

1. Granting the Variance does not constitute a special privilege extended to an individual property owner in that variances would be granted to other property owners facing similar circumstances;
2. Granting the Variance would not be detrimental to the public welfare and not result in the creation of a public nuisance in that the trash enclosure will be located in an area where the garbage collector will not need access into the gated parking area for residents and the location is not adjacent to a residential use and preserves open space on the site for the benefit of the residents.
3. The project will provide adequate capacity, number, and distribution of recycling and trash enclosures and receptacles to serve the new development;
4. The project is consistent with the General Plan policies of utilizing the Multifamily Design Guidelines for residential development since the view of the trash enclosure from the public street will be minimized with the proposed location.

**Z. Variance:** The Variance to allow the new school to site to deviate from the required 26 feet of maneuvering room for 90 degree parking spaces has been **withdrawn**.

Section 3. The City Council approves the Project entitlements subject to the following conditions of approval:

**F. Tentative Map:** The Tentative Map to merge and resubdivide 16.51± acres into 3 lots for the Mercy Medical Campus (13.25± acres), Multifamily Development (.7± acres), and the Sacred Heart Parish Campus (2.56± acres) is approved subject to the following conditions of approval:

**CONDITIONS:** Tentative Map

**NOTE: These conditions shall supersede any contradictory information shown on the Tentative Map approved for this project (P04-215). The design of any improvement not covered by these conditions shall be to City standard.**

The applicant shall satisfy each of the following conditions prior to filing the Parcel Map unless a different time for compliance is specifically stated in these conditions. Any condition requiring an improvement that has already been designed and secured under a City Approved improvement agreement may be considered satisfied at the discretion of the Development Engineering Division.

The City strongly encourages the applicant to thoroughly discuss the conditions of approval for the project with their Engineer/Land Surveyor consultants prior to City Planning Commission approval. The improvements required of a Tentative Map can be costly and are completely dependent upon the condition of the existing improvements. Careful evaluation of the potential cost of the improvements required by the City will

enable the applicant to ask questions of the City prior to project approval and will result in a smoother plan check process after project approval:

**SPECIAL DISTRICTS:**

- F1. Pay off existing assessments, or file the necessary segregation requests and fees to segregate existing assessments.

**DEVELOPMENT ENGINEERING: Streets**

- F2. Comply with requirements included in the Mitigation Monitoring Plan developed by, and kept on file in, the Planning Division Office (P04-215).
- F3. Construct standard subdivision improvements as noted in these conditions pursuant to section 16.48.110 of the City Code. All improvements shall be designed and constructed to the satisfaction of the Development Engineering Division. Improvements required shall be determined by the city. Any public improvement not specifically noted in these conditions or on the Tentative Map shall be designed and constructed to City standards. This shall include street lighting and the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk per City standards to the satisfaction of the Development Engineering Division.
- F4. The project shall conform to A.D.A. requirements in all respects. This shall include the replacement of any curb ramp that does not meet current A.D.A. standards.
- F5. The applicant shall install a 4-way stop at the intersection of 39<sup>th</sup> Street and newly proposed school/Mercy driveways on 39<sup>th</sup> Street.
- F6. The design and placement of walls, fences, signs and Landscaping near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height. The area of exclusion shall be determined by the Development Engineering Division.

**CITY UTILITIES**

- F7. The applicant shall show all easements that serve the subject parcels and existing utilities including services that cross property lines.
- F8. The applicant shall show all easements that serve the subject parcels and existing utilities including services that cross property lines.

- F9. Any new domestic water services shall be metered. Per City Code 13.04.070, except for separate irrigation service connections and fire service connections, each lot or parcel shall only have one (1) metered domestic water service. Requests for multiple domestic water service connections to a single commercial lot or parcel, consistent with the Department of Utilities "Commercial Tap Policy", may be approved on a case-by-case basis by the Department of Utilities. Excess services shall be abandoned to the satisfaction of the Department of Utilities.
- F10. Multiple fire services are allowed per parcel and may be required.
- F11. All water connections shall comply with the City of Sacramento's Cross Connection Control Policy.
- F12. Dedicate a 10 foot wide Public Utility easement for an existing water main along the east property line of parcel 3 from H Street to J Street.
- F13. Dedicate a 10 foot wide Public Utility easement for an existing water main along the north property line of parcel 3 from the east property line to 180 linear feet to the west.
- F14. City records indicate a 6" public water main located on parcel 3 running approximately 300 feet east from 39<sup>th</sup> Street at the I Street entrance to the Mercy Hospital Campus. Either dedicate a 15 foot wide Public Utility Easement for the existing water main or reconfigure the system with the City's new point of service at the 39<sup>th</sup> Street property line. Reconfiguration will require at a minimum an approved backflow device and meter. The reconfiguration of this main shall be constructed to the satisfactions of the Department of Utilities.
- F15. Dedicate a 5 foot wide private utility easement for sewer service that is required on parcel 1 to serve 852 39<sup>th</sup> Street.
- F16. The applicant shall construct a 12" water main and sewer in 39<sup>th</sup> Street between H Street and J Street. The water distribution system shall be designed and constructed to City standards and installed in public streets as required by the Department of Utilities. The timing for construction shall meet the requirements as specified in condition R5.
- F17. The applicant must comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinances. This ordinance requires the applicant to show erosion and sediment control methods on the improvement plans. These plans shall also show the methods to control urban runoff pollution from the project site during construction.
- F18. This project is greater than 1 acre; therefore the project is required to comply with the "NPDES General Permit for Stormwater Discharges Associated with

Construction Activity” (State Permit). To comply with the State Permit, the applicant will need to file a Notice of Intent (NOI) with the State Water Resources Control Board (SWRCB) and prepare a Stormwater Pollution Prevention Plan (SWPPP) prior to construction. A copy of the State Permit and NOI may be obtained from [www.swrcb.ca.gov/stormstr/construction.html](http://www.swrcb.ca.gov/stormstr/construction.html). The SWPPP will be reviewed by the Department of Utilities prior to issuing a grading permit. The following items shall be included in the SWPPP: (1) vicinity map, (2) site map, (3) list of potential pollutant sources, (4) type and location of erosion and sediment BMP's, (5) name and phone number of person responsible for SWPPP and (6) certification by property owner or authorized representative.

- F19. Post construction (permanent), stormwater quality control measures shall be incorporated into the development to minimize the increase of urban runoff pollution caused by development of the area. Since the project is in an area served by a regional water quality control facility only source control measures are required. Refer to the “Guidance Manual for On-Site Stormwater Quality Control Measures” dated January 2000 for appropriate source control measures.

**G. Special Permit:** The Special Permit to allow a private school and parish ministries in the existing R-1 and R-3 zones and in the proposed R-1A zone is approved with the following conditions of approval:

- G1. The applicant shall obtain all necessary building and/or encroachment permits prior to commencing construction.
- G2. Any modification to the project shall be subject to review and approval by the Planning Division staff prior to the issuance of building permits.
- G3. A sign permit shall be obtained prior to construction or installation of any attached or detached sign.
- G4. The new private school and proposed fencing shall be reviewed and approved by design review staff.
- G5. The applicant shall construct the private school with the alternative plan which allows the Bunya Bunya tree to remain and the applicant shall work with the Urban Forest Services department to protect the tree during construction.
- G6. If lane closures are required on H or J Street, the closure shall maintain a minimum of one open lane for vehicular traffic. Lane closures shall be limited to 5 days total on H Street and 5 days total on J Street. In no case shall lane closures for the project occur on H and J Street simultaneously. If there is a closure of 39<sup>th</sup> Street, the work shall be completed in 90 days or less. If more time is needed, the extension shall require additional approval from the City

Council. The applicant shall coordinate with the Department of Transportation to minimize impacts for required street closures.

#### *Utilities*

- G7. Any new domestic water services shall be metered. Per City Code 13.04.070, except for separate irrigation service connections and fire service connections, each lot or parcel shall only have one (1) metered domestic water service. Requests for multiple domestic water service connections to a single commercial lot or parcel, consistent with the Department of Utilities "Commercial Tap Policy", may be approved on a case-by-case basis by the Department of Utilities. Excess services shall be abandoned to the satisfaction of the Department of Utilities.
- G8. All water connections shall comply with the City of Sacramento's Cross Connection Control Policy.
- G9. Per City Code, the point of service for water, sewer and storm drain service is located at the back of curb for separated sidewalks and at the back of sidewalk for attached sidewalks. The onsite water, sewer and storm drain systems shall be private systems maintained Owner.
- G10. Post construction (permanent), stormwater quality control measures shall be incorporated into the development to minimize the increase of urban runoff pollution caused by development of the area. Since the project is in an area served by a regional water quality control facility only source control measures are required. Refer to the "Guidance Manual for On-Site Stormwater Quality Control Measures" dated January 2000 for appropriate source control measures.
- G11. This project is greater than 1 acre; therefore the project is required to comply with the "NPDES General Permit for Stormwater Discharges Associated with Construction Activity" (State Permit). To comply with the State Permit, the applicant will need to file a Notice of Intent (NOI) with the State Water Resources Control Board (SWRCB) and prepare a Stormwater Pollution Prevention Plan (SWPPP) prior to construction. A copy of the State Permit and NOI may be obtained from [www.swrcb.ca.gov/stormstr/construction.html](http://www.swrcb.ca.gov/stormstr/construction.html). The SWPPP will be reviewed by the Department of Utilities prior to issuing a grading permit. The following items shall be included in the SWPPP: (1) vicinity map, (2) site map, (3) list of potential pollutant sources, (4) type and location of erosion and sediment BMP's, (5) name and phone number of person responsible for SWPPP and (6) certification by property owner or authorized representative.
- G12. The applicant must comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinances. This ordinance requires the applicant to show erosion and sediment control methods on the improvement plans. These plans

shall also show the methods to control urban runoff pollution from the project site during construction.

*Urban Forest Services*

- G13. The applicant shall retain a qualified consulting arborist (ISA Certified w/ verifiable construction management experience or ASCA Registered Consulting Arborist) as a project arborist. The project arborist shall advise the applicant and monitor construction activities that have potential to impact existing trees that are to be preserved. Arborist recommendations and monitoring reports shall be copied to Urban Forest Services staff.
- G14. Chain link (or other approved material) fencing shall be established as barriers to create exclusion zones surrounding trees. Exclusion barrier placement shall be subject to approval by Urban Forest Services staff prior to establishment. Barriers are to remain in place at all times during construction. Any necessary encroachment into exclusion zones shall be under the supervision of the project arborist. Exclusion zones shall be as large as possible.
- G15. In addition to exclusion zones, a root protection zone with a larger radius, as determined by the project arborist and approved by staff, shall be noted on all civil construction drawings noting the potential for root damage during excavation, trenching, or other grade changes. Excavation or other activities outside of exclusion zones but having the potential to affect roots shall be brought to the attention of the project arborist for review and consultation.
- G16. Prior to construction, trees to be preserved shall be pruned per current ANSI standards to remove dead wood, improve structure if necessary, and to provide adequate clearance for construction activity.
- G17. Trees to be preserved shall be maintained in a safe and healthy state and shall be irrigated, fertilized, or otherwise treated as necessary per the project arborist recommendations.
- G18. General tree health shall be monitored by the project arborist for a three year period following the end of construction. The project arborist shall advise the applicant on best management practices to maintain long term health of the trees.

*Development Engineering*

- G19. Construct standard subdivision improvements as pursuant to section 16.48.110 of the City Code (if not already in place). All improvements shall be designed and constructed to the satisfaction of the Development Engineering. Any public improvement not specifically noted in these conditions shall be designed and constructed to City Standards. This shall include any required street lights (if not already installed) and the repair or replacement/reconstruction of any

existing deteriorated curb, gutter and sidewalk (if improvements already exist) per City standards to the satisfaction of the Development Engineering Division.

- G20. All driveways shall be designed and constructed/reconstructed to City Standards to the satisfaction of the Development Engineering Division with the exception of the proposed 39<sup>th</sup> street school/mercy driveways which shall be constructed as a standard intersection.
- G21. The applicant shall install a 4-way stop at the intersection of 39<sup>th</sup> Street and newly proposed school/mercy driveways on 39<sup>th</sup> Street
- G22. The project shall conform to A.D.A. requirements in all respects. This shall include the replacement of any curb ramp or driveway that does not meet current A.D.A. standards.
- G23. The site plan shall conform to the parking requirements set forth in chapter 17 of City Code (Zoning Ordinance).
- G24. The design of walls fences and signage near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height at maturity. The area of exclusion shall be determined by the Development Engineering Division.

*Fire Department*

- G25. Fire apparatus access will be required into the "Hard Court/event parking" area. Identify a lane for Fire Department access and mark "No Parking Fire Lane".
- G26. The "Hard Court" area requires an approved Fire Department turnaround as part of the access lane. Turning radii for fire turnaround shall be designed as 35' inside and 55' outside. The exact configuration may be worked out by contacting the Fire Department.
- G27. Roads used for Fire Department access shall have an unobstructed width of not less than 20' and unobstructed vertical clearance of 13'6" or more.
- G28. Fire apparatus access roads shall be designed and maintained to support the imposed loads of fire apparatus and shall be provided with a roadbed consisting of material unaffected by the introduction of water based upon fire flow or rain based on 25 year storm and a surface consisting of a minimum of a single layer of asphalt. CFC 902.2.2.2

- G29. Provide the required fire hydrants in accordance with CFC 903.4.2 and Sacramento Municipal Code 15.36.040, TABLE NO. A-III-B-1. Hydrant spacing shall be every 300 feet along the fire access route.
- G30. Timing and Installation. When fire protection, including fire apparatus access roads and water supplies for fire protection, is required to be installed, such protection shall be installed and made serviceable prior to and during the time of construction.
- G31. Provide a water flow test. (Make arrangements at the Downtown Permit Center's walk-in counter: New City Hall, 3<sup>rd</sup> Flr, 915 I St. OR the North Permit Center's walk-in counter: 2101 Arena Blvd., Suite 200, Sacramento, CA 95834)
- G32. The furthest projection of the exterior wall of a building shall be accessible from within 150 ft of an approved Fire Department access road and water supply as measured by an unobstructed route around the exterior of the building. (CFC 902.2.1)
- G33. Provide appropriate Knox access for site.
- G34. Locate and identify Fire Department Connections (FDCs) on address side of building no further than 40 feet and no closer than 5 feet from a fire hydrant.

**H. Special Permit:** The Special Permit to allow a new heart center to exceed the 45 foot height requirement in the H is approved with the following conditions of approval:

- H1. The project shall conform to the plans submitted and any changes including modifications in design, materials, and landscaping shall require additional review and approval by Planning staff.

**I. Special Permit:** The Special Permit to allow 35 offsite parking for the school on the Mercy site and to share parking during off-peak hours with the school for evening assemblies and events on the weekend is approved with the following conditions of approval:

- I1. The designated parking spaces for faculty and visitors shall be noted with signage and/or striping.
- I2. All new parking spaces shall meet the 50% tree shading and all other development standards.
- I3. Lighting for the new surface parking lot shall be directed and focused downward to minimize any glare on the adjacent residential homes.

**J. Special Permit:** The Special Permit to allow offsite parking for the Mercy McMahon

Terrace on the Mercy General Hospital site is approved based on the following conditions of approval:

- J1. The designated parking spaces for MMT users shall be noted with signage and/or striping.
- J2. All new parking spaces shall meet the 50% tree shading and all other development standards.
- J3. Lighting for the new surface parking lot shall be directed and focused downward to minimize any glare on the adjacent residential homes.

**K. Special Permit:** The Special Permit to allow offsite parking for the residential development on the Mercy site is approved based on the following conditions of approval:

- K1. The parking spaces for the residential development on the Mercy site shall be designated with signage or striping indicating the exclusive use of the residents and their guests only.
- K2. All new parking spaces shall meet the 50% tree shading and all other development standards.
- K3. Lighting for the new surface parking lot shall be directed and focused downward to minimize any glare on the adjacent residential homes.

**N. Plan Review:** The Plan Review for the development of 20 residential units in the proposed R-3 zone is approved based on the following conditions of approval:

- N1. The applicant shall work with design review staff and the community to finalize the design of the multifamily complex. The City Urban Design Manager shall approve the final design.
- N2. If lane closures are required on H or J Street, the closure shall maintain a minimum of one open lane for vehicular traffic. Lane closures shall be limited to 5 days total on H Street and 5 days total on J Street. In no case shall lane closures for the project occur on H and J Street simultaneously. If there is a closure of 39<sup>th</sup> Street, the work shall be completed in 90 days or less. If more time is needed, the extension shall require additional approval from the City Council. The applicant shall coordinate with the Department of Transportation to minimize impacts for required street closures.

*Development Engineering*

- N3. Construct standard subdivision improvements as pursuant to section 16.48.110 of the City Code (if not already in place). All improvements shall be designed and constructed to the satisfaction of the Development Engineering. Any public

improvement not specifically noted in these conditions shall be designed and constructed to City Standards. This shall include any required street lights (if not already installed) and the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk (if improvements already exist) per City standards to the satisfaction of the Development Engineering Division.

- N4. The project shall conform to A.D.A. requirements in all respects. This shall include the replacement of any curb ramp that does not meet current A.D.A. standards.
- N5. The site plan shall conform to the parking requirements set forth in chapter 17 of City Code (Zoning Ordinance).
- N6. The design of walls fences and signage near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height at maturity. The area of exclusion shall be determined by the Development Engineering Division.

*Fire Department*

- N7. All turning radii for fire access shall be designed as 35' inside and 55' outside.
- N8. Fire apparatus access roads shall be designed and maintained to support the imposed loads of fire apparatus and shall be provided with a roadbed consisting of material unaffected by the introduction of water based upon fire flow or rain based on 25 year storm and a surface consisting of a minimum of a single layer of asphalt. CFC 902.2.2.2
- N9. Roads used for Fire Department access shall have an unobstructed width of not less than 20' and unobstructed vertical clearance of 13'6" or more.
- N10. Provide the required fire hydrants in accordance with CFC 903.4.2 and Appendix III-B, Section 5. The required number of hydrants is based on the construction type and the fire flow requirement. From the preliminary information provided, the flow requirement should be 3750 gpm for 3 hours, with a hydrant requirement of 4 (a hydrant must be within 150' of all points of the exterior of the building).
- N11. For operational fire fighting purposes, it is necessary for fire apparatus to access the Mercy Campus on the adjacent parcel. A reciprocal ingress egress agreement shall be provided for review by City Attorney for all shared driveways being used for Fire Department access.
- N12. Timing and Installation. When fire protection, including fire apparatus access roads and water supplies for fire protection, is required to be installed, such

protection shall be installed and made serviceable prior to and during the time of construction.

- N13. Provide a water flow test. (Make arrangements at the Downtown Permit Center's walk-in counter: New City Hall, 3<sup>rd</sup> Flr, 915 I St. OR the North Permit Center's walk-in counter: 2101 Arena Blvd., Suite 200, Sacramento, CA 95834).
- N14. The furthest projection of the exterior wall of a building shall be accessible from within 150 ft of an approved Fire Department access road and water supply as measured by an unobstructed route around the exterior of the building. (CFC 902.2.1).
- N15. Provide appropriate Knox access for site.
- N16. Roads used for Fire Department access that are less than 28 feet in width shall be marked "No Parking Fire Lane" on both sides; roads less than 36 feet in width shall be marked on one side.
- N17. Locate and identify Fire Department Connections (FDCs) on address side of building no further than 40 feet and no closer than 5 feet from a fire hydrant.
- N18. Provide a minimum of 5' clear setback for emergency rescue ladder access to 2<sup>nd</sup> story bedroom egress windows and an additional 3' for 3<sup>rd</sup> story bedroom egress windows. Provide clear access to building openings, free of landscaping and other obstructions. Exterior doors and openings required by this code or the Building Code shall be maintained readily accessible for emergency access by the Fire Department. CFC 902.3.1.

#### *Utilities*

- N19. Any new domestic water services shall be metered. Per City Code 13.04.070, except for separate irrigation service connections and fire service connections, each lot or parcel shall only have one (1) metered domestic water service. Requests for multiple domestic water service connections to a single commercial lot or parcel, consistent with the Department of Utilities "Commercial Tap Policy", may be approved on a case-by-case basis by the Department of Utilities. Excess services shall be abandoned to the satisfaction of the Department of Utilities.
- N20. Per City Code, the point of service for water, sewer and storm drain service is located at the back of curb for separated sidewalks and at the back of sidewalk for attached sidewalks. The onsite water, sewer and storm drain systems shall be private systems maintained Owner.
- N21. Post construction (permanent), stormwater quality control measures shall be incorporated into the development to minimize the increase of urban runoff pollution caused by development of the area. Since the project is in an area served by a regional water quality control facility only source control measures

are required. Refer to the "Guidance Manual for On-Site Stormwater Quality Control Measures" dated January 2000 for appropriate source control measures.

N22. The applicant must comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinances. This ordinance requires the applicant to show erosion and sediment control methods on the improvement plans. These plans shall also show the methods to control urban runoff pollution from the project site during construction.

**O. Special Permit Major Modification:** The Special Permit Major Modification to demolish the East Wing and replace with a surface parking lot is approved with the following conditions of approval:

- O1. The lighting for the new surface parking lot shall be directed and focused downward to minimize any glare on the adjacent residential homes.
- O2. All new parking spaces shall meet the 50% tree shading and all other development standards.
- O3. The East Wing shall be demolished within one year following the certificate of occupancy for the new heart center.

**P. Special Permit Major Modification:** The Special Permit Major Modification to demolish the chapel and replace with a surface parking lot is approved with the following conditions of approval:

- P1. The lighting for the new surface parking lot shall be directed and focused downward to minimize any glare on the adjacent residential homes.
- P2. All new parking spaces shall meet the 50% tree shading and all other development standards.
- P3. A public plaza area shall be incorporated into the site as detailed further in Condition R2.

#### *Utilities*

- P4. An onsite surface drainage system is required and shall be connected to the street drainage system by means of a storm drain service tap. All onsite systems shall be designed to the standard for private storm drainage systems (per Section 11.12 of the Design and Procedures Manual).
- P5. A grading plan showing existing and proposed elevations is required. Adjacent off-site topography shall also be shown to the extent necessary to determine impacts to existing surface drainage paths. No grading shall occur until the grading plan has been reviewed and approved by the Department of Utilities.

- P6. The applicant must comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinances. This ordinance requires the applicant to show erosion and sediment control methods on the improvement plans. These plans shall also show the methods to control urban runoff pollution from the project site during construction.
- P7. Post construction (permanent), stormwater quality control measures shall be incorporated into the development to minimize the increase of urban runoff pollution caused by development of the area. Since the project is in an area served by a regional water quality control facility only source control measures are required. Refer to the "Guidance Manual for On-Site Stormwater Quality Control Measures" dated January 2000 for appropriate source control measures.
- P8. Any new domestic water services shall be metered. Per City Code 13.04.070, except for separate irrigation service connections and fire service connections, each lot or parcel shall only have one (1) metered domestic water service. Requests for multiple domestic water service connections to a single commercial lot or parcel, consistent with the Department of Utilities "Commercial Tap Policy", may be approved on a case-by-case basis by the Department of Utilities. Excess services shall be abandoned to the satisfaction of the Department of Utilities.

**Q. Special Permit Major Modification:** The Special Permit Major Modification to demolish the Sacred Heart Parish School and replace with a surface parking lot is approved with the following conditions of approval:

- Q1. The lighting for the new surface parking lot shall be directed and focused downward to minimize any glare on the adjacent residential homes.
- Q2. All new parking spaces shall meet the 50% tree shading and all other development standards.
- Q3. A public plaza shall be incorporated into the site as detailed further in Condition R2.

#### *Utilities*

- Q4. An onsite surface drainage system is required and shall be connected to the street drainage system by means of a storm drain service tap. All onsite systems shall be designed to the standard for private storm drainage systems (per Section 11.12 of the Design and Procedures Manual).
- Q5. A grading plan showing existing and proposed elevations is required. Adjacent off-site topography shall also be shown to the extent necessary to determine impacts to existing surface drainage paths. No grading shall occur until the grading plan has been reviewed and approved by the Department of Utilities.

- Q6. The applicant must comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinances. This ordinance requires the applicant to show erosion and sediment control methods on the improvement plans. These plans shall also show the methods to control urban runoff pollution from the project site during construction.
- Q7. Post construction (permanent), stormwater quality control measures shall be incorporated into the development to minimize the increase of urban runoff pollution caused by development of the area. Since the project is in an area served by a regional water quality control facility only source control measures are required. Refer to the "Guidance Manual for On-Site Stormwater Quality Control Measures" dated January 2000 for appropriate source control measures.
- Q8. Any new domestic water services shall be metered. Per City Code 13.04.070, except for separate irrigation service connections and fire service connections, each lot or parcel shall only have one (1) metered domestic water service. Requests for multiple domestic water service connections to a single commercial lot or parcel, consistent with the Department of Utilities "Commercial Tap Policy", may be approved on a case-by-case basis by the Department of Utilities. Excess services shall be abandoned to the satisfaction of the Department of Utilities.

**R. Special Permit Major Modification:** The Special Permit Major Modification to construct a new 123,350 square foot heart center is approved with the following conditions of approval:

- R1. Neither Mercy nor any of its affiliates shall acquire an ownership or leasehold interest in or apply for the rezoning or utilization of any residentially zoned real property located within the area bounded by Alhambra and Elvas/65th Street, the UPRR tracks to Hwy 50, excluding any property currently occupied by Sutter Memorial Hospital and excluding the existing Mercy General Hospital campus, as defined on the north by H Street, on the south by J Street, on the west by the property line of the Sacred Heart Parish School site, and on the east by the hospital's property line along 41<sup>st</sup> Street. For purposes of this condition, "residentially zoned real property" shall include but not be limited to property having any one of the following zoning designations: R-1, R-1A, R-1B, R-2, R-2A, R-2B, R-3, R-3A, R-4, R-5, RMX, or RO. Any modification to this condition shall require approval from the City Council. Mercy shall cooperate with the City to incorporate this condition into the City's General Plan Update in 2008 and any subsequent East Sacramento Community Plan.
- R2. Representatives from Mercy shall work with the Urban Design Manager and the community to develop and implement as part of the heart center project a "pedestrian and streetscape master plan" for the site. The plan shall address internal connections on the campus to ensure the safety and convenience of patients, visitors, and the public who navigate the campus. In addition, the plan shall incorporate an enhanced, shaded, well-articulated, widened and signed

pedestrian streetscape around the perimeter of and through key buildings on the entire campus. For example, industrial facilities such as the proposed emergency power generator and oxygen tank at the driveway entrance at 39<sup>th</sup> and I Streets shall be adequately screened or relocated to a less visible internal location. The plan shall include plazas with fountains and/or gathering areas at key entry points, including the proposed surface parking lot north of I Street on the east side of 39<sup>th</sup> Street, the area south of the driveway entrance at 39<sup>th</sup> and I Streets, the bus stop on J Street in front of the South Building, and the proposed surface parking lot where the current chapel is located. Preliminary landscaping plans being developed by the applicant show an estimated 10 parking spaces removed to incorporate the improvements to the site. The plan shall also consider pedestrian-level lighting, undergrounding the utilities along the east side of 39<sup>th</sup> Street, exploring the reduction of the curb radii at the 39<sup>th</sup> and J intersection to improve the pedestrian landings at both the northeast and northwest corners, widening the curb cut at the southwest corner of 39<sup>th</sup> and H Streets, and other traffic calming measures along the street to enhance pedestrian safety and slow automobile and truck traffic. The City's Urban Design Manager shall review and approve the master plan.

- R3. The community shuttle route shall be modified to include 39<sup>th</sup> Street from H Street to the 39<sup>th</sup> Street Light Rail Station. The applicant shall work on a Task Force basis with Regional Transit and other transit providers to coordinate shuttle service within East Sacramento.
- R4. The tentative map shall be finalized before the final occupancy of the heart center.
- R5. Construction shall begin for the residential development on H Street immediately following the relocation of Sacred Heart Parish School to the new school campus.
- R6. If lane closures are required on H or J Street, the closure shall maintain a minimum of one open lane for vehicular traffic. Lane closures shall be limited to 5 days total on H Street and 5 days total on J Street. In no case shall lane closures for the project occur on H and J Street simultaneously. If there is a closure of 39<sup>th</sup> Street, the work shall be completed in 90 days or less. If more time is needed, the extension shall require additional approval from the City Council. The applicant shall coordinate with the Department of Transportation to minimize impacts for required street closures.
- R7. The East Wing shall be demolished within one year following the certificate of occupancy for the new heart center.

*Development Engineering*

- R8. Construct standard subdivision improvements as pursuant to section 16.48.110 of the City Code (if not already in place). All improvements shall be designed and constructed to the satisfaction of the Development Engineering. Any public

- improvement not specifically noted in these conditions shall be designed and constructed to City Standards. This shall include any required street lights (if not already installed) and the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk (if improvements already exist) per City standards to the satisfaction of the Development Engineering Division.
- R9. All driveways shall be designed and constructed/reconstructed to City Standards to the satisfaction of the Development Engineering Division with the exception of the proposed 39<sup>th</sup> street school/mercy driveways which shall be constructed as a standard intersection.
- R10. The applicant shall install a 4-way stop at the intersection of 39<sup>th</sup> Street and newly proposed school/mercy driveways on 39<sup>th</sup> Street.
- R11. The project shall conform to A.D.A. requirements in all respects. This shall include the replacement of any curb ramp or driveway that does not meet current A.D.A. standards.
- R12. The site plan shall conform to the parking requirements set forth in chapter 17 of City Code (Zoning Ordinance).
- R13. The design of walls fences and signage near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height at maturity. The area of exclusion shall be determined by the Development Engineering Division.
- R14. An automatic fire sprinkler system shall be installed and equipped with an electronic monitoring system as follows: in every building where there is an addition to the floor area in existence of 20 percent or more and the aggregate floor area of the building exceeds 4,999 square feet. Sacramento Municipal Code 15.36.1003 Amendment of Article 10, Section 1003 or other alternative as approved by the Fire Marshall.
- R15. All turning radii for fire access shall be designed as 35' inside and 55' outside.
- R16. Fire apparatus access roads shall be designed and maintained to support the imposed loads of fire apparatus and shall be provided with a roadbed consisting of material unaffected by the introduction of water based upon fire flow or rain based on 25 year storm and a surface consisting of a minimum of a single layer of asphalt. CFC 902.2.2.2
- R17. Roads used for Fire Department access shall have an unobstructed width of not less than 20' and unobstructed vertical clearance of 13'6" or more.

- R18. Provide the required fire hydrants in accordance with CFC 903.4.2 and Appendix III-B, Section 5.
- R19. Timing and Installation. When fire protection, including fire apparatus access roads and water supplies for fire protection, is required to be installed, such protection shall be installed and made serviceable prior to and during the time of construction.
- R20. Provide a water flow test. (Make arrangements at the Downtown Permit Center's walk-in counter: New City Hall, 3<sup>rd</sup> Flr, 915 I St. OR the North Permit Center's walk-in counter: 2101 Arena Blvd., Suite 200, Sacramento, CA 95834).
- R21. The furthest projection of the exterior wall of a building shall be accessible from within 150 ft of an approved Fire Department access road and water supply as measured by an unobstructed route around the exterior of the building. (CFC 902.2.1).
- R22. Provide appropriate Knox access for site.
- R23. Roads used for Fire Department access that are less than 28 feet in width shall be marked "No Parking Fire Lane" on both sides; roads less than 36 feet in width shall be marked on one side.
- R24. Locate and identify Fire Department Connections (FDCs) on address side of building no further than 40 feet and no closer than 5 feet from a fire hydrant.
- R25. Provide clear access to building openings, free of landscaping and other obstructions. Exterior doors and openings required by this code or the Building Code shall be maintained readily accessible for emergency access by the Fire Department. CFC 902.3.1.

#### *Utilities*

- R26. Any new domestic water services shall be metered. Per City Code 13.04.070, except for separate irrigation service connections and fire service connections, each lot or parcel shall only have one (1) metered domestic water service. Requests for multiple domestic water service connections to a single commercial lot or parcel, consistent with the Department of Utilities "Commercial Tap Policy", may be approved on a case-by-case basis by the Department of Utilities. Excess services shall be abandoned to the satisfaction of the Department of Utilities.
- R27. All water connections shall comply with the City of Sacramento's Cross Connection Control Policy.
- R28. Per City Code, the point of service for water, sewer and storm drain service is located at the back of curb for separated sidewalks and at the back of sidewalk

for attached sidewalks. The onsite water, sewer and storm drain systems shall be private systems maintained Owner.

- R29. An onsite surface drainage system is required and shall be connected to the street drainage system by means of a storm drain service tap. All onsite systems shall be designed to the standard for private storm drainage systems (per Section 11.12 of the Design and Procedures Manual).
- R30. A grading plan showing existing and proposed elevations is required. Adjacent off-site topography shall also be shown to the extent necessary to determine impacts to existing surface drainage paths. No grading shall occur until the grading plan has been reviewed and approved by the Department of Utilities.
- R31. The applicant must comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinances. This ordinance requires the applicant to show erosion and sediment control methods on the improvement plans. These plans shall also show the methods to control urban runoff pollution from the project site during construction.
- R32. This project is greater than 1 acre; therefore the project is required to comply with the "NPDES General Permit for Stormwater Discharges Associated with Construction Activity" (State Permit). To comply with the State Permit, the applicant will need to file a Notice of Intent (NOI) with the State Water Resources Control Board (SWRCB) and prepare a Stormwater Pollution Prevention Plan (SWPPP) prior to construction. A copy of the State Permit and NOI may be obtained from [www.swrcb.ca.gov/stormstr/construction.html](http://www.swrcb.ca.gov/stormstr/construction.html). The SWPPP will be reviewed by the Department of Utilities prior to issuing a grading permit. The following items shall be included in the SWPPP: (1) vicinity map, (2) site map, (3) list of potential pollutant sources, (4) type and location of erosion and sediment BMP's, (5) name and phone number of person responsible for SWPPP and (6) certification by property owner or authorized representative.
- R33. Post construction (permanent), stormwater quality control measures shall be incorporated into the development to minimize the increase of urban runoff pollution caused by development of the area. Since the project is in an area served by a regional water quality control facility only source control measures are required. Refer to the "Guidance Manual for On-Site Stormwater Quality Control Measures" dated January 2000 for appropriate source control measures.
- R34. This project is served by the Combined Sewer System (CSS). Therefore, the developer/property owner will be required to pay the Combined System Development Fee prior to the issuance of any building permit. The impact to the CSS due to the construction (at build-out) of an additional 70,000 square feet of medical office space is estimated to be 28 ESD. The Combined System fee at time of building permit is estimated to be \$10,524. The final Combined Sewer Fee will be calculated when the building permit is issued.

- R35. Foundation or basement dewatering discharges to the CSS and/or storm drainage system shall not be allowed. Foundations and basements shall be designed without the need for dewatering.
- R36. The project shall follow the “Green Guide for Healthcare” and the project’s adherence of to the set of the best practices shall be reviewed and approved by the Chief Building Official. The hospital project shall meet cool roof and cool paving standards. The cool paving standard shall apply for new parking lots and also for existing parking lots when they are resurfaced in the future.
- V. Variance:** The Variance to allow the private school to deviate from the required setbacks on H Street in the R-1 and R-3 and proposed R-1A zone is **approved** with the following conditions of approval:
- V1. The library element that projects into the setback area on H Street shall be limited to one story.
- W. Variance:** The Variance to waive the masonry wall between the new private school and a single family home is approved with the following conditions of approval:
- W1. The applicant shall work with the property owner to select the most appropriate type and style of wood fencing for the perimeter of the single family home site.

#### **ADVISORY NOTES:**

The following advisory notes are informational in nature:

- A. House move proposals will require additional review and approval.
- B. The applicant should make every effort to reuse the stained glass windows in the chapel to be demolished in the new heart center.
- C. If unusual amounts of bone, stone, or artifacts are uncovered, work within 50 meters of the area will cease immediately and a qualified archaeologist shall be consulted to develop, if necessary, further mitigation measures to reduce any archaeological impact to a less than significant effect before construction resumes. A note shall be placed on the final improvement plans referencing this condition.
- D. This project is served by the Combined Sewer System (CSS). Therefore, the developer/property owner will be required to pay the Combined System Development Fee prior to the issuance of any building permit. The impact to the CSS due to the construction (at build-out) of an additional 70,000 square feet of medical office space is estimated to be 28 ESD. The Combined System fee at time of building permit is estimated to be \$10,524. The final Combined Sewer

Fee will be calculated when the building permit is issued.

- E. Foundation or basement dewatering discharges to the CSS and/or storm drainage system shall not be allowed. Foundations and basements shall be designed without the need for dewatering.
- F. The proposed project is located in the Flood zone designated as an **X** and Shaded X zone on the Federal Emergency Management Agency (FEMA) Federal Insurance Rate Maps (FIRMs) that have been revised by a Letter of Map Revision effective February 18, 2005. Within the X and Shaded X zone, there are no requirements to elevate or flood proof.
- G. Special consideration should be given during the design phase of a development project to address the benefits derived from the urban forest by installing, whenever possible, large shade trees and thereby increasing the shade canopy cover on residential lots and streets. Trees in the urban environment reduce air and noise pollution, furnish habitat for wildlife, provide energy saving shade and cooling, enhance aesthetics and property values, and contribute to community image and quality of life.
- H. As per City Code, the applicant will be responsible to meet his/her obligations regarding the following:
  - a. Title 18, 18.44 Park Development Impact Fee (PIF), due at the time of issuance of building permit. The Park Development Impact Fee due for this project is estimated at \$93,257. This is based on 20 multi-family residential units at \$2,647 per unit, 123,350 square feet of new construction for the Heart Center at \$0.32 per square foot, and 2,640 square feet of South Wing Renovations at \$0.32 per square foot. Any change in these factors will change the amount of the PIF due. The fee is calculated using factors at the time that the project is submitted for building permit.
  - b. Community Facilities District 2002-02, Neighborhood Park Maintenance CFD Annexation.
- I. Many projects within the City of Sacramento require booster pumps for fire suppression and domestic water system. Prior to design of the subject project, the Department of Utilities suggests that the applicant request a water supply test to determine what pressure and flows the surrounding public water distribution system can provide to the site. This information can then be used to assist the engineers in the design of the fire suppression and domestic water systems.
- J. The proposed project is located in the Flood zone designated as an **X** and Shaded X zone on the Federal Emergency Management Agency (FEMA) Federal Insurance Rate Maps (FIRMs) that have been revised by a Letter of Map Revision

effective February 18, 2005. Within the X and Shaded X zone, there are no requirements to elevate or flood proof.

Exhibit 1A: Overall Site Plan

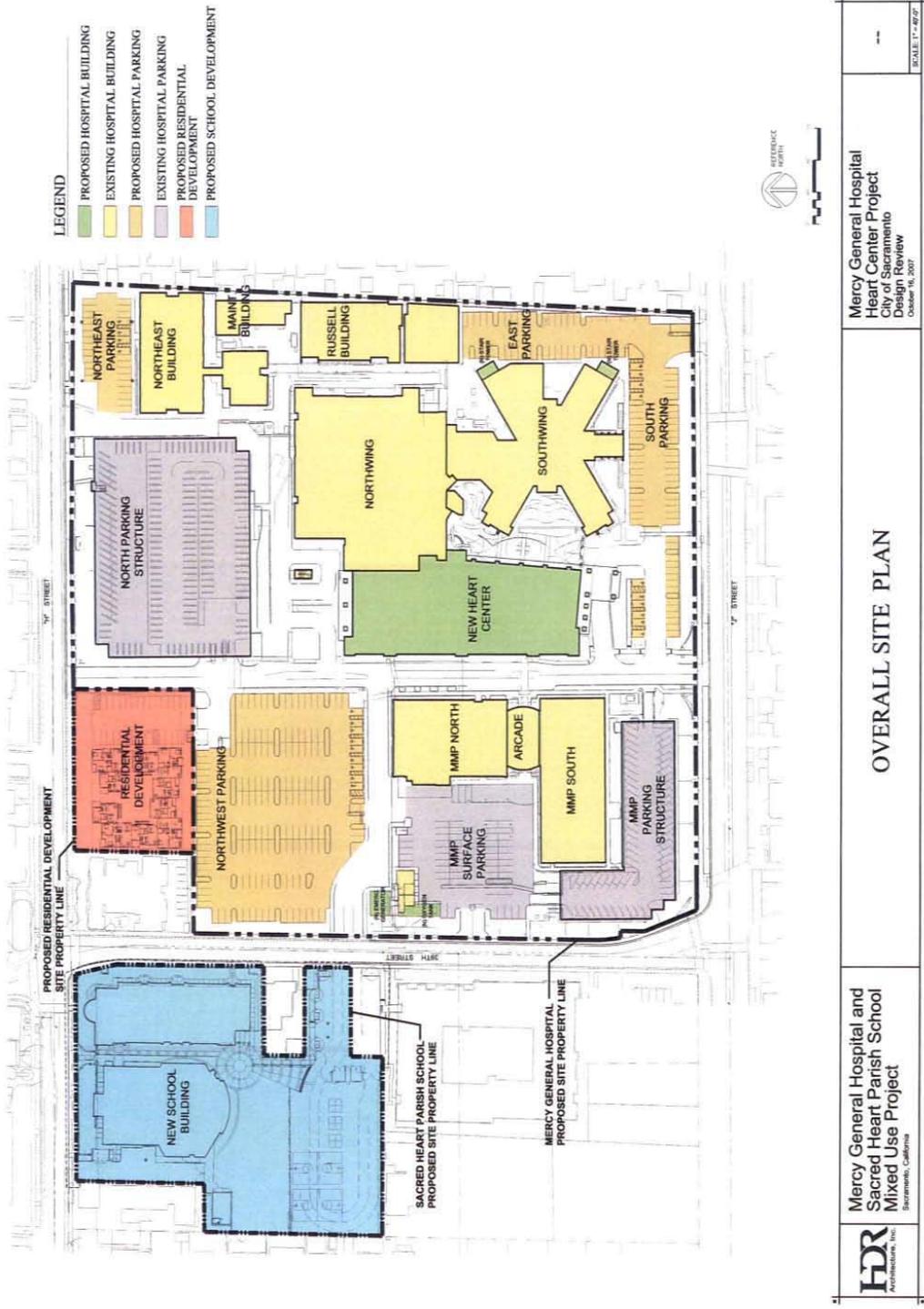


Exhibit 2A Heart Center Building Perspective (South/West)



VIEW LOOKING AT SOUTH / WEST ELEVATION

 HDR ARCHITECTURE, INC. Sacramento, California	Mercy General Hospital and Sacred Heart Parish School Mixed Use Project Sacramento, California	<b>BUILDING PERSPECTIVE</b>	Mercy General Hospital Heart Center Project City of Sacramento Design Review October 16, 2007	A.R-1 SCALE NONE
--	---	-----------------------------	---	---------------------

Exhibit 3A: Heart Center Building Perspective (South/East)



VIEW LOOKING AT SOUTH / EAST ELEVATION

 HDR ARCHITECTURE, INC. Sacramento, California		Mercy General Hospital and Sacred Heart Parish School Mixed Use Project Sacramento, California	Mercy General Hospital Heart Center Project City of Sacramento Design Review October 16, 2007	A.R-2 SCALE: NONE
--	--	---	--	----------------------

Exhibit 4A: Heart Center Building Perspective (North/West)



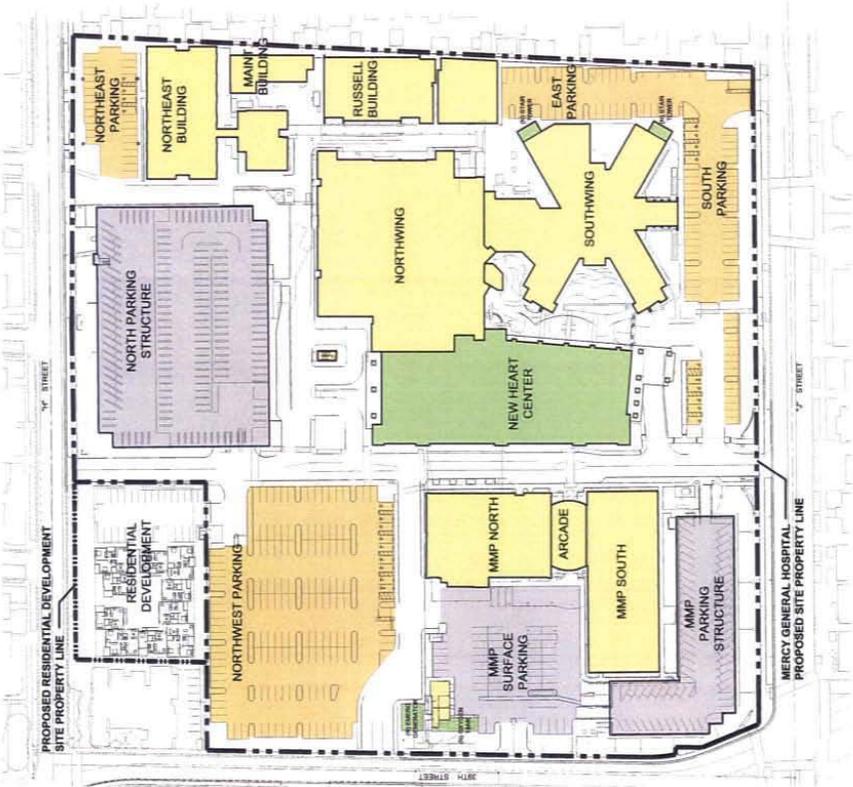
VIEW LOOKING AT NORTH / WEST ELEVATION

	<p>Mercy General Hospital and Sacred Heart Parish School Mixed Use Project Sacramento, California</p>	<p><b>BUILDING PERSPECTIVE</b></p>	<p>Mercy General Hospital Heart Center Project City of Sacramento Design Review October 11, 2007</p>	<p>A.R.-3 SCALE: NONE</p>
---	---	------------------------------------	--	-------------------------------

Exhibit 5A: Proposed Site Plan

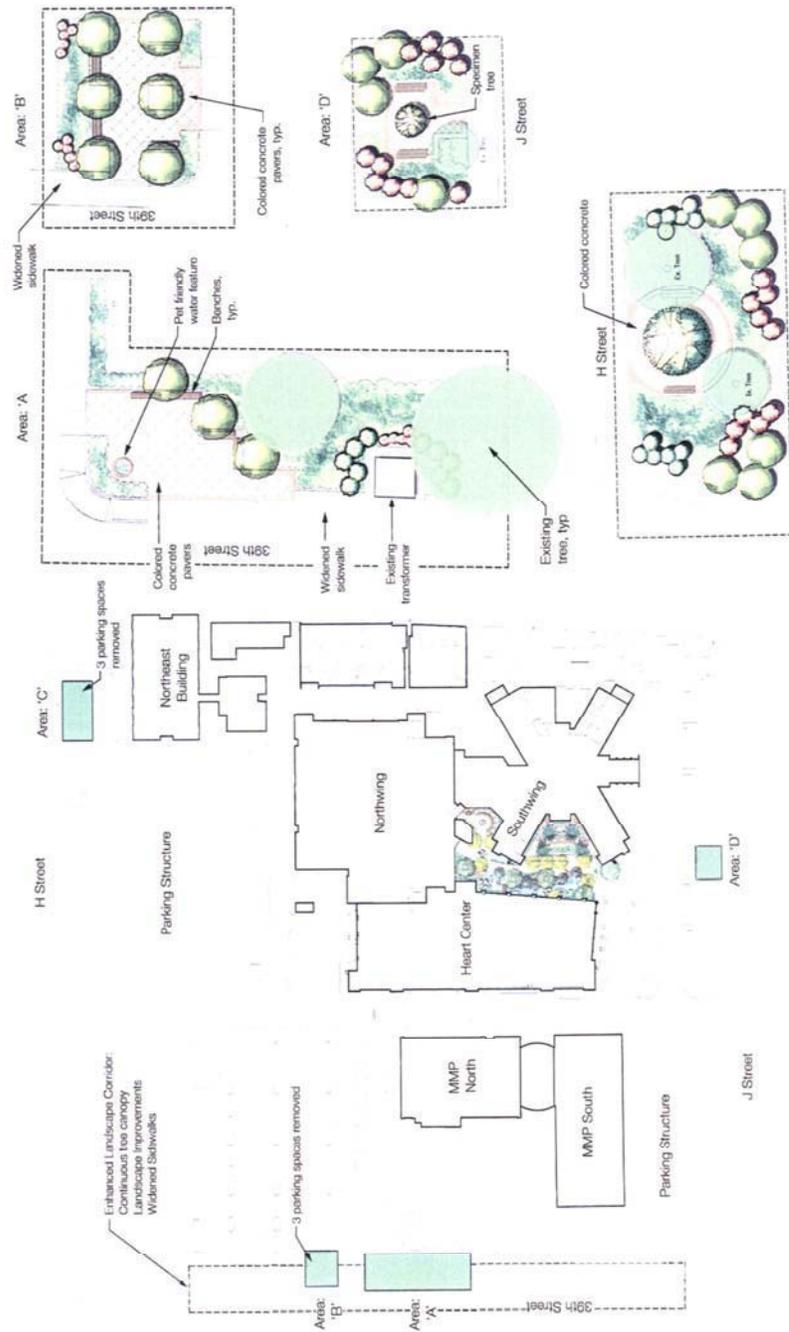
- PROJECT DATA**
1. EXISTING CAMPUS DESCRIPTION
  2. LOCATION: SACRAMENTO, CALIFORNIA
  3. CLIENT: SACRAMENTO COUNTY HEALTH SERVICES CENTER
  4. PROJECT TYPE: FULLY APPLICATED
  5. PROJECT AREA: 4.5000 ACRES
  6. PROJECT AREA: 4.5000 ACRES
  7. PROJECT AREA: 4.5000 ACRES
  8. PROJECT AREA: 4.5000 ACRES
  9. PROJECT AREA: 4.5000 ACRES
  10. PROJECT AREA: 4.5000 ACRES
  11. PROJECT AREA: 4.5000 ACRES
  12. PROJECT AREA: 4.5000 ACRES
  13. PROJECT AREA: 4.5000 ACRES
  14. PROJECT AREA: 4.5000 ACRES
  15. PROJECT AREA: 4.5000 ACRES
  16. PROJECT AREA: 4.5000 ACRES
  17. PROJECT AREA: 4.5000 ACRES
  18. PROJECT AREA: 4.5000 ACRES
  19. PROJECT AREA: 4.5000 ACRES
  20. PROJECT AREA: 4.5000 ACRES
  21. PROJECT AREA: 4.5000 ACRES
  22. PROJECT AREA: 4.5000 ACRES
  23. PROJECT AREA: 4.5000 ACRES
  24. PROJECT AREA: 4.5000 ACRES
  25. PROJECT AREA: 4.5000 ACRES
  26. PROJECT AREA: 4.5000 ACRES
  27. PROJECT AREA: 4.5000 ACRES
  28. PROJECT AREA: 4.5000 ACRES
  29. PROJECT AREA: 4.5000 ACRES
  30. PROJECT AREA: 4.5000 ACRES
  31. PROJECT AREA: 4.5000 ACRES
  32. PROJECT AREA: 4.5000 ACRES
  33. PROJECT AREA: 4.5000 ACRES
  34. PROJECT AREA: 4.5000 ACRES
  35. PROJECT AREA: 4.5000 ACRES
  36. PROJECT AREA: 4.5000 ACRES
  37. PROJECT AREA: 4.5000 ACRES
  38. PROJECT AREA: 4.5000 ACRES
  39. PROJECT AREA: 4.5000 ACRES
  40. PROJECT AREA: 4.5000 ACRES
  41. PROJECT AREA: 4.5000 ACRES
  42. PROJECT AREA: 4.5000 ACRES
  43. PROJECT AREA: 4.5000 ACRES
  44. PROJECT AREA: 4.5000 ACRES
  45. PROJECT AREA: 4.5000 ACRES
  46. PROJECT AREA: 4.5000 ACRES
  47. PROJECT AREA: 4.5000 ACRES
  48. PROJECT AREA: 4.5000 ACRES
  49. PROJECT AREA: 4.5000 ACRES
  50. PROJECT AREA: 4.5000 ACRES

