



Agenda City of Sacramento Design Commission

COMMISSION MEMBERS:

*Edmonds Chandler, Chair
David Nybo
Phyllis Newton, Vice Chair*

*James Fong, AIA, NCARB
Brian Sehnert, AIA, LEED*

*Vacancy
Todd Rudd*

CITY STAFF:

*William R. Crouch, AIA, NCARB, LEED AP, Urban Design Manager
Luis R. Sanchez, AIA, LEED AP, Senior Architect
Sheryl Patterson, Senior Deputy City Attorney*

*New City Hall
915 I Street, 1st Floor – Council Chambers*

March 16, 2011 – 5:30 P.M.

The City Design Commission was created by the City Council. Its powers and duties include: to develop and recommend to the City Council policies and programs in support of the urban design program, including but not limited to urban design policies for inclusion in the General Plan; develop standards for review, evaluate and submit comments on items that are not subject to review under Title 17, Chapter 17.132 of the City Code and that may affect the physical development of urban design in the city; to approve design projects of major significance and appeals of the Design Director per the Design Review Chapter, Title 17, Chapter 17.132, of the City Code.

NOTICE TO THE PUBLIC

You are welcomed and encouraged to participate in this meeting. Public comment is taken (3 minutes maximum) on items listed on the agenda when they are called. Public Comment on items not listed on the agenda will be heard at the end of the meeting as noted on the agenda. Comments on controversial items may be limited and large groups are encouraged to select 3-5 speakers to represent the opinion of the group.

Notice to Lobbyists: When addressing the Commission you must identify yourself as a lobbyist and announce the client/business/organization you are representing (City Code 2.15.160).

Speaker slips are located in the lobby of the hearing room and should be completed and submitted to the Commission Secretary.

Government Code 54950 (The Brown Act) requires that a brief description of each item to be transacted or discussed be posted at least 72 hours prior to a regular meeting. The City posts Agendas at City Hall as well as offsite meeting locations.

The order of agenda items is for reference; agenda items may be taken in any order deemed appropriate by the Commission. The agenda provides a general description and staff recommendations; however, the Commission may take action other than what is recommended. The agenda is available for public review on the Friday prior to the meeting. Hard copies of the agenda, synopsis, and staff reports are available from the Community Development Department at 300 Richards Blvd, 3rd Floor (.25 cents per page), during regular business hours or can be downloaded at www.cityofsacramento.org/dsd.

Meeting facilities are accessible to persons with disabilities. If you require special assistance to participate in the meeting, notify the Community Development Department at (916) 264-5011 at least 48 hours prior to the meeting.



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AGENDA

March 16, 2011

*New City Hall
915 I Street – 1st Floor, Council Chambers*

All items listed are heard and acted upon by the Design Commission unless otherwise noted.

Call to Order – 5:30 p.m.

Roll Call

Consent Calendar

All items listed under the Consent Calendar are considered and acted upon by one motion. Anyone may request that an item be removed for separate consideration.

1. **Approval of Minutes from January 12, 2011**

Location: Citywide

Recommendation: Approve Commission Minutes from January 12, 2011.

Contact: William R. Crouch, AIA, NCARB, LEED AP, Urban Design Manager, 916-808-8013

Public Hearings

Public hearings may be reordered by the Chair at the discretion of the Commission. If you challenge the decision of this Commission you may be limited to raising only those issues that are raised in this hearing or in written correspondence received by the Commission prior to the hearing.

2. **P10-058 2500 R Housing Project** (Continued from 1/12/11) (Noticed on 3/04/11 & 12/21/10)

Location: 1/2 block on R Street between 25th and 26th Street, District 4, 010-0043-001-0000,

Recommendation: Approve - Item A: Environmental Exemption (Per CEQA 15332);
Item B: Design Review for the development of 34 detached single family residential units in the Central City Design Review District.

Contact: Elise Gumm, LEEP AP, Associate Planner, 916-808-1927, Luis R. Sanchez, AIA, LEED AP, Senior Architect, 916-808-5957

3. **P10-089 Courtyard Condominiums** (Noticed on 3/04/11)

Location: 1918 24th Street, District 4, 010-0036-011-0000, 010-0036-012-0000

Recommendation: Approve - Item A: Environmental Exemption (Per CEQA 15332);
Item B: Design Review for a 6-unit multi-family development with separate 5-car garage building in the Central City Design Review District.

Contact: David Hung, Associate Planner, 916-808-5530; Luis R. Sanchez, AIA, LEED AP, Senior Architect, 916-808-5957

Staff Reports

Staff reports include oral presentations including those recommending Receive and File.

- 4. **IR11-041 The Gateway on Fremont Park** (Noticed on 3/04/11)
Location: 1601 16th Street, District 3, 006-0293-001-0000, 006-0293-002-0000, 006-0293-026-0000

Recommendation: Review and Comment- Design Review for a residential mixed use project consisting of 30 residential units with ground floor retail in the Central Core Design Review District. This is a Capitol Area Development Authority (CADA) project and is only subject to review and comment.

Contact: Evan Compton, Associate Planner, 916-808-5260; Luis R. Sanchez, AIA, LEED AP, Senior Architect, 916-808-5957

- 5. **Recognition of Chair** (Oral)

Location: Citywide

Recommendation: Recognition of outgoing chair, Brian Sehnert.

Contact: William R. Crouch, AIA, NCARB, LEED AP, Urban Design Manager, 916-808-8013

Public Comments- Matters Not on the Agenda

- 6. **To be announced.**

Questions, Ideas and Announcements of Commission Members

- 7. **To be announced.**

Adjournment

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Special Meeting Minutes City of Sacramento Design Commission

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COMMISSION MEMBERS:

*Edmonds Chandler, Vice Chair
David Nybo
Phyllis Newton*

*James Fong, AIA, NCARB
Brian Sehnert, AIA, LEED, Chair*

*H. Kit Miyamoto, SE
Todd Rudd*

CITY STAFF:

*William R. Crouch, AIA, NCARB, LEED AP, Urban Design Manager
Luis R. Sanchez, AIA, LEED AP, Senior Architect
Sheryl Patterson, Senior Deputy City Attorney*

*New City Hall
915 I Street, 1st Floor – Council Chambers
January 12, 2011 – 5:30 P.M.*

The City Design Commission was created by the City Council. Its powers and duties include: to develop and recommend to the City Council policies and programs in support of the urban design program, including but not limited to urban design policies for inclusion in the General Plan; develop standards for review, evaluate and submit comments on items that are not subject to review under Title 17, Chapter 17.132 of the City Code and that may affect the physical development of urban design in the city; to approve design projects of major significance and appeals of the Design Director per the Design Review Chapter, Title 17, Chapter 17.132, of the City Code.

NOTICE TO THE PUBLIC

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MINUTES

January 12, 2011

*New City Hall
915 I Street – 1st Floor, Council Chambers*

All items listed are heard and acted upon by the Design Commission unless otherwise noted.

Call to Order – 5:30 p.m.

Roll Call- All commissioners present except for Miyamoto.

Consent Calendar

All items listed under the Consent Calendar are considered and acted upon by one motion. Anyone may request that an item be removed for separate consideration.

1. **Approval of Minutes from November 17, 2010**

Location: Citywide

Recommendation: Approve Commission Minutes from November 17, 2010.

Contact: William R. Crouch, AIA, NCARB, LEED AP, Urban Design Manager, 916-808-8013

Action: Moved, seconded, and carried (Fong/Chandler; 6:0:0) to approve minutes.

2. **2011 Design Commission Calendar**

Location: Citywide

Recommendation: Approve the 2011 Design Commission meeting calendar as the third Wednesday of every month.

Contact: William R. Crouch, AIA, NCARB, LEED AP, Urban Design Manager, 916-808-8013

Action: Moved, seconded, and carried (Fong/Chandler; 6:0:0) to approve the calendar, with the exception of moving the December meeting date to December 14, 2011.

Public Hearings

Public hearings may be reordered by the Chair at the discretion of the Commission. If you challenge the decision of this Commission you may be limited to raising only those issues that are raised in this hearing or in written correspondence received by the Commission prior to the hearing.

3. **LR09-021 Northeast Line Implementation Plan** (Noticed on 12/21/10)

Location: Properties in the vicinity of the Globe, Arden/Del Paso and Royal Oaks light rail stations in the North Sacramento Community Plan Area, District 2

Recommendation: Forward Recommendation of Approval to City Council- Item A: Previously Adopted Environmental Impact Report; **Item B:** Amendments of the North Sacramento Design Guidelines, as part of the Northeast Line Implementation Plan.

Contact: Greg Sandlund, Associate Planner, 916-808-8931; Jim McDonald, Senior Planner, 808-5273

Action: Moved, seconded, and carried (Sehnert/Chandler; 5:0:1, Abstain-Newton) to forward recommendation of approval to City Council.

4. **M10-012 River District Design Review District and Urban Design Guidelines**
(Noticed on 12/07/10)
Location: South of American River, North of B Street, East of the Sacramento River, and West of 28th Street, Sacramento, California. (Multiple Parcels), Districts 1 and 3

Recommendation: Forward Recommendation of Approval to City Council- Item A: Environmental Impact Report; **Item B:** Final Recommendation to City Council for the Creation of the River District Design Review District and adoption of the River District Design Guidelines.

Contact: Evan Compton, Associate Planner, 916-808-5260; Stacia Cosgrove, Senior Planner, 916-808-7110

Public comments made by Betsy Weiland and Janet Baker.

Action: Moved, seconded, and carried (Sehnert/Newton; 6:0:0) to forward recommendation of approval to City Council.

5. **P10-058 2500 R Housing Project** (Noticed on 12/21/10)
Location: 1/2 block on R Street between 25th and 26th Street, 010-0043-001-0000, District 4

Recommendation: Approve - Item A: Environmental Exemption (Per CEQA 15332); **Item B:** Design Review for the construction of 34 pre-fabricated detached single family residential units.

Contact: Elise Gumm, LEEP AP, Associate Planner, 916-808-1927, Luis R. Sanchez, AIA, LEED AP, Senior Architect, 916-808-5957

Public comment made by Kate McBurney and Peter Manston.

Action: Moved, seconded, and carried (Nybo/Chandler; 6:0:0) to approve the environmental determination, indicate support for project and continue this item to February 16, 2011 to allow for the applicant to submit revised plans regarding roofing design and to provide more information on materials and landscaping .

Staff Reports

Staff reports include oral presentations including those recommending Receive and File.

6. **Annual Report- Urban Design Team**

Location: Citywide

Recommendation: Receive and file

Contact: William R. Crouch, AIA, NCARB, LEED AP, Urban Design Manager, 916-808-8013

Action: Received and filed.

7. **Election of the Chair and Vice-chair of the Design Commission for 2011**

Location: Citywide

Recommendation: Nomination and selection of Chair and Vice-Chair for 2011.

Contact: William R. Crouch, AIA, NCARB, LEED AP, Urban Design Manager, 916-808-8013

Action: Moved, seconded, and carried (Nybo/Fong; 6:0:0) to select Edmonds Chandler as Chair and Phyllis Newton as Vice-Chair of the Design Commission for 2011.

Public Comments- Matters Not on the Agenda

8. **None.**

Questions, Ideas and Announcements of Commission Members

9. **None.**

Adjournment – 8:45 PM



REPORT TO DESIGN COMMISSION City of Sacramento

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915 I Street, Sacramento, CA 95814-2671

PUBLIC HEARING
March 16, 2011

To: Members of the Design Commission

Subject **2500 R Housing Project (P10-058)**

A request to subdivide one undeveloped parcel, totaling 1.19 acres, into 34 lots for the construction of 34 detached single-family homes and four common lots for landscaping and vehicle access, in the Residential Mixed Use (RMX-SPD) zone within the R Street Corridor Special Planning District.

- A. Environmental Determination: Exempt per 15332, Infill Development;
- B. Design Review request for the construction of 34 pre-fabricated detached single-family dwellings in the Residential Mixed Use (RMX-SPD) zone within the R Street Corridor Special Planning District.

Location

Half Block on R Street between 25th and 26th Streets
Assessor's Parcel Number: 010-0043-001-0000
Council District 4
R Street Corridor Special Planning District & Central City Design Review District

Recommendation

Staff recommends the Design Commission approve the request based on the findings and subject to the conditions listed in Attachment 1. The Design Commission has final approval authority over items A&B above, and its decision is appealable to City Council.

Contact Elise Gumm, LEED AP, Associate Planner, (916) 808-1927;
Luis R. Sanchez, AIA, LEED AP, Senior Architect (916) 808-5957

Applicant Pacific Housing Inc., c/o: Mark Wiese, (916) 638-5200
2115 J Street, Suite 201, Sacramento, CA 95816

Owner 25th/R Partners Limited Partnership, (916) 443-3797
1722 3rd Street, Suite 202 Sacramento, CA 95811

Summary

This project was previously heard at the January 12, 2011 Design Commission Hearing. The development consists of 34 detached Single Family Residential units and a large landscaped common area on an approximately 1.19 acre parcel within the R Street Corridor Special Planning District General Commercial (C-2-SPD) zone. Each unit has its own single car garage and all garages are accessible from public streets, public alley, or the private driveways. The project was heard and unanimously approved by the Planning Commission on January 13, 2011.

Due to the neighbors' opposition on the design of the project, the Design Commission continued the project and requested the applicant to re-examine the design of the buildings so they are more consistent with the historic homes in the neighborhood. The Design Commission made the following statements on January 12, 2011 in regards to the proposed project. As a result, the applicant revised the project to be conventional site built homes rather than its original proposal of pre-fabricated houses.

1. Roof Lines

Commissioners commented on the butterfly roof and the disadvantages of this roof design. In addition, commissioners and neighbors would like the roof pitches to be more comparable with the historic homes in the surrounding area. The original proposed roof pitches are lower compared with other historic homes.

The applicant changed all units to 8:12 pitches from the previous design of 5:12 pitches. The homes on every lot are oriented with the roofs sloping to the south for optimal solar exposure as well as consistently matching the surrounding historic homes.

2. Architectural Details

Commissioners commented to provide more architectural details on the exterior elevations in order to be consistent with the historic homes in the surrounding neighborhood.

The applicant changed the original proposal of pre-fabricated buildings to conventional site built product, so there are more architectural details, such as adding balconies, additional details on windows sill and trims, and glazing on doors.

3. Materials

Commissioners commented to provide various materials on the elevations. The original pre-fabricated project only consisted of fiber cement siding.

The applicant modified the project by using brick wainscot and column bases along public streets, adding board and batten accent siding in addition to the cement board siding.

4. Landscaping

Commissioners required a comprehensive landscaping plan to be reviewed instead of concept plan that was originally submitted.

The applicant revised the landscaping plans to provide comprehensive details on the landscaping plan, including layout of the common area-Lot A, selection of species, and different paving layouts for the complex.

Staff believes the intent of the Design Commission's comments have been largely addressed through the applicant's response and updated plans. Please see the Staff Recommendations to Commission below for further elaboration.

The project has received Planning Commission approval of entitlements for Environmental, Tentative Map, a Special Permit for the construction of 34 detached single family alternative ownership housing units. The Design Commission action is the final action unless the project is appealed to the City Council.

Table 1: Project Information

General Plan designation: Urban Corridor Low
Existing zoning of site: Residential Mixed Use zone (RMX-SPD)
Existing use of site: Vacant
Property dimensions/area: 1.19± acres; 320'x160'
Building square footage: 47,600 ± square feet
Building height: 18'± to top plate, 23'± to top of pitch.
Exterior building materials: Pre-fabricated buildings. Fiber cement horizontal lap siding, metal panels, sustainable wood, and sustainable composite shingles.

Public/Neighborhood Outreach and Comments

Staff has routed the application package to Walk Sacramento, Sacramento Housing Alliance, Midtown Neighborhood Association, Capitol Area R Street Association, and Newton Booth Neighborhoods Association (NBNA) on September 1, 2010 and the project revision on October 28, 2010. Staff also mailed hearing notices to all property owners within the 500 foot radius on January 05, 2011 for the Design Commission public hearing and the Planning Commission public hearing. The applicant has also contacted adjacent property owners and neighborhood associations during the initial planning and design phase, and has received general support for the project. Staff received comments from Walk Sacramento, and the letter is attached for reference (Attachment 4). The applicant has incorporated some of the comments from Walk Sacramento into its revised site plan. Staff has not received any opposition comments from any neighborhood associations and property owners at the time of writing the staff report.

At the previous Design Commission and Planning Commission Hearing, some neighbors spoke about the project and want to see more traditional design that is more consistent with the historic homes in the neighborhood. The applicant and staff talked to the

neighbors after the meetings and largely addressed neighbors' concerns in the revised plans.

Environmental Considerations

The Community Development Department, Environmental Planning Services Division has reviewed this project and determined that this is exempt from the provisions of the California Environmental Quality Act (CEQA) Section 15332, In-fill Development Projects. The project consists of the construction of 34 residential units that occurs in an urban area served by utilities and public services, on a site that is less than 5 acres, has no habitat value, is consistent with all applicable land uses, and would not result in any significant effects to traffic, noise, air, or water quality.

Sustainability Considerations

The City has adopted a Sustainability Master Plan to complement the City's General Plan. This was done to ensure that the City set the standard for the practices of sustainability within its own organization as well as becoming a model for any construction projects within the City. Projects should consider the following goals adopted by the City as projects are proposed within the City: reduce consumption of materials, encourage the reuse and local recycling of materials, reduce the use of toxic materials; establish and continuously improve "green" building standards for both residential and commercial development--new and remodeled, reduce dependence on the private automobile by working with community partners to provide efficient and accessible public transit and transit supportive land uses, reduce long commutes by providing a wide array of transportation and housing choices near jobs for a balanced, healthy city; improve the health of residents through access to a diverse mix of wellness activities and locally produced food, promote "greening" and "gardening" within the City, create "Healthy Urban Environments" through Restorative Redevelopment, and maintain and expand the urban forest.

Although the project was changed from pre-fabricated buildings to conventional site building product, but it is still keeping many sustainable features as original proposed. The revised project has been designed to be LEED for Homes Certified, and listed below are some of the features that the project will include:

- Photovoltaic power with lithium-ion energy storage
- Ultra-efficient building envelope
- High efficiency lighting - LED and CFL
- EnergyStar™ appliances
- Environmentally preferred products throughout
- High efficiency HVAC
- Drought tolerant landscaping

The proposed project is consistent with the goals of the 2030 General Plan and the vision of the City Council for Sacramento. It is located in an urban area that is in proximity to public transportation and public services, such as hospitals, library, etc. It

also will demonstrate to the Sacramento region how the residential project incorporated sustainable features to create a “Green” community.

Policy Considerations

The 2030 General Plan Update was adopted by City Council on March 3, 2009. The 2030 General Plan’s goals, policies, and implementation programs define a roadmap to achieving Sacramento’s vision to be the most livable city in America. The 2030 General Plan updated designation of the subject site is Urban Corridor Low, which provides for a development pattern with moderate lot coverage, limited side yard setbacks, and buildings sited up to the corridor to create a consistent street wall. Building heights vary from 2-4 stories; and other characteristics, such as building orientation, frontage-type, access, parking, streetscape, and open space, are consistent with the R Street Corridor Special Planning District Design Guidelines.

General Plan

The 2030 General Plan has identified goals and policies under the Land Use and Urban Design Element. Some of the goals and policies supported by this project are:

- **Leading Infill Growth.** The City shall facilitate infill development through active leadership and the strategic provision of infrastructure and services and supporting land uses. (Policy LU 1.1.4)
- **Infill Development.** The City shall promote and provide incentives (e.g., focused infill planning, zoning/rezoning, revised regulations, provision of infrastructure) for infill development, redevelopment, mining reuse, and growth in existing urbanized areas to enhance community character, optimize City investments in infrastructure and community facilities, support increased transit use, promote pedestrian- and bicycle-friendly neighborhoods, increase housing diversity, ensure integrity of historic districts, and enhance retail viability. (Policy LU 1.1.5)
- **Complete and Well-Structured Neighborhoods.** The City shall promote the design of complete and well-structured neighborhoods whose physical layout and land use mix promote walking to services, biking, and transit use; foster community pride; enhance neighborhood identity; ensure public safety; are family-friendly and address the needs of all ages and abilities. (Policy LU 2.1.3)
- **Neighborhood Enhancement.** The City shall promote infill development, redevelopment, rehabilitation, and reuse efforts that contribute positively (e.g., architectural design) to existing neighborhoods and surrounding areas. (Policy LU 2.1.6)
- **Unique Sense of Place.** The City shall promote quality site, architectural and landscape design that incorporates those qualities and characteristics that make Sacramento desirable and memorable including: walkable blocks, distinctive parks and open spaces, tree-lined streets, and varied architectural styles. (Policy LU 2.4.1)

- **Sustainable Development Patterns.** The City shall promote compact development patterns, mixed use, and higher-development intensities that use land efficiently; reduce pollution and automobile dependence and the expenditure of energy and other resources; and facilitate walking, bicycling, and transit use. (Policy LU 2.6.1)
- **Walkable Blocks.** The City shall require new development and redevelopment projects to create walkable, pedestrian scaled blocks, publicly accessible mid-block and alley pedestrian routes where appropriate, and sidewalks appropriately scaled for the anticipated pedestrian use. (Policy LU 2.7.6)
- **R Street Housing Development.** The City shall work with SHRA, the Department of General Services, and Capitol Area Development Authority (CADA) to establish the R Street Corridor as a priority location for future housing opportunities in conjunction with the Capitol Area Plan.

This proposed project is consistent with the R Street Corridor Special Planning District Design Guidelines and meets the 2030 General Plan goals and policies related to use infill urban site. The proposed project establishes a unique sense of place for its residents within the transit corridor and in proximity with the vibrant midtown neighborhood.

Design Policy Considerations

1. **Context:** Promote transit friendly site design at the four light rail stations. Foster opportunities for alley development and a variety of architectural styles in keeping with the surrounding neighborhood.
2. **Character:** maintaining the historic sense of shared space between pedestrians, bicycles and vehicular traffic in existing areas, and where possible, extending it to the remaining areas in the Corridor.
3. **Scale:** Respect the neighborhood context and scale of existing neighborhoods through appropriate setbacks, massing and height limits.
4. **Pedestrian:** Create a pedestrian-friendly environment within the corridor through neighborhood scale streetscape improvements.
5. **Materials:** Promote efforts to utilize high quality building materials, detailing & landscaping.
6. **Integrated Services:** absorb minimal additional through traffic and maintain its comfortable pedestrian environment.
7. **Sustainable Design:** Promote sustainability in building design, construction and operation.

Design Guidelines Considerations

1. **Parks and Plazas:** Mid block mini parks and public facilities should be provided to serve residents, transit patrons and workers in the corridor and neighboring areas.
2. **Parking:** Alley access to parking structures is encouraged.

3. **Massing:** Mass and height of the transit-oriented mixed use development should step down and transition to the smaller scale context of the existing neighborhood
4. **Pedestrian Connection:** Walkways should be short and direct from entrance to entrance between adjacent developments and from buildings to adjacent transit stops.
5. **Fences:** Walls and fences which lengthen distances between main entrances of adjacent commercial or multi-family residential structures are discouraged;
6. **Windows and Detailing:** Large windows and protective awnings or overhangs on building facades that face sidewalks are encouraged;
7. **Walkways:** Pedestrian walkways should be constructed of some sort of alternative paving materials (i.e., stepping stones, pavers);

Project Design

The proposed residential project is located on a half block on R Street, is facing to an existing warehouse building, adjacent to similar small detached residential units, and in proximity to existing light rail stations. The proposed site design is generally consistent with the design principles and guidelines of the R Street Corridor Special Planning District. This section of the R Street Corridor is outside of the R Street Preservation District that contains more industrial uses and historic homes. The site is surrounded by small commercial uses and newer residential units. All proposed units are fronting public streets or its internal pedestrian path. Majority of the garages are accessed through alley or its private driveway courts, except the 6 units that face R Street. Each unit has its own yard area, either at the front of the units facing the pedestrian path or at the side of the units.

Staff Evaluation

Staff is supportive of this project and recommends some refinements to the overall design as noted below. Staff has continued to partner and coordinate the Design Commission comments with the design team. The design team has largely addressed the Design Commission comments and provided responsive revised plans. Staff requests that the Design Commission approve the proposed project, and allow staff to finalize some minor details, which the Design staff believes require further attention by the design team.

Site Comments

1. The overall site has not been modified from the previous submittal because it is approved by the Planning Commission and the overall setbacks are adequate and supported by staff.
2. Comprehensive landscaping plan has been provided for the site based upon the Design Commission comments. Staff supports the overall design provided on the plans and the species proposed on the plans. Staff recommends the Design Commission approve it and allow staff to work on details prior to obtaining building permits.

3. Staff would like further clarification where sidewalk light poles and exterior building lights are located and the design of the exterior lighting fixtures. Staff conditioned the applicant to provide cut sheets of the exterior light fixtures to the Urban Design Manager and obtain approval prior to obtaining building permits.
4. Staff would like further clarification on various paving materials presented on the plans. Staff conditioned the applicant to provide details and specification of the various paving materials to the Urban Design Manager and obtain approval prior to obtaining building permits.

Building Comments

1. The architectural design was completely changed from previous pre-fabricated contemporary design to the current proposal of more traditional look of a conventional site built units. The architectural design is sensitive to adjacent homes and other uses, which also addressed Commission and neighbors' previous comments. Their massing and scale are suitable in this section of the R Street Corridor, and its design meets the R Street Corridor Special Planning District Design Guidelines. The proposed building height is 2 to 3 story, which is below the maximum height limit of 45 feet and is consistent with the surrounding residential units.
2. Two corner units were designed to be more related to the industrial nature of the R Street Corridor. The monumental brick volumes with recessed windows create landmarks for the neighborhood, while the third story loft steps back from the parapet to ease the scale at the corner. Staff recommends the continued use of brick at bases and columns of other units along public streets, so it provides various materials on the units and creates a unifying feature to the complex.
3. As per previous Commission comments, the revised elevations provide more traditional roof lines for all units except the two corner ones. The relatively high pitched gable roof is more comparable with the adjacent historic homes. It also decreases the three-story massing on R Street. The proposed three-story corner units with step back lofts are also supported by Staff.
4. The applicant has strategically designed the roof in order to place the solar roof panels to be visible from alley and interior courts only, which helps to minimize the view from public streets. Staff recommends the applicant select a comparable color for the composition roofing that will also help to minimize the visibility of the solar panels.
5. The design team has provided a couple pop outs at the street elevations of the corner units. Staff supports this feature as it adds further articulation and interest at the corner as requested by the Design Commission. Staff recommends the pop outs that are not brick to be painted in a darker color to contrast with the body

color. Final color/material shall be reviewed and approved by the Urban Design Manager prior to obtaining building permits.

6. Brick wainscot, panels, and board and batten accent siding are proposed on the revised plans. Staff supports the changes in material as it further articulates the street façades.
7. Revised plans are including architectural details on the building elevations. Glazing on garage doors that are facing R Street, pop outs and inset windows, gable end vent details, and inset windows with brick at the corners units, are supported by staff. Staff recommends the applicant provide cut sheets for unit front doors for review and approval by the Urban Design Manager. These doors shall be integrated with the exterior elevations.
8. The project uses various colors to differentiate the similar building facades, which provides an interesting streetscape to the R Street Corridor. Staff recommends the Green and Grey colors on the proposed material board to be warmer shades. The applicant shall provide final color samples to be reviewed and approved by the Urban Design Manager prior to obtaining the building permits.
9. The revised plans provide various elevations for one floor plan, which will help to articulate and interesting street facades. The applicant shall provide a site plan that indicates which unit goes with which elevation and colors for each lot to be reviewed and approved by the Urban Design Manager prior to obtaining building permits.

Recommendation

Staff requests that the Design Commission approve the proposed project subject to the final conditions of approval.

Respectfully submitted by: 
ELISE GUMM, LEED AP
Associate Planner

Approved by: 
LUIS R. SANCHEZ, AIA, LEED AP
Senior Planner

Recommendation Approved:

WILLIAM CROUCH, AIA, FRAIA,
NCARB, CBO, Casp, LEED (AP)
Urban Design Manager

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Attachment 1 Recommended Findings of Fact and Conditions of Approval

Findings of Fact

- A. Environmental Determination:** Exemption - Based on the determination and recommendation of the City's Environmental Planning Services Manager and the oral and documentary evidence received at the hearing on the Project, the Planning Commission finds that the Project is exempt from review under Section **15332, In-Fill Development Projects** of the California Environmental Quality Act (CEQA) Guidelines as follows:

The project consists of the construction of a building that occurs in an urban area served by utilities and public services, on a site that is less than 5 acres, has no habitat value, is consistent with all applicable land uses, and would not result in any significant effects to traffic, noise, air, or water quality.

- B. The Design Review** request to develop 34 detached residential units with an approximately 2,700 square foot common recreational area is approved, subject to the following Findings of Fact and Conditions of Approval:

1. The project is based upon sound principles of land use, in that the proposed residential use is allowed in the Residential Mixed Use (RMX-SPD) zone within the R Street Corridor Special Planning District, and the project generally meets the R Street Corridor Special Planning District Design Guidelines.
2. The proposed use will be consistent with the applicable policies of the City of Sacramento 2030 General Plan.
3. The project, as conditioned, will complement structures in the vicinity, and conforms to the design criteria set forth by the Design Commission.

Conditions of Approval

The Design Review request to develop 34 detached residential units with an approximately 2,700 square foot common recreational area is hereby approved subject to the following conditions:

- A. The design of the site (see plans attached) is hereby approved subject to the following conditions. These conditions must be met prior to the building permit submittal:**

1. The buildings shall be sited as indicated in the report and exhibits.

2. Auto access and site layout shall be as indicated in the report and exhibits. The Applicant shall coordinate with the appropriate agencies regarding alley improvements associated with the overall project.
3. The project shall have building entries and setbacks as indicated in the exhibits.
4. The project shall include landscaping elements as indicated on the report and exhibits. Automatic irrigation shall be provided for all planting and landscaping. **Final landscape plans and details shall be provided for review and approval by Design Review staff prior to Building Permit submittal.**
5. Large canopy street trees along 25th Street, 26th Street, and R Street, shall be provided as indicated in the report, exhibits, and per Urban Forest requirements.
6. Applicant shall provide a site lighting plan for review and approval by Design Review staff prior to submitting for Building Permit. Exterior lighting style and design shall be compatible and consistent with the building design, and the site should be adequately illuminated for safety and security with a minimum 1.0 foot candle throughout. Street lighting shall be provided per Development Engineering standards and reviewed by Development Engineering and Design Review staff prior to Building Permit submittal. Appropriate lighting should light up wall surfaces and/or landscape areas. **The applicant shall submit all site light fixtures cut sheets and plan locations for review and approval by Design Review staff prior to submitting for Building Permit.**
7. Outdoor amenities at the common area shall be reviewed and approved by Design Review staff prior to Building Permit issuance.
8. Any outdoor furniture proposed for exterior seating shall be provided to Design Review Staff for review and approval prior to Building Permit issuance.

B. The design of the new building (see plans attached) is hereby approved subject to the following conditions:

9. The design of the building shall be as indicated in the report and exhibits.
10. Final heights and massing shall be as indicated in the report and exhibits.
11. The building elevations shall have a consistency of detail and quality as indicated in the report and exhibits.

12. All building materials shall have a consistency of detail and quality as indicated in the report and exhibits.
13. All window sills and trims shall be made of wood and painted.
14. The corner units shall provide the inset windows as indicated in the report and exhibits.
15. Provide decorative glazing panels at garage doors and front doors. **Final cut sheets shall be reviewed and approved by Design Review staff prior to Building Permit submittal.**
16. No roof mounted mechanical units shall be constructed. All mechanical units shall be screened and not visible from public areas.
17. Exterior lighting style and design shall be compatible and complementary to the building design. **Final building lighting plans and light fixture cut sheets shall be reviewed and approved by Design Review staff prior to Building Permit submittal.**
18. **Final selections of color and materials shall be reviewed and approved by the Design Review staff prior to Building Permit submittal.**

C. General Conditions

19. All final details affecting the exterior building design that are not determined at the time of the Design Commission final review shall be reviewed and approved by Design Review staff prior to Building Permit submittal.
20. All other notes and drawings on the final plans as submitted by the applicant are deemed conditions of approval. Any changes to the final set of plans stamped by Design Review staff shall be subject to review and approval prior to Building Permit submittal. Applicant shall comply with all current building code requirements.
21. Any modification to the final approved design plans are subject to review and approval by the Design Commission.
22. All required new and revised plans shall be submitted for review and approval by Design Review staff prior to building permit submittal. A set of the appropriate plans (reduced to 11 x 17 set) along with a Letter of Compliance indicating how the project is in compliance with each Condition of Approval with detailed sheet references shall be submitted

directly to Design Review Staff two weeks prior to Building Permit submittal.

23. All necessary entitlements and City Requirements shall be approved by the Planning Commission (P10-058), Development Engineering, Urban Forest and Utilities, prior to final Design Review sign-off of plans.
24. Development of this site shall be in compliance with all conditions of approval by Planning Commission (P10-058).
25. The approval shall be deemed automatically revoked unless required permits have been issued and construction begun within three years of the date of the approval. Prior to expiration, an extension of time may be granted by the Design Commission upon written request of the applicant.
26. The Design Commission decision may be appealed to City Council. Appeals must be filed within 10 calendar days of written notice of the Design Commission action.
27. Building permit shall not be issued until the expiration of the 10 day appeal period. If an appeal is filed, no permit shall be issued until final approval is received.
28. Final occupancy shall be subject to approval and may involve an on-site inspection by Design Review Staff.
29. The Record of Decision shall be scanned and inserted into the final set as a general sheet to be submitted for building permit.
30. A signed copy of the Affidavit of Zoning Code Development Standards shall be scanned and inserted into the final set as a general sheet to be submitted for building permit.

