEVATION WITH ENH. RIGHT SIDE



RIGHT SIDE ELEVATION

UNIT 3/4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'E'

NORTHWEST LAND PARK - URBAN COURTS  
NORTHWEST LAND PARK, LLC



4.25.11

TYPICAL BUILDING MATERIALS

- ☐ INTEGRAL COLOR TILE ROOF
  - ☐ INTEGRAL COLOR FLAT TILE ROOF
  - ☐ INTEGRAL COLOR PLASTER
  - ☐ 2X PASCAL
  - ☐ 2X RAISE BOARD
  - ☐ PLASTER CORNICE
  - ☐ PLASTER COLUMN
  - ☐ PLASTER FINISH TRIM
  - ☐ DECORATIVE PLASTER PROJECTION
  - ☐ PLASTER TRIM
  - ☐ WALL FINISH
  - ☐ HANDBOARDED SHINGLE SIDING
  - ☐ HORIZONTAL SIDING
  - ☐ BRICK PATTERNS - VENER
  - ☐ DECORATIVE SHUTTER
  - ☐ METAL RAIL / BOARDWALK
  - ☐ FIBERGLASS ENTRY DOOR
  - ☐ SLUING GLASS ROOF
  - ☐ METAL SECTIONAL GARAGE DOOR
  - ☐ DECORATIVE POST/RAIL
  - ☐ DECORATIVE ACCENT TILE
  - ☐ DECORATIVE VENT
  - ☐ HEAVY PLASTER TEXTURE
  - ☐ PLASTER MANICOT
  - ☐ BOARD AND BATTEN
- \*ALL METAL AND HOOD ELEMENTS TO BE PAINTED

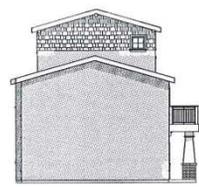




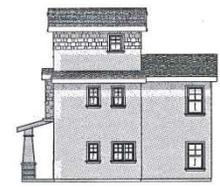
**MOGAVERO  
ARCHITECTS  
ASSOCIATES**

3012 K STREET  
SACRAMENTO  
CA 95811  
P 916.443.1000  
F 916.443.7234  
MOGAVERO.COM

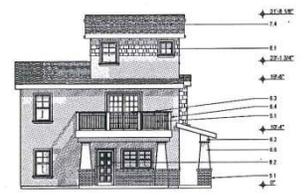
Architecture | Planning | Urban Design



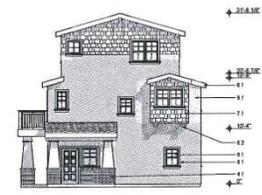
1 EAST ELEVATION  
SCALE 1/8" = 1'-0"



2 NORTH ELEVATION  
SCALE 1/8" = 1'-0"



3 SOUTH ELEVATION  
SCALE 1/8" = 1'-0"



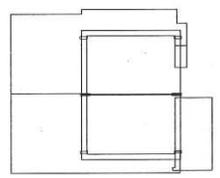
4 WEST ELEVATION  
SCALE 1/8" = 1'-0"

**EXTERIOR ELEVATION KEYNOTES:**

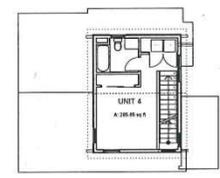
- |  |   |
|--|---|
| <p><b>CONCRETE</b></p> <ul style="list-style-type: none"> <li>01 EXPOSED CONCRETE</li> </ul> <p><b>MASONRY</b></p> <ul style="list-style-type: none"> <li>01 VENEER BRICK</li> </ul> <p><b>METALS</b></p> <ul style="list-style-type: none"> <li>01 FINISHED METAL FLASHING</li> <li>02 METAL LOUVER</li> </ul> <p><b>WOOD &amp; PLASTIC</b></p> <ul style="list-style-type: none"> <li>01 FINISHED WOOD TRIM</li> <li>02 DECORATIVE WOOD BEAM</li> <li>03 PAINTED WOOD BALCONY RAIL</li> <li>04 FINISH WOOD BALCONY FIBER</li> <li>05 PAINTED WOOD OR VINYL SHUTTER</li> <li>06 PAINTED DECORATIVE WOOD COLUMN</li> </ul> | <p><b>TERRAZZO &amp; MOUNTING PROTECTION</b></p> <ul style="list-style-type: none"> <li>71 FIBER CEMENT SHIMMED WOOD SHINGLE</li> <li>72 FIBER CEMENT FIBERGLASS BOND</li> <li>73 FIBER CEMENT TRIM</li> <li>74 ASPHALT FIBERGLASS SHAKE MATTING</li> <li>75 CUSHY ROOF FLEE (SHIMMED BARREL TILES)</li> </ul> <p><b>GLASS</b></p> <ul style="list-style-type: none"> <li>01 VINYL WINDOW</li> <li>02 PAINTED METAL GLASS DOOR</li> <li>03 PAINTED METAL GLASS DOOR</li> </ul> <p><b>FINISHES</b></p> <ul style="list-style-type: none"> <li>01 INSULATED PLASTER FINISH - ELASTOMERIC</li> <li>02 METHACRYLIC COLOR</li> </ul> |
|--|---|

**AREA CALCULATIONS**

UNIT 3	740F
PORCH 3	820F
UNIT 4	1200F
PORCH 4	220F
BALCONY	350F



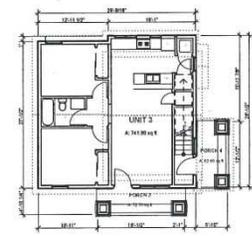
5 Roof  
SCALE 1/8" = 1'-0"



6 3rd Floor  
SCALE 1/8" = 1'-0"



7 2nd FLOOR  
SCALE 1/8" = 1'-0"



8 1st FLOOR  
SCALE 1/8" = 1'-0"

PROJECT TITLE  
NO. 200402

Cottage Plan 3 & 4  
Version 2  
RStreet  
RCity

Jim Muzar  
Northwest Land Park LLC

SHEET DESCRIPTION

Presentation

PROJECT PHASE  
Schematic Design

March 3, 2011

REVISIONS


SHEET NUMBER

**A-002.1**  
Cottage 2A V2  
NO. 200402

Revised: 03/03/2011 09:00 AM 1/1 Sheet Cottage Plan 3 & 4

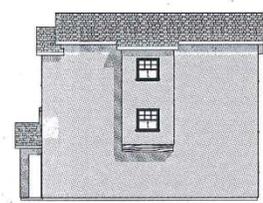




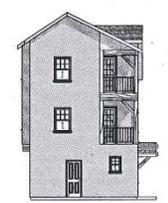


**McGAVERO  
NOTSTINE  
ASSOCIATES**  
2524 K STREET  
SACRAMENTO  
CA 95811  
P 916.442.1022  
F 916.442.7234  
MCGAVERO.COM

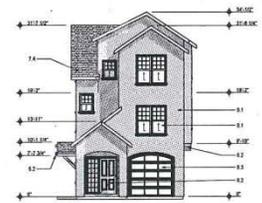
Architecture | Planning | Urban Design



1 EAST ELEVATION  
SCALE 1/8" = 1'-0"



2 NORTH ELEVATION  
SCALE 1/8" = 1'-0"



3 SOUTH ELEVATION  
SCALE 1/8" = 1'-0"



4 WEST ELEVATION  
SCALE 1/8" = 1'-0"

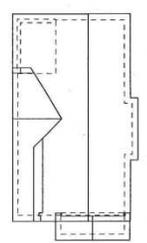
**EXTERIOR ELEVATION KEYNOTES:**

- CONCRETE**  
C1 EXPOSED CONCRETE  
C2 VENEER BRICK
- WOODS & PANELING**  
W1 PAINTED METAL RAILING  
W2 METAL LOUVER  
W3 PAINTED WOOD TRIM  
W4 OCCUPANCY WOOD TRIM  
W5 PAINTED WOOD RAILING CAP  
W6 PAINTED WOOD RAILING POST  
W7 PAINTED WOOD OR UNPAINTED  
W8 PAINTED OCCUPANCY WOOD COLUMN

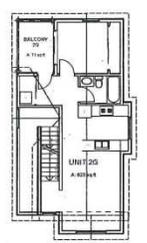
- TRIMM & MOISTURE PROTECTION**  
T1 FIBER CEMENT SIMULATED WOOD SHINGLE  
T2 FIBER CEMENT HORIZONTAL BOARD  
T3 FIBER CEMENT TRIM  
T4 ASPHALT SHINGLES (SHAKE PATTERNS)  
T5 CONCRETE ROOF TILE (SIMULATED MARBLE TILE)
- FINISHES**  
F1 UNPAINTED  
F2 PAINTED METAL CLAD DOOR  
F3 PAINTED METAL GARAGE DOOR  
F4 INSULATED PLASTER FINISH (GLASS BLOCK WITH METALLIC COLOR)

**AREA CALCULATIONS**

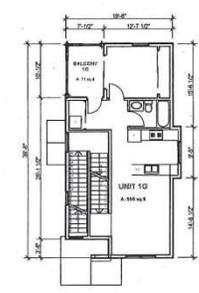
UNIT 10	81M <sup>2</sup>
PORCH 10	27M <sup>2</sup>
BALCONY 10	71M <sup>2</sup>
UNIT 20	87M <sup>2</sup>
PORCH 20	29M <sup>2</sup>
BALCONY 20	71M <sup>2</sup>
GARAGE 10 & 20	42M <sup>2</sup>



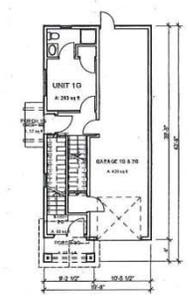
5 Roof  
SCALE 1/8" = 1'-0"



6 3rd FLOOR  
SCALE 1/8" = 1'-0"



7 2nd FLOOR  
SCALE 1/8" = 1'-0"



8 1st FLOOR  
SCALE 1/8" = 1'-0"

**PROJECT TITLE**

COTTAGE PLAN 1G/2G

Version 1

#Street

#City

Jim Muzar  
Northwest Land Park LLC

**SHEET DESCRIPTION**

Presentation

**PROJECT PHASE**

Schematic Design

March 11, 2011

**REVISIONS**

**SHEET NUMBER**

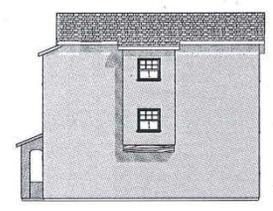
A-005.1

COTTAGE PLAN 1000  
NO. 000042

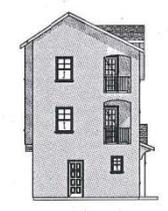


**MOOAVERO  
NOTEBYNE  
ASSOCIATES**  
3024 S STREET  
SACRAMENTO  
CA 95811  
P 916 442 1032  
F 916 442 1204  
MOONOT.COM

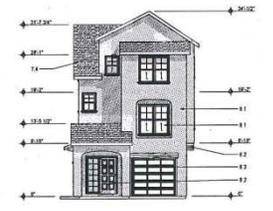
Architecture | Planning | Urban Design



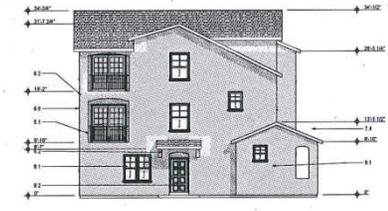
1 EAST ELEV  
SCALE 1/8" = 1'-0"



2 NORTH ELEV  
SCALE 1/8" = 1'-0"



3 SOUTH ELEV  
SCALE 1/8" = 1'-0"



4 WEST ELEV  
SCALE 1/8" = 1'-0"

**EXTERIOR ELEVATION KEYNOTES:**

- CONCRETE**  
C1 EXPOSED CONCRETE
- BRICKWORK**  
K1 VENEER BRICK
- METAL**  
M1 PAINTED METAL BRAILING  
M2 METAL LOUVER
- WOOD & PLASTER**  
W1 PAINTED WOOD TRIM  
W2 OCCUPANT WOOD BEAM  
W3 PAINTED WOOD WALKING CAP  
W4 PAINTED WOOD WALKING SURFACE  
W5 PAINTED WOOD OR VINYL SHUTTER  
W6 PAINTED DECORATIVE WOOD COLUMN

**AREA CALCULATIONS**

- THERMAL & MOISTURE PROTECTION**  
T1 FIBER GLASS INSULATED BRICK SHINGLE  
T2 FIBER GLASS INSULATED WOOD SHINGLE  
T3 FIBER GLASS INSULATED  
T4 ASPHALT SHINGLES SHINGLE PATTERN  
T5 CERAMIC FLOOR TILE (SALICATED WOOD TILE)
- OPENINGS**  
O1 VINYL WINDOW  
O2 PAINTED METAL SLID DOOR  
O3 PAINTED METAL GARAGE DOOR
- FINISHES**  
F1 SINK/STAINLESS STEEL FINISH - EAST/WEST  
F2 METALLIC COLOR

UNIT 10	85sf
FLOOR 10	77sf
BALCONY 10	77sf
UNIT 20	72sf
FLOOR 20	65sf
BALCONY 20	77sf
GARAGE 10 & 20	42sf

**PROJECT TITLE**

COTTAGE 1G/2G

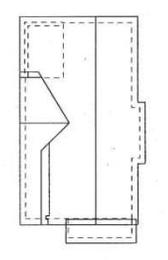
Version 2

#Street  
#City

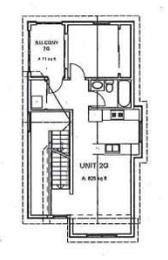
Jim Musar  
Northwest Land Park LLC

**SHEET DESCRIPTION**

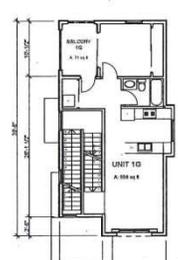
Presentation



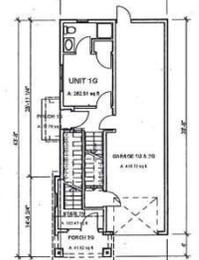
5 Roof  
SCALE 1/8" = 1'-0"



6 3rd FLOOR  
SCALE 1/8" = 1'-0"



7 2nd FLOOR  
SCALE 1/8" = 1'-0"



8 1st Floor  
SCALE 1/8" = 1'-0"

**PROJECT PHASE**

Schematic Design

March 11, 2011

**REVISIONS**

**SHEET NUMBER**

A-006.1

COTTAGE PLAN 1000S V2  
NO.200642

Date: August 19, 2010

Preliminary 2007 CBC Review  
R-3 residential, type V-B (non-rated) construction, no sprinklers

1. Occupancy
  - a. Residential: R-3 Duplex and Single Family (Section 310.1)
  - b. Private Garages A & B (406.1); Group U accessory to R-3 occupancy
  - c. Private Garage C (406.1) Group U
2. Maximum Stories/Height
  - a. Measured to average height of highest roof surface (502)
  - b. Maximum height: 3 stories and 40' (Table 503)
3. Maximum Building Area (Table 503)
  - a. Unlimited
4. Buildings on the Same Lot: (704.3, Exception)
  - a. Imaginary line assumed between duplex buildings and between single family
  - b. R-3 area unlimited (Table 503)
5. Fire Resistance of Building Elements (Table 601)
  - a. Structural Frame: 0 Hr
  - b. Bearing Walls, Int & Ext: 0
  - c. Non-bearing walls and partitions (Table 601): 0
  - d. Floors: 0
  - e. Roof: 0
6. Fire Resistance Rating Requirements for Exterior Walls based on Fire Separation Distance (Table 602)
  - a.  $0 < X \leq 5'$ : 1 Hr
  - b.  $X > 5'$ : 0
7. Projections (Including Balconies)
  - a. Maximum Projection: Max 12 inches where openings prohibited (704.2.2)
  - b. Openings prohibited where  $X < 3'$  (Table 704.8)
  - c. Projections allowed where  $X \geq 2'$
  - d. Combustible projections where openings are not permitted shall be 1-hour min (704.2.3)
8. Maximum Area of Exterior Openings (Table 704.8.6&g):
 

X: fire separation distance	Not Permitted
1. $0 < X < 3'$	25%
2. $3' < X < 5'$	Unlimited
3. $X \geq 5'$	
9. Unit/Occupancy Separations
  - a. Between Units:
    - i. Walls and Supporting Structure: 1 hour fire partitions (708.3, 708.4)
    - ii. Floors and Supporting Structures: 1 hour (711.3, 711.4)
  - b. Residential to Private Garage: per 406.1.4
- c. Residential to Private Garage C: not serving units above, not accessory, separated occupancies (508.3.3)
  - i. Floor, Wall and Supporting Structure: 2 hours (Table 508.3.3)

10. Sprinklers:

a. Not required per 2007 CBC

11. Egress

- a. Stairs within individual units:
  - i. Min Width (1009.1, exception 1): 36", occupant load <50
  - ii. Max. rise (1009.3.4) = 7.75"
  - iii. Min. tread = 10"
  - iv. Min. winder tread width = 6"
- b. Landings
  - i. Doors opening onto landing shall not reduce landing to less than one-half required width (1009.4.2)
  - ii. In R-3, a floor or landing is not required at the top of an interior flight of stairs provided door does not swing over stairs (1009.4.3)

c. Exiting

- i. One exit required from each unit, Common Path of Egress Travel limits do not apply in R-3. (1019.2.2)

d. Egress Court

- i. Width: 36" minimum (1024.5.1)

12. Accessibility

- a. Duplexes and single family are exempt (1102A.1.2)
- b. Duplexes and single family are exempt from Multi-story Dwelling requirements (1102A.3.1)

13. Yards or Courts (1205)

- a. 1205 applies only to openings that provide required natural light or ventilation. (1205.1 and IBC Commentary)
- b. Yards or courts with openings that do not provide required natural light or ventilation do not need to comply with 1205 requirements
- c. Space bounded on only 2 sides is a yard (202)
- d. 3 story yards with required openings shall be 4' minimum width (3' min + 1'/story over 2 = 3' + (1 x 1) (1205.2)
- e. Yard minimum width shall be unobstructed from the ground to the sky (202)
- f. Space bounded on 3 sides is a court (202)
- g. Note: There are no openings that provide required natural light or ventilation at courts

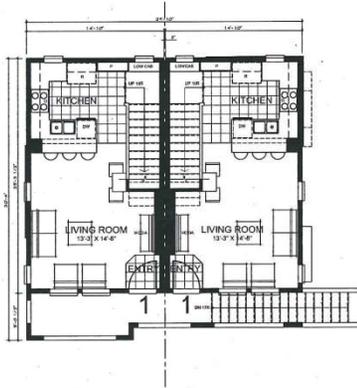
CODE ANALYSIS

N.T.S.

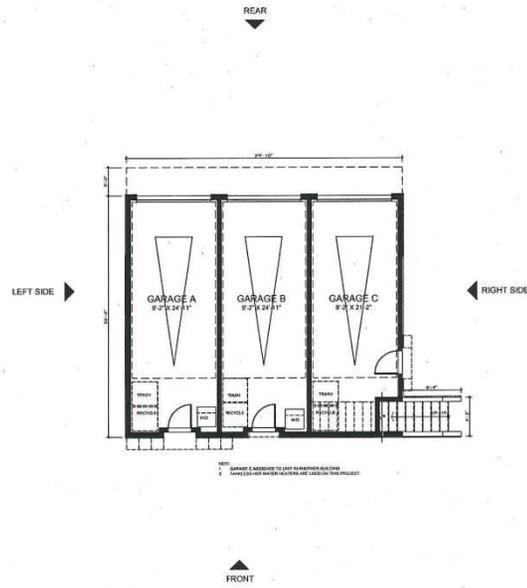
NORTHWEST LAND PARK - COTTAGE TOWNHOMES  
NORTHWEST LAND PARK, LLC



08.19.10



SECOND FLOOR



FIRST FLOOR

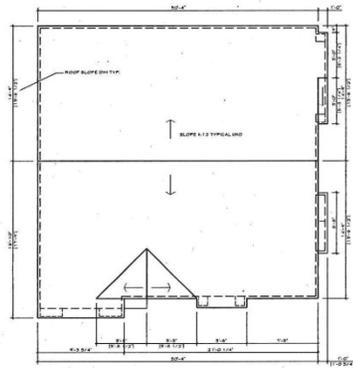
UNIT 1/1  
FIRST / SECOND FLOOR BUILDING PLANS  
ELEVATION STYLE - 'A'

NORTHWEST LAND PARK - COTTAGE TOWNHOMES  
NORTHWEST LAND PARK, LLC

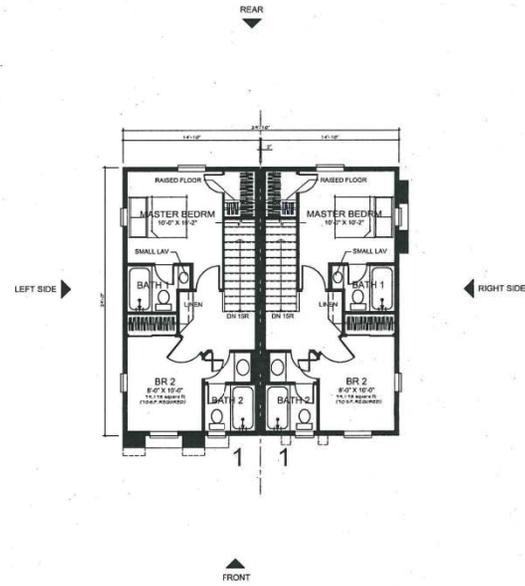


2016SKP01

AREAS	
UNIT 1	
SECOND FLOOR	385.72 S.F.
THIRD FLOOR	421.14 S.F.
TOTAL LIVING AREA	716.86 S.F.
GARAGE A	258.79 S.F.
GARAGE B	262.87 S.F.
GARAGE C (FOR ADJ. BLDG)	261.42 S.F.



ROOF PLAN



THIRD FLOOR

UNIT 1/1  
 THIRD FLOOR BUILDING PLAN / ROOF PLAN  
 ELEVATION STYLE - 'A'

1/4" = 1'-0"  
 NORTHWEST LAND PARK - COTTAGE TOWNHOMES  
 NORTHWEST LAND PARK, LLC



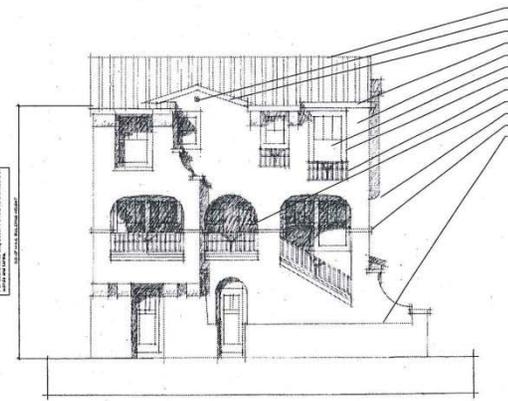
08.30.10

AREAS	
UNIT 1	
SECOND FLOOR	365.72 S.F.
THIRD FLOOR	431.14 S.F.
TOTAL LIVING AREA	796.86 S.F.
GARAGE A	288.79 S.F.
GARAGE B	262.83 S.F.
GARAGE C/POR AND BLDG	201.43 S.F.



LEFT SIDE ELEVATION

1. The drawing shall be used for the purpose of construction. It shall not be used for any other purpose without the written consent of the architect. 2. The drawing shall be used for the purpose of construction. It shall not be used for any other purpose without the written consent of the architect. 3. The drawing shall be used for the purpose of construction. It shall not be used for any other purpose without the written consent of the architect.



FRONT ELEVATION

UNIT 1/1  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

1/8"=1'-0"

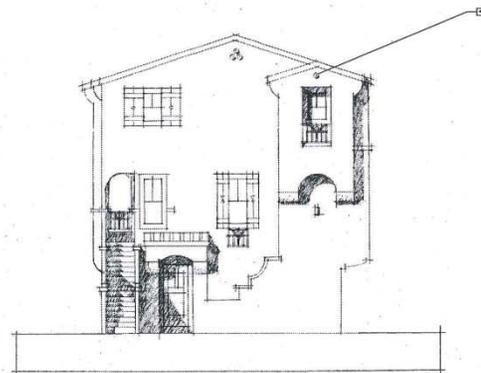
NORTHWEST LAND PARK - COTTAGE TOWNHOMES  
NORTHWEST LAND PARK, LLC



08.26.18

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR 12" TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR FLAT TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	3X FASCIA
<input type="checkbox"/>	3X RAKE BOARD
<input type="checkbox"/>	PLASTER CORBEL
<input type="checkbox"/>	PLASTER RAINSCOT
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	DECORATIVE VENT
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	HARDBOARD SHINGLE SIDING
<input type="checkbox"/>	HORIZONTAL SIDING
<input type="checkbox"/>	HEAVY PLASTER TEXTURE
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FRIBERGLASS ENTRY DOOR
<input type="checkbox"/>	DECORATIVE ACCENT TILE
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR

\* ALL METAL AND FIBERGLASS ELEMENTS TO BE PAINTED



ENHANCED RIGHT SIDE ELEVATION



REAR ELEVATION

UNIT 1/1  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

1/8"=1'-0"

NORTHWEST LAND PARK - COTTAGE TOWNHOMES  
NORTHWEST LAND PARK, LLC

2018103020

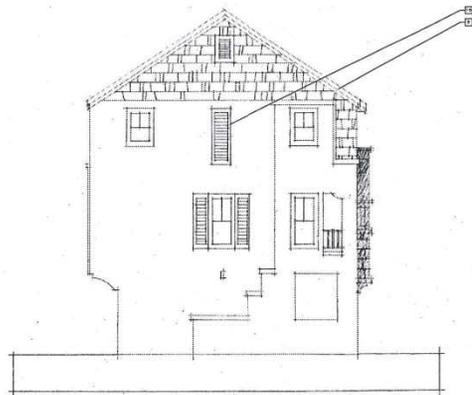


08.30.19

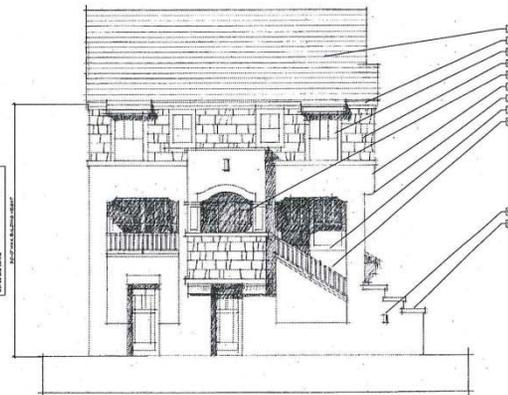
TYPICAL BUILDING MATERIALS

- INTEGRAL COLOR 12" TILE ROOF
- INTEGRAL COLOR FLAT TILE ROOF
- INTEGRAL COLOR PLASTER
- 3X FASCIA
- 2X RAISE BOARD
- PLASTER CORBEL
- PLASTER HANDCOT
- PLASTER WINDOW TRIM
- DECORATIVE VENT
- PLASTER TRIM
- VINYL WINDOW
- HANDBOARDED SHINGLE SIDING
- HORIZONTAL SIDING
- HEAVY PLASTER TEXTURE
- DECORATIVE SHUTTER
- METAL RAIL / GUARDRAIL
- FIBERGLASS ENTRY DOOR
- DECORATIVE ACCENT TILE
- METAL SECTIONAL GARAGE DOOR

\* ALL METAL AND HOOD ELEMENTS TO BE PAINTED



LEFT SIDE ELEVATION



FRONT ELEVATION

UNIT 1/1  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'

1/4"=1'-0"

NORTHWEST LAND PARK - COTTAGE TOWNHOMES  
NORTHWEST LAND PARK, LLC

2019/03/28

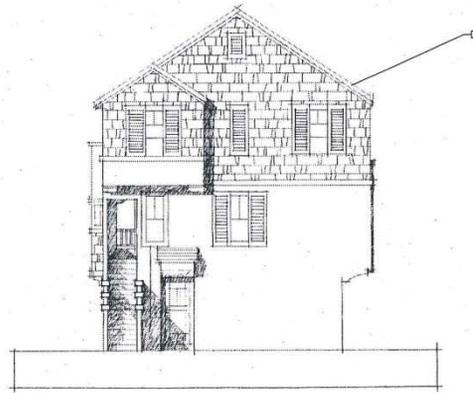


08.30.19

TYPICAL BUILDING MATERIALS

- INTERNAL COLOR 3" TILE ROOF
- INTERNAL COLOR FLAT TILE ROOF
- INTERNAL COLOR PLASTER
- 3X FASCIA
- 2X RAKE BOARD
- PLASTER CORBEL
- PLASTER MANSARD
- PLASTER WINDOW TRIM
- DECORATIVE VENT
- PLASTER TRIM
- VINYL WINDOW
- HARDBOARD SHINGLE SIDING
- HORIZONTAL SIDING
- HEAVY PLASTER TEXTURE
- DECORATIVE SHUTTER
- METAL RAIL / GUARDRAIL
- FIBERGLASS ENTRY DOOR
- DECORATIVE ACCENT TILE
- METAL SECTIONAL GARAGE DOOR

\* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



ENHANCED RIGHT SIDE ELEVATION



REAR ELEVATION

UNIT 1/1  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'

1/4"=1'-0"

NORTHWEST LAND PARK - COTTAGE TOWNHOMES  
NORTHWEST LAND PARK, LLC

2018103701



08.30.19

TYPICAL BUILDING MATERIALS

- INTEGRAL COLOR 3" TILE ROOF
- INTEGRAL COLOR FLAT TILE ROOF
- INTEGRAL COLOR PLASTER
- 2X FASCIA
- 2X RAFTER BOARD
- PLASTER CORBEL
- PLASTER RAINSCOT
- PLASTER WINDOW TRIM
- DECORATIVE VENT
- PLASTER TRIM
- VINYL WINDOW
- HARDBOARD SHINGLE SIDING
- HORIZONTAL SIDING
- HEAVY PLASTER TEXTURE
- DECORATIVE SHUTTER
- METAL RAIL / GUARDRAIL
- FIBERGLASS ENTRY DOOR
- DECORATIVE ACCENT TILE
- METAL SECTIONAL GARAGE DOOR

\* ALL METAL AND WOOD ELEMENTS TO BE PAINTED



LEFT SIDE ELEVATION



FRONT ELEVATION

UNIT 1/1  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'E'

1/4"=1'-0"

NORTHWEST LAND PARK - COTTAGE TOWNHOMES  
NORTHWEST LAND PARK, LLC

201008320



08.30.10

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR W/ TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR FLAT TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	3X PASCAL
<input type="checkbox"/>	3X RAWE BOARD
<input type="checkbox"/>	PLASTER GORBEL
<input type="checkbox"/>	PLASTER MANSBOT
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	DECORATIVE VENT
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	HARDBOARD SHINGLE SIDING
<input type="checkbox"/>	HORIZONTAL SIDING
<input type="checkbox"/>	HEAVY PLASTER TEXTURE
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	PIEDIMENTS ENTRY DOOR
<input type="checkbox"/>	DECORATIVE ACCENT TILE
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR

\* ALL METAL AND HOOD ELEMENTS TO BE PAINTED



ENHANCED RIGHT SIDE ELEVATION



REAR ELEVATION

UNIT 1/1  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'E'

1/4"=1'-0"

NORTHWEST LAND PARK - COTTAGE TOWNHOMES  
NORTHWEST LAND PARK, LLC

201803020



08.30.18

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR W/ TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR FLAT-TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	3X FASCIA
<input type="checkbox"/>	3X RAISE BOARD
<input type="checkbox"/>	PLASTER CORBEL
<input type="checkbox"/>	PLASTER HANGBOT
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	DECORATIVE VENT
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	HARDBOARD SHINGLE SIDING
<input type="checkbox"/>	HORIZONTAL SIDING
<input type="checkbox"/>	HEAVY PLASTER TEXTURE
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	DECORATIVE ACCENT TILE
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR

\* ALL METAL AND HOOD ELEMENTS TO BE PAINTED

Date: July 12, 2010

Preliminary 2007 CBC Review  
R-3 residential duplex and 3 townhouse building  
Type V-B (non-rated) construction, no sprinklers

1. Occupancy
  - a. Residential: R-3 Duplex and 3 townhouse building (Section 310.1)
  - b. Private Garages (406.1): Group U accessory to R-3 occupancy
2. Maximum Stories/Height
  - a. Measured to average height of highest roof surface (502)
  - b. Maximum height: 3 stories and 40' (Table 503)
3. Maximum Building Area (Table 503)
  - a. Unlimited
4. Townhouses
  - a. Townhouses considered separate buildings (419.4)
  - b. Common 2-hour fire resistance-rated wall shall separate townhouses (419.3, Ex)
5. Buildings on the Same Lot: (704.3, Exception)
  - a. Imaginary line assumed between duplex buildings and between 3 townhouse buildings
  - b. R-3 area unlimited (Table 503)
6. Fire Resistance of Building Elements (Table 601)
  - a. Structural Frame: 0 Hr
  - b. Bearing Walls, Int & Ex: 0
  - c. Non-bearing walls and partitions (Table 601): 0
  - d. Floors: 0
  - e. Roof: 0
7. Fire Resistance Rating Requirements for Exterior Walls based on Fire Separation Distance (Table 602)
  - a. 0-x<5' 1 Hr
  - b. X->5' 0
8. Projections (Including Balconies)
  - a. Maximum Projection: Max 12 inches where openings prohibited (704.2.2)
  - b. Openings prohibited where X->2' (Table 704.8)
  - c. Projections allowed where X->2'

- d. Combustible projections where openings are not permitted shall be 1-hour min (704.2.3)
9. Maximum Area of Exterior Openings (Table 704.8, 8.0.8g)
    - a. X-fire separation distance
      1. 0-x<5' Not Permitted
      2. 2-x<5' 25%
      3. 5'-X Unlimited
  10. Unit/Occupancy Separations
    - a. Between Units:
      - i. Walls and Supporting Structure: 1 hour fire partitions (705.3, 708.4)
      - ii. Floors and Supporting Structures: 1 hour (711.3, 711.4)
    - b. Between townhouses:
      - i. Common 2-hour fire-resistance-rated wall shall separate townhouses (419.3, Ex)
      - c. Residential to Private Garage: per 406.1.4
  11. Sprinklers:
    - a. Not required per 2007 CBC
  12. Egress
    - a. Stairs within individual units:
      - i. Min Width (1009.1, exception 1): 36", occupant load <=50
      - ii. Max. rise (1009.3.4) = 7.75"
      - iii. Min. tread=10"
      - iv. Min. winder tread width=6"
    - b. Landings
      - i. Doors opening onto landing shall not reduce landing to less than one-half required width (1009.4.2)
      - ii. In R-3, a floor or landing is not required at the top of an interior flight of stairs provided door does not swing over stairs (1009.4.3)
    - c. Egress
      - i. One exit required from each unit, Common Path of Egress Travel limits do not apply in R-3. (1019.2.2)
    - d. Egress Court
      - i. Width: 36" minimum (1024.5.1)
  13. Accessibility
    - a. Duplexes and condominiums with 3 units are exempt (1102A.1.2)
    - b. Duplexes and condominiums with 3 units are exempt from Multi-story Dwelling requirements (1102A.3.1)
  14. Yards or Courts (1206)
    - a. 1206 applies only to openings that provide required natural light or ventilation. (1206.1 and IBC Commentary)
    - b. Yards or courts with openings that do not provide required natural light or ventilation do not need to comply with 1206 requirements

- c. Space bounded on only 2 sides is a yard (202)
- d. 3 story yards with required openings shall be 4' minimum width (2' min = 1' story over 2 = 3' = 1' x 1'). (1206.2)
- e. Yard minimum width shall be unobstructed from the ground to the sky (202)
- f. Space bounded on 3 sides is a court (202)
- g. Note: There are no openings that provide required natural light or ventilation at courts

CODE ANALYSIS

MTX

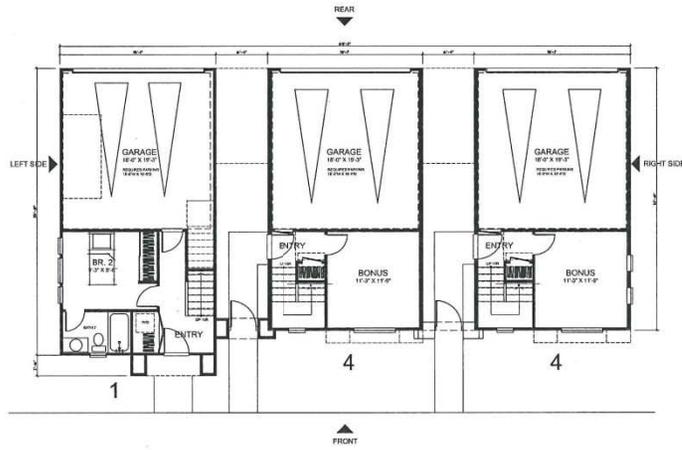
NORTHWEST LAND PARK - BUNGALOWS  
NORTHWEST LAND PARK, LLC



08.23.11



SECOND FLOOR



FIRST FLOOR

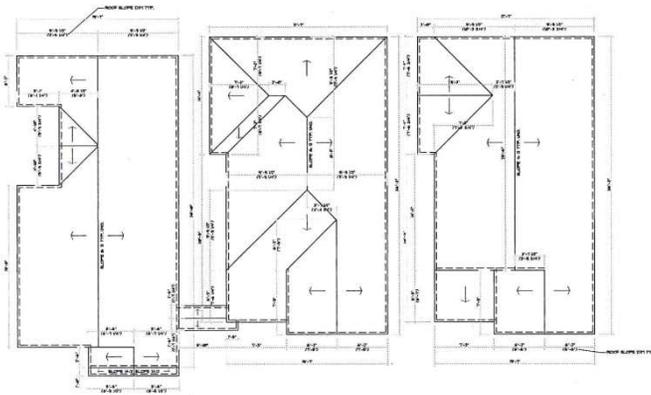
3.1 PLEX

FIRST / SECOND FLOOR PLANS  
UNITS 1 & 4

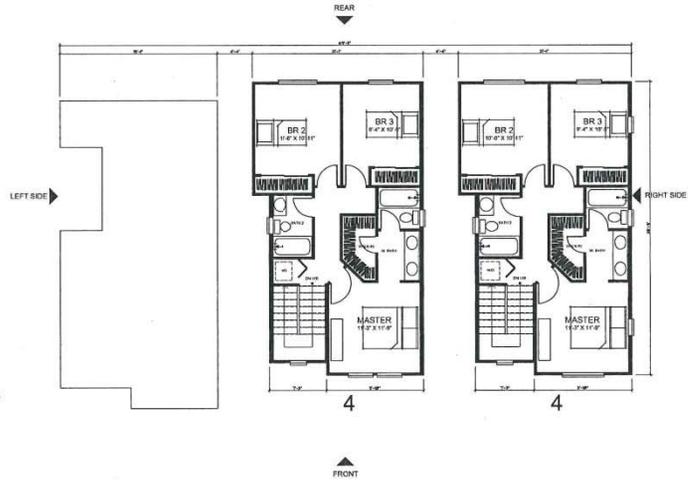
NORTHWEST LAND PARK - BUNGALOWS  
NORTHWEST LAND PARK, LLC



AREA	
<b>UNIT 1</b>	
FIRST FLOOR	383.50 S.F.
SECOND FLOOR	413.21 S.F.
TOTAL LIVING AREA	796.71 S.F.
CLOSET	300.82 S.F.
DECK	41.45 S.F.
total area	1248.98 S.F.
<b>UNIT 2</b>	
FIRST FLOOR	76.82 S.F.
SECOND FLOOR	384.11 S.F.
THIRD FLOOR	424.54 S.F.
TOTAL LIVING AREA	885.47 S.F.
CLOSET	300.82 S.F.
DECK	307.48 S.F.
total area	1593.77 S.F.
<b>UNIT 3</b>	
FIRST FLOOR	76.82 S.F.
SECOND FLOOR	384.11 S.F.
THIRD FLOOR	424.54 S.F.
TOTAL LIVING AREA	885.47 S.F.
CLOSET	300.82 S.F.
DECK	307.48 S.F.
total area	1593.77 S.F.
<b>UNIT 4</b>	
FIRST FLOOR	213.74 S.F.
SECOND FLOOR	357.98 S.F.
THIRD FLOOR	404.86 S.F.
TOTAL LIVING AREA	976.58 S.F.
CLOSET	213.74 S.F.
DECK	106.87 S.F.
total area	1297.19 S.F.



ROOF PLAN



THIRD FLOOR

3.1 PLEX  
 THIRD FLOOR / ROOF PLAN  
 UNITS 1 & 4  
 NORTHWEST LAND PARK - BUNGALOWS  
 NORTHWEST LAND PARK, LLC



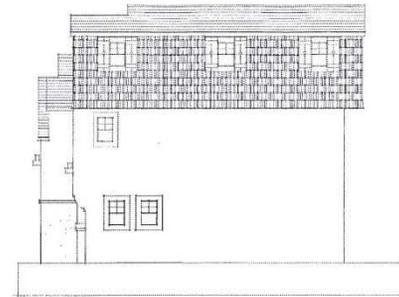
AREA	
UNIT 1	
FIRST FLOOR	285.50 S.F.
SECOND FLOOR	312.25 S.F.
TOTAL LIVING AREA	597.75 S.F.
CORRIDOR	21.45 S.F.
CL. / C.	21.45 S.F.
WALL AREA	1288.87 S.F.
UNIT 2	
FIRST FLOOR	76.82 S.F.
SECOND FLOOR	285.11 S.F.
THIRD FLOOR	325.93 S.F.
TOTAL LIVING AREA	687.86 S.F.
CORRIDOR	360.88 S.F.
CL. / C.	207.88 S.F.
WALL AREA	1788.42 S.F.
UNIT 3	
FIRST FLOOR	76.80 S.F.
SECOND FLOOR	285.86 S.F.
THIRD FLOOR	325.39 S.F.
TOTAL LIVING AREA	688.05 S.F.
CORRIDOR	360.88 S.F.
CL. / C.	207.88 S.F.
WALL AREA	1788.07 S.F.
UNIT 4	
FIRST FLOOR	213.74 S.F.
SECOND FLOOR	227.88 S.F.
THIRD FLOOR	483.48 S.F.
TOTAL LIVING AREA	925.10 S.F.
CORRIDOR	412.51 S.F.
CL. / C.	120.00 S.F.
WALL AREA	1580.00 S.F.



1B  
FRONT ELEVATION

4E

4C



4C RIGHT SIDE ELEVATION

### 3.1 PLEX

EXTERIOR ELEVATIONS  
UNITS 1 & 4

1/16" = 1'

## NORTHWEST LAND PARK - BUNGALOWS

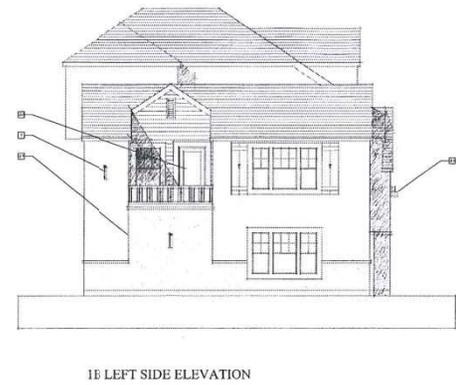
NORTHWEST LAND PARK, LLC



04.11.11

#### TYPICAL BUILDING MATERIALS

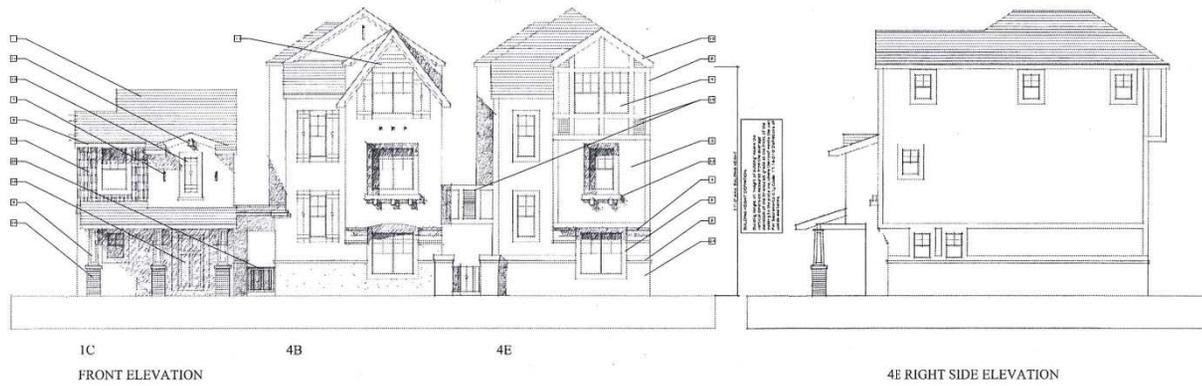
- ☐ INTEGRAL COLOR FLAT TILE ROOF
  - ☐ INTEGRAL COLOR PLASTER
  - ☐ 3" X FASCIA
  - ☐ PLASTER GORBEL
  - ☐ PLASTER GORJIN
  - ☐ PLASTER PINEOXYTER
  - ☐ DECORATIVE PLASTER PROJECTION
  - ☐ PLASTER TRIM
  - ☐ VINYL FINISH
  - ☐ HORIZONTAL SHINGLE SIDING
  - ☐ HORIZONTAL SIDING
  - ☐ DECORATIVE SHUTTER
  - ☐ 3/4" RAPE BOARD
  - ☐ BENTY / BEAN SIDING
  - ☐ BRICK PATTERN VENEER
  - ☐ RAIL / GUARDRAIL / GATE
  - ☐ METAL RAIL / GUARDRAIL
  - ☐ FIBERGLASS ENTRY DOOR
  - ☐ SLIDING GLASS DOOR
  - ☐ FRENCH DOORS
  - ☐ METAL SECTIONAL GARAGE DOOR
  - ☐ DECORATIVE FOOTWEAR
  - ☐ DECORATIVE VENT
  - ☐ PLASTER HANDGOT
  - ☐ JAMB
  - ☐ HEAVY TEXTURE PLASTER
  - ☐ EXPANSION JOINT
- \* ALL METAL AND FOOD ELEMENTS TO BE PAINTED



3.1 PLEX  
 EXTERIOR ELEVATIONS  
 UNIT 1 & 4  
 NORTHWEST LAND PARK - BUNGALOWS  
 NORTHWEST LAND PARK, LLC



TYPICAL BUILDING MATERIALS	
[Symbol]	INTERNAL COLOR FLAT TILE ROOF
[Symbol]	INTERNAL COLOR PLASTER
[Symbol]	3" X 6" PAISIE
[Symbol]	PLASTER CORBEL
[Symbol]	PLASTER COLUMN
[Symbol]	PLASTER WINDOW TRIM
[Symbol]	DECORATIVE PLASTER PROJECTION
[Symbol]	PLASTER TRIM
[Symbol]	VINYL WINDOW
[Symbol]	HANDBOARD SHINGLE SIDING
[Symbol]	HORIZONTAL SIDING
[Symbol]	DECORATIVE BUTTER
[Symbol]	2" X RAKE BOARD
[Symbol]	BEAM / BEAM END
[Symbol]	BRICK PATTERNS / VENEER
[Symbol]	PAV / GUARDRAIL / GATE
[Symbol]	METAL PAV. / SUBPARCEL
[Symbol]	FRONT GLASS ENTRY DOOR
[Symbol]	SLIDING GLASS DOOR
[Symbol]	PANORAMIC DOOR
[Symbol]	METAL SECTIONAL GARAGE DOOR
[Symbol]	DECORATIVE FOTOSHELF
[Symbol]	DECORATIVE VENT
[Symbol]	PLASTER HANDSOT
[Symbol]	GATE
[Symbol]	HEAVY TEXTURE PLASTER
[Symbol]	EXPANSION JOINT
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



1C  
FRONT ELEVATION

4B

4E

4E RIGHT SIDE ELEVATION

3.2 PLEX

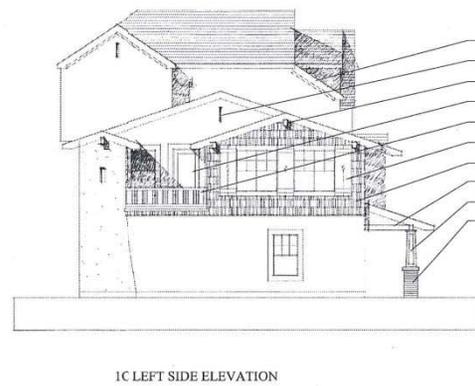
EXTERIOR ELEVATIONS  
UNITS 1 & 4

1/4" = 1'-0"  
NORTHWEST LAND PARK - BUNGALOWS  
NORTHWEST LAND PARK, LLC



03.21.11

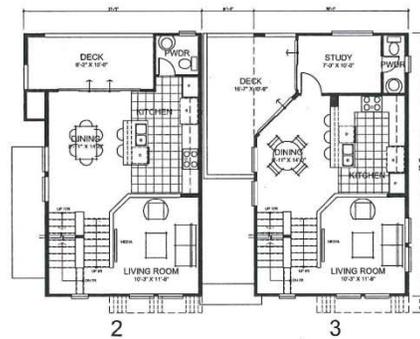
TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR FLAT TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	3/4" PLYWOOD
<input type="checkbox"/>	PLASTER CORBEL
<input type="checkbox"/>	PLASTER COLUMN
<input type="checkbox"/>	PLASTER WINDOY TRIM
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	HORIZONTAL SHINGLE SIDING
<input type="checkbox"/>	HORIZONTAL SIDING
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	3/4" RAKE BOARD
<input type="checkbox"/>	BRUSHED BRASS FINISH
<input type="checkbox"/>	BRICK PATTERN VENEER
<input type="checkbox"/>	RAIL / GUARDRAIL / GATE
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FRAMING GLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOORS
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR
<input type="checkbox"/>	DECORATIVE POST/RIELP
<input type="checkbox"/>	DECORATIVE VENT
<input type="checkbox"/>	PLASTER HANDSOT
<input type="checkbox"/>	GATE
<input type="checkbox"/>	HEAVY TEXTURE PLASTER
<input type="checkbox"/>	EXPANSION JOINT
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



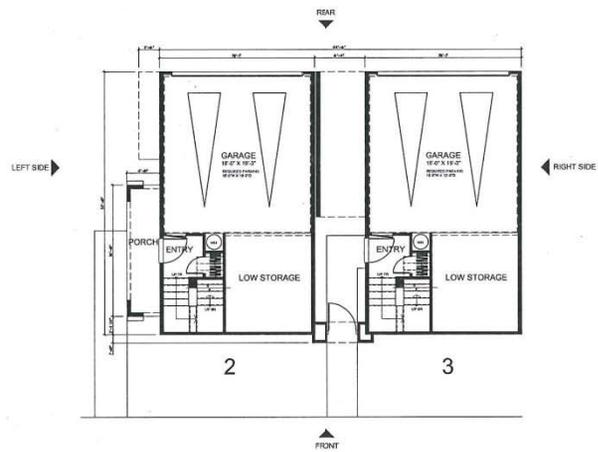
3.2 PLEX  
 EXTERIOR ELEVATIONS  
 UNITS 1 & 4  
 1/4" = 1'-0"  
 NORTHWEST LAND PARK - BUNGALOWS  
 NORTHWEST LAND PARK, LLC



TYPICAL BUILDING MATERIALS	
☐	INTEGRAL COLOR FLAT TILE ROOF
☐	INTEGRAL COLOR PLASTER
☐	2" X FASCIA
☐	PLASTER CORBEL
☐	PLASTER COLUMN
☐	PLASTER WINDOW TRIM
☐	DECORATIVE PLASTER PROJECTION
☐	PLASTER TRIM
☐	VINYL WINDOW
☐	HANDICAPPED SINGLE STAIR
☐	HORIZONTAL SINGE
☐	DECORATIVE SHUTTER
☐	3/4" PANE BOARD
☐	BEAM / BEAM END
☐	BRICK PATTEREN VENER
☐	RAIL / GUARDRAIL / BATE
☐	METAL RAIL / GUARDRAIL
☐	FIBERGLASS ENTRY DOOR
☐	SLIDING GLASS DOOR
☐	FRENCH DOORS
☐	METAL SECTIONAL GARAGE DOOR
☐	DECORATIVE POTTHOLE
☐	DECORATIVE VENT
☐	PLASTER HANGBOT
☐	WATE
☐	HEAVY TEXTURE PLASTER
☐	EXPANSION JOINT
* ALL METAL AND HOOD ELEMENTS TO BE PAINTED	



SECOND FLOOR



FIRST FLOOR

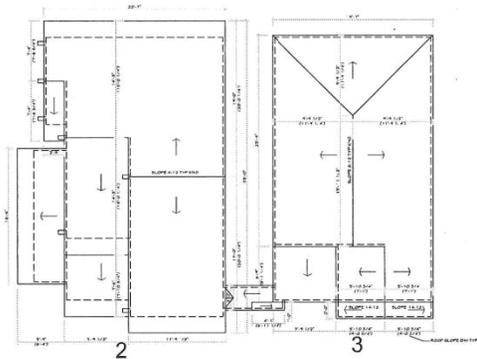
2.2 PLEX  
FIRST / SECOND FLOOR PLANS  
UNITS 2 & 3

NORTHWEST LAND PARK - BUNGALOWS  
NORTHWEST LAND PARK, LLC

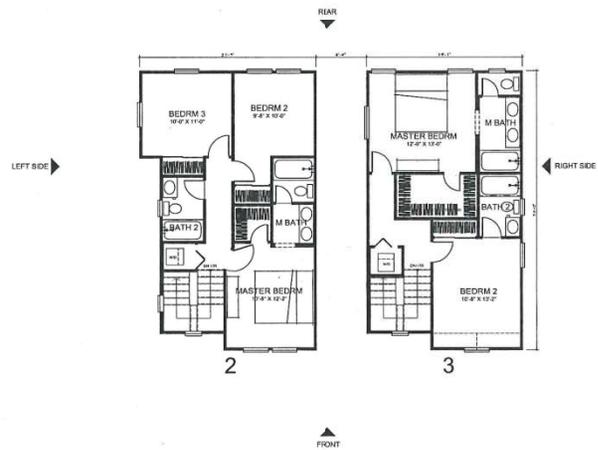


01.20.11

AREA	
UNIT 1	
FIRST FLOOR	281.00 S.F.
SECOND FLOOR	611.11 S.F.
TOTAL LIVING AREA	892.11 S.F.
CLOSET	128.00 S.F.
DECK	21.65 S.F.
total area	1232.87 S.F.
UNIT 2	
FIRST FLOOR	76.82 S.F.
SECOND FLOOR	58.11 S.F.
TOTAL LIVING AREA	134.93 S.F.
CLOSET	261.88 S.F.
DECK	68.00 S.F.
DECK	87.00 S.F.
total area	1769.81 S.F.
UNIT 3	
FIRST FLOOR	76.82 S.F.
SECOND FLOOR	58.11 S.F.
TOTAL LIVING AREA	134.93 S.F.
CLOSET	261.88 S.F.
DECK	68.00 S.F.
DECK	87.00 S.F.
total area	1769.81 S.F.
UNIT 4	
FIRST FLOOR	213.14 S.F.
SECOND FLOOR	71.00 S.F.
TOTAL LIVING AREA	284.14 S.F.
CLOSET	411.00 S.F.
DECK	152.82 S.F.
total area	1838.80 S.F.



ROOF FLOOR



THIRD FLOOR

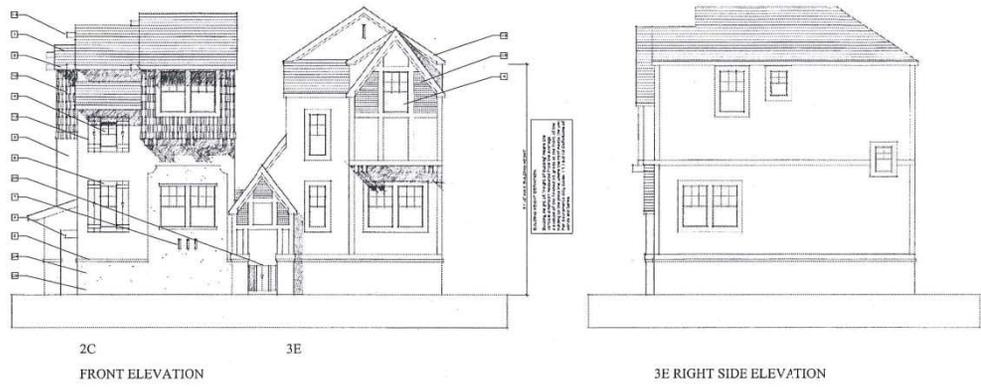
2.2 PLEX  
THIRD FLOOR / ROOF PLAN  
UNITS 2 & 3

NORTHWEST LAND PARK - BUNGALOWS  
NORTHWEST LAND PARK, LLC



06.21.11

AREAS	
<b>UNIT 1</b>	
FIRST FLOOR	388.00 S.F.
SECOND FLOOR	637.33 S.F.
TOTAL LIVING AREA	1025.33 S.F.
CLAMOR	393.42 S.F.
DECK	11.88 S.F.
POOL	1341.47 S.F.
<b>UNIT 2</b>	
FIRST FLOOR	76.81 S.F.
SECOND FLOOR	586.11 S.F.
THIRD FLOOR	618.93 S.F.
TOTAL LIVING AREA	1281.85 S.F.
CLAMOR	393.42 S.F.
DECK	42.00 S.F.
POOL	1341.47 S.F.
<b>UNIT 3</b>	
FIRST FLOOR	76.81 S.F.
SECOND FLOOR	543.99 S.F.
THIRD FLOOR	618.93 S.F.
TOTAL LIVING AREA	1240.73 S.F.
CLAMOR	393.42 S.F.
DECK	42.00 S.F.
POOL	1341.47 S.F.
<b>UNIT 4</b>	
FIRST FLOOR	613.74 S.F.
SECOND FLOOR	617.98 S.F.
THIRD FLOOR	663.48 S.F.
TOTAL LIVING AREA	1895.20 S.F.
CLAMOR	393.42 S.F.
DECK	11.88 S.F.
POOL	1438.32 S.F.



2C  
FRONT ELEVATION

3E

3E RIGHT SIDE ELEVATION

2.2 PLEX

EXTERIOR ELEVATIONS  
UNITS 2 & 3

NORTHWEST LAND PARK - BUNGALOWS  
NORTHWEST LAND PARK, LLC



TYPICAL BUILDING MATERIALS	
☐	INTERNAL COLOR PLAT FILE ROOF
☐	INTERNAL COLOR PLASTER
☐	3 X FASGA
☐	PLASTER CORNICE
☐	PLASTER COLUMN
☐	PLASTER HINDERTIM
☐	DECORATIVE PLASTER PROJECTION
☐	PLASTER TRIM
☐	VINYL WINDOW
☐	HANDBOARD BRIMBLE SIDING
☐	HORIZONTAL SIDING
☐	DECORATIVE BUTTER
☐	3/4" X 3/4" BOARD
☐	BEAM / BEAM ENDS
☐	BRICK PATTERN VENER
☐	RAIL / GUARDRAIL / GATE
☐	METAL SILL / JAMBURAL
☐	FIBERGLASS ENTRY DOOR
☐	INSULATED GLASS DOOR
☐	FRENCH DOORS
☐	METAL SECTIONAL GARAGE DOOR
☐	DECORATIVE PORTHOLE
☐	DECORATIVE VENT
☐	PLASTER HANGSOT
☐	GATE
☐	HEAVY TEXTURE PLASTER
☐	EXPANSION JOINT
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



3E  
REAR ELEVATION



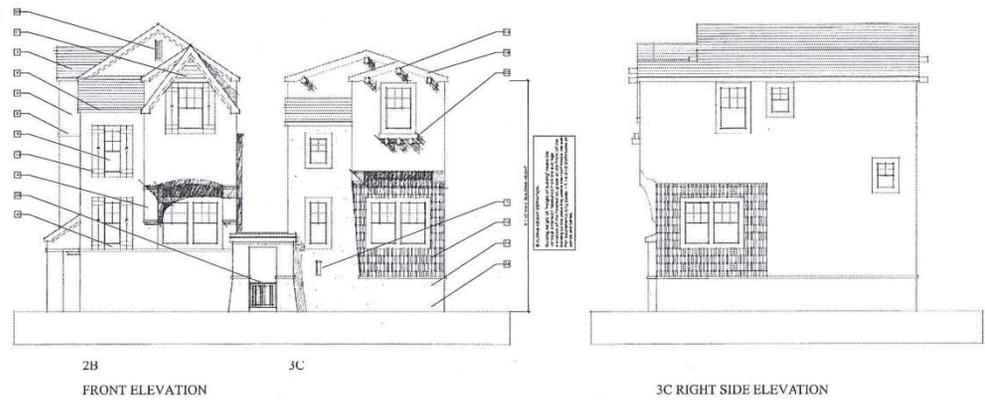
2C LEFT SIDE ELEVATION

2.2 PLEX  
EXTERIOR ELEVATIONS  
UNITS 2 & 3  
NORTHWEST LAND PARK - BUNGALOWS  
NORTHWEST LAND PARK, LLC



01.21.11

TYPICAL BUILDING MATERIALS	
☐	INTEGRAL COLOR FLAT TILE ROOF
☐	INTEGRAL COLOR PLASTER
☐	3" X 6" PAVING
☐	PLASTER CORBEL
☐	PLASTER COLUMN
☐	PLASTER RIBBON TRIM
☐	DECORATIVE PLASTER PROJECTION
☐	PLASTER TRIM
☐	VINYL WINDOW
☐	HANDICAPABLE SINGLE EDGE
☐	HORIZONTAL SERR
☐	DECORATIVE BUTTER
☐	3/4" FINE BOARD
☐	BEAM / BEAM ENDS
☐	BRICK PATTERN VENEER
☐	RAIL / GUARDRAIL / RAKE
☐	METAL RAIL / QUADRANT
☐	FRIGGLASS ENTRY DOOR
☐	SLIDING GLASS DOOR
☐	FRENCH DOORS
☐	METAL SECTIONAL GARAGE DOOR
☐	DECORATIVE FOTHELF
☐	DECORATIVE VENT
☐	PLASTER HANGING
☐	GATE
☐	HEAVY TEXTURE PLASTER
☐	EXPANDED JOINT
ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



2.1 PLEX  
 EXTERIOR ELEVATIONS  
 UNITS 2 & 3  
 1/4" = 1'-0"  
 NORTHWEST LAND PARK - BUNGALOWS  
 NORTHWEST LAND PARK, LLC



TYPICAL BUILDING MATERIALS	
☐	INTERGRAL COLOR FLAT TILE ROOF
☐	INTERGRAL COLOR PLASTER
☐	3" X FASCIA
☐	PLASTER CORBEL
☐	PLASTER COLUMN
☐	PLASTER PINDOR TRIM
☐	DECORATIVE PLASTER PROJECTION
☐	PLASTER TRIM
☐	VINYL PINDOR
☐	HARDBOARD SHINGLE SIDING
☐	HORIZONTAL SIDING
☐	DECORATIVE SHUTTER
☐	3/4" PANE BOWED
☐	BEAM / BEAM END
☐	BRICK PATTERN / END
☐	RAIL / GUARDRAIL / GATE
☐	METAL RAIL / GUARDRAIL
☐	FIBERGLASS ENTRY DOOR
☐	ELECTRO GLASS DOOR
☐	FRENCH DOORS
☐	METAL SECTIONAL GARAGE DOOR
☐	DECORATIVE POTTIEP
☐	DECORATIVE VENT
☐	PLASTER HANGGOT
☐	GATE
☐	HEAVY TEXTURE PLASTER
☐	EXPANSION JOINT
* ALL METAL AND FOOD ELEMENTS TO BE PAINTED	



2.1 PLEX  
 EXTERIOR ELEVATIONS  
 UNITS 2 & 3  
 NORTHWEST LAND PARK - BUNGALOWS  
 NORTHWEST LAND PARK, LLC



TYPICAL BUILDING MATERIALS	
☐	INTERNAL COLOR FLAT TILE ROOF
☐	INTERNAL COLOR PLASTER
☐	2" FASCIA
☐	PLASTER CORBEL
☐	PLASTER COLUMN
☐	PLASTER INDENTION
☐	DECORATIVE PLASTER PROJECTION
☐	PLASTER TRIM
☐	VENTIL. FINISH
☐	HANDICAPED SINGLE BEND
☐	HORIZONTAL SING.
☐	DECORATIVE SHUTTER
☐	3" RAKE BOARD
☐	BRASS / BRASS FINISH
☐	BACK PATTERNS VENEER
☐	RAIL / GUARDRAIL / GATE
☐	METAL RAIL / GUARDRAIL
☐	FIBERGLASS ENTRY DOOR
☐	SLIDING GLASS DOOR
☐	FRENCH DOORS
☐	METAL SECTIONAL GARAGE DOOR
☐	DECORATIVE POTHOLE
☐	DECORATIVE VENT
☐	PLASTER HANGOUT
☐	GATE
☐	HEAVY FEATURE PLASTER
☐	EXPANSION JOINT
* ALL METAL AND FOOD ELEMENTS TO BE PAINTED	



## Attachment 10: Comment Letters

### **Table of Contents:**

- Exhibit A: LPCA initial comments dated October 13, 2010 – 1 page
- Exhibit B: Craig Chaffee comments dated July 31, 2010 – 2 pages
- Exhibit C: Craig Chaffee comments dated November 2, 2010 – 2 pages
- Exhibit D: Greater Broadway Partnership letter dated June 14, 2011 – 2 pages
- Exhibit E: WalkSacramento letter dated November 17, 2010 – 4 pages
- Exhibit F: ECOS letter dated March 24, 2011 – 16 pages
- Exhibit G: Kathy Griest letter dated July 13, 2011 – 1 page

## Exhibit A: LPCA initial comments dated October 13, 2010



### Initial LPCA Comments: Northwest Land Park Development

1. Many people are concerned about the economic viability of the project, especially with current plan to begin development at the southeast corner of the overall planned development. The concern is that this location is somewhat isolated, with industrial and public housing surrounding it and no connection to Broadway or the central city. Will empty nesters and young professionals, the development's target market, be interested in such a location?
2. A number of people have expressed approval of the developer's original plan to have a park meander along the old railroad right of way rather than the square park the Parks Department has imposed.
3. A number of people also think the tunnel under Interstate 5 should be opened to provide access to the Sacramento River.
4. A number of people have also expressed support of the developer's idea of retaining some of the "iconic" industrial buildings such as the produce market and an incinerator to give the area a sense of place.
5. Most everyone believes that there should be some sort of commercial area within the project to provide such basic services to the new residents such as a grocery store and a drug store. There were some suggestions of incorporating this as part of a mixed-use area and also utilization of the iconic structures in this fashion.
6. Everyone recognizes that 5th Street borders the entire east end of the project and is a direct line into the city. There was strong support for insuring that this street becomes a "complete" street providing bicycle and pedestrian access rather than an auto thoroughfare.
7. There were a number of generalized comments that the project needs some zip. Possibly the previously listed items would take care of this suggestion, but there was some support for incorporation of more urban design elements or architecture. (Some felt current architectural design was fine.)

October 13, 2010

Exhibit B: Craig Chaffee comments dated July 31, 2010

Thanks for sharing the current information on development plans (date routed - July 23, 2010). My wife and I live in on 4th Avenue in the neighborhood southeast of the planned development. We attended an early meeting the Land Park Neighborhood Association (LPCA) held with the developers in January 2010. The community meeting provided a chance to meet the developers, hear about their plans and raise questions/concerns.

There is a real opportunity for a good new neighborhood close to downtown, near the river, and on the edge of the Land Park neighborhoods.

At the January 2010 meeting we were relieved to hear there are no plans to build additional low income housing in the area. The area already includes two of Sacramento's main public housing complexes, and they are high density developments. Any additional units would only increase related problems including increased community crime and a high density of persons with needs for public services and assistance.

We raised concerns with the developer representatives at the meeting about high density development, particularly in the initial phase (s) that are near the existing residential neighborhood. We were glad to hear that lower density units were planned in the initial phases for the southeast portion of the area (near existing neighborhoods and the high density public housing) and higher density near Broadway in later phases. This made sense since 5th Street and Vallejo Way (the street that goes into our neighborhood off 5th Street) already gets crowded with public housing-related and other traffic. Positioning higher density development in later phases near Broadway makes sense since Broadway can easily handle the additional traffic, and we were assured that most vehicle traffic would be going downtown, and not into our neighborhoods.

But now, the July 23, 2010 document you have shared shows higher density buildings planned in Phase One. Attachment B details all 3 or 4 stories, some with additional lofts. We request that lower density, less multiple story residential buildings be placed in the early phases of the build-out, as originally promised.

After hearing the presentation at the LPCA meeting, we still had concerns about the density of units being proposed and the resulting increases in people in the area. We know the project must pencil out to be profitable, but less dense construction is preferred by the community. While building is planned in stages over 6 or so years, the initial plans include much higher density than the proposed development in the rail yard next to Curtis Park that was agreed to with community input. The proposed four-story Brownstone and three-story living quarters add substantial population density to the area.

With the nearby high density public housing, a small city of people could be left demanding schools, public transportation, and other city infrastructure in this Land Park area. Because the Health Professionals High School is very small and specialized, we wonder if McClatchy would be the high school for 9 -12th grade for the new development. Would Cal be the middle school for this new development? There is an elementary school right next to the Setzer property, but is there room there to accompany all the new elementary students? If not would, Crocker Riverside be their school? Again traffic moving east on Vallejo Way and into our neighborhood could easily double or triple.

Certainly the higher densities of residential development planned will tax existing public resources. Less density would assure less demand on such costly services and infrastructure.

As we expressed at the LPCA meeting, the proposed high density of development would result in concerns about increased traffic in our nearby neighborhoods. My wife and I and other nearby neighbors worked for years with the City on traffic calming solutions for existing issues in the area. For instance, we had tree wells contracted in the middle of Vallejo Way between 5th Avenue and Riverside Blvd. to reduce the danger to children, pets and others from speeders, including public transit buses.

While the developer offered some initial thinking about most new resulting traffic heading north to downtown, higher density development would certainly cause traffic problems in our now quiet and peaceful neighborhood. The higher the density of development (especially in the phases of development furthest from Broadway), the more likely that our existing neighborhood will bear the brunt of more traffic, including frequent trips to such places as the schools mentioned above, and to grocery stores and other businesses on Freeport Blvd.

In closing, we hope that City of Sacramento works with the developers and our community to make this new neighborhood the best it can be. Very high on the issue list should be trying to reduce the density of units built, especially near the existing neighborhoods furthest from Broadway. Otherwise the development will over tax our existing community.

Please let us know if you need clarification of any of the points made here, or need anything else from us as the community input continues.

Sincerely,

Craig & Janene Chaffee

Exhibit C: Craig Chaffee comments dated November 2, 2010

Thank you for resending this material. We appreciate your help and development planning expertise.

I read through the documents you shared. They include a lot of thoughtful development plans. But I found Chapter 4 and Transportation Development Guidelines (P37) especially lacking.

I hate to sound like a broken record, but we have voiced and still have major concerns about the high density development and the resulting traffic issues.

I do not see the City or LPCA really concerned about the high density and traffic in an area already dealing with two huge City of Sacramento high density public housing units, Land Park Woods, and a new high school.

You told me that the Sacramento City general plan guidelines for the area call for 33 units per acre. I reminded you in previous phone calls and e-mails that the two large existing high density public housing complexes, plus the high density Land Park Woods apartments, plus the high density apartments on Vallejo Way (just east of the corner of 5th avenue) - are all in our area. All create major traffic problems for our neighborhood. I said that because of this already existing concentration of high density housing and traffic issues in the immediate neighborhood, there should be some ability to vary from the Sacramento City general plan requirements for 33 units per acre.

I know that higher density housing can also be more profitable to developers and land owners, but our neighborhood should not suffer.

When we talked on the phone about a month ago you seemed to agree that higher density (e.g., 3 and 4 story units -some also with lofts) development would be better placed in the later phases (four phases in all) that are nearest Broadway, and farthest away from the existing neighborhoods and Vallejo. You said that the latest Phase I plan does not quite meet the 33 unit requirement, and that you did not have a problem with that. We as neighbors really appreciate lower density development.

Over the last several decades our neighborhood has worked with the City, public housing officials, nearby apartment complex owners and others on related issues. We worked with Tom Chin and then Jimmie Yee. We had 5th Street closed where it intersected with 4th Avenue because of the very high traffic and related problems. We worked with the City on traffic issues. The City did a traffic study which showed the significant impact on our neighborhood of all the traffic coming from 5th Street, and using our neighborhood streets as a thorough fare. Our neighborhood worked with the City and Jimmie Yee over several years to have tree wells placed in the middle of Vallejo Way to discourage speeding and use of that last main street through our neighborhood. The City also implemented other "traffic calming measures" in our

neighborhood. Our neighborhood worked on the planning of the new high school to ensure it would be small and not create further traffic and related problems in our neighborhood.

The risks of unbearable traffic problems coming into our neighborhood are still extremely high. 750 to 1,000 units are planned when all four phases are completed. I figure that several thousand new people will be living in the development and using 5th street if the existing development plans are approved. Reductions in overall units and related populations, and reducing traffic problems to our neighborhood should be primary goals of your City planning efforts.

I was wondering if a street to connect 5th Street to Muir Way could be created to provide more major streets to reduce traffic congestion, especially on Vallejo Way. I realize that opportunities to connect the two major streets are limited because of the other high-density City of Sacramento public housing on Muir Way.

Also, I'm wondering if there are any street planning opportunities to encourage the new development residents to use Broadway as the major driving corridor, not 5th Street and Vallejo Way. For instance, do all the new streets in Phase of the development have to be connected to 5th Street? Instead can some dead-end, so that new residents are routed up towards Broadway through 3rd Avenue and 1st Avenue? If we lived there in Phase One I, would not want all the streets connected to 5 Street since this would encourage vehicle and foot traffic use by others in the high density public housing and apartments I mention above. I would prefer the privacy resulting from not connecting the streets through to 5th Street. I know there are fire and police access requirements, but that's where your planning expertise comes in – to blend both needs and make sure this development results in the best overall community possible.

I have CCed Luree Stetson on this e-mail. She is now heading efforts to work on major issues facing our neighborhood. I thought including her on this e-mail might help to impress on you that my wife and I are not lone voices with just personal concerns. These are major LONG TERM issues for our neighborhood. If these high density and traffic issues are not dealt with effectively, our existing neighborhood will be permanently damaged. To be frank, my wife and I have discussed moving if the development results in things getting too bad. We want to stay in the quite little neighborhood we've been in for over 30 years. We appreciate your help. Thank you for making sure that these issues are recorded and addressed as development plans move forward.



**GREATER BROADWAY PARTNERSHIP**

P.O. BOX 188182  
Sacramento, CA 95818  
(916) 737-1427

June 14, 2011

David Kwong  
Planning Director  
City of Sacramento Community Development Department  
300 Richards Boulevard, 3rd Floor  
Sacramento, CA 95811

**RE: NORTHWEST LAND PARK DEVELOPMENT**

Dear Mr. Kwong,

This letter is sent on behalf of the Greater Broadway Partnership (GBP) in regards to the Northwest Land Park Development. While we are pleased that substantive changes have been made during the course of working with the developer, we would respectfully request that the Planning Commission consider other modifications that we believe will greatly enhance the likelihood of the project's success.

First of all, we would like to acknowledge the developer's willingness over the last two years to meet with us, answer questions and provide clarification regarding the subject project. In a February 11, 2011 letter our Board of Directors documented numerous key concerns/principles that we hoped would be integrated into the project. We appreciate that three of these issues have been positively addressed. This includes: (1) a commitment on the part of the developer that an on site management and maintenance of the development will begin immediately with Phase 1. We were concerned with the prospect of "absentee" neglect and the subsequent deterioration that might occur. (2) a clarification by city staff, the developer and his attorney that the PUD guidelines are indeed binding and not discretionary. We were concerned that once approved, a lesser quality product might be built, and (3) a commitment to retain the historically significant (existing produce/market area) for adaptive re-use rather than demolition. We believed this will not only keep the history of the area alive but would give the development a true sense of place.

Several other suggested modifications were discussed but not adopted. As we feel these issues continue to have merit, we have included them for your consideration and action.

---

**1) Increase the Mixed Use in the Project.**

We would like to see more commercial uses incorporated into the earlier phases of the project, to truly provide a mix of uses and an urban character to the community, reflecting the character of Land Park's collector streets and Broadway's neighborhood corridor.

We would like to suggest "RMX-Special Planning District" zoning for the two areas scheduled to be completed in Phase 3 and 4, and designate these areas as MXD in the PUD Guidelines. This would give more flexibility to future developers by allowing the construction of residential, mixed-use or commercial buildings depending on future economic conditions. This zoning would create the potential for more commercial space to be incorporated into the areas closest to Broadway and encourage residential units to be built above commercial space.

**2) Modify the Street Layout to Increase Walkability and Separate the Residential Area from the Industrial Area.**

We are concerned about the proximity of the residential homes to the industrial area and would like to see better separation. Specifically, the homes that front 1<sup>st</sup> Avenue are too close to the industrial property. A sound wall has been proposed to address this concern but we think that this is inadequate. We would prefer to have 1<sup>st</sup> Street separate the existing industrial use from the new residential uses by aligning the street to the edge of the industrial property and locating residential uses across the street. Making this change would also address the dangerous zigzag intersection that is created by not aligning up 1<sup>st</sup> street with the parallel street to its west.

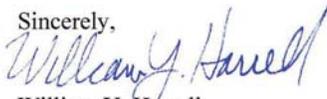
**3) Improve Open Space District**

The most recent iteration of the Open Space District (OSD) is a missed opportunity. We preferred the previous design of a meandering greenbelt running through the development. We like the concept of a trail or bike path leading through the community to the Marina, and to Front Street, as this will connect the community to non-residential uses. Even though the current park configuration still provides a trail connection by means of Setzer Run, the park itself does not have any adjacent uses to activate it. Urban parks are normally highly programmed spaces adjacent to commercial uses, so that constant pedestrian activity keeps the park safe and lively. The current proposal is suburban in character, surrounded by residential uses and lined by only two streets.

In conclusion, while we do not oppose the project we think it could be a much stronger development and a real asset to the area, if these remaining changes were made. We are hopeful the Planning Commission will agree with our observations.

If you have any questions regarding the above, please feel free to contact either myself or our Executive Director, Teresa Rocha, at (916) 737-1427 or at our email address: [greaterbroadwaypartnership@gmail.com](mailto:greaterbroadwaypartnership@gmail.com).

Sincerely,



William Y. Harrell  
President, Board of Directors

Exhibit E: WalkSacramento letter dated November 17, 2010



November 17, 2010

David Hung, Associate Planner  
City of Sacramento Planning Division  
300 Richards Blvd, 3rd Floor  
Sacramento, CA 95811

**RE: Northwest Land Park Project (P10-039)**

Dear Mr. Hung:

WALKSacramento has reviewed the July 23, 2010 and September 24, 2010 routings for the Northwest Land Park project (P10-039). The project location is a 32-acre site south of Broadway and east of I-5. The first of four phases of development will include 208 residential units on an 8-acre site. We appreciate the opportunity to submit the following comments.

The City of Sacramento - "City of Trees" and "Walking Capital" - needs trees in both public and private spaces to improve air quality, reduce heating and cooling energy demands, and present an attractive and pleasant environment to pedestrians, bicyclists, and motorists.

The hot, sunny summers and autumns in Sacramento make it important to shade walking routes with deciduous trees. It's also important to have vegetated areas and permeable areas that provide additional cooling of the air near ground level.

Looking at the land use summary table on the phase 1 site plan drawing, it appears the land area of the Northwest Land Park project site will be about 51% permeable surface. This should provide appreciable cooling for the project site, but the total area of shaded surface will be just slightly more than permeable surface area because only a small portion of the hardscape areas will be under the tree canopy.

The street tree canopy is severely compromised by the proposed subdivision design. The typical building setback for the paved public streets is ten feet. The public streets are proposed to have attached sidewalks, so trees planted halfway between back of sidewalk and building will be ten feet from the curb and five feet from the building. Due to the five-foot spacing between the street trees and the three-story façades, the tree size will be limited and the opportunity to shade the street will be lost. If the streets are not high-albedo concrete or permeable, the streets will sink a lot of solar energy and contribute to warmer air along the public sidewalks.

Trees provide other benefits besides a cooler environment for residents. A portion of the particulates from vehicles on the two nearby freeways can be intercepted and the noise from those vehicles can be attenuated. Trees with leaves or needles during the rainy season can also reduce the storm drainage infrastructure requirements.

We recommend the following to allow for planting of medium and large trees to create a cooler, quieter, healthier, more sustainable neighborhood for pedestrians, bicyclists, and residents.

1. **Use the City's pedestrian-friendly street standard 53' right-of-way residential street (with detached sidewalks) in place of the proposed street cross sections with attached sidewalks for 4<sup>th</sup> Street, Tailoff Way, Lug Way, and Cleat Way.**
2. **Use the City's pedestrian-friendly street standard 59' right-of-way local commercial street (with detached sidewalks) in place of the proposed street cross section with attached sidewalks on 5<sup>th</sup> Street.**

Compaction of the soil in the narrow landscape setbacks inhibits growth and shortens the life of trees, further diminishing the shade canopy. Structural soil improves the growth and health of trees by allowing more oxygen, water, and nutrients to reach the roots. We recommend that structural soil be used where the planter areas are small and the tree canopy size is important.

3. **Use structural soil wherever possible for planting shade trees.**

Even if detached sidewalks were constructed, the small landscape setbacks would still place limits on the ultimate size of mature trees. Table 2.4 in the PUD Guidelines allows for upper story portions of buildings to extend three feet into the primary building setbacks and this would further limit tree sizes. The upper story setback should be greater, not less, than the primary building setback to allow for more tree canopy growth.

4. **Upper story front setbacks should be greater than primary building setbacks.**

The phase 1 tentative map shows 4<sup>th</sup> Street adjacent to the eastern edge of Arthur Benjamin High School, which allows for houses facing the school. The opportunity for "eyes on the street" to provide a sense of greater safety hasn't been fully exploited, though. While both of "The Towns" units, all three of the "City Homes" units, and three of the five "Bungalows" units have active living space at the front facing the school, only two of the ten "Urban Courts" units have "eyes on the street".

5. **Add more "eyes on the street" to the "Urban Courts" homes along 4<sup>th</sup> street.**

The internal pedestrian circulation, as shown on the Phase 1 Circulation Plan drawing, provides connections between the streets and to most of the residential units (the rest are accessed by private entry walks). The Special Permit Site Plan – Phase 1 drawing identifies the internal walkways as three-foot wide, typical, while the private entry walks are not shown. Three feet is not wide enough for two people to walk side by side or to pass one

another comfortably. We recommend the internal and private entry walkways be widened to at least five feet.

**6. Increase width of internal walkways and private access walkways to at least five feet.**

The “Neighborhood Center” is located midpoint between the phase 1, 2, and 3 areas. If it was a facility for private use by the residents of Northwest Land Park, the central location would be logical. Since the facility, if constructed, would be built by the City for public use, the center should be located closer to the surrounding communities and within walking distance of more residents.

**7. Move the “Neighborhood Center” to 5<sup>th</sup> Street.**

The drawings are not consistent in showing the location of the multi-use trail that connects Setzer Run trail to Festival Way. Most of the drawings show the trail separated from 4<sup>th</sup> Street, but the Overall Circulation Plan shows the trail at the edge of the street where the sidewalk would be. Placing a multi-use trail within the public right-of-way makes it difficult for users to determine the function of the trail and how they and others should behave.

**8. Separate the multi-use trail (Class I bikeway) from sidewalks and streets.**

We recommend the Setzer Run multi-use trail have as direct a path as possible between the Miller Park Tunnel and 5<sup>th</sup> Street. This will provide efficient travel and reduce conflicts between modes. The proposed pathway adds an extra four hundred to five hundred feet of travel distance.

**9. Add a Class I bikeway directly between Festival Way and Setzer Run.**

The pedestrian travel way between Tailoff Way at 4<sup>th</sup> Street to the southern segment of Log Pond Loop (Circle?) is too long. Connecting Tailoff Way to Log Pond Loop could improve access and mobility for all modes and improve “eyes on the park” and “eyes on the schools”. The additional street would come close to completing the grid in Northwest Land Park.

As an aside, note that the grid is incomplete for two reasons. One, already mentioned, is that Tailoff Way is not connected to Log Pond Loop. The second reason, although not on the project site, is the lack of a roadway along the west side of Jedediah Smith Elementary School. A new street between the existing McClatchy Way and Log Pond Loop would provide better access to the schools and the Northwest Land Park neighborhoods. It appears there might be room for a residential street with no parking between the school playground fence and the Caltrans right-of-way.

**10. Extend Tailoff Way (Phase 1) to Log Pond Loop (Phase 3).**

The phase 1 block lengths of 400' to 450' are acceptable, but phase 2 blocks are from over 500' to over 700'. Blocks of this length are not pedestrian friendly in suburbs, much less in a medium-density urban residential neighborhood such as Northwest Land Park.

**11. Reduce the Phase 2 block lengths to less than 500'.**

Providing mixed-use commercial space is a benefit to residential neighborhoods, but it should be in a convenient location to be neighborhood-serving. The northwest corner of the project site is, perhaps, the least convenient location for pedestrians to access the mixed-use commercial space. There are few trip generators directly north and to the west because of the freeways, so most pedestrian trips will be residents to the south and east. Also, because it's on the west side of the project, it's not located where it will be convenient for transit riders using the existing local bus route.

**12. Move the neighborhood-serving mixed-use to the east side of the project site.**

The Miller Park tunnel option provides for a possible pedestrian and bicycle connection between the Land Park neighborhood and the Sacramento River Parkway bicycle trail and destinations along the river. The tunnel would greatly improve pedestrian and bicycle access to the Central Business District, the River District, and West Sacramento, making non-motorized commuting more attractive to Land Park residents.

**13. Ensure that access to the potential Miller Park tunnel is retained.**

We believe the Northwest Land Park project has good potential to be excellent infill development. The sidewalk, street tree, and connectivity issues we have identified may be easier to resolve with a change to block sizes. Although we prefer smaller blocks to provide shorter travel distances and more travel routes, wider blocks might allow detached sidewalks and more streets without affecting the density. The proposed internal walkway network for phase 1 is extensive, although of inadequate width, and provides connections to the public sidewalks. Wider blocks in phase 1 would not diminish pedestrian mobility and access if mid-block internal walkways provided public right-of-way through an easement.

*WALKSacramento* encourages people to walk and bicycle in their communities. The benefits include improved physical fitness, less motor vehicle traffic congestion, better air quality and a stronger sense of cohesion and safety in local neighborhoods.

*WALKSacramento* reviews land-use development proposals and provides recommendations to create neighborhoods, schools, shopping centers, office buildings, streets, and parks more walkable, bikeable, and transit friendly.

Thank you for your consideration of these comments and recommendations. If you have questions or need additional information, please contact me at (916) 446-9255 or [cholm@walksacramento.org](mailto:cholm@walksacramento.org).

Sincerely,



Chris Holm  
Project Analyst  
*WALKSacramento*  
909 12<sup>th</sup> Street, Suite #122  
Sacramento, CA 95814

Exhibit F: ECOS letter dated March 24, 2011

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

---

**From:** keitheroberts@aol.com [mailto:keitheroberts@aol.com]  
**Sent:** Friday, March 25, 2011 10:25 AM  
**To:** ramrhein@shra.org; Jennifer Hageman; Dana Allen  
**Subject:** 7th and K DEIR comments

**Hi Jennifer, Ms. Amrhein,**

Attached are ECOS comments on the 7<sup>th</sup> and K Street Block DEIR. Please let me know if you get this email, otherwise I'll deliver a hard copy before 4/4.

This email contains only the summary letter and Attachment A. I will send Attachments B through F separately.

**Hi Dana,**

ECOS made some improvement in the 7<sup>th</sup> and K DEIR review that would be applicable to the NW Land Park DEIR. Certainly the City need not address this late comment, but it could make the project more attractive to all parties involved.

For the NWLP review, ECOS simply asked for the project proponents to reach towards meeting a fair share greenhouse gas (GHG) emissions reduction. ECOS did not like asking a good project to incur costs to reach towards an aspirational goal, so for the 7<sup>th</sup> and K project, we modified the comment and asked for the City to develop both a mandatory GHG threshold and a VOLUNTARY stretch GHG threshold (aka Tier 1 threshold).

To encourage the project proponents to meet the voluntary Tier 1 threshold, ECOS also suggested that a concept recommended from the Greenwise Initiative be implemented- using an investment pool to fund cost effective energy efficiency. See Attachment A, page 6 for more detail.

I hope that these additional concepts can help make these 2 projects more efficient (e.g. improve upon Title 24 by 15%), not affect project budgets, provide a reasonable return to interested investors, and meet many of the goals of the Greenwise Initiative!

Keith  
205-6085



**Via Electronic Mail**

24 March 2011

Jennifer Hageman, Senior Planner  
City of Sacramento, Community Development Department  
300 Richards Blvd, 3<sup>rd</sup> Floor  
Sacramento, CA 95811  
Email: [jhageman@cityofsacramento.org](mailto:jhageman@cityofsacramento.org)

Shelly Amrhein  
Sacramento Housing & Redevelopment Agency  
801 12th Street  
Sacramento, CA 95814  
Email: [ramrhein@shra.org](mailto:ramrhein@shra.org)

**Re: Comments on 7<sup>th</sup> and K Street Block Draft Environmental Impact Report**

Ms. Hageman and Ms. Amrhein,

These comments are submitted on behalf of the Environmental Council of Sacramento (ECOS) on the 7<sup>th</sup> and K Street Block Project (Project) Draft Environmental Impact Report (DEIR). ECOS is a coalition of environmental and civic organizations with a combined membership of more than 12,000 citizens throughout the Sacramento Region. Our mission is to achieve regional and community sustainability and a healthy environment for existing and future residents.

ECOS supports infill development and is therefore very supportive of the Project. The proposed roof top garden, reuse of existing buildings, great mix of street level business and mid-level residences will help revitalize the blighted area and assist in bringing the downtown to life! Although not well analyzed (which is required by CEQA), this project appears to be one of the lowest greenhouse gas (GHG) emitting projects per capita in the Sacramento region!

**Congratulations are due to the project proponents!**

Addressing climate change in EIR documents is a nascent process that is rapidly evolving. Because of the newness and due to City staff cutbacks, the City has not been able to develop the tools necessary to clarify the process for project

proponents and thus confusing and inaccurate EIR documents are prepared for the City's use (e.g. North West Land Park DEIR (see ECOS comments dated February 17, 2011) and this DEIR). Several of ECOS comments, if implemented, could improve clarity of this **and future** EIR's. Some of these global comments are:

- The City needs to address the conflict that exists between the General Plan Update (GPU) estimate of greenhouse gas (GHG) emissions in 2030 and Sustainability Master Plan's (SMP) 2020 and 2050 GHG targets
  - GPU is also internally inconsistent with policy ER-6.1.3
- Adopt BAAQMD screening criteria (interim)
- Adopt BAAQMD GHG threshold of 4.6 MT/yr-sp (interim)
- Work with SMAQMD and other local jurisdictions to develop a regional GHG threshold (permanent)
- Develop a Frequently Asked Questions fact sheet (or flow chart) regarding GHG requirements for projects (see Attachment D)
- Develop a Best Management Practices (BMP) requirements sheet for small (de minimis) projects (see Attachment E)
- Consider requiring projects to self-certify how well they comply with Greenwise Initiative Objectives, Strategies and Action (see Attachment F)
- The City has not apparently attempted to optimize the life cycle cost of water, sewer and storm systems. Infrastructure and efficiency should be evaluated using integrated resource planning concepts

In addition, since this project seems to meet the BAAQMD GHG threshold, consider developing a voluntary Tier 1 "stretch" threshold and use Mayor Kevin Johnson's Greenwise investment concept to encourage this project to "stretch" beyond the mandatory 4.6 threshold, which in turn will minimize "lost opportunities"<sup>1</sup>.

In addition to the Greenwise Initiative's innovative financing strategies that help make energy efficiency improvements a non-budget issue to project proponents, design features and ECOS recommendations combine to achieve over 5-dozen Objectives, Strategies and Actions from the Mayor's Plan! See Attachment F.

Beyond the global comments summarized above, ECOS' main concern on this Project deals with the DEIR's treatment of climate change. Our major comments are summarized as follows:

- DEIR improperly uses GPU as a programmatic document to state that GHG mitigation and enforcement is not required
- GHG analysis is not provided

The comments summarized above are more fully addressed in Attachment A.

---

<sup>1</sup> A lost opportunity is a greenhouse gas mitigation measure that is cost effective when installed as part of new construction, but prohibitively expensive when considered as a retrofit measure.

In closing, ECOS does appreciate the opportunity to comment on the Draft Environmental Impact Report for the 7<sup>th</sup> and K Street Block Project. To reiterate, ECOS supports infill development and is **very** supportive of this project. Our goal is to ensure that this **and future** projects effects upon climate change are well-addressed, properly mitigated in EIR documents and subsequent construction.

With the incorporation of the design recommendations made by SABA (DEIR page 2-3) and addressing numerous deficiencies that we have identified concerning climate change in this document, this could not only be an exemplary infill project, but it could be a State gem! If you would like to meet with ECOS representatives responsible for these comments, please contact Keith Roberts [keitheroberts@aol.com](mailto:keitheroberts@aol.com) .

Yours very truly,

/s/ Jonathan Ellison

Jonathan Ellison, President  
ECOS Board President

Attachment A- Global and Project Recommendations

Attachment B- BAAQMD Case Studies

Attachment C- 7<sup>th</sup> and K Measurable and Enforceable Climate Action Plan  
(concept)

Attachment D- FAQ: How To Address Climate Change In Development Projects  
(concept)

Attachment E- Best Management Practices for Small Projects (concept)

Attachment F- Consistent with Greenwise Initiative Goals

## **ATTACHMENT A- Global and Project Recommendations**

### **I. Two Official City Documents Conflict and GPU is Internally Inconsistent**

#### **A. General Plan Update (GPU) and Sustainability Master Plan (SMP) Conflict**

The GPU, approved by City Council in March 2009 states that GHG emissions in the City will INCREASE by 49% to 7.57 million tons per year by 2030<sup>1</sup> (6.87 million metric tons per year or 20,737 US tons/day) relative to 2005 base year.

The SMP, approved by City Council in December 2007 states:

- that the City GHG emissions will DECREASE by 15% to 3.9 million metric tons per year by 2020<sup>2</sup>
- that the City GHG emissions will DECREASE by 80% to 790,000 metric tons per year by 2050 to meet its share of SACOG emissions reductions

The projected GPU increase and required SMP decrease in emissions clearly conflict. Additionally, it should be noted that the SMP's 2020 and 2050 targets are based on science, are similar to other's jurisdictional long-term targets, and that substantial evidence exists to justify the targets.

Addressing this inconsistency will reduce confusion of EIR preparers and clarify this **and future** EIR's.

#### **B. GPU Internal Inconsistency**

GPU MEIR Appendix K states:

ER 6.1.3 Greenhouse Gas Reduction Goal. The City shall work with the California Air Resources Board to comply with statewide greenhouse gas reduction goals as established in the Global Warming Solutions Act of 2006 (AB32) for 2020 and any subsequent targets. (RDR)

The GPU, Table 8.3 states that GHG emissions in 2030 will be 20,737 tons/day (or 7.57 million tons per year or 6.87 million metric tons per year).

The GPU Table 8.3 and Appendix K are internally inconsistent.

- Table 8.3 indicates that GHG emissions will increase from 4.6 million metric tons per year in 2005 (Jones and Stokes Study, Feb 2009) to 6.87 million metric tons per year in 2030- a 49% INCREASE
- Appendix K calls for a 15% DECREASE from 2005 emission levels by 2020

#### **C. Consistency With State Law**

<sup>1</sup> GPU MEIR, Table 8-3

<sup>2</sup> Sustainability Master Plan, December 2007

The SMP and the GPU's policy ER 6.1.3 are both consistent with AB32 and should be used as a basis for all GHG analyses.

The GPU GHG analysis is inconsistent with AB32 and should not be used for any type of GHG analyses or as a programmatic EIR for GHG mitigation.

## **II. Improper Use of GPU as a Programmatic Mitigation Document**

A discussion of greenhouse gases (GHG) and analyses performed under the General Plan Update (GPU) MEIR takes place beginning on page 7-6. The discussion indicates that GHG mitigation is not necessary because the project is consistent with the GPU GHG projections.

- There are only specific instances where a DEIR can use a programmatic document (GPU) to determine that a GHG impact is less than significant and that mitigation is not required. The criteria are outlined in the new CEQA Guideline 15183.5 and 15064(h)(3). The programmatic document must show that it (GPU or CAP) actually has specific and enforceable measures in place that reach the City's GHG goal and that the goal is sufficient based on substantial evidence, to render impacts less than significant.

The 2009 GPU does not meet these criteria so the DEIR can't say that the GPU nullifies the need to adopt GHG mitigation for the Project.

The City is currently developing a climate action plan (CAP) that is intended to amend the GPU to some extent so that it can serve as a programmatic document for development projects. Until the CAP is complete, City projects must mitigate GHG emissions on a project-by-project basis.

By developing a FAQ fact sheet (see Attachment D) this inconsistency seen in the last 2 EIR's (also NW Land Park) will no longer confuse EIR preparers and this and **future EIR's** will be much clearer.

## **III. GHG Significance Threshold Is Not Identified**

The City has not adopted a GHG threshold of significance, which is typical throughout the State; this does not mean that there is no reasonable way to determine significance, or to determine a project's "fair share" of emissions reductions.

By adopting a significance threshold, the City would be informing, and minimizing confusion of, project proponents of the City's requirements and improve clarity of this **and future** EIR's.

CEQA Guidelines include:

- The GHG emissions from the Project are individually limited, but cumulatively considerable and therefore have a significant effect on the environment. (State CEQA Guidelines, § 21083(b).)
- A cumulatively considerable impact can be made less than significant if a project implements or funds a fair share of applicable mitigation measures. (State CEQA Guidelines, § 15130(a)(3).)
- For CEQA purposes, a significance threshold identifies a level below which an environmental impact will normally be less than significant. (State CEQA Guidelines, § 15064.7(a).)

The above infers that the Project GHG emissions would be less than significant if the Project were to meet a significance threshold that is based on achieving a “fair share” of the necessary GHG reductions required to stabilize the Earth’s climate.

**A. Fair Share GHG Significance Threshold**

Many air districts are attempting to develop GHG thresholds for new development, although no air district has suggested that development projects meet their “fair share” of GHG emissions reductions as is required by CEQA.

ECOS has used Statewide estimates of population to determine a projects “fair share” emissions rate within the State. The following table identifies the results:

Year Building Permit Issued	Fair Share Emissions Threshold (MT/yr-sp)
2011	2.5
2015	2.2
2020	1.9
2025	1.5
2030	1.2
2040	0.8
2050	0.7

The above table is based upon:

- Statewide service populations for 2020 and 2050
- Statewide emissions targets for 2020 and 2050 (based on AB32’s 2020 target and S 3-05’s and Sustainability Master Plan’s 2050 target)
- Average 50 year building life
- Weighted average of service personnel metric over the buildings life time

See EXAMPLE CALCULATIONS, below.

ECOS has used a 50-year building life in the analysis because although it may be reasonable to use 2020 GHG targets for short-lived GHG emitters such as cars (e.g. +/- 10 year life), long-term 2050 targets must be considered in any analysis when long-lived GHG emitters such as buildings and land use (e.g. 50+ year life) are considered.

- **In other words, TODAY's land-use decisions WILL affect emissions in 2050.**

There may be other scientific methods for calculating a projects "fair share" emissions, however ECOS has seen no other method proposed. Any other scientifically reasonable method will likely yield similar results.

EXAMPLE CALCULATIONS:

1. Allowable emissions rate for 2020 (one year only)

$$4.6 \text{ MT/yr-sp} = (295.5 \text{ million metric tons/yr}) / (64.3 \text{ million service personnel})^3$$

where:

- 295.5 million metric tons/yr are allowable land-use related GHG emissions for 2020 (i.e. 1990 emissions)
- 64.3 million service personnel forecast for 2020 (44.1 million residents and 20.2 million workers)

2. Allowable emissions rate for 2050 (one year only)

$$0.7 \text{ MT/yr-sp} = (59.1 \text{ million metric tons/yr}) / (80.1 \text{ million service personnel})$$

where:

- 59.1 million metric tons/yr are allowable land-use related GHG emissions for 2050 (i.e. 1990 emissions – 80%)
- 80.1 million service personnel forecast for 2050 (extrapolated by ECOS)

3. Allowable emissions rate for building permitted in 2020 (50 year average)

$$1.9 \text{ MT/yr-sp} = \{30 \text{ years} * [1/2(4.6+0.7)] + 20 \text{ years} * [0.7]\} / (50 \text{ years})$$

where:

- Building will emit GHG's from 2020 through 2069
- From 2020 through 2049, annual rate will drop from 4.6 to 0.7
- From 2050 to 2069, annual rate will remain constant at 0.7

**B. Bay Area Air Quality Management District Significance Threshold**

The Bay Area Air Quality Management District (BAAQMD) recently adopted a 4.6 MT/yr-sp significance threshold using the 2020 targets and service population (see calculation A.1 above). Note that this threshold is 80% higher than the 2.5 MT/yr-sp "fair share" threshold identified for projects permitted in 2011!

<sup>3</sup> BAAQMD, California Environmental Quality Act, Air Quality Guidelines, (June 2010); Table D-7

The adopted threshold, which does not achieve a “fair share” emissions reduction, was a hard fought compromise settled upon by a diverse pool of stakeholders. ECOS believes that implementing a “fair share” emissions threshold would be politically impossible to achieve at this point in time.

- Following sections suggests that a voluntary “stretch” threshold (with incentive mechanism) be developed to encourage projects to reach towards meeting a “fair share” emissions rate.

Also, the BAAQMD has found that:

- well-designed infill projects comply with the basic 4.6 threshold with little to no effort and
- that suburban sprawling projects have great difficulty in complying without the use of off-site mitigation

See Attachment B for examples.

### C. 7<sup>th</sup> and K Street Block Plan Emissions

Based on the following assumptions<sup>4</sup> this project will emit 4.4 MT/yr-sp, which WOULD satisfy the BAAQMD significance threshold if it were in place in Sacramento! Where:

4.4 MT/yr-sp = 2,165 metric tons/year / (306 residents + 182 workers)

- 2,165 metric tons/yr (page 7-7 of DEIR)
- 306 residents (153 du at 2.0 residents per du)
- 182 workers (63,780 SF / 350 SF per worker)

If all assumptions are correct this project would NOT require any GHG mitigation including measures that are typically always cost effective such as:

- Beating Title 24 energy code by 15 to 20%<sup>5</sup>
  - Page 7-7 of DEIR indicated that construction would meet State energy code, but ECOS found nothing in the DEIR suggesting that the Project was attempting to improve upon code energy requirements, yet the PG&E document suggests that cost effectiveness would be reasonable for this type of new construction
  - Analysis of beating Title 24 energy code needs to be done
- Allowing fewer car spaces than code requires, this measure could and should be linked to implementing other non-auto, transportation friendly GHG mitigation measures such as:
  - Unbundling of apartment rents and car space rents (SABA request)
  - Provide community car share vehicles

---

<sup>4</sup> GHG analysis is not documented in DEIR

<sup>5</sup> PG&E Codes and Standards; Title 24 Energy-Efficient Local Ordinances; Title: Climate Zone 12 Energy Cost-Effectiveness Study (SMUD Electricity & PG&E Gas Rates); Feb 7, 2011

- Provide community NEV's (for short trips)
- Provide community bicycles for residents
- Provide bike locker room for residents

Many GHG mitigation measures are low cost when installed as new construction, but prohibitively expensive when considered as retrofit measures. These types of measures are known as "lost opportunities". Beating Title 24 by 15 to 20% would be considered a lost opportunity, if not included.

**There is a dilemma! The Project apparently complies with a reasonable GHG threshold, yet cost effective "lost opportunities" will exist. What can be done?**

- **Consider a voluntary Tier 1 GHG significance threshold**

#### **D. Voluntary Tier 1 GHG Significance Threshold**

The California Green Code<sup>6</sup> was recently adopted by the State. The Green Code contains mandatory measures that are required of all new construction within the State and also contains voluntary "stretch" goals known as Tier 1 and Tier 2 standards.

The climate crisis demands that all feasible action be taken to reduce greenhouse gas impacts from all project types, yet:

- BAAQMD experience indicates that:
  - sprawling suburban projects have a difficult time meeting the basic threshold of 4.6 MT/yr-sp;
  - well designed infill projects seem to meet the basic threshold with little trouble;
- this Project which is well designed should not be asked to perform better than a poorly designed project, unless it is incentivized to do so;
- "lost opportunity" GHG mitigation measures should be minimized and;
- effort should be made to cost effectively reach towards meeting a "fair share" emissions rate.

As part of **Mayor Kevin Johnson's Greenwise Initiative**, the concept of developing a program whereby local residents and businesses could invest in local, cost effective energy efficiency projects through an investment program was recommended<sup>7</sup>.

The combination of these events leads ECOS to suggest that the City develop a voluntary Tier 1 significance threshold and that projects that attempt to reach Tier 1 be allowed to use Greenwise investment funds.

<sup>6</sup> California Building Standards Commission; [www.bsc.ca.gov](http://www.bsc.ca.gov)

<sup>7</sup> PACE type financing might also meet this need if implementation problems are solved

The Greenwise Investment Pool (GIP) could operate similar to the SMUD Solar Shares program. Property owners would be given first right of refusal to pay an extra \$1,000<sup>8</sup> for their home or they could agree to pay a fixed monthly fee of \$10.12 for 10 years (3% APR dividend for GIP shareholders and 1% admin fee<sup>9</sup>). If average monthly utility savings is less than \$10.12, then loan term could be set for 15 (\$7.40/ month) or 20 years (\$6.06/ month) so that cash flow would always remain positive. The fee would be shown as a surcharge on SMUD (and/or PG&E) bills (and the estimated dollar savings could be shown as a credit). The project proponents could be given second right of refusal to be shareholders. GIP shareholders would be default investors. All appropriate utility incentives and tax credits would be used to reduce capital cost of the efficiency.

- If an investment pool can't be formed in the necessary time frame, perhaps SMUD or the City has some unexpended ARRA funds from the Community Block Grant Program that could be used to invest in this pilot project and test some of the tenets of the proposed investment pool.
- If SMUD (and/or PG&E) were the sole shareholder(s), this would be known as "on-bill financing"

Advantages that project proponents would get out of this process are:

- public relation kudos for allowing the City to implement an innovative program to reduce carbon
- additional kudos for green job growth
- no up front capital cost increases
- reasonable construction overhead and profit on added efficiency
- second right of refusal to become shareholders

In addition, the CARB Cap and Trade program (not available until 2012) and SMAQMD Indirect Source Rule program (currently on hold) should be reviewed to determine if investment in projects that strive to meet "stretch" goals is appropriate.

#### **E. Recommend City Adopt Mandatory Basic and Voluntary Tier 1 Significance Thresholds**

To provide guidance, reduce confusion amongst EIR preparers, improve clarity of this and future EIR's and to achieve parity with other jurisdictions in the State (e.g. Bay Area, Santa Barbara), ECOS recommends that the City as quickly as possible:

- adopt an interim mandatory basic GHG significance threshold, and furthermore ECOS suggests that the BAAQMD per service personnel metric of 4.6 be adopted as a minimum
- adopt a voluntary "stretch" Tier 1 significance threshold of [4.0]<sup>10</sup> MT/yr-sp

---

<sup>8</sup> \$1,000 is an example; the 10, 15 and 20 year repayment amounts are correct for \$1,000 expense at 4% APR

<sup>9</sup> actual dividend and admin percentage rates are to be determined

<sup>10</sup> to be determined

Without guidance from the City, confusion will continue as is evidenced by this DEIR and by the NW Land Park DEIR; (see ECOS comment letter of February 17, 2011) and climate change mitigation will continue to be inadequately addressed in City EIR's. This is not "Bringing The Customer To Success".

The Sacramento Metropolitan Air Quality Management District (SMAQMD) is beginning the process of developing a significance threshold for use in its district. ECOS recommends that the City work with the SMAQMD and other local jurisdictions to develop a permanent regional GHG significance threshold.

#### **IV. Screening Criteria**

Screening criteria is used by lead agencies to determine whether a particular project emits a sufficient quantity of pollution to warrant evaluation in an EIR. Screening criteria is generally selected by a lead agency to assure that 75% of emissions of a particular pollutant released to the atmosphere by a sequence of new projects are evaluated in EIRs.

The City has not adopted screening criteria for GHG. By adopting screening criteria, the City would be informing, and minimizing confusion of, project proponents of the City's requirements and improve clarity of this **and future** EIR's.

Table 3-1<sup>11</sup> suggests that the Project is roughly 3 times larger than the minimum criteria and thus should require a GHG analysis in the EIR.

#### **V. Other Potential Clarifying Documents**

##### **A. Frequently Asked Questions (or Flow Chart)**

It may be possible to reduce the confusion of project proponents if the City were to develop a Frequently Asked Questions fact sheet (or flow chart).

- See Attachment D for a concept FAQ fact sheet

##### **B. Small Project Best Management Practices Requirements**

If the City does adopt screening criteria for GHG, it may be reasonable to require exempt (de minimis) projects to proscriptively mitigate for GHG. Mitigation proscriptions could be provided in a checklist of requirements that must be implemented.

- See Attachment E for a concept requirements list

##### **C. Consistent with Greenwise Initiative and Sustainability Master Plan Goals**

---

<sup>11</sup> BAAQMD, California Environmental Quality Act, Air Quality Guidelines, (June 2010);

In May 2010, Mayor Kevin Johnson introduced the Sacramento region to the Greenwise Initiative, an effort to move not only the City, but the region, towards the vision of an Emerald Valley where green jobs would be created, the environment would be protected and citizens would be engaged to work towards achieving a sustainable life style.

To provide better connectivity between the City's aspirational goals and day-to-day business operations, it might be reasonable to request project proponents to self-certify how well their project meets City sustainability goals.

ECOS compared the Project as designed with ECOS recommendations implemented and found that the Project achieves **well over 5 dozen of the Objectives, Strategies and Actions (OS&A) identified in the Greenwise Initiative Regional Action Plan!**

Some of these OS&A's include:

- Innovative Financing to Improve Energy Efficiency
- Increase Green Jobs
- Invest in Sacramento's Green Economy
- Reduce Water Consumption by 20%
- Reduce Energy Consumption by 15%

For a more complete view of the Project's potential accomplishments with respect to the Mayor's Plan, see Attachment F.

The Project with ECOS recommendations also compared favorably to the Greenwise Initiative's predecessor - the Sustainability Master Plan.

## **VI. The DEIR's Analysis of Impacts from the Project's Greenhouse Gas Emissions is Inadequate**

CEQA requires a "good faith effort at full disclosure." Guideline § 15151. The DEIR claims that Project emissions are 2,165 metric tons. However, the brevity of the discussion makes it unclear if this is attributable only to mobile source emissions or the entirety of the Project's carbon footprint, including emissions from electricity and natural gas, water consumption, solid waste, and refrigerants.

Modeling is readily available to provide data on emissions from these sources. (See, e.g., SCAQMD, CalEEMod (February 2011); BAAQMD, Greenhouse Gas Model User's Manual (April 2010); CAPCOA, CEQA & Climate Change (2008).)

To comply with CEQA's informational requirements, please provide additional analysis on the greenhouse gas emissions resulting from the Project.

### **A. The DEIR Fails to Adequately Mitigate Project Impacts**

Once the Project's greenhouse gas impacts are properly quantified and analyzed, the DEIR should consider adoption of specific measures to reduce emissions.

Recommended Guidelines: CAPCOA recently issued *Quantifying Greenhouse Gas Mitigation Measures* (Aug. 2010). This important document offers specific guidance on potential GHG mitigation and how GHG reductions from adopting this mitigation would be quantified. Please review these measures and adopt all feasible mitigation set forth in the CAPCOA document. If a measure is rejected as infeasible, please explain the basis for rejecting that measure. Adoption of measures in the CAPCOA Mitigation report can provide an informative route to reducing per capita emissions from the Project to 4.6 MT and below.

Other Sources of Guidance: In new and emerging processes such as the CEQA treatment of GHG emissions and climate change impacts, it is frequently of value to review the actions of others and incorporate the positive aspects of their findings. Attachment B is a case study that the BAAQMD performed in support of its effort to develop a GHG threshold. The case study reviews the design features of a selection of Bay Area projects and estimates the threshold with and without the added design features. Some of the common threads that run throughout the case study are:

- a. Beat Title 24 by 20%
- b. Drought tolerant landscaping
- c. Low flush toilets and fixtures
- d. Reduce solid waste by 10%
- e. Solid waste energy recovery at landfill
- f. Solar hot water or electric
- g. Various transit demand management features

Attachment C was first submitted to Sacramento County as a concept/example of an effective, measurable and enforceable climate action mitigation plan (CAMP) in January 2010 for the Florin-Vineyard Gap (FVG) DEIR; again in October 2010 for the FVG FEIR; and again in February 2011 for NW Land Park DEIR. The CAMP has been slightly modified to meet the design aspects of the Project. The CAMP although similar to a few other measurable lists has two innovations that attempt to make it more effective than other lists:

- Market Transformation: The CAMP rewards project proponents that implement market transforming GHG reducing measures
- Rewards Local Governments: Local governments are increasingly implementing policies and ordinances that reduce GHG emissions within their communities; the CAMP rewards project proponents that develop projects in jurisdictions that have implemented specific GHG reduction policies and ordinances; this in turn allows the City to "Bring The Customer To Success".

Similar to LEED, both market transforming project measures and state-of-the-art ordinances and policies should be periodically updated.

### **B. The DEIR Skirts its Obligation to Adopt Effective Mitigation for Project Greenhouse Gas Impacts**

The DEIR's improper analysis that led to the flawed conclusion that GHG mitigation is not required amounts to an improper end-run around CEQA's requirement to adopt all feasible mitigation and alternatives. As a result, the DEIR fails to adopt meaningful measures that would reduce Project impacts, including improved efficiency, reduced solid waste, improved transit options and increased use of on-site renewable energy.

### **VII. Economics of Water Supply and Demand Is Not Evaluated**

Few sectors attempt to optimize costs between supply and demand of a commodity, however for over 20 years the electricity and natural gas energy sectors have attempted to optimize and integrate the overall economics of supply and demand of their commodity. This process is known as integrated resource planning (IRP). There are many reasons for IRP such as:

- Energy utilities are generally for-profit and try to minimize overall costs<sup>12</sup>;
- Supporting infrastructure is very expensive, especially electricity
  - so is sewer (and probably potable water);
- To reduce environmental impacts;

but the overriding goal is to reduce overall costs to the consumer. Optimizing the end use of a commodity is frequently called demand side management or DSM.

On a smaller scale, UC Davis - in many respects a small city - owns both supply infrastructure and demand resources (i.e. buildings). UC Davis has understood since the early 1990's that **making its buildings more resource efficient** through DSM **is frequently far less expensive** than building the supply infrastructure to support inefficient buildings (i.e. code compliant buildings do not necessarily minimize overall capital costs). **In addition, DSM reduces long-term operating costs!** Since the mid-1990's other UC and CSU campuses, as well as other Universities, have adopted the UC Davis life cycle costing concept.

- The analogy becomes a bit more tenuous when the City owns the supply infrastructure and the citizens of the City own the buildings, however if one assumes that the City exists to serve its citizens, the analogy is identical to the UC Davis example.
- UC Davis found that it was sometimes spending as much as \$3 to \$5 per unit of infrastructure, when \$1 per unit in building side DSM would have permanently eliminated the need for the unit of infrastructure.
  - Does the City and Regional Waste Water Treatment Plant unknowingly subsidize building project infrastructure?

---

<sup>12</sup> Investor owned utilities such as PG&E have greater reason to implement DSM than municipally owned utilities such as SMUD, however DSM is a valuable tool to all electric utilities.

- In other words, meeting code is not necessarily the best long-term, cost-effective solution for the community.

Demand side management (DSM) by the water, sewer and storm purveyor (City) is not well documented in the DEIR. The Project does include a roof top garden which is a **wonderful** storm water management measure, the City does reduce storm water impact fees for projects that use low impact storm water management, and the City does have a small water efficiency improvement office, yet little of this is outlined in the DEIR and there is no analysis of other DSM features that could be implemented to reduce long-term costs for the City's water, sewer and storm customers (and the region's sewer customers).

Would it be cost effective for the community to mandate water efficiency that exceeds code (e.g. Green Code Tier 1 or Tier 2)? Based on UC Davis' experience and the high cost of water and sewer infrastructure, better-than-code water efficiency may be a bargain.

Should the City consider scaling up its DSM program (including water reuse) instead of planning for future potable water plant expansions? Absolutely, if it is found to be cost effective through integrated resource planning!

## Exhibit G: Kathy Griest letter dated July 13, 2011

**David Hung**

---

**From:** Kathy Griest [kgriest2002@yahoo.com]  
**Sent:** Wednesday, July 13, 2011 3:54 PM  
**To:** David Hung  
**Cc:** lstetson2@earthlink.net  
**Subject:** Planning Commission Hearing Thurs July 14th

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

I am new to the neighborhood (Vallejo Way - Feb 2011) and have already had furniture stolen from my front porch due to heavy and unsafe traffic up and down the street.

I will not be able to attend the hearing this thursday but would like to request the planning commission to: 1) Protect Vallejo Way, Muir Way and smaller residential streets from increased and unsafe traffic; 2) Add traffic calming measures on 5th St to prevent 5th St from directing more traffic onto Vallejo Way and other residential streets; 3) Request the City to redefine Vallejo Way from a "minor collector" to a "neighborhood" street with a lower traffic threshold; and 4) Require the City to conduct a traffic survey on Vallejo and Muir Ways after Phase 2 and subsequent phases of the total project.

Thank you,  
Kathy Griest  
956 Vallejo Way