- LEGEND**
- PROPOSED HOSPITAL BUILDING
  - EXISTING HOSPITAL BUILDING
  - PROPOSED HOSPITAL PARKING
  - EXISTING HOSPITAL PARKING



 <p>Mercy General Hospital and Sacred Heart Parish School Mixed Use Project Sacramento, California</p>	<p><b>SITE PLAN</b></p>	<p>Mercy General Hospital Heart Center Project City of Sacramento Design Review October 16, 2007</p>
AS-2		SCALE: 1" = 40' ±

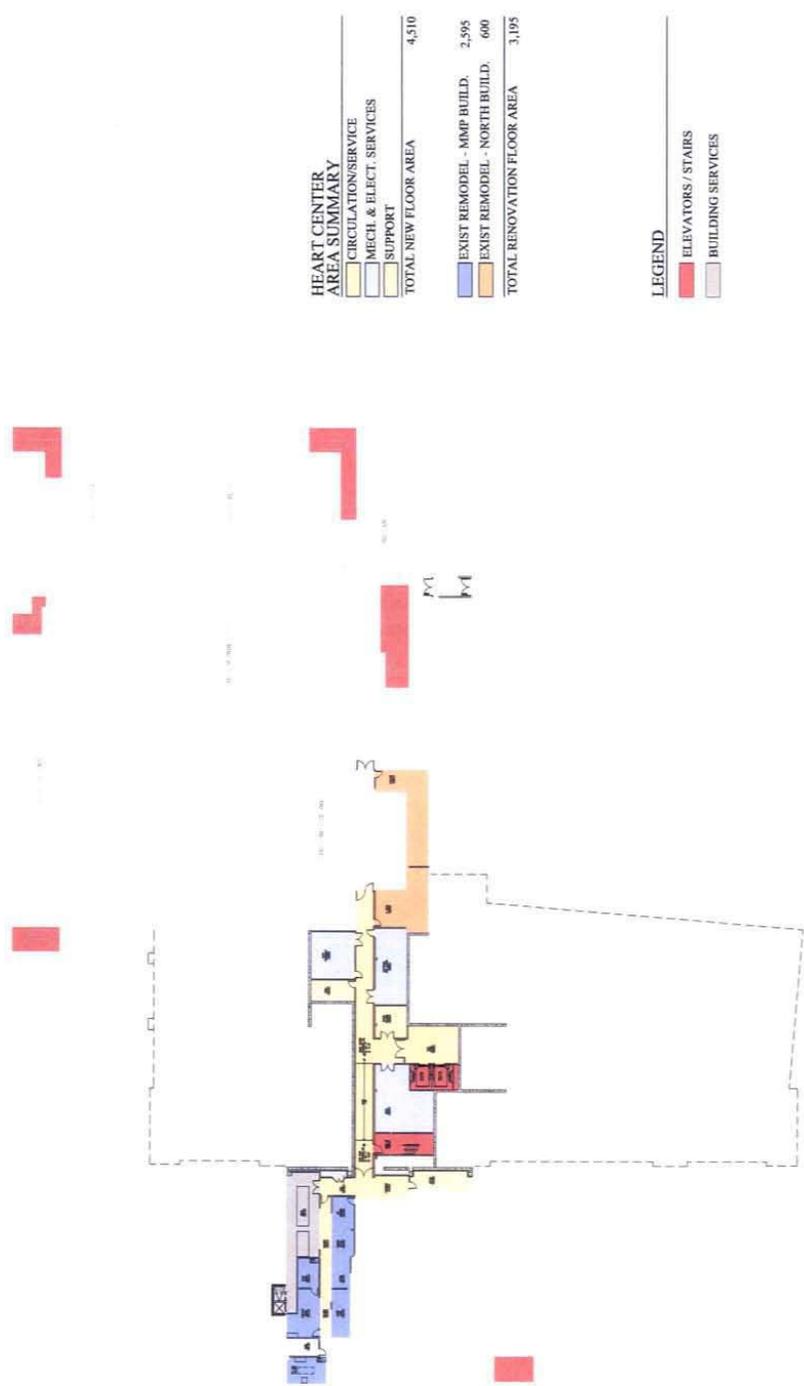
Exhibit 6A: Landscape Plan



**Mercy General Heart Center Project**  
 Conceptual Green Spaces  
 Sacramento, California



Exhibit 7A: Heart Center Basement Level



**HEART CENTER AREA SUMMARY**

CIRCULATION/SERVICE	2,595
MECH. & ELECT. SERVICES	600
SUPPORT	4,510
<b>TOTAL NEW FLOOR AREA</b>	<b>4,510</b>

EXIST REMODEL - MMP BUILD.	2,595
EXIST REMODEL - NORTH BUILD.	600
<b>TOTAL RENOVATION FLOOR AREA</b>	<b>3,195</b>

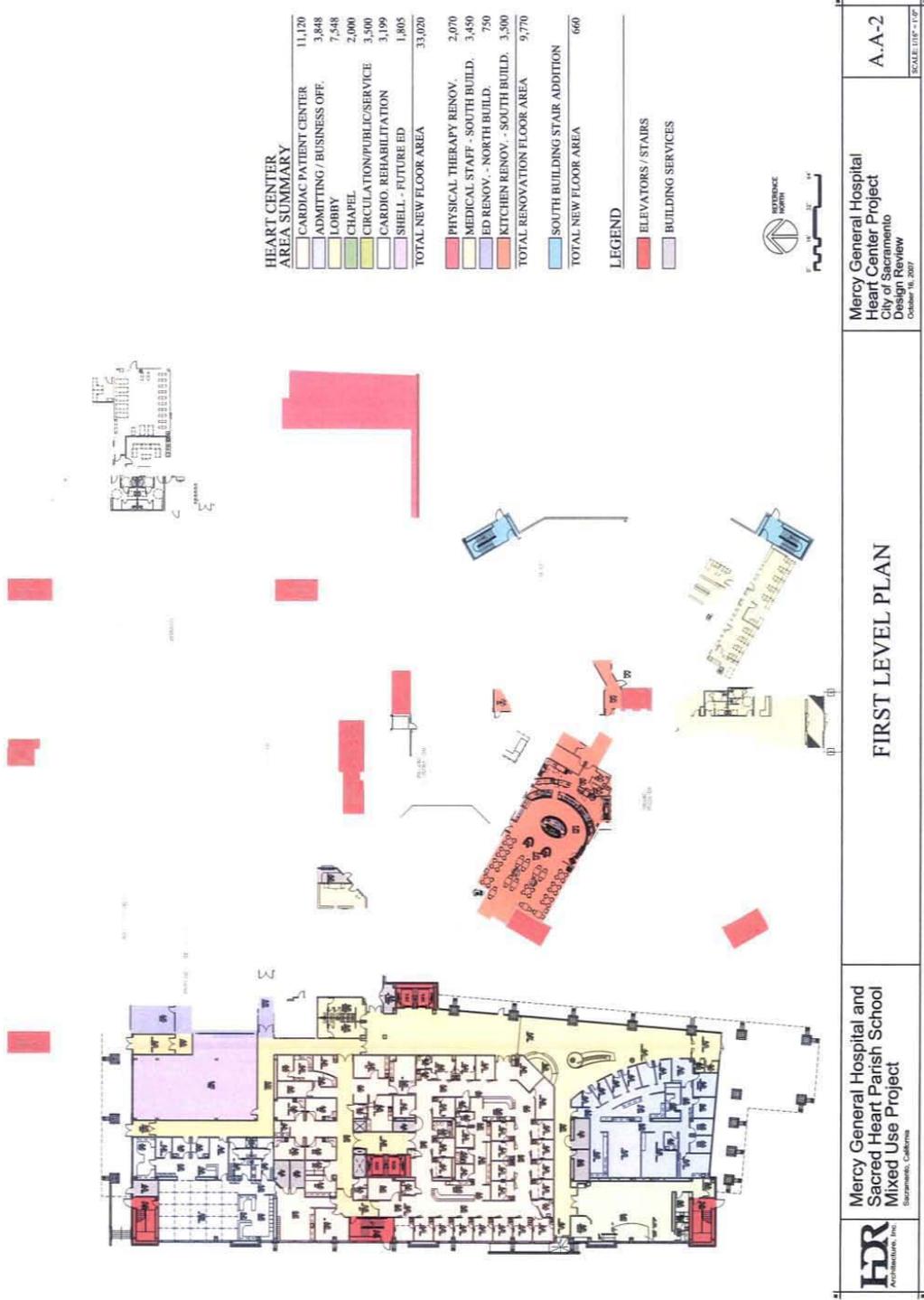
**LEGEND**

[Red Box]	ELEVATORS / STAIRS
[Grey Box]	BUILDING SERVICES



 <p><b>HDR</b> ARCHITECTURE, INC.</p>	<p>Mercy General Hospital and Sacred Heart Parish School Mixed Use Project Sacramento, California</p>	<p><b>BASEMENT LEVEL PLAN</b></p>	<p>A.A-1 SCALE: 1/8" = 1'-0"</p>
<p>Mercy General Hospital Heart Center Project City of Sacramento Design Review October 16, 2007</p>			

Exhibit 8A: Heart Center First Level



**HEART CENTER AREA SUMMARY**

CARDIAC PATIENT CENTER	11,120
ADMITTING / BUSINESS OFF.	3,848
LOBBY	7,548
CHAPEL	2,000
CIRCULATION/PUBLIC/SERVICE	3,500
CARDIO. REHABILITATION	3,199
SHELL - FUTURE ED	1,805
<b>TOTAL NEW FLOOR AREA</b>	<b>33,020</b>

PHYSICAL THERAPY RENOV.	2,070
MEDICAL STAFF - SOUTH BUILD.	3,450
ED RENOV. - NORTH BUILD.	750
KITCHEN RENOV. - SOUTH BUILD.	3,500
<b>TOTAL RENOVATION FLOOR AREA</b>	<b>9,770</b>

**LEGEND**

SOUTH BUILDING STAIR ADDITION	660
TOTAL NEW FLOOR AREA	
ELEVATORS / STAIRS	
BUILDING SERVICES	

	Mercy General Hospital and Sacred Heart Parish School Mixed Use Project Sacramento, California	Mercy General Hospital Heart Center Project City of Sacramento Design Review October 16, 2007	A.A-2 SCALE: 1/8" = 1'-0"
	<b>FIRST LEVEL PLAN</b>		

Exhibit 9A: Heart Center Second Level

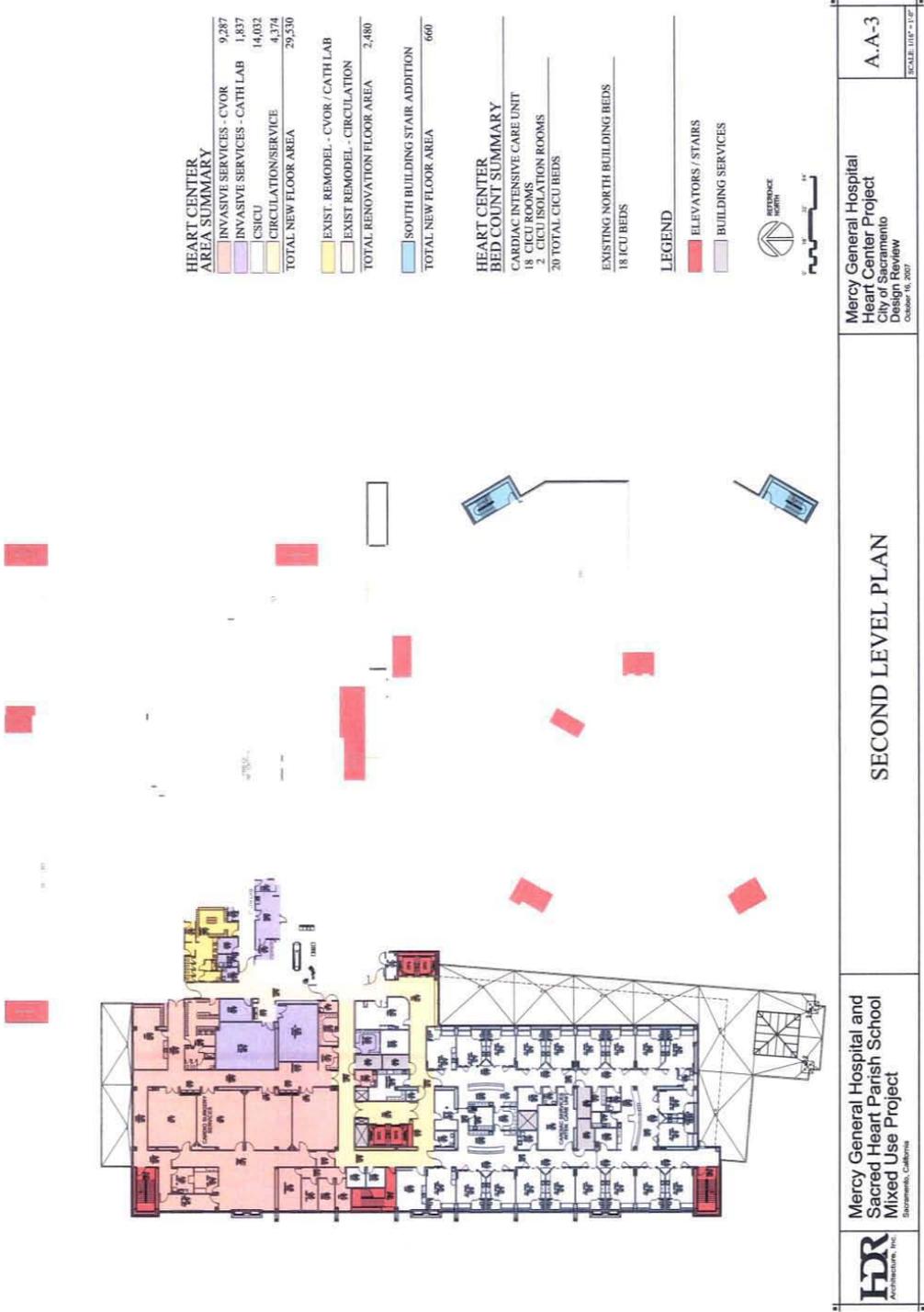


Exhibit 10A Heart Center Third Level

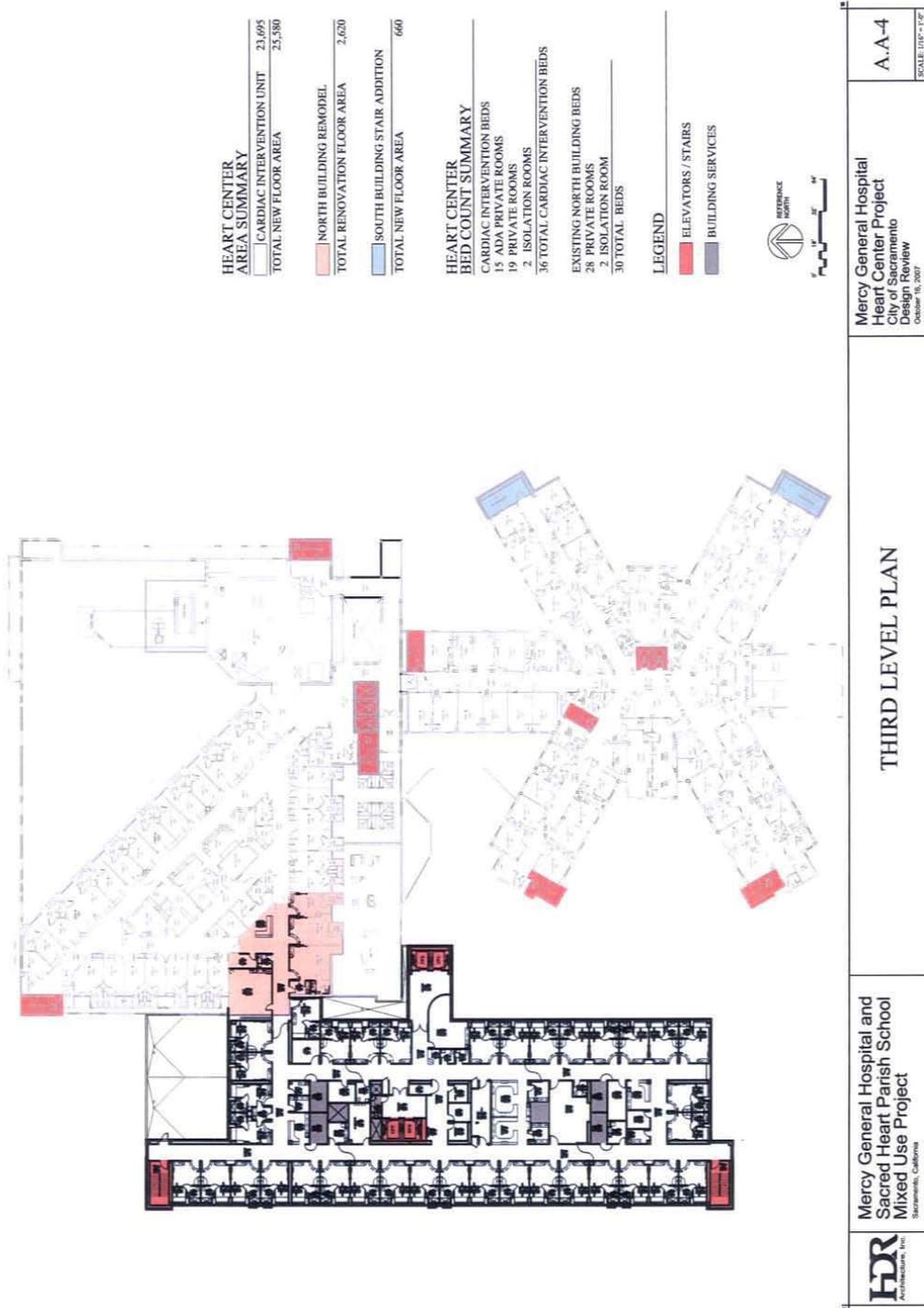


Exhibit 11A: Heart Center Fourth Level

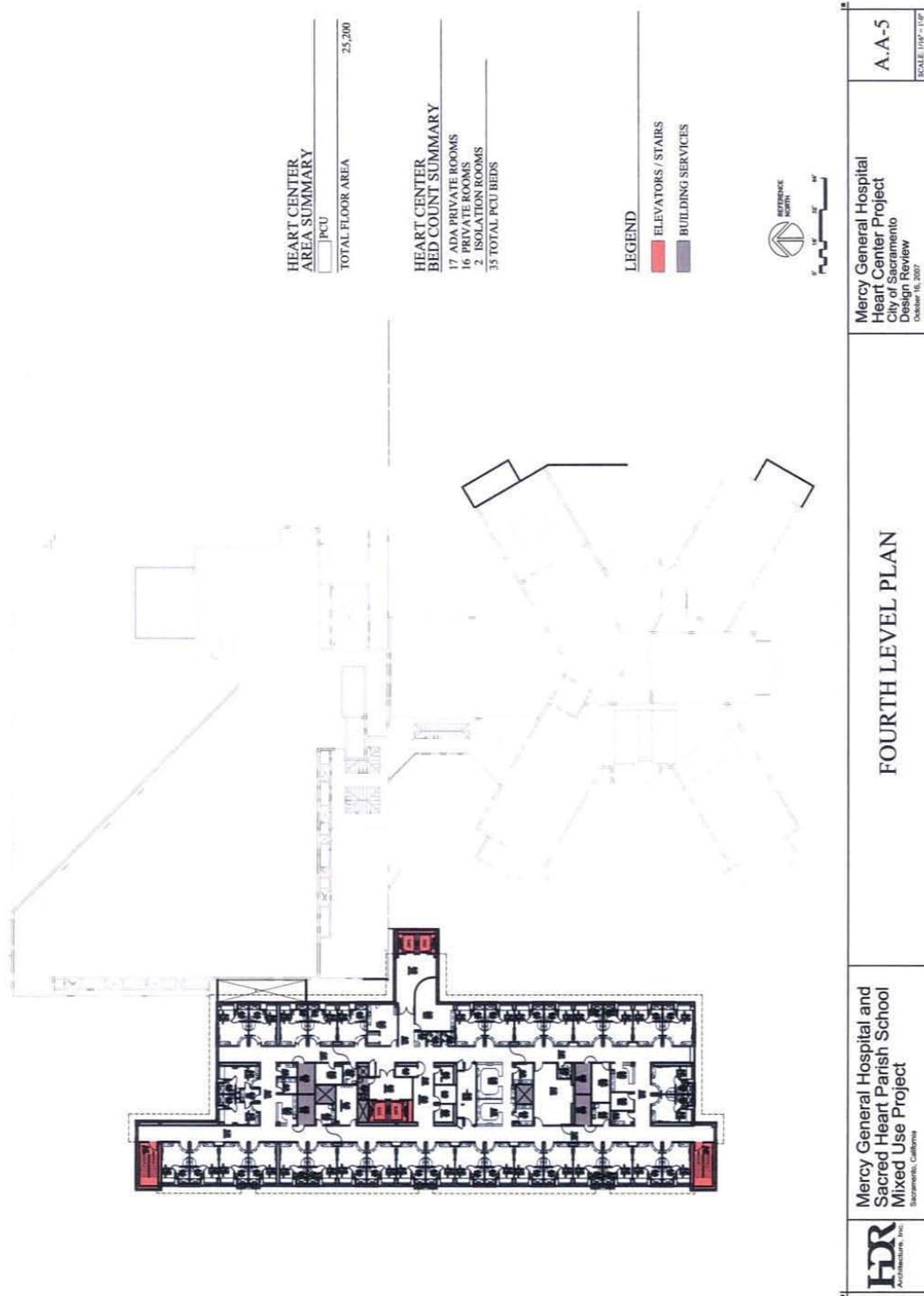


Exhibit 12A: Heart Center Penthouse Level

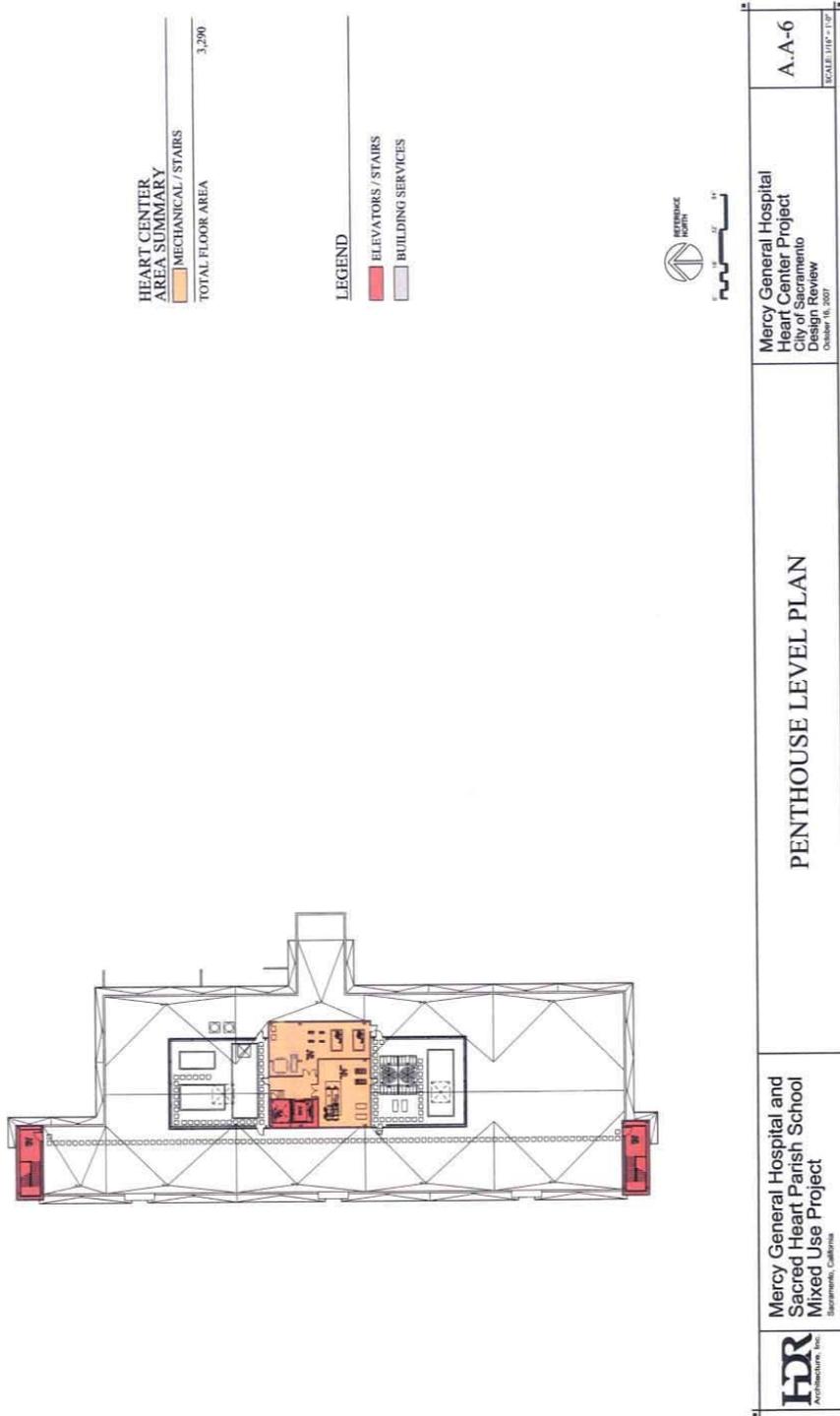




Exhibit 14A: Heart Center South/North Elevations

**01 - SOUTH ELEVATION**

**02 - NORTH ELEVATION**

**LEGEND**

- 1. BRICK (EXTERIOR) - SEE 101
- 2. BRICK (INTERIOR) - SEE 101
- 3. STONE (EXTERIOR) - SEE 101
- 4. STONE (INTERIOR) - SEE 101
- 5. CONCRETE (EXTERIOR) - SEE 101
- 6. CONCRETE (INTERIOR) - SEE 101
- 7. GLASS (EXTERIOR) - SEE 101
- 8. GLASS (INTERIOR) - SEE 101
- 9. METAL (EXTERIOR) - SEE 101
- 10. METAL (INTERIOR) - SEE 101
- 11. WOOD (EXTERIOR) - SEE 101
- 12. WOOD (INTERIOR) - SEE 101
- 13. PLASTER (EXTERIOR) - SEE 101
- 14. PLASTER (INTERIOR) - SEE 101
- 15. PAINT (EXTERIOR) - SEE 101
- 16. PAINT (INTERIOR) - SEE 101
- 17. TILE (EXTERIOR) - SEE 101
- 18. TILE (INTERIOR) - SEE 101
- 19. CARPET (EXTERIOR) - SEE 101
- 20. CARPET (INTERIOR) - SEE 101
- 21. FLOORING (EXTERIOR) - SEE 101
- 22. FLOORING (INTERIOR) - SEE 101
- 23. CEILING (EXTERIOR) - SEE 101
- 24. CEILING (INTERIOR) - SEE 101
- 25. ROOFING (EXTERIOR) - SEE 101
- 26. ROOFING (INTERIOR) - SEE 101
- 27. LIGHTING (EXTERIOR) - SEE 101
- 28. LIGHTING (INTERIOR) - SEE 101
- 29. MECHANICAL (EXTERIOR) - SEE 101
- 30. MECHANICAL (INTERIOR) - SEE 101
- 31. ELECTRICAL (EXTERIOR) - SEE 101
- 32. ELECTRICAL (INTERIOR) - SEE 101
- 33. FINISH (EXTERIOR) - SEE 101
- 34. FINISH (INTERIOR) - SEE 101
- 35. OTHER (EXTERIOR) - SEE 101
- 36. OTHER (INTERIOR) - SEE 101

**EXL. MATERIAL KEYNOTES**

1. BRICK (EXTERIOR) - SEE 101

2. BRICK (INTERIOR) - SEE 101

3. STONE (EXTERIOR) - SEE 101

4. STONE (INTERIOR) - SEE 101

5. CONCRETE (EXTERIOR) - SEE 101

6. CONCRETE (INTERIOR) - SEE 101

7. GLASS (EXTERIOR) - SEE 101

8. GLASS (INTERIOR) - SEE 101

9. METAL (EXTERIOR) - SEE 101

10. METAL (INTERIOR) - SEE 101

11. WOOD (EXTERIOR) - SEE 101

12. WOOD (INTERIOR) - SEE 101

13. PLASTER (EXTERIOR) - SEE 101

14. PLASTER (INTERIOR) - SEE 101

15. PAINT (EXTERIOR) - SEE 101

16. PAINT (INTERIOR) - SEE 101

17. TILE (EXTERIOR) - SEE 101

18. TILE (INTERIOR) - SEE 101

19. CARPET (EXTERIOR) - SEE 101

20. CARPET (INTERIOR) - SEE 101

21. FLOORING (EXTERIOR) - SEE 101

22. FLOORING (INTERIOR) - SEE 101

23. CEILING (EXTERIOR) - SEE 101

24. CEILING (INTERIOR) - SEE 101

25. ROOFING (EXTERIOR) - SEE 101

26. ROOFING (INTERIOR) - SEE 101

27. LIGHTING (EXTERIOR) - SEE 101

28. LIGHTING (INTERIOR) - SEE 101

29. MECHANICAL (EXTERIOR) - SEE 101

30. MECHANICAL (INTERIOR) - SEE 101

31. ELECTRICAL (EXTERIOR) - SEE 101

32. ELECTRICAL (INTERIOR) - SEE 101

33. FINISH (EXTERIOR) - SEE 101

34. FINISH (INTERIOR) - SEE 101

35. OTHER (EXTERIOR) - SEE 101

36. OTHER (INTERIOR) - SEE 101

**Mercy General Hospital and Sacred Heart Parish School Mixed Use Project**  
Sacramento, California

**HDR**  
Architecture, Inc.

**Mercy General Hospital  
Heart Center Project  
City of Sacramento  
Design Review**  
October 16, 2007

**BUILDING ELEVATIONS WITH MATERIAL NOTES**

**A.A-13**

SCALE: \_\_\_\_\_



Exhibit 16A: Oxygen Tank & Emergency Generator Elevation



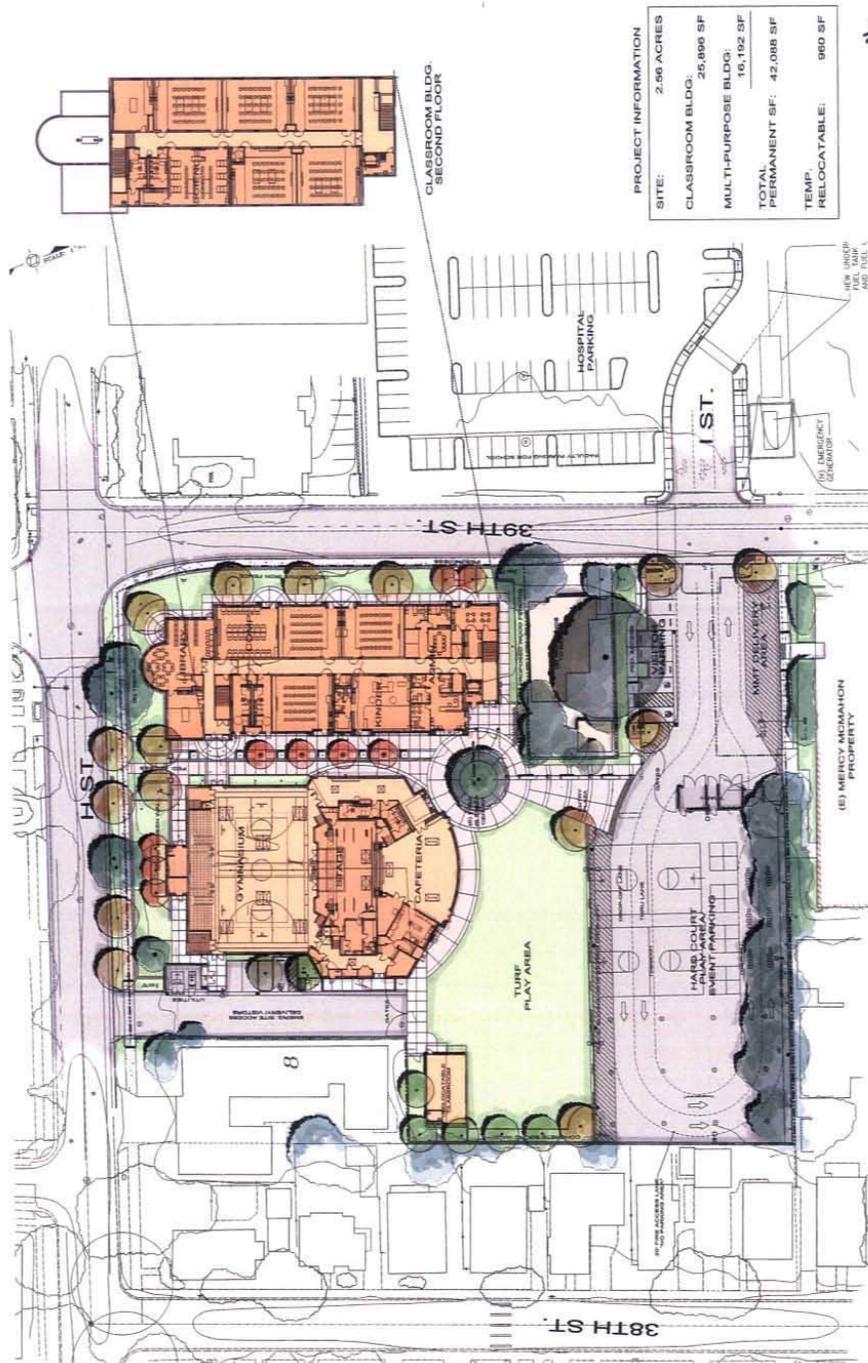
November 16, 2007

Mercy General Heart Center Project  
Corner Plaza Concept  
South East Corner of I & 39th Streets

The HLA Group Landscape Architects & Planners, Inc.  
10700 International Blvd., Suite 1000, San Antonio, TX 78241-1978  
Phone: (214) 740-0700 Fax: (214) 740-0701



Exhibit 1B: Overall Site Plan (Tree Removed)



**PROJECT INFORMATION**

SITE:	2.56 ACRES
CLASSROOM BLDG.:	25,898 SF
MULTI-PURPOSE BLDG.:	16,192 SF
TOTAL PERMANENT SF:	42,098 SF
TEMP. RELOCATABLE:	960 SF

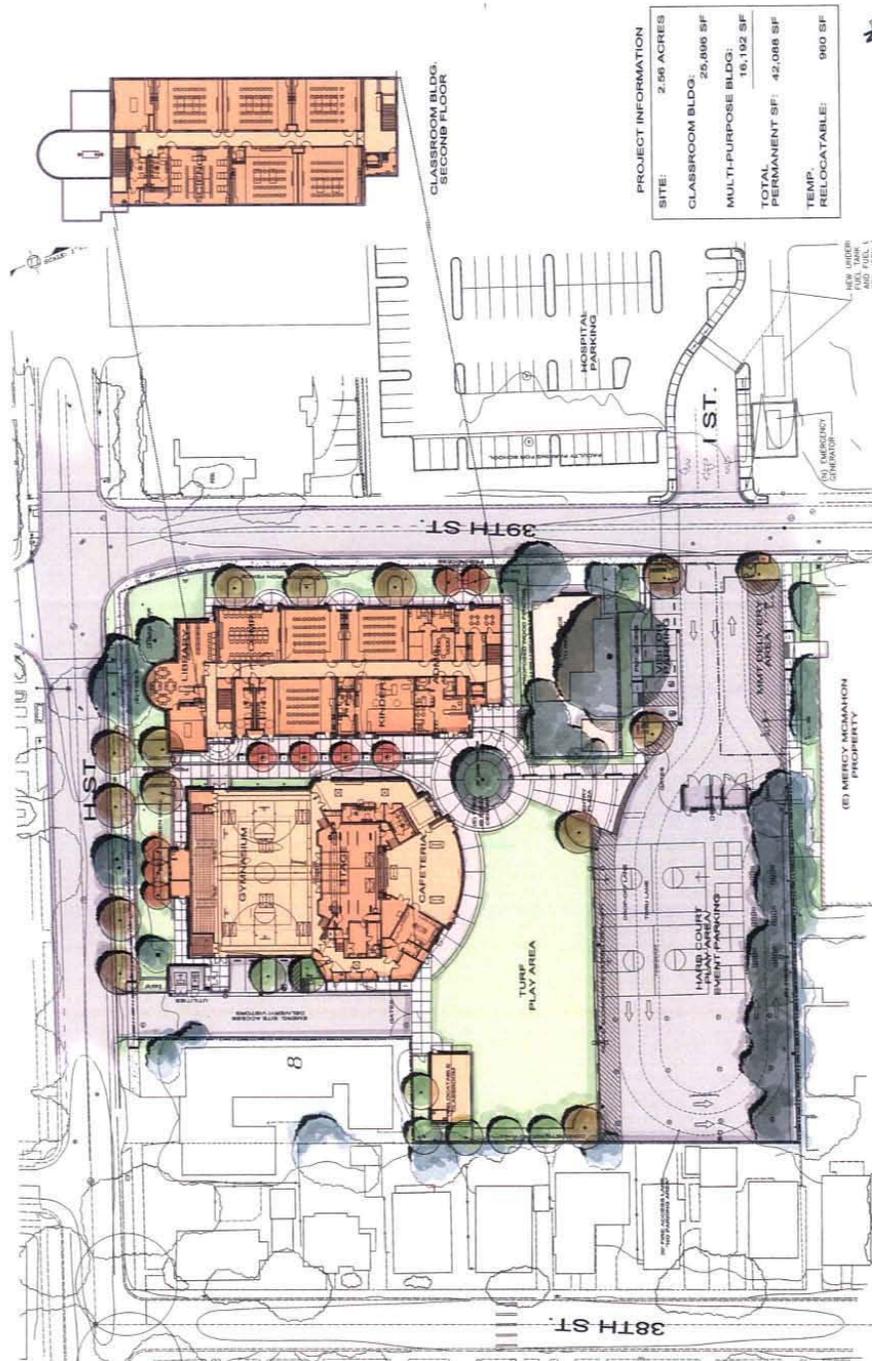


SCALE: 1" = 20'  
 010 766-8178  
 LEARNING ENVIRONMENTS GROUP  
 WILLIAMS + PADDON, ARCHITECTS + PLANNERS

**MERCY GENERAL HOSPITAL  
 AND SACRED HEART PARISH SCHOOL  
 MIXED USE PROJECT  
 SACRAMENTO, CALIFORNIA**

**B.A-1 - OVERALL SITE PLAN**  
 OCTOBER 8, 2007  
 CATHOLIC DIOCESE OF SACRAMENTO

Exhibit 2B: Overall Site Plan (Tree Preserved)



PROJECT INFORMATION	
SITE:	2.95 ACRES
CLASSROOM BLDG:	26,866 SF
MULTI-PURPOSE BLDG:	16,192 SF
TOTAL PERMANENT SF:	42,058 SF
TEMP. RELOCATABLE:	960 SF



SCALE: 1" = 20'  
 916.786-9178  
 LEARNING ENVIRONMENTS GROUP  
 WILLIAMS + PADDON, ARCHITECTS + PLANNERS

MERCY GENERAL HOSPITAL  
 AND SACRED HEART PARISH SCHOOL  
 MIXED USE PROJECT  
 SACRAMENTO, CALIFORNIA

B.A-1 - OVERALL SITE PLAN  
 (REVISED)  
 OCTOBER 2007  
 CATHOLIC DIOCESE OF SACRAMENTO

Exhibit 3B: Site Wall Plan

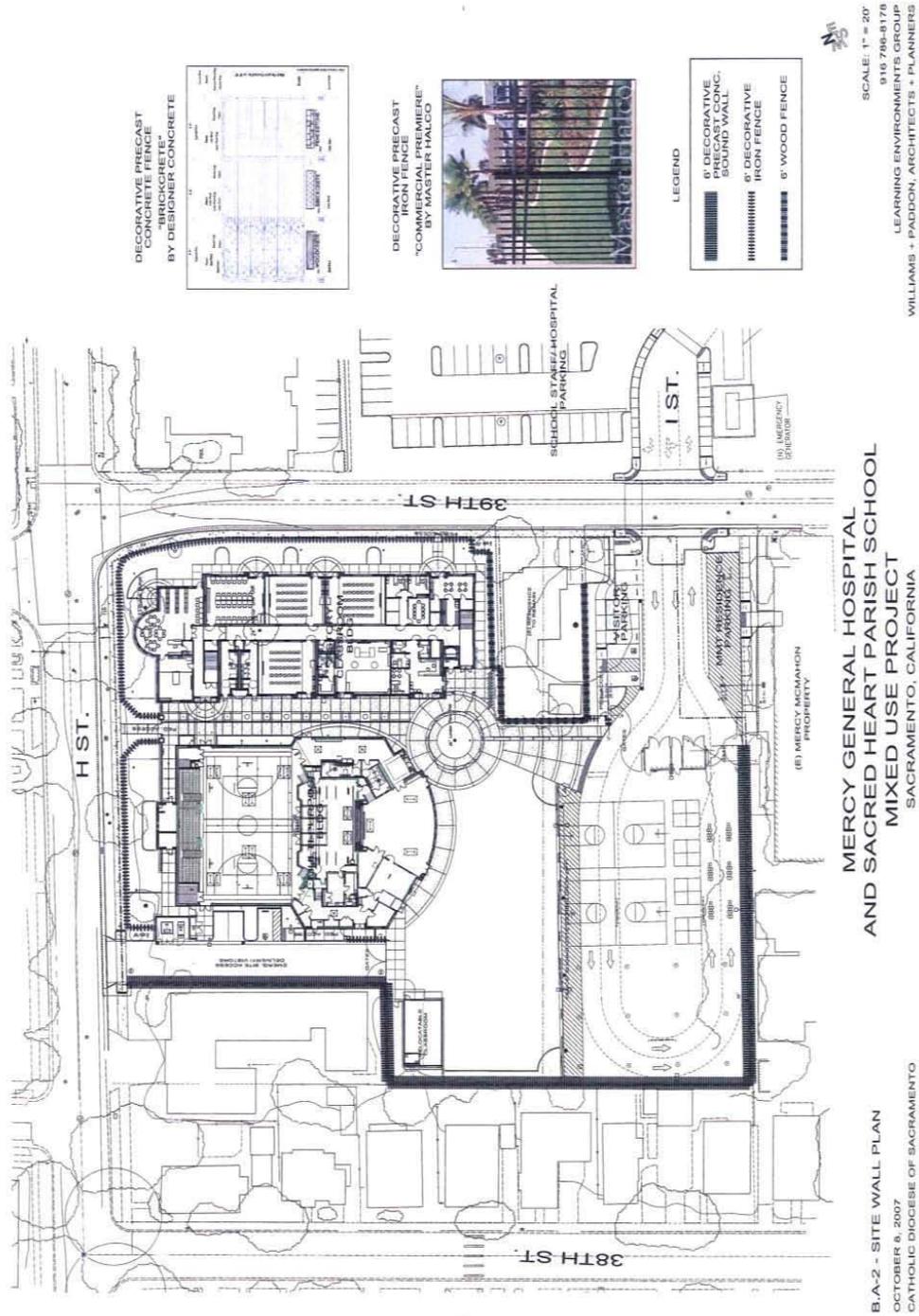
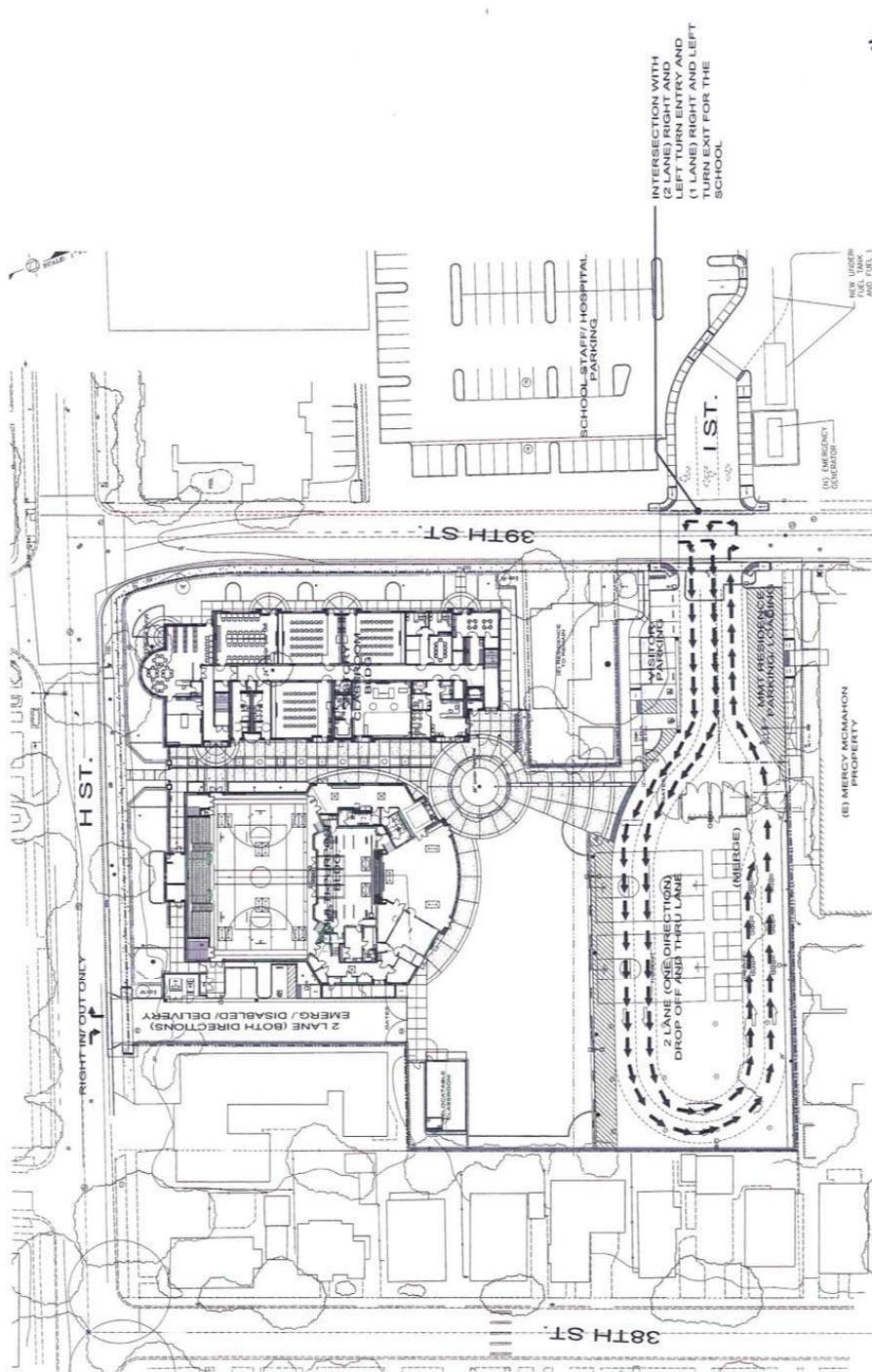


Exhibit 4B: Circulation Plan

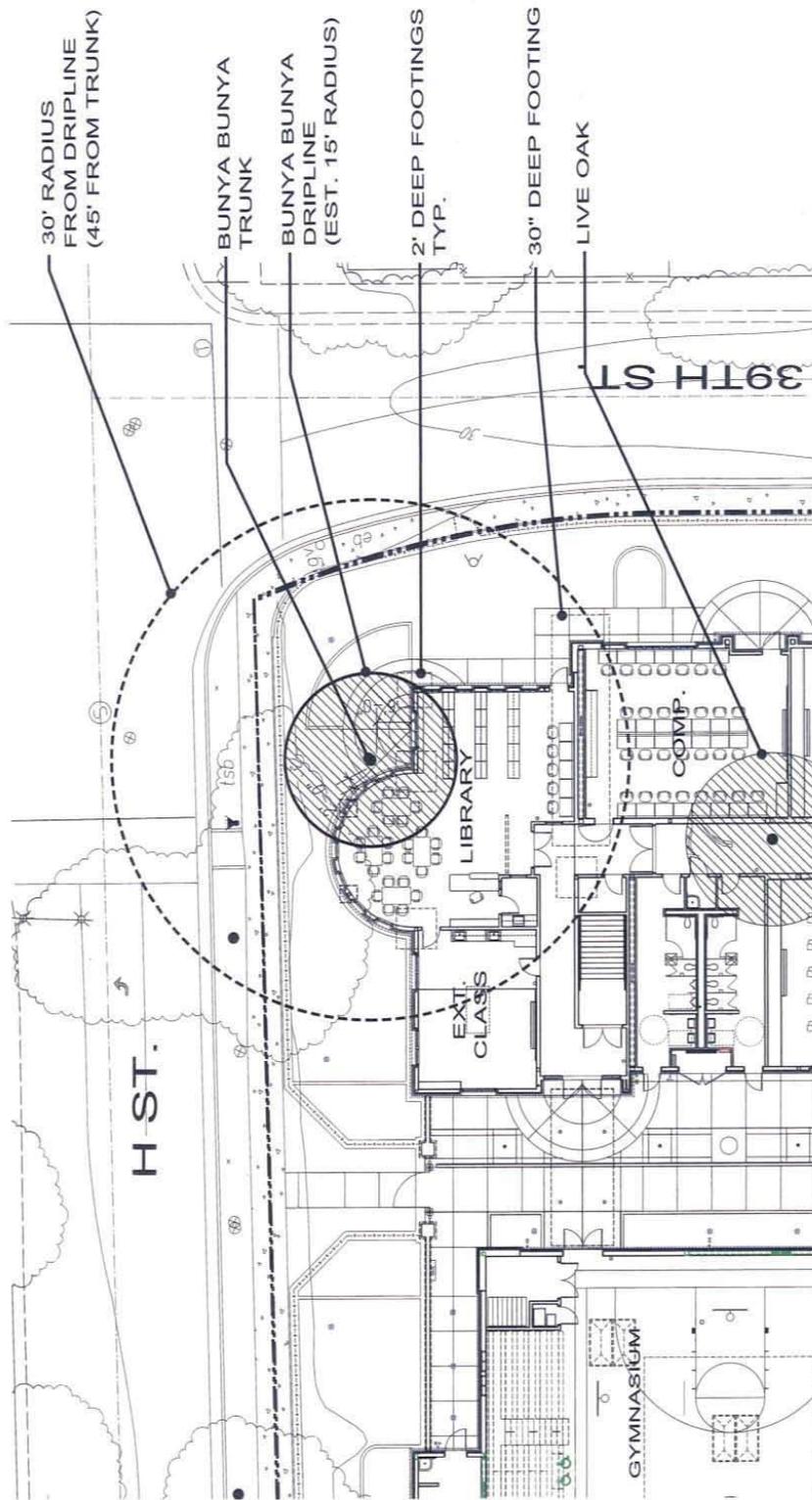


SCALE: 1" = 20'  
 916 786-8178  
 LEARNING ENVIRONMENTS GROUP  
 WILLIAMS + PADDON, ARCHITECTS + PLANNERS

MERCY GENERAL HOSPITAL  
 AND SACRED HEART PARISH SCHOOL  
 MIXED USE PROJECT  
 SACRAMENTO, CALIFORNIA

B.A-3 - CIRCULATION PLAN  
 OCTOBER 8, 2007  
 CATHOLIC DIOCESE OF SACRAMENTO

Exhibit 5B: Classroom Building (Tree Removed)

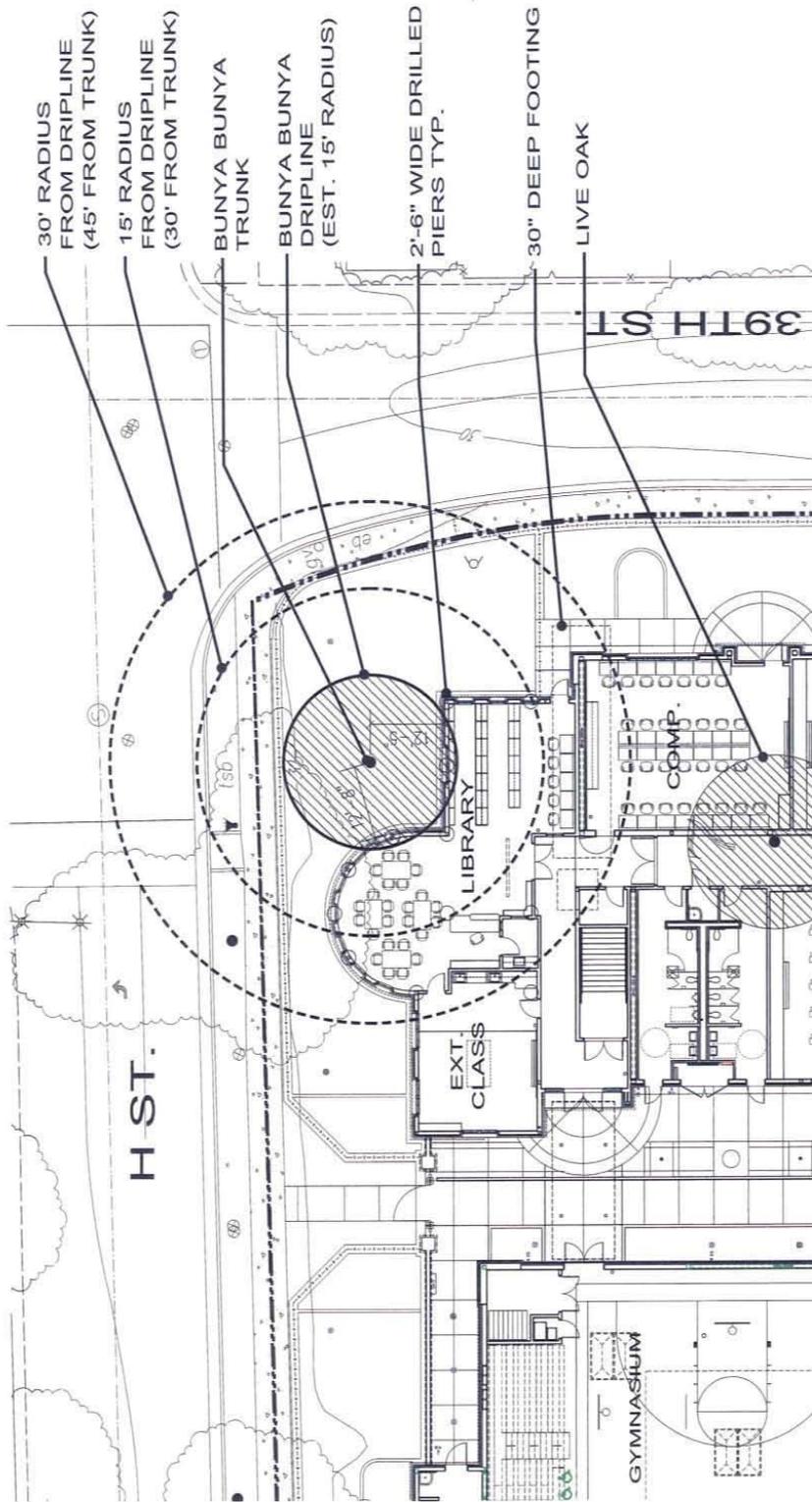


SCALE: 1/16" = 1'-0"  
 910.786-8178  
 LEARNING ENVIRONMENTS GROUP  
 WILLIAMS + PADDON, ARCHITECTS + PLANNERS

MERCY GENERAL HOSPITAL  
 AND SACRED HEART PARISH SCHOOL  
 MIXED USE PROJECT  
 SACRAMENTO, CALIFORNIA

CLASSROOM/LIBRARY WING  
 AS PROPOSED  
 OCTOBER 2, 2007  
 CATHOLIC DIOCESE OF SACRAMENTO

Exhibit 6B: Classroom Building (Tree Preserved)

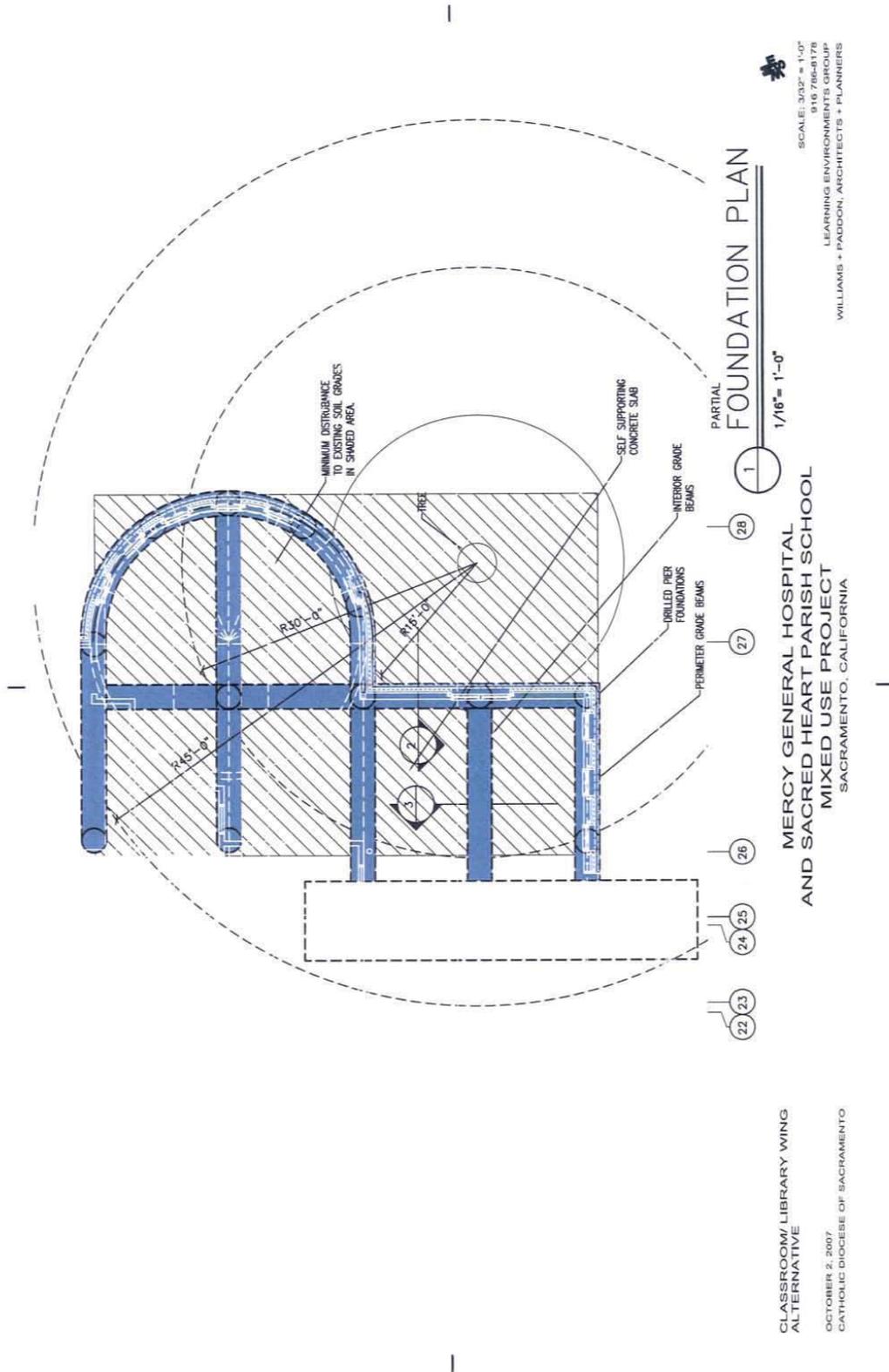


SCALE: 1/16" = 1'-0"  
 916 786-8178  
 LEARNING ENVIRONMENTS GROUP  
 WILLIAMS + PADDON, ARCHITECTS + PLANNERS

MERCY GENERAL HOSPITAL  
 AND SACRED HEART PARISH SCHOOL  
 MIXED USE PROJECT  
 SACRAMENTO, CALIFORNIA

CLASSROOM/ LIBRARY WING  
 ALTERNATIVE  
 OCTOBER 2, 2007  
 CATHOLIC DIOCESE OF SACRAMENTO

Exhibit 7B: Foundation Plan for Preservin Tree



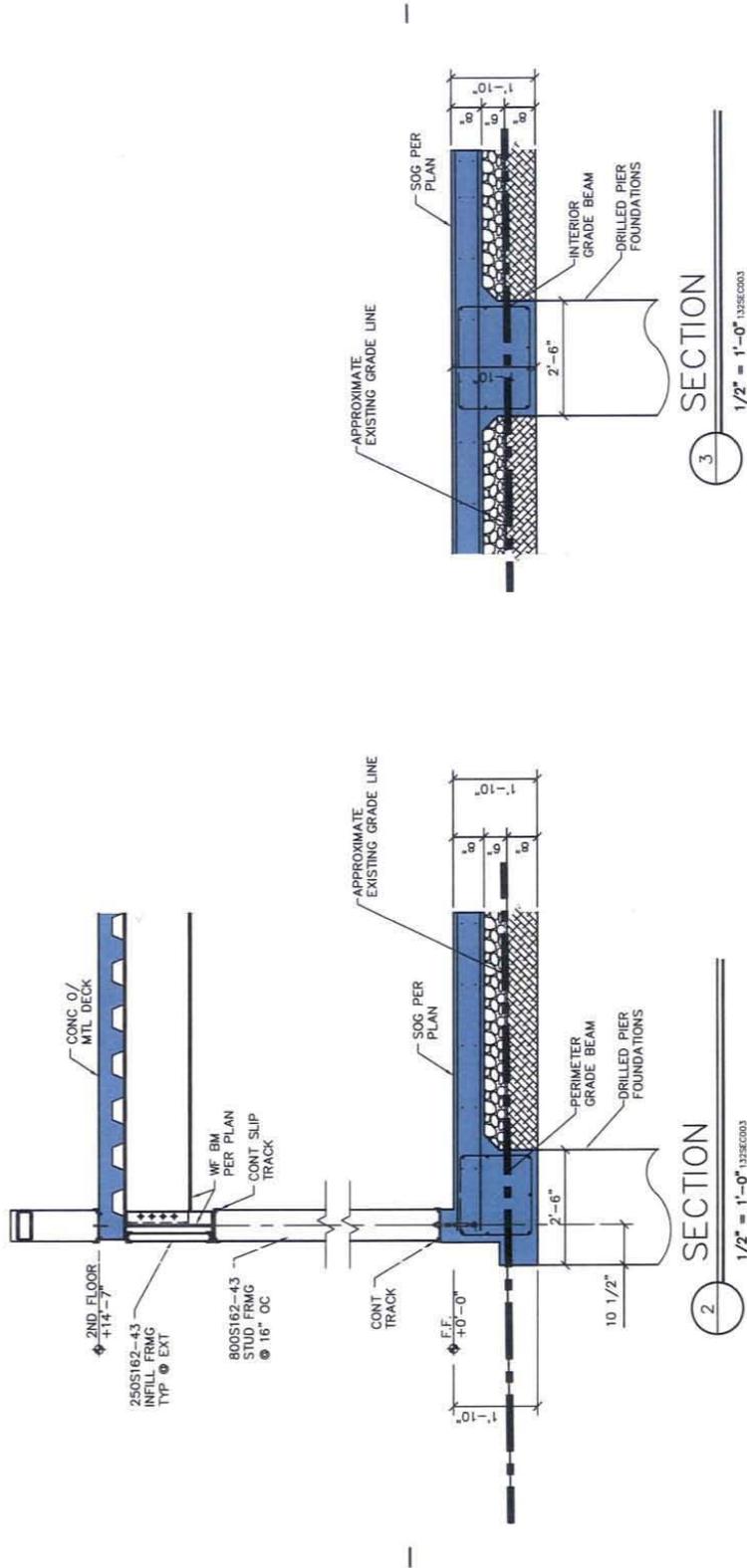
PARTIAL FOUNDATION PLAN  
1/16" = 1'-0"

SCALE: 3/32" = 1'-0"  
1/16" = 1'-0"  
LEARNING ENVIRONMENTS GROUP  
WILLIAMS + PADDON, ARCHITECTS + PLANNERS

MERCY GENERAL HOSPITAL  
AND SACRED HEART PARISH SCHOOL  
MIXED USE PROJECT  
SACRAMENTO, CALIFORNIA

CLASSROOM/LIBRARY WING  
ALTERNATIVE  
OCTOBER 2, 2007  
CATHOLIC DIOCESE OF SACRAMENTO

Exhibit 8B: Foundation Sections for Preserving Tree

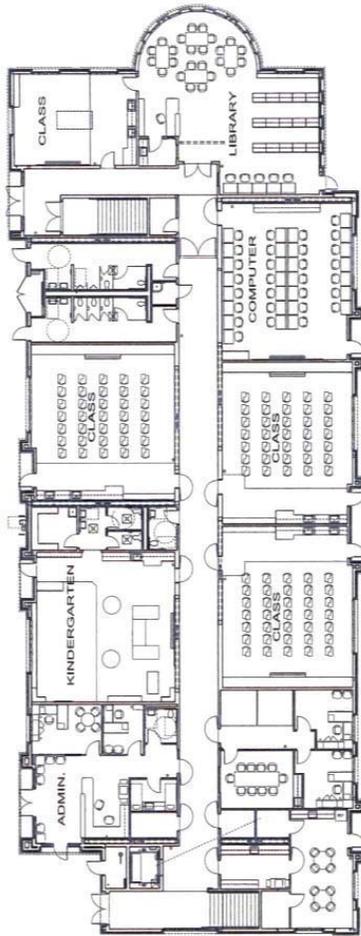


MERCY GENERAL HOSPITAL  
AND SACRED HEART PARISH SCHOOL  
MIXED USE PROJECT  
SACRAMENTO, CALIFORNIA

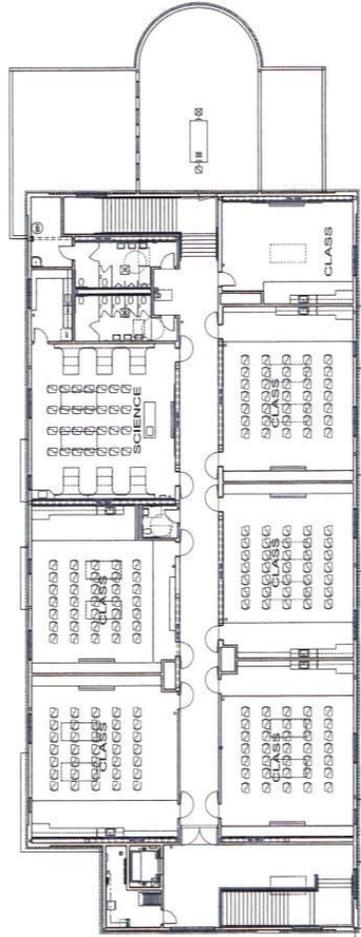
CLASSROOM/ LIBRARY WING  
ALTERNATIVE  
OCTOBER 2, 2007  
CATHOLIC DIOCESE OF SACRAMENTO

015 786-8178  
LEARNING ENVIRONMENTS GROUP  
WILLIAMS + PADDON, ARCHITECTS + PLANNERS

Exhibit 9B: Classroom Building Plans



FIRST FLOOR PLAN



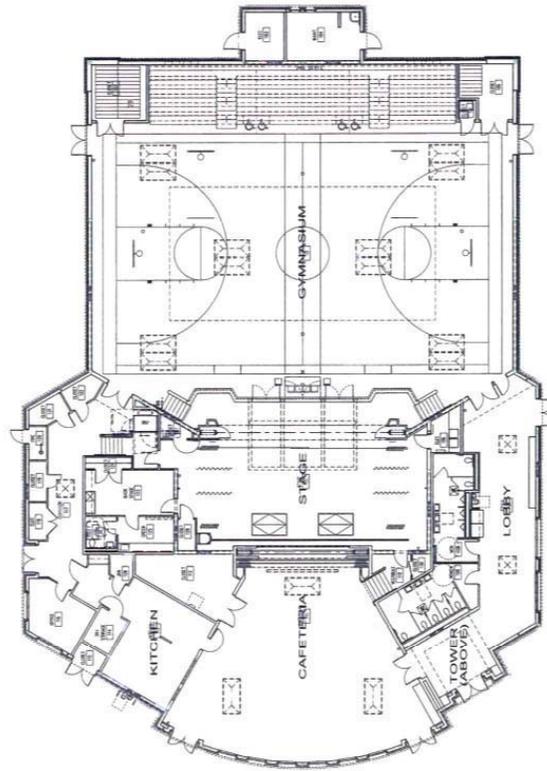
SECOND FLOOR PLAN

MERCY GENERAL HOSPITAL  
 AND SACRED HEART PARISH SCHOOL  
 MIXED USE PROJECT  
 SACRAMENTO, CALIFORNIA

B.A-6 - CLASSROOM BLDG PLANS  
 OCTOBER 8, 2007  
 CATHOLIC DIOCESE OF SACRAMENTO

SCALE: 1/8"=1'-0"  
 915 786-8178  
 LEARNING ENVIRONMENTS GROUP  
 WILLIAMS + PADDON, ARCHITECTS + PLANNERS

Exhibit 10B: Multipurpose Room Floor Plan

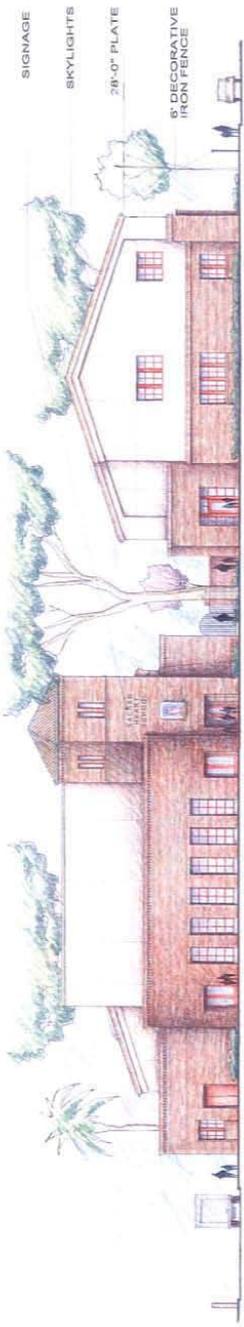


  
SCALE: 1/8"=1'-0"  
916 766-8176  
LEARNING ENVIRONMENTS GROUP  
WILLIAMS + PADDON, ARCHITECTS + PLANNERS

**SACRED HEART PARISH SCHOOL**  
CONCEPTUAL PLANS  
SACRAMENTO, CALIFORNIA

B.A-7 - MULTI-PURPOSE BLDG PLAN  
OCTOBER 6, 2007  
CATHOLIC DIOCESE OF SACRAMENTO

Exhibit 11B: SHPS Elevations (North, South, East)



B.A-9 - BUILDING ELEVATIONS  
 FEBRUARY 08, 2007  
 CATHOLIC DIOCESE OF SACRAMENTO

CLASSROOM BLDG EAST ELEVATION (39TH ST.)

**MERCY GENERAL HOSPITAL  
 AND SACRED HEART PARISH SCHOOL  
 MIXED USE PROJECT**  
 SACRAMENTO, CALIFORNIA

916 766-8178  
 LEARNING ENVIRONMENTS GROUP  
 WILLIAMS + PADDON, ARCHITECTS + PLANNERS

Exhibit 12B: SHPS Elevations (East and West)



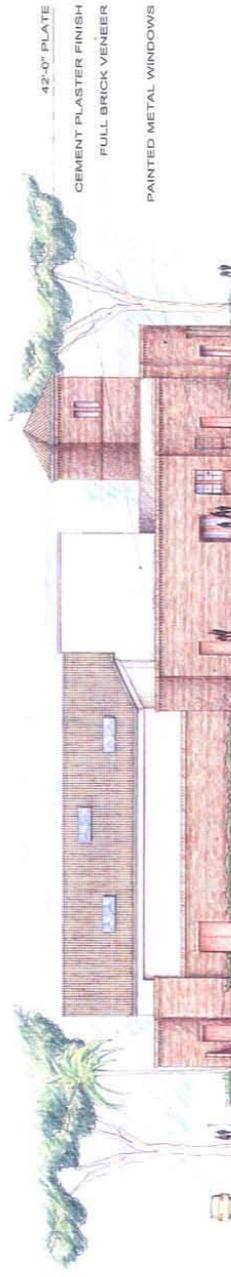
MECH. ROOF WELLS  
(BARRELL TILE ROOF)  
CEMENT PLASTER FINISH  
PAINTED METAL WINDOWS  
FULL BRICK VENEER

CLASSROOM BLDG EAST ELEVATION (FACING QUAD AREA)



SKYLIGHTS  
CONCRETE TILE ROOF  
FULL BRICK VENEER  
19'-0" PARAPET

MULTI-PURPOSE BLDG EAST ELEVATION (FACING QUAD AREA)



32'-0" PLATE  
CEMENT PLASTER FINISH  
FULL BRICK VENEER  
PAINTED METAL WINDOWS

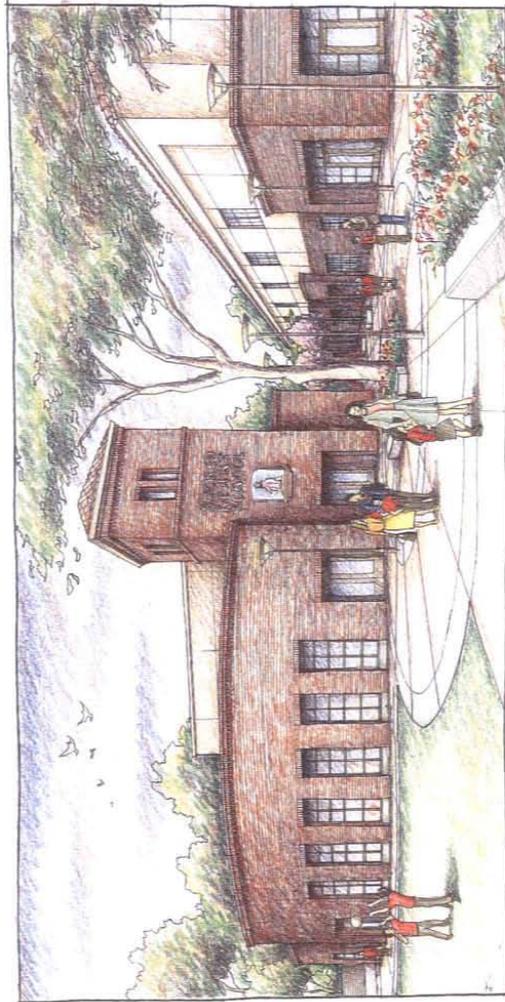
MULTI-PURPOSE BLDG WEST ELEVATION

**MERCY GENERAL HOSPITAL  
AND SACRED HEART PARISH SCHOOL  
MIXED USE PROJECT**  
SACRAMENTO, CALIFORNIA

B.A-10 - BUILDING ELEVATIONS  
FEBRUARY 08, 2007  
CATHOLIC DIOCESE OF SACRAMENTO

916 786-8176  
LEARNING ENVIRONMENTS GROUP  
WILLIAMS + PADDON, ARCHITECTS + PLANNERS

Exhibit 13B: View of Campus Entry Perspective



VIEW OF THE CAMPUS ENTRY

MERCY GENERAL HOSPITAL  
AND SACRED HEART PARISH SCHOOL  
MIXED USE PROJECT  
SACRAMENTO, CALIFORNIA

B.A-11 - VIEW OF THE CAMPUS ENTRY  
FEBRUARY 06, 2007  
CATHOLIC DIOCESE OF SACRAMENTO

916.705.8176  
LEARNING ENVIRONMENTS GROUP  
WILLIAMS + PADDON, ARCHITECTS + PLANNERS



Exhibit 1C: Perspective of Housing Option 1 and 2



**INDEX OF DRAWINGS**

- C-A-0 COVER / PERSPECTIVES / INDEX OF DRAWINGS
- C-A-1 FLOOR PLANS
- C-A-2 FLOOR PLANS
- C-A-3 OPTION 1 - PERSPECTIVE
- C-A-4 OPTION 1 - EXTERIOR ELEVATIONS
- C-A-5 OPTION 2 - PERSPECTIVE
- C-A-6 OPTION 2 - EXTERIOR ELEVATIONS
- C-A-7 CONTEXTUAL ELEVATIONS
- C-A-8 CONTEXTUAL IMAGES
- CL-1 LANDSCAPE SITE PLAN



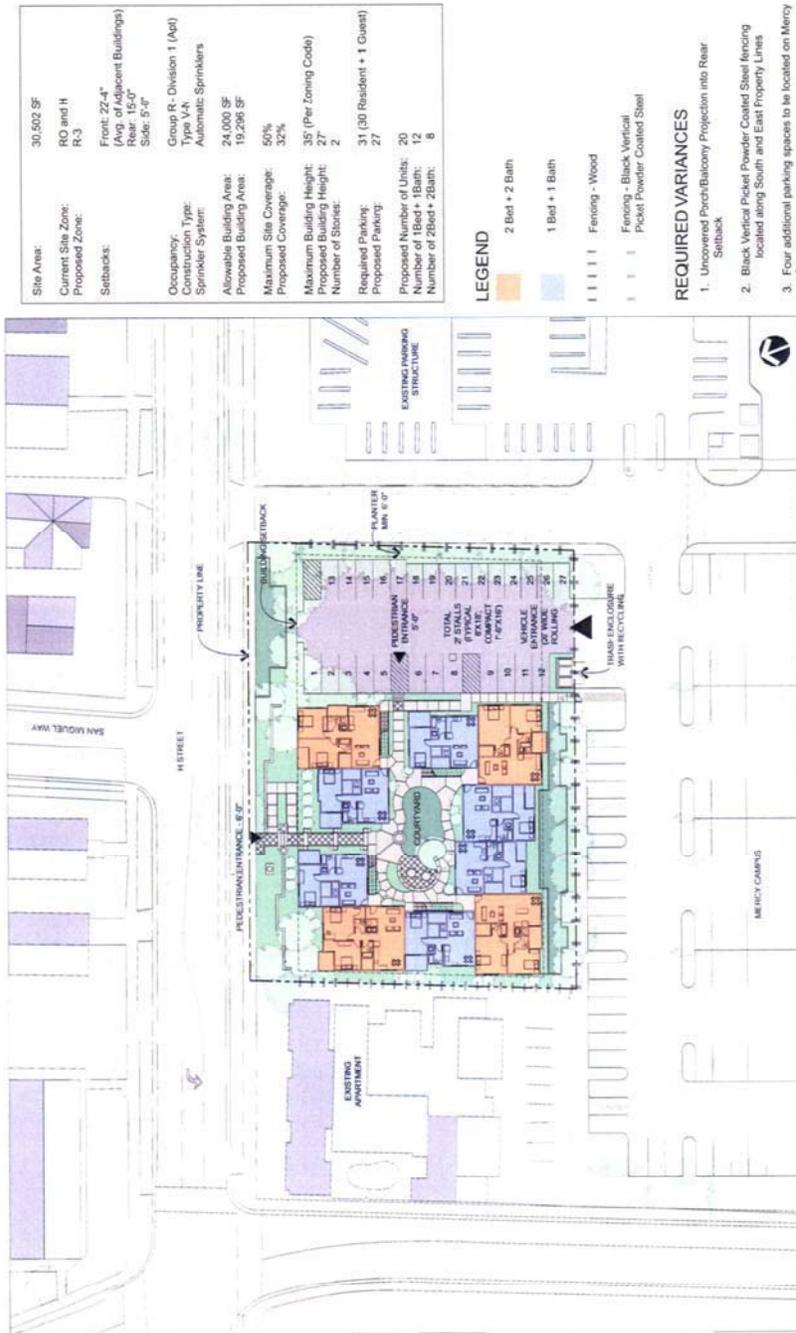


**Schematic Design**  
 Residential Development  
 November 23, 2007 - City Council Hearing

**Mercy General Hospital and Sacred Heart Parish School Mixed Use Project**  
 Sacramento, California

C.A-0

Exhibit 2C: Site Plan



SITE PLAN | 1/16"=1'-0"



C.A-1

Schematic Design  
Residential Development  
November 25, 2007 City Council Hearing

Mercy General Hospital and Sacred Heart Parish School Mixed Use Project  
Sacramento, California

Exhibit 3C: Floor Plans



LEGEND

- Public - Living, Dining, Outdoor Space
- Private - Bedroom Accessory - Utility, Storage
- Bath

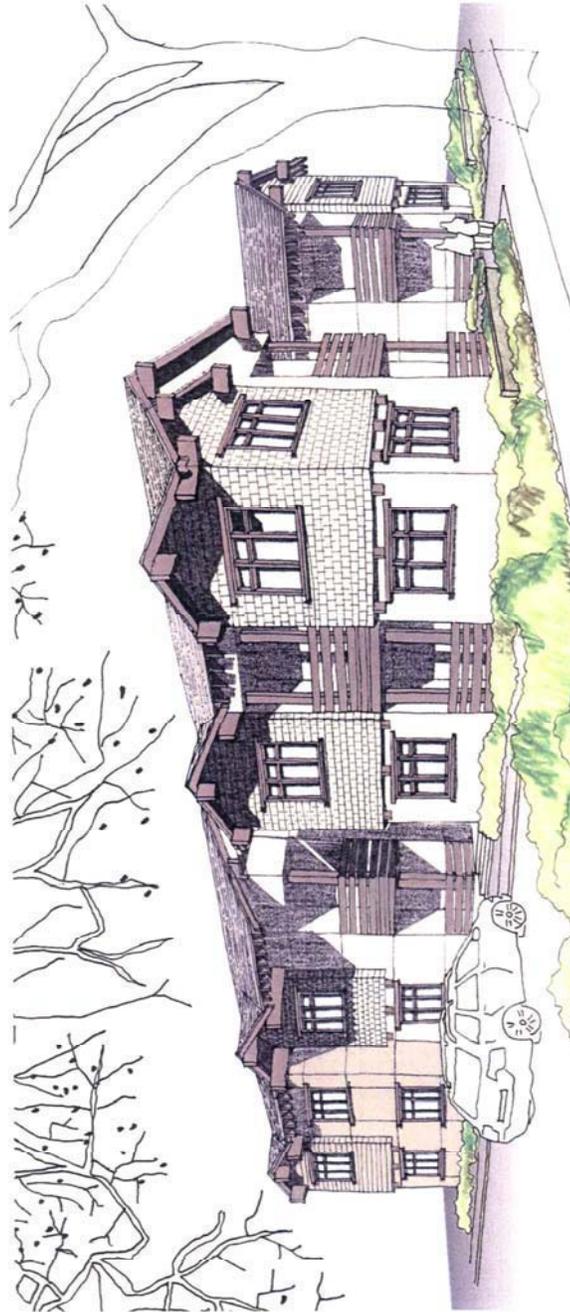
Mercy General Hospital and Sacred Heart Parish School Mixed Use Project  
Sacramento, California

Schematic Design Residential Development  
November 26, 2007 City Council Meeting



C.A-2

Exhibit 4C: Perspective of Housing Option 1



OPTION 1



RAVEL  
KASHNITSKY  
PROPERTIES



C.A-3

Schematic Design  
Residential Development  
November 26, 2007 City Council Hearing

Mercy General Hospital and  
Sacred Heart Parish School  
Mixed Use Project  
Sacramento, California

Exhibit 5C: Elevations of Housing Option 1



- LEGEND**
- ① CEMENT PLASTER
  - ② WOOD WINDOW / DOOR SYSTEM
  - ③ WOOD BEAMS / RAFTERS
  - ④ WOOD SHINGLES / SIDING
  - ⑤ WOOD RAILING
  - ⑥ WOOD RAILING SYSTEM / STAIR
  - ⑦ WOOD SIDING SOFFIT
  - ⑧ 1/2" REVEAL
  - ⑨ ROOFING

OPTION 1

C.A-4




**Schematic Design**  
 Residential Development  
 November 25, 2007 City Council Hearing

Mercy General Hospital and  
 Sacred Heart Parish School  
 Mixed Use Project  
 Sacramento, California

Exhibit 6C: Perspective of Housing Option 2



OPTION 2



RAVEL  
RASMUSSEN  
PROPERTIES

C.A-5

Schematic Design  
Residential Development  
November 25, 2007 City Council Hearing

Mercy General Hospital, and  
Sacred Heart Parish School  
Mixed Use Project  
Sacramento, California

Exhibit 7C: Elevations of Housing Option 2



OPTION 2

Mercy General Hospital and Sacred Heart Parish School Mixed Use Project  
Sacramento, California

Schematic Design Residential Development  
November 25, 2007 City Council Hearing

Stantec

RAVEL KASSELSEN PROPERTIES

C.A-6

Exhibit 8C: Streetscape



OPTION 1



OPTION 2

Mercy General Hospital and  
Sacred Heart Parish School  
Mixed Use Project  
Sacramento, California

Schematic Design  
Residential Development  
November 26, 2007 City Council Meeting

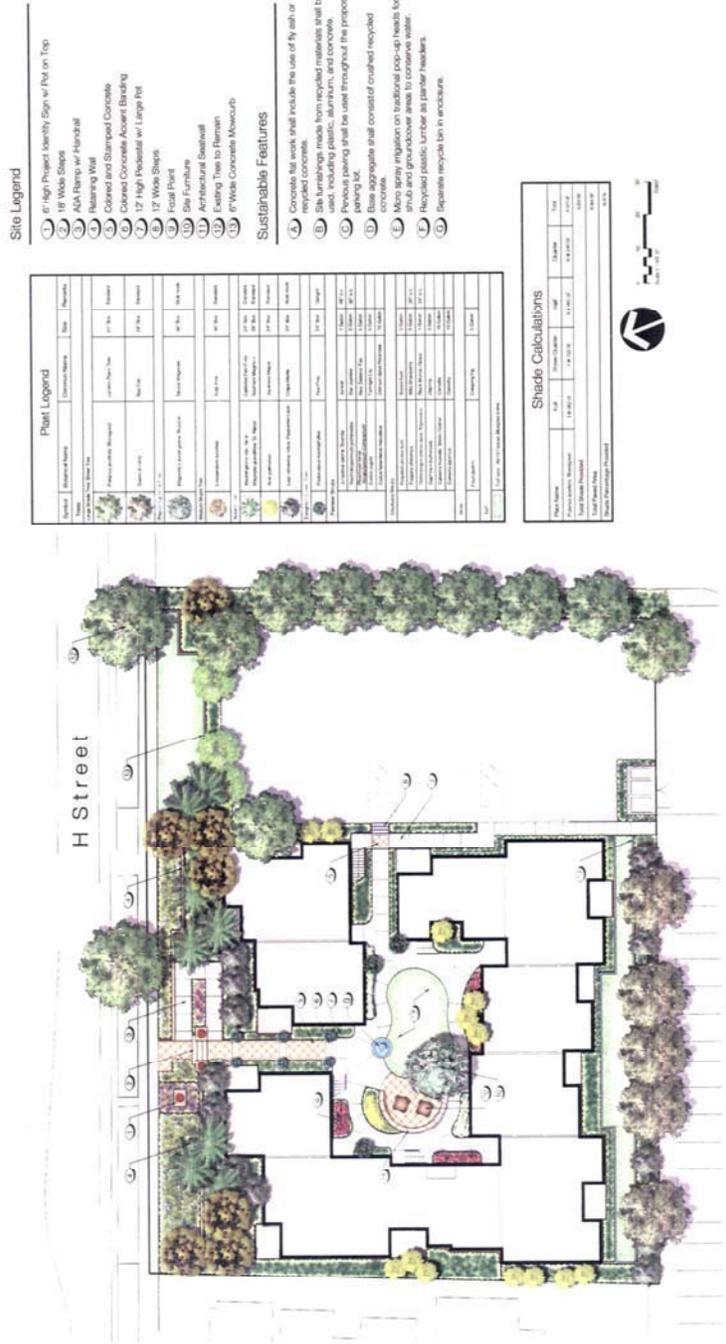


C.A-7



Sacramento, California

Exhibit 9C: Landscaping



The HGA Group Landscape Architects & Planners, Inc.  
 1000 J Street, Suite 200  
 Sacramento, CA 95811  
 Tel: 916.441.1000 Fax: 916.441.1001  
 www.hgagroup.com

**Schematic Design**  
 Residential Development  
 March 30, 2007 - Design Review Submittal

**CHONG | PARTNERS ARCHITECTURE**  
 RAYEL RASMUSSEN  
 PROPERTIES

C.L-1

**Mercy General Hospital and Sacred Heart Parish School Mixed Use Project**  
 Sacramento, California

**MERCY GENERAL HOSPITAL  
&  
SACRED HEART PARISH SCHOOL  
MIXED USE PROJECT**

**TRANSPORTATION SYSTEMS  
MANAGEMENT PLAN - UPDATE**



Prepared for:  
MERCY GENERAL HOSPITAL  
&  
CITY OF SACRAMENTO TRANSPORTATION DEPARTMENT



Prepared by:



September 10, 2007

TABLE OF CONTENTS

BACKGROUND .....1

1.0 TRANSPORTATION SYSTEMS MANAGEMENT .....1

2.0 PROJECT DESCRIPTION .....2

    2.1 Trip Reduction Goal .....3

        Site Plan .....4

        Landscape Plan.....5

3.0 EMPLOYEE TRANSPORTATION COORDINATOR.....6

4.0 TRANSPORTATION MANAGEMENT ASSOCIATION MEMBERSHIP .....8

5.0 GUARANTEED RIDE HOME (GRH) PROGRAM.....8

6.0 PARKING PROGRAM .....9

7.0 PREFERENTIAL PARKING FOR CARPOOLS/VANPOOLS/CLEANER FUEL VEHICLES .....9

8.0 FREE PARKING CARPOOLS/VANPOOLS/CLEANER FUEL VEHICLES.....10

9.0 PERSONAL MATCHING ASSISTANCE .....10

10.0 TRANSIT ACCESS .....11

    10.1 Sacramento Regional Transit (RT) .....11

        Table 1 Local Transit Service to Mercy General Hospital .....12

        Regional Transit Map .....13

        Table 2 Light Rail Transit Trips Available.....14

        Regional Transit Light Rail System Map .....15

        Table 3 Regional Commuter Service to Sacramento .....16

    10.2 Shuttle Program.....16

    10.3 Mercy General Hospital Activities to Promote Enhanced Transit Services.....18

        e-tran Transit Map.....20

11.0 TRANSIT PASS AND VANPOOL FARE SUBSIDY .....21

12.0 BICYCLE FACILITIES .....22

    12.1 Showers and Clothes Lockers.....23

        Bicycle Routes Map.....24

13.0 MERCY GENERAL HOSPITAL EASY BREATHIN', EASY BUCKS PROGRAM CHANGE.....25

14.0 CAFETERIA AND FOOD SERVICES.....29  
15.0 ON-SITE AMENITIES.....30  
16.0 FITNESS CENTER.....30  
17.0 ANNUAL REPORT.....31  
18.0 CONCLUSION .....31

ATTACHMENTS

TSM Information Contacts

**BACKGROUND**

The Mercy General Hospital Alex G. Spanos Heart Center Transportation Systems Management (TSM) Plan was originally submitted on October 18, 2004 and was approved by the City of Sacramento on March 24, 2005. Since the Plan was approved, many changes were made to the project and additional TSM programs and services, such as shuttle service were implemented into Mercy's approved alternative commute program. Because of the changes, Mercy General chose to voluntarily revise and update their TSM Plan. The following TSM Plan applies to the Alex G. Spanos Heart Center only, as other uses such as the Sacred Heart Parish School and the home-sites are not required to provide TSM programs; however, those other uses may benefit from many of the improvements and services provided within this plan such as the new shuttle program.

**1.0 TRANSPORTATION SYSTEMS MANAGEMENT**

Until recently the answer to relieving congestion on roads in the United States has been to build more roads. Current economics and limited resources affect the ability to build and maintain more roads. This reality necessitates better utilization of the existing transportation infrastructure. Transportation System Management measures support the transition from building new roads to utilizing what we have.

The basic premise of Transportation Systems Management (TSM) is strategizing to maximize the use and efficiency of existing transportation resources in order to shape travel demand to the available capacity while optimizing the use of existing and planned infrastructure through a wide range of strategies and technology policies and initiatives.

The Sacramento region, as is typical of urban areas in the United States, has billions of dollars invested in roadway infrastructure, and hundreds of millions of dollars invested in public transit infrastructure. The objective of TSM is to more efficiently and economically take advantage of these major capital investments.

The following are three basic goals that can be achieved through effective utilization of TSM measures:

- 1) Convert trips to an alternative mode of transportation (i.e., transit, carpools or vanpools, bicycling and walking)
- 2) Eliminate trips (i.e., compressed work weeks, telecommuting)
- 3) Improve air quality through technological solutions (i.e., compressed natural gas, electric/hybrid vehicles, or other zero emission vehicles)

## 2.0 PROJECT DESCRIPTION

While creating a new facility that matches the high caliber of Mercy General Hospital's nationally renowned heart program, Mercy General Hospital has been working with neighborhood and community leaders, and nearby businesses to create a livable atmosphere as part of the project scope. Mercy General Hospital has redesigned their current campus to create an improved mixed use environment that will compliment the neighboring community and allow for better connectivity between uses. The campus is located within the blocks of 39<sup>th</sup> and 41<sup>st</sup> Streets and H and J Streets, in the City of Sacramento's East Sacramento Community Plan area. Three main components of the project are proposed:

- 1) Development of a state-of-the-art heart center (Alex G. Spanos Heart Center) and additional surface parking lots
- 2) Removal, relocation, and rebuilding of the existing Sacred Heart School
- 3) Construction of twenty new residential units, a new chapel and healing garden

Originally, the proposed project design included a 171,246-square-foot, five-story heart center. After the community voiced their concerns about the building height and the amount of traffic that would affect their neighborhood, the plans were reduced to a four-story, 123,350-square-foot building. To further enhance the community and improve the campus layout, the 50,928-square-foot East Wing Building will be demolished and in its place parking will be constructed. Other buildings to be demolished include the Sacred Heart Parish School, 17 residential units, a vacant skilled nursing building, and the chapel. A new chapel and healing garden along with 20 new residential units will be developed on the campus. The school will be relocated to the adjacent block (in between 38<sup>th</sup> and 39<sup>th</sup> Streets and H and I Streets). The new school site has developed a highly effective drop-off/pick-up circulation system that will double as a hard surface play area. A net increase of 71,046 square feet will have been added to the campus. The main hospital entrance will be reoriented to face J Street, which will improve patient flow while reflecting the architectural style of East Sacramento. This proposed project does not add services or increase the overall number of patients (the project actually results in a slight reduction in the number of licensed beds) and per the DEIR, no significant increase in traffic is expected to be generated.

Currently, the hospital campus provides 1,276 off-street parking spaces (two parking structures and surface parking lots). Total proposed parking for hospital campus project will increase by 106 spaces on-site (total 1,462 parking spaces on-site) and by approximately 100 spaces off-site (approximately 150 off-site parking spaces). Parking fees are charged to all users of the parking structures. However, carpooling employees are not charged a fee to park.

The site plan on page 4 shows both the existing campus and the proposed changes. Please refer to page 5 for the Landscape Plan.

## 2.1 Trip Reduction Goal

Mercy General Hospital is committed to promoting ridesharing and providing programs and services to reduce traffic and parking impacts on the surrounding neighborhood, and to maximizing mobility options for employees and the community.

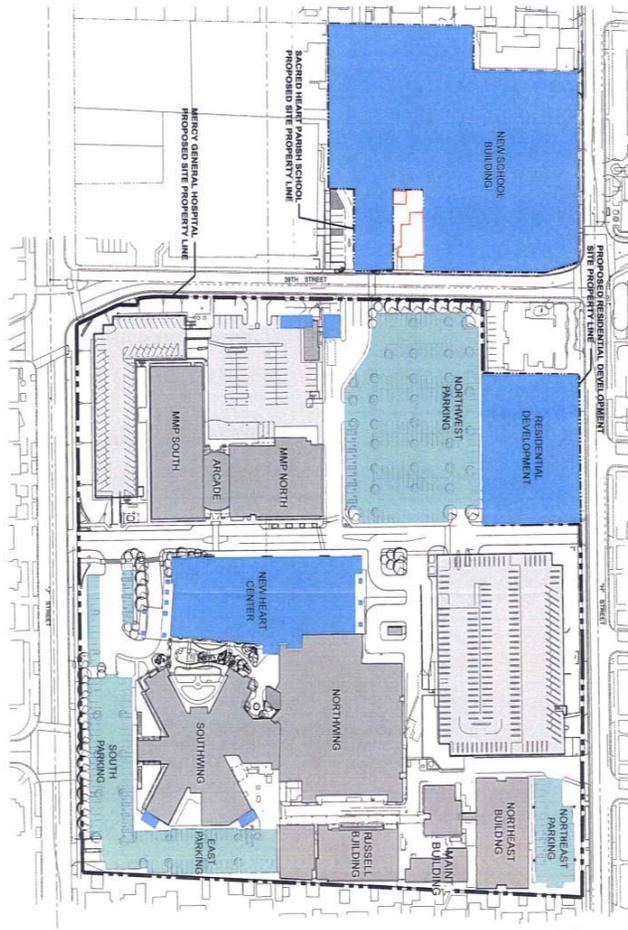
Mercy General Hospital currently provides TSM measures and programs that support the trip reduction goals:

- 100% Transit pass and vanpool fare subsidies
- Shuttle service connecting to the 29<sup>th</sup> Street light rail station
- Financial incentives for employees to rideshare
- Preferred and free parking for carpool/vanpools/clean fuel vehicles
- Ridematching services for carpoolers
- Bicycle lockers and racks/showers and clothing lockers
- On-site employee transportation coordinator
- Free Guaranteed Ride Home program
- Sacramento Transportation Management Association membership
- Employee commute survey
- Annual transportation fair

The ability to survey employees and learn of their commute patterns is an important endeavor: information learned is used to make improvements and changes to Mercy's existing employee programs and successes can be tracked year to year. In August 2006, Mercy General Hospital conducted a survey (78% response rate) to identify how employees are getting to and from work. The results indicated that nearly 18% of Mercy General Hospital employees use an alternative mode of transportation for their commute to work (carpool, transit, bicycle, walk, etc.). Although alternative mode use is rising, the analysis of the survey data supports the need to strengthen the Mercy General Hospital Alternative Commute Program. Mercy General Hospital has a goal to reduce employee commute trips by 35%.

The intention of this Transportation Systems Management Plan is to provide updated information on both new and improved programs, strategies and services offered to employees for the purposes of increased alternative mode use.

This Transportation Systems Management (TSM) Plan encompasses elements that apply to the existing Mercy General Hospital site and the proposed Alex G. Spanos Heart Center only. References to the school and residential uses are provided as information only. Although the school and housing elements are not required to provide trip reduction programs, those uses will benefit from the new project design that enhances mobility and connectivity, and services such as the Mercy's new shuttle program.



**LEGEND**

- PROPOSED BUILDING
- PROPOSED PARKING
- EXISTING BUILDING
- EXISTING PARKING

**SUMMARY**

Building	123,350 s.f.
Parking Provided	1,612 stalls
On-site	1,465 stalls
Off-site	150 stalls
Carpool Parking	100
Bike racks	36
Bike lockers	36



**HDR**  
The Heart Center

Mercy General Hospital and  
Sacred Heart Parish School  
Mixed Use Project



Modified by  
**HDR**  
The Heart Company

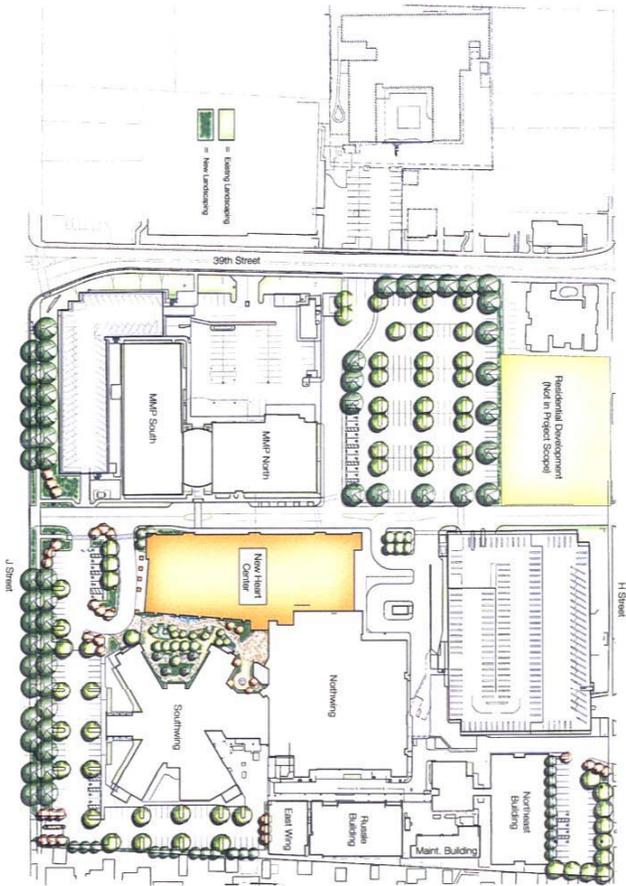
OVERALL SITE PLAN  
EXISTING & PROPOSED

Mercy General Hospital  
Heart Center Project  
Date of Submission  
October 9, 2007

SCALE: 1" = 200'

Mercy General Hospital & Sacred Heart Parish School Mixed Use Project TSM Plan Update

September 10, 2007



THE HOK GROUP LANDSCAPE ARCHITECTURE SERVICES, INC.  
 2001-2002, 2004-2005, 2006-2007, 2008-2009  
 1000 B STREET, SUITE 1000, SACRAMENTO, CA 95811  
 TEL: 916.441.1000 FAX: 916.441.1001 WWW.HOKGROUP.COM

<p><b>HDR</b>          Architecture Inc.          1000 California Street          Sacramento, CA 95811</p>	<p><b>Mercy General Hospital and Sacred Heart Parish School Mixed Use Project</b></p> <p style="text-align: center;"><b>Landscape Plan</b></p>	<p><b>Mercy General Hospital Heart Center Project</b>          City of Sacramento          Design Review          February 7, 2007</p>
--	--	--

HDR | THE HOK GROUP

Page 5

### 3.0 EMPLOYEE TRANSPORTATION COORDINATOR

Management support through marketing and implementation efforts are crucial to the success of this TSM Plan. In order to ensure that the programs and incentives found in this Plan are offered and marketed to employees within Mercy General Hospital, an Employee Transportation Coordinator (ETC) is a necessity.

Transportation Coordinators play the key role of liaison between the employees, employer, and the supporting agencies, such as the Sacramento Transportation Management Association (TMA) and Regional Transit (RT). The Employee Transportation Coordinator provides the following services:

- Market and promote alternative commute options, trip reduction and air quality strategies to employees of Mercy General Hospital.
- Manage and distribute free RT passes
- Be the main point of contact for employees wanting to commute using an alternative mode.
- Conduct annual employee surveys and evaluate survey results to identify program course corrections.
- Catalog and promote all existing incentives that encourage employees to utilize alternative transportation programs. Direct and target market alternatives to employees in specific transit corridors and in high-density carpool areas identified by zip codes.
- Serve as the main point of contact for the Sacramento Transportation Management Association (TMA) and on-site Guaranteed Ride Home voucher services for employees.
- Work with the Sacramento TMA and local agencies (such as Regional Transit (RT), Roseville Transit, Amador Regional Transit, San Joaquin Regional Transit, all other commuter transit organizations, and the Sacramento Area Council of Governments), and post informational materials on the Mercy General Commute Information Display and various bulletin boards in employee common areas, as well as disperse alternative program information to employees via posters, flyers, banners, Inside Mercy newsletter, new employee orientation, etc.
- Coordinate and manage various aspects of the Plan that require periodic updating or monitoring, such as Guaranteed Ride Home (GRH) program, carpool and vanpool registration, parking enforcement, bicycle locker assignment and enforcement, PMA/511 programs and encouraging future shuttle usage.

In an effort to boost alternative mode usage, the ETC has developed new programs, increased marketing, and increased program budgets:

- ✓ Carpoolers are now offered monthly gas cards and bicyclists and walkers are offered \$4 cafeteria vouchers for each day that they bicycle or walk to work. Employees who carpool but do not require a parking space because they are dropped off by the carpool driver will also be provided with \$4 cafeteria vouchers.
- ✓ Both the shuttle program and annual transportation fair have been expanded.
- ✓ The Easy Breathin' Easy Bucks program has been replaced by new programs such as the "Bonus Bucks" referral program and enhanced marketing efforts.
- ✓ Employees are recognized through the "Commuter of the Month" program. The ETC identifies candidates for the honor of being named "Commuter of the Month" and presents nominations to the Commuting Options Task Force. Those employees selected are featured in an article of the Inside Mercy newsletter. The story portrays their alternative commute such as carpooling, walking or biking to work. The articles are geared towards promoting alternative commuting modes."

A critical component of the success of this TSM Plan will be the marketing of the ETC. Survey results have indicated that employees do not know who to contact to find out about alternative programs. The ETC contact information will be promoted within the Inside Mercy employee newsletter and through other regular shared information including the Commuting Options bulletin board. The ETC will regularly visit departments and hold promotions during lunch and dinner hours in the cafeteria to establish face recognition.

The ETC position is filled by:

Name: Ms. Anne Simpson  
Mercy General Hospital

Address: 4001 J Street  
Sacramento, CA 95818

Phone: (916) 453-4699

#### 4.0 TRANSPORTATION MANAGEMENT ASSOCIATION MEMBERSHIP

Mercy General Hospital is a long-standing and active member of the Sacramento Transportation Management Association (TMA). The Mercy General, Alex G. Spanos Heart Center project will maintain membership in the Sacramento TMA.

TMAs are private, nonprofit organizations run by a voluntary Board of Directors, typically with a small staff. They help businesses, developers, building owners, local government representatives, and others work together to collectively establish policies, programs, and services to address local transportation problems. The key to TMAs lies in the synergism of multiple groups banding together to address and accomplish more than any one employer, building operator, developer, or resident.

The following is a listing of services that the Sacramento TMA provides:

- Guaranteed Ride Home (GRH) Program
- Vanpool Start-up Subsidy Program
- Online Personalized Carpool Matching links
- Information on local issues
- Web site/Resource links
- Training
- Newsletter

These TMA programs and services augment Mercy General Hospital's employee commute program. Working directly with the TMA Executive Director, the ETC disseminates information on TMA programs, services and incentives to employees. The ETC will market the TMA's Web site ([www.sacramento-tma.org](http://www.sacramento-tma.org)) to employees through the Inside Mercy newsletter and the Commuting Options bulletin board.

#### 5.0 GUARANTEED RIDE HOME (GRH) PROGRAM

Survey results continue to show a strong correlation between the employee's desire to use an alternative to driving alone to work and their fear that they will be stranded in case of a personal emergency if they are without their car. The Guaranteed Ride Home Program helps alleviate those fears, making it easier for the employee to choose to an alternative to driving alone to work.

All employees who commute to work using transit, carpool or vanpool, bicycle, walk or rollerblade are guaranteed a free ride home in the case of a personal emergency, or when they unexpectedly have to work late thereby missing the last bus, or their normal carpool home. Mercy General Hospital provides this program to employees through their membership in the Sacramento TMA.

When asked what would encourage employees to use an alternative to driving home...Employees have expressed that a guaranteed ride home in case of an emergency would be their number one choice.

Survey results also indicate that employees do not know that they have this amenity available to them. The Mercy General ETC will continue to market this supporting commute benefit to employees. Flyers will be posted on the Commuting Options bulletin board, information will be provided within the Inside Mercy newsletter, and information will continue to be shared at transportation fairs or other fairs targeted towards employees. An enhanced campus-wide campaign to educate employees about this existing program will include mini-transportation fairs that focus on carpooling. Mini-fairs will take place on site at a minimum of twice yearly and may be incorporated into existing employee fairs such as the Health Fair.

**6.0 PARKING PROGRAM**

**Carpoolers  
Park for Free!**

Mercy General Hospital and the proposed Alex G. Spanos Heart Center will continue to charge for parking at all parking facilities. Currently, employees pay \$15/month or 75¢/day. Charging for parking creates an incentive for employees to consider other alternative transportation modes. Charging employees who drive alone to work to park, along with offering incentives such as free transit or free carpool parking provides a greater incentive for employees to choose an alternative.

**7.0 PREFERENTIAL CARPOOL PARKING**

Employees who agree to carpool 60% of the time with another Mercy General Hospital employee receive a preferentially located parking space located in the parking garage.



Example of preferential carpool parking signage

Mercy General Hospital designates a sufficient number of employee parking spaces to accommodate all carpoolers. As the number of carpoolers increases, additional spaces are added to accommodate the increase. Carpool spaces are located in prime locations (in the covered parking garage). The spaces are clearly marked for "Carpool/Vanpool/Cleaner Fuel

Vehicles Only." Designated carpool parking is only available to Mercy General employees who carpool with other Mercy General employees. Monthly carpoolers must also have a valid carpool permit. Carpool registration and permits are updated annually at the required annual employee safety-training event.

#### 8.0 FREE PARKING CARPOOLS/VANPOOLS/CLEANER FUEL VEHICLES

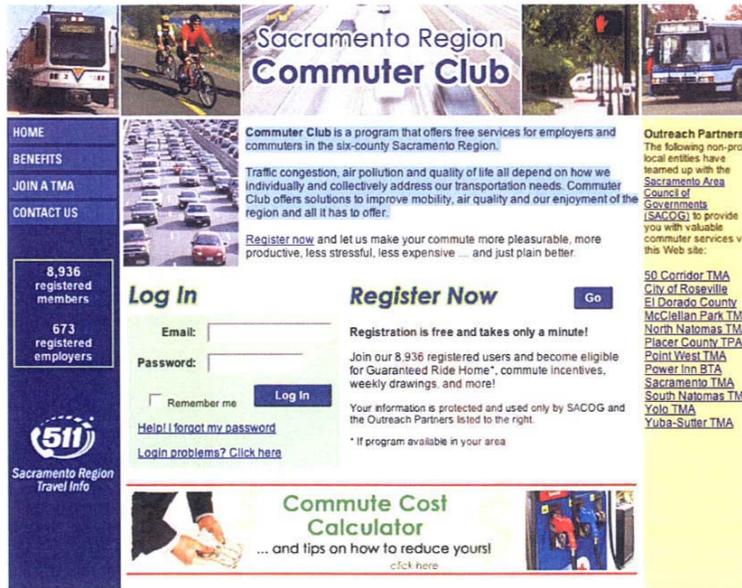
Carpooling is a strong component of the Mercy General Hospital Alternative Commute Program. It is the most widely used mode choice of employees who do not drive alone to work. It will continue to be the most important alternative mode for the Alex G. Spanos Heart Center. Employees who agree to carpool 60% of the time with another Mercy General Hospital employee will receive free monthly parking. All employees in a full-time carpool will be required to register for this program. Numbered carpool permits will be issued to registrants.

Carpooling employees that do not require a parking space because their carpool partner works elsewhere and they get dropped off also benefit. \$4 cafeteria voucher are given to these carpooling employees for every 4 days that they carpool.

#### 9.0 PERSONAL MATCHING ASSISTANCE

Forming carpools is important in reducing the number of commute trips into and out of the Sacramento area. Carpooling helps to reduce traffic congestion on surface streets and, typically, reduce commute time, especially on freeways with High Occupancy Vehicle lanes (also known as carpool lanes). The ETC will work directly with employees who would like to carpool to work by providing assistance in forming a carpool or finding a ride with an existing carpool. One way that the ETC will provide assistance is through the utilization and promotion of a regional database program sponsored by the Sacramento Area Council of Governments (SACOG) called Commuter Club. The Commuter Club offers free services for employers and commuters within the six-county Sacramento Region.

Commuter Club is primarily accessible via the Internet and is user friendly. ([www.http://sacregioncommuterclub.org](http://sacregioncommuterclub.org)). When accessed, Commute Club will give commuters the information they need to make better choices when planning commute trips. Commuters can get up-to-the-minute information about traffic conditions, public transportation options, ridesharing, and bicycling anytime, anywhere throughout the Greater Sacramento area and Northern California. The ETC will also work to promote in-house carpooling and vanpooling. Employees who want to participate will be matched to other interested employees with similar schedules and home addresses.



Commuter Club home page

Employees will be provided with information on the benefits of carpooling (including cost savings of carpooling over driving alone) and be provided with promotions through the TMA, as well as personal assistance from the ETC if necessary, in order to register their commute information or to match Mercy General employees with one another directly.

10.0 TRANSIT ACCESS

10.1 Sacramento Regional Transit (RT)

Local Service

Sacramento Regional Transit (RT) provides direct and nearby access to the project site via bus routes 30, 31, and 34. Routes 30 and 31 directly serve Mercy General Hospital at a bus stop near the front entrance on J Street with 244 trips per day. Route 34 is easily accessible within three blocks north from the project. In total, there are 338 Regional Transit bus trips that provide service to Mercy General Hospital. Table 1 reflects local transit service available to Mercy General Hospital. Please refer to page 13 to view the project site in relation to local Regional Transit service.

**Table 1**  
**Local Transit Service to Mercy General Hospital**

Route #	Span of Service	# of Trips Per Weekday	Communities Served
30	7 Days/Week 5:35 a.m. – 10:24 p.m.	218	Sac Valley Station, J & 8th, J & 28th, J & 39th, CSUS Admin. Bldg., L & 29th, L & 9th
31	Monday – Friday 6:17 a.m. – 6:18 p.m.	26	Sac Valley Station, J & 8th, J & 28th, J & 39th, CSUS Admin. Bldg., and Carlson & H (River Park), L & 29th, L & 9th
34*	7 Days/Week 5:44 a.m. – 7:34 p.m.	94	8th & O, 8th & K, F & 12th, F & 29th, F & 52nd, CSUS Admin. Bldg., 65th Street LRT Station
All buses are lift equipped for handicapped, elderly, or those in need.			
<b>Total Local Transit Trips</b>		<b>338</b>	<b>Per Weekday</b>

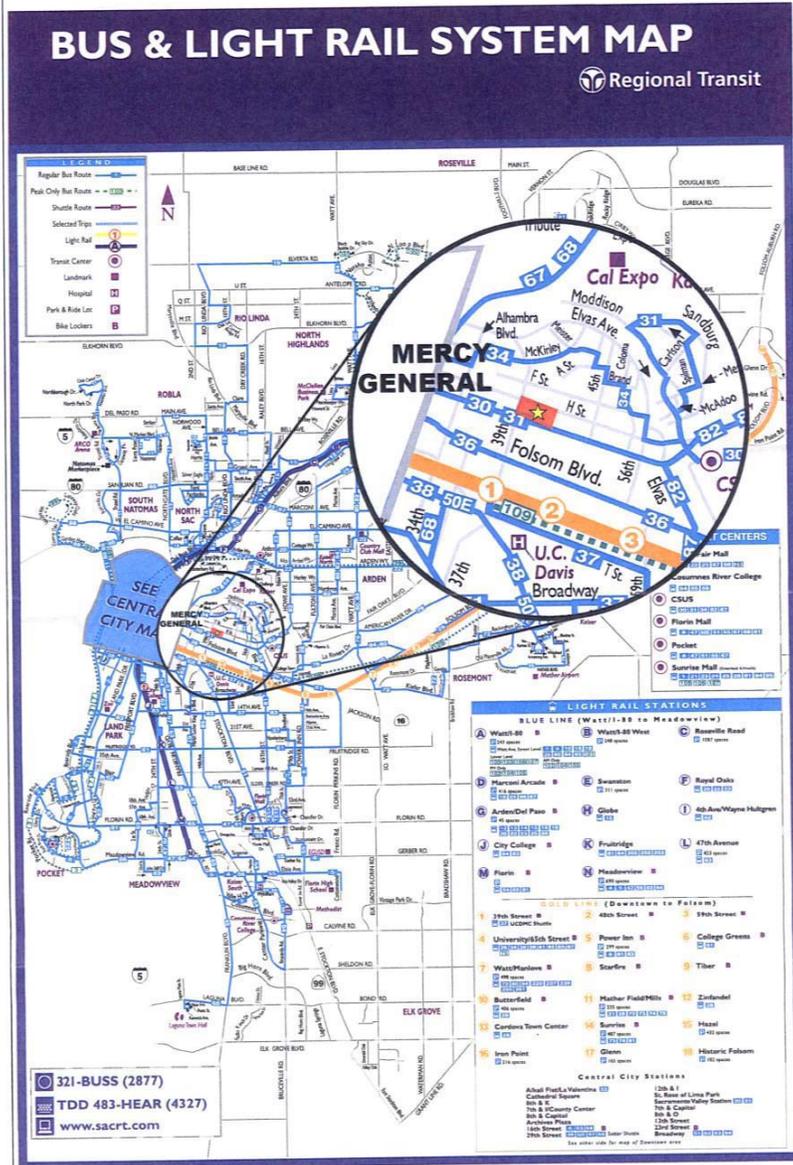
\*Route 34 is within three blocks north from Mercy General Hospital.

Below is a photo of the RT bus stop located at Mercy General Hospital on J Street across from the entrance to the facility. RT Routes 30 and 31 provide direct and frequent (every 15 minutes) service to this site.



RT rider waiting for bus at Mercy General Hospital – J Street bus stop

HEAR Regional Transit



Mercy General Hospital & Sacred Heart Parish School Wood Use Project TSM Plan Update

Page 13

September 10, 2007

**Regional Transit Light Rail Service**

Light rail service is available via a short shuttle ride or bicycle ride and provides 684 trips per day. Table 2 reflects the number of light rail trips available on the RT Light Rail system and page 15 includes the map of the light rail system.

**Table 2  
Light Rail Transit Trips Available**

Route	Span of Service	# of Trips Per Weekday	Communities Served
LRT	7 Days/Week 3:55 a.m. – 12:51 a.m.	684	<p><b>Watt/I-80 - Downtown - Meadowview</b> Watt/I-80, Marconi/Arcade, Arden/Del Paso, St. Rose of Lima, 8th &amp; O Streets, 16th Street, University/65th Street, Meadowview Road, Florin Road, Fruitridge Road, Sacramento City College, 4th Avenue/ Wayne Hultgren, 16th Street, 8th &amp; O Streets, and St. Rosa Lima.</p> <p><b>Downtown to Folsom</b> Watt/Manlove, Mather/Mills, Sunrise Blvd., Hazel Road, Historic Folsom, University/ 65th Street, 16th Street, 29th Street, 8th &amp; O Streets, and St. Rosa Lima.</p>

Shuttle riders will have no trouble finding the Mercy Shuttle stop after disembarking light rail. The 29<sup>th</sup> Street Light Rail Station has Mercy General Shuttle stop signage prominently displayed.



**Regional Service**

Additional commuter transit services from around the region are also available to Mercy General employees via RT system transfers. If an employee takes bus route 30, 31, or 34 they can transfer to another transit operators' bus and travel outside of Sacramento to locations such as Roseville, El Dorado Hills, or Fairfield. Table 3 shows the various regional commuter services available to by transfer.

**Table 3  
Regional Commuter Service to Sacramento**

Route #	Span of Service	# of Trips Per Weekday	Communities Served
San Joaquin Transit	Monday-Friday Commute Service	4	Q & 5 <sup>th</sup> St, 8 <sup>th</sup> & N St, J & 11 <sup>th</sup> St, 15 <sup>th</sup> & N St, N & 16 <sup>th</sup> St, I & 15 <sup>th</sup> St, Capitol & 7 <sup>th</sup> St, P & 5 <sup>th</sup> St.
Amador Transit	Monday-Friday Commute Service	6	SACRAMENTO EXPRESS, Hwy 16 & Watt Ave., 65 <sup>th</sup> St LRT Station, 29 <sup>th</sup> & L St, 28 <sup>th</sup> & J St.
Roseville Transit	Monday-Friday Commute Service	6	Roseville, transfer to RT.
Yuba-Sutter Transit	Monday-Friday Commute Service	13	J & 4 <sup>th</sup> St, 8 <sup>th</sup> St, 11 <sup>th</sup> St, 15 <sup>th</sup> & K St, P & 13 <sup>th</sup> St, 9 <sup>th</sup> St, 5 <sup>th</sup> St.
Placer Transit	Monday-Friday Commute Service	6	Commuter Express – J & 4 <sup>th</sup> St, 8 <sup>th</sup> St, 11 <sup>th</sup> St, 15 <sup>th</sup> & K St, N St, P & 13 <sup>th</sup> St, 9 <sup>th</sup> St, 5 <sup>th</sup> St.
Fairfield/Suisun Transit	Monday-Friday Commute Service	10	Route 30 – Capitol Mall, J & 6 <sup>th</sup> St, 9 <sup>th</sup> & L St, 9 <sup>th</sup> & O St, P & 8 <sup>th</sup> .
El Dorado Transit	Monday-Friday Commute Service	23	P & 30 <sup>th</sup> St, 21 <sup>st</sup> St, 16 <sup>th</sup> St, 13 <sup>th</sup> St, 11 <sup>th</sup> St, 9 <sup>th</sup> & S St, 8 <sup>th</sup> & N St, K, I & L St, 5 <sup>th</sup> & N St, H & 11 <sup>th</sup> St, J & 6 <sup>th</sup> St, 9 <sup>th</sup> & L St, N & 14 <sup>th</sup> St.
Folsom Stage Line	Monday-Friday	16	Route 10 – Connects to Iron Point Road Light Rail Station.
Yolobus	Monday-Friday Commute Service	103	J & 8 <sup>th</sup> St, 9 <sup>th</sup> & N St, 10 <sup>th</sup> & N St, 16 <sup>th</sup> & Capitol, L & 13 <sup>th</sup> St, L & 6 <sup>th</sup> St, H & 11 <sup>th</sup> St, 15 <sup>th</sup> & N St, 3 <sup>rd</sup> & Capitol, 3 <sup>rd</sup> & G St, Capitol & Front, N & 4 <sup>th</sup> St, 8 <sup>th</sup> & N St, L & 7 <sup>th</sup> St.
Capitol Corridor/Amtrak	Monday-Friday Commute Service	32	Auburn, Rocklin, Roseville, Downtown Sacramento, Davis, Fairfield and Bay Area.
All buses are lift equipped for handicapped, elderly, or those in need.			
<b>Total Commuter Transit Trips</b>		<b>219</b>	<b>Per Weekday</b>

**10.2 Shuttle Program**

Mercy General Hospital has a new shuttle program that supports transit ridership. The exciting new program allows residents, patients, and employees the opportunity to ride for free and connect to the 29<sup>th</sup> Street Light Rail Station. The 29<sup>th</sup> Street Light Rail Station provides the greatest opportunity for connections to other transit operators.

- Initiated in January 2006
- Shuttle provided during peak a.m. and p.m. commute hours to and from the 29<sup>th</sup> Street Light Rail Station
- Shuttle ridership is approximately 18/day (nearly double from 2006 opening)
- Shuttle is free to employees and open to the community
- Expanded shuttle service began July 2, 2007 improving service from every 20 minutes to every 15 minutes during the peak commute hours



Mercy General Shuttle



Shuttle Stop and Signage

This shuttle has the potential to reduce vehicle trips in the surrounding neighborhoods as it is open to residents who may wish to hop on and connect to light rail to finish their commute.

### 10.3 Mercy General Hospital Activities to Promote Enhanced Transit Services

On Thursday, November 9, 2006, Mercy General Hospital invited the top three local Sacramento healthcare providers together to discuss expanding existing hospitals shuttle services and to discuss possible transit opportunities such as improved regional service. The meeting was attended by representatives from Mercy General Hospital, UC Davis Medical Center, and Sutter Medical Center, Sacramento.