Exhibit A

Cover Sheet



4132 C Street
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ellis.architects@yahoo.com



2500 R
25TH AND R STREETS
SACRAMENTO, CA 95811
APN # 010-0043-001-0000

DATE: 02/22/11

DESIGN REVIEW
RE-SUBMITTAL

SHEET TITLE:
COVER SHEET

SHEET NO.
A1

2500 R
SACRAMENTO, CA 95811
DESIGN REVIEW RE-SUBMITTAL
02/22/11

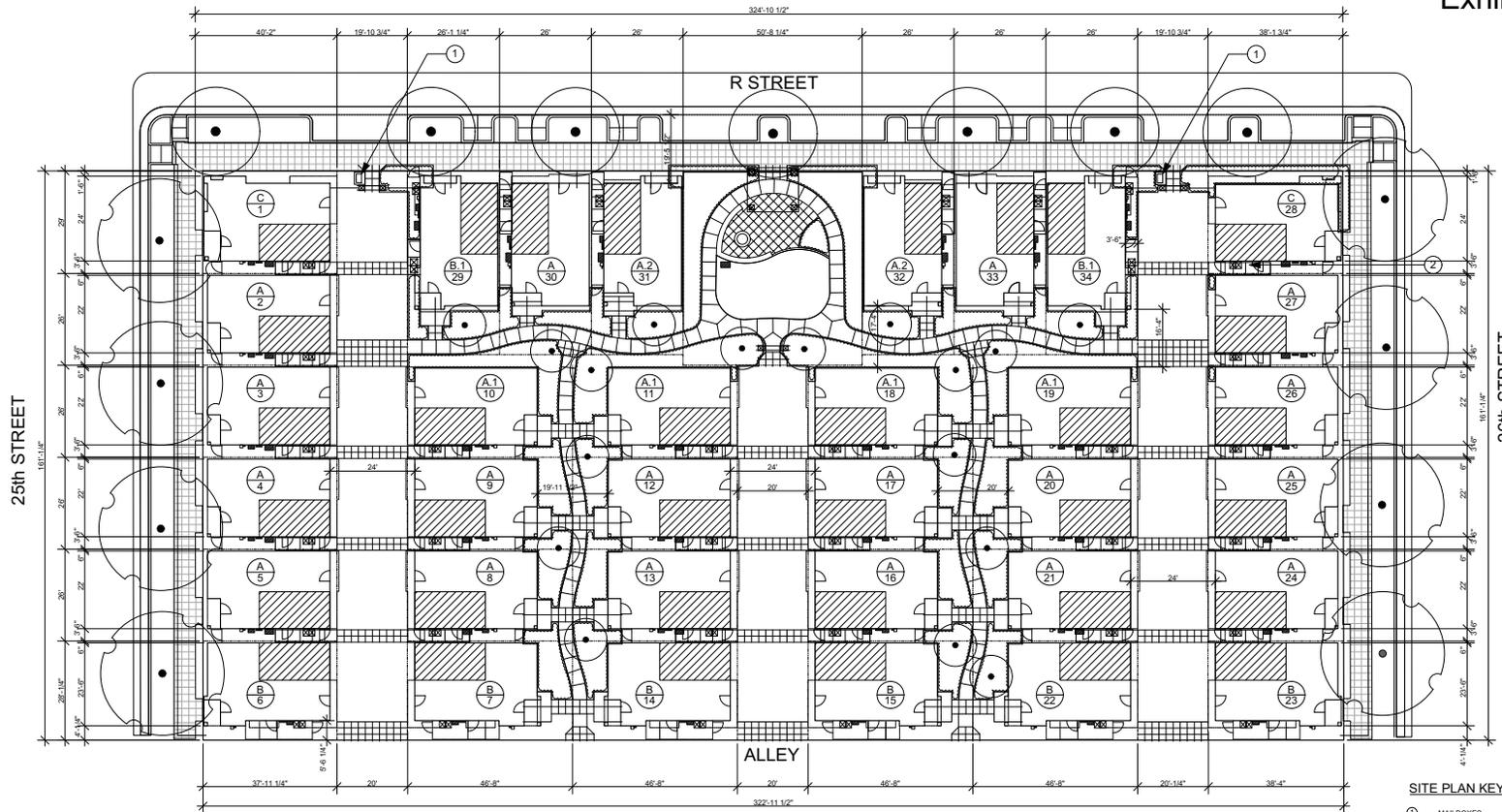
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Exhibit C Site Plan



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SITE PLAN KEYNOTES

- ① MAILBOXES
- ② TRASH/RECYCLING BINS TYPICAL EACH UNIT

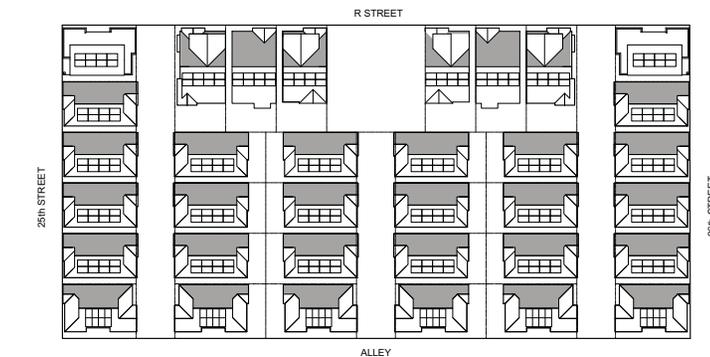
LEGEND

- [Hatched Box] LOCATION OF ONE-CAR GARAGE
- [Circle with X] UNIT TYPE LOT NUMBER
- [Grid Box] SOLAR ROOF PANELS

PROJECT DATA

UNIT TYPE	# OF UNITS	DESCRIPTION	AREA
A	18	Standard unit - 3 bedroom, 2 1/2 bath	1,263
A.1	4	Units with side windows on passio	1,400
A.2	2	R Street unit with 3rd floor loft	1,669
B	6	R-S alley units	1,288
B.1	2	B Street unit with 3rd floor loft	1,710
C	2	Corner unit with 3rd floor loft	1,693
Total Units	34		

② **SITE PLAN**
 SCALE: 1/16" = 1'-0"



① **ROOF/SITE PLAN DIAGRAM**
 SCALE: 1" = 30'



DATE: 02/22/11

DESIGN REVIEW
 RE-SUBMITTAL

SHEET TITLE:
 SITE PLAN

SHEET NO.
A2

Exhibit D Streetscape Elevations



R STREET ELEVATION

SCALE: 3/32" = 1'

SCALE: 0' 5' 10' 20'



25th STREET ELEVATION

SCALE: 3/32" = 1'

SCALE: 0' 5' 10' 20'



26th STREET ELEVATION

SCALE: 3/32" = 1'

SCALE: 0' 5' 10' 20'



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DESIGN REVIEW
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SHEET TITLE:
STREETSCAPE
ELEVATIONS

SHEET NO.
A3

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Exhibit D Streetscape Elevations



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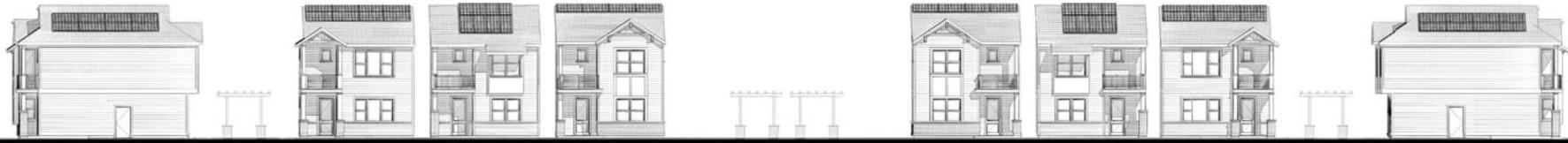
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DESIGN REVIEW
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SHEET TITLE:
 INTERIOR
 STREETScape
 ELEVATIONS

SHEET NO.
A4



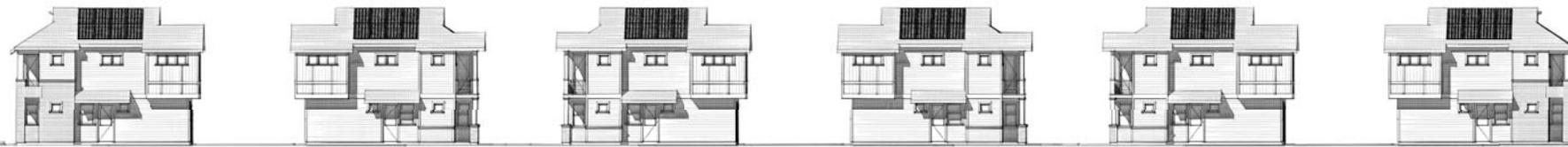
VIEW NORTH AT NORTH PASEO
 SCALE: 3/32" = 1'

SCALE: 0' 5' 10' 20'



VIEW EAST AT PRIVATE ALLEY - LOT B
 SCALE: 3/32" = 1'

SCALE: 0' 5' 10' 20'



VIEW NORTH AT R-S ALLEY
 SCALE: 3/32" = 1'

SCALE: 0' 5' 10' 20'

Exhibit E Enlarged Elevations



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ENLARGED PARTIAL 26TH STREET ELEVATION



GABLE END VENT DETAIL



FIBER CEMENT SIDING -
 SMOOTH LAP BOARDS,
 PAINTED



ORIEL WINDOW
 DETAIL - FIBER
 CEMENT SIDING -
 SMOOTH LAP
 BOARDS AND
 PANELS, PAINTED



WOOD
 COMPOSITE
 WINDOWS -
 ANDERSEN 100
 SERIES, COLOR
 "WHITE"



ASPHALT COMPOSITION
 ROOFING - TIMBERLINE
 COOL-SERIES, COLOR
 "COOL WEATHERED
 WOOD"



BRICK - H.C. MUDDOX
 COLOR "CALIFORNIA
 HANDMOLD"

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DATE: 02/22/11

DESIGN REVIEW
 RE-SUBMITTAL

SHEET TITLE:
 ENLARGED
 ELEVATIONS

SHEET NO.
A5

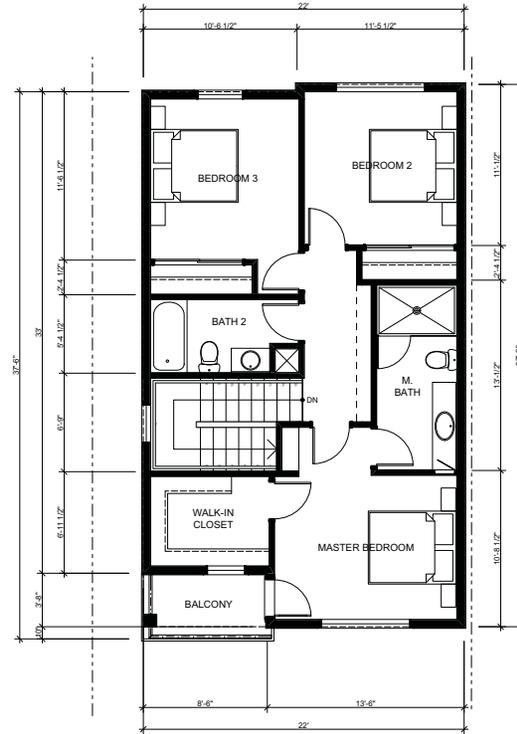
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Exhibit F Unit A

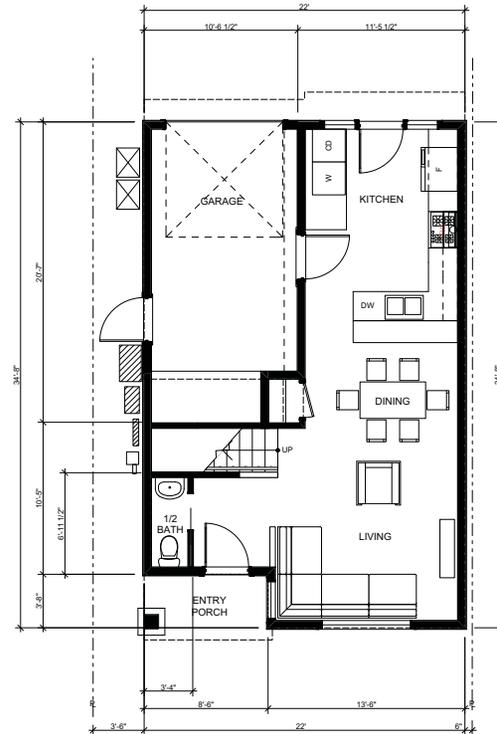


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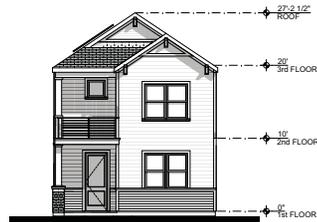
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 SCALE: 1/4" = 1'-0"



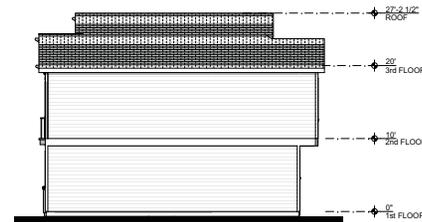
1 UNIT A - FIRST FLOOR PLAN (551 S.F.)
 SCALE: 1/4" = 1'-0"



7 UNIT A - SIDE ELEVATION
 SCALE: 1/8" = 1'-0"



6 UNIT A - FRONT ELEVATION
 SCALE: 1/8" = 1'-0"



5 UNIT A - SIDE ELEVATION
 SCALE: 1/8" = 1'-0"



4 UNIT A - REAR ELEVATION
 SCALE: 1/8" = 1'-0"

DATE: 02/22/11

DESIGN REVIEW
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SHEET TITLE:
 UNIT A FLOOR
 PLANS AND
 ELEVATIONS

SHEET NO.

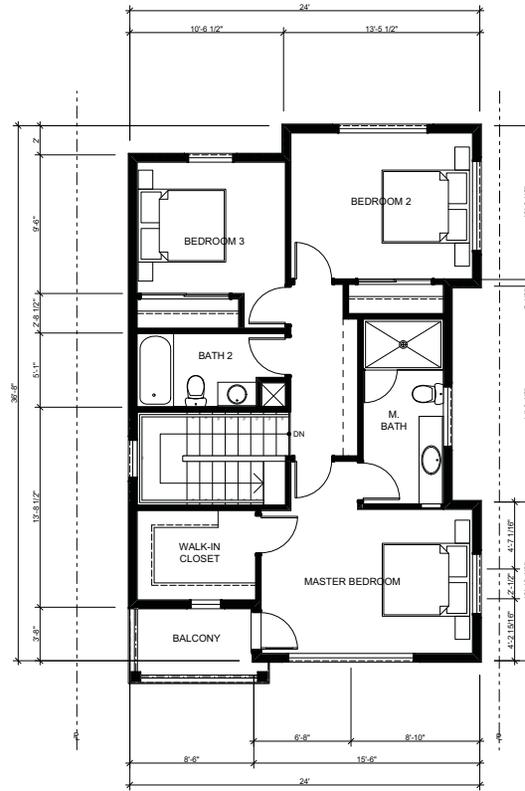
A6.1

Exhibit G Unit A.1

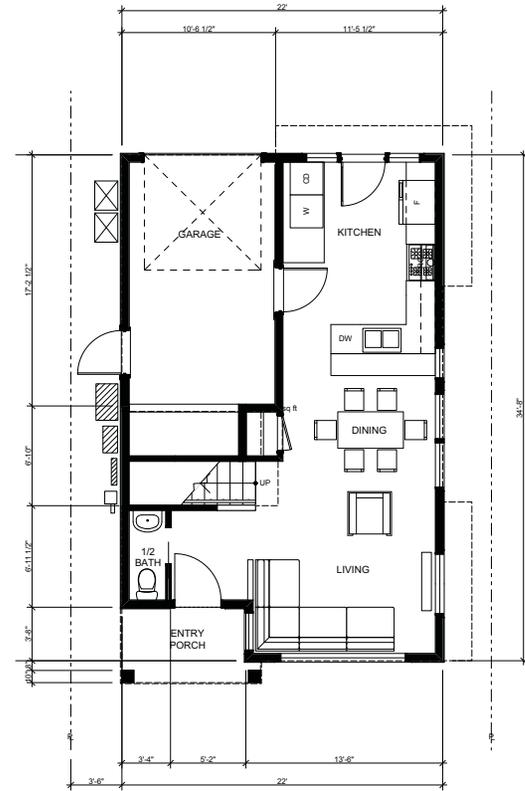


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3 UNIT A.1 - SECOND FLOOR PLAN (665 S.F.)
 SCALE: 1/4" = 1'-0"



2 UNIT A.1 - FIRST FLOOR PLAN (735 S.F.)
 SCALE: 1/4" = 1'-0"



7 UNIT A.1 - SIDE ELEVATION
 SCALE: 1/8" = 1'-0"



6 UNIT A.1 - FRONT ELEVATION
 SCALE: 1/8" = 1'-0"



5 UNIT A.1 - SIDE ELEVATION
 SCALE: 1/8" = 1'-0"



4 UNIT A.1 - REAR ELEVATION
 SCALE: 1/8" = 1'-0"

DATE: 02/22/11

DESIGN REVIEW
 RE-SUBMITTAL

SHEET TITLE:
 UNIT A.1 FLOOR
 PLANS AND
 ELEVATIONS

SHEET NO.

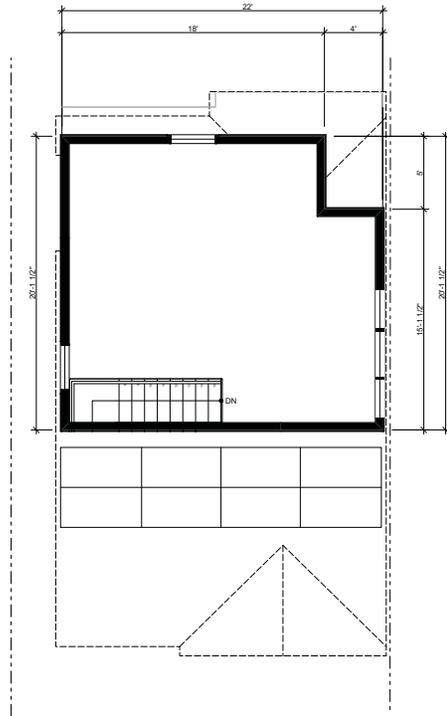
A6.2

Exhibit H Unit A.2

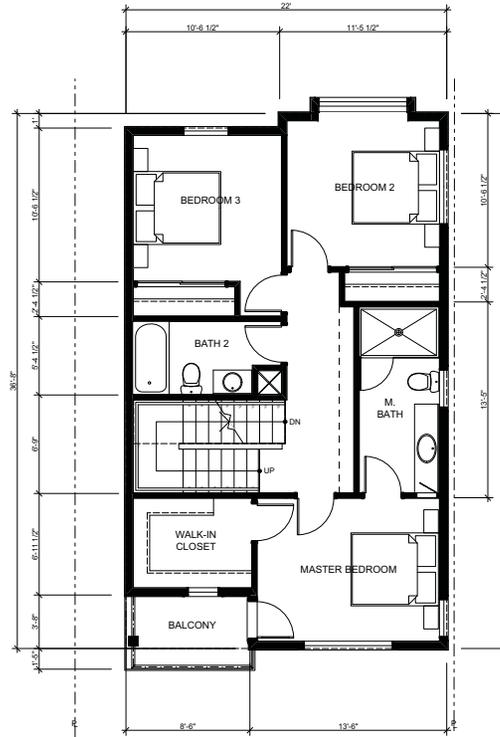


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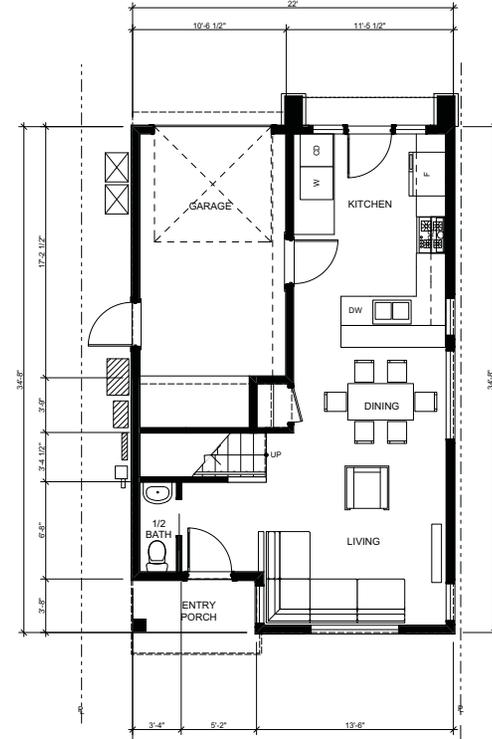
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3 UNIT A.2 - THIRD FLOOR PLAN (422 S.F.)
 SCALE: 1/4" = 1'-0"



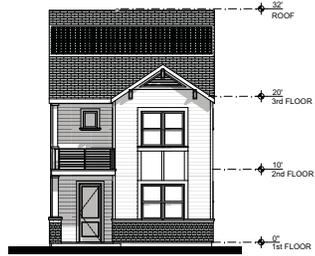
1 UNIT A.2 - SECOND FLOOR PLAN (714 S.F.)
 SCALE: 1/4" = 1'-0"



2 UNIT A.2 - FIRST FLOOR PLAN (533 S.F.)
 SCALE: 1/4" = 1'-0"



7 UNIT A.2 - SIDE ELEVATION
 SCALE: 1/8" = 1'-0"



6 UNIT A.2 - FRONT ELEVATION
 SCALE: 1/8" = 1'-0"



5 UNIT A.2 - SIDE ELEVATION
 SCALE: 1/8" = 1'-0"



4 UNIT A.2 - R STREET ELEVATION
 SCALE: 1/8" = 1'-0"

DATE: 02/22/11

DESIGN REVIEW
 RE-SUBMITTAL

SHEET TITLE:
 UNIT A.2 FLOOR
 PLANS AND
 ELEVATIONS

SHEET NO.

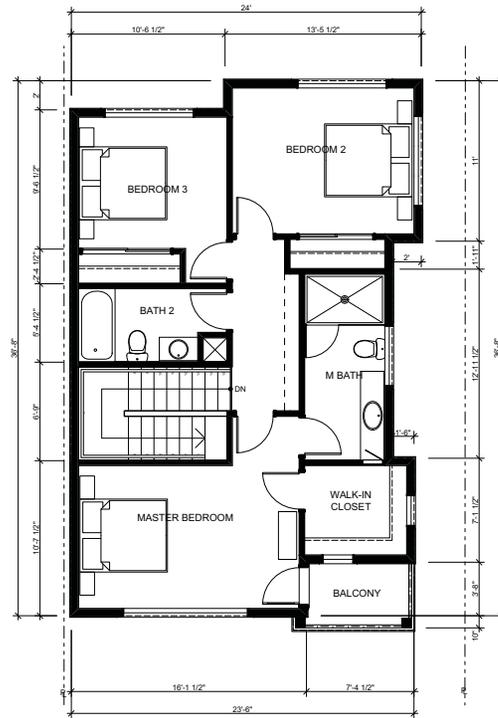
A6.3

Exhibit I Unit B

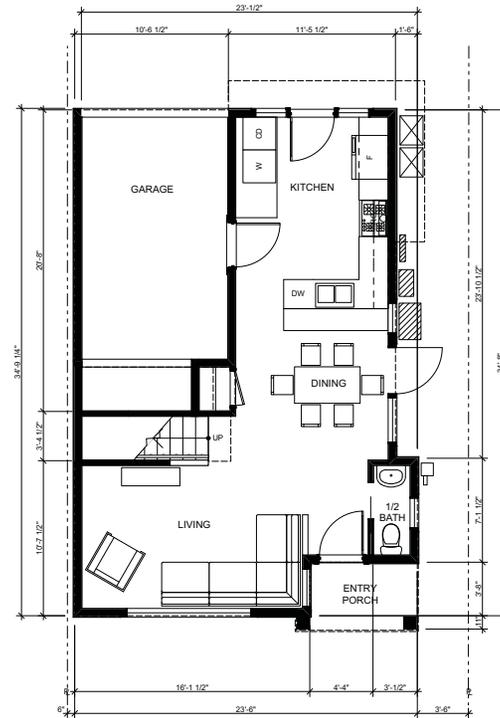


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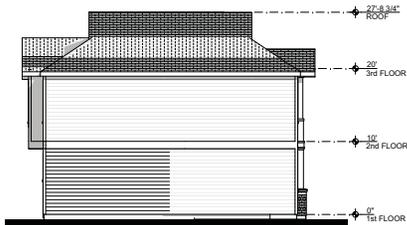
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③ UNIT B - SECOND FLOOR PLAN (727 S.F.)



② UNIT B - FIRST FLOOR PLAN (551 S.F.)



⑥ UNIT B - SIDE ELEVATION



⑤ UNIT B - FRONT ELEVATION



④ UNIT B - ALLEY ELEVATION



⑦ UNIT B - REAR ELEVATION

DATE: 02/22/11

DESIGN REVIEW
 RE-SUBMITTAL

SHEET TITLE:
 UNIT B FLOOR
 PLANS AND
 ELEVATIONS

SHEET NO.
A6.4

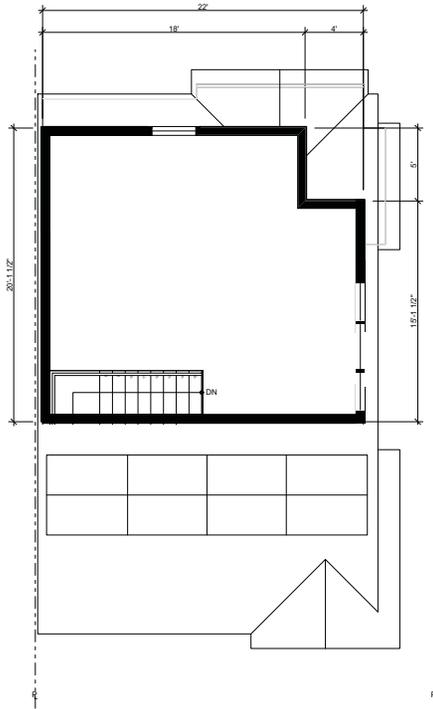
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Exhibit J Unit B.1

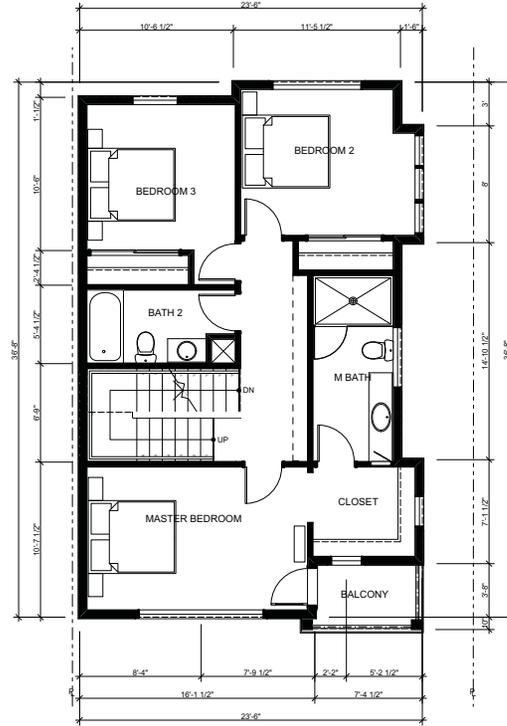


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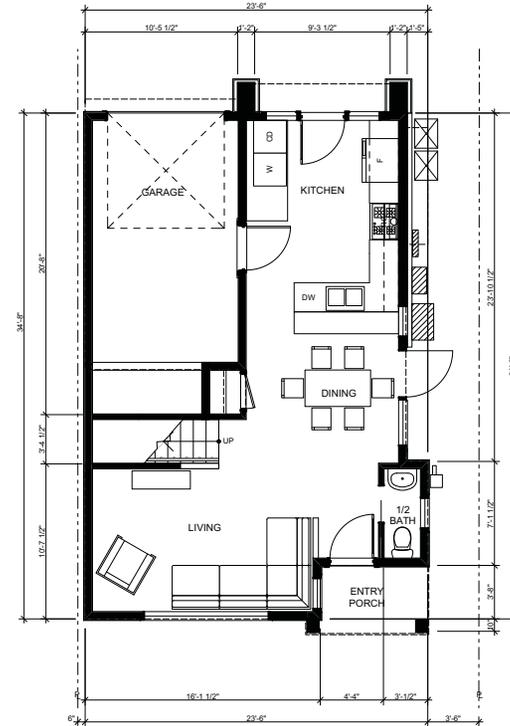
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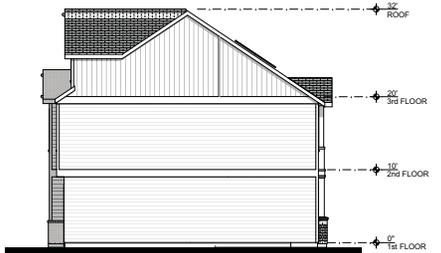
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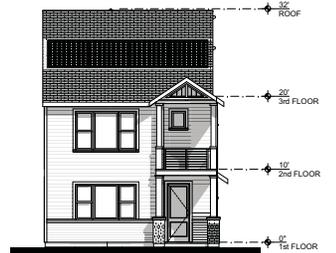
2 UNIT B.1 - SECOND FLOOR PLAN (727 S.F.)
SCALE: 1/4" = 1'-0"



1 UNIT B.1 - FIRST FLOOR PLAN (561 S.F.)
SCALE: 1/4" = 1'-0"



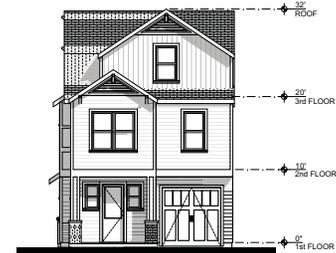
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SCALE: 1/8" = 1'-0"



6 UNIT B.1 - FRONT ELEVATION
SCALE: 1/8" = 1'-0"



5 UNIT B.1 - SIDE ELEVATION
SCALE: 1/8" = 1'-0"



4 UNIT B.1 - R STREET ELEVATION
SCALE: 1/8" = 1'-0"

DATE: 02/22/11

DESIGN REVIEW
RE-SUBMITTAL

SHEET TITLE:
UNIT B.1 FLOOR
PLANS AND
ELEVATIONS

SHEET NO.

A6.5

Exhibit K Unit C



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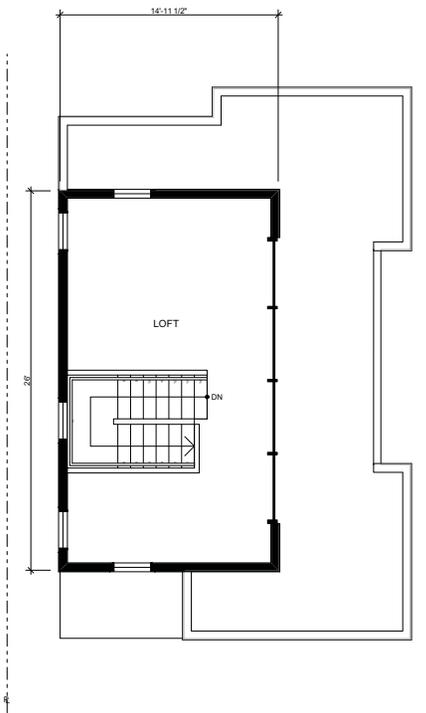
DATE: 02/22/11

DESIGN REVIEW
 RE-SUBMITTAL

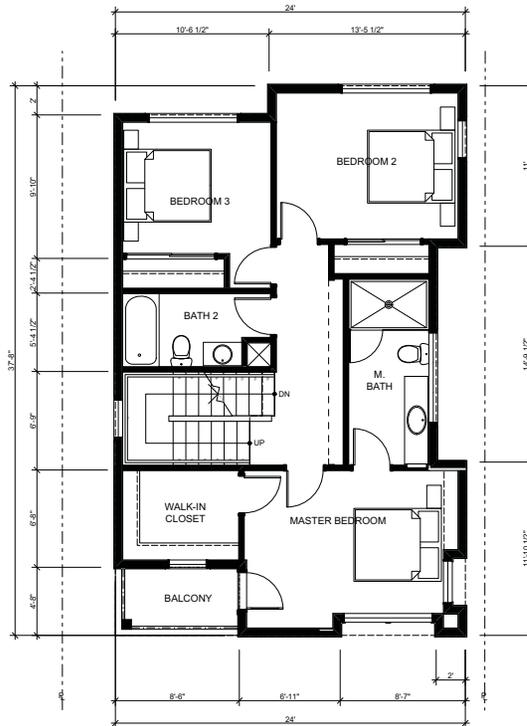
SHEET TITLE:
 UNIT C FLOOR
 PLANS AND
 ELEVATIONS

SHEET NO.

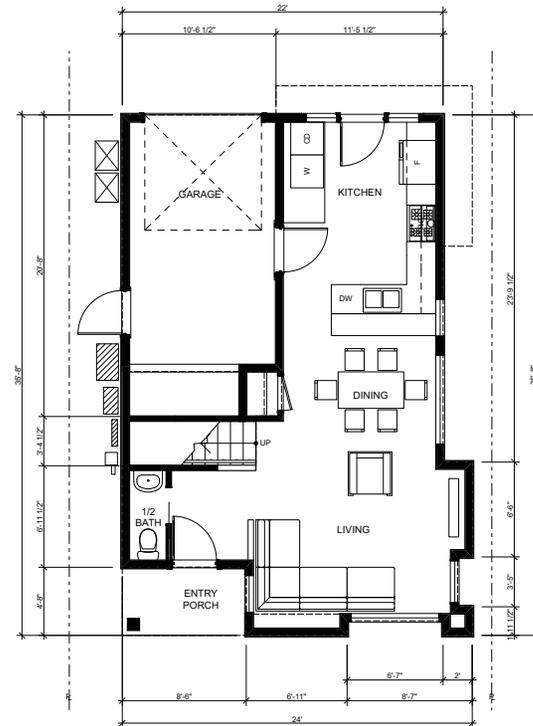
A6.6



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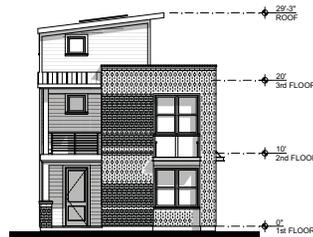
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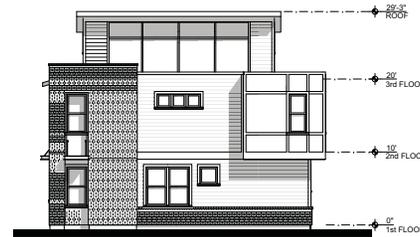
1 UNIT C - FIRST FLOOR PLAN (574 S.F.)
 SCALE: 1/4" = 1'-0"



7 UNIT C - SIDE ELEVATION
 SCALE: 1/8" = 1'-0"



6 UNIT C - FRONT ELEVATION
 SCALE: 1/8" = 1'-0"



5 UNIT C - R STREET ELEVATION
 SCALE: 1/8" = 1'-0"



4 UNIT C - REAR ELEVATION
 SCALE: 1/8" = 1'-0"

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Exhibit L Landscaping Plan



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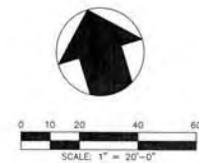
DATE: 02/22/11

DESIGN REVIEW
 RE-SUBMITTAL



SHEET TITLE:
 CONCEPTUAL
 LANDSCAPE
 PLAN

SHEET NO.
L1



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Exhibit L Landscaping Plan



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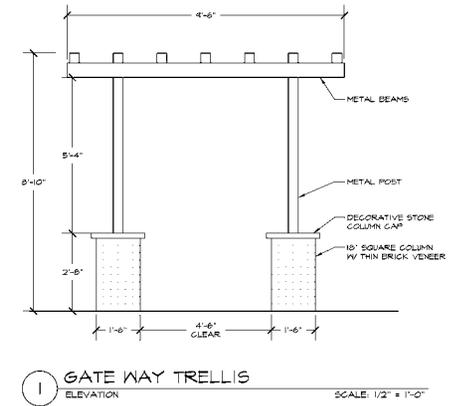
2500 R
 25TH AND R STREETS
 SACRAMENTO, CA 95811
 APN # 010-0043-001-0000

CONCEPTUAL PLANT LEGEND

SYMBOL	BOTANICAL NAME/ COMMON NAME	CONTAINER SIZE	MATURE SIZE (HxW)	WATER USE	QUANTITY
SHRUBS					
(AM)	ABUTILON H. MOBILE PINK/ CHINESE BELLS FLOWER	15 GAL	6'x6'	M	3
(A)	ASAPANTHUS MIDNIGHT BLUE/ LILY-OF-THE-NILE	1 GAL	3'x3'	M	43
(B)	BUNUS SCHRIMPERVIRENS 'GREEN TOPPER' BOONWOOD	5 GAL	3'x2'	L	1
(C)	CAMELLIA SASANGUA WHITE DOVES/ CAMELLIA	5 GAL	4'x3'	M	14
(D)	DIETES VEGETA TORTONSHIRT LILY	1 GAL	3'x3'	L	121
(E)	EURYOPIS PECTINATUS 'MUNCHKIN' 'YELLOW BUSH DAISY'	5 GAL	3'x3'	L	15
(F)	LAVANDULA ANGSTIFOLIA 'MUNSTEAD' ENGLISH LAVENDER	5 GAL	4'x3'	M	67
(G)	LOROPETALUM CHINESE 'SUZZLING PINK' CHINESE FRINGE FLOWER	5 GAL	4'x3'	M	28
(H)	PHORUM 'SPRING GIGI' 'RAINBOW WAVE' & 'YELLOW WAVE' NEW ZEALAND FLAX	5 GAL	3'x4'	L	4
(I)	RHAPHIOLEPS INDICA 'ELEANOR TAYLOR' & 'SPRING RAPTURE' INDIAN HARTHORN	5 GAL	4'x3'	L	140
(J)	VIBURNUM TIBUS 'SPRING BOUQUET' LAURUSTINUS	5 GAL	4'x4'	M	32
(K)	XYLODIA CONGESTUM 'COMPACTA' SHINY XYLODIA	5 GAL	4'x4'	L	37
GRASSES					
(L)	BOUTELOA BRAGELIS BLUE GRAMA GRASS	1 GAL	2'x2'	L	15
(M)	CALAMAGROSTIS AGUTIFLORA 'KARL FODERSTER' FEATHER REED GRASS	1 GAL	3'x4'	L	41
(N)	MULLEBERGIA CAPILLARIS PINK MILLY	1 GAL	3'x3'	L	37
GROUNDCOVERS					
(O)	ASAPANTHUS 'RANGHO WHITE' SPARKY LILY-OF-THE-NILE	1 GAL	2'x2'	M	50
(P)	DIANELLA CACULIDA 'LITTLE BECCA' LITTLE BECCA FLAX LILY	FLATS	12" O.C.	M	253
(Q)	ERIGERON KARVINSKIANUS 'HONGAN BUSH DAISY'	1 GAL	18" x 18"	M	84
(R)	HEMEROCALLIS X 'HAPPY RETURNS' & 'STAREBURST' 'DIARY' EVERGREEN DAY LILY	1 GAL	2'x2'	M	40
(S)	NANDINA DOMESTICA 'FIRE POWER' COMPACT HEAVENLY BAMBOO	1 GAL	3'x3'	L	50
(T)	NERITA X 'FAASSENII SELECT BLUE' GATHRINT	4"	1'x8" O.C.	M	67
(U)	FLOVER CARPET ANSER & WHITE GROUNDCOVER ROSE	2 GAL	2'x3'	M	44
(V)	TULBAGHIA VIOLACEA SOCIETY GARLIC	1 GAL	1'x8" O.C.	M	51
(W)	TURF SOG	-	-	H	545 SF
VINES					
(X)	HARDENBERGIA VIOLACEA 'HAPPY HANDS' LILAC VINE	5 GAL	-	M	2
(Y)	ROSA 'CECILE BRUNNER' CECILE BRUNNER CLIMBING ROSE	5 GAL	-	M	2
(Z)	TRACHELOSPERMUM JASMINOIDES STAR JASMINE	5 GAL	-	M	4

CONCEPTUAL TREE LEGEND

SYMBOL	BOTANICAL NAME/ COMMON NAME	CONTAINER SIZE	MATURE SIZE (HxW)	WATER USE	QUANTITY
ACCENT TREES					
(A)	CER-CAN CERES CANADENSIS 'FOREST PANSY' 'FOREST PANSY' REDBUD	15 GAL	12'x12'	L	5
(B)	LAS-MUS LAGERSTROEMIA 'MUSKOGEE' GRAPE HYDRATEL	15 GAL	25'x12'	L	4
(C)	LAS-NAT LAGERSTROEMIA 'NATCHEZ' GRAPE HYDRATEL	15 GAL	25'x12'	L	3
(D)	MAG-SOU MAGNOLIA SOLANSIANA 'LILLIPUTIAN' SAUGER MAGNOLIA	15 GAL	18'x12'	L	7
SCREEN TREES					
(E)	ACE-BUE ACER BURSERANUM TRIDENT MAPLE	15 GAL	20'x20'	M	2
(F)	LAS-WAT LAGERSTROEMIA INDICA 'WATERMELON RED' GRAPE HYDRATEL	15 GAL	20'x20'	L	4
(G)	QUE-RUB QUERCUS RUBRA RED OAK	15 GAL	60'x35'	M	4
(H)	QUE-SUB QUERCUS SUBER CORK OAK	15 GAL	50'x35'	L	4



DATE 02/22/11

DESIGN REVIEW
 RE-SUBMITTAL



SHEET TITLE:
 CONCEPTUAL
 PLANT LEGENDS
 & DETAILS

SHEET NO.
L2

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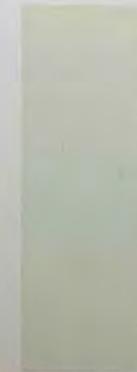
FIBER CEMENT SIDING - PLANK, PANEL, AND BATTEN



ROOFING, ASPHALT SHINGLES, TIMBERLINE COOL SERIES - COLOR "COOL WEATHERED WOOD"



WINDOWS - ANDERSEN 100 SERIES, COLOR "WHITE"



TRIM COLOR



BRICK - HC MUDDOX, THIN BRICK

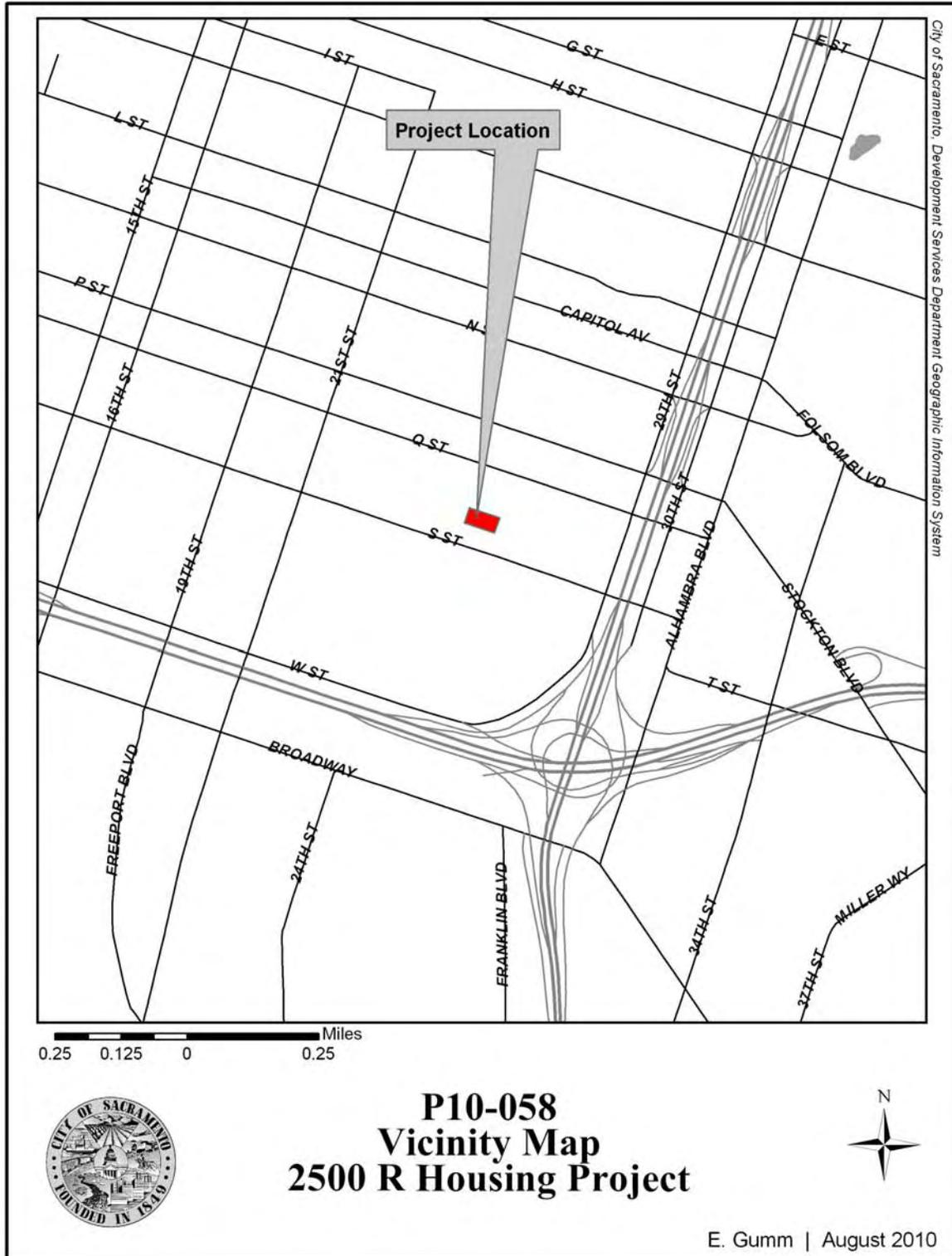


BODY COLORS

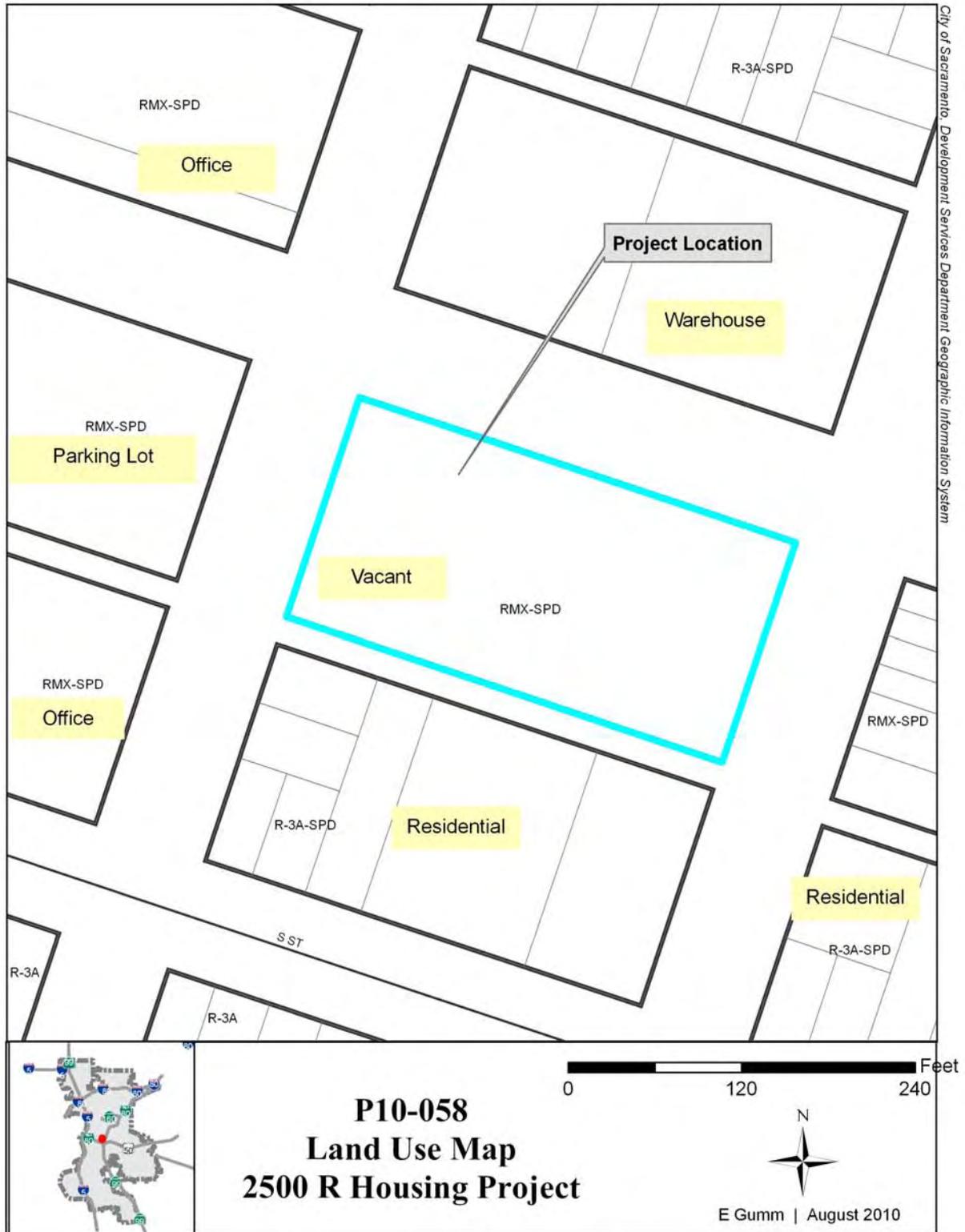
COLOR AND MATERIALS BOARD

2500 R STREET
SACRAMENTO, CA

FEBRUARY 22, 2011



Attachment 3 Land Use and Zoning Map



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REPORT TO DESIGN COMMISSION City of Sacramento

3

915 I Street, Sacramento, CA 95814-2671

PUBLIC HEARING
March 16, 2011

To: Members of the Design Commission

Subject: Courtyard Condominiums (P10-089)

A request to develop a six-unit condominium development with a separate 5-car garage building on approximately 0.19 acres in the Multi-Family (R-3A) zone.

- A.** Environmental Determination: Exempt (CEQA Guidelines Section 15332 for Infill Development);
- B.** Design Review request to construct a six-unit multi-family development with separate 5-car garage building in the Central City Design Review District.

Location/Council District:

Northwest Corner of T Street and 24th Street, Sacramento, CA 95816

Assessor's Parcel Number 010-0036-011-0000 and 010-0036-012-0000

Council District 4

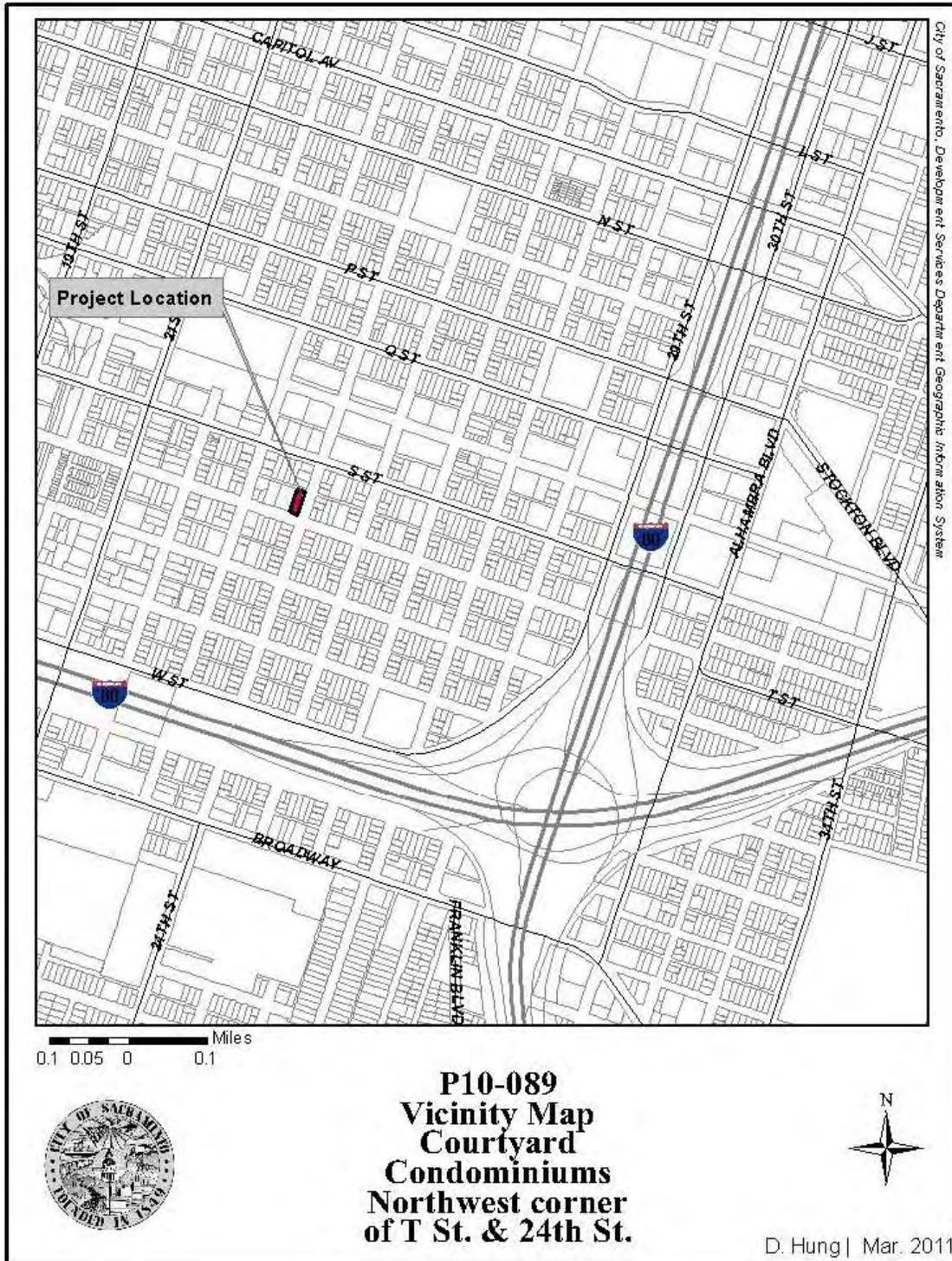
Recommendation: Staff recommends the Commission approve the request based on the findings and subject to the conditions listed in Attachment 1. The Commission has final approval authority over items A and B above, and its decision is appealable to City Council.

Contact: David Hung, Associate Planner, (916) 808-5530; Luis R. Sanchez, AIA, LEED AP, Senior Architect, (916) 808-5957

Applicant: Rosen Development LLC, Attn: Andrea Rosen, (916) 508-6721, 2226 Portola Way, Sacramento, CA 95818

Owner: Rosen Development LLC, Attn: Andrea Rosen, (916) 508-6721, 2226 Portola Way, Sacramento, CA 95818; Benjamin Rosen (916) 761-1912

Vicinity Map



Summary: A proposal to develop six condominium units on approximately 0.19 acres in the Multi-Family (R-3A) zone. The project was elevated from the Design Director to the Design Commission level due to opposition from neighbors and community groups stemming from concerns on the density and project design. The project is before the Commission for the first time. The project also requires Planning Commission approval for a Tentative Map for one condominium lot, a Special Permit to develop six condominium units, a Special Permit to reduce one required parking, a Special Permit to reduce street side setback at the accessory structure and a Variance to waive the requirement for a trash enclosure.

Table 1: Project Information
Existing zoning of site: R-3A (Multi-family zone)
Existing use of site: Vacant (previously residential)
Property dimensions/area: Approximately 0.193 acres (8,430 square foot)
Building square footage: North Building: 3,280 square feet (excluding patio); South Building: 2,843 square feet (excluding patio); Garages: 1,028 square feet
Building height: North and South Buildings: 23'-0" to top of parapet; Garages: 12'-0" to top of building
Exterior building materials: Cement plaster at exterior walls; brick veneer at unit entries; aluminum clad windows; single-ply membrane roofing; raised brick planters

Background Information: The site was previously developed with one single-family home which was approved for demolition by the Preservation Director on June 21, 2010 (file IR10-196). The building was demolished in September of 2010 and the site is now vacant. The applicant has previously submitted a Zoning Administrator application for the development of six apartment units (Z10-142) on September 9, 2010. Due to intense opposition by neighbors at a community meeting on November 18, 2010, staff decided to elevate the project from the Zoning Administrator level to the Planning Commission level and from Design Director level to the Design Commission level. On December 17, 2010, the applicant submitted a new Planning Commission application to develop six condominium units in place of the withdrawn Zoning Administrator file; the application will also be subject to approval by the Design Commission.

Public/Neighborhood Outreach and Comments: The project was routed to various advisory groups including the Newton Booth Neighborhood Association, the Southside Neighborhood Association and the Richmond Grove Neighborhood Association. An Early Notice was also sent to property owners within 500 feet radius of the project site on February 11, 2011. During the processing of the project, staff received support letters from WALKS Sacramento, Sacramento Area Bicycle Advocates (SABA), Sacramento Area Council of Governments (SACOG), Sacramento Metropolitan Air Quality Management District (SMAQMD), Midtown Business Association, Regional Transit, Friends of Light Rail & Transit, Policy in Motion, Design Sacramento 4 Health and Smart Growth Leadership Recognition Program as well as various community members. Some of the reasons for the support include: 1) The project will help reduce

vehicle trips due to its proximity to light rail; 2) The project promotes walking and bicycling due to its proximity to nearby shops and offices; 3) The project is consistent with the General Plan goals for density goals and diversity in housing; 4) The project is a quality infill development with buildings that help activate the street frontages. Support letters are attached to the staff report.

Staff also received a number of letters of opposition to the project which are attached to the staff report. It was due to the overwhelming opposition that staff has elevated the project to the Planning Commission and Design Commission. Neighbors who opposed the project expressed concerns of the addition of six new units in a densely populated neighborhood, that the project will overburden the on-street parking in the neighborhood, and that the proposed architectural design does not blend with the surrounding buildings. Some neighbors were concerned that they weren't notified of the demolition of the previous home on the lot; however, the demolition was not subject to notification to neighbors. The applicant has also met with many of the neighbors to discuss the project; in response to concerns over the original proposal for apartment units, the applicant is now requesting for condominium units.

Staff received two separate petitions to oppose the project. The first petition (attached), submitted on October 18, 2010, by a group of 21 neighbors, stated the following concerns:

1. There are already multiple apartment complexes in the neighborhood.
2. Parking in the neighborhood is already a problem.
3. The architectural design is out of character with the neighborhood.
4. The project will disrupt the existing condition of the neighborhood.

A second petition (attached) came from a group of 40 neighbors and described the following concerns:

1. The overall historic and aesthetic profile of a neighborhood that is already challenged by inappropriate and haphazard design approvals from prior decades.
2. The already overwhelming density of on-street parking.
3. The delicate and tenuous balance that presently exists between single family residences and large multi-unit complexes.

The Concerned Neighbors of Newton Booth and Poverty Ridge has suggested the following stipulations to the project:

1. A reasonable increase of density from that of the previous single-dweller, one-story home on .19325 acres, to three housing units.
2. An architectural style of good faith integrity that is appropriate to the immediate surrounding neighborhood within a 300 square foot radius--an area that includes, albeit not exclusively, Tudor cottages, Craftsman bungalows, and Depression-era brick duplexes.
3. Pitched roofs with a height not markedly greater than that of the Mirabella Apartments immediately adjacent to the site on T Street.
4. Setbacks that adhere to city requirements.
5. Porches that evoke an "eyes on the neighborhood" affect while still adhering to the aforementioned setback requirements.

6. Off-street parking accommodations provided for all units.

In light of these comments, the applicant expressed that a feasible project will require up to six units. The design concept of the buildings is to echo the Prairie style mansions and some of the Art Deco apartment buildings in the neighborhood and by adding interest to the existing buildings with Tudor and Craftsman style architecture. The two residential structures on the site comply with all setback requirements; only the accessory structure encroaches into the street side setback. Most units have raised stoop entries that faces the street. The project requires the reduction of just one parking space.

Environmental Considerations: The Community Development Department, Environmental Planning Services Division has reviewed this project and determined that this is exempt from the provisions of the California Environmental Quality Act (CEQA) Section 15332, In-Fill Development Projects. The project is consistent with the applicable general plan designations and all applicable general plan policies as well as with applicable zoning designations and regulations, occurs within city limits on a project site that is no more than 5 acres substantially surrounded by urban uses, site has no habitat value for endangered, rare or threatened species, site can be adequately served by all required utilities and public services, and would not result in any significant effects relating to traffic, air quality, noise or water quality.

Sustainability Considerations: The City has adopted a Sustainability Master Plan to complement the City's General Plan. This was done to ensure that the City set the standard for the practices of sustainability within its own organization as well as becoming a model for any construction projects within the City. Projects should consider the following goals adopted by the City as projects are proposed within the City: reduce consumption of materials, encourage the reuse and local recycling of materials, reduce the use of toxic materials; establish and continuously improve "green" building standards for both residential and commercial development--new and remodeled, reduce dependence on the private automobile by working with community partners to provide efficient and accessible public transit and transit supportive land uses, reduce long commutes by providing a wide array of transportation and housing choices near jobs for a balanced, healthy city; improve the health of residents through access to a diverse mix of wellness activities and locally produced food, promote "greening" and "gardening" within the City, create "Healthy Urban Environments" through Restorative Redevelopment, and maintain and expand the urban forest.

Staff recommends that the applicant introduce sustainable practices during the construction of the proposed project. Staff recommends the use of energy efficient design, and the use of local materials as a minimum standard for this project.

Policy Considerations:

Following is how the proposed project adhere to policies within the 2030 General Plan, the Central City Community Plan, Zoning, Smart Growth principles and Multi-Family Design Principles.

General Plan/Zoning

The 2030 General Plan Update was adopted by City Council on March 3, 2009. The 2030 General Plan's goals, policies, and implementation programs define a roadmap to achieving Sacramento's vision to be the most livable city in America. The 2030 General Plan Update designation of the subject site is Traditional Neighborhood Medium Density which provides for provides for higher intensity medium-density housing and neighborhood-support uses and allows a density from 8 units per acre to 21 units per acre. The 2030 General Plan has identified goals and policies under the Land Use and Urban Design Element and the Housing Element. Some of the goals and policies supported by this project are:

1. *Land Use and Urban Design Element (Goal LU 4.1) Neighborhoods.* Promote the development and preservation of neighborhoods that provide a variety of housing types, densities, and designs and a mix of uses and services that address the diverse needs of Sacramento residents of all ages, socio-economic groups, and abilities.
2. *Replacement of Non-Conforming Densities in Traditional Neighborhoods. (Policy LU 4.3.2)* The City shall preserve the existing diversity of housing types and densities on each block of Traditional Neighborhoods. Where proposed residential development on a parcel within a Traditional Neighborhood block would exceed the maximum allowed density, the City may allow the development if it would not cause the overall density for the block to be exceeded. Where the density of existing development on a Traditional Neighborhood block falls outside the applicable density range of its land use designation, the City shall allow replacement development on the parcel that maintains the same density.
3. *Housing Element (Policy H-2.2.1)* The City shall promote quality residential infill development through the creation/adoption of flexible development standards and with funding resources.
4. *Central City Community Plan. Land Use and Urban Design (CC.LU 1.3)* Interrelated Land Uses. The City shall provide for organized development of the Central City whereby the many interrelated land use components of the area support and reinforce each other and the vitality of the community.

Based on the General Plan land use designation (Traditional Neighborhood Medium Density), the site may be developed to a density from 8 units per acre to 21 units per acre. The size of the subject parcel is 0.193 acres (8,430 square feet), which would allow the site to be developed with two (2) to four (4) units.

The General Plan does provide policy direction for the replacement of non-conforming densities in Traditional neighborhoods. Within a Traditional Neighborhood, Land Use

policy 4.3.2 (see above), allows the density on a particular parcel to exceed the maximum General Plan density if it would not cause the overall density for the Traditional Neighborhood block to be exceeded. The proposed project is situated on a block approximately 2.4 acres in size. Based on the maximum density of 21 units per acre, a total of 50 units could be allowed on this block. Staff has tabulated that approximately 37 units currently exist on the block containing the subject site, and with the addition of six units, the block will be under 50 units. Therefore, in considering the entire block, the development does not cause the overall density for the block to be exceeded.

Development on the subject site is restricted to seven (7) units based on the parcel's zoning designation of Multi-Family (R-3A). The R-3A zone allows a maximum density of 36 units and a minimum lot size of 1,200 square feet per residential unit. The proposed project is located on 8,430 square feet and can therefore, accommodate a maximum of seven (7) units.

Overall, the proposed project meets the 2030 General Plan goals and policies related to Citywide Land Use and Urban Design for development within the Traditional Neighborhood Medium Density designation.

Smart Growth Planning Principles:

“Smart Growth” is a term coined by the United States Environmental Protection Agency (USEPA) as an umbrella term for the many initiatives intended to address some of the negative consequences of urban sprawl. Smart Growth generally occurs when development patterns are sustainable and balanced in terms of economic objective, social goals, and use of environmental/natural resources. The following Smart Growth principles apply to the proposed project:

- Higher-density, cluster development.
- Multi-modal transportation and land use patterns that support walking, cycling and public transit.
- Streets designed to accommodate a variety of activities.
- Planned and coordinated projects between jurisdictions and stakeholders.

The proposed project has been designed to incorporate many of the Smart Growth Principles listed above.

Multi-Family Residential Design Guidelines:

The Multi-Family Residential Design Guidelines were approved by the City Council on August 5, 2000 (Resolution CC2000-487). This document articulates design principles for multi-family residences to assist the Planning Commission, City Council, City staff and project planners and designers by identifying the City's design criteria for multi-family development. The intent is to achieve well-designed projects to enhance the community's overall value and appearance. The project is generally consistent with the

Courtyard Condominiums (P10-089)

Multi-Family Residential Design Guidelines as identified in the building design section of this staff report.

Parking & Setback Requirements: Below are the tables indicating parking and setback requirements.

Table 2: Parking			
Use	Required Parking	Proposed Parking	Difference
Condominiums	6 spaces	5 spaces	-1

Setbacks, height and bulk

Table 3: Height and Area Standards			
Standard	Required	Proposed	Deviation?
Height	35'-0" maximum	23'-0" to top of parapet	No
Front Setback	Average of adjacent	9'-0" (same setback as adjacent building)	No
Rear/Accessory Structure Setback	6'-0"	6'-0"	No
Street Side Setback (condo buildings)	5'-0"	5'-0"	No
Interior Side Setback (condo buildings)	5'-0"	5'-0"	No
Street Side Setback (accessory structure)	5'-0"	2'-0"	Yes
Interior Side Setback (accessory structure)	No requirement	0'-6"	No
Lot Coverage	Maximum 60%	Approximately 48.6%	No

As indicated above, the project meets or exceeds parking requirements.

Project Design: Per the applicant, the design concept is to ‘echo the Prairie style mansions and the vintage apartment buildings in the neighborhood, some of which are Art Deco in style’. In providing a central courtyard area for the occupants, the project hopes to emulate the Mirabella and Tudor style apartments on the same block. The project includes unit entries that face the street, with stoops to the front doors, and should enhance the pedestrian experience along the street frontages. This proposed project is generally consistent with the Sacramento Central City Neighborhood Design Plan. Staff is generally supportive of the design and recommends potential Commission review and discussion on the following items, in order to assist the Applicant with project development. Staff recommends particular attention to the base of the proposed structure and the development of the pedestrian experience.

Staff has been informed by neighbors and members of the Newton Booth neighborhood association, that they do not support the project massing or design. Although the proposed project does not mimic the pitched roofs on some adjacent properties, staff feels it fits within the neighborhood, which has an eclectic blend of design styles, including the two story prairie style architecture proposed by the architect. Staff has discussed an alternate design with the applicant, and they wish to proceed with the proposed design.

Design Policy Considerations:

1. Purpose (Section 1.A.3, Page 1-1). An integrated variety of styles and design approaches will contribute to the aesthetic vitality of the Central City.
2. Flexibility (Section 2.D, Page 2-6). The guidelines are a reference source for project design and review which encourages creativity, flexibility, and variety. The staff and Board (commission) does not encourage or support any one particular architectural style. Allow alternative designs that, while not meeting every design principle, contribute positively to the neighborhood.
3. Location of Structures (Section 3.A.2, Page 3-4). Locate structures to create usable outdoor places and continuity of desirable characteristics of adjoining structures along the street face.
4. Design Concept (Section 3.C.1, Page 3-22). Provide a coherent design concept appropriate in scale, consistent with the palette of materials, textures, and colors, and achieving continuity on all faces.
5. Materials/Textures/Colors (Section 3.C.8, Page 3-35). Incorporate complementary materials of the highest quality, with material textures and colors selected to further articulate the building design.

Design Guidelines Considerations:

1. Residential Private Open Space (Section 3.A.3, Page 3-6). Where consistent with the design concept provide usable outdoor open space designed for the exclusive use of the dwelling unit at grade or in the form of a porch for upper story dwellings.
2. Entries (Section 3.A.5, Page 3-8). Provide clearly defined site and building entries that are in scale with the proposed project, and that relate directly to the street frontage(s).
3. Scale/Height/Massing (Section 3.C.3, Page 3-26). Make a building or group of buildings compatible with its surroundings through the 1) Rhythm of spaces

between buildings, 2) Building scale, mass, and setbacks, 3) Building orientation and relation to the street.

4. Accessory Structure Criteria (Section 3.L.1, Page 3-59). Design accessory structures to reflect and complement the design, materials and colors of the primary building, and place where least disruptive to existing streetscape.

Staff Evaluation:

Staff is supportive of this project and recommends some refinements to the overall design as noted below. Staff requests that the Design Commission approve the proposed project, and allow staff to finalize details, which the Design staff believes require further attention by the design team.

A. Site Design

1. **Building Layout:** The proposed site plan has three structures; there are two two-story residential buildings and a one-story garage building. A large courtyard is located between the two residential buildings to provide community open space for the occupants. Each of the six units also contains a private patio area. The garage building faces the alley to the north and is not disruptive to the existing streetscape. Staff supports the building layout due to consistency with location policy cited above and both residential buildings meet setback and lot coverage requirements as required by the Zoning Code.
2. **Landscaping and Walls/Fencing:** Brick-faced raised planters are placed on the street facing sides of the buildings; accent plants and trailing groundcovers will be installed in these planters. Foundation shrubs and groundcovers in informal grouping will also be installed in the front landscape area. Decorative landscape boulders will be placed adjacent to the entry gate to the courtyard. Accent planting is proposed between garage units facing the alley. Screening shrubs are placed along west side of walkway adjacent to the private patios at the North Building. Vines in self-watering planters, total of three, are placed along the walkway to the west of the South Building. A quercus shumardii tree and a pistacia chinensis tree are planted within the courtyard area. A 15 gallon tree is planted at each of the three private patios outside the North Building. A courtyard wall with entry gate is shown between the North and South Buildings facing onto 24th Street. A pedestrian access gate is shown between the North Building and garage structure, and a second pedestrian access gate is shown just to the west of the South Building. Wood fencing is shown between the subject site and the residential property to the west. Staff supports the landscape design and placement of walls and gates on the plans. Staff has conditioned that final landscape plans, along with specifications on walls and gates, shall be reviewed and approved by Design Review staff prior to Building Permit submittal.
3. **Courtyard Amenities:** Kitchen gardens, one per unit, are located at the courtyard adjacent to the south side of the North Building. A decorative fountain with hidden water return, along with brick-veneered maintenance storage unit and stacking composting unit, are shown near the west side of the courtyard. Staff supports the courtyard amenities since they enhance the usable outdoor space within the project.

4. **Street Trees:** One new street tree will be planted in front of the North Building facing 24th Street; all other trees on 24th Street will remain. One new street tree is proposed in front of the South Building on T Street to replace existing tree in poor health. Informal low/medium shrub groupings in decomposed granite mulch will be used within the planter strip. Staff supports the proposed street trees.
5. **Hardscape:** In the courtyard area, accent concrete paving will be installed at the central area while standard gray concrete paving will be used at all the interior walkways. Staff has conditioned the applicant to provide details and specification of the various paving materials to be reviewed and approved by Design Review staff prior to Building Permit submittal.
6. **Lighting:** Staff would like further clarification where sidewalk light poles and exterior building lights and courtyard lights are located and the design of the exterior lighting fixtures. Staff has conditioned the applicant to provide cut sheets of the exterior light fixtures and locations to be reviewed and approved by Design Review staff prior to Building Permit submittal.
7. **Utilities:** A fire stand pipe is shown in the landscape area in front of the courtyard wall. Water service utilities and backflow preventer are placed in the planter strip adjacent to alley entrance.

B. Building Design

1. **Context:** Although the existing neighborhood contains Craftsman bungalows and Tudor style homes and many pitched roof structures, the guidelines encourages creativity, flexibility, and variety, and to allow alternative designs that, while not meeting every design principle, contribute positively to the neighborhood. Within this Newton Booth neighborhood, there is a variety of style and roof forms even though there are dominant ones.
2. **Height and Massing:** The North and South Buildings have a proposed height of 23'-0" to the top of the parapet. The garage structure has a proposed height of 12'-0" to the top of the building. With multiple structures on the site and incorporation of a courtyard area, the project softens the massing on the streetscape and does not overpower the surrounding structures in scale and height.
3. **Exterior Materials and Colors:** Exterior materials include cement plaster at exterior walls/patio walls, brick veneer at unit entries, metal railings and single-ply membrane roofing. The applicant has submitted proposed color scheme for the walls, windows and railings as shown on the attached Materials Board exhibit. Staff supports the proposed materials and colors and has conditioned that final material and color selections shall be submitted to Design Review staff for approval prior to Building Permit submittal.
4. **Fenestrations:** Aluminum clad windows with grids are used throughout the proposed buildings. Windows are either single or ganged. Window operations are proposed to be a combination of casement, awning & single or double hung. In most cases, the ground floor and second story windows are aligned vertically. A number of the windows have eyebrow overhang/awning that is wood faced with steel support; these awnings provide shade, shadow casting on walls and interest

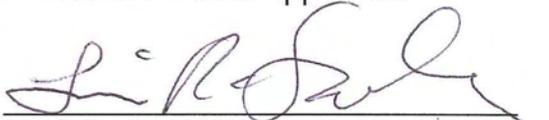
- to the overall design of the buildings. Final window specifications shall be reviewed and approved by Design Review staff prior to Building Permit submittal.
5. **Corner Element:** The South Building has pop-out corner elements spanning both floors on the street facing elevations. The North Building also has a pop-out corner element spanning both floors where it faces 24th Street. The corner elements provide accentuation and interest to the buildings.
 6. **Entries/Door:** Four out of the six units have front entries that face the street that provides eyes on the street and enhance the pedestrian experience; the other two units have an entry that faces into the courtyard to allow eyes on the interior area. All street facing entries have raised entry stoop. Entry doors are single panel doors. Final entry door specification shall be reviewed and approved by Design Review staff prior to Building Permit submittal.
 7. **Garage Door:** The garage doors face the alley to the north and will be painted to complement the garage structure. Staff has conditioned that final material and color specifications shall be reviewed and approved by Design Review staff prior to Building Permit submittal.
 8. **HVAC:** The locations of HVAC equipment for each individual unit are not shown on the plans. No roof mounted HVAC equipment will be allowed. Location of HVAC equipments shall be reviewed by Design Review staff prior to Building Permit submittal. All HVAC equipment shall be screened from street view. Staff suggests that the install safeguard features on all ground-mounted HVAC equipments to prevent theft of components.

Recommendation

Staff requests that the Design Commission approve the proposed project subject to the final conditions of approval.

Respectfully submitted by: 
DAVID HUNG
Associate Planner

Recommendation Approved:


LUIS R. SANCHEZ, AIA, LEED AP
Senior Architect


WILLIAM CROUCH, AIA, FRAIA,
NCARB, CBO, Casp, LEED (AP)
Urban Design Manager

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Attachment 1
Proposed Findings of Fact and Conditions of Approval
Courtyard Condominiums (P10-089)
Northwest Corner of T Street and 24th Street

Findings of Fact

A. Environmental Determination: Exemption-Infill

Based on the determination and recommendation of the City's Environmental Planning Services Manager and the oral and documentary evidence received at the hearing on the Project, the Planning Commission finds that the Project is exempt from review under Section 15332 (Infill) of the California Environmental Quality Act Guidelines as follows:

- a. The project complies with all applicable policies of the General Plan, as well as with the applicable zoning regulations;
- b. The proposed development occurs within City limits on a project site of no more than five (5) acres substantially surrounded by urban uses;
- c. The project site has no value as habitat for endangered, rare or threatened species;
- d. Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality; and
- e. The site can be adequately served by all required utilities and public services.

B. The **Design Review request to construct a new multi-family development in the Central City Design Review area is approved, subject to the following Findings of Fact and Conditions of Approval:**

1. The project, as conditioned, complies with the standards within the Central City Neighborhood Design Guidelines.
2. The proposed residential buildings are well-articulated and provide adequate building setback on all sides to adjacent properties.
3. The project adheres to the principle that the Design Guidelines are a reference source for project design and review which encourages creativity, flexibility and variety and that staff does not encourage or support any one particular architectural style.

4. The proposed single-family residential use is consistent with the goals and policies of the 2030 General Plan designation of Traditional Neighborhood Medium Density.

Conditions of Approval

The Design Review request to construct a new multi-family development in the Central City Design Review area is hereby approved subject to the following conditions:

A. The design of the site (see plans attached) is hereby approved subject to the following conditions. These conditions must be met prior to the issuance of a building permit:

1. The buildings shall be sited as indicated in the report and exhibits.
2. The project shall have building setbacks as indicated in the exhibits.
3. The project shall have building entries as indicated in the exhibits.
4. The project shall include auto access and landscaping as indicated on the reports and exhibits.
5. The Applicant shall submit details and specifications of the various paving materials to be reviewed and approved by Design Review staff prior to Building Permit submittal.
6. The project shall include fences and walls with landscaping elements as indicated on the reports and exhibits, and final landscape plans shall be reviewed and approved by Design Review staff prior to Building Permit submittal.
7. The Applicant shall submit final building and site lighting locations and cut sheets to be reviewed and approved by Design Review staff prior to Building Permit submittal.

B. The design of the new building (see plans attached) is hereby approved subject to the following conditions:

8. The design of the building shall be as indicated in the report and exhibits. The building elevations shall have a consistency of detail and quality as indicated on the plans
9. Cement plaster and brick veneer shall be as indicated on plans.

10. Windows shall be aluminum clad as shown on the plans. Final window specifications shall be reviewed and approved by Design Review staff prior to Building Permit submittal.
11. Awnings and iron railings shall be installed at locations shown on plans.
12. Final entry door specification shall be reviewed and approved by Design Review staff prior to Building Permit submittal.
13. Garage door material and color specifications shall be review and approved by Design Review staff prior to Building Permit submittal.
14. Location of HVAC equipments shall be reviewed by Design Review staff prior to Building Permit submittal. No roof mounted HVAC equipment is allowed. All HVAC equipment shall be screened from street view.
15. Final building lighting plans and fixture cut sheets shall be reviewed and approved by Design Review staff prior to Building Permit submittal.
16. All final material and color selections shall be submitted to Design Review staff for approval prior to Building Permit submittal.
17. Any new signage shall be subject to review and approval by Design Review.
18. All final details affecting the exterior building design that are not determined at the time of the Commission's final review shall be reviewed and approved by Design Review staff prior to Building Permit submittal.
19. All required new and revised plans shall be submitted for review and approval by Design Review staff prior to issuance of building permits. A set of the appropriate plans shall be submitted directly to Design Review Staff. All necessary planning entitlements shall have been approved by the Planning Commission prior to final Design Review sign-off of plans.
20. The approval shall be deemed automatically revoked unless required permits have been issued and construction begun within three years of the date of the approval. Prior to expiration, an extension of time may be granted by the Design Commission upon written request of the applicant.
21. The Record of Decision shall be scanned and inserted into the final set as a general sheet to be submitted for building permit.
22. A signed copy of the Affidavit of Zoning Code Development Standards shall be scanned and inserted into the final set as a general sheet to be submitted for building permit.

Exhibit 1A – Cover Sheet



24th & T Courtyard Condominiums

	First Floor (SF)	Second Floor (SF)	Private Patio (SF)	Project Totals (SF)
South Bldg.				Bldg (balcony not included)
Unit 1	519	543	88	1,063
Unit 2 - ground	900	*	90	900
Unit 3 - Level 2	*	881	97	881
North Bldg.				Bldg (balcony not included)
Unit 4	553	523	100	1,076
Unit 5	550	552	110	1,102
Unit 6	550	552	110	1,102
Garages (G)	*	*	*	1,028
Totals	3,072	3,051	595	

Total Building:	6,123 sf
Total Patio:	995 sf
Total Garage:	1,028 sf

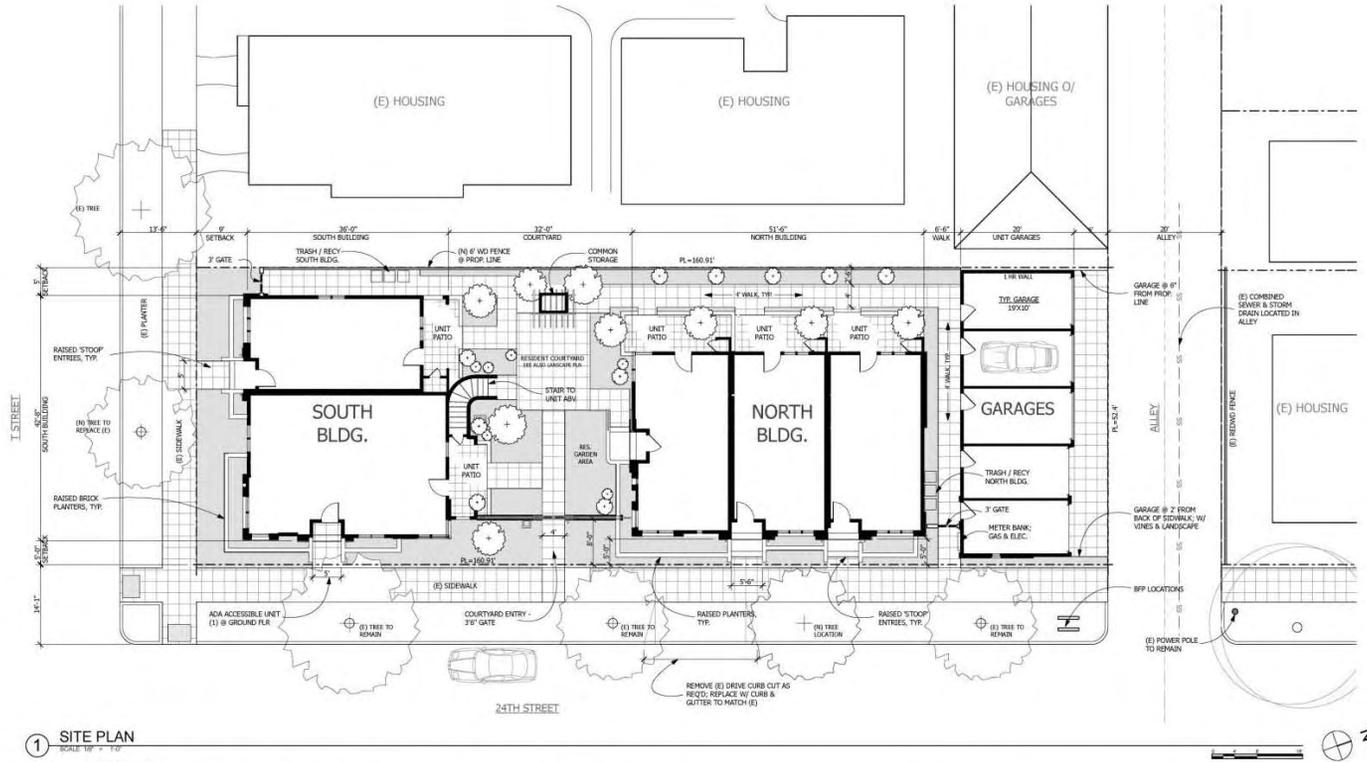
PROJECT DATA



VICINITY MAP

ARCHITECT: WILKINS SYRINIA
 24th & T Courtyard Condominiums
 DATE: 03/09/10
 03/09/11

Exhibit 1B – Site Plan



1 SITE PLAN
SCALE: 1/8" = 1'-0"

BUILDING DATA:

	First Floor (SF)	Second Floor (SF)	Private Patios (SF)	Project Totals (SF)
South Bldg.				
Unit 1	519	543	88	1062
Unit 2 - ground	900	*	90	900
Unit 3 - Level 2	*	*	97	881
North Bldg.				
Unit 4	553	523	100	1076
Unit 5	550	552	110	1102
Unit 6	550	552	110	1102
Garages (G)	*	*	*	1028
Totals	3072	3051	595	

Total Building: 6,123 sf
 Total Patios: 595 sf
 Total Garage: 1,028 sf

SITE DATA:

LOT SIZE:	8430 sf
SOUTH BLDG FOOTPRINT:	1419 sf
NORTH BLDG FOOTPRINT:	1653 sf
GARAGE:	1028 sf

SETBACKS:

T ST. FRONT SETBACK:	AVE. OF STREET
24TH STREET SIDEYARD:	5 FT
INTERIOR SIDEYARD:	5 FT
ALLEY SETBACK:	6 FT

SITE DRAINAGE:

ONSITE RUNOFF AND BUILDING DRAINAGE TO BE COLLECTED AND ROUTED TO TO (E) UNDERGROUND STORM DRAIN SYSTEM LOCATED IN STREETS & ALLEY.

LANDSCAPE: (See attached landscape plan)
ON SITE: DROUGHT TOLERANT PLANTS & TREES; GRASS IN SMALL LAWN AREAS

OFF SITE: (E) STREET TREES AND GRASS PLANTER TO REMAIN EXCEPT WHERE NOTED.

PATTERN INDICATES ON SITE & OFF SITE PEDESTRIAN CIRCULATION

WILKAS ARCHITECTS
 2431 T Street, Sacramento
 DATE: 09.06.10
 REV: 02.06.11

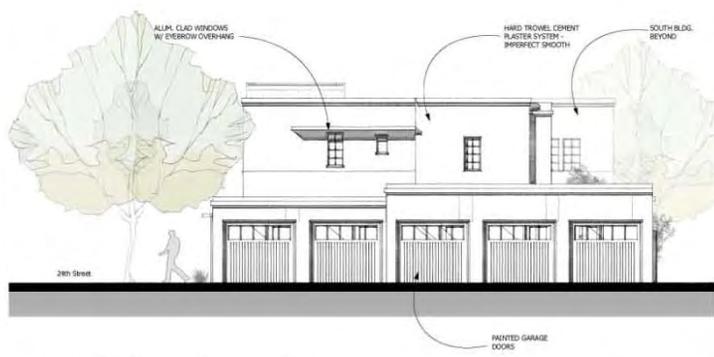
Exhibit 1C – Building Elevations (1)



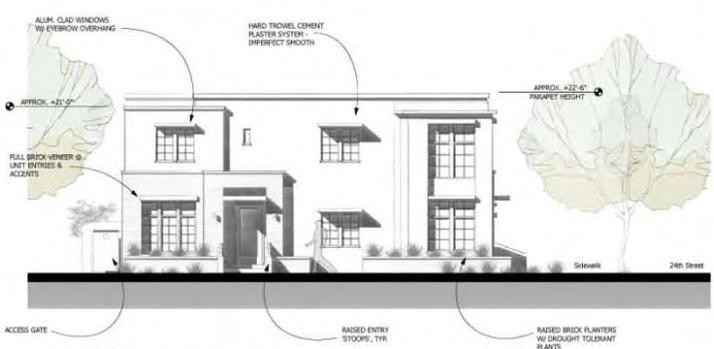
1 East Elevation - 24th Street
SCALE: 3/8" = 1'-0"

PROPOSED FINISH SCHEDULE

FINISH	MATERIAL	FIN. / COLOR
ROOFING:	SINGLE PLY MEMBRANE ROOFING SYSTEM	COOL ROOF / TPO OR FPO
SIDING 1:	HARD TROWEL CEMENT PLASTER SYSTEM	WESTERN BLENDED PRODUCTS / 206 STONEWALL
SIDING 2:	FULL BRICK VENEER	PAINTED BRICK T80
STEEL ACCENTS & TRIM:	RAILINGS, COURTYARD DETAILS, ETC.	CLEAR COAT WEATHERED STEEL
WINDOWS OPTION 1:	ALUMINUM CLAD WOOD WINDOW & DOORS	LINCOLN / COOL WEATHERED COPPER
WINDOWS OPTION 2:	ALUMINUM FRAME WINDOW SYSTEM	MI WINDOWS / ANODIZED FRAMES - BRONZE/ALUM



3 North Elevation - Garages at Alley
SCALE: 3/8" = 1'-0"



2 South Elevation - T Street
SCALE: 3/8" = 1'-0"

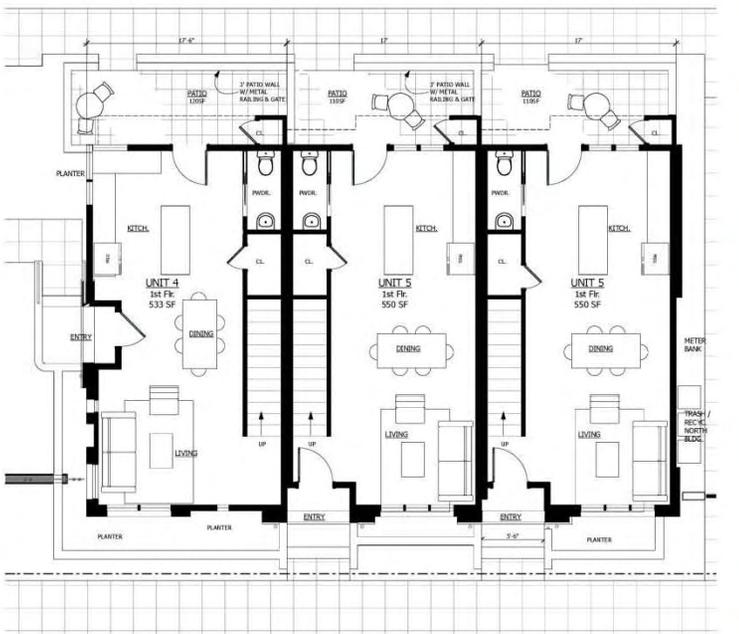
WILLIAMS ARCHITECTS
 1100 S. 10TH STREET, SUITE 200
 DENVER, CO 80202
 COURTYARD CONDOMINIUMS
 24TH STREET - T & 14TH
 DATE: 03-09-11
 REV: 02-28-11

Exhibit 1E – Floor Plans (North Building)

PROJECT: 24TH & I - 1
OWNER: SUMMITRI
DATE: 09/28/10
REV: 02/28/11

summitri
24th & I - 1
Sacramento, CA

DATE: 09/28/10
REV: 02/28/11



① Ground Floor - North Bldg.



② 2nd Floor - North Bldg.

Exhibit 1F – Floor Plans (South Building)

Architect: PETERSON ARCHITECTS, INC. 1100 UNIVERSITY AVENUE, SUITE 1000, SEASIDE, CA 92082
Contract: S10000010
DATE: 03/09/10
REV: 02/24/11

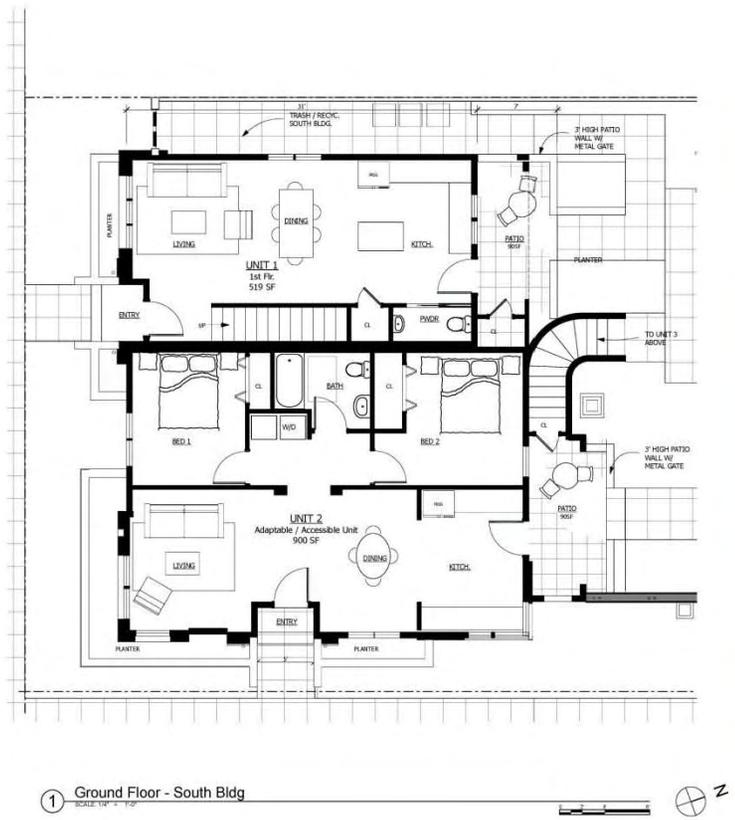


Exhibit 1G – Materials Board

24th & I - Courtyard Condominiums
DATE: 09.09.10
REV: 02.09.11

The architectural drawing shows a two-story building facade with a mix of materials. Callouts point to various elements: a brick veneer on the upper left, a dark wood finish on the upper right, a light-colored plaster on the main wall, a dark metal railing on a balcony, windows with dark frames, and a brick planter at the base.

- FULL BRICK VENEER - PAINTED; TBD** (points to the upper left brick section)
- 'EYEBROW' AWNINGS - WOOD FACE W/ STEEL SUPPORT PAINT - 'OLD REDWOOD' ICI #160** (points to the dark wood awning above the balcony)
- PLASTER - IMPERFECT SMOOTH; WESTERN BLENDED PRODUCTS / 'NAVAJO' (BASE B)** (points to the main light-colored wall)
- RAILING DETAILS - IRON / DARK PAINTED STEEL** (points to the balcony railing)
- WINDOWS: ALUMINUM CLAD; LINCOLN 'COOL WEATHER COPPER' OR 'ADOBE' (shown)** (points to the window frames)
- RAISED PLANTERS - PAINTED BRICK** (points to the brick planter at the base)

Materials Board

Exhibit 1H – Streetscape Elevations



1 Streetscape Elevation: 24th St
SCALE 1/8" = 1'-0"



2 Streetscape Elevation: T St
SCALE 1/8" = 1'-0"

COURTYARD CONDOMINIUMS (P10-089) - 24TH ST & T ST
 ARCHITECT: SWANSON
 DATE: 08.02.10
 REV: 02.09.11

Exhibit 11 – Photos

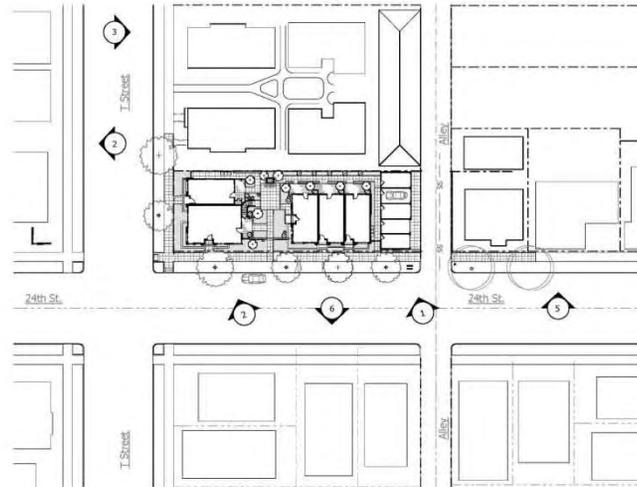
24th St. & T Street, Sacramento
24th & T - Courtyard Condominiums
DATE: 09/29/10
REV: 02/26/11



1 Project Site North - View to Alley



2 Project Site South from 24th St.



Site Context Plan
SCALE: 1" = 20'



3 North View of T St. (Adjacent to Site)



4 South View of T St. (Across from Site)



5 West View of 24th Street (Adjacent to Site)



6 East View of 24th St. (Across from Site)

Preliminary Landscape Notes

1. Refer to Architectural Site Plan for building layout, setbacks, and site information.
2. All construction shall comply with the Standard Specifications of the City of Sacramento, unless otherwise noted in the Supplemental Specifications.
3. Existing street trees are to be retained as shown on plan. Refer to Urban Forest Service recommendations for tree protection.
4. Landscape design concepts:

The landscape within and around the 24/T Courtyard Condominiums has been designed to fit in with and respond to desirable patterns found in the surrounding neighborhood. The design relies on informal groupings of low and medium height shrubs, groundcovers, and decorative landscape features to provide visual interest at the pedestrian scale along the street facing fronts of the buildings.

Plant selection will emphasize the use of native and/or climate adapted plants to promote water efficiency and habitat value in addition to creating a pleasing visual effect.

Trees and screening shrubs have been carefully considered to provide both aesthetic value and privacy for the homeowners and for residents of the adjacent buildings.

The design includes a central courtyard for use by the homeowners, separated from the sidewalk to provide improved privacy without the need for additional screening around the entry gate on 24th Street.

5. Irrigation design concepts:

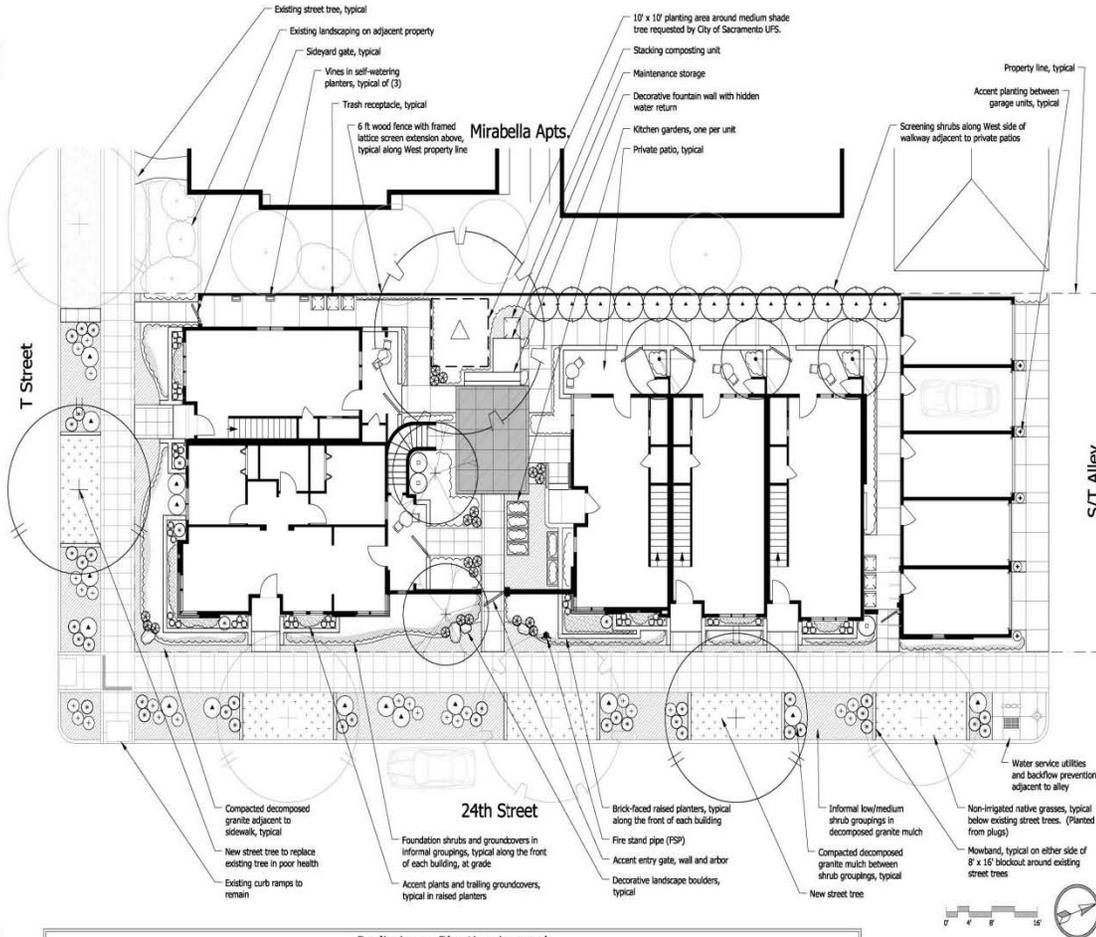
Irrigation for planting around each residence and for planting in the parkway strip will be provided by a common system connected to a separate water meter. The irrigation system will have an ET-based automatic irrigation controller, and will primarily utilize drip irrigation to provide water to each plant.

The irrigation system will be designed to meet the requirements of the City of Sacramento Water Efficient Landscape Ordinance.

6. Calculations (on-site):

Planting area	1550 sqft
Kitchen garden area	125 sqft
Decomposed granite mulch	250 sqft
On-site walks/paving	1575 sqft
Total landscape area:	3500 sqft

7. * Preliminary plant legend is intended to provide a guideline for plant species proposed. Additional plant selections may be included in the final design, in keeping with the overall design intent.



Paving Legend

	Concrete paving, standard gray
	Accent concrete paving at central courtyard
	Existing concrete sidewalk
	Decomposed granite mulch

Preliminary Planting Legend

Proposed trees	Size	Water Use	Proposed screening shrubs	Size	Water Use	Proposed shrubs & perennials	Size	Water Use
Quercus shumardii	24" Box	M	Cupressus sempervirens 'Fastigiata'	15 gallon	L	Arctostaphylos 'Emerald Carpet'	1 gallon	L
Pistacia chinensis	24" Box	M	Koeleruteria paniculata 'Fastigiata'	15 gallon	M	Festuca californica 'San Rafael Blue'	1 gallon	L
Cercis canadensis	15 gallon	M	Prunus caroliniana 'Bright n' Tight'	15 gallon	M	Lavandula spp.	1 gallon	L
Lagerstroemia x.	15 gallon	L	Taxus x media 'Hicksii'	15 gallon	L	Muhlenbergia rigens	1 gallon	L
						Myoporum parvifolium 'Prostratum'	1 gallon	L
						Myrsine arifolius	1 gallon	L
						Nassella tenuissima	1 gallon	L
						Rosmarinus 'Hillis Hardy'	1 gallon	L
						Rosmarinus o. 'Huntington Carpet'	1 gallon	L
						Salvia gregii 'Furhmanns Red'	1 gallon	L
						Salvia leucantha	1 gallon	L



Attachment 2 – Support Letters

1. WALKSacramento
2. Sacramento Area Bicycle Advocates (SABA)
3. Sacramento Area Council of Governments (SACOG)
4. Sacramento Metropolitan Air Quality Management District (SMAQMD)
5. Midtown Business Association
6. Regional Transit
7. Friends of Light Rail & Transit
8. Policy in Motion
9. Design Sacramento 4 Health
10. Smart Growth Leadership Recognition Program
11. Karen Jacques
12. Kay Kneprath
13. Michael Monasky
14. William Burg
15. Ruth Ann Bertsch, M.D., Ph.D., F.A.C.P.
16. J. Matthew Gerken, AICP
17. Dr. Jon B. Marshack
18. Dr. Nita Davidson
19. Sarah Underwood



January 18, 2011

David Hung
City of Sacramento
City Hall
Sacramento, CA 95814

RE: 24th & T Street Courtyard Condominiums – R-3A Multi-Family Zoning

Dear Mr. Hung:

Our review of the project finds that it is an excellent example of a mini-infill project with features that will add to the walkability of the neighborhood. The location is very walkable with convenient pedestrian access to many nearby destinations as well as light rail. www.walkscore.com gives this location an 84 or a "very walkable" score. Transit is good as well.

The orientation of the units to the street is very positive as is the elimination of a driveway and the extension of the planter space. Additionally, having housing units facing the street where there has been a vacant lot and vacant house is a positive for the pedestrian environment.

The layout of the buildings provides an internal pedestrian circulation system that supports neighborly interaction along the pathways and in the courtyard. The separation of the garages from the units will add both to the neighbor interaction as well as giving residents pause before they use their cars which could result in less auto usage in this pedestrian and transit friendly environment.

For these reasons, we urge that the City approve the project and allow it to move forward. If you have questions about these comments, please contact me at 916 – 446-9255.

Best regards,

A handwritten signature in cursive script that reads "Anne Geraghty".

Anne Geraghty
Executive Director
WALKSacramento
909 - 12th Street #122
Sacramento, CA 95814

David Hung

From: Walt Seifert [bikesaba@gmail.com]
Sent: Tuesday, November 02, 2010 2:15 PM
To: David Hung
Cc: andreaosen@sbcglobal.net
Subject: Support for 24th and T Courtyard Housing

Follow Up Flag: Follow up
Flag Status: Flagged

The Sacramento Area Bicycle Advocates (SABA) supports urban infill projects and compact development. The 24th and T Courtyard Housing project as proposed meets SABA's criteria for the type of project that supports increased transit use and bicycle use in Sacramento.

Projects, such as this one, located near transit and bikeways encourage transit and bicycle use, decrease vehicle traffic, improve traffic safety, provide environmental benefits and improve community quality of life.

SABA is an award-winning nonprofit organization with more than 1400 members. We represent bicyclists. Our aim is more and safer trips by bike. We are working for a future in which bicycling for everyday transportation is common because it is safe, convenient, and desirable. Bicycling is the healthiest, cleanest, cheapest, quietest, most energy efficient, and least congesting form of transportation.

Walt Seifert
Executive Director
Sacramento Area Bicycle Advocates (SABA)
(916) 444-6600
saba@sacbike.org
www.sacbike.org

"SABA represents bicyclists. Our aim is more and safer trips by bike."



1415 L Street,
Suite 300
Sacramento, CA
95814

tel: 916.321.9000
fax: 916.321.9551
tdd: 916.321.9550
www.sacog.org



December 2, 2010

Mr. David Kwong
Planning Director
City of Sacramento Community Development Dept.
300 Richards Boulevard, 3rd Floor
Sacramento, CA 95811

Mr. Kwong:

This letter is in response to your request for review of the proposed development at 24th and T Street in the midtown area of Sacramento. Thank you for the invitation to comment on this project as it relates to the Preferred Blueprint Scenario map and principles.

The proposed project was compared to the Preferred Blueprint Scenario. The Preferred Blueprint Scenario is a conceptual map based on the principles of smart growth. This Preferred Scenario is not intended to direct how a specific parcel should or should not be developed in a particular manner, but rather give some direction on how the region needs to develop generally to reap the benefits of the Preferred Scenario. For this reason, it is not possible to apply them at a parcel level. With that caveat, the proposed site plan is consistent with the Preferred Blueprint Scenario map.

SACOG staff evaluated the proposed project, which is a six-dwelling apartment on 0.19 acres. The land will be the result of two merged parcels at 2331 T Street and 1918 24th Street. The project site is located 1.5 blocks from the 24th Street light rail station. The project applicant states that this will be built in the tradition of courtyard apartments found throughout midtown Sacramento. The applicant also states that the project will construct two new 2-story buildings each with 3 two-bedroom units. A detached accessory structure including 5 garages will be built with garage doors facing the alley.

Findings and Evaluation:

- Infill projects in general offer the opportunity to reduce the amount of automobile traffic and reduce the impacts of greenhouse gas emissions. A commonly used planning measure to determine whether automotive transportation is reduced is vehicle miles traveled (VMT). VMT is the amount of mileage the average residential household and/or employee in an area will travel in one day. SACOG modeling and observed data show that residents of locations like this project – those with housing densities similar to midtown – will generate less than one-half the VMT of residents of typical suburban locations. The residents will also walk, bike, or use transit

- Auburn
- Citrus Heights
- Colfax
- Davis
- El Dorado County
- Elk Grove
- Folsom
- Gait
- Isleton
- Lincoln
- Live Oak
- Loomis
- Marysville
- Placer County
- Placerville
- Rancho Cordova
- Rocklin
- Roseville
- Sacramento
- Sacramento County
- Sutter County
- West Sacramento
- Wheatland
- Winters
- Woodland
- Yolo County
- Yuba City
- Yuba County

Mr. David Kwong
Page 2
December 2, 2010

at two to four times the rate of residents of typical suburban locations. In addition to its location, these travel benefits are in part due to the higher density housing in the proposed plan. A comparison of this project to a more suburban project is shown in the table below:

Area Type	Land Use Characteristics ¹				Travel Characteristics ²	
	Total Density ³	Proximity to Transit ⁴	Proximity to Frequent Transit ⁵	Street Pattern ⁶	Non-Auto Mode Share ⁷	Weekday VMT per Household ⁸
Typical Suburban Residential Area	5	1/4 to 1/2 mi	n/a	20	5 to 10%	40 to 50 miles
Low Density Suburban Residential Area	3	>1.5 mi	n/a	15	<5%	>50 miles
24th & T Location	21	<1/4 mi	<1/4 mi	>100	12 to 22%	20 to 25 miles

Source: SACOG, November 2010.
Notes:
1 Land use characteristics based on SACOG's parcel land use datasets
2 Travel characteristics based on household travel survey data
3 Total Density = Jobs + Dwellings Per Gross Acre within 1/2 mile of place of residence
4 Distance to nearest transit of any frequency
5 Distance to transit with peak headway of 15 minutes or less
6 Number of "good" intersections (3 or 4 leg) within 1/2 mile of place of residence
7 Non-Auto Mode Share = the % of trips made using bike, walk or transit modes by residence at location, on average
8 VMT = Vehicle miles traveled

- Infill development is a strategy essential to the success of the Blueprint and our Metropolitan Transportation Plan (MTP). The proposed project clearly supports this principle as it is on a vacant lot located in close proximity to transit in the midtown area. The Blueprint Preferred Scenario, the currently adopted MTP, and the latest scenarios developed for the MTP update achieve transportation, air quality, and other quality of life benefits by relying in part on infill projects, such as this one, to be developed at the densities allowed in local general plans and zoning codes. In some infill areas near major transit, our plans also include targeted increases in allowed densities. The benefits analyzed under these regional plans cannot be achieved if projects in these areas are built to a density that is lower than what the current zoning allows for, such as if the city permitted only a single family home on this site.
- Compact development and a variety of housing options are critical Blueprint planning principles. The proposed project supports both principles by locating housing near existing jobs and services and providing multi-family housing. The midtown/downtown area of Sacramento represents some of the most compact and diversified land uses in the region.

Mr. David Kwong
Page 3
December 2, 2010

This project fits the spirit and nature of the existing community while promoting the Blueprint principles.

- Quality of design, another key Blueprint Principle, is a strong determinant of whether or not developments create stronger communities. As with any proposed infill development, it will be essential that the City's design review process carefully examine the design of the proposed structures to ensure the attractiveness of the project and its enhancement of the surrounding area. However, the project's design team, led by Ron Vrilakis, has much experience in development similar to this in the urban core of Sacramento that have been used as model examples of Blueprint-style development by SACOG.

In conclusion, the proposed development plan, its location in a neighborhood that provides a surrounding mix of uses – retail, residential, office, and its close proximity to transit – make this opportunity an ideal Blueprint implementation project. SACOG has been actively trying to encourage this type of infill development throughout the region. Jurisdictions that can expedite the review process of smaller infill projects that are as clearly consistent as this project with intent of the Blueprint Preferred Alternative and the current zoning will serve to encourage the smart growth that the region is striving for under the Blueprint.

If you have any questions, please feel free to contact myself or SACOG staff members Kacey Lizon (916) 340-6265 or Greg Chew (916) 340-6227.

Sincerely,



Mike McKeever
Executive Director

S:\SECURED\Mike\24th and T BP Review Ltr. 120110.doc



Larry Greene
AIR POLLUTION CONTROL OFFICER

January 13, 2011

Councilmember Robert King Fong
City of Sacramento
915 I Street, 5th Floor
Sacramento, CA 95814

Subject: Proposed development of Courtyard Housing at 24th and T Street

Dear Councilmember Fong,

The Sacramento Metropolitan Air Quality Management District (District) is writing to express support for the proposed development of 6 courtyard condominiums at 24th and T Street. It is the District's position that the project's density, design, and location are consistent with smart growth principals that will reduce the per capita vehicle miles travelled (VMT) and associated emissions of air pollutants. The project is also consistent with goals of the SACOG Regional Blueprint, the City's General Plan designation of Traditional Neighborhood- Medium Density, and the existing zoning for the site. The project presents Sacramento City Council with an opportunity to approve a project that puts into practice essential policies from the General Plan.

Recognizing that 47 percent of our/the region's ozone precursor emissions come from on-road mobile sources¹, it is essential that new residential units minimize the need for use of personal motor vehicles. The Courtyard Housing at 24th and T Street has a compact design, minimum parking, and a transit-supportive density proximate to a light rail station - features that have been linked to a reduction in personal motor vehicle use².

There is a causal relationship between land use decisions and air quality. Consequently, California Health and Safety Code Section 40961 directs the District to "represent the citizens of the Sacramento District in influencing the decisions of other public and private agencies whose actions may have an adverse impact on air quality." The Code also states in Section 41015 that in exercising this duty, the District may not infringe upon the authority of local governments to plan or control land use. The District is always cognizant that it is up to the Sacramento City Council to shape the land use policies for the City, but it is our responsibility to attempt to bring air quality considerations to the forefront. These comments are consistent with the City's goals for Inter-governmental Coordination³, as outlined in the Administration and Implementation section of the General Plan.

The District encourages the City Council to consider the project's potential to benefit regional air quality when it is presented for approval.

Sincerely,

A handwritten signature in black ink, appearing to read "Larry Greene".

Larry Greene
Executive Director

¹ Sacramento Region 2005 Ozone-Precursor Emissions Inventory, available online at: <http://www.airquality.org/>

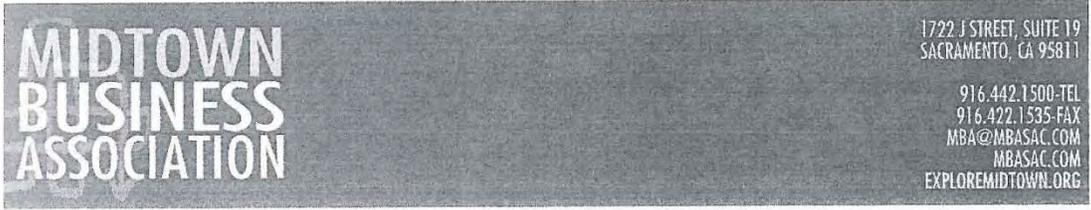
² Online TDM encyclopedia, Victoria Transportation Institute; <http://www.vtpi.org/tm/tm81.htm>

³ City of Sacramento General Plan, Part 4 Administration and Implementation, page 4-8

777 12th Street, 3rd Floor ■ Sacramento, CA 95814-1908

916/874-4800 ■ 916/874-4899 fax

www.airquality.org



February 11, 2011

Ms. Andrea Rosen
Project Proponent
24th & T Courtyard Condominiums
Delivered via email

Dear Ms. Rosen:

As the Executive Director of the Midtown Business Association, I am writing to offer our support for the project the 24th & T Courtyard Condominiums in Midtown.

The MBA Land Use Policy committee reviewed the plans for the project and unanimously agreed that it will make a valuable addition to the Midtown residential community. The committee members were particularly impressed with the design and features of the proposal, including the proposed density. They supported the inclusion of six units in the project.

At the MBA, we believe that the one of the key strengths of Midtown is the neighborhoods. Your project will make a meaningful addition to both the quality and diversity of the housing that Midtown has to offer.

In closing, let me thank you for undertaking this project and offer our support and assistance for its speedy approval and completion.

Sincerely,

Rob Kerth
Executive Director



**Regional
Transit**

**Sacramento Regional
Transit District**
A Public Transit Agency
and Equal Opportunity Employer

Mailing Address:
P.O. Box 2110
Sacramento, CA 95812-2110

Administrative Office:
1400 29th Street
Sacramento, CA 95816
(916) 321-2800
(29th St. Light Rail Station/
Bus 36,38,50,67,88)

Light Rail Office:
2700 Academy Way
Sacramento, CA 95815
(916) 648-8400

Public Transit Since 1973

www.sacrt.com

Date

Name of project manager
Title
Org
Address

NAME OF DEVELOPMENT: Courtyard Condominiums

CONTROL NUMBER: P10-089

TYPE OF DOCUMENT: Tentative Map, Special Permit

The Courtyard Condominiums project proposes six condominium units on .19 acres in an R-3A zone with reduced parking. The site is located on the northwest corner of T and 24th streets in the Central City.

The 23rd Street light rail station is within ¼ mile of the project site. Sacramento Regional Transit (RT) supports the overall project as a high-density residential project within the Central Business District that supports existing transit facilities. RT also supports reduced parking for the project, as that will encourage transit use. The project is also providing improved pedestrian connections to transit facilities.

RT staff has reviewed the proposed project and recommends the following:

Conditions:

- Transit information shall be displayed in a prominent location in the residential sales/rental office, through a homeowner's association, or with real estate transactions. Please use the attached Request Form (also available on www.sacrt.com) to order transit information materials.

Thank you for the opportunity to comment. Please send any subsequent documents and hearing notices that pertain to this project as they become available. If you have further questions regarding these recommendations, please contact me at (916) 556-0513 or tcnfield@sacrt.com.

Sincerely,

Traci Canfield
Planner

c: RoseMary Covington, AGM Planning and Transit System Development, RT
Andrea Rosen, Rosen Development

Attachment



Friends of Light Rail & Transit
1818 L Street, Suite 615
Sacramento, CA 95811
916.447.1960

February 18, 2011

David Hung
c/o City of Sacramento Planning Department
300 Richards Blvd
Sacramento, CA 95814

Dear Mr. Hung:

On behalf of the Board of Directors of Friends of Light Rail & Transit (FLRT) we would like to express our support for the 24th and T Streets Courtyard Condominiums project proposed by Rosen Development, LLC.

The FLRT Executive Committee received a project presentation from Andrea Rosen at our February 17, 2011 meeting and unanimously voted to support the project. It was clear to the committee that the major benefits of this infill project were the close proximity to the 24th and R Street light rail station (1.5 blocks), the high quality design, and the pedestrian elements.

After much discussion about the site plan, we believe that the addition of a seventh unit (as originally designed) reflects a better utilization of the available land and is not disruptive to the neighborhood (in terms of massing and scale).

FLRT is happy to support smart growth, transit-oriented development projects like the 24th and T Condos. Further, we applaud Ms. Rosen's extensive outreach efforts and her careful attention to detail. We look forward to hearing the commission's discussion and hope that you will approve this project.

Sincerely,

A handwritten signature in black ink, appearing to read 'Dain Domich', written in a cursive style.

Dain Domich
President

cc: Andrea Rosen, Rosen Development, LLC
cc: Robert King Fong, City of Sacramento, Councilmember, Dist. 4
cc: David Kwong, City of Sacramento, Planning Director

(Support letter circulated by email)

David Hung

From: Lauren Michele [lauren.michele@policyinmotion.com]
Sent: Friday, February 25, 2011 5:37 PM
To: David Hung
Subject: 24th and T Courtyard Condominiums or P10-089
Attachments: Letter of Support for 24th and T Courtyard Condos P10-089.pdf; Growing Beautiful Communities_Policy in Motion.pdf

Dear Mr. Hung,

Please find attached a letter of support for the 24th and T Street Courtyard Condominiums. As a resident of Newton Booth neighborhood, I am writing in strong support of the City's approval of the six units proposed by this applicant. I also am a certified Small and Underutilized Disadvantaged Business Enterprise owner in this neighborhood (Policy in Motion) and I believe this project *as proposed* would greatly enhance my neighborhood. I have spoken with my neighbors and fellow business owners in Newton Booth and they agree.

My letter outlines three major points:

- 1) The City of Sacramento has a responsibility to implement the community-approved General Plan, and this project is consistent with those goals and standards/codes
- 2) Abandoned lots NOT developed in Newton Booth are causing businesses and residents nuance and safety issues – with three businesses and myself as a resident often calling City Police
- 3) The City should support this project because it will increase economic development and competitiveness locally and puts the region in a better position to secure federal housing/transit funding
- 4) I work on sustainable community implementation and policy with the federal government, and they are using Sacramento as a nation model in promoting residential infill development near transit consistent with “Transit Priority Areas” in Senate Bill 375 (Steinberg, 2008)

In addition to being a proud Newton Booth resident and business owner, I am also have the following credentials:

- Board Member of WALK Sacramento Since 2007
- Car-free Sacramento Resident and Tenant Since 2006
- Transportation Planner/Engineer with Fehr & Peers during City of Sac General Plan Update
- B.S. from UC Davis in Environmental Policy Analysis and Planning
- M.S. from UC Davis in Transportation Policy under the Urban Land Use and Transportation Center
- Policy in Motion Work on Federal and State Legislation on Implementing Sustainable Developments in Communities

Please feel free to contact me with any questions,

~Lauren Michele~



Woman Business Enterprise | 530.848.4342 | lauren.michele@policyinmotion.com

HOW CAN TRANSPORTATION POLICY GROW BEAUTIFUL COMMUNITIES?
www.policyinmotion.com

Lauren Michele / 530.848.4342 / lauren.michele@policyinmotion.com / www.policyinmotion.com



Lauren Michele
 2715 V Street, Unit B
 Sacramento, CA

February 25, 2011

RE: 24th and T Courtyard Condominiums P10-089

As a resident of Newton Booth neighborhood, I am writing in strong support of the City's approval of the six units proposed by this applicant. I also am a certified Small and Underutilized Disadvantaged Business Enterprise owner in this neighborhood (Policy in Motion) and I believe this project *as proposed* would greatly enhance my neighborhood. I have spoken with my neighbors and fellow business owners in Newton Booth and they agree. Three other neighborhood supportive business owners include:

- Dr. Richard Detrano, Dentistry at 2714 V Street
- Dr. Lance Cassazza, Cassazza Chiropractic at 2716 V Street
- Gina Geneshla, Revolution Wines at S and 28th

Dr. Cassazza and Detrano both share similar concerns of mine regarding the problems associated with abandoned lots that are NOT developed – having encountered numerous problems with homelessness and nuisances on a lot at 27th and V Street which is similar in size to the lot at 24th and T Street. We all frequently call Sacramento City Police over the problems associated with this abandoned lot. Further, there are plans for a future school on V Street between 26th and 27th and it will not be safe for our children to have ANY abandoned lots in our neighborhood, including that at 24th and T Street. The City needs to recognize the SAFETY and NUISANCE problems associated with abandoned lots.

Residents and business owners benefit from neighborhood-scale, high-end residential development such as the proposed project because it would:

- 1) Remove the safety and nuance problems associated with abandoned lots;
- 2) Benefit the economic development of the surrounding property home values;
- 3) Bring additional economic development to local businesses

I am also proud to be a car-free resident of Newton Booth and this project, being so close to the 24th Street light rail station, will make it much easier for residents who live there to pursue car-free or less auto-dependent lifestyles. There are so many benefits from living close to transit and in a walk-friendly, bike-friendly location such as 24th and T, I welcome these new residents to our neighborhood who I would image to be very similar in socio-demographic characteristics to myself as a young professional. We are very fortunate that this applicant is willing to spend the considerable sums it will take to build such a well-designed project that is proposed.

It is commendable that this applicant has crafted a project for this parcel that perfectly matches the requirements of the current multi-family zoning and the Sacramento General Plan. This is exactly the type of project that the City planned for this location. Objections to a use that is consistent with existing zoning and the General Plan should be summarily rejected. Lower density suggestions are

Lauren Michele / 530.848.4342 / lauren.michele@policyinmotion.com / www.policyinmotion.com



not consistent with the General Plan's infill goals, transportation goals, air pollution and greenhouse gas reduction goals. Why miss an opportunity like this?

Midtown Sacramento must use one of its most precious resources- land near light rail- for the highest and best use. Land use decisions such as this one have very long-term impacts. This project is medium-density, modest-scale and traditionally laid out. Putting the garages on the alley follows the tradition of the apartments on this half-block and makes the 24th Street side much more pedestrian-friendly. The applicant requests minor non-substantive variances which should be granted. The applicant should be applauded for putting a parking space plus back onto 24th Street by eliminating the existing curb cut which also makes 24th Street more pedestrian-friendly.

As a professional transportation planner and policy analyst on implementing sustainable communities, I urge the City of Sacramento to expedite approval of this project. If Sacramento wants to be a sustainable community as it claims, it should be sending the right message to developers of Smart Growth projects. I work on federal policy which frequently highlights the efforts in the Sacramento region in supporting infill projects, and it would be an embarrassment to the City of Sacramento to report to the Federal Partnership for Sustainability that efforts locally in supporting economically, environmentally and socially responsibly development is getting emotionally hijacked by irrational concerns from other neighbors. The Sacramento Region recently received \$1.5 million from this Partnership to implement residential housing within half a mile of transit stations, in alignment with California's Senate Bill 375 (2008) stimulations to incentivize "Transit Priority Areas." NOT approving this project will decrease the region's economic competitiveness for future funding.

I have attached to this letter an illustration of how the City of Sacramento can choose to be a model in supporting the building of beautiful communities. Please feel free to contact me should you have any questions, and I look forward to seeing you at the March 10th Planning Commission and any upcoming City Council meetings where I will be happy to point out the economic impacts of the City's choices.

Sincerely,

~Lauren Michele~



Woman Business Enterprise | 530.848.4342 | lauren.michele@policyinmotion.com

HOW CAN TRANSPORTATION POLICY GROW BEAUTIFUL COMMUNITIES?
www.policyinmotion.com



David Hung, Planner
City of Sacramento
Sent by Email

January 13, 2011

Dear David Hung:

Design Sacramento 4 Health is a group of physicians and other health professionals who are committed to improving public health through changes in our built environment. Design Sacramento 4 Health supports community development that supports and promotes human health, such as urban infill, compact development, and mixed-use development that encourages walking, bicycling, and use of public transit. Our goals are:

- High-quality, compact development where people can live, work, shop and play without necessarily needing a car
- Communities that are safe for pedestrians and bicyclists
- Reduced generation of air pollution through replacement of vehicle trips with walking and bicycling

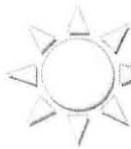
We have reviewed the information on the infill project proposed by Rosen Development LLC in midtown Sacramento at 24th and T Streets. Many features of the project will promote public health and physical activity, including that it's a short walk to the closest light rail station, its proximity to many retail outlets and services, the interior bicycle storage, and its pedestrian and bike-friendly orientation. The car is accommodated by the alley-loaded garage but it is not emphasized. We support the five proposed off-street parking spaces and see no need for any additional parking as this project will attract residents who do not rely exclusively on cars for transportation. The project would improve infrastructure for walking by eliminating a driveway, which results in one less conflict point with cars. These features motivate residents to walk, bicycle, or use public transit to arrive at nearby jobs, services, and shopping, providing them the benefits of increased physical activity and giving them a direct, personal opportunity to reduce air pollution by driving less. Also, as more residents walk, bicycle, and take public transit to their destinations, increased social interaction and "eyes on the street" form a safety net that promotes community connectedness, reduces incidence of depression and other mental health issues, and can reduce the incidence of crime and violence.

Design Sacramento 4 Health supports the proposed project at 24th and T Streets as currently proposed for its potential to contribute to improving public health in the city of Sacramento. Further, we support your compliance with the current Sacramento General Plan by building at least six dwelling units on this large parcel near light rail giving more central city residents a unique opportunity to live in newly- built high quality housing near light rail. If you have any questions, please do not hesitate to contact me at 916-874-5257, or TDuarte@airquality.org.

Sincerely,

Teri H. Duarte, MPH, Chair
Design Sacramento 4 Health

Cc: Councilman Robert King Fong
Mayor Kevin Johnson
David Kwong, City of Sacramento
Andrea Rosen, Rosen Development LLC



SMART GROWTH LEADERSHIP RECOGNITION PROGRAM

October 28, 2010

Rosen Development, LLC.
2226 Portola Way
Sacramento, CA 95818
Andrea Rosen: (916) 457-6721, cell:(916) 508-6721
andrearosen@sbcglobal.net
Ben Rosen: cell:(916) 761-1912 bcrosen@gmail.com

Re: Letter of Endorsement for Courtyard Housing at 24th and T Project

Dear Andrea Rosen:

On behalf of the Smart Growth Leadership Recognition Program, we are pleased to inform you that your project **Courtyard Housing at 24th and T**, on the basis of the application and exhibits you submitted and your presentation to the program review committee on October 7th 2010, the committee and Boards of Directors of the American Institute of Architects Central Valley (AIACV), and the Environmental Council of Sacramento (ECOS), find that the project meets the guidelines of the Smart Growth Leadership Recognition Program and have awarded this Endorsement.

A copy of this endorsement letter will be posted on the websites of ECOS and the AIACV in a locked PDF format. Copies of endorsement letters will only be provided to third parties with your written consent.

We appreciate the value and sustainability that the proposed Courtyard Housing at 24th and T project will provide the community. We hope this Endorsement will help you advance your project. This endorsement must be re-evaluated if the project undergoes significant changes and this Endorsement letter is then no longer valid for use as a vehicle of support.

Endorsements are based on the overall quality of a project and made in an effort to be proactive without waiting for completion of legal and environmental review. Endorsement does not reflect an opinion as to project consistency with any requirements that may apply to governmental agency consideration or approval.

Any recognition regarding this Endorsement should be made in the name of the Smart Growth Leadership Recognition Program, a joint program of AIACV and ECOS.

We are looking forward to seeing the Courtyard Housing at 24th and T project completed and wish you success.

Sincerely,



President
AIA Central Valley



President
ECOS

David Hung

From: Karen Jacques [threegables@macnexus.org]
Sent: Sunday, September 26, 2010 9:04 PM
To: David Hung
Cc: Rosen Andrea
Subject: Z10-142

Dear Mr. Hung,

I am a long term Central City activist and also own property within the Newton Booth neighborhood. (My four properties are within approximately three blocks of the project.)

I have had the opportunity to review this project in detail and am writing in support of it. The house and garage that were formerly on the site (the only properties on the site) were both in extremely poor condition and it did not appear to be economically feasible to repair them. This project, which consists of six apartments and five garages, is both attractively designed and makes good use of the entire site. It is a modern take on two older courtyard apartments that are located in the same block. While modern in design, the project is attractive and is compatible with those earlier apartments in terms of height and massing. It will eliminate the blight of a building that has been vacant and deteriorating for the past several years and will be a pleasant addition to the neighborhood. I also appreciate that the project design will result in the elimination of a driveway that is currently on 24th Street and thus create an extra on street parking space on the block. In my view, that extra space provides mitigation for the fact that the project has six units and only five garages.

The project owner, Andrea Rosen, has indicated that she is considering building the apartments to condominium standards so that they could be converted to individual ownership at some future date. I hope she will do this as the units are attractive enough that owner occupants would be interested in them and the neighborhood, like all Central City neighborhoods, could benefit from more owner occupants.

I appreciate Ms. Rosen taking the time to meet with me about this project and I also appreciate the opportunity to comment on it.

Sincerely,

Karen Jacques
threegables@macnexus.org

cc: Andrea Rosen

David Hung

From: ANDREA ROSEN [andrearosen@sbcglobal.net]
Sent: Monday, October 11, 2010 9:46 AM
To: David Hung
Subject: Fw: Re: 24th and T Courtyard Housing

Follow Up Flag: Follow up
Flag Status: Flagged

Andrea Rosen

(916) 457-6721
andrearosen@sbcglobal.net

--- On Sun, 10/10/10, Gene and Kay Knepprath <kaygenek@saclink.csus.edu> wrote:

From: Gene and Kay Knepprath <kaygenek@saclink.csus.edu>
Subject: Re: 24th and T Courtyard Housing
To: "ANDREA ROSEN" <andrearosen@sbcglobal.net>
Date: Sunday, October 10, 2010, 10:07 PM

I sent it in Word X which is not the latest version.

KAY KNEPPRATH

2620 P STREET

Sacramento, CA, 95816

David Hung
Planning Department
City of Sacramento

RE: Z10-142

Dear Mr. Hung:

I am writing to support the proposed development of the 24th & T Street Courtyard Housing by Rosen Development, LLC. I have reviewed the plans for this site and believe that the proposed project will be an improvement to the area.

A known and reliable mother and son although a new corporation, form Rosen Development, LLC. Andrea Rosen is active in the community and in her neighborhood. Her son, Ben, works for a non-profit housing developer. Both are experienced in developing housing. They have contracted with Ron Vrilakas, one of

Sacramento's architects known for his sensitivity to neighborhood values. This team will build a project of which the Poverty Ridge neighborhood will be proud.

This proposal is innovative in its land use, merging two lots, one of which has lain fallow and the other that has had a vacant house on it for some time. Six units of superior rental housing will be added to the neighborhood, and a blighted corner will be transformed into an eye-catching building.

I urge that the special permit to waive one parking space be granted. On street parking is historically acceptable in the neighborhood. Few residences had parking when they were built in the early 1900's. Additionally, one curb cut will be filled, making on-street parking accessible. I also urge that the special permit to reduce the street side setback for the garage building be granted and that the Plan Review-New Site Plan be approved.

If you would like to contact me for more information, I'm at 916-457-3793.

Sincerely,

Kay Knepprath

On Oct 10, 2010, at 9:48 PM, ANDREA ROSEN wrote:

Thanks Kay. but I can't open it. what do you suggest?

Congrats to Paul on his BIG upcoming birthday. I remember mine!

Andrea Rosen
(916) 457-6721
andrearosen@sbcglobal.net

-- On Sun, 10/10/10, Gene and Kay Knepprath <kaygenek@saclink.csus.edu> wrote:

From: Gene and Kay Knepprath <kaygenek@saclink.csus.edu>
Subject: Re: 24th and T Courtyard Housing
To: "ANDREA ROSEN" <andrearosen@sbcglobal.net>
Date: Sunday, October 10, 2010, 4:15 PM

Andrea: Here's my letter. Glad to do it. Hope it helps.
Kay

March 16, 2011

Michael Monasky
9035 Plaza Park Drive
Elk Grove, CA 95624
916-832-5750
Thursday, January 13, 2011

David Hung, Planner
City of Sacramento

RE: ROSEN PROJECT AT 24TH & T STREETS

Dear Mr. Hung:

At the January 6, 2011 Design Sacramento For Health (DS4H) meeting, I listened to the project proposed by developer Andrea Rosen and I am in support of the concepts she communicated.

First, the project includes five garages for six units. This will require a variance from the city, but it will put cars in the alley instead of on the street or in the front yards.

The project includes six large units with individual patios.

There is a large, outdoor commons area in the center of the project, which includes bike racks for visitors.

The project will accommodate a flat or pitched roof, whichever the neighbors prefer.

The project is endorsed by: Michael McKeever and the Sacramento Area Congress of Governments which fits the Regional Blueprint as smart growth, and; the Sacramento Area Bicycle Advocates, as the project allows for reduced automobile use and increased bicycle and pedestrian use.

The project is less than two blocks from light rail.

The project features upscale, moderately priced rentals that serve the midtown, government office, and hospital service sectors.

Ms. Rosen's project deserves special consideration by the Planning Department, the Planning Commission, and the Sacramento City Council. It reflects the best conversion of property to smart growth development. The city is fortunate to have such a thoughtfully designed project before it for consideration. I heartily endorse the concept as it builds community with its shared commons spaces, and is a pedestrian-friendly project in the interest of smart infill growth that will encourage healthy, aerobic activities such as walking, bicycling, and use of public transit.

Sincerely,

Michael Monasky

David Hung

From: b.burg@comcast.net
Sent: Monday, February 14, 2011 2:24 PM
To: David Hung; Robert King Fong; ANDREA ROSEN; Lisa Nava
Subject: Letter of Support for Z10-142, Courtyard Condominiums at 24th and T

To whom it may concern:

I am a resident of the Winn Park neighborhood, four blocks from the project site. As a student of Sacramento's history and the history of urban planning, transport and development, I wish to address how the 24th & T project reflects and meets the historic patterns of the neighborhood where it is planned.

This letter is intended to express my support for Project Z10-142, the 24th & T Courtyard Condominiums project. The project is located between the Poverty Ridge and Newton Booth neighborhoods, whose development was driven by the presence of a streetcar line along T Street that allowed residents easy access to downtown Sacramento. Unlike postwar suburban neighborhoods, streetcar suburbs like this neighborhood included both single-family and multi-family housing on small lots, with the highest property values and highest densities adjacent to the transit line. The built environment of the neighborhood reflects this diversity of density, as a mixture of single-family homes and 1-3 story apartment buildings, apparent even after 70 years without a streetcar on T Street. The end of streetcar service, changes in development policy and other factors caused a shift in later neighborhoods to promote exclusive single-family neighborhoods over the mixed, diverse and walkable neighborhoods of previous eras, and dependence upon the automobile encouraged low-density neighborhoods that limited walkability and excluded public transit. Even in that era, this neighborhood maintained its walkability, reflected by the creation of high-density apartments on single lots.

As American cities like Sacramento rediscover the urban planning lessons learned a century ago, using terms like "transit-oriented development" and "walkable neighborhoods", development projects like this one represent a return to traditional neighborhood densities. The project site still has close proximity to transit and a street network well-suited to walking and cycling. The proposed density is effectively identical to the neighborhood's historic density in the era of its construction. The proposed units are lower in density than many of the mid-century apartment buildings in the neighborhood, with unit sizes more reflective of the neighborhood's historic homes. This project represents an excellent balance between unit size, affordability and density, within a walkable historic neighborhood.

While the project area is not within a currently listed city historic district, the neighborhood's architectural character is very similar to many of Sacramento's currently listed historic neighborhoods. The proposed buildings have a height and form that does not overpower or overshadow adjacent buildings, with roof height equal to or lower than neighboring buildings. Their architecture does not seek to mimic a particular historic architectural style, but is visually similar to earlier flat-roofed Art Deco and Prairie style buildings found within the neighborhood, constructed during the streetcar era. Since their function replicates the neighborhood's historic use as a mixed-density residential area, their architectural form expresses a return to that historic function.

The current project will add at least one on-street parking space by filling in a former curb cut, but I encourage the applicant and the city to include the condition that parking along 24th Street be converted to angled parking. This will provide additional parking spaces to address neighborhood

concerns about increased parking demand due to this project. I also encourage the city and the applicant to use a lighting plan that provides safe and well-lit routes from these residences along the sidewalks to the street, an amenity that will benefit the safety of both the project residents and those already in the neighborhood.

I strongly encourage the City of Sacramento to support this project.

William Burg

David Hung

From: Ruth Ann Bertsch [rbertsch@stanfordalumni.org]
Sent: Saturday, February 26, 2011 6:20 AM
To: David Hung
Cc: 'ANDREA ROSEN'
Subject: Sacramento needs Andrea Rosen to build the courtyard houses at 24th and T
Attachments: Andrea Rosen email for the Planning Commission b.doc

4237 J Street
 Sacramento, CA 95819-3743
 (916) 454-4021
 Pager (916) 499-0239
rbertsch@stanfordalumni.org

February 26, 2011

Dear Planning Commissioners:

Andrea Rosen's proposed six-unit housing development at 24th and T is a wonderful opportunity for Sacramento and should be approved. Speaking as a physician, her method of elegantly housing several families in this medium density plan will reduce pollution in Sacramento and improve its health. From a taxpayer's and amateur urban planner's perspective, locating up-scale housing so close to existing light rail supports the local economy and increases tax revenue to our city.

The residents of the 24th and T Courtyard will be more likely to use public transit than most Sacramentans. T and 24th is only 1.5 blocks away from light rail. It is within the area of the city that is easily navigable by bicycle. The Courtyard project offers a single car garage, promoting families that prefer to rely heavily on alternative modes of transportation. The more middle and upper class denizens use public transportation, the more everyone uses it.

This translates into less pollution (and more revenue for public transit). If the handful of households of the 24th and T Courtyard weren't living in the Courtyard, they would be living in the suburbs, driving cars more and polluting the air Sacramento breathes. It may seem hard to believe that a handful of homes can make a difference in public health. However, when Atlanta increased its bus services during the Olympic games, the number of pediatric visits to emergency rooms for asthma attacks dropped (*JAMA* 2001 285:897-905). Speaking as a hospital physician, Sacramento suffers from a disproportionately high level of respiratory disease that is worsened by pollution, including emphysema, COPD, and asthma.

Infill housing encourages more people to use active modes of transportation more often. This helps reduce obesity, depression, arthritis, diabetes, and hypertension. Obesity and its companion complications are the biggest growing public health problem of this century. Approximately half of Americans are overweight now. One third of the kids born in the last decade will develop diabetes. Diabetes shortens people's life spans approximately ten years, and the last year of life is frequently not fun – often full of painful neuropathies, amputations, and dialysis. Although *individuals* can often commit to losing weight and exercising more, a public health problem needs a public health solution. Encouraging people to walk or bike more and making it more difficult to drive is very effective. Just a few blocks of walking to a light rail station can reduce a population's weight by approximately five pounds over 1-1.5 years (*Am J Prev Med* 2010;39(2):105-112.)

From the perspective of a fellow resident of Sacramento, having more people outdoors walking and biking in the streets fosters strong communities, for which some studies show American consumers are willing to pay

more per house. Denser housing also increases spending within Sacramento, as opposed to more far-flung locales which are more accessible by car. People who use active modes of transit tend to spend within a very small radius around their home.

Sincerely,

Ruth Ann Bertsch, M.D., Ph.D., F.A.C.P.

Ruth Ann Bertsch, M.D., Ph.D., F.A.C.P.
Assistant Clinical Professor, U.C. Davis School of Medicine
pager (916) 499-0239
email: rbertsch@stanfordalumni.org
home: (916) 454-4021

March 16, 2011

Courtyard Condominiums (P10-089)

David Hung

From: Matthew Gerken [jmatthewgerken@gmail.com]
Sent: Saturday, February 26, 2011 7:48 AM
To: David Hung
Subject: Support for 24th and T Street Courtyard Condominiums

Dear Mr. Hung,

This email indicates my support for approval of the 24th and T Street Courtyard Condominiums project.

This project would greatly enhance the neighborhood in which it is proposed, as well as provide a range of benefits for the City as a whole. Additional "rooftops" in this area will support existing businesses and could, along with other similar residential activity in the area, provide the criteria needed by future businesses to open in the area. Improvements to property and structures will increase the property tax base for the City. Activating this area with buildings that are oriented to the street and sidewalk, with additional "eyes on the street" will enhance safety and security. Establishing more compact housing choices near the 23rd Street light rail station will help support Regional Transit ridership and provide the opportunity for more workers to live near job centers downtown and in midtown and to access those jobs via light rail.

At times, private and localized concerns can derail thoughtful development plans and projects with broader benefits for the entire community. In those instances, it is the responsibility of urban planners to help our appointed and elected leaders understand the full range of issues and benefits. It is important to frame the discussion for projects such as this at the neighborhood and communitywide level, and to underscore the consistency between the project at hand and the recently adopted General Plan. I am confident that any objective analysis along these lines would lead staff and the Planning Commission to support the proposed project.

Thank you for considering my comments and for your public service.

Sincerely,
J. Matthew Gerken, AICP
Sacramento, CA 95819

David Hung

From: Jon Marshack [jon.marshack@att.net]
Sent: Saturday, February 26, 2011 3:20 PM
To: David Hung
Cc: Andrea Rosen; Steve Cohn; Robert King Fong; Ron Vrilakas
Subject: 24th and T - Courtyard Condominiums

Mr. Hung,

I am writing to express my support for the Courtyard Condominiums project proposed for the northwest corner of 24th and T Streets in Midtown. I have reviewed the plans and elevations for the project, have walked the neighborhood where this project is proposed to be built, and have met with the project proponent, Andrea Rosen. I believe that this project is a good fit for the neighborhood. Its scale, massing, style, and density of dwelling units is quite compatible with the context in which it is proposed to be built. It fits the 2030 General Plan goal of adding additional density to the Central City while also meeting the Central City Neighborhood Design Guidelines that govern this project. Drawing on art deco and art moderne stylistic elements of the 1920s and '30s, the Courtyard Condominiums project will complement its surrounding, somewhat eclectic neighborhood of high water bungalows, tudor courtyard apartments, and two story apartment buildings. (Note: I am ignoring the shingle-fronted four-plex immediately east of the project across 24th Street as an aberration from a time before neighborhood design guidelines were created.)

This project situation is nothing like that of the proposed 2207 C Street project that was recently called up to City Council by Councilman Cohn. That proposed single family project had both scale and design that were out of place in the immediate neighborhood of small, single family pre-WW II residential structures. While in my opinion not completely supportable by the Central City Neighborhood Design Guidelines, the City Council's decision to either accept the proposed project's scale or its design, but not both, was an appropriate compromise. No comparison should be made between 2207 C Street project and the proposed Courtyard Condominiums project.

Dr. Jon B. Marshack
2308 H Street
(916) 202-8331

David Hung

From: Nita Davidson [nadavidson@att.net]
Sent: Sunday, February 27, 2011 11:19 PM
To: David Hung
Cc: Robert King Fong; Steve Cohn
Subject: Courtyard Condominiums, T & 24th Streets

Dear Mr. Hung,

I'm writing in support of the infill project, the Courtyard Condominiums, proposed for 24th and T streets.

As both an environmentalist and preservationist—and resident of Midtown since 1988—I support sensible infill. I've seen the plans for the Courtyard Condominiums and am impressed by the well-designed Art Deco look of the project and the number of units for this double parcel. The size of each unit is perfect for people who want manageable, affordable space in a prime urban spot. The project's location—close to light rail, jobs, and Midtown attractions—characterizes it as smart, sustainable infill.

I opposed the project at 2207 C Street because it didn't represent good infill. Unlike the Courtyard Condominiums, the proposed C Street project was a three-story, single-family home that towered over the quaint, one-story C Street houses like a suburban eyesore. The neighbors who opposed that project did so because the house was architecturally discordant with their houses. Also, the house's location would have compromised the problematic sewer system. The C Street neighbors do not embrace NIMBYism. In 2004 they actively rallied behind a major infill project, the development of Courtyard School, despite the traffic and noise the school has generated in the area.

The Courtyard Condominiums project is ideally situated for its location, fits in well with the Sacramento General Plan, and exemplifies smart growth for Midtown.

Sincerely,

Dr. Nita Davidson
714 21st Street
Sacramento 95811

David Hung

From: sarah underwood [skunderwood@gmail.com]
Sent: Saturday, February 26, 2011 10:49 AM
To: David Hung
Subject: YES on 24th and T Street Courtyard Condos (P10-089)

I encourage you to support the T Street Courtyard Condos. I'm a mid-town resident, and in favor of this development.

Sincerely,
Sarah Underwood

Attachment 3 – Opposition Letters

1. Petition #1
2. Petition #2
3. Bill Robertson
4. Heather C. Scott
5. Mabel Lee Robbins
6. Christina Jewett and Floyd Marvin
7. Susan Woodward (“Kelley”)
8. John Hagar
9. Alex Zabelin (President of Newton Booth Neighborhood Association)
10. Pat Melarkey
11. Pamela J. Wade
12. Bridget Whitted
13. Steve Whitted
14. Marlene Rice
15. Linda A. McNamara
16. Timothy Gussner
17. Alan LoFaso
18. Claire Pomeroy, MD, MBA
19. Letter sent to Claire Pomeroy
20. Michael Trostel

PETITION #1

October 18, 2010

City Councilman Rob Fong
City of Sacramento
916 I Street
Sacramento, CA 95814

Subject: 24th and T Streets Courtyard Apartments Project

Dear Councilman Fong:

We are strongly opposed to the proposed Courtyard Apartments Project for the following reasons:

1. Currently, there are multiple apartment complexes located in the neighborhood where this project is being proposed, please see the attachment.
2. Parking in the neighborhood is already a problem. Currently, there are two four-unit apartment complexes directly across the street from the proposed site which do not provide parking and require on-street parking only.
3. We strongly oppose the design of the project. It doesn't fit in with the architectural design of the current neighborhood. Most of the current homes were built in the 1920's and 1930's. The flat top roofing, aluminum windows, and vertical wood siding and stained cedar siding, we believe would not only look out of character for our neighborhood but be an eye-sore.
4. We value the quiet neighborhood, single family homes from a specific era and feel there is no need to add additional multi-unit housing in these areas. We strongly oppose this multi-unit project.

We and most of our other neighbors are appalled that the home at this location was destroyed without any advance notice – at least within a block of the site. We are angry that we were not given an opportunity to express our concerns to preserve this home and maintain the character of our neighborhood.

Respectfully submitted,

Neighbors

24th and T Streets Courtyard Apartment Project – OPPOSITION

Name	Address	Phone or Email
<u>Jenny Gomez</u>	<u>1914 24th St.</u>	<u>marygomez1@srbcglobal.net</u>
<u>Mary Gomez</u>	<u>1914 24th St.</u>	<u>marygomez1@srbcglobal.net</u>
<u>Linda M. Thomas</u>	<u>1911 24th St.</u>	<u>amaroo2@sbcsglobal.net</u>
<u>Anna Miller</u>	<u>2400 S St.</u>	<u>tanitrosteL@comcast.net</u>
<u>Kristine Celorio</u>	<u>2320 T Street.</u>	<u>Kristine.Celorio@yahoo.com</u>
<u>Jose H. Pardo</u>	<u>2305 T St</u>	<u>JHASKO@ATT.NET</u>
<u>Mabel Lee Robbins</u>	<u>2010 24th Street</u>	<u>mrobbin@pacbell.net</u>
<u>Reed Richerson</u>	<u>1915 24th St.</u>	<u>reed.richerson@gmail.com</u>
<u>Tim Gussner</u>	<u>1911 24th St</u>	
<u>Terry Guilford</u>	<u>1921 24th St.</u>	<u>tguil75_@hotmail.com</u>
<u>Aracelio Reyes</u>	<u>2112 24th St</u>	<u>451-41691</u>
<u>Susan Woodward</u>	<u>2006 24th St. So</u>	<u>2006SEW@comcast.net</u>
<u>Susan Maastucht</u>	<u>2325 T. Street</u>	<u>Apt E suzmaasto@yahoo.com</u>
<u>Nikki Corbett</u>	<u>1914 24th St</u>	<u>nikkicorbett@gmail.com</u>
<u>ALBERT MONCADA</u>	<u>2009 24th ST</u>	<u>916-416-1786</u>
<u>Eulalia Moncada</u>	<u>" "</u>	<u>000-000-0000</u>
<u>Ma. A. Moncada</u>	<u>2009-24 St.</u>	
<u>Charles Hippie</u>	<u>2020 24th St</u>	<u>(916) 320-4084</u>
<u>Leonard M. Chy</u>	<u>2623 St</u>	<u>(916) 617 6661</u>
<u>Nicole Pettit</u>	<u>1915 24th St</u>	<u>chole531@gmail.com</u>
<u>Atencia Romeo</u>	<u>1908 24th St</u>	<u>(916) 739-0576</u>

Attachment

See attached Land Use Map of 24th & T Streets:

A survey was done of the areas between from 23rd and S to 25th and S Streets and from 23rd and T to 25th and T Streets and there were 156 multi-family units and 13 single family homes found, which has been highlighted on the attached map:

PETITION #2

PETITION AGAINST THE APPROVAL OF PROPOSAL Z10-142
(6-dwelling unit courthouse apartment building at 24th & T Street)

We the undersigned homeowners and residents of the Poverty Ridge and Newton Booth neighborhoods who live immediately around and near the intersection of T Street and 24th Street wish to express our opposition to the requested 6-dwelling unit apartment structure with the file number Z10-142 as it is currently proposed.

While we acknowledge that the developer and the architect of the proposed structure have contended a sensitivity to the concerns of the surrounding neighborhood, and while the proposal, for better or worse, may fall within the City of Sacramento's current zoning and development guidelines, we contend that the impact of the structure's scope and scale, as well as the waivers requested, will adversely affect the quality of life in the neighborhood with regard to:

- The overall historic and aesthetic profile of a neighborhood that is already challenged by inappropriate and haphazard design approvals from prior decades.
- The already overwhelming density of on-street parking.
- The delicate and tenuous balance that presently exists between single family residences and large multi-unit complexes.

Further, while acknowledging the courtesy and assistance of City planners when contacted, as a point of interest, we wish to express our displeasure with existing City guidelines:

- That allow for the demolition of a home and its vegetation in a residential neighborhood without fair notification to surrounding homeowners before approval of a replacement structure has been granted.
- That in so doing leverages debate in such a way that contesting neighbors must accept either a dirt lot with a chain-link fence or undesired construction.

NAME

ADDRESS

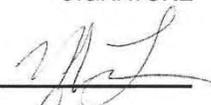
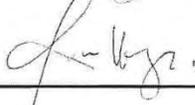
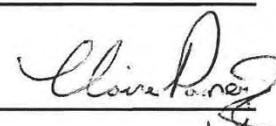
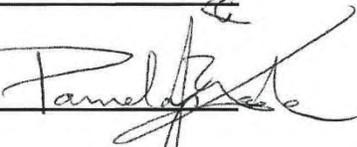
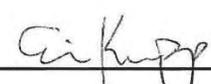
SIGNATURE

William P. Robertson 2009 23rd St. William P. Robertson

Bridget Whitted 2314 T Street, 95816 Bridget Whitted

Jacqueline Spitalo Jacqueline Spitalo
2030 24th St

PETITION AGAINST THE APPROVAL OF PROPOSAL Z10-142
 (6-dwelling unit courthouse apartment building at 24th & T Street)

NAME	ADDRESS	SIGNATURE
Nick Timothy	2110 24th St	
JOHN HAGAR John Hagar	2417 U Street	
Biane Crawford	2413 U St.	
Bill Rader	2423 U ST	
Steve Whitted	2314 T Street	
DANNY RENTSCHLER	2021 24TH STREET	
Eddie Bulesteros	2325 T St. Apt. A	
Claire Pomeroy	2009 23rd St.	
Pamela S. Wade	2020 24th St	
" Landlord	2408 T Street	
Eric Knapp	2016 24th St.	

PETITION AGAINST THE APPROVAL OF PROPOSAL Z10-142
 (6-dwelling unit courthouse apartment building at 24th & T Street)

NAME	ADDRESS	SIGNATURE
<u>Dana Ngo</u>	<u>2016 24th St Sacramento, 95818</u>	<u>Dana Ngo</u>
<u>Alan LoFaso</u>	<u>2001 24th Street Sac 95818</u>	<u>Alan LoFaso</u>
<u>Christina Jewett</u>	<u>2424 T St. Sacramento 95816</u>	<u>Christina Jewett</u>
<u>Floyd C. Marvitt</u>	<u>2424 T St. Sacramento, CA 95816</u>	<u>Floyd C. Marvitt</u>
<u>Susan Woodward</u>	<u>2006 24th St Sacramento, 95818</u>	<u>Susan E Woodward</u>
<u>Cole Miller</u>	<u>2000 24th St. Sacramento, 95818</u>	<u>Cole Miller</u>
<u>Huy Ngo</u>	<u>2016 24th St Sac, 95818</u>	<u>Huy Ngo</u>
<u>Kathleen Strawser</u>	<u>2308 T St #1 SAC CA 95816</u>	<u>Kathleen Strawser</u>
<u>James J. Sims</u>	<u>1308 T St #5 SAC CA 95816</u>	<u>James J. Sims</u>
<u>Rene Strawser-Brusacoram</u>	<u>2308 T St. #1 Sac., CA. 95816</u>	<u>Rene Strawser-Brusacoram</u>
<u>Ellen Shinfuku</u>	<u>2015 23rd St SAC CA 95818</u>	<u>Ellen Shinfuku</u>

PETITION AGAINST THE APPROVAL OF PROPOSAL Z10-142
 (6-dwelling unit courthouse apartment building at 24th & T Street)

NAME	ADDRESS	SIGNATURE
YUJI P. SHINFUKU	2015 23RD ST	<i>Yuji P. Shinfuku</i>
Robert McCartney	2014 25th St.	<i>Robert McCartney</i>
Karen A. Nelson	2014 25th St.	<i>Karen A. Nelson</i>
Frick Whisenhunt	2408 T St	<i>Frick Whisenhunt</i>
Melissa Shinfuku	2015 23rd St.	<i>Melissa Shinfuku</i>
Richard Haroun	2217 U St.	<i>Richard Haroun</i>
Carrie Camarena	2109 25th St	<i>Carrie Camarena</i>
Doug Morrow	2319 T St	<i>Doug Morrow</i>
Susan Maastricht	2325 T. Street	<i>Susan Maastricht</i>
Amber Rankin	2325 T St. Apt P	<i>Amber Rankin</i>
Zenaida Gutierrez	2325 T St. Apt. B	<i>Zenaida Gutierrez</i>

PETITION AGAINST THE APPROVAL OF PROPOSAL Z10-142
(6-dwelling unit courthouse apartment building at 24th & T Street)

NAME ADDRESS SIGNATURE

Daniel Larson 2319 T St. apt A [Signature]

Jennifer Law 2319 T St #A [Signature]

(per email) owner: R Street Mkt. Lofts X
Paul Petrevich Whiskey Hill Lofts

Cynthia Dore 2420 T St. Sec. Co. (916) 838-1503
95816

Empty lines for additional signatures and names.

**WILLIAM PRESTON ROBERTSON
2009 23RD STREET
SACRAMENTO, CA 95818**

Email: w.preston.robertson@gmail.com | Phone: (916) 607-2405

David Hung, Associate Planner
Community Development Department
300 Richards Boulevard, 3rd Floor
Sacramento, CA 95811

RE: Opposition to P10-089

David:

Thank you for allowing me to condense several months worth of detailed correspondences into one final letter regarding the proposed 6-unit development at 24th & T Street (P10-089).

As a member of the core group of neighborhood representatives that formed in the wake of an emotional community meeting facilitated by Councilman Fong's office in November of 2010, I have been a key participant in communications between the developer, Andrea Rosen, and the concerned residents and property owners of Poverty Ridge and Newton Booth who are affected by this project.

Due to fundamentals of the project that by consensus the neighborhood feels are inappropriate to this specific site, and due to the developer's refusal to negotiate or even discuss those fundamentals in meetings, I must strongly oppose the project at this time.

The developer proposes a two-story, flat-roofed courtyard structure comprised of six two bedroom units with five garage spaces on a centrally located corner lot that previously held a 1940's one-story, single family home with garage until it was demolished this past fall to make way for the project.

The consensus of the neighborhood is that the scale, style and ultimately, the ambition of the project as proposed, if constructed, will represent an inappropriate encroachment into the heart of the neighborhood by the sort of high-density infill development more appropriate to the industrial R Street Corridor a block away.

Rather than fulfilling a nuanced transition from lower density historic residences to higher density urban structures, this project promises an abrupt and unsubtle shift in style and density that effectively redraws the R Street corridor to T Street and in so doing, erodes the integrity of a neighborhood that is bordered by rail lines and freeways and that, despite bearing the scars of decades-past missteps in city planning, is nevertheless still a definable neighborhood worthy of respect.

With nuanced transition in mind, the neighborhood proposed an increase of density to three units with garages rather than six, and in a pitched roof style that more appropriately referenced the neighborhood. This was understood to be our starting point for a negotiation that we hoped would work toward a compromise between the developer's goals and the neighborhood's. Sadly, our negotiations never advanced beyond this stage. The developer stated emphatically that the number of units was "not up for discussion," and the expected war of words followed.

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The architect of this project has a reputation in Sacramento for noteworthy urban infill projects. Similarly, the developer has her own reputation within the culture of local urban planning buffs--and drawing on this, she invokes well-honed principles of Smart Growth to promote her project.

The principles of Smart Growth are laudable and embraced by our neighborhood's residents every day. We walk, we bike, we use light rail--property owner and renter alike. We welcome diversity. And we enjoy watching our portion of the city molt its tired industrial skin and yield to a more vibrant retail/residential city as evidenced by the R Street Market development, the Whisky Hill Lofts, the Tuli Bistro condo development and Tapestri Square.

This is not now nor has it ever been a debate on Smart Growth principles. This is a debate over how best to apply Smart Growth principles in a specific location.

No one wants a vacant lot. But nor does one want an inappropriately oversized structure in a high profile neighborhood location.

I, along with other neighborhood representatives, remain open to discussions with the developer over how best to apply Smart Growth in our neighborhood. Until then, I must oppose this project's approval.

Sincerely,

William P. Robertson

1. Initial communication from W.P. Robertson to A. Rosen regarding concerns.
2. A. Rosen responds to W.P. Robertson's initial email.
3. Neighborhood petition of opposition submitted to Rob Fong and David Hung.
4. Account of informal presentation on 12-11-10 by A. Rosen to invited neighbors.
5. A. Rosen informed of 12-14-10 neighborhood meeting: creation of consensus and formation of "core group" of representatives.
6. Account to R. Fong & D. Hung of 12-14-10 neighborhood meeting and initial "core group" meeting; official consensus and names of "core group" submitted; scheduled meeting with A. Rosen for 1-9-11 mentioned.
- 6-A. Neighborhood consensus document as presented to A. Rosen on 1-9-11.
7. Account to R. Fong & D. Hung of 1-9-11 meeting between "core group" and A. Rosen: presentation to her of official neighborhood consensus and discussion.
8. A. Rosen response to neighborhood consensus.
- 8-A. Account to R. Fong, L. Nava, & D. Hung re. "core group" follow-up request by H. Scott to A. Rosen for clarification of her consensus response & A. Rosen's reply; desire by "core group" for negotiation affirmed; request by "core group" to R. Fong for design & density workshop.
9. Request by A. LoFaso to R. Fong for design & density workshop.
10. Account to R. Fong & L. Nava re. A. Rosen email finding fault with "core group" for terminating communications; explanation to Fong & Nava of "core group" position; request for mediation by R. Fong.

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1. Initial communication from W.P. Robertson to A. Rosen regarding concerns.

Date: Wed, 6 Oct 2010 17:05:05 -0700
Subject: 24th & T Courtyard Housing proposal Z10-142
From: Bill Robertson <w.preston.robertson@gmail.com>
To: Andrea Rosen <andrearosen@sbcglobal.net>
Cc: David Hung <dhung@cityofsacramento.org>

Hi, Andrea--

I'm Bill Robertson, the Poverty Ridge resident you called last Friday (but were unable to reach) to discuss your proposed courtyard apartment building at the corner of 24th & T Street. First off, thanks for your swift response after hearing from Associate Planner David Hung. I apologize for not returning the courtesy and getting back to you over the weekend. My wife Claire Pomeroy is vice chancellor and dean at the UC Davis Health System and a string of back-to-back events throughout the weekend that required my participation prevented me from contacting you telephonically. Hopefully this email address, which I pulled from the building proposal will reach you.

To set context, my wife and I live in the big Prairie Style house located at the corner of 23rd and T street, across the street and down the block from your proposed building. About 3 years ago, Claire and I undertook a major renovation of our side yard to replace the off-putting 6-foot wood fence, and rotting wood stairs and decks we inherited when we bought the home. Our new design incorporated a 4-foot stucco wall that matched the house, with 2 feet of wire lattice for vines accented by downward-shining lights on the wall's capped posts. In the yard's interior we built two stucco-and-iron railed porches with stairs, as well as a stamped concrete patio and a large architectural stucco fountain.

Our design intent was two-fold. We wanted to create something that looked architecturally consistent with our 1912 house, and also "communicate" somewhat with the neighborhood. We placed two illuminated corral maples in a recessed part of the exterior wall and we allowed some degree of street interaction with the yard through the vines and iron gates.

Our effort was pretty successful, I think. Neighbors walking home from work have told us that they've changed their route to and from the light rail so that they can pass by our yard. Mothers frequently bring their children and lift them up to peer at our fountain.

So that's where I'm coming from. I like having a nice home that makes my neighbor's feel good, too. My wife and I are big proponents of integrated gentrification.

With this in mind, let me say that I appreciate the design effort you've shown in creating a courtyard apartment. As you know, there are a number of rental structures from the 70's in the neighborhood that gravely lack architectural respect for the neighborhood's

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historical flavor. One of the worse offenders abuts our property on T Street. The people living there are very nice folk. But the apartment building they live in is an eyesore. So thank you for respecting the neighborhood.

Having read your proposal, there are a couple of concerns I would like to raise and they are somewhat linked.

My first concern is with the parking waiver request.

Living on 23rd, I enjoy full, but reasonably uncompetitive parking. This is not the case along T Street or 24th Street. In the evening, those streets are pretty full--particularly when patrons of the Round Corner Bar at S and 24th are taken into account. A busy night there tips the scale. At its current parking capacity, I would say that the neighborhood is manageably saturated at night.

The addition of a 6-unit apartment risks further tipping the scale. The five units of the proposal that have garage parking provided may still yield extras cars on the street, depending on the occupants. Having an entire dwelling's worth of cars definitively added on top of that seems untenable.

My second concern, as well as my wife's, is one of street profile--or the overall roof line of the buildings of the T Street block--and I think it plays into the occupancy issue.

In the current proposal, one of the tallest and chunkiest sections of the structure stands dominantly right at the corner of T and 24th, fairly close to the sidewalk. The artist's depiction in the proposal suggests the height of the structure will be not much higher than the pitched roof of the 1-story Tudor apartment next door. I think the reality will be much different. The "weight" of the building at that corner, will, I fear, impact the overall architectural profile of T Street between 24th and 23rd.

It seems to me that the wish to house 6 dwellings total on the site is a strong influence in this design. I can't help but wonder that if the structure housed 5 dwellings as opposed to 6, a less dominant appearance might be accomplished, with no need for parking waivers.

So that's my spiel. Again, I applaud and thank you for your interest in communicating with the neighborhood, and in the goal to create something attractive. However, I do have concerns about having 6 units on the site both because of parking availability and because of the heavy design it dictates at the corner.

Thanks,

Bill Robertson
2009 23rd Street
Sacramento, CA 95818
916-607-2405

2. A. Rosen responds to W.P. Robertson's initial email.

Re: 24th & T Courtyard Housing proposal Z10-142

ANDREA ROSEN <andrearosen@sbcglobal.net>

Wed, Oct 6, 2010 at 8:50 PM

To: Bill Robertson <w.preston.robertson@gmail.com>

Cc: ron vrilakas <Ron@vrilakasarchitects.com>, Ben Rosen <bcrosen@gmail.com>

Hi Bill

thanks for your email. I'm glad to hear from you. Claire Pomeroy is a local celeb and I know her name if not her likeness.

You'll be tickled to hear that my son (my development partner) and I LOVE your house and its partner, and largely selected our design approach based on your house. We are also inspired by your side yard which graces T Street.

I think the best next step is to meet in person so we can go over the design and I'd like to include my architect so he can explain better the massing and scale.

I'm off to Ashland tomorrow for my annual Shakespeare and other theatre fix and will return late Sunday night.

Can I set something up for us early next week? if so, please give me some available times. Would you like to meet in the neighborhood, say at Temple Coffee or at the architect's office near Zocalo?

I'll bring some larger drawings and we can go over whatever you wish.

Many thanks,

best way to reach me is by cell phone 916 5086721.

Andrea Rosen
(916) 457-6721
andrearosen@sbcglobal.net

3. Neighborhood petition of opposition submitted to Rob Fong and David Hung.

Date: Thu, Nov 18, 2010 at 12:28 AM
Subject: Petition against Z10-142
From: Bill Robertson <w.preston.robertson@gmail.com>
To: "Robert K. Fong" <rkfong@cityofsacramento.org>, Lisa Nava <LNava@cityofsacramento.org>
Cc: David Hung <dhung@cityofsacramento.org>

Rob and Lisa:

Attached please find a petition (in .pdf format) opposing the 6-dwelling unit apartment building at T & 24th Street as it is currently proposed. The project is identified by the Zoning Administration with the file number of Z10-142.

The petition is signed by 39 homeowners, renters and property owners in the neighborhoods of Poverty Ridge and west Newton Booth. This petition is independent of the previously submitted petition, which was submitted by a different group of concerned neighbors. This petition has different wording and different signatures, with, I believe, only one repetition.

I will be hand-delivering the original copy to the Zoning Administration representatives at the community meeting on the 18th.

Thank you,

William P. Robertson
2009 23rd Street
Sacramento, CA

916-607-2405

Z10-142 petition.pdf
1758K

1

4. Account of informal presentation on 12-11-10 by A. Rosen to invited neighbors.

Sent: Tuesday, December 21, 2010 1:02 PM
Subject: Update on Dec. 11 informal meeting w/Andrea Rosen
From: Bill Robertson [mailto:w.preston.robertson@gmail.com]
To: Robert King Fong; Lisa Nava
Cc: David Hung

Rob and Lisa:

I want to catch you up to speed on two recent meetings pertaining to Andrea Rosen's proposed 6-unit apartment development at 24th & T Streets, and where things currently stand in my estimation. This email will concern itself with an informal meeting with the developer on Dec. 11. In a follow-up email, I'll report on the outcome of a Dec. 14 private neighborhood meeting that was held at my house.

Ms. Rosen organized a meeting at the Vizcaya mansion on Saturday, Dec. 11 with a small group of neighbors of her choosing. The neighbors were: me, Kelley Woodward, Heather Scott, Alan LoFaso, Steve Whitted, Stephanie Fiore, Alex Zabelin and Morris Lum. Alex Zabelin and Morris Lum are both board members of the Newton Booth Neighborhood Association, but their attendance was not in that capacity, I don't believe, rather as concerned neighbors. Also, Kelley Woodward and Steve Whitted were present at my urging, since both live near the proposed development site.

It was unclear why Ms. Rosen had called the meeting with this specific group of people, and there was concern among the larger neighborhood (as represented in an email list compiled from the Nov. 18 community meeting), as well as by the attendees themselves, that this group would be considered somehow officially representational of the larger neighborhood when that was not the case. A meeting to create just such a small representational group was scheduled for the following Tuesday, Dec. 14, at my home. Any meetings with the developer before that seemed premature, but the selected neighbors felt that demonstrating a willingness to communicate was also important. The neighbors affirmed their non-representational status throughout the meeting with the Dec. 11 meeting with her.

For context's sake, let me say that there's a certain collective paranoia in the neighborhood that was generated by the unannounced demolition of the house on the proposed project's location as well as by the speed with which the proposal seemed to be advancing in the beginning. This collective paranoia has caused a lot of second-guessing on the part of neighbors as they enter a dialogue with the developer. However, we're all aware of the phenomenon and are doing our best.

The December meeting with Andrea and her selected group was extremely amicable. I think the neighbors present were pleasantly surprised at her congeniality as compared to her more aggressive stance before the project was elevated to its current status with the City. Ms. Rosen presented a new drawing of the proposed 6-unit building's exterior that addressed, to her understanding, some of the concerns expressed by the

neighbors at the larger community meeting on Nov. 18. Also presented was a helpful list that summarized changes to her proposal she was willing to consider.

For clarity's sake, I am attaching to this email three .pdf files of what was in part presented by the developer to the attendees with the understanding that she may already have shown them to you.

The overall impression of the small group of neighbors present at the Dec. 11 meeting was that the drawing and listed changes represented an improvement over the drawing in the Z10-142 proposal currently being considered by the City. The building as represented in the new (and unofficial) drawing was far less visually incongruous in its modernness to the surrounding historic architecture, which neighbors have generally characterized as "modest bungalows." Rather than the sort of exterior one sees in buildings located in industrial or retail-heavy urban environments, the new drawing suggested a more "residential" and "traditional" look, with small decorative porches, smaller windows, a lowered courtyard wall and a modestly pitched roofline among other touches.

The developer noted that she was no longer requesting a waiver on setback and was putting in bike racks, among other touches. She also said that her intention now was to sell the units as condos at the outset rather than renting them for ten years and then selling them as condos as she had previously said, and that an application for this change was being filed.

At the end of the meeting, I stated to Ms. Rosen that while all present seemed to find the artist's drawing an improvement, the larger neighborhood, as I had expressed previously, had yet to meet and establish a consensus. After that had occurred, I explained, whatever representative body was formed might present a different set of concerns and represented opinions, and that she should expect to re-explain what she had shown that day.

After this socially congenial, informal and noncommittal meeting with Ms. Rosen, neighbors who attended the meeting spoke with each other in person and in subsequent communications, and a number of points were noted about the developer's presentation:

The developer stated categorically that she was not going to spend money on further redesigns until a specific redesign was agreed to by neighbors and developer. At least one neighbor at the meeting understood this to mean that a letter of neighborhood support for the project had to be issued to Councilman Fong's office before money was spent on a redesign. The neighbors informally agreed that this required an inequitable leap of faith on the part of the neighbors and a level of trust that had not yet been achieved.

While the artist's drawing was an improvement over the drawing presented in the existing proposal, it was also noted that scale was not significantly diminished. It was

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also noted that photographic examples of similar apartment structures presented by the developer were, like the examples presented by architect Vrillakas at the community meeting of Nov. 18, not from the affected neighborhood specifically and, indeed, did not represent any structure near the site.

Finally, it was noted that while the developer emphasized her application to make the structures condominiums for sale sooner rather than later, she did say that if the economy did not improve the structures would still be rented. It was agreed that the application to sell as condos sooner rather than later was an unimportant factor, and that its relevance to the larger neighborhood was questionable in the first place since many of the concerned neighbors are themselves long-term renters and not homeowners.

In the end it was agreed by the neighbors in attendance that what was accomplished at the meeting with Andrea Rosen on Dec. 11 lay more in the realm of interpersonal dynamics among select individuals rather than anything substantive with regard to the proposed development. All agreed that the private neighborhood meeting on Dec. 14 at my house would mark the true beginning of any negotiation process with Ms. Rosen, and that this had been expressed to her.

In a follow-up email, I'll provide an account of the Dec. 14 meeting at my house.

Thanks,

Bill

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William P. Robertson
2009 23rd Street
Sacramento, CA 95818
916-607-2405

5. A. Rosen informed of 12-14-10 neighborhood meeting: creation of consensus and formation of "core group" of representatives.

Date: Fri, 17 Dec 2010
Subject: neighborhood update
From: Bill Robertson <w.preston.robertson@gmail.com>
To: Andrea Rosen <andrearosen@sbcglobal.net>
Cc: "Robert K. Fong" <rkfong@cityofsacramento.org>, Lisa Nava <LNava@cityofsacramento.org>, David Hung <dhung@cityofsacramento.org>

Andrea--

As promised during the informal meeting at the Vizcaya on Saturday, Dec. 11, this is to let you know that a private neighborhood meeting was held at my house on Tuesday, Dec. 14 during which over 30 members of the neighborhood met to establish an unambiguous consensus regarding your proposed development at 24th & T Street, as well as to create a means by which this consensus could be communicated to you and through which future discussions with you might be held. It was an extremely productive and organized meeting, and included unofficial attendance by members of the NBNA board.

The purpose of this email is not to present any details about the neighborhood's collective stance, but rather to let you know the organizational structure that was created. A core group of approximately 8 neighbors was established to meet with you and represent the larger neighborhood. It will not have bylaws, and it will act wholly independently of the NBNA and not as a sub-committee of the neighborhood association. There are some members of the NBNA board represented in the core group, but they do so in their capacity as private citizens.

I know that your preference is for an ad hoc committee under the guidance of NBNA, but it was decided by all present that greater clarity could be accomplished this way and that an added layer of representation did little to convey the neighborhood's position. You can certainly continue to communicate with the NBNA if you so wish, but please do so with the understanding that with regard to your development, they do not represent an official voice of the neighborhood, nor, as I understand it, do they pretend to with any formal intent.

The informal secretary of our core group suffered a family loss and so we have been delayed pulling together our notes and contact information in a more timely manner, and for this I apologize. I would ask that you give us a few days to mobilize at which point we can set up a meeting with you. At that time, you can present the redesign you presented on Dec. 11, and you can hear from us the consensus that we have been charged with presenting to you. I'm fairly certain that I will be your contact person with this core group, if only due to precedent. But I expect the reigns of communication to be a bit tighter than previously, with less off-the-cuff opining on my part so as not to confuse the neighborhood's position.

Talk to you soon.

Bill Robertson

6. Account to R. Fong & D. Hung of 12-14-10 neighborhood meeting and initial "core group" meeting; official consensus and names of "core group" submitted; scheduled meeting with A. Rosen for 1-9-11 mentioned.

Date: Fri, 7 Jan 2011 18:52:30 -0800
Subject: 24th & T Street development (Z10-142) - neighborhood update
From: Bill Robertson <w.preston.robertson@gmail.com>
To: "Robert K. Fong" <rkfong@cityofsacramento.org>, David Hung <dhung@cityofsacramento.org>, Lisa Nava <LNava@cityofsacramento.org>
Cc: Alan LoFaso <alofaso@sbcglobal.net>, Christina Jewett <christina.jewett@gmail.com>, Doug Morrow <douglas.morrow@asm.ca.gov>, Ed Randolph <efr3@yahoo.com>, Heather Scott <Heather.Scott@sen.ca.gov>, Kelley Woodward <2006sew@comcast.net>

Rob and David:

I trust you both had pleasant holidays. I want to bring you up to date on where things stand regarding Andrea Rosen's 24th & T Street 6-unit building (Z10-142) and the position of concerned neighbors in the Newton Booth and Poverty Ridge areas who have organized over the issue.

As you may recall, after the fairly passionate community meeting arranged by Rob in November, Ms. Rosen arranged an informal private meeting on Dec. 11 with select members of the neighborhood chosen by her, during which she presented a revised drawing of the building. It was a less volatile meeting than the larger community event, and in fact I, along with everyone present, I believe, felt it was even cordial in tone. At the same time, however, the neighbors present asserted to Ms. Rosen that they didn't consider themselves representative of the neighborhood and given that, she should not misconstrue the importance of the Dec. 11 meeting insofar as outreach to the neighborhood was concerned. She was told there would be an organizational meeting of concerned neighbors at my home on Dec. 14, during which a clearly defined consensus regarding her project was hoped to be established along with a more formalized process by which the neighborhood's consensus could be conveyed to Ms. Rosen. After that Dec. 14 meeting, she was told, we would consider good faith communications between the neighborhood and developer to have begun.

The meeting at my house on Dec. 14 was well attended, with a group of more than 30 people present. I was surprised and pleased at how orderly the meeting ran, given the passions expressed at the November event--and I was further impressed, if not amazed, at the consistency of opinions expressed with regard to the preferences for the development. Consensus was pretty instantaneous.

A core group of 8 representatives, comprised of both renters and homeowners, was created to communicate the neighborhood's preferences to Ms. Rosen and pursue good faith communication with her thereafter. The decision was made not to act under the aegis of the Newton Booth Neighborhood Association in our dealings with Ms. Rosen--not for any contrarian reason, but rather for the sake of clarity and decisiveness.

There was some minor confusion between Ms. Rosen and myself in the days following the Dec. 14 meeting with regard to how soon a meeting with her could be arranged. Understandably, she was eager to move forward as soon as possible. And understandably the neighbors, despite their organizational accomplishment, were distracted with the usual activities of the holiday season. I believe you both received the exchange of emails I had with Ms. Rosen during this time, and a quick reading shows that the confusion was of no great consequence.

Now that we are into the New Year, I am pleased to relay that the core group of neighborhood representatives has been able to meet among themselves and that a meeting between that group and Ms. Rosen has been arranged for 2:00 pm this Sunday afternoon, Jan. 9, at the home of Heather Scott, a member of the group.

The core group of representatives was 8, but is now 7, as one of our members had to step down due to work conflicts. Here are the names:

Heather Scott <Heather.Scott@sen.ca.gov>
Christina Jewett <christina.jewett@gmail.com>
Alan LoFaso <alofaso@sbcglobal.net>
Doug Morrow <douglas.morrow@asm.ca.gov>
Kelley Woodward <2006sew@comcast.net>
Ed Randolph <efr3@yahoo.com>
Bill Robertson <w.preston.robertson@gmail.com>

The consensus of the neighborhood that we will be conveying to Ms. Rosen is as follows.

"We, the Concerned Neighbors of Newton Booth and Poverty Ridge, embrace and welcome the growth of a combined neighborhood that represents the very best of urban life--a neighborhood rich in diversity with regard to the culture and economic status of its residents, renter and homeowner alike, and with regard to the architectural history abundantly evidenced in its homes, whether grand or modest.

In the spirit of this, we would suggest that the development proposed for the joined parcels at 2331 T Street and 1918 24th Street at the northwest corner of 24th and T (known as Proposal Z10-142) would best serve the urban planning goals of both the City of Sacramento at large and the specifically affected neighborhoods of Newton Booth and Poverty Ridge, by adhering to the following stipulations:

1. A reasonable increase of density from that of the previous single-dweller, one-story home on .19325 acres, to three housing units.
2. An architectural style of good faith integrity that is appropriate to the immediate surrounding neighborhood within a 300 square foot radius--an area that includes, albeit not exclusively, Tudor cottages, Craftsman bungalows, and Depression-era brick duplexes.

3. Pitched roofs with a height not markedly greater than that of the Mirabella Apartments immediately adjacent to the site on T Street.
4. Setbacks that adhere to city requirements.
5. Porches that evoke an "eyes on the neighborhood" affect while still adhering to the aforementioned setback requirements.
6. Off-street parking accommodations provided for all units."

Rob and David--I realize that there is a striking disparity between Ms. Rosen's proposal and what is entailed above. But at the Dec. 14 meeting at my house, we calmly went around the room of 30-plus neighborhood residents in attendance and asked ourselves the question, "Ideally, what would you like to see?" The answers were consistent for renters and homeowners alike. Our simple goal in this first official meeting with Ms. Rosen is merely to present her with the neighborhood's ideal. It is not our intention to be unreasonable combative, only to represent.

At her informal gathering on Dec. 11, Ms. Rosen voluntarily stated to those of us invited that she was not willing to negotiate on the number of units. Since that gathering was not deemed by us to be a representational negotiation, we don't regard her remark as a fixed declaration of a consideration "not on the table," to use her vernacular. Still, she did say it and we are all aware that she said it.

I am hoping for a productive and reasonable exchange. I will let you know the outcome.

Sincerely,

Bill Robertson

--

William P. Robertson
2009 23rd Street
Sacramento, CA 95818
916-607-2405

6-a. Neighborhood consensus document as presented to A. Rosen on 1-9-11.

We, the Concerned Neighbors of Newton Booth and Poverty Ridge, embrace and welcome the growth of a combined neighborhood that represents the very best of urban life--a neighborhood rich in diversity with regard to the culture and economic status of its residents, renter and homeowner alike, and with regard to the architectural history abundantly evidenced in its homes, whether grand or modest.

In the spirit of this, we would suggest that the development proposed for the joined parcels at 2331 T Street and 1918 24th Street at the northwest corner of 24th and T (known as Proposal Z10-142) would best serve the urban planning goals of both the City of Sacramento at large and the specifically affected neighborhoods of Newton Booth and Poverty Ridge, by adhering to the following stipulations:

1. A reasonable increase of density from that of the previous *single-dweller, one-story home* on .19325 acres, to *three housing units*.
2. An architectural style of good faith integrity that is appropriate to the immediate surrounding neighborhood within a 300 square foot radius--an area that includes, albeit not exclusively, Tudor cottages, Craftsman bungalows, and Depression-era brick duplexes.
3. Pitched roofs with a height not markedly greater than that of the Mirabella Apartments immediately adjacent to the site on T Street.
4. Setbacks that adhere to city requirements.
5. Porches that evoke an "eyes on the neighborhood" affect while still adhering to the aforementioned setback requirements.
6. Off-street parking accommodations provided for all units.

Delivered to Andrea Rosen, developer, on January 9, 2011, per neighborhood consensus taken December 14, 2010.

Representing the neighborhood:

Christina Jewett
Alan LoFaso
Doug Morrow
Ed Randolph
Bill Robertson
Heather Scott
Kelley Woodward

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7. Account to R. Fong & D. Hung of 1-9-11 meeting between "core group" and A. Rosen: presentation to her of official neighborhood consensus and discussion.

Date: Tue, 11 Jan 2011 16:39:55
 Subject: Update on neighborhood meeting with 24th & T developer Jan. 9
 From: Bill Robertson <w.preston.robertson@gmail.com>
 To: "Robert K. Fong" <rkfong@cityofsacramento.org>, Lisa Nava <LNava@cityofsacramento.org>, David Hung <dhung@cityofsacramento.org>
 Cc: Alan LoFaso <alofaso@sbcglobal.net>, Christina Jewett <christina.jewett@gmail.com>, Doug Morrow <douglas.morrow@asm.ca.gov>, Ed Randolph <efr3@yahoo.com>, Heather Scott <Heather.Scott@sen.ca.gov>, Kelley Woodward <2006sew@comcast.net>

Rob and David:

I wanted to check in with a report on the first meeting between the core group of neighborhood representatives from the Newton Booth-Poverty Ridge areas, whose assemblage I reported to you in an earlier email, and Andrea Rosen, the developer of the proposed 6-unit apartment building at the corner of 24th & T Streets. It goes without saying that this is our particular take on what transpired and was communicated, and that Ms. Rosen will have her own interpretation. It would be wonderful if there were at least some similarities between our two accounts, given that common ground is so crucial to the commencement of good faith negotiation.

The meeting was held at the home of Heather Scott on Sunday, Jan. 9. The core group representatives present were:

Heather Scott <Heather.Scott@sen.ca.gov>
 Christina Jewett <christina.jewett@gmail.com>
 Alan LoFaso <alofaso@sbcglobal.net>
 Doug Morrow <douglas.morrow@asm.ca.gov>
 Kelley Woodward <2006sew@comcast.net>
 Bill Robertson <w.preston.robertson@gmail.com>

At the meeting, Ms. Rosen was presented with the document of neighborhood consensus I emailed to you both on Friday. That document remains unchanged, but for the sake of convenience, I am attaching it to this email as a .pdf file. Upon reading the document, Ms. Rosen said that she was "shocked" by the disparity between her position and ours. While this characterization of her emotional state was duly noted, we did not necessarily take the proclamation at face value given the neighborhood passions that were exhibited at the community meeting arranged by Rob in November, and given whatever psychological motivation one might intuit, rightly or wrongly, from her PRA request to the City.

That having been said, and to be fair, she did seem if not flummoxed, at least unprepared or simply unready to offer much in the way of a counter response. She focused on certain details of the document that she felt were "inaccurate" to a degree that required correction. One such detail was that the Zoning Administration ID of her proposal was not Z10-142, and that it had an entirely different ID in light of her

application for condominium status. Another detail she termed "inaccurate" was the document's assessment of previous lot density, given that the lot was comprised of two separate parcels. Neither of these points became subjects of debate, as the core group of representatives had discussed in advance a desire not to become mired in arguing minutiae, but rather to view the meeting as a starting point of basic positions and to try to stay focused on that objective.

One concern expressed by Ms. Rosen that seems reasonable is what the neighborhood meant by the term "three units." At first blush, we felt that "three units" should carry with it at least some general meaning from which a discussion might be launched, but given that issues like parking variances and garages are concerns for any developer, we conceded that clarity on this point was helpful to all. Of particular concern to Ms. Rosen was whether "three units" meant three free-standing structures or three units in a single building, and whether garages were understood inclusions with each unit or not, in which case, apparently, the neighborhood was expressing a preference for one house with two garages or two houses with one garage. We told Ms. Rosen that we would try to get a clearer definition of "three units" from the larger neighborhood and would report back to her.

At this point, Ms. Rosen said that she was not sure how to react to our proposal and would have to think about it. She made an off-the-cuff suggestion that Rob Fong should be brought in to mediate a negotiation between herself and the core group of neighborhood representatives, but the subject was not pursued. Ms. Rosen also referred, as she has in the past, to various Zoning Administration guidelines and urban planning principles that supported her desires for property that she rightfully noted she owned. She also referenced letters of support she had from urban planning organizations. And, as she had stated previously at the informal and non-representational get-together she held among a select group of neighbors chosen by her on Dec. 11, the financial reckoning of her project did not calculate to her benefit with "anything less" than what she was now proposing.

At this point, we attempted to clarify matters by giving our understanding of the process before us. We explained in various voices and with various articulations that we were not her business partners, nor were we zoning guideline experts or urban planning philosophers. The latter two subjects were of interest to us, of course, as they should be to any engaged citizen, but that our primary mission that day was to convey the consensus of the neighborhood as defined by the 30-plus people who had met privately on Dec. 14. (I used the phrase "we don't care" during my own remarks concerning her finances and the zoning legitimacy of her project. It was a histrionic and intentionally colloquial use of language, however, swiftly and rightfully refined by another member of the group.)

We expressed to Ms. Rosen our understanding of the type of situation we all found ourselves in and what we saw our roles to be. The "upside" of being a developer, we noted, was that she stood to make money from her project. However, it was noted, there were challenges to being a developer as well, and one of those was that while she

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may own a property, and while city guidelines might allow development of that property, she must also work in good faith with an intangible known as "the neighborhood." And we were the neighborhood. The neighborhood we said, was comprised of residents who live in an area, who open their doors, and who then simply react to what they see. The developer must deal with the neighborhood's reaction or not at her peril. And the same, we noted, goes for the neighborhood, with respect to their dealings with the developer. This, we said, was called "negotiation." And that was what we hoped for.

At this point, the core group of representatives suggested a walk around the neighborhood to look at the architecture and the general neighborhood profile surrounding the proposed site of Ms. Rosen's development. We had discussed this in advance among ourselves because it seemed like a socially amiable, non-confrontational approach by which we might soft-sell our position to her with a firm reality. I can't say with any conviction that that our objective was successful and perhaps understandably so. We wanted to walk past single-family homes and low profile structures; she wanted to count utility meters and deduce density of multi-unit structures. We wanted to walk in residential areas; she preferred the more industrial landscape of S Street. None of this was contentious. It was mostly an issue of subtle steering of the group and visual and conversational focus. All-in-all, it seemed to produce nothing on which we could build future discussions.

Concluding our meeting, we emphasized to Ms. Rosen that the neighborhood's consensus had been presented that day in the spirit of negotiation, and that we hoped to hear a counter proposal from her. She asked whether such a counter might include "information," and by this we understood her to mean more information about zoning guidelines and urban planning principles. We told her that we didn't find that as useful to forward progress as we did more substantive changes in her proposal. We told her that we were not interested in being unreasonable, that we wanted to negotiate, and we expressed a general desire to "get to yes." We also again said we would try to extract from the neighbors what they meant by "three units."

All in all, I would characterize our meeting with Ms. Rosen as tense at moments, but overall not unfriendly. I should note, however, that no concession to us was given by her--even in the form of a noncommittal consent to negotiate. We did not schedule a second meeting with her--nor did either side suggest one. The only concrete step suggested by Ms. Rosen during our afternoon with her was idea that Rob might somehow act as a mediator between our two parties. I know from email exchanges with Lisa Nava that Ms. Rosen had made such a request previously--and even before our Jan. 9 meeting of Sunday. My recollection was that she was told she must first try to work things out with us herself. I don't think that has happened yet.

It is our hope that she will.

Cordially,

Bill Robertson

8. A. Rosen response to neighborhood consensus.**Response to January 9, 2011 Stipulations Requested by the Concerned Neighbors of Newton Booth and Poverty Ridge from Andrea Rosen and Ben Rosen Regarding P10-089 24th and T Courtyard Condominiums**

In the spirit of good faith negotiation, I offer the following for your consideration and for further discussion. I appreciate the neighbors' recognition of the importance of diversity in urban neighborhoods, such as this one, in both culture and economic status of its residents, multi-family mixed in with single family and in architectural designs.

STIPULATIONS WHICH HAVE BEEN MET OR DISCUSSED AS PART OF ALTERNATIVE DESIGNS**2. Architectural style of good faith integrity that is appropriate to the surrounding neighborhood.**

The current filed design is a contemporary take on many, very nice buildings in the neighborhood including ones not far from our parcel. It repeats the key theme of the largest developments on this half-block- the courtyard- and expands and celebrates this feature. Your stipulation recognizes the architectural diversity of the neighborhood which includes art deco, Moderne and prairie-style designs in addition to Tudor and Craftsman. The materials proposed in the filed design are stucco and brick which are common in this neighborhood as are many of the design features such as stoops. This project will enrich the existing architectural diversity of this neighborhood, however it's worth noting that design is very personal and subjective.

3. Pitched roofs of height not markedly greater than that of the Mirabella.

The current filed design does not feature a pitched roof and is a two story building 23' in height. There are many two-story houses and apartment buildings in the vicinity of this parcel in all directions. Two- storey was selected over the allowed 3 stories in order to minimize massing. See the Sutter Brownstones at 26th and N. The neighbors there worked with the architect and agreed that the best way to reduce massing was to go with a flat roof. Two storey was chosen order to allow open green space in the form of a courtyard for residents and large patios. I can't tell if this stipulation is requesting single story; but if so, it's not an option here. The courtyard will be behind a 4 foot wall which will allow passersby to enjoy it and the fountain. A pitched roof alternative design was presented to neighbors on December 11, 2011. On January 9th, I was told that the alternative designs were discussed at the Dec. 14, 2011 neighborhood meeting.

4. Setbacks that adhere to city requirements.

City required front and side setbacks have been met. The only other setback is for the accessory structure (garage) and a 4' side setback variance is requested in order to allow for 10' wide garages which will accommodate both a vehicle and a few bicycles. This is a limited variance that will run only for the length of the garage -17' out of the 160' length of 24th St side of the parcel.

5. Porches that evoke " eyes on the neighborhood".

Multi-family developments in this neighborhood typically have patios or private backyard spaces like the Mirabella has. They don't typically have porches since porches in the neighborhood are typically raised several feet and set back and up from the sidewalk giving the porch-sitter some privacy and separation from the sidewalk. Adding porches to this project was proposed as integral to the alternative design (not yet discussed). Porches at grade will need to carefully balance and realistically consider the user's feeling of security in order to be worthwhile in practice. Townhome owners report that more real "eyes on the street" results from windows from living spaces on the corners of buildings looking out on the street.

Development Options for this Parcel to Discuss:

Based on the current zoning ordinance which defines the rules for R-3A zoning and Sacramento General Plan designation of Traditional Neighborhood- Medium Density and based on what I can afford to build on this site, here are options to discuss:

A. Single building, 3 stories(35 ft at point where roof starts), seven 2- bedroom units, all City setbacks met, seven parking spaces onsite accessed via the alley(5) and via double driveway off 24th St (2) . Pitched roof, stucco with brick accents. No courtyard. Common patio and walkways. Vintage Traditional design.

B. Two triplex buildings. 7 units total. 3 2-story buildings. 3 2-bedroom units in each two buildings with one 1-bedroom apartment over garage. Accessory structure with 5 garages; plus two on slab at grade parking spaces accessed via double driveway off 24th St. No courtyard. Limited open space. All city setbacks met. All parking onsite. 24th St parking spaces might be under buildings and those units might be one bedrooms as a result.

C. Two triplex buildings with 3 2-bedroom units. Six at grade on slab parking spaces (no garage) accessed via alley. All City setbacks met. Current filed design. Retains Courtyard.

D. Two triplex buildings, six units total. Five garage parking spaces; One parking space accessed off 24th St under north building; May result in 2 2-bedrooms and 1 1-bedroom in north building. All City Setbacks met; Smaller courtyard due to onsite 24th St. parking space. Curb cut on 24th.

E. Two triplex buildings; six units; 5 spaces in garage off alley; one space under North Bldg. Two 2- bedroom units in North building and one apartment over garage. Courtyard smaller (due to onsite parking off 24th St).

I offer these development options as ones that we could afford to build, that would meet within the City's existing zoning ordinance and General Plan designation and may meet the requested Stipulations regarding pitched roofs, onsite parking and City setbacks.

One idea that I would propose for discussion is that we consider petitioning the City for angled parking on this stretch of 24th Street. A Newton Booth property owner suggested this idea and noted that it has been tried in other parts of Newton Booth and midtown with success. It was

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suggested that the City traffic engineer be consulted as to which side of 24th St be converted as only one side is eligible. Angled parking increases the number of spaces possible and slows traffic. I am supportive of exploring this option.

I look forward to meeting soon to discuss these ideas and your concerns.

Signed,

Andrea Rosen and Ben Rosen

January 17, 2011

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8-A. Account to R. Fong, L. Nava, & D. Hung re. "core group" follow-up request by H. Scott to A. Rosen for clarification of her consensus response & A. Rosen's reply; desire by "core group" for negotiation affirmed; request by "core group" to R. Fong for design & density workshop.

Date: Tue, 25 Jan 2011 20:02:57 -0800
Subject: Revised: Update on 24th & T Street development -- Jan. 25
From: Bill Robertson <w.preston.robertson@gmail.com>
To: David Hung <dhung@cityofsacramento.org>, Lisa Nava <LNava@cityofsacramento.org>, "Robert K. Fong" <rkfong@cityofsacramento.org>
Cc: Alan LoFaso <alofaso@sbcglobal.net>, Christina Jewett <christina.jewett@gmail.com>, Doug Morrow <douglas.morrow@asm.ca.gov>, Heather Scott <Heather.Scott@sen.ca.gov>, Kelley Woodward <2006sew@comcast.net>

Rob, Lisa, and David:

I believe this email exchange was CC'd to Lisa last night, but to be safe, I thought I would cut-and-paste the exchange and send it to David and Rob as well in the hope of creating an ongoing account of events.

As you may remember, a neighborhood letter of neighborhood consensus was presented to developer Rosen in the hope of engaging in good faith negotiations over her proposed 6-unit building. After meeting with her, she sent a response to our letter and I forwarded it on to you without commentary because our core group of neighborhood representatives had not yet met to discuss it and form an official position. We have now met.

The core group of neighborhood representatives was concerned that Ms. Rosen's official point-by-point numbered response to us began with **number 2** and did not pointedly address the neighborhood's number one issue, both numerically and literally, which was: "a reasonable increase of density from that of the previous single-dweller, one-story home on .19325 acres, to three housing units." While trying to juggle schedules to meet with Ms. Rosen, we sent an email via core group member Heather Scott, requesting her to directly address the issue in writing.

Below is that exchange. I will continue with my commentary following it.

>>>>Hello Andrea-

Our group is happy to meet with you again to discuss issues related to the 24th and T proposed project, however, with respect to the process, we feel that you need to address one of our primary concerns that this document does not address before we can proceed.

You seem to have omitted a response to issue number one: a reasonable increase of density from that of the previous single-dweller, one-story home on .19325 acres, to three housing units.

Please amend your attached original document then we can reschedule a time to talk.

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Thank you,
Heather Scott<<

>>Heather

I thought that we were trying to have a dialogue- talking together in a group- and that's what I am committed to do. I offered a written response this time because I got a sense that the group at our first meeting that the group expected a written response. I believe the group, especially Doug, acknowledged early in the meeting that the number of units for the parcel is the toughest nut to crack. And I was expecting we would try to address this question head on

I apologize if my more lengthy section on Development Options for this Parcel to discuss: copied below was not sufficiently clear. This section was in response to Requested Stipulation #1; please see now bolded sentence from our response below.

As Bill mentioned either at the most recent meeting or earlier, I did explain at the December 11, 2010 meeting that my proposal is for six units as I cannot afford to build fewer than that number of units and build a high quality project which is my commitment to the neighborhood and myself and the community at large. It's very important to me to build buildings that I can be proud of and that make a positive contribution to the neighborhood and the City. For these and other reasons, our proposal remains at six units.

Six units on 8500 s.f. of land matches the Mirabella (expanded most recently in the 1950's), the Tudor apartments and the other land use of parcels on this half- block. In other words, our proposed use of this double parcel is entirely consistently with the historic land use on this half-block. I have no explanation as to why there was a single house on one of the two parcels but the City has never adopted single family residential for that parcel in spite of the fact that someone chose to build a single family house on that parcel in 1940. The Mirabella apartments has already been built by 1940 at the time that the former house on this parcel was built.

Lastly, as I suggested earlier, I urge you to touch base with David Hung regarding the last possible date he has given my project to submit changes to the City. My hearing date is now set for March 10, 2011 and last Friday David informed me that he must have everything finalized by February 10 for my project. You don't have to take my word for it; contact him.

If these negotiations are going to produce anything in the way of changes to the project, we don't have much time left. I am committed to meeting to try to work something out, but we've got to keep moving forward.

From our written response to the group:

Development Options for this Parcel to Discuss:

Based on the current zoning ordinance which defines the rules for R-3A zoning and Sacramento General Plan designation of Traditional Neighborhood- Medium Density and **based on what I can afford to build on this site**, here are options to discuss:

A. Single building, 3 stories(35 ft at point where roof starts), seven 2- bedroom units, all City setbacks met, seven parking spaces onsite accessed via the alley(5) and via double driveway off 24th St (2) . Pitched roof, stucco with brick accents. No courtyard. Common patio and walkways. Vintage Traditional design.

B. Two triplex buildings. 7 units total. 3 2-story buildings. 3 2-bedroom units in each two buildings with one 1-bedroom apartment over garage. Accessory structure with 5 garages; plus two on slab at grade parking spaces accessed via double driveway off 24th St. No courtyard. Limited open space. All city setbacks met. All parking onsite. 24th St parking spaces might be under buildings and those units might be one bedrooms as a result.

C. Two triplex buildings with 3 2-bedroom units. Six at grade on slab parking spaces (no garage) accessed via alley. All City setbacks met. Current filed design. Retains Courtyard.

D. Two triplex buildings, six units total. Five garage parking spaces; One parking space accessed off 24th St under north building; May result in 2 2-bedrooms and 1 1-bedroom in north building.

All City Setbacks met; Smaller courtyard due to onsite 24th St. parking space. Curb cut on 24th.

E. Two triplex buildings; six units; 5 spaces in garage off alley; one space under North Bldg. Two 2- bedroom units in North building and one apartment over garage. Courtyard smaller (due to onsite parking off 24th St).

I offer these development options as ones that we could afford to build, that would meet within the City's existing zoning ordinance and General Plan designation and may meet the requested Stipulations regarding pitched roofs, onsite parking and City setbacks.

I look forward to hearing from you soon.

Andrea Rosen
(916) 457-6721
andrearosen@sbcglobal.net<<<<<<

Rob, Lisa and David, we seem to be at an unfortunate impasse here in our negotiations with the developer of this project--if indeed what has transpired thus far could be considered "negotiation." My own understanding, and that of my fellow neighborhood representatives, is that negotiations are supposed to be an exchange of proposals and counter proposals that build to a common compromise. This was always our intention. "Three units" was our ideal, just as "six units" was developer Rosen's ideal. We accepted and embraced the idea that common ground had to be found--we still do.

But we have been unable to get developer Rosen to respect and consider our ideal of "three units," so that we can all proceed toward a just and reasonable resolution. Instead, what we have been subjected to is a continued assertion that she is right and we are wrong. When parties counter each other with the exact same position previously stated, offering nothing but variations and "new information" to support their points, this is not, to our understanding, "negotiation." It is simple debate. Debate is a competition

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eventually decided on by panel of judges. Debate is not a process in which "both parties win," which is the true objective of negotiation as we understand it. We want both parties to win. We want to negotiate.

It is correct that developer Rosen, in an obvious proactive maneuver, told the group of hand-picked neighbors with whom she met with on Dec.11 at the Vizcaya House that "anything less than 6 units was not up for discussion," but as that meeting was not representational, but rather an informal gathering of neighborhood individuals chosen by her, we did not consider that statement the beginning of any sort of "negotiation." We told her to allow us to establish a process of communication and negotiation, and for her to respect that process. I think it is fair to say that she has not respected the process. Indeed, she has even taunted us with the absurd suggestion that we consider "7 units."

As I said, our meetings with developer Rosen have been structurally no more forward-moving than a debate. In our case, the aforementioned "panel of judges" who will determine the winner of this debate is in Ms. Rosen's mind apparently the City Planning Board--and if the debate continues beyond that, the City Council. I can't help but feel that in these very challenging times for government, the City has larger issues to manage than a combative situation between a development naif and the neighborhood on whose metaphorical foyer rug she has tracked something unwanted. So let me reiterate:

We want to negotiate.

As a neighborhood, the NewtonBooth/Poverty Ridge area has not had a lot of experience dealing with this sort of controversy. But there is one thing on which we all agree, particularly in light of the passions present at Rob's community meeting last November: We know that we do not want to be a shrill, unreasonable group who are stridently resistant of any and all change in their neighborhood. The "not in my backyard" psychopathology that is so frequently demonstrated in other communities is nothing we wish to emulate. We know that Newton Booth/Poverty Ridge is a city neighborhood and not a suburban one. We are aware that a city must grow and be ever-changing if it is to thrive. We understand and we welcome that reality. It's why we live here.

Common ground is a beautiful thing, I think. Both in a city and in negotiations.

Please consider this the first of what will no doubt be other requests, including one with a lot of signatures from the neighborhood, for a design and density workshop to handle this impasse with the developer of the 24th & T Street proposal.

Cordially,

Bill Robertson

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9. Request by A. LoFaso to R. Fong for design & density workshop.

Date: Wed, 2 Feb 2011 14:05:15 -0800 (PST)
Subject: 24th & T Street Project - Thank you - and request
From: Alan LoFaso <alofaso@sbcglobal.net>
To: rkfong@cityofsacramento.org
Cc: LNava@cityofsacramento.org, dhung@cityofsacramento.org,
Bill Robertson <w.preston.robertson@gmail.com>,
Heather Scott <Heather.Scott@sen.ca.gov>,
Doug Morrow <douglas.s.morrow@gmail.com>,
Kelly Woodward <2006sew@camcast.net>,
Christina Jewett <christina.jewett@gmail.com>

Council Member Fong,

I would like to belatedly thank you for hosting the November 18th community meeting regarding the proposed 6-unit project at the north-western corner of the intersection of 24th and T Streets (No. P10-089). As I am sure you appreciated, there are strong feelings in our neighborhood regarding the historic integrity and architectural authenticity of the Newton Booth/Poverty Ridge area. Many residents have misgivings regarding poorly conceived developments from decades past, and many place great weight on the value of preserving period homes to the greatest extent possible. I associate myself with those views.

At the outset, I believe the community meeting allowed residents to voice their frustration with the unnoticed demolition of the historic home previously located on the corner lot. Although not entirely satisfying, the clear explanation by city staff, in my view, allowed neighbors to move on from that issue to what faces us now-- development of the now vacant lot. Moreover, I believe the meeting helped give focus to neighbors' anxiety regarding poorly communicated intentions of the developer by making the city planning process and resources more accessible to those not familiar with the workings of City Hall.

As I know you're also aware, my neighbors and I are not opposed to development on the now vacant lot. In fact, the neighbors have offered to discuss with the developer a proposal focusing on a 3-unit development with a mass and scale more consistent with the immediately surrounding architecture. While there are many differences in details and emphasis, there is close to consensus among the neighbors regarding the appropriate size of the development.

Efforts to work positively with the developer have not been successful, as the developer has shown no willingness to discuss the 6-unit mass/scale or any inclination to revise the project along those lines. Most neighbors understand that a fair negotiation between neighbors and developer is likely to result in a project that will not conform to our ideal 3-unit suggestion. However, if there is no dialogue, we cannot arrive at a fair and reasonable result for all.

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My neighbors and I have recently become aware of the idea of a design and density workshop to enable neighbors to express their concerns and offer constructive suggestions to inform the city planning process of alternatives supported by the neighborhood. Given the lack of constructive engagement by the developer, I join my neighbors in requesting that the city conduct such a workshop for this particular project. I believe it would be a productive use of this process to give positive, focused input into the planning process regarding this project.

Again, thank you for your actions in support of the Newton Booth/Poverty Ridge neighborhood and, in advance, for your consideration of my request.

Regards,

Alan LoFaso
2001 24th Street

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10. Account to R. Fong & L. Nava re. A. Rosen email finding fault with “core group” for terminating communications; explanation to Fong & Nava of “core group” position; request for mediation by R. Fong.

Date: Wed, 9 Feb 2011 13:24:03 -0800
Subject: 24th & T Street development - request for mediation
From: Bill Robertson <w.preston.robertson@gmail.com>
To: "Robert K. Fong" <rkfong@cityofsacramento.org>, Lisa Nava <LNava@cityofsacramento.org>
Cc: David Hung <dhung@cityofsacramento.org>, Luis Sanchez <lsanchez@cityofsacramento.org>, "Alan LoFaso" <alofaso@sbcglobal.net>, "Christina Jewett" <christina.jewett@gmail.com>, "Doug Morrow" <douglas.morrow@asm.ca.gov>, "Heather Scot" <Heather.Scott@sen.ca.gov>, "Kelley Woodward" <2006sew@comcast.net>

Rob and Lisa--

We have received the following email from Andrea Rosen. After it I will make comments.

>>Heather -

As the designated liaison for the neighborhood group, through this communication to you, I am asking the group if they would like to meet in the next week or two to discuss the design of the 24th and T Courtyard Condominiums. I have made some changes and am offering to meet with neighbors to present them and get feedback from you.

It is my impression that discussions were prematurely and unilaterally terminated by the group as I have not received any emails or phone calls since you told me that you were canceling our last scheduled meeting which had been set for 7:15pm on January 26, 2011.

Please let me know asap as I know how challenging it is to accommodate folks' schedules.

I believe it is important to keep the lines of communications open and have been endeavoring to do that.

Thanks.

Andrea Rosen
(916) 457-6721
andrearosen@sbcglobal.net<<

Our neighborhood core group representative Heather Scott will respond and accept her offer to meet. We will do so despite our understanding that her application deadline is February 10 and no practical input is being sought from us.

It should be noted that Ms. Rosen's remark about our group "prematurely and unilaterally" terminating discussions is a fairly obvious and artless attempt on her part to cast our neighborhood as uncooperative. I should note that she uses the word "discussions" and not "negotiations". "Discussions" and "communications" are, of course noble things. "Negotiations", however are even nobler.

What Ms. Rosen calls "discussion" and "communication" in actuality has been little more than a repetition by her to us that a.) she is in the right and our neighborhood is in the wrong and b.) she will not discuss with us or communicate about those issues that are of the greatest concern to the neighborhood. A lack of interest in subjecting oneself to repeated condescension and insult can perhaps accurately be termed "unilateral" termination in a court of facts, but it is puerile on a human level. We are fully aware that developer Rosen wanted to continue her condescension and insult, we just felt it was...let me find a word here...unilateral in its benefit.

As previously stated, our neighborhood wanted to discuss and negotiate the number of units, which we felt had a direct relationship on the scale and design. She refused our overtures to even discuss the issue and indeed countered with suggestions in writing that she make the project larger and taller.

I spoke with the always very helpful David Hung this morning at some length and was told that the Zoning Administration has declined our neighborhood request for a Design and Density Workshop to facilitate between neighborhood and developer what we had hoped simple maturity and adult respect would have generated but did not. We are, it seems, left to lobby in advance and then present our positions to the Planning board and, beyond that, the City Council. We are more than willing to do this, but it seems needlessly contentious and a colossal waste of time on everyone's part. Mr. Hung suggested that we might request that Rob serve the function of mediator to a negotiation much in the way that Councilman Steve Cohn mediated a recent controversy in his neighborhood.

I am happy to make that request. Indeed developer Rosen mused about the prospect of such a process herself at one point.

That having been said, I wish to express my great disappointment that increasingly the City Council is being called upon to do a job that other areas of government have been budgeted to handle, but wish not to.

Part of this is, I must confess, is very personal to me, and here I am speaking solely for myself and not the other members of our core group.

As you know, my wife is Claire Pomeroy, Vice Chancellor of Human Health Science and Dean of the School of Medicine at UC Davis. She is following this development with great concern and is not happy about Ms. Rosen's behavior. She is more than willing to become involved as a resident of the neighborhood, but she is also very busy with enormous responsibilities barely imaginable to Ms. Rosen. I confess I resent having to

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further waste my wife's precious time to deal with an issue that we pay taxes to have the City handle with greater competence than I have seen to this point.

Rob himself made a comment addressing this problem during the last City Council meeting--and I couldn't agree more. This should not be the role of the City Council, nor should unreasonable developers with small neighborhood-oriented projects along with the Zoning Administration conduct themselves as though it were. If the City of Sacramento is ever to have a quality of greatness to it, we should stop treating the City Council as though it were a neighborhood association.

Until such greatness arrives, however, I must humbly and regretfully request of Rob mediation in this matter.

Sincerely,

Bill Robertson

--

William P. Robertson
2009 23rd Street
Sacramento, CA 95818
916-607-2405

October 4, 2010

Mr. David Hung
Associate Planner
Community Development Department
300 Richards Boulevard, 3rd Floor
Sacramento, CA 95811

Dear Mr. Hung:

As a resident of the Newton Booth neighborhood in Midtown Sacramento, I am writing to express my concerns with the Courtyard Housing project being considered for the corner of 24th Street at T Street.

My first concern is with increasing the number of apartment units in proportion to single family homes and the corresponding number of designated parking spaces for the apartments. When I learned of the plan to build more apartments in this neighborhood, I surveyed the number of units currently in existence in the immediate vicinity of the property under discussion. From what I could determine, the results are as follows:

- The 2300 block of T Street has 30 apartment units and 18 dedicated parking spaces or garages.
- The 2400 block of T Street has 48 apartment units and 23 dedicated parking spaces or garages.
- 24th Street between T Street and S Street has 13 apartment units and 6 dedicated parking spaces or garages.
- Thus, the total number of apartment units on just these three blocks is 91, with only 47 designated parking spaces and garages.
- On these same three blocks, there are currently 17 single family homes.

In looking at the plans for the 24th & T Courtyard Housing project, I see that a total of six apartment units are proposed with five corresponding detached garages. Given the square footage of the intended units, there is occupancy for easily ten to twelve residents with a potentially equal number of cars. As the plan only includes five garages, this development could put another five to seven cars on the street.

As a result of the already large number of apartments and roughly half as much parking, the streets are often full of the parked cars of residents and can not accommodate visitors. As I live on a corner lot, I can honestly say that most nights I have two cars parked in front of my house and three cars parked alongside of my house. Particularly on weekend nights, the cars come and go quite frequently, which is noisy and a nuisance to the peace of the neighborhood. Further, there are always vacancies in these apartments, as indicated by the constant presence of the "For Rent" signs. Parking would be even more challenging if every apartment were continuously occupied.

My next concern with the Courtyard Housing project is the design and architecture of the proposed units. Newton Booth and Poverty Ridge are neighborhoods dominated by early 20th century cottage style homes and bungalows; many with wide porches, basements, and peaked roofs. The style of the apartments is purported to be, "flat-roofed buildings...that echo the Art Deco era buildings of the neighborhood..." Yet, the drawings included with the Development Project Routing Form indicate that the buildings will more closely resemble the modern architecture reflected in the Whiskey Flats condos located at 22nd and S Street.

I am also troubled by the speed at which this project seems to be progressing. I walk by the property everyday on my way home from work, and I was surprised that over the course of three days the existing home was demolished, the basement and foundation dug out, the debris removed, and the chain link fence erected. While I do agree that the single family home that was demolished was neglected and the property unkempt, I do think it could have been rehabbed and resold. Many homes in the area have undergone such transformations, as evidenced by the recently remodeled property at 1915 24th Street that sold a short time after it was put up for sale.

Finally, I am concerned with tipping the balance of this neighborhood more in the direction of renters and away from property owner/residents. I am proud to know so many of my neighbors and call them friends. I can honestly say that the homeowners of this area are all familiar with each other and frequently stop to with each other. Not surprisingly, this project has been a considerable topic amongst many of us. The spirit of this community seems like it would be jeopardized by a greater influx of renters that are often temporary residents that have little or no stake in the long term health and development of Newton Booth. I believe the community would be better served by building one or two single family homes on this site, providing another anchoring property to this region and contributing to the neighborhoods' stability.

Thank you for your consideration of my concerns regarding this project. I am happy to further discuss my concerns and suggestions with the City of Sacramento or the Planning Commission if they so desire. I would also ask that you include me as part of your distribution list for any changes to or development of this plan in the future.

Sincerely,

Heather C. Scott
2430 T Street
Sacramento, CA 95816

David Hung

From: Scott, Heather [Heather.Scott@sen.ca.gov]
Sent: Friday, February 25, 2011 12:22 PM
To: David Hung
Cc: Lisa Nava; Robert King Fong
Subject: Concerns with project P10-089

Dear Mr. Hung-

As a resident of the Newton Booth neighborhood in Midtown Sacramento, I am writing to express my concerns with the Courtyard Condominium project being considered for the corner of 24th Street at T Street (P10-089).

My first and primary concern with this project is the number of units. The merged lots that this project may be built upon initially had one single family home before the developer had it demolished. The plans for the 24th & T project propose six condominium units with five corresponding detached garages. Given the square footage of the intended units, there is occupancy for easily ten to twelve residents with a potentially equal number of cars. As the plan only includes five garages, this development could easily put another five to seven cars on the street.

To be clear, I do support building more housing in this space; but I would argue that a reasonable increase in density, say to that of three single family homes with a corresponding number of garages, keeps with the vibrant and mixed nature of the neighborhood.

My second concern is that proposed design of the condos is in too far contrast to the immediate surrounding structures and is not complementary. Newton Booth and Poverty Ridge are known for their high water bungalows, cottages, and Tudor style homes. These architectural styles also have components that make the neighborhood safer and more welcoming than the proposed structure. Porches and front yard space give neighbors more opportunity to interact with each other and create a more secure environment. The development instead closes off the residents from the rest of the neighborhood with its walled in courtyard and lack of porches.

My final concern is with the lack of meaningful discourse between the developer and the neighborhood. As a neighbor that has been involved with this project since September 2010, I feel that the many discussions have yielded little in the way of compromise from the developer. This is especially troubling given that the developer does not live, nor intend to live, in this neighborhood and that it is investment property instead. As residents and owners we do have to live with what the city decides is appropriate for this parcel and the consequences of those decisions.

As one of the core group of neighbors that has been working with Ms. Rosen, I feel that we have tried to be clear, rational, and flexible with our requests. I feel that we have been open and tried to work within the process that the city prescribes. I do not feel that we received reciprocal consideration from the developer about our concerns.

Thank you for your consideration of these comments and for your work and helpfulness.

Sincerely,

Heather Scott
2430 T Street

Sacramento, Ca 95816

David Hung

From: Mabel Lee Robbins [mlrobbin@pacbell.net]
Sent: Thursday, October 07, 2010 2:17 PM
To: David Hung
Subject: Property: 2231 T Street & 1918 24th Stree
Attachments: mlrobbin.vcf

Follow Up Flag: Follow up
Flag Status: Flagged

Mr. Hung,

How will the parking to be handled for this new apartment complex? I live at 2010 24th Street, and I am concern about the parking situation in this neighborhood already. It seems that with the inclusion of six additional apartment units, there will be a need for at least six additional parking spaces, and more than likely, a total of twelve spaces (2 per unit) to this neighborhood. It's getting very crowded, and I am very concern about the parking situation in this area.

How many spaces will be built into the apartment unit or will this be street parking only? We already have multiple apartment units in the immediate area, and with the addition of six more units, I feel that the apartment complex needs to include enough parking spaces for its tenants before approving the the building permit.

Thank you for your assistance.

Respectfully,
Mabel Lee Robbins
916.453.0680

David Hung

From: Mabel Lee Robbins [mlrobbin@pacbell.net]
Sent: Monday, February 28, 2011 11:58 AM
To: David Hung
Cc: Robert King Fong; Lisa Nava; 2006sew@comcast.net
Subject: RE: PROPOSED PROPERTY, "24TH & T STREETS COURTYARD HOUSING PROJECT" (P10-089)
Attachments: mlrobbin.vcf

Hi Mr. Hung,

My apology for the delay in submitting my email ... if possible, can this email be included.

I am Mabel Lee Robbins, located at 2010 24th Street (24th between T & U Streets), and I have some comments in regards to the staff report to the Planning Commission proposed "24th and T Streets Courtyard Housing Project" (P10-089). The project proposes a **six** two-story condominiums with **five** garages with a International/Moderne style of architecture in a predominantly single family home neighborhood.

I am concern that the new development will change the dynamics of the neighborhood. I have been living in this neighborhood for over 27 years and it has remained relatively the same, except for increased traffic. With the inclusion of an additional six unit condos, plus 12+ additional people with cars, it will heavily impact the parking situation; especially on our block that has a very narrow width for cars and bicyclists. Aside from the parking situation, six condo units crammed in to a small space will not be esthetically pleasing, much less reflective of the neighborhood.

Please reconsider the development of this proposed Courtyard Housing Project. I agree with the Neighborhood Core Group that the property will provide for a more appropriate placement of:

Three residential housing units with adequate size garages, pitched roofs, porches (insert # of sq ft divided by 3 times 80% density formula). There are a number of appropriate examples in the immediate neighborhood (1, 2, and 3 unit). There are no flat-roof examples that would be appropriate for this proposed project.

Architectural design (California Craftsman style bungalow; brick style; or Tudor style) There are a number of single family homes, duplex, and tri-plex structures in the immediate neighborhood. I can provide current photos of the structures in the immediate neighborhood if needed for design examples.

Height of structures: No more than 2 stories. There are no 3-story housing structures in the immediate neighborhood.

Please reconsider the proposed "24th & T Streets Courtyard Housing Project" (P10-089). Your assistance is greatly appreciated.

Have a good day.
Mabel Lee Robbins
2010 24th Street
Sacramento, CA 95818
916.453.0680

David Hung
Associate Planner
Community Development Department
300 Richards Boulevard, 3rd Floor
Sacramento, CA 95811

Oct. 15, 2010

Dear David –

Greetings. My fiancé and I are new homeowners on the 2400 block of T Street. We made an offer on our home, a high-water bungalow, the first day it was offered for sale this spring. We loved the big, open mahogany porch that's perfect for a swing and rocking chairs. We were not excited about the amount of apartment units on the block because we knew from firsthand experience that renters have no incentive to protect and preserve the virtues of a neighborhood. However, we overlooked that concern and are proud owners of our home.

We are displeased to learn about the plans for courtyard housing at 24th and T Streets. For one, the configuration of the apartments is oriented away from the sidewalk and the street and toward the courtyard, as the name implies. The development is planned to be gated. Both of these attributes reduce the vitality, energy and vigilance on the street, thus reducing the crime deterrent effect that eyes on the street can have. The gates, like bars on a window, imply that there is a security threat in the neighborhood. As any student of the "broken windows" theory knows, such an implication can be a self-fulfilling prophecy and erode the safety and security of a neighborhood.

My concerns about crime are not without backing. There was a drug-motivated home-invasion robbery at 26th and T Street just weeks ago. A quick look at crime statistics shows that auto burglaries and vandalism are common. Plans for a gated, insular fortress will do little to enhance the security of the surrounding area.

My concern is also that the area directly surrounding the proposed project is at a tipping point where the number of apartment-filled lots is on the verge of eclipsing the family atmosphere of the area. I bought this home hoping to raise children here. I believed that the "story" of Midtown's historic areas was that too many apartments were built in the 70s. The ideal scenario is to preserve the historic charm of Arts and Crafts and Victorian homes that were picked up from trains on R Street by horse and buggy. I regret to see a project that packs too many units in too small of a space, further upsetting the delicate balance that separates a stable, historic neighborhood from a transient and forgettable one.

Despite these concerns, I want to be clear that I am in favor of multi-family development and housing in close proximity to light rail. I am keenly aware and pleased that the R Street corridor is zoned for multi-family units. I believe that there is no better place in the neighborhood for apartments and condos and stand behind plans for such development and investment along that corridor.

However, my position remains that T Street is the place for single-family homes or at minimum, development in the spirit of Metro Square that conforms and blends with a historic neighborhood like Newton Booth or Poverty Ridge. I think a development that keeps eyes on the street and preserves the character of the bungalow-style, Tudor and Victorian homes are best for the neighborhood.

I respectfully ask that no more than four units are approved for the site at 24th and T Streets. I think it would be in the best interest of the developer and the neighbors if the units are built to look like two single-family homes that are oriented to the street with a lawn or fountain or porch facing the neighbors. Orientation toward a private, gated courtyard turns a blind eye to the assets of the area and more resembles the ugly 'courtyard' building on the north side of T Street between 24th and 25th Streets.

Such a compromise would also maintain the appearance of a neighborhood with an equitable balance of single-family homes and apartments while still allowing the developer to reap the financial rewards of upscale multi-family units.

Sincerely,

Christina Jewett and Floyd Marvin

DRAFT – Started October 24, 2010

“GOOD NEIGHBORS” vs. “EARLY NOTICE OF PLANNING APPLICATION (z10-142)”

CONCERNS:

1. PRESERVATION OF THE NEIGHBORHOOD

- a. *“Good Neighbors” maintain their properties to reduce deterioration of good structures and landscaping.*
- b. *“Good Neighbors” watch out for other neighbors.*
- c. *“Good Neighbors” keep their properties clean and safe with no sidewalk/gutter hazards.*
- d. *“Good Neighbors” follow the City of Sacramento’s regulations regarding parking, noise, loitering, litterbugging, etc.*
- e. *“Good Neighbors” know the history of this historic neighborhood and fight to preserve it.*
- f. *“Good Neighbors” reduce trash/glass and other hazards that have been deposited on the sidewalks and front yards by “commuters” in order to facilitate reasonable accommodation for the elderly and disabled in the neighborhood*
- g. *“Good Neighbors” do not Graffiti or do other minor vandalations on neighbors’ properties.*
- h. *“Good Neighbors” try to minimize the amounts of pet excrements deposited on other neighbors’ lawns.*

2. GENERAL SAFETY OF THE NEIGHBORHOOD

- a. *During the weekdays*
- b. *During the weekends*
- c. *At night*

3. PEOPLE CONGESTION:

- a. *There are 156 multi-family units currently in the immediate neighborhood bounded by S/T STREETS AND 23RD/25TH STREETS*
 - i. *THERE ARE APPROXIMATELY 50 “COMMUTERS” THAT ACTUALLY ROAM THE STREETS AND LIVE IN THE ALLEYS OR WHEREEVER THEY CAN FIND SHELTER.*
 - ii. *FOR APPROXIMATELY 400 NEIGHBORS IN THE 156 MULTI-FAMILY UNITS*
 - iii. *THERE ARE APPROXIMATELY 100 ON-FOOT “COMMUTERS” THAT PASS THROUGH THE NEIGHBORHOOD DAILY*
 - iv. *THERE ARE APPROXIMATELY 150 VEHICLES THAT PASS THROUGH THE NEIGHBORHOOD DAILY*

4. PARKING and TRAFFIC DISRUPTIONS

- a. *There are approximately 400 neighbors that live in the multi-family units.*

- b. *There are approximately 80 on-site parking spaces for the 156 multi-family units.*
- c. *The lightrail station at 24th & R Streets does not provide adequate parking beside the tracks for commuters; there are no parking spaces designated for the disabled.*
- d. *The City of Sacramento's garbage collectors and street sweepers/cleaners and garbage collectors use the alley and find it difficult to do a good job because of vehicle and "commuter" obstructions.*
- e. *The City of Sacramento's garbage collectors and street sweepers find it difficult to do a complete job on the streets when vehicles are parked too close to and on top of trash piles.*

5. PROPOSED ARCHITECTURE

- a. *The proposed architectural style is not compatible with this neighborhood of Arts & Crafts Style, Art Deco/Moderne style, brick cottage style, and other styles for the period 1900 – 1930. The "Application of Intent" does not agree with the plans (proposed architecture).*
- b. *The proposed structures do not appear to be the most energy efficient (e.g. Flat roofs are not as energy efficient as sloped roofs.)*
- c. *The proposed garages are not big enough for a standard sized vehicle. The proposed garages do not have storage considerations.*

6. SIDEWALK/GUTTER/STREET HAZARDS CURRENTLY TAKEN CARE OF BY "GOOD NEIGHBORS"

- a. *Uncollected trash*
- b. *Glass/trash deposits.*
- c. *Graffiti*
- d. *Other minor vandalations not reported or corrected by the City of Sacramento*

7. REASONABLE ACCOMMODATION CONSIDERATIONS FOR ELDERLY AND DISABLED NEIGHBORS

- a. *Wheelchair hazards*
- b. *Walking hazards*
- c. *Transportation to hearings, etc.*
- d. *Notifications regarding re-zoning, demolitions, etc.*

8. CITY OF SACRAMENTO'S RESPONSIBILITIES

- a. **SAFETY**
- b. **LIGHTRAIL (transportation and parking)**
- c. **STREETS & ALLEYS; STREETLIGHTS**
- d. **SEWAGE; GARBAGE; RECYCLING; YARD TRASH**
- e. **STREET CLEANING; STREET TRASH**

9. DISTRICT 4'S RESPONSIBILITIES TO "GOOD NEIGHBORS"

- a. **NEIGHBORHOOD PRESERVATION**
- b. **NEIGHBORHOOD SAFETY**

- c. **NEIGHBORHOOD ISSUES LIKE PARKING AND TRAFFIC**
- d. **NEIGHBORHOOD MAINTENANCE & UPKEEP**
- e. **NEIGHBORHOOD REPAIRS**

ALTERNATIVES:

1. **Two single family units with angled roofs:**
 - a. **1918 24th Street – A two bedroom/two bath brick cottage-style home with a full garage (including storage space) on alley.**
 - b. **2331 24th Street – A three bedroom/two bath Art Deco/Moderne style home with a two-car garage and driveway on 24th Street.**
2. **Four multi-family units with angled roofs:**
 - a. **Each unit with two bedrooms/two baths**
 - b. **Four full garages with storage space on alley**
 - c. **Arts & Crafts style; Art Deco/Moderne Style; or brick cottage style.**
3. **Sell the properties to “good neighbors” at no more than what was paid for it in June (?). “Good Neighbors” will do the right thing for the neighborhood.**

ABOUT 2331 T STREET/1918 TWENTY FOURTH STREET (APN010-0036-011-0000 and 010-0036-012-0000)

The unique Art Deco/Moderne home was built in the 1940s on a double lot (2331 T Street/1918 Twenty Fourth Street). The home had a detached garage and a very large back yard on 24th Street to the alley. A fence ran from the garage to and up the alley.

The first home owner (1944/45) was Chester Dong (meat cutter). The second (and final) homeowner was the Warren A. O'Brien family who moved into the home in 1956/57. Mr. O'Brien worked for Western Electric Co. and retired in 1980. Daughters, Rozanne and Carol, babysat many of the children in the neighborhood in the 1960s. Rozanne (O'Brien) McPhee lived in the home with her parents for many years after her parents had become disabled. A wheelchair ramp was installed in the 1990s to facilitate entry into the home for both Rozanne and her parents. Rozanne moved out of the home about 2001 when her parents had passed. Rozanne and Carol continued doing yard work as they could. The neighbors painted graffiti off the fence for them and swept up glass/trash that frequently ended up on their sidewalk, lawn, and in the gutters. Neighbors expressed interest in purchasing the home when the daughters (O'Brien Family Trust) were ready to sell. Unbeknownst to the neighbors, the home was sold about June 2010. Neighbors continued keeping an eye on the home for the glass/trash hazards that needed to be picked up immediately. The City of Sacramento

continued picking up trash, recycles, and yard refuse. However, the City of Sacramento has never removed yard trash that is under vehicles that have parked right on top of it. The home was demolished sometime between September 17 and September 30, 2010 (Demolition approved September 13, 2010 - # RES-1009306) with no advance notice to the neighbors. Neighbors never received notification regarding re-zoning and merging the double lot. In early October 2010, neighbors received the "Early Notice of Planning Application (Z10-142)" to develop six apartment dwelling units ("multi-family residential" on a "1.9 acre" parcel at 2331 T Street/1918 Twenty Fourth Street.

I'm finding out that no laws were broken when they demolished the home and took out the trees without reasonable notification to the neighbors in advance. Had it been a two-story structure, then the property owner would have been required to give us a two-week notice. Also, the review that the police and fire departments signed off on was just that the demolition could be done safely without impacting the physical safety of the immediate neighbors and their structures. A home that is habitable can just be demolished, unless it is protected in a historic heritage district (like my Poverty Ridge Historic District, right across the street). There does not appear to be a city code that protects and regulates reasonable accommodation and any adverse impacts on the elderly and disabled neighbors in the "hood". The City of Sacramento should have provided enough parking for Lightrail commuters by the lightrail station at 24th and R Streets. This causes parking overflow right onto the parking spaces along 24th Street as far as my bungalow.

Availability for comments and questions:

Susan Woodward ("Kelley")
2006 Twenty Fourth Street
Sacramento, CA 95818

(916) 837-8991

2006sew@comcast.net

David Hung

From: 2006sew@comcast.net
Sent: Monday, February 21, 2011 4:19 PM
To: David Hung
Cc: Robert King Fong; Lisa Nava; Kelley Woodward
Subject: Proposed "24th & T Street Courtyard Housing Project" (P10-089)

21 February 2011

Hello, David Hung -

I am Susan Woodward ("Kelley"), and I have comments for the staff report to the Planning Commission regarding the proposed "24th & T Streets Courtyard Housing Project" (P10-089). This project proposes to have six two-story condominiums with five garages in an International/Moderne style of architecture in the heart of what is predominantly a single family home neighborhood with bungalow, brick, and Tudor styles of architecture appropriate to the time period 1900 - 1940's.

I am a concerned neighbor in the Newton Booth neighborhoods. I live at 2006 24th Street, across the street from the proposed project. I am submitting these comments on behalf of myself and other concerned 24th Street neighbors. I am a member of the Core Group of concerned neighbors that have had many communications with you, Council Member Fong, and Lisa Nava these past five months. I am a member of the Newton Booth Neighborhoods Association.

We believe in Smart Growth and understand infill, urban development, and other health and safety issue concerns of the City. We also believe in what is reasonable and appropriate, within the City's Guidelines, for our small, but fine old historic neighborhood.

For us, reasonable and appropriate is:

Three residential housing units with adequate size garages, pitched roofs, porches (insert # of sq ft divided by 3 times 80% density formula). There are a number of appropriate examples in the immediate neighborhood (1, 2, and 3 unit). There are no flat-roof examples that would be appropriate for this proposed project.

Architectural design (California Craftsman style bungalow; brick style; or Tudor style) There are a number of single family homes, duplex, and tri-plex structures in the immediate neighborhood. I can provide current photos of the structures in the immediate neighborhood if needed for design examples.

Height of structures: No more than 2 stories. There are no 3-story housing structures in the immediate neighborhood.

I understand that it is not necessary to resubmit the two petitions that we did in November, or to send copies of previous correspondence, September 2010 - January 2011 to your office.

I can be reached at this e-mail 2006sew@comcast.net or phone (916) 837-8991 if you have questions.

Thank you for providing this opportunity to address our concerns about the proposed "24th & T Streets Courtyard Housing Project" (P10-089).

March 16, 2011

Courtyard Condominiums (P10-089)

Susan Woodward ("Kelley")

2006 24th Street
Sacramento, CA 95818

----- Original Message -----

From: "David Hung" <DHung@cityofsacramento.org>
To: "lauren@scottadamson.net" <lauren@scottadamson.net>, "morris411@gmail.com" <morris411@gmail.com>, "pjwade@fcusd.org" <pjwade@fcusd.org>, "kiddv@csus.edu" <kiddv@csus.edu>, "mpavisich@juno.com" <mpavisich@juno.com>, "Alex Zabelin" <alexegon@gmail.com>, "Heather' Scott" <Heather.Scott@sen.ca.gov>, "bgwhitted@att.net" <bgwhitted@att.net>, "Stephen Whitted" <sbwhitted@att.net>, "marlenerice@sbcglobal.net" <marlenerice@sbcglobal.net>, "helmed@cwo.com" <helmed@cwo.com>, "alexives12@yahoo.com" <alexives12@yahoo.com>, "H2ngo@hotmail.com" <H2ngo@hotmail.com>, "j.konopka@sbcglobal.net" <j.konopka@sbcglobal.net>, "carrie camarena" <cdcamarena@yahoo.com>, "Alan LoFaso" <alofaso@sbcglobal.net>, "nomar98@yahoo.com" <nomar98@yahoo.com>, "ed@loftgardens.com" <ed@loftgardens.com>, "whitenightc@live.com" <whitenightc@live.com>, "Christina Jewett" <christina.jewett@gmail.com>, "2006sew@comcast.net" <2006sew@comcast.net>, "Bill Robertson" <w.preston.robertson@gmail.com>, "baxmag@sbcglobal.net" <baxmag@sbcglobal.net>, "amaroo2@sbcglobal.net" <amaroo2@sbcglobal.net>, "Tim and Lynne Gussner" <TimGussner@sbcglobal.net>, "fiores@saccounty.net" <fiores@saccounty.net>, "mlrobbin@pacbell.net" <mlrobbin@pacbell.net>, "marygomez1@sbcglobal.net" <marygomez1@sbcglobal.net>, "tamitrostel@comcast.net" <tamitrostel@comