

Meeting Date: 5/24/2016

Report Type: Consent

Report ID: 2016-00565

Title: (Pass for Publication) Meadowview Road Les Schwab Tire Center

Location: District 8

Recommendation: 1) Review a) a Resolution to an addendum to the adopted Mitigated Negative Declaration (MND) for Freeport Marketplace; b) an Ordinance rezoning approximately 1.5 acres from the Shopping Center (SC) Zone to the General Commercial (C-2) Zone; and c) a Resolution adopting findings of fact and approving a Conditional Use Permit (CUP) to allow an auto service and repair use within the General Commercial (C-2) Zone and Site Plan and Design Review with deviations for the construction of an 11,500 square foot tire center; and 2) pass for publication the Ordinance title as required by Sacramento City Charter 32c to be considered on May 31, 2016.

Contact: Garrett Norman, Assistant Planner, (916) 808-7934; Antonio Ablog, Senior Planner, (916) 808-7702, Community Development Department

Presenter: None

Department: Community Development Dept

Division: Current Planning

Dept ID: 21001221

Attachments:

01-Description/Analysis

02-Background

03-Resolution Environmental

04-Exhibit A Mitigation Monitoring Program

05-Ordinance for Rezone

06-Exhibit A Rezone Map

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08-Exhibit A (Site Plan)

09-Exhibit B (Landscape Plan)

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11-Exhibit D (Floor Plan)

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14-Neighborhood Resolution of Support and Good Neighbor Policy Agreement

15-WalkSacramento Comment Letter

City Attorney Review

Approved as to Form
Jeffrey Heeren
5/16/2016 10:41:01 AM

Approvals/Acknowledgements

Department Director or Designee: Ryan Devore - 5/11/2016 12:45:00 PM

Description/Analysis

Issue Detail: The applicant is proposing to construct an 11,500± square foot Les Schwab Tire Center on a vacant 1.5± acre parcel, located at 1430 Meadowview Road. The Les Schwab Tire Center will perform routine maintenance services limited to tire replacement and repair, brakes, alignment, and shocks. The site is zoned Shopping Center (SC). In order to establish the tire center, the proposed project requires a CUP and rezone from Shopping Center (SC) to General Commercial (C-2).

Policy Considerations: The 2035 General Plan Update was adopted by City Council on March 3, 2015. The 2035 General Plan's goals, policies, and implementation programs define a roadmap to achieving Sacramento's vision to be the most livable city in America. The General Plan designation for the subject site is Suburban Corridor. Sacramento's suburban corridors are envisioned as auto-oriented, moderate-density retail, office, and residential corridors that support surrounding suburban neighborhoods.

The 2035 General Plan has identified goals and policies under the Land Use and Urban Design Element that relate to this project, including the following:

- **Policy LU 1.1.5: Infill Development.** The City shall promote and provide incentives (e.g., focused infill planning, zoning/rezoning, revised regulations, provision of infrastructure) for infill development, reuse, and growth in existing urbanized areas to enhance community character, optimize City investments in infrastructure and community facilities, support increased transit use, promote pedestrian- and bicycle-friendly neighborhoods, increase housing diversity, ensure integrity of historic districts, and enhance retail viability.
- **Policy LU 2.1.2: Protect Established Neighborhoods.** The City shall preserve, protect, and enhance established neighborhoods by providing sensitive transitions between these neighborhoods and adjoining areas, and requiring new development, both private and public, to respect and respond to those existing physical characteristics, buildings, streetscapes, open spaces, and urban form that contribute to the overall character and livability of the neighborhood.
- **Policy LU 2.1.7: Good Neighbors.** The City shall encourage businesses located within and adjacent to residential developments to conduct their business in a courteous manner by limiting disturbances and nuisances from operations and patrons, and to act as members of the community by making themselves available to respond to complaints and by participating in neighborhood/community meetings
- **Policy LU 2.1.8: Neighborhood Enhancement.** The City shall promote infill development, reuse, rehabilitation, and reuse efforts that contribute positively (e.g., architectural design) to existing neighborhoods and surrounding areas.

The Les Schwab Tire Center is consistent with the goals and policies of the 2035 General Plan because it offers a supportive commercial use to the area while developing an underutilized vacant parcel within the urbanized City limits. Furthermore, the project is sensitive to the established neighborhoods by respecting the existing physical characteristics of the area by maintaining a consistent setback line on the Meadowview Road street frontage, preserving existing heritage trees on the site, and constructing new fencing on the eastern and southern property lines. Furthermore, a good neighbor policy was agreed upon with the Meadowview Neighborhood Association.

South Area Community Plan

The project is part of the South Area Community Plan which outlines specific opportunities and constraints at the neighborhood level. The following policies from the South Area Community Plan positively address the project:

- **Policy SA.LU 1.9: Meadowview/Mack Road Revitalization.** The City shall provide for revitalization of the Meadowview and Mack Road corridors through streetscape improvements (e.g., design and construction of a median, gateways, signage, monuments, and masonry walls to replace blighted fences) and reuse of vacant retail shells.
- **Policy SA.LU 1.13: Street/Shade Trees.** The City shall encourage the planting of street/shade trees in areas lacking a continuous tree canopy, particularly in the Meadowview and Valley Hi neighborhoods.

Economic Impacts: None.

Environmental Considerations: The Community Development Department, Environmental Planning Services has reviewed the project for compliance with the requirements of the California Environmental Quality Act (CEQA). The Les Schwab Tire Center (P16-009) project is determined to fall within the scope of the Mitigated Negative Declaration for Freeport Marketplace (P03-018) adopted November 20, 2007 (Resolution No. 2007-842). The adopted Mitigated Negative Declaration adequately described the effects of the proposed development of the fast food restaurant with drive-through and retail uses at the subject location. In compliance with Section 15070(b)(1) of the California Environmental Quality Act (CEQA) Guidelines the City has incorporated mitigation measures to avoid identified impacts or to mitigate such impacts to a point where clearly no significant impacts will occur. The analyses and mitigation measures in the previously-adopted Mitigated Negative Declaration are affirmed and attached as Exhibit A of the CEQA findings.

Transportation mitigation measures T-1 and T-2 and Biological Resources mitigation measure BR-3 have been satisfied as part of the previously occurring initial development of the larger Freeport Marketplace project site. Additionally, Biological Resources mitigation measures BR-4, BR-5, and BR-6 are not applicable to the Les Schwab Tire Center (P16-009) project as they were established for a location of the original larger project which has been developed.

An Addendum to the Mitigated Negative Declaration has been prepared describing the proposed development of the Les Schwab Tire Center (P16-009) project and evaluating the potential environmental effects of the proposed project within Freeport Marketplace. The Addendum defines the project description and justification for use of an Addendum pursuant to the California Environmental Quality Act (CEQA) Guidelines (Section 15164). Therefore, it is recommended that the City Planning and Design Commission consider the adopted Mitigated Negative Declaration as amended by an Addendum for the Les Schwab Tire Center (P16-009) project. The addendum and the adopted Mitigated Negative Declaration are available at the Community Development Department Environmental Impact Report webpage at the following url: <http://www.cityofsacramento.org/Community-Development/Planning/Environmental/Impact-Reports>

Sustainability: The applicant is proposing the equivalent to LEED Silver; however, they are not seeking certification.

Commission/Committee Action: On April 21, 2016, the Planning and Design Commission held a public hearing on the Les Schwab Tire Center project and unanimously passed a motion to forward a recommendation of approval to the City Council.

Rationale for Recommendation: Staff recommends the City Council approve the requests based on findings of fact and subject to conditions listed in Attachment 7. Staff supports the project because it: a) activates an underutilized parcel in the City limits; b) provides a retail support service to the area; c) has been conditioned to minimize the tire center's impact on the adjacent residential uses; and d) is consistent with the General Plan Designation of Suburban Corridor, the General Commercial (C-2) zone, and the South Area Community Plan.

Financial Considerations: None.

Local Business Enterprise (LBE): Not applicable.

Background

The subject site is surrounded by a mix of land uses. To the west is a commercial retail building and a veterinarian clinic that are interconnected with Les Schwab site through shared driveway access and internal pedestrian and vehicle connections. To the south and east is a multi-unit apartment complex. Directly north is a larger shopping center that offers a variety of commercial retail services.

Table 1: Project Information

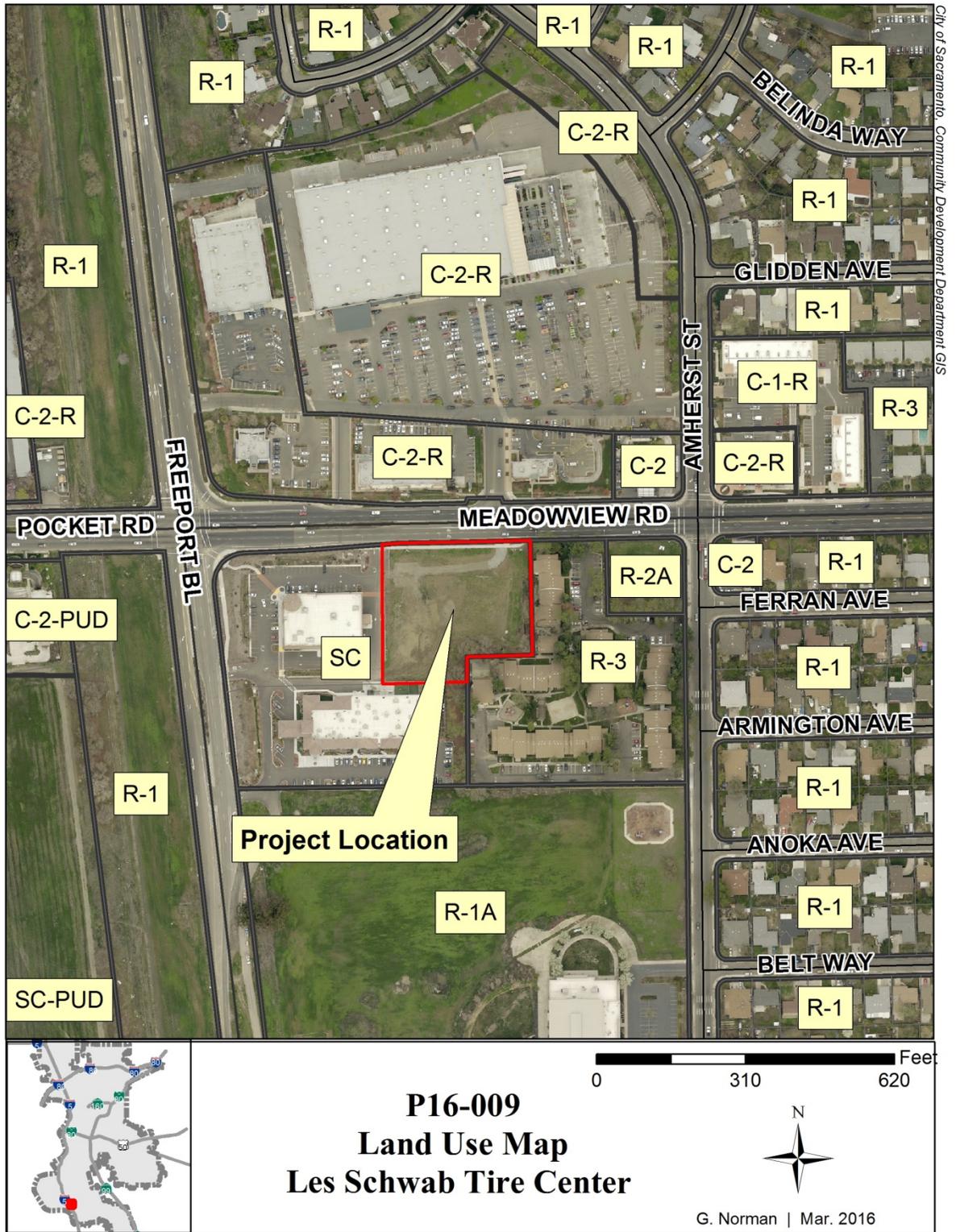
General Plan designation:	Suburban Corridor
PUD designation (if applicable):	None
Existing zoning of site:	Shopping Center (SC)
Proposed zoning of site:	General Commercial (C-2)
Existing use of site:	Vacant
Property area:	1.5± acres
Proposed Floor Area Ratio (FAR):	.15

On November 20, 2007 the City Council adopted a series of resolutions and an ordinance for the project known as Freeport Marketplace (P03-018) including: a Resolution adopting a Mitigated Negative Declaration and Mitigation Monitoring Program; a Resolution amending the General Plan Land Use Map from Low Density Residential to Community/Neighborhood Commercial & Office; a Resolution amending the Airport Meadowview Community Plan Land Use Map from Residential to Commercial; a Resolution approving a Tentative Map, Special Permits, Variances, and Plan Review; and an Ordinance rezoning the site from single-family alternative (R-1A) to Shopping Center (SC).

The aforementioned entitlements were for the construction of a 52,000 square foot shopping center known as Freeport Marketplace. The 6.1 acre site was slated for the development of four commercial buildings; a Walgreens, a veterinary clinic, a fast food restaurant, and a retail building. The Walgreens and veterinary clinic have been constructed. The Les Schwab project site was previously approved for a fast food restaurant and a subsequent retail grocery building. Neither the drive-through, nor grocery store were ever constructed.

On March 23, 2016, a Site Plan and Design Review application (DR16-004) was approved to construct a 26 stall parking lot for the veterinarian clinic. A lot line adjustment (COC15-0048) was completed to combine the 0.31 acre parking lot with the parcel containing the veterinarian clinic. The existing parcel configuration of the Les Schwab Tire Center site is the result of this lot line adjustment.

Figure 1: Land Use Map



Public/Neighborhood Outreach and Comments

As part of the application review process, the proposal was routed to the Meadowview Neighborhood Association, Walk Sacramento, and Sacramento Area Bicycle Advocates. Staff also mailed notices to all property owners within 300 feet of the project site and posted the site prior to the public hearing.

The applicant reached out to the Meadowview Neighborhood Association on November 18, 2015. The meeting resulted in a unanimous and formal adoption of a Resolution of Support for the Les Schwab Tire Center Planning Application along with a Good Neighbor Policy agreement. A copy of the Resolution of Support and the Good Neighbor Policy is provided in Attachment 14.

Walk Sacramento submitted a comment letter to planning staff (Attachment 15) with concerns related to a direct pedestrian connection from the entrance of the building to the sidewalk on Meadowview Road. Staff found this concern to be valid and requested the applicant to revise the plans to include the sidewalk. The applicant agreed to this request and revised their plans accordingly.

As of writing this report, staff has not received any additional comments from the public or community organization recipients.

Rezone

The current zoning of the site is Shopping Center (SC). The SC zone prohibits the use of a tire store, which is defined as auto service and repair in the Planning and Development Code. Rezoning the parcel to General Commercial (C-2) permit a tire store, subject to approval of a Conditional Use Permit (CUP).

The C-2 zone is a compatible zoning designation for the area. There is C-2 zoning for an existing 18.4± acre shopping center directly north of the subject site. Additionally, the C-2 zone is well-matched with the SC zone in terms of permitted uses. For example, the permitted uses allowed by right in the C-2 zone are all permitted by right in the SC zone. There are only a few additional permitted uses in the C-2 zone that are not permitted in the SC zone, including housing, lodging, and wholesale and manufacturing uses less than 6,400 square feet. Those manufacturing and wholesale uses allowed in the C-2 zone are uses that are not expected to produce nuisances.

Conditional Use Permit

Approval of a Conditional Use Permit (CUP) is required to construct an auto service and repair use within the General Commercial (C-2) zoning district. A tire store is inclusive of an auto service and repair use, as defined in Planning and Development Code section 17.108.020. This Les Schwab Tire Center will perform routine maintenance services limited to tire replacement and repair, brakes, alignment, and shocks. The Les Schwab Tire Center proposed hours of operation are Monday through Friday from 8:00 a.m. to

6:00 p.m. and Saturday from 8:00 a.m. to 5:00 p.m. The business will be closed on Sundays. The store is anticipated to have truck deliveries once per week to stock the warehouse and will have weekly pickups of recycled tires.

The CUP process is designed to evaluate a project's potential impact on a site and the surrounding area. In evaluation of the proposed Les Schwab Tire Center, staff considered the potential generated impacts related to noise, outdoor storage and service, and traffic. The noise and traffic concerns were analyzed in the addendum to the Mitigated Negative Declaration. It was determined that all noise produced by the tire center is mitigated through building design and site layout. Specifically, the service bays will not be constructed south facing where residential use is. Instead they are constructed to face Meadowview, directing all noise onto Meadowview Road. Furthermore, a six foot solid wall and fence will bound the southern and eastern property line. Traffic was analyzed at the time the original proposal of the fast food restaurant and retail building and staff determined that a tire center does not generate more traffic than both uses combined. Placing project specific conditions helps ensure a clean and safe operation of the use. Below is a list of conditions for the project that address the above mentioned impacts:

- Approval of the Conditional Use Permit is for auto service and repair uses only (tire replacement and repair, brakes, alignment, and shocks). The addition of auto service uses beyond those requested shall be reviewed by Planning Staff and may be subject to additional entitlements.
- All auto related service and repair work shall be performed completely inside of the building. No outdoor work is permitted.
- All material(s) used for the auto service and repair business shall be stored entirely inside. No outdoor storage of auto service and repair material(s) is permitted.
- The roll up door located on the rear of the building (south elevation) shall remain closed during business hours, except for deliveries.
- The business shall be subject to the following hours of operation: Monday-Saturday from 8:00 a.m. until 6:00 p.m. and Sundays from 8:00 a.m. until 5:00 p.m.
- No service bays shall be constructed on the south elevation or rear of the building.
- The outside premises shall be kept clean of all trash and/or debris and landscaped areas shall be properly maintained, free of weeds. Disposal of tires or materials shall not be in public view.

- Prior to issuance of a building permit, the business owner(s) shall propose and submit for review and approval by the Planning Director a “Good Neighbor Policy” including, but not limited to, the following:
 - Establish a process for neighbors to communicate directly with the management staff should there be any problems or issues;
 - Provide a 24-hour emergency contact person and phone number on-site, in a clearly identified place.

The conditions of the requested CUP for this site would apply to any new subsequent business categorized under the auto service and repair definition, as long as the previous business did not cease or discontinue their operation for more than two years; therefore, this CUP could be transferred to a different auto service and repair business, however, the same conditions of approval listed in Attachment 7 would still apply. Any additional auto service uses beyond tire replacement and repair, brakes, alignment, and shocks shall require additional review and may require modification(s) to the terms of the requested use.

Per Section of 17.808.200 of the Planning and Development Code, a request for a CUP may be granted based on the following findings:

1. The proposed use and its operating characteristics are consistent with the general plan and any applicable specific plan or transit village plan.

The proposed project is consistent with General Plan policy related to infill development and enhancing and protecting established neighborhoods. The project is also supportive to the South Area Community Plan related to streetscape and neighborhood enhancement. Additionally, the project has been conditioned to ensure a good neighbor policy by establishing a mechanism for the community to contact the business should any necessary issues arise.

2. The proposed use and its operating characteristics are consistent with the applicable standards, requirements, and regulations of the zoning district in which it is located, and of all other provisions of this title and this code.

The project as proposed complies with the relative development standards of the C-2 zoning district. Staff finds that the setback and wall deviations are appropriate given the residential nature of the adjacent multi-unit apartment complex and staff’s desire to protect existing heritage trees to the south of the project site. Furthermore, the project is conditioned to comply with the relative development standards related to landscaping, waste management, and bicycle facilities.

3. The proposed use is situated on a parcel that is physically suitable in terms of location, size, topography, and access, and that is adequately served by public services and utilities.

The project will have access from Meadowview Road, will manage its stormwater through appropriate grading and paving, and will be served by public and private utilities needed to ensure proper operation of the use.

4. The proposed use and its operating characteristics are not detrimental to the public health, safety, convenience, or welfare of persons residing, working, visiting, or recreating in the surrounding neighborhood and will not result in the creation of a nuisance

The building design and site improvements will attenuate noise to neighboring properties through appropriate site design measures such as fencing, building placement, and the utilization of masonry construction materials for the building. The project has been conditioned to ensure a long-lasting operation that will function harmoniously with the existing shopping center and surrounding uses.

Site Plan and Design Review

Site Plan and Design Review is required for the project to confirm compliance with applicable development standards and design guidelines. As illustrated in Table 2 below, the project complies with the development standards of the C-2 zoning district, with exception to the maximum front yard setback of 25 feet. The deviation for the front yard setback is analyzed below.

Table 2: C-2 Zone Development Standards

Standard Type	Code Requirement	Proposed	Deviation Required?
Setbacks Front	No Min. 25' Max.	66.7'	Yes
Rear	No Min.	20'	No
Interior Sideyard (east)	No Min.	69.5'	No
Interior Sideyard (west)	No Min.	68'	No
Maximum Height	45'	25'	No
Lot Coverage	No Max.	18%	No
Vehicle Parking	1/500 s.f. = 20	34	No

The site will be directly accessed from Meadowview Road by two curb cuts that are right turn in and out only. The western curb cut is an existing shared driveway that serves the

Walgreens and veterinary clinic and will also serve Les Schwab. Pedestrian and bicycle access will be from the Meadowview Road right-of-way with a designated pathway that will meet ADA accessibility requirements. The project site design balances the location of the building on site with landscaping and parking needs while complementing adjacent commercial and multi-unit uses along the street.

1. Street Wall/Setback: The proposed project generally responds to the street wall of adjacent commercial with similar setbacks and landscaping buffer to the parking.
2. Parking: Parking lot spaces along Meadowview are kept to one row of spaces, with a generous landscape buffer in keeping with development along this street.
3. Active areas: Areas of most activity are placed closest to adjacent commercial with less active areas adjacent to multi-unit residences.

Front Yard Setback Deviation

The applicant is requesting a front yard setback deviation from the required maximum 25 feet in the C-2 zone. The applicant is proposing an approximate 67 foot front yard setback. Staff finds a deviation to this requirement is necessary for this specific land use in order to efficiently move traffic in and out of the service bays, while reducing conflict areas from vehicle ingress and egress onto Meadowview Road. Additionally, the strict adherence to the maximum setback requirement could encourage a site design where the service bays would be facing the rear of the property. This type of design is highly discouraged to help noise attenuation to the neighboring multi-unit dwellings south and east of the site. Furthermore, the proposed setback is consistent with the existing Walgreens setback on Meadowview Road, located directly west of the site, creating a uniform streetscape.

Fencing

There are four existing mature Valley Oak trees located along the southern property line. These trees have been evaluated by the City Arborist and have been determined to meet the criteria for heritage trees. In an effort to preserve the health and longevity of the trees a solid wall is highly discouraged. Furthermore, the previously approved Plan Review for the entire shopping center, part of the aforementioned entitlement package (P03-018), included a variance to the requirement of a six foot solid wall in this area, based on the former rationale. Ultimately, the applicant has proposed a design solution that includes a five foot retaining wall slightly north of the heritage trees to not infringe on the root system, as the existing topography slopes to the south. A six foot composite fence will be placed on top of the retaining wall. This fence line will run eastward to the proposed trash enclosure on the southeast corner of the parcel. The entire eastern property line will have a six foot solid masonry wall.

Architectural Analysis

The building design creates a clean, simple aesthetic that complements surrounding structures.

1. Design and materials: The proposed design is well articulated for the tire center and provides variation with areas of interest along the facade. Special attention is paid to street facing elevation, with corner showroom windows in active areas.

The service bays provide a fenestration break to the façade, while the paint scheme breaks up the overall massing with strategic placement of colors. Smooth faced CMU and metal panels provide a mix of materials that create interest and textural changes on the façade. Mechanical equipment is screened with integral metal panel detailing.

2. Building heights and context: The single story façade with high bays for auto servicing provides a street wall complementary to adjacent commercial structures and is in context with neighborhood development.

Per Section of 17.808.180 of the Planning and Development Code, approval of the Site Plan and Design Review shall be based on the following findings:

1. The design, layout, and physical characteristics of the proposed development are consistent with the general plan and any applicable specific plan or transit village plan

The design, layout, and physical characteristics of the proposed development are consistent with the General Plan Suburban Corridor designation and are consistent with the General Plan goals and policies related to infill development, enhancing and protecting established neighborhoods, while ensuring good neighbor policy. The project has been conditioned to ensure a good neighbor policy by establishing a mechanism for the community to contact the business should any necessary issues arise.

2. The design, layout, and physical characteristics of proposed development are consistent with all applicable design guidelines and with all applicable development standards or, if deviations from design guidelines or development standards are approved, the proposed development is consistent with the purpose and intent of the applicable design guidelines and development standards.

The proposed building is centrally located on the site and adequately addresses neighboring uses through compatible design and site improvements. Where deviations are requested, staff finds that the setback and wall deviations are appropriate given the residential nature of the adjacent multi-unit apartment complex and staff's desire to protect existing heritage trees to the south of the project site.

3. All streets and other public access ways and facilities, parking facilities, and utility infrastructure are adequate to serve the proposed development and comply with all applicable design guidelines and development standards.

The proposed project provides onsite and offsite improvements to sufficiently serve the project and its users through appropriate stormwater management, extension of necessary public and private utility infrastructure, and parking and landscaping improvements.

4. The design, layout, and physical characteristics of the proposed development are visually and functionally compatible with the surrounding neighborhood.

The surrounding neighborhood is a mix of commercial and residential uses. The facade of the building utilizes a similar material and color pallet from nearby buildings ensuring a compatible architectural design. The onsite improvements will enhance the existing streetscape creating uniformity with development to the west and north.

5. The design, layout, and physical characteristics of the proposed development ensure energy consumption is minimized and use of renewable energy sources is encouraged.

By preserving the existing heritage trees and planting the necessary trees to meet the 50 percent shade requirement helps shade the building and surfaces with a low albedo, ultimately reducing energy consumption needed to condition or cool the interior of the building.

6. The design, layout, and physical characteristics of the proposed development are not detrimental to the public health, safety, convenience, or welfare of persons residing, working, visiting, or recreating in the surrounding neighborhood and will not result in the creation of a nuisance.

The building design and site improvements will attenuate noise to neighboring properties through appropriate site design measures such as fencing, building placement, and the utilization of masonry construction materials for the building. The project has been conditioned to ensure a long-lasting operation that will function harmoniously with the existing shopping center and surrounding uses.

RESOLUTION NO. 2016 –

Adopted by the Sacramento City Council

ADOPTING THE ADDENDUM TO THE MITIGATED NEGATIVE DECLARATION AND THE MITIGATION MONITORING PROGRAM FOR THE LES SCWHAB TIRE CENTER PROJECT (P16-009)

BACKGROUND

- A. On April 21, 2016, the City Planning and Design Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve with conditions the Les Schwab Tire Center (P16-009) project.
- B. On May 31, 2016, the City Council conducted a public hearing, for which notice was given pursuant to Sacramento City Code Section 17.812.010 (2)(b) and received and considered evidence concerning the Les Schwab Tire Center (P16-009) project.

BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:

Section 1. The City Council finds as follows:

- A. On November 20, 2007, pursuant to the California Environmental Quality Act (Public Resources Code §21000 *et seq.* ("CEQA")), the CEQA Guidelines (14 California Code of Regulations §15000 *et seq.*), and the City of Sacramento environmental guidelines, the City Council adopted a mitigated negative declaration (MND) and a mitigation monitoring program and approved Freeport Marketplace (P03-018)(Project).
- B. The Les Schwab Modification (P16-009) (Project Modification) proposes to modify the previously approved Project as follows: Rezoning 1.5 acres of remaining vacant land from Shopping Center (SC) to General Commercial (C-2); obtaining a conditional use permit and site plan and design review with deviations for setbacks for the development of an approximate 11,500 square feet tire center in place of the approved fast food restaurant with drive-through (~3,177 square feet) and retail space (~5,952 square feet).
- C. the proposed changes to the original Project do not require the preparation of a subsequent mitigated negative declaration. An addendum to the previously adopted MND addresses the modification to the Project.

Section 2. The City Council has reviewed and considered the information contained in the previously adopted MND for the Project, the addendum, and all oral and documentary evidence received during the hearing on the Project Modification. The City Council had determined that the previously adopted MND and the addendum constitute an adequate, accurate, objective, and complete review of the proposed

Project Modification and finds that no additional environmental review is required based on the reasons set forth below:

A. No substantial changes are proposed by the Project Modification that will require major revisions of the previously adopted MND due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;

B. No substantial changes have occurred with respect to the circumstances under which the Project Modification will be undertaken which will require major revisions to the previously adopted MND due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;

C. No new information of substantial importance has been found that shows any of the following:

1. The Project Modification will have one or more significant effects not discussed in the previously adopted MND;

2. Significant effects previously examined will be substantially more severe than shown in the previously adopted MND;

3. Mitigation measures previously found to be infeasible would in fact be feasible and would substantially reduce one or more significant effects of the Project Modification; or

4. Mitigation measures which are considerably different from those analyzed in the previously adopted MND would substantially reduce one or more significant effects on the environment.

Section 3. Based on its review of the previously adopted MND for the Project, the addendum, and all oral and documentary evidence received during the hearing on the Project Modification, the City Council finds that the MND and addendum reflect the City Council's independent judgment and analysis and adopts the MND and the addendum for the Project Modification and readopts the findings of fact in support of the MND.

Section 4. The mitigation monitoring program (MMP) for the Project is adopted for the Project Modification, and the mitigation measures shall be implemented and monitored as set forth in the program, based on the following findings of fact:

1. The mitigation monitoring program has been adopted and implemented as part of the Project;

2. The addendum to the MND does not include any new mitigation measures, and has not eliminated or modified any of the mitigation measures included in the mitigation monitoring program;

3. The mitigation monitoring plan meets the requirements of CEQA section 21081.6 and CEQA Guidelines section 15074.

Section 5. Upon approval of the Project, the City Manager shall file or cause to be filed a Notice of Determination with the Sacramento County Clerk and, if the project requires a discretionary approval from any state agency, with the State Office of Planning and Research, pursuant to section 21152(a) of the Public Resources Code and the State EIR Guidelines adopted pursuant thereto.

Section 6. Pursuant to Guidelines section 15091(e), the documents and other materials that constitute the record of proceedings upon which the City Council has based its decision are located in and may be obtained from, the Office of the City Clerk at 915 I Street, Sacramento, California. The City Clerk is the custodian of records for all matters before the City Council.

Exhibits

Exhibit 3b: Mitigation Monitoring Program

Exhibit 3b: Mitigation Monitoring Program

			VERIFICATION OF COMPLIANCE		
Mitigation Measure	Implementing Responsibility	Monitoring Responsibility	Compliance Standards	Timing	Verification of Compliance
6. TRANSPORTATION AND CIRCULATION					
T-1 Provide and construct a dedicated right-turn lane on the northbound approach at the intersection of Meadowview Road at Freeport Boulevard to the satisfaction of the City of Sacramento Development Services Department, Development Engineering Division. The project applicant shall revise the site plan and include the dedicated right-turn lane in the improvement plans.	Applicant / Developer	City Development Services Department	Include the dedicated right-turn lane in the improvement plans.	At the time of submitting the improvement plans	SATISFIED
T-2 Modify the timing of the traffic signal at the southbound I-5 exit ramp to Pocket Road to allocate more green time to the southbound approach. The applicant/developer for the proposed project shall pay a fair share to recover the costs for the City's Traffic Operation Center monitoring and future retiming of this signal.	Applicant / Developer	City Development Services Department and City DOT	Project Applicant pay fair share to City DOT	Prior to issuance of building permit	SATISFIED
7. BIOLOGICAL RESOURCES					
BR-1. Prior to issuance of grading permits, the project applicant/developer shall have a biologist conduct a pre-construction survey to determine whether the stick nest identified in the 19-inch valley oak (Tree #: 6421 in the Abacus report) is being used. If so, no removal of the nest tree or disturbance of the active nest should occur during the nesting season for the species using the nest (generally March through July).	Applicant / Developer	City of Sacramento – Development Services Department (DSD); DFG	Pre-construction biological surveys shall be completed as specified and submitted with grading/ building plans.	Prior to issuance of any grading or building permit, measures identified on plans shall be verified for compliance. The Development Services Department shall assure that measures are identified on	
BR-2a Prior to issuance of a grading permit, a pre-construction survey shall be completed by a qualified biologist, within 30 days prior to construction, to determine whether any	Applicant / Developer	City of Sacramento – DSD; DFG	Mitigation Measures, shall be included on the Construction Specifications. Pre-construction		

			VERIFICATION OF COMPLIANCE		
Mitigation Measure	Implementing Responsibility	Monitoring Responsibility	Compliance Standards	Timing	Verification of Compliance
<p>Swainson's hawk nest trees will be removed on-site, or active Swainson's hawk nest sites occur within ½ mile of the development site. These surveys shall be conducted according to the Swainson's Hawk Technical Advisory Committee's (May 31, 2000) methodology or updated methodologies, as approved by the U.S. Fish and Wildlife Service (USFWS) and California Department of Fish and Game (CDFG), using experienced Swainson's hawk surveyors.</p> <p>2b. If breeding Swainson's hawks (i.e. exhibiting nest building or nesting behavior) are identified, no new disturbances (e.g. heavy equipment operation associated with construction) shall occur within ½ mile of an active nest between March 1 and September 15, or until a qualified biologist, with concurrence by CDFG, has determined that young have fledged or that the nest is no longer occupied. If the active nest site is located within ¼ mile of existing urban development, the no new disturbance zone can be limited to the ¼ mile versus the ½ mile.</p> <p>2c. If construction or other project related activities which may cause nest abandonment or forced fledgling are proposed within the ¼ mile buffer zone, intensive monitoring (funded by the project sponsor) by a Department of Fish and Game approved raptor biologist will be required. Exact implementation of this measure will be based on specific site conditions.</p> <p>2d. Trees on the site that need to be removed to accommodate construction shall be felled between September 15 and January 31, outside of the general nesting season for raptors and other birds.</p>			<p>biological surveys shall be completed as specified and submitted with grading/ building plans.</p>	<p>construction plans and specifications and confirm compliance prior to issuance of any grading or building permit. Measures shall also be implemented concurrent with construction activities.</p> <p>Prior to issuance of any grading, and/or construction permit, measures identified on plans shall be verified for compliance. The Development Services Dept. shall assure that measures are identified on construction plans and confirm compliance prior to issuance of any grading permit.</p>	

			VERIFICATION OF COMPLIANCE		
Mitigation Measure	Implementing Responsibility	Monitoring Responsibility	Compliance Standards	Timing	Verification of Compliance
<p>Alternately, a pre-construction survey for nesting birds shall be conducted prior to tree removal between February 1 and September 15. Temporal restrictions shall be determined by a qualified biologist.</p> <p>BR-3. Prior to issuance of grading permits, the project applicant shall be required to purchase compensatory Swainson's hawk foraging habitat credits for each developed acre, at the required ratio, from an approved mitigation bank, or develop other arrangements acceptable to and approved by the CDFG.</p>	Applicant / Developer	City of Sacramento – DSD;	Applicant / Developer shall provide written verification prior to issuance of Grading Permit.	Measures shall be implemented prior to and concurrent with construction activities.	SATISFIED
BR-4. If Tree #'s 6415 and 6417 are removed, prior to issuance of Certificate of Occupancy removal of Tree #6411, a 14-inch diameter-at-breast height (DBH) valley oak, shall require the planting of six (6) 24-inch box trees on the subject site.	Applicant / Developer	City of Sacramento – DSD and Urban Forest Services Division	Verification of compliance from the Urban Forest Services Arborist shall be provided to Development Services Staff prior to issuance of building permits.	Measure shall be implemented prior to issuance of any building permits or as agreed upon by the Urban Forest Services Arborist	NOT APPLICABLE
BR-5. If Tree #'s 6415 and 6417 are removed, prior to issuance of Certificate of Occupancy, removal of Tree #6415, a 14-inch DBH valley oak shall require the planting of six (6) 24-inch box trees on the subject site.	Applicant / Developer	City of Sacramento – DSD and Urban Forest Services Division	Mitigation Measures, shall be included on the Construction Specifications and Plan		NOT APPLICABLE
BR-6. If Tree #'s 6415 and 6417 are removed, prior to issuance of Certificate of Occupancy, removal of Tree #6417, a 3 stemmed (18, 15, 9-inch DBH) valley oak, shall require the planting ten (10) 24-inch box trees on the subject site.	Applicant / Developer	City of Sacramento – DSD and Urban Forest Services Division			NOT APPLICABLE
14. CULTURAL RESOURCES:					
CR-1 In the event that any prehistoric subsurface archeological features or deposits, including	Applicant / Developer	City Development	Mitigation	Measures shall	

		VERIFICATION OF COMPLIANCE			
Mitigation Measure	Implementing Responsibility	Monitoring Responsibility	Compliance Standards	Timing	Verification of Compliance
<p>locally darkened soil (“midden”), that could conceal cultural deposits, animal bone, obsidian and/or mortars are discovered during construction-related earth-moving activities, all work within 50 meters of the resources shall be halted, and the City shall consult with a qualified archeologist to assess the significance of the find. Archeological test excavations shall be conducted by a qualified archeologist to aid in determining the nature and integrity of the find. If the find is determined to be significant by the qualified archeologist, representatives of the City and the qualified archeologist shall coordinate to determine the appropriate course of action. All significant cultural materials recovered shall be subject to scientific analysis and professional museum curation. In addition, a report shall be prepared by the qualified archeologist according to current professional standards.</p>	Applicant / Developer	Services Department	<p>Measures shall be included on the Map and within the Standard Construction Specifications.</p> <p>Verification of compliance shall be provided to the Development Services Staff</p>	be implemented in field during grading and construction activities.	
<p>CR-2 If a Native American site is discovered, the evaluation process shall include consultation with the appropriate Native American representatives.</p> <p>If Native American archeological, ethnographic, or spiritual resources are involved, all identification and treatment shall be conducted by qualified archeologists, who are certified by the Society of Professional Archeologists (SOPA) and/or meet the federal standards as stated in the Code of Federal Regulations (36 CFR 61), and Native American representatives, who are approved by the local Native American community as scholars of the cultural traditions.</p> <p>In the event that no such Native American is available, persons who represent tribal</p>		City Development Services Department	<p>Mitigation Measures shall be included on the Map and within the Standard Construction Specifications.</p> <p>Verification of compliance shall be provided to the Development Services Staff</p>	Measures shall be implemented in field during grading and construction activities.	

		VERIFICATION OF COMPLIANCE			
Mitigation Measure	Implementing Responsibility	Monitoring Responsibility	Compliance Standards	Timing	Verification of Compliance
<p>governments and/or organizations in the locale in which resources could be affected shall be consulted. If historic archeological sites are involved, all identified treatment is to be carried out by qualified historical archeologists, who shall meet either Register of Professional Archeologists (RPA), or 36 CFR 61 requirements.</p> <p>CR-3 If a human bone or bone of unknown origin is found during construction, all work shall stop in the vicinity of the find, and the County Coroner shall be contacted immediately. If the remains are determined to be Native American, the coroner shall notify the Native American Heritage Commission, who shall notify the person most likely believed to be a descendant. The most likely descendant shall work with the contractor to develop a program for re-internment of the human remains and any associated artifacts. No additional work is to take place within the immediate vicinity of the find until the identified appropriate actions have taken place.</p>	Applicant / Developer	City Development Services Department	Mitigation Measures shall be included on the Map and within the Standard Construction Specifications. If required, verification of compliance shall be provided to the Development Services Staff	Measures shall be implemented in field during grading and construction activities.	

ORDINANCE NO. 2016 –

Adopted by the Sacramento City Council

AMENDING TITLE 17 OF THE SACRAMENTO CITY CODE BY REZONING 1.5± ACRES LOCATED AT 1430 MEADOWVIEW ROAD FROM THE SHOPPING CENTER (SC) ZONE TO THE GENERAL COMMERCIAL (C-2) ZONE (P16-009) (APN: 052-0290-008-0000)

BE IT ENACTED BY THE COUNCIL OF THE CITY OF SACRAMENTO:

SECTION 1

As used in this ordinance, “**Property**” means the real property depicted in attached and incorporated Exhibit A and located on Meadowview Road between Freeport Boulevard and Amherst Street (APN: 052-0290-008-0000), consisting of approximately 1.5 acres, and legally described as:

BEGINNING AT THE NORTH EAST CORNER OF SAID PARCEL 1, SAID **POINT OF BEGINNING** BEING ON THE SOUTHERLY RIGHT OF WAY LINE FOR MEADOWVIEW ROAD, AS SHOWN ON SAID CERTIFICATE OF COMPLIANCE; THENCE FROM SAID POINT OF BEGINNING ALONG SAID SOUTHERLY RIGHT OF WAY LINE SOUTH 89°11'51" WEST 274.95 FEET TO THE BEGINNING OF A TANGENT CURVE TO THE LEFT HAVING A RADIUS OF 982.50 FEET; THENCE ALONG SAID TANGENT CURVE THROUGH A CENTRAL ANGLE OF 2°00'06" AN ARC LENGTH OF 34.32 FEET TO THE NORTH WEST CORNER OF SAID PARCEL 1; THENCE ALONG THE WEST LINE OF SAID PARCEL 1 SOUTH 0°39'05" EAST 193.85 FEET; THENCE LEAVING SAID WEST LINE NORTH 90°00'00" EAST 127.53 FEET; THENCE SOUTH 59°03'40" EAST 54.89 FEET TO AN ANGLE POINT IN SAID PARCEL 1; THENCE ALONG THE PROPERTY LINE OF SAID PARCEL 1 NORTH 89°20'31" EAST 135.00 FEET TO THE SOUTH EAST CORNER OF SAID PARCEL 1; THENCE NORTH 00°39'23" WEST 225.45 FEET TO **THE POINT OF BEGINNING** FOR THIS DESCRIPTION.

SAID LAND CONSISTS OF 65,165 SQ. FT., OR 1.496 ACRES, MORE OR LESS.
END OF DESCRIPTION.

THE BASIS OF BEARINGS FOR THIS DESCRIPTION IS THE CENTERLINE FOR FREEPORT BOULEVARD, TAKEN AS NORTH 06°41'45" WEST, AS SHOWN ON THE PARCEL MAP RECORDED IN BOOK 210 PAGE 6, SACRAMENTO COUNTY RECORDS.

SECTION 2

Title 17 of the Sacramento City Code (“Planning and Development Code”) is hereby amended by rezoning the Property from Shopping Center (SC) to General Commercial (C-2).

SECTION 3

As required by section 17.808.230.C.1.d of the Planning and Development Code, the City Council finds that the rezoning of the property by this ordinance is consistent with the applicable general plan land-use designation, use, and development standards in that the project complies with the applicable development standards of the C-2 zone; the goals, policies, and other provisions of the general plan in that the project is consistent with policy related to infill development and enhancing and protecting established neighborhoods; and promotes the public health, safety, convenience, and welfare of the city by the building design and site improvements and the project will function harmoniously with the existing shopping center.

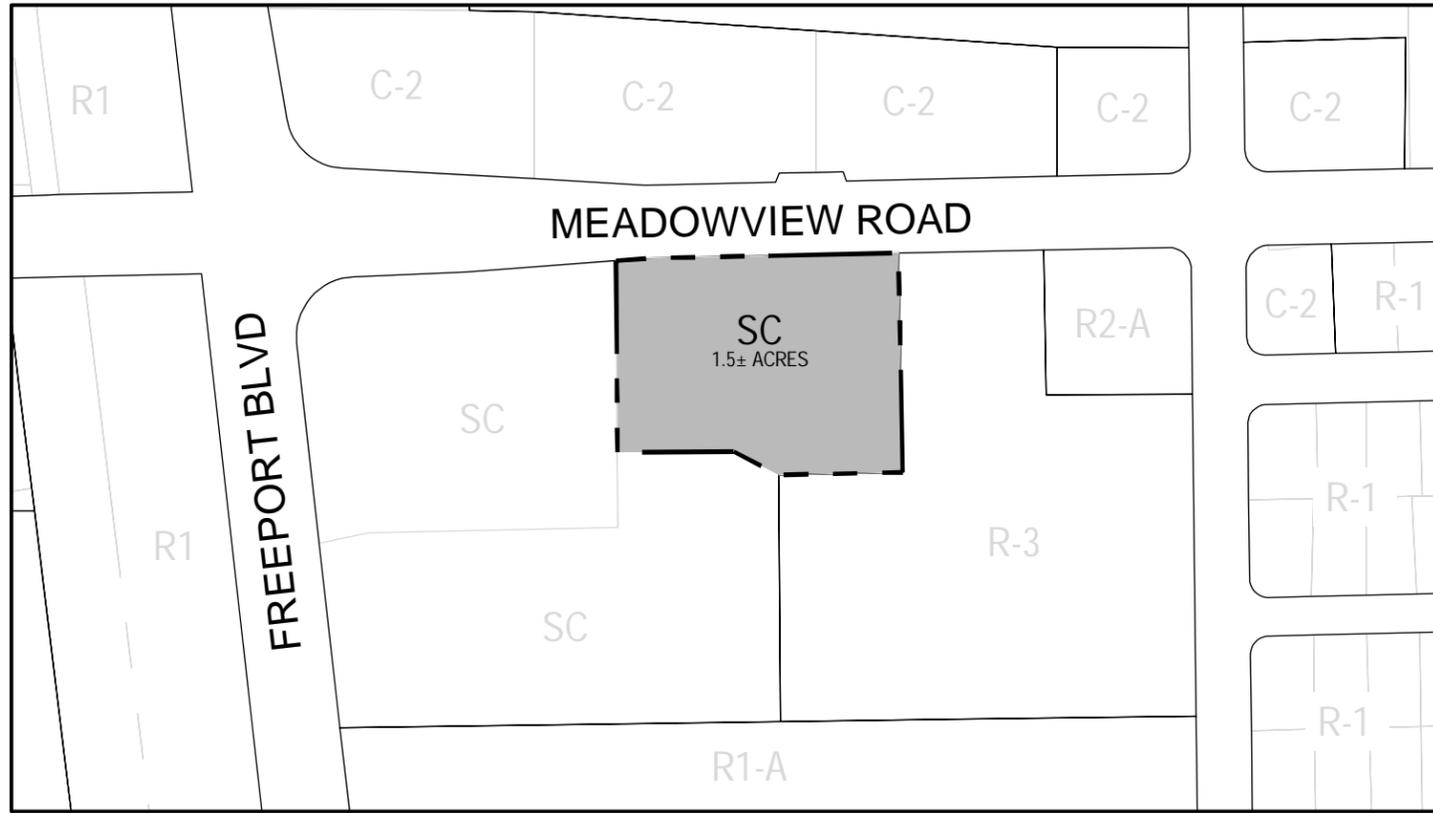
SECTION 4

The City Clerk is hereby directed to amend the City’s official zoning maps to conform to this ordinance.

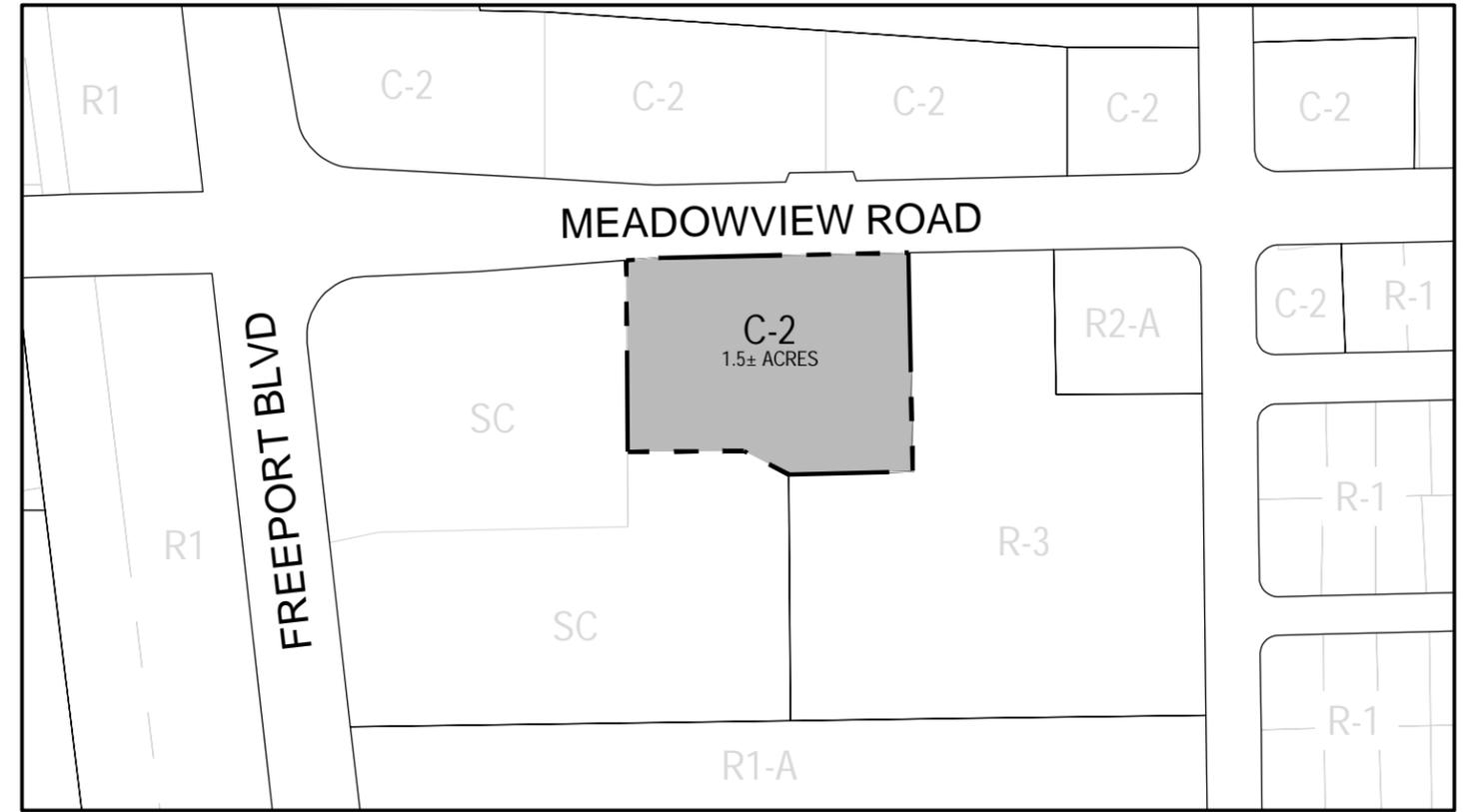
Exhibits

Exhibit 4b: Rezone Map (1 Page) is part of this ordinance

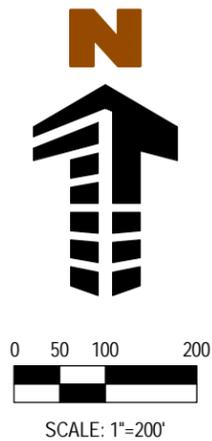
REZONE
LES SCHWAB TIRE COMPANY
 CITY OF SACRAMENTO, CALIFORNIA
 FEBRUARY 24, 2016



EXISTING ZONING



PROPOSED ZONING



REZONE SUMMARY TABLE				
DESIGNATION	LAND USE	EXISTING	PROPOSED	DIFFERENCE
SC	SHOPPING CENTER	1.5	0.0	-1.5
C-2	GENERAL COMMERCIAL ZONE	0.0	1.5	+1.5
		1.5	1.5	

RESOLUTION NO. 2016 –

Adopted by the Sacramento City Council

ADOPTING FINDING OF FACT AND APPROVING THE LES SCHWAB TIRE CENTER PROJECT (P16-009)

BACKGROUND

A. On April 21, 2016, after conducting a public hearing, the City Planning and Design Commission forwarded to the City Council a recommendation to approve the Les Schwab Tire Center Project (P16-009).

B. On May 31, 2016, after giving notice as requested by the Sacramento City Code section 17.812.010 (2)(b), the City Council conducted a public hearing on the Project, receiving and considering evidence concerning it.

BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:

Section 1. Based on the verbal and documentary evidence received at the hearing on the Les Schwab Tire Center Project, the City Council approves the Project entitlements based on the findings of fact and subject to the conditions of approval as set forth below.

Section 2. The City Council approves the Project entitlements based on the following findings of fact:

A&B. Environmental Determination: The addendum to the adopted MND and Mitigation Monitoring Program for the Project has been adopted by Resolution No. 2016-_____.

C. Rezone: The amendment of Title 17 to rezone 1.5± acres from the Shopping Center (SC) zone to the General Commercial (C-2) zone has been approved by Ordinance No. 2016-_____.

D. Conditional Use Permit to establish an auto service and repair use (tire center) in the General Commercial (C-2) zone is approved based on the following findings of fact:

1. The proposed use and its operating characteristics are consistent with the general plan and South Area Community Plan in that the project is consistent with General Plan policy related to infill development and enhancing and protecting established neighborhoods. The project is also supportive to the South Area Community Plan related to streetscape and neighborhood enhancement;

2. The proposed use and its operating characteristics are consistent with the applicable standards, requirements, and regulations of the zoning district in which it is located, and of all other provisions of this title and this code in that the project generally complies with the relative development standards of the C-2 zoning district. Where deviations are requested, staff finds that the setback and wall deviations are appropriate given the residential nature of the adjacent multi-unit apartment complex and staff's desire to protect existing heritage trees to the south of the project site;
3. The proposed use is situated on a parcel that is physically suitable in terms of location, size, topography, and access, and that is adequately served by public services and utilities in that the project will have access from Meadowview Road and will manage its stormwater and be served by the utilities needed to operate the store;
4. The proposed use and its operating characteristics are not detrimental to the public health, safety, convenience, or welfare of persons residing, working, visiting, or recreating in the surrounding neighborhood and will not result in the creation of a nuisance in that the building design and site improvements will attenuate noise to neighboring properties based on site design and the project has been designed, and is subject to conditions, such that the proposed use will function harmoniously with the existing shopping center and surrounding uses.

E. Site Plan and Design Review with deviations for the construction of an 11,500± square foot tire center is **approved** based upon the following findings of fact:

1. The design, layout, and physical characteristics of the proposed development are consistent with the General Plan Suburban Corridor designation and are consistent with the General Plan goals and policies related to infill development, enhancing and protecting established neighborhoods, while ensuring good neighbor policy.
2. The design, layout, and physical characteristics of proposed development are consistent with all applicable design guidelines and with the intent of all applicable development standards in that the proposed building is centrally located on the site and adequately addresses the neighboring uses through compatible design and site improvements. Where deviations are requested, staff finds that the setback and wall deviations are appropriate given the residential nature of the adjacent multi-unit apartment complex and staff's desire to protect existing heritage trees to the south of the project site;
3. All streets and other public access ways and facilities, parking facilities, and utility infrastructure are adequate to serve the proposed development and comply with all applicable design guidelines and development standards as

the project provides onsite and offsite improvements to sufficiently serve the project and its users through appropriate stormwater management, extension of necessary public and private utility infrastructure, and parking and landscaping improvements.

4. The design, layout, and physical characteristics of the proposed development are visually and functionally compatible with the surrounding neighborhood as the surrounding neighborhood is a mix of commercial and residential uses and the facade of the building utilizes a similar material and color pallet from nearby buildings ensuring a compatible architectural design. The onsite improvements will enhance the existing streetscape creating uniformity with development to the west and north.
5. The design, layout, and physical characteristics of the proposed development ensure energy consumption is minimized and use of renewable energy sources is encouraged. By preserving the existing heritage trees and planting the necessary trees to meet the 50 percent shade requirement helps shade the building and surfaces with a low albedo, ultimately reducing energy consumption needed to condition or cool the interior of the building.
6. The design, layout, and physical characteristics of the proposed development are not detrimental to the public health, safety, convenience, or welfare of persons residing, working, visiting, or recreating in the surrounding neighborhood and will not result in the creation of a nuisance through appropriate site design measures such as fencing, building placement, and the utilization of masonry construction materials for the building. The project has been conditioned to ensure a long-lasting operation that will function harmoniously with the existing shopping center and surrounding uses.

Conditions of Approval

- D. Conditional Use Permit** to establish an auto service and repair use (tire center) in the General Commercial (C-2) zone is **approved** based on the following conditions of approval:

PLANNING

- D 1. Approval of the Conditional Use Permit is for auto service and repair uses only (tire replacement and repair, brakes, alignment, and shocks). The addition of auto service uses beyond those requested shall be reviewed by Planning Staff and may be subject to additional entitlements.
- D 2. The project shall comply with applicable recycling and solid waste disposal regulations as prescribed in Planning and Development Code section 17.616.

- D 3. All auto related service and repair work shall be performed completely inside of the building. No outdoor work is permitted.
- D 4. All material(s) used for the auto service and repair business shall be stored entirely inside. No outdoor storage of auto service and repair material(s) is permitted.
- D 5. The roll up door located on the rear of the building (south elevation) shall remain closed during business hours, except for deliveries.
- D 6. The business shall be subject to the following hours of operation: Monday-Saturday from 8:00 a.m. until 6:00 p.m. and Sundays from 8:00 a.m. until 5:00 p.m.
- D 7. No service bays shall be constructed on the south elevation or rear of the building.
- D 8. The outside premises shall be kept clean of all trash and/or debris and landscaped areas shall be properly maintained, free of weeds. Disposal of tires or materials shall not be in public view.
- D 9. The project is required to meet the Sacramento City Code regulations, regarding bicycle parking (Chapter 17.608). Bicycle parking shall be located in a secure area in close proximity to public view, in well-lit areas near building entrances, and shall be installed prior to the issuance of the Certificate of Occupancy.
- D 10. Prior to issuance of a building permit, the business owners shall propose and submit for review and approval by the Planning Director a "Good Neighbor Policy" including, but not limited to, the following:
 - a. Establish a process for neighbors to communicate directly with the management staff should there be any problems or issues;
 - b. Provide a 24-hour emergency contact person and phone number on-site, in a clearly identified place.
- D 11. The parking lot shall be power washed every six months.

PUBLIC WORKS

- D 12. All new and existing driveways shall be designed and constructed to City Standards to the satisfaction of the Department of Public Works.
- D 13. Reciprocal access easements are required for shared use of the driveways w/ the adjacent parcel, if not already in place.

- D 14. The site plan shall conform to the parking requirements set forth in City Code 17.608.040 (Development standards for off-street parking facilities, Zoning Ordinance).
- D 15. The design of walls fences and signage near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height at maturity. The area of exclusion shall be determined by the Department of Public Works.
- E. Site Plan and Design Review** with deviations for the construction of an 11,500± square foot tire center is **approved** based upon the following conditions of approval:

PLANNING

- E 1. All proposed signage is subject to a separate review and approval process through a sign permit and/or building permit.
- E 2. Mechanical equipment shall be screened from all street views. Final plans indicating size and location of units shall be reviewed and approved by Design Review staff prior to issuance of building permits to ensure full compliance of requirement to fully screen all mechanical equipment.
- E 3. Fencing on southern property line adjacent to existing heritage trees shall be constructed to a minimum height of six feet made of composite or equivalent material, similar to the fencing detail provided in Exhibit F.
- E 4. The building shall be sited per approved plans.
- E 5. Final height and massing shall be per approved plans.
- E 6. The project shall have building entry and setbacks per approved plans.
- E 7. The design for the building shall be per approved plans.
- E 8. Any changes to the final approved set of plans shall be subject to review and approval by Design Review prior to Building Permit submittal. Project shall comply with all current building code requirements.
- E 9. The approval shall be deemed automatically revoked unless required permits have been issued and construction begun within three years of the date of the approval. Prior to expiration, an extension of time may be granted upon written request of the applicant.

- E 10. Final occupancy shall be subject to planning approval and involve an on-site inspection by Design Review staff.
- E 11. Project lighting shall comply with all applicable Sacramento City Code lighting regulations. Fixtures shall be unobtrusive and complementary to the architectural design of the building. Lighting shall be designed so as not to produce hazardous and annoying glare to motorists and building occupants, adjacent residents, or the general public. All fixtures should be placed in a manner that avoids glare when observed from the street or other public areas.

DOU

- E 12. Per City Code Section, 13.04.070, multiple water services to a single lot or parcel may be allowed if appropriate DOU Development Review and Field staff have reviewed the proposed number of taps and associated location and have no issues or concerns. Any new water service (other than fire) shall be metered. Excess services shall be abandoned to the satisfaction of the DOU.
- E 13. An on-site drainage system is required and shall be connected to the street drainage system by means of a storm drain service tap. No storm drain services can enter the back of public drop inlets.
- E 14. The finish floor shall be above the 100-yr HGL or 1.5-feet above the local controlling overland flow release elevation, whichever is higher or as approved by the DOU.
- E 15. All lots shall be graded so that drainage does not cross property lines or private drainage easements shall be dedicated.
- E 16. A grading plan showing existing and proposed elevations is required. Adjacent off-site topography shall also be shown to the extent necessary to determine impacts to existing surface drainage paths. At a minimum, one foot off-site contours within 100' of the project boundary are required. No grading shall occur until the grading plan has been reviewed and approved by the Department of Utilities.
- E 17. This project will disturb more than one acre of land; therefore, the project is required to comply with the State's "Construction General Permit" (Order 2009-0009 DWQ or most current). To comply with the State Permit, the applicant must file a Notice of Intent (NOI) through the State's Storm Water Multiple Application and Report Tracking System (SMARTS), located online at <http://smarts.waterboards.ca.gov/smarts/faces/SwSmartsLogin.jsp>
- E 18. A valid WDID number must be obtained and provided to the DOU prior to the issuance of any grading permits.

- E 19. The applicant must comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinance. This ordinance will require the applicant to prepare erosion and sediment control plans for both during and after construction of the proposed project, prepare preliminary and final grading plans, and prepare plans to control urban runoff pollution from the project site during construction.
- E 20. Per Section 11.2.2 of the City's Design and Procedures Manual, under no circumstances shall proposed infill drainage systems result in increased flooding that does harm. The applicant shall provide a drainage study that meets the "Do No Harm" criteria. The drainage study must be reviewed and approved by the Department of Utilities prior to building permit issuance. The applicant is advised to contact the City of Sacramento Utilities Department Drainage Section (916-808-1400) at the early planning stages to address any drainage related requirements.
- E 21. Post construction, stormwater quality control measures shall be incorporated into the development to minimize the increase of urban runoff pollution caused by development of the area. Since the project is not served by an existing regional water quality control facility, both source control and on-site treatment control measures (e.g., stormwater planters, detention basin, infiltration basin and/or trench, media filters (Austin Sand Filter), multi-functional drainage corridors, vegetated filter strips and/or swales, and proprietary devices) are required. A maintenance agreement is required for all on-site treatment control measures. Contact DOU for a list of accepted proprietary devices if considered for treatment control. Refer to the latest edition of the "Stormwater Quality Design Manual for the Sacramento and South Placer Regions (May 2007)" for appropriate source control measures. Runoff reduction measures (e.g. porous pavement) are optional control measures. Refer to the Runoff Reduction Credit Worksheet in the above Manual for porous pavement design. (Note: Onsite treatment control measures are required and may affect site design and site configuration and should be considered during the early planning stages.)

FIRE

- E 22. All turning radii for fire access shall be designed as 35' inside and 55' outside. CFC 503.2.4
- E 23. Roads used for Fire Department access shall have an unobstructed width of not less than 20' and unobstructed vertical clearance of 13'6" or more. CFC 503.2.1
- E 24. Roads used for Fire Department access that are less than 28 feet in width shall be marked "No Parking Fire Lane" on both sides; roads less than 36 feet in width shall be marked on one side.

- E 25. Fire Apparatus access roads shall be designed and maintained to support the imposed loads of fire apparatus and shall be surfaced so as to provide all-weather driving capabilities. CFC 503.2.3
- E 26. Provide the required fire hydrants in accordance with CFC 507 and Appendix C, Section C105.
Note: Appendix C, Table C105.1 has been amended by the City of Sacramento so that the required number of hydrants is based on the required GPM of Table B105.1 before any allowed fire sprinkler reduction with modified spacing distances between hydrants.
- E 27. Timing and Installation. When fire protection, including fire apparatus access roads and water supplies for fire protection, is required to be installed, such protection shall be installed and made serviceable prior to and during the time of construction. CFC 501.4
- E 28. Provide a water flow test. (Make arrangements at the Permit Center walk-in counter: 300 Richards Blvd, Sacramento, CA 95814). CFC 507.4
- E 29. Provide appropriate Knox access for site. CFC Section 506
- E 30. An automatic fire sprinkler system shall be installed in any portion of a building when the floor area of the building exceeds 3,599 square feet. CFC Fire Code Amendments 903.2 (a)
- E 31. Locate and identify Fire Department Connections (FDCs) on address side of building no further than 50 feet and no closer than 15 feet from a fire hydrant and not more than 30 feet from a paved roadway.
- E 32. An approved fire control room shall be provided for all buildings protected by an automatic fire extinguishing system. The room shall contain all system control valves, fire alarm control panels and other fire equipment required by the Fire Code Official. Fire Control rooms shall be located within the building at a location approved by the Fire Code Official, and shall be provided with a means to access the room directly from the exterior. Durable signage shall be provided on the exterior side of the access door to identify the fire control room. Fire Control rooms shall not be less than 50 square feet. CFC Amendments 903.4.1.1
- E 33. Construct standard public improvements as noted in these conditions pursuant to Title 18 of the City Code. Improvements shall be designed to City Standards and assured as set forth in Section 18.04.130 of the City Code. All improvements shall be designed and constructed to the satisfaction of the Department of Public Works. Any public improvement not specifically noted in these conditions shall be designed and constructed to City Standards. This shall include the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk

adjacent to the subject property per City standards to the satisfaction of the Department of Public Works.

PUBLIC WORKS

- E 34. All new and existing driveways shall be designed and constructed to City Standards to the satisfaction of the Department of Public Works.
- E 35. Reciprocal access easements are required for shared use of the driveways w/ the adjacent parcel, if not already in place.
- E 36. The site plan shall conform to the parking requirements set forth in City Code 17.608.040 (Development standards for off-street parking facilities, Zoning Ordinance).
- E 37. The design of walls fences and signage near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height at maturity. The area of exclusion shall be determined by the Department of Public Works.

POLICE

- E 38. Exterior lighting shall be white light using LED lamps with full cutoff fixtures to limit glare and light trespass. Color temperature shall be between 2700K and 4100K.
- E 39. Broken or damaged exterior lighting shall be repaired or replaced within 48 hours of being noted.
- E 40. Parking and bicycle parking shall be illuminated to a minimum of 1.5 foot candles per square foot of parking area during business hours and ¼ foot candles per square foot of surface area of all walkways, alcoves, and passageways serving the parking lot from one-half hour before dusk to one-half hour after dawn (SCC 17.608.040)
- E 41. Exterior lighting distribution and fixtures shall be approved by the Sacramento Police Department CPTED Sergeant (or designee) prior to issuance of a building permit.
- E 42. Exterior lighting shall be designed in coordination with the landscaping plan to minimize interference between the light standards and required illumination and the landscape trees and required shading (SCC 17.608.040).

- E 43. All light fixtures shall be vandal resistant (SCC 17.608.040).
- E 44. Exterior lighting shall be shielded or otherwise designed to avoid spill-over illumination to adjacent streets and properties (SCC 17.608.040).
- E 45. All mature landscaping shall follow the two foot six foot rule. All landscaping shall be ground cover, two feet or less and lower tree canopies of mature trees shall be above six feet. This increases natural surveillance and eliminates hiding areas within the landscape.
- E 46. Tree canopies shall not interfere with or block lighting. This creates shadows and areas of concealment. The landscaping plan shall allow for proper illumination and visibility regarding lighting and surveillance cameras through the maturity of trees and shrubs.
- E 47. Business shall be equipped with a monitored burglary alarm system.
- E 48. All solid core exterior doors shall be equipped with a 180 degree viewing device to screen persons before allowing entry, and shall remain locked at all times except for emergencies and deliveries.
- E 49. Recorded Video Assessment and Surveillance System (VASS) shall be employed.
- E 50. Cameras and VASS storage shall be digital high definition or better.
- E 51. VASS storage shall be kept off-site or in a secured area accessible only to management.
- E 52. VASS shall support standard MPEG formats.
- E 53. VASS shall be capable of storing no less than 30 days worth of activity.
- E 54. Manager with access to VASS storage shall be able to respond within 30 minutes during business hours.
- E 55. Manager shall have the ability to transfer recorded data to another medium (e.g. DVD, thumb drive, etc.).
- E 56. Cameras shall be equipped with low light capability, auto iris and auto focus.
- E 57. VASS shall provide comprehensive coverage of:
 - a. all points of sale
 - b. areas of ingress and egress (doors, driveways)
 - c. parking lot
 - d. loading areas

- e. areas not clearly visible from public streets
- f. coverage of all four (4) exterior sides of the property
- g. trash enclosure area

- E 58. No public pay phones/telephones shall be allowed on the premises.
- E 59. No coin operated games or video machines shall be allowed on the premises.
- E 60. The applicant is responsible for reasonably controlling the conduct of persons on the site and shall immediately disperse loiterers.
- E 61. All dumpsters shall be kept locked.
- E 62. Exterior trash receptacles shall be of a design to prevent unauthorized removal of articles from the trash bin.
- E 63. Any graffiti painted or marked upon the premises or on any adjacent area under the control of the applicant shall be removed or painted over with matching paint within 72 hours of being applied.
- E 64. The operator shall be responsible for the daily removal of all litter from the site.

SMUD

- E 65. There are existing underground facilities along the east property line running north/south and existing underground facilities on the south end running east/west. The developer will need to maintain GO 128 clearances. If requested to relocate, will 100% billable to the customer.
- E 66. Developer to comply with SMUD requirements; i.e. panel size/location, clearances from SMUD equipment, transformer location and service conductors.
- E 67. Structural setbacks less than 14-feet from Road R/W shall require the developer to conduct a pre-engineering meeting with all utilities to ensure proper clearances are maintained.
- E 68. Building foundations must have a minimum horizontal clearance of 5-feet from any SMUD trench. Developer to verify with other utilities (Gas, Telephone, etc.) for their specific clearance requirements.
- E 69. Proposed SMUD facilities located on the customer's property outside of the existing or proposed PUE(s) may require additional PUE and/or a dedicated SMUD easement.

- E 70. SMUD equipment shall be accessible to a 26,000-pound SMUD service vehicle in all weather. SMUD equipment shall be no further than 15-feet from a drivable surface. The drivable surface shall have a minimum width of 20-feet.

REGIONAL SAN

- E 71. Developing this property will require the payment of Regional San sewer impact fees. Regional San sewer impact fees shall be paid at the time of building permit issuance. For questions pertaining to Regional San sewer impact fees, please contact the Sewer Fee Quote Desk at (916) 876-6100.

URBAN FORESTY

- E 72. The project shall comply with the tree shading requirements for parking lots, prescribed in the Planning and Development Code section 17.612.040.
- E 73. Prior to any construction, protection fencing must enclose existing tree trunks approximately 20 inches south of the existing top of bank/slope, to define a planning and construction exclusion zone.
- E 74. Applicant shall obtain a City tree permit prior to pruning or removal of city or heritage trees.

Advisory Notes

- E 75. The proposed project is located in a shaded Zone X, on the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Maps (FIRMs), defined as areas of 0.2% annual chance flood; areas of 1% annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 1% annual chance flood. Accordingly, the project site lies in an area with no requirements to elevate or flood proof.
- E 76. There are existing street lights in the application area. No additional street lighting is required for this application.
- E 77. The applicant will be responsible to pay a Park Development Impact Fee (PIF) prior to issuance of a building permit. The Park Development Impact Fee due for this project is estimated at \$4,813. This estimate is based upon construction of a 11,458 square foot commercial retail building at the City's standard commercial retail rate of \$0.42 per square foot.
- E 78. The subject property is outside the boundaries of the SASD but within the Urban Service Boundary and SRCSD shown on the Sacramento County General Plan. SRCSD will provide ultimate conveyance and treatment of the sewer generated from this site, but the Sacramento City Utilities Department's approval will be required for local sewage service.



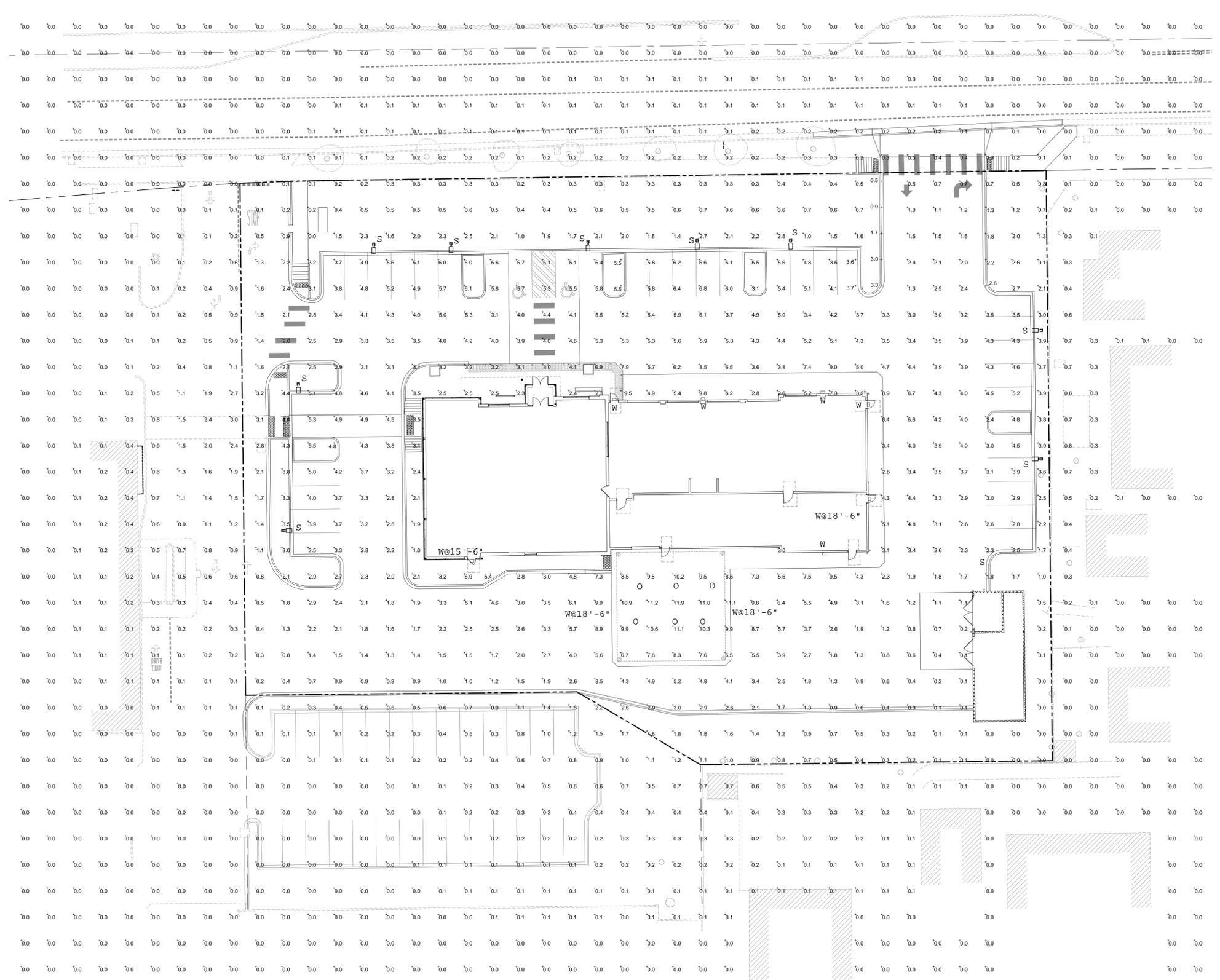
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PLANNING DOCUMENTS
 LES SCHWAB TIRE CENTER
 SACRAMENTO, CA
 MEADOWVIEW ROAD
 SACRAMENTO, CA

#	Date	Issue / Description	Init.
1	12/30/2015	1st Planning Submittal	NRS
2	03/25/2016	2nd Planning Submittal	NRS

Project No: LST10017
 Drawn By: JMG
 Checked By: JMG
 Date: NOVEMBER 30, 2015

PHOTOMETRIC PLAN



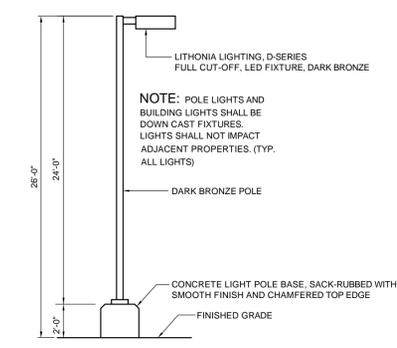
CONTRACTOR TO ENSURE THE LIGHT POLE AND HANDICAP PARKING SIGN ARE AT LEAST 2' FROM THE BACK OF CURB TO PREVENT VEHICLES FROM STRIKING THE LIGHT POLE OR HANDICAP PARKING SIGN. ALL PROPOSED 'D' FIXTURES ARE PLACED 3'-0" OFF OF BACK OF CURB. ENGINEER TO VERIFY PROPOSED LOCATIONS FOR ANY CONFLICTS OR MISPLACEMENTS. REPORT TO LIGHTING DESIGNER ANY DISCREPANCIES OR CONFLICTS

NOTES:
 PLAN WAS BASED ON THE INFORMATION PROVIDED. ALL DIMENSIONS, LUMINAIRE LOCATIONS SHOWN REPRESENT RECOMMENDED POSITIONS. THE ENGINEER AND/OR ARCHITECT MUST DETERMINE APPLICABILITY OF THE LAYOUT TO EXISTING OR FUTURE FIELD CONDITIONS.

THE PHOTOMETRIC PLAN REPRESENTS ILLUMINATION LEVELS CALCULATED FROM LABORATORY DATA TAKEN UNDER CONTROLLED CONDITIONS UTILIZING CURRENT INDUSTRY STANDARD LAMP RATINGS IN ACCORDANCE WITH ILLUMINATING ENGINEERING SOCIETY (IES) APPROVED METHODS. LABORATORY TESTS ARE MADE UNDER OPTIMUM CONDITION, WITH LAMP OUTPUT AT RATED VALUE, AND IN ACCORDANCE WITH ILLUMINATING ENGINEERING SOCIETY APPROVED METHODS.

ACTUAL ILLUMINANCE LEVELS MAY DIFFER DUE TO VARIABLE FIELD CONDITIONS SUCH AS (BUT NOT LIMITED TO): VARIANCE IN LAMP LUMEN OUTPUT; LAMP TILT FACTOR; BALLAST WATTAGE OUTPUT; LINE VOLTAGE AT BALLAST; REFLECTOR SPECULARITY; LAMP LUMEN DEPRECIATION; AND LUMINAIRE DIRT DEPRECIATION.

THIS PLAN IS FOR RELATIVE LAYOUT AND SCOPE OF WORK PURPOSES ONLY. REFER TO SITE PLAN PREPARED BY LOCAL CONSULTANT FOR RELATIONSHIP OF THESE LUMINAIRES AND THEIR LOCATIONS TO EXISTING STRUCTURES AND REFERENCE.



2 AREA LIGHT DETAIL
 SCALE: NOT TO SCALE

CALCULATION SUMMARY

LABEL	UNITS	AVG	MAX	MIN	AVG/MIN	MAX/MIN
PARKING	FC	4.68	6.8	2.3	2.03	2.96

LUMINAIRE SCHEDULE

SYMBOL	QTY	LABEL	TYPE	MODEL NUMBER	WATTS	LUMENS	DESCRIPTION
■	9	S	SINGLE	GLEON-AE-03-LED-E1-SL4-BZ	157	14896	COOPER LIGHTING, GALLEON SERIES LED AREA LUMINAIRE, 3 LIGHT SQUARES, 4000K CCT, SINGLE HEAD, TYPE IV WITH SPILL CONTROL DARK BRONZE COLOR, MOUNTED ON 24'-0" POLE WITH 2'-0" CONCRETE BASE SEE POLE SCHEDULE THIS SHEET
□	9	W	SINGLE	XTOR5A-N-MS-DIM-L20-CBP	41	4409	COOPER LIGHTING, CROSS TOUR MAXX SERIES LED WALL LUMINAIRE, 3500K CCT, DARK BRONZE, WALL MOUNTED AT 14'-0" UNLESS NOTED OTHERWISE
□	6	O	SINGLE	CNC-B04-LED-E1-WQ-TW-SM	95	9387	COOPER LIGHTING, CONCISE SERIES LED CANOPY LUMINAIRE, 4000K CCT, TRUE WHITE COLOR, SURFACE MOUNTED AT 16'-8" UNDER CANOPY

NOTE:
 ALL PROPOSED LIGHT FIXTURES ARE FULL CUT-OFF AND DOWNCAST.

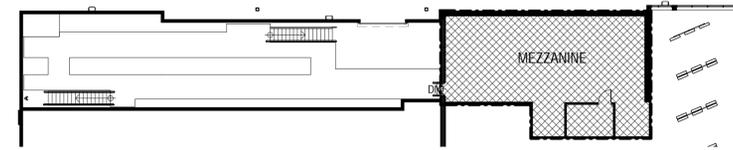
POLE SCHEDULE

SYMBOL	QTY	LABEL	ARRANGEMENT	MODEL NUMBER	DESCRIPTION
■	9	S	SINGLE	SSX-5A-24S-BZ-1-V	SQUARE STRAIGHT STEEL POLE, 24'-0", SINGLE FIXTURE, DARK BRONZE





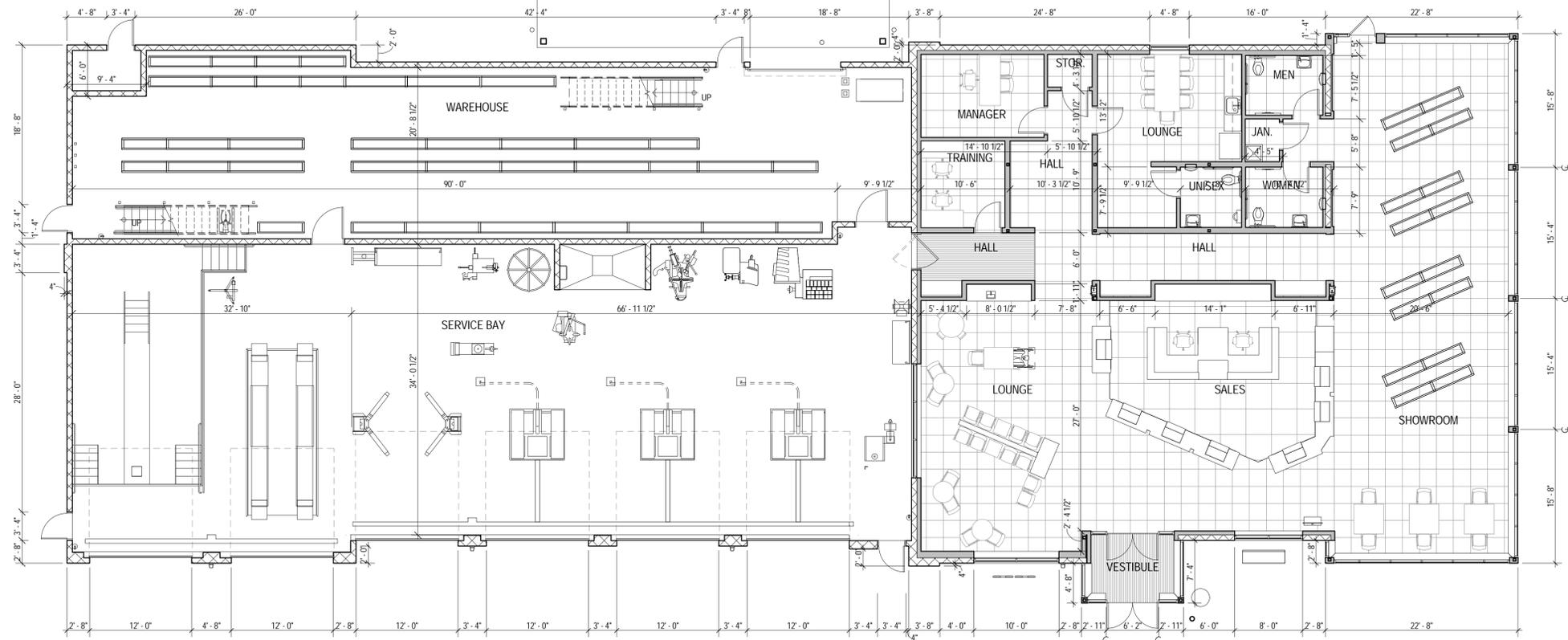
2 GROUND LEVEL DIAGRAM
SCALE: 1" = 20' 0"



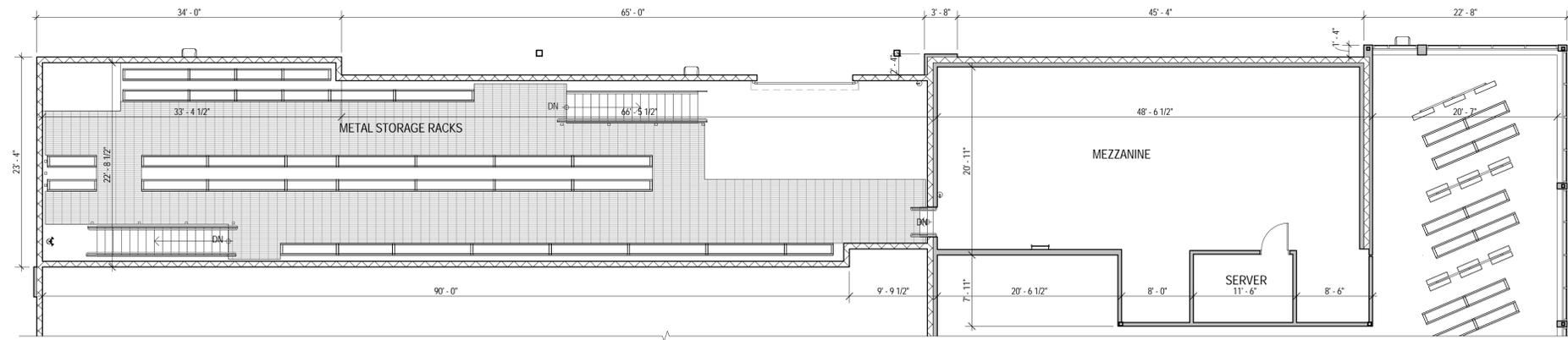
4 MEZZANINE LEVEL DIAGRAM
SCALE: 1" = 20' 0"

BUILDING FOOTPRINT AREA	
BUILDING FOOTPRINT	11878 ft ²
	11878 ft ²

FLOOR AREA	
GROUND LEVEL	
OFFICE/EMPLOYEE/SUPPORT	1668 ft ²
RETAIL	2687 ft ²
SERVICE BAY	3641 ft ²
WAREHOUSE	2144 ft ²
MEZZANINE LEVEL	
MEZZANINE	1317 ft ²
	11458 ft ²



1 GROUND LEVEL
SCALE: 1/8" = 1' 0"



3 MEZZANINE LEVEL
SCALE: 1/8" = 1' 0"



STAMP

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PLANNING DOCUMENTS
LES SCHWAB TIRE CENTER
SACRAMENTO, CA

MEADOWVIEW ROAD
SACRAMENTO, CA

#	Date	Issue/Description
1	12/30/15	1st Planning Submittal

Project No:	LST100017
Drawn By:	Author
Checked By:	Checker
Date:	PROJECT DATE

SHEET TITLE:
FLOOR PLANS



STAMP

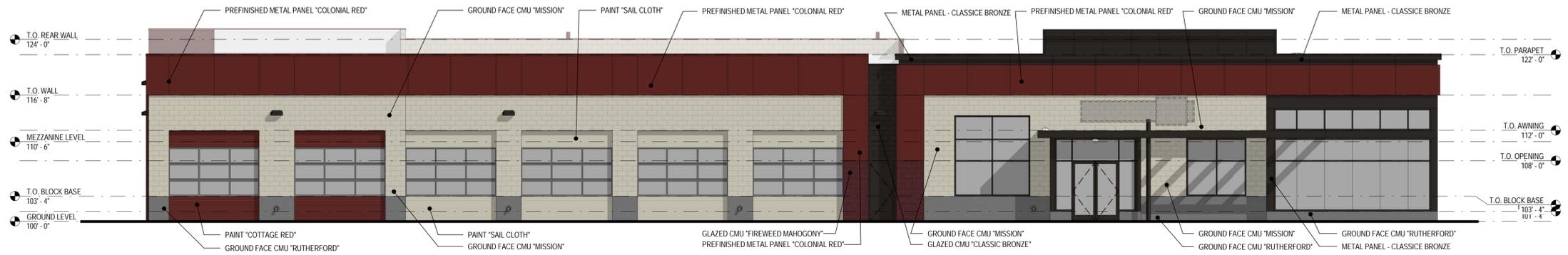
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PLANNING DOCUMENTS
 LES SCHWAB TIRE CENTER
 SACRAMENTO, CA
 MEADOWVIEW ROAD
 SACRAMENTO, CA

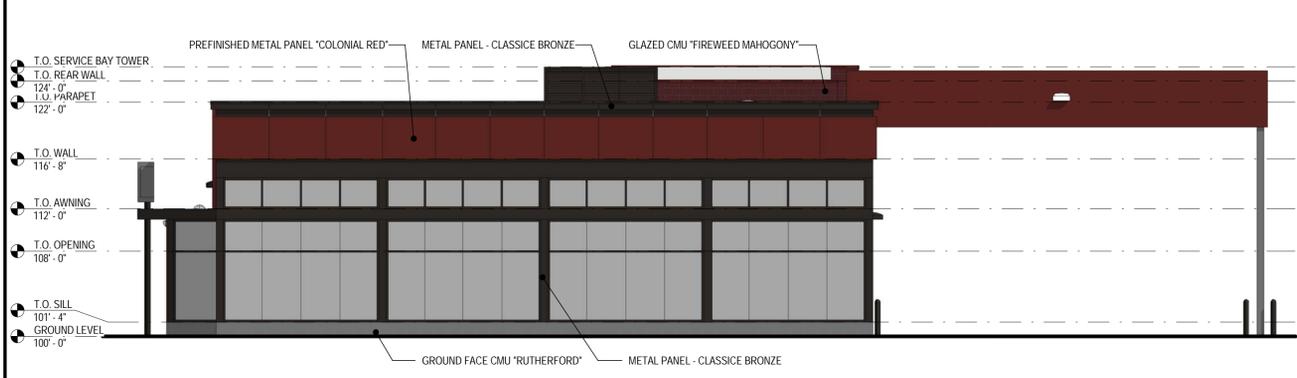
#	Date	Issue/Description
1	12/30/15	1st Planning Submittal

Project No:	LST100017
Drawn By:	Author
Checked By:	Checker
Date:	PROJECT DATE

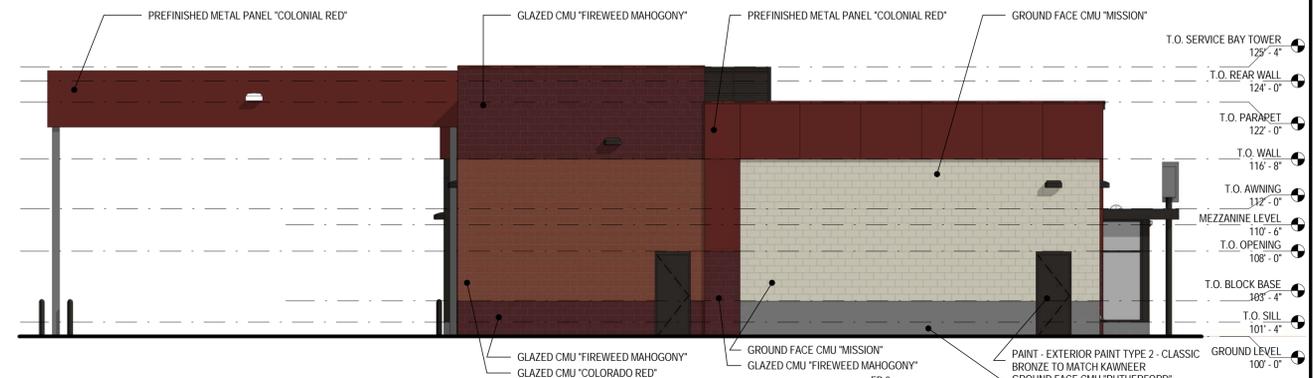
SHEET TITLE:
 COLOR ARCHITECTURAL
 ELEVATIONS
 (FOR REFERENCE ONLY)



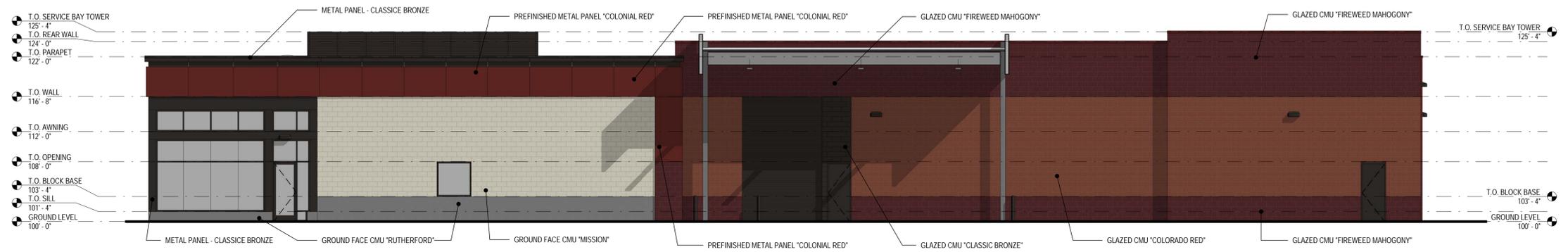
1 FRONT ELEVATION
 SCALE: 1/8" = 1'-0"



2 SHOWROOM ELEVATION
 SCALE: 1/8" = 1'-0"



3 LOADING ELEVATION
 SCALE: 1/8" = 1'-0"



4 REAR ELEVATION
 SCALE: 1/8" = 1'-0"

Trex Seclusions® Composite Fence Profiles

Post

Length 108" / 144"
 Inner Dimensions 3.75" varies
 Outer Dimensions 4.875 to 5"

Top Rail

Length 91"
 Thickness 1/2"
 Inner Depth 4 1/2"
 Top Width 4"

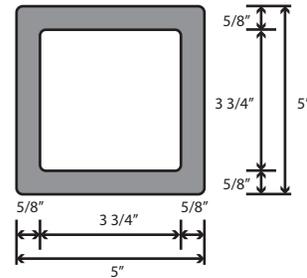
Bottom Rail Cover / Picket

Length 91" / 67" ±1/4"
 Height / Width 5 3/4"
 Interlock 1/2"

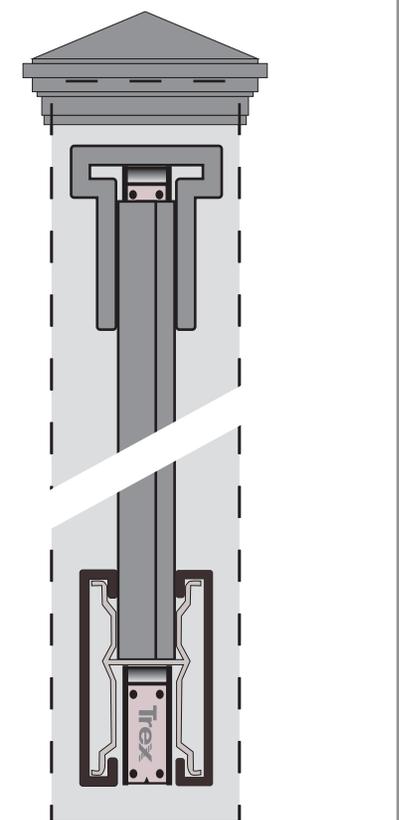
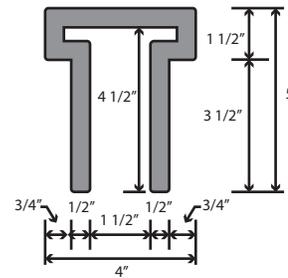
Aluminum Bottom Rail

Gauge 11 ga.
 Length 90.5"
 Height 5 1/8"
 Depth (Top Channel) 2 3/32"
 Depth (Bottom Channel) 2 15/32"

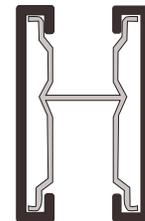
Post



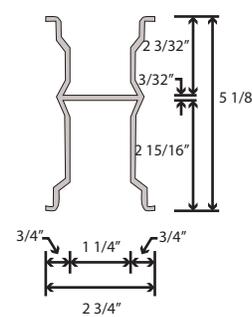
Top Rail



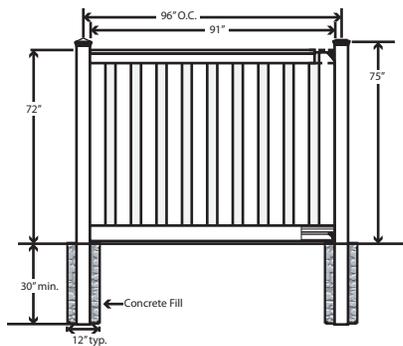
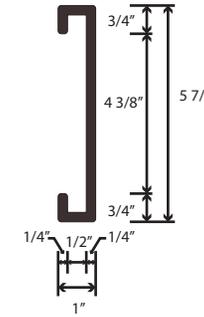
Bottom Rail Assembly



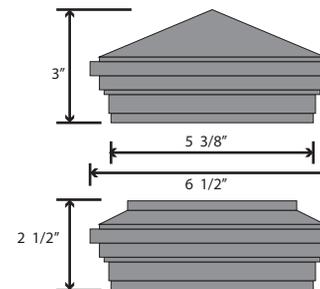
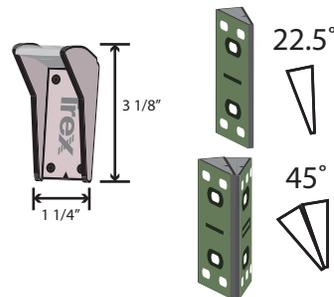
Aluminum Bottom Rail



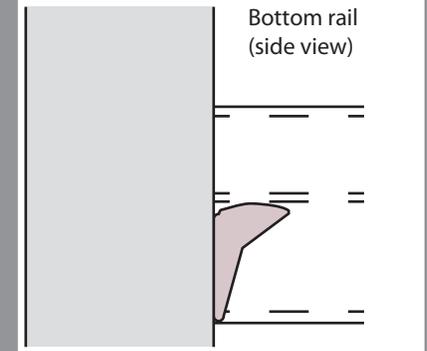
Bottom Rail Cover / Picket



Rail Bracket Angle Adapter



Bottom rail (side view)



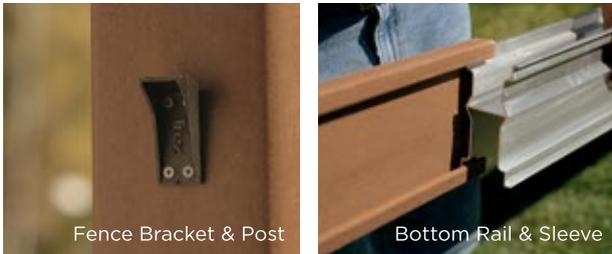


Trex® Fencing



Top Rail

Interlocking Pickets



Fence Bracket & Post

Bottom Rail & Sleeve



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COMPOSITE FENCING SYSTEM

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DURABILITY.
PRIVACY.**

BEAUTY AND PRIVACY FROM EVERY ANGLE

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High performance

- >> Never needs painting or staining
- >> Resists insect damage and won't warp, rot, or splinter
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- >> Wind rating of 110 mph steady and 130 mph gusts
- >> Customizable for height, style, and slope

Perennial beauty

- >> Three rich, natural colors that compliment any home
- >> Board-on-board look; same on both sides

Trex through and through

- >> Contains 95% recycled materials
- >> Backed by the Trex 25-year Limited Residential Warranty



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Trex fencing is used throughout the country for more than just residential properties. Its quality makes Trex Fencing a superb choice for commercial and governmental projects as well.

NATURAL MATTE COLOR FINISHES



Resolution of Support for the Les Schwab Application

WHEREAS, Les Schwab based in Bend, Oregon is seeking to establish a tire retail facility in the Freeport Marketplace, located in the Meadowview Neighborhood in Sacramento, CA;

WHEREAS, Les Schwab is seeking to purchase property and build a new retail store;

WHEREAS, Les Schwab provides tires for purchase, then installation and ancillary tire services such as brakes, shocks and alignments should be considered retail operations not requiring a parcel rezone thus preserving community and neighborhood jurisdiction and control for future applicants of the parcel, if needed;

WHEREAS, Les Schwab will establish presence with a proposed center on 1.5 acres located at east of the southeast corner of Freeport Boulevard and Meadowview Road, within the corporate limits of the City of Sacramento, CA.

WHEREAS, Les Schwab has accepted the "Good Neighbor" policy with the Meadowview Neighborhood Association;

WHEREAS, the Meadowview Neighborhood Association agrees with Walgreens and the Mueller Pet Medical Center, Animal Hospital that Les Schwab would a good retail addition to the Freeport marketplace;

WHEREAS, as a new tenant addition to the Freeport Marketplace, Les Schwab will provide goods and services needed by the community;

WHEREAS, direct tire sales and tires related services constitute more than 75% of business operations and will not provide oil, transmission or other hazardous fluids;

WHEREAS, Les Schwab's corporate philosophy and business vision to "...be like a hometown neighbor—reliable, valued and trustworthy," represents values that provide the foundation for a high-quality infill project in an existing neighborhood such as Meadowview;

THEREFORE BE IT RESOLVED, that the Meadowview Neighborhood Association supports the development of a Les Schwab in the Freeport Marketplace, adjacent to Walgreens; and

THEREFORE IT BE FURTHER RESOLVED, that the Meadowview Association encourages the Office of the City Manager and Community Development to support this effort.

Adopted this _____ Day of _____, 2015.

Meadowview Neighborhood Association
President

or

Meadowview Neighborhood Association
Secretary

(Signed copy attached)

**Les Schwab, Freeport Marketplace and the Meadowview Neighborhood
Association Good Neighbor Agreement Policy**

- Promote retail operations that does not encourage attractive nuisances such as: *loitering, littering, or any illicit activity;*
- ~~Promote a retail operation that does not sell alcohol or tobacco products;~~
- Work closely with existing retail and residential neighbors for the betterment of the entire neighborhood;
- ~~Promote local employment opportunities when possible; especially those residents who received tire installation certificate training at Cosumnes River College;~~
- Establish operating hours compatible with neighborhood commercial located near a residential area and consistent with existing retail operations (Home Depot Mon.-Sat.: 6:00 am to 9:00 pm, Sun: 7:00 am to 8:00 pm);
- Promote sustainable, healthy business practices and encourage Les Schwab to participate in community events;
- Keep immediate vicinity of the store clean and landscaped with no retail product (tires/related materials) disposed in containers or places exposed to public view;
- Provide lighting that is compatible with the adjacent residential area while enhancing security and safety;
- Employ noise cancellation and soundproofing architecture and construction to neutralize noise. Impacts on the existing neighborhood shall not exceed 90 decibels or the noise level of a lawnmower;
- Community will work with retailer to enhance the security, livability and positive shopping experience and to create a flourishing environment that allows for residents, business and customers to thrive;
- Finally, for Les Schwab to view the neighborhood and, for the neighborhood to view Les Schwab as a community partner invested in both the short and long term prosperity of the community, city and region.



3/4/2016

VIA EMAIL

Elise Gumm, Associate Planner
City of Sacramento
Community Development Department
300 Richard Boulevard, 3rd Floor
Sacramento, CA 95811

RE: Les Schwab Tire Center – Meadowview (P16-009)

Dear Ms. Gumm,

WALKSacramento has reviewed the Development Project Routing for the Les Schwab Tire Center at 1430 Meadowview Road. Thank you for the opportunity to review and provide comment on the project.

Development projects that lead to more walking and active travel are critical to our community's future. Although this project proposes a predominantly auto oriented use, there are opportunities to facilitate safe and convenient pedestrian travel to and from the site for customers and employees. To best encourage and accommodate those trips it is imperative that the site provides direct pedestrian routes to and throughout the site. Based on our review we offer the following observations and recommendations:

Extend the North/South Pedestrian Walkway to Meadowview Road.

Although the project narrative describes a continuous pedestrian path of travel from the building's entrance north to Meadowview road, the project plans show landscaping separating the walkway from the sidewalk. The proposed north/south walkway from the building's entrance should be continued through the landscaping to Meadowview Road to provide direct access between the sidewalk and the building. The extended walkway would ensure that employees accessing the site from the adjacent community or the nearby bus stop would not be required to walk through the parking lot to access the building. Additionally, the continuous path of travel to Meadowview Road would allow for customers to more directly access nearby commercial destinations and/or transit stops after dropping off their vehicle.

WALKSacramento is working to support increased physical activity such as walking and bicycling in local neighborhoods as well as helping to create community environments that support walking and bicycling. The benefits include improved physical fitness, less motor vehicle traffic congestion, better air quality, and a stronger sense of cohesion and safety in local neighborhoods.

Thank you for your consideration of these comments and recommendations. If you have questions or need additional information, please contact me at (916) 446-9255.

Sincerely,

Kirin Kumar
Project Manager