The group discussed their desire to pursue better transit services for their employees traveling from south Sacramento and Elk Grove areas. A shared Hospital shuttle service could provide greater connectivity for these employees if regional and local transit (RT and e-tran) would provide service to the 29<sup>th</sup> Street light rail station. Furthermore, a shared shuttle concept could provide excellent connectivity service for Mercy and Sutter General Hospital because of their proximity to each other.

A letter was drafted, signed by all three hospital executive officers, and sent to the General Managers of Sacramento Regional Transit (RT) and Elk Grove Transit (e-tran) requesting more transit service to a central location for South Area and Elk Grove employees. Since the letter was sent, the hospital representatives have met with e-tran and requested enhanced service to the 29th Street Light Rail Station. If this comes to fruition, the hospitals, in partnership, could consolidate shuttle efforts and improve service and efficiency of the shuttle programs run by all three hospitals.

In August of 2007 e-tran provided information and pending near-term changes that include additional clean natural gas bus service. Potential changes and enhancements are not approved but if implemented would benefit hospital employees, residents and patients:

- Add a fourth trip to the Route #57 to the 29th Street Light Rail Station (serves Calvine/Elk Grove Florin)
- Expand late night services for Route #49 (provides location demand service West of State Route 99 and East of State Route 99 in Elk Grove) to the 29th Street Light Rail Station to 9:00 p.m. hour – requires commitment of 8 to 12 new riders per day
- Move pick up/drop off location from 16th Street for Route #58 (East Elk Grove Express – serving Bradshaw at Bond, Waterman at Kliever and Mosher at Berens Park) to serve 29th Street Light Rail Station
- Add new Route #46 (Bilby/Laguna Express) to DMV or 29th Street Light Rail Station (number of trips unknown)
- Establish reverse/return service Route #49 as a Route #60 (Elk Grove Park and Ride Express/East Stockton Blvd.) with five trips providing pick ups at park and ride lots for graveyard-shift employees (7:00 p.m. to 7:00 a.m.) – requires commitment of 15 to 20 new riders per day
- January 2008 – possible new Hwy 99/Stockton Corridor/Broadway route (number of trips unknown)

Mercy General Hospital & Sacred Heart Parish School Mixed Use Project TSM Plan Update September 10, 2007

In order to consider these transit service enhancements, e-tran identified needs and commitments from the three hospitals such as shuttle consolidation and ridership commitments.

Currently the three hospitals are working on providing e-tran with e-tran's requests and with Regional Transit to discuss improvement options. e-tran's system map is provided on the following page.



#### 11.0 TRANSIT PASS AND VANPOOL FARE SUBSIDY

Mercy General Hospital provides a 100% transit and vanpool subsidy for employees who use transit or vanpool options at least 60% of their commute activities (up to a maximum of \$85/month). A 100% subsidy is an increase from the 50% subsidy that was provided. A 100% subsidy is a significant factor in the success of the shuttle and growth in transit ridership. Increased subsidies will encourage more transit ridership for employees who commute from longer distances. Commuter services such as Roseville Transit, Yuba-Sutter Transit, Amador Regional Transit, Yolo County Transit, San Joaquin Regional Transit and Capitol Corridor/Amtrak are viable regional commute options for employees who live in other areas of the Sacramento region via connections with the RT system.

The transit subsidy program is heavily promoted. Information is provided at new employee orientations and is publicly displayed. Presentations are made to managers, and at quarterly employee forums and through on-site events or fairs. Additionally newsletter articles, at least one story per edition, which may occur twice monthly, include an article on the "Commuter of the Month".

RT passes are made conveniently available on-site at the Parking Office. The latest survey results suggest that there continues to be opportunities to provide further information to employees about the free transit pass program. Additional promotions and marketing of this benefit are needed to encourage more transit users. Transit specific mini-fairs will be held at least three times throughout the year on the campus.

The Bureau of Transportation Statistics references the national 2006 average vehicle operating and ownership costs to be 52.2 cents per mile. The approximate savings per year for an employee commuting from Roseville to Mercy General Hospital using a transit option is \$5,727. This annual figure also includes the savings realized from not needing to pay for parking. Additional savings could be found in lowered automobile insurance rates when employees use transit commute options.

12.0 BICYCLE FACILITIES

Currently, there is a locked bicycle area located on the first level of the Mercy General Hospital parking structure. This caged facility contains racks that accommodate 22 bicycles. Employees and the public have access to this bicycle cage. The parking attendant unlocks this area for those who need access.



Class I bicycle parking

A bicycle rack is also available near the entrance of the hospital building and contains space for five bicycles.



Class II bicycle racks

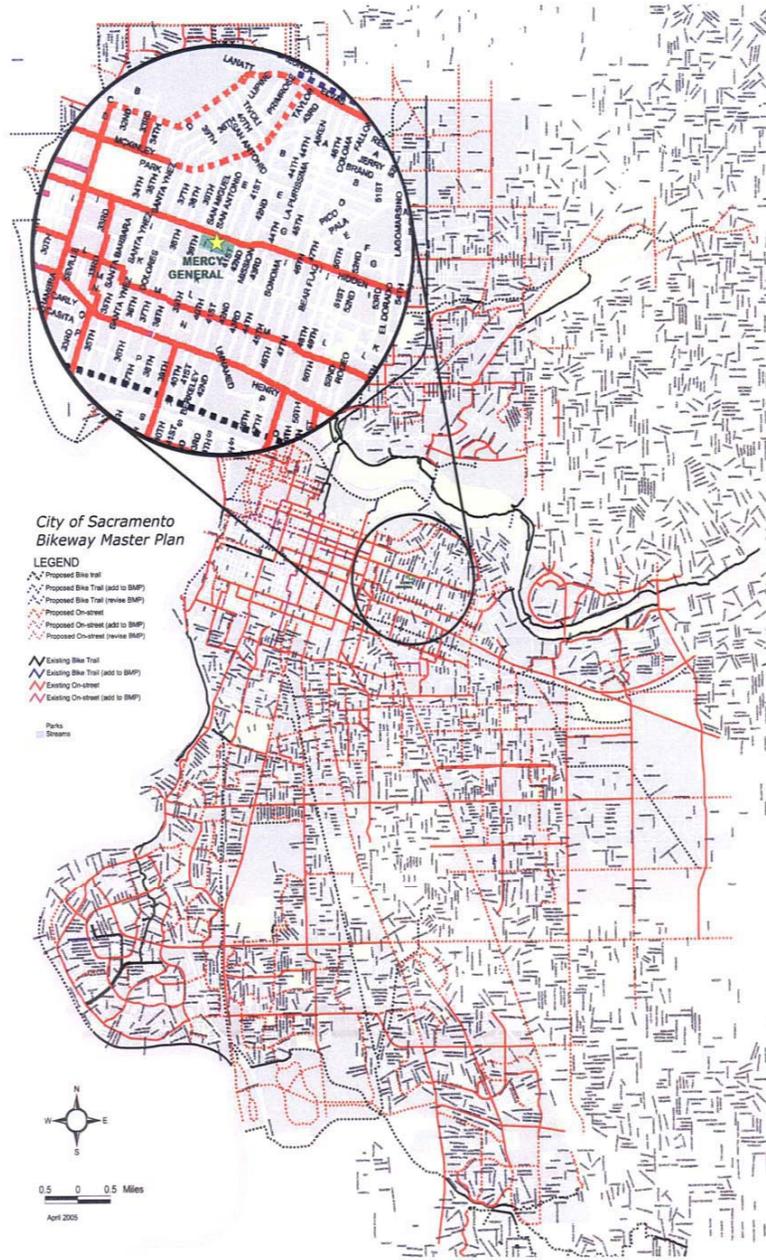
In order to maximize bicycle access for the proposed project, bicycle parking that meets City code specifications will be provided. A total of 36 Class I bicycle facilities will be made available in the current cage facility. Class I storage (locked and covered from the elements), provide the optimum storage for bicycles. In addition, a total of 36 Class II bicycle racks will be placed near various building entrances throughout the campus.

Secured bicycle parking is provided free of charge to employees, visitors and others. Existing on-street bikeways near the Mercy General Hospital facility are shown on the following page. The closest on-street bikeway is immediately adjacent on H Street.

### 12.1 Showers and Clothes Lockers

Mercy General Hospital provides one (1) shower facility in the Engineering Department. Two showers and 10 lockers are located in the current Russell Hall, which houses the outpatient therapy pool program. Four additional showers are provided, two for each gender in the Cardiopulmonary Rehabilitation Center. A total of seven showers and 70 clothes lockers will be provided for employee commuters who bicycle, walk or jog to work at Mercy General Hospital.

FCR | Sacred Heart Community



Mercy General Hospital & Sacred Heart Parish School Mixed Use Project TSM Plan Update

September 10, 2007

Page 24

**13.0 MERCY GENERAL HOSPITAL EASY BREATHIN', EASY BUCKS PROGRAM CHANGE**

Recent survey results and program participation have indicated a need to make changes. Mercy General Hospital had been providing a quarterly drawing for cash rewards through a program called Easy Breathin' Easy Bucks. This program was available to all employees who used an alternative to driving alone to get to work. Each quarter, five employee names would be drawn to receive a \$300 cash prize for their participation. This program was conducted at an annual cost of \$6,000. This program has been replaced with expanded programs and new marketing efforts such as the Bonus Bucks program.

**Bonus Bucks Program**

A new special incentive program has recently been introduced called the Bonus Bucks Employee Referral program. An employee who refers an employee who is starting to use another commuting mode will receive a \$100 bonus. In order to receive the bonus the employee who joins the Commuting Options Program must be in the program for 3 months and use the alternative commute mode at least 60% of the days they work. There is not limit to the number of bonuses available.

**Gas Cards and Cafeteria Vouchers**

Carpoolers are offered \$35 monthly gas cards for two carpoolers. Carpools with three people riding receive a \$50 gas card and carpools with 4 people riding get a \$75 gas card. Bicyclists and walkers receive a \$4 meal cafeteria voucher. For those employees who carpool with someone not working at Mercy and are dropped off at they hospital, they will receive a \$4 meal cafeteria voucher for every four days they carpool.

*Don't Pay High Gas Prices.... Avoid The Traffic...Save the Earth!!*

Choose **Employee Commuting Options**

**Monthly Subsidy**

Light Rail & Bus Passes  
Free (\$80.00 Value)

Carpoolers

2 Employees - \$35 Gas Card

3 Employees - \$50 Gas Card

4 Employees - \$75 Gas Card  
(Free parking to all carpoolers)




**Daily Subsidy**

Cyclists & Walkers & non-Parking Carpoolers  
- **\$4.00 Greenhouse Meal Ticket**

### Annual Transportation Fair

The annual transportation fair has also been given a helping hand. Because of an increased budget, the 2007 fair was the most successful to date.

Each year, Mercy General Hospital (MGH) hosts an employee transportation fair to promote commute alternatives, reduce parking demands and traffic congestion, and improve air quality. This year the scale of the event was expanded and it was opened to the public. Promotion of the new light rail shuttle that provides employees, and now patients and residents, with free peak-hour service to the 29th Street Light Rail Station was a key feature of the event.

The Mercy General Hospital (MGH) Transportation Fair, which took place on-site on a Saturday, provided alternative commute, air quality and health information to employees and community members. The event was a resounding success with approximately 350 people in attendance over the course of the four-hour period. This event resulted in the distribution of 348 servings of ice cream, 248 cups of lemonade, 250 bottles of water, more than 200 flower and vegetable seed packets (thank you gifts), and approximately 35 bicycle tune-ups, all of which were provided free of charge to attendees throughout the event. Channel 13 covered the event and aired the story on the evening news.

The event featured the following 34 participating vendors:

- Spare the Air – Sacramento Metropolitan Air Quality Management District
- Breathe California of Sacramento – Emigrant Trails
- California Air Resources Board (CARB) – Fuel Cell Vehicle
- Carbon Footprint Calculator online quiz
- Bicycle Chef – Free bike tune-ups
- CA Department of Transportation, Bicycle Facilities Unit
- City Bicycle Works – Free bike tune-ups
- Mercy General Hospital Bicycle Parking Registration
- Sacramento Area Bike Advocates (SABA)
- Sacramento Wheelmen
- Davis Electric Cars Green E Motor – Electric Scooters
- Sacramento Municipal Utility District (SMUD) – Hybrid Plug-in Prius
- ZAP PowerBike "The Charger" Electric Bicycle
- California Fuel Cell Partnership – Fuel Cell Vehicle
- Guaranteed Ride Home – Yellow Cab Company of Sacramento
- Mercy General Hospital Alex G. Spanos Heart Center
- Mercy General Hospital Carpool Parking Registration
- Mercy General Hospital Parking Information
- City of Sacramento, Alternative Commute Coordinator
- Mercy General Hospital Employee Commute Program
- SacRegion 511 & SacRegion Commuter Club – carpool matching and membership sign-ups
- Mercy General Hospital Community Light Rail Shuttle & free rides

- Capitol Corridor/AMTRAK
- e-tran (Elk Grove Transit)
- Mercy General Hospital Transit Pass Sales
- Sacramento Regional Transit District (RT)
- Vanpool Services Incorporated (VPSI)
- WalkSacramento
- Mercy Heart Institute
- Retinal Institute
- Burr's Fountain - Free ice cream
- Free lemonade and water provided by MGH
- Fantacea – Live folk music
- Banjo Players Extraordinaire – Live banjo music

In advance of the event, the following outreach efforts were completed to notify the community, employees, media and stakeholders of the event:

- Mass mailing of flyers to area community groups and all residents within a 1/4-block radius of the hospital
- Posted event announcement in the employee newsletter, and at key locations throughout the hospital
- Designed and secured publication of advertisements in neighborhood
- Reminder calls were made to community groups including the East Sacramento Chamber of Commerce, East Sacramento Improvement Association and the McKinley East Sacramento Neighborhood Association
- Email reminders were sent to key stakeholders, neighbors and community associations
- Sent press release information to local television, radio and print media outlets



Community members enjoying ice cream at the Community Spare the Air Transportation Fair

Mercury General Hospital & Sacred Heart Parish School Mixed Use Project TSM Plan Update September 10, 2007

The event was also announced on the Web sites for the Environmental Council of Sacramento and the California Fuel Cell Partnership, as well as Sacramento Meetup (a Web site that helps individuals find and participate in special interest groups) in two different categories.

MGH's shuttle promotions for the new service to light rail were highlighted throughout the event. The new MGH shuttle schedule materials were distributed to interested attendees. Shuttle materials were finalized in advance of the event and shuttle stop signage was created and placement at bus stops and at the 29th Street Light Rail Station was coordinated with RT.

A festive atmosphere, which helped to draw additional attendees, was achieved through placement of the event along a major thoroughfare, utilization of a large quantity of balloons, 2 large A-frame signs located at key entrance points, numerous shade tents, multi-colored table cloths, live music (two bands), free refreshments, displays of 10 alternative commute vehicles (e.g., multiple fuel cell vehicles, hybrid electric plug-in, all electric car, scooter and bicycle, vanpool and shuttle), and the large number and interactive focus of the participant vendors offering activities, free give-away items, free bicycle tune-ups, etc.

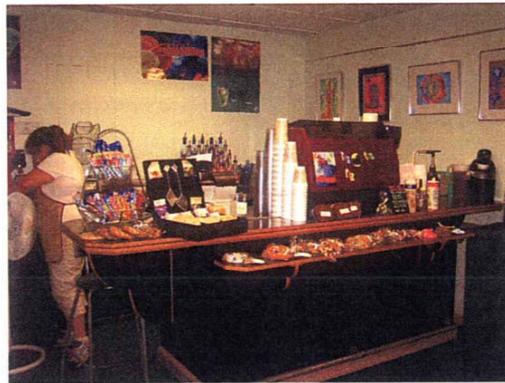
All of the promotions, preparation and coordinated activities resulted in a successful and highly visible event that is sure to reduce traffic congestion, parking demand and air quality impacts at the Mercy General Hospital campus and surrounding area.



Mercury General employees inquire about electric scooters

**14.0 CAFETERIA AND FOOD SERVICES**

Currently, Mercy General Hospital provides an on-site, full-service cafeteria (the Greenhouse) in the hospital building, food services (Plaza Café) in the physicians' office building, and various other food-vending services for employees. The availability of food services helps reduce employee reliance upon single occupant vehicles in anticipation of mid-day trips to restaurants. Employees can walk, bicycle, or carpool to the site with the knowledge that they can purchase a meal (morning, noon, or evening) on-site.



An enhanced Coffee Cafe will also be available in the new Alex G. Spanos Heart Center building.

#### 15.0 ON-SITE AMENITIES

To support alternative commuters, Mercy General Hospital provides convenient on-site amenities to reduce the need for mid-day or off-site employee travel. These on-site amenities include:

- ATM banking machine near the Greenhouse (cafeteria)
- Gift shop with sundry items, snacks, drinks, etc.
- Pharmacy – sunscreen, lip balm, other sundry items
- Postage resources (in gift shop)
- Commuter Information display board
- Free RT pass sales
- Cafeteria and vending food services
- Showers and lockers
- On-site Sales Events: books, hospital uniforms (scrubs), leather jackets, purses, wallets, jewelry, crafts, homemade desserts (10 to 12 per year)
- Laundry and dry-cleaning services

#### 16.0 FITNESS CENTER

The Mercy General Hospital provides free access for employees to the Cardiopulmonary Fitness Center. This fitness center is located in the Mercy Medical Plaza Lower Level and is shared with Pulmonary Rehabilitation. The center is open seven days per week and is available for employee use at various posted times. Equipment includes:

- Treadmills
- Stationary bicycles
- Elliptical trainer
- Stair stepper machines
- Nordic Track
- Free weights
- Multi-stack weight machine

The facility also provides TV with cable, stereo equipment and 24/7 closed circuit monitoring for safety. An employee wellness evaluation program is provided by the Choose Health Wellness program and includes a free health risk appraisal, aerobic fitness testing, flexibility and strength testing, cholesterol and body fat check, and staff recommendations.

Tai Chi classes are held each Friday from 9:00 a.m. to 9:30 a.m.

#### 17.0 ANNUAL REPORT

Each year an employee survey with a 50% plus one or better response rate will be conducted to gather quantitative data (e.g., mode split – how an employee commutes to work) and qualitative data (e.g., employee perception of the alternative transportation programs). This data will provide information about the status and progress of adherence to the 35% mode use goal. Results will be submitted to the City of Sacramento Planning Department each year on the occupancy anniversary date in order to receive the Transportation Management Certificate renewal. The process should include the following three elements:

- 1) Distribute (or conduct online) commuter surveys to all employees with accompanying instructions
- 2) Collect and summarize the information related to the Employer Commute Survey
- 3) Report the findings to the City of Sacramento for review and processing

**Failure to comply with reporting is a violation and shall be enforced pursuant to Section 18.12.060 of the Zoning Ordinance.**

#### 18.0 CONCLUSION

An essential element to realizing the trip reduction potential for the project is the comprehensive plan of trip reduction strategies and measures identified in this Plan, and the enhancement and growth of its existing Alternative Commute Program. The reorganization and consolidation of the commute program elements under one management entity via the ETC will help improve employee marketing and outreach activities. The significant and expanded transit benefits via shuttle service and 100% subsidies and prizes provide the necessary enhancements needed to build employee ridership. These critical factors will provide the synergism necessary to develop and maintain a 35% trip reduction for Mercy General Hospital.

**TSM Information Contacts**

## INFORMATION CONTACTS

**Breathe California of Sacramento - Emigrant Trails**  
909 12th Street  
Sacramento, California 95814  
Phone: (916) 444-5900  
Fax: (916) 444-6661  
Email: smichell@sacbreathe.org  
Website: <http://www.sacbreathe.org>  
*--Request information about Clean Air Month (May), and Bike to Work Week.*

**Sacramento Transportation Management Association (TMA)**  
Ms. Marilyn Bryant  
917 7th Street  
Sacramento, California 95814  
Phone: (916) 441-7074  
Fax: (916) 448-4909  
Website: <http://www.sacramento-tma.org>  
*--Newsletters and program information on website.*

**California Air Resource Board (CARB)**  
1001 I Street  
Sacramento, California 95814  
Phone: (916) 322-2990  
Website: <http://www.arb.ca.gov>  
*--Review site for updates in air quality.*

**Sacramento Regional Transit District (RT)**  
P.O. Box 2110  
Sacramento, California 95812-2110  
Customer Relations Phone: (916) 321-2850 or (916) 321-2877 (BUSS)  
Customer Relations Fax: (916) 444-0502  
Website: <http://www.sacrt.com>  
*--Request system maps and pocket timetables for light rail trains and applicable south area bus routes (currently; service may expand in area as time goes on). Also request First Time Rider coupons.*

**Sacramento Area Council of Government (SACOG)**  
1415 L Street, Suite 300  
Sacramento, California 95816-7056  
Phone: (916) 321-9000  
Fax: (916) 321-9551  
Website: <http://www.sacog.org>  
*--Request information about 511 program, Rideshare Matching and Bicycle Buddy matching.*

**Sacramento Metropolitan Air Quality Management District (SMAQMD)**  
777 12th Street, 3rd Floor  
Sacramento, California 95814-1908  
Community Education Phone: (916) 874-4888  
Community Education Fax: (916) 874-4899  
Website: <http://www.airquality.org>  
*--Request information about the Spare the Air programs.*



THE MERCY GENERAL HOSPITAL/  
SACRED HEART PARISH SCHOOL  
EXPANSION PROJECT

What's Proposed?

What are our Objections?

What Alternatives Do We Have?

Prepared and Presented By

Volunteers From

The East Sacramento Preservation Task Force

September 2007

TABLE OF CONTENTS

	<u>Page</u>
1. Introduction and Overview	1
• Who are we?	1
• What is this document and why are we presenting it?	1
• Our Four Key Points in Opposition	1
• Since we oppose the project, do we have an Alternative Plan?	2
2. What's Proposed by Mercy?	3
• A Summary of the Joint Mercy Hospital/Sacred Heart Parish School Project Proposal in Their Own Words (Exhibit 1)	
• What is required for Mercy to complete the project?	4
• What will the site look like if completed as planned?	5
3. What Are Our Objections?	6
• The Setting: The East Sacramento Community	6
• The History of Mercy Expansion	6
• The J Street site is too small for the project as proposed	6
• The project as proposed would violate the Medical Facilities, Residential Land Use and Streets and Roads sections of the City's General Plan	8
• Mercy owns other hospital sites that the Draft EIR identifies as feasible alternatives for the heart center project	12
• Centralizing medical facilities is bad planning, creates risks and leaves some communities underserved	14
4. What Alternatives Do We Have?	15
• A Summary of our alternative proposal	15
• An architect's rendering of our plan (Exhibit 12)	15
• Does our plan meet Mercy's and Sacred Heart School's objectives ?	15
• What conditions are we attaching to the proposal in exchange for our support?	15
5. Appendices	

## **Introduction and Overview**

### **Who are we?**

The East Sacramento Preservation Task Force (ESPTF) is a volunteer grass-roots neighborhood organization formed to protect and preserve the residential character of the East Sacramento neighborhood and community. We oppose the proposed expansion at Mercy General Hospital's campus located between H & J and 39th and 41st streets. The group is made up of concerned residents from all over East Sacramento. We are entirely a volunteer organization, supported by donations from members and friends.

### **What is this document and why are we presenting it?**

The proposal to expand Mercy General Hospital and move Sacred Heart Parish School is one of the most controversial and divisive proposals to face the residents of East Sacramento in a long while. It finds neighbors disagreeing with neighbors, and neighborhood groups waging active campaigns before the city government bodies to bring major changes to what is being proposed. ESPTF has been leading this opposition.

ESPTF has been called a group of NIMBY'S who are opposed to development without providing any reasonable alternatives or constructive objections. We are presenting this document to present in a reasonable, well documented fashion our objections to the project as proposed, and to present a reasonable alternative which would make us supporters of the project.

We also want to set the record straight. We are in no way NIMBYS. We accept the hospital as it exists and have for many, many years. We do object to these plans which call for a massive, out of scale new project and the demolition or moving of seven residential structures west of 39<sup>th</sup> street from the present campus in order to relocate a new Sacred Heart School. We think there is a better way. We will outline that later in this presentation.

### **Our Four Key Points in Opposition to the Project as Proposed**

Here are our four key points in opposition to the project as proposed. We will explain these four points in more detail in the remainder of this document:

1. The J Street campus is the wrong site for a regional medical center.
2. Destroying residences and increasing traffic in residential neighborhoods violates the City's General Plan.
3. There are feasible and reasonable alternate sites for a regional heart center that will better serve the community at large.
4. Centralizing medical facilities is bad planning, creates risks and leaves some communities underserved.

**Since we oppose the project as proposed, do we have an alternative plan?**

Yes we do. It is a plan that contains the project on the present campus. The plan would construct new school buildings precisely as drawn by Sacred Heart architects but on the present site to the east of 39<sup>th</sup> Street. It also proposes a new hospital building at the site of the proposed heart center, but reduced in size to replace the present buildings while still increasing the size of the hospital by ten percent.

This alternate plan will meet almost all of the objectives of the hospital and the school, but will be a plan we can support and allow construction to proceed. Of course we have some conditions to our approval, which we think are reasonable.

Our alternative plan is described in detail later in this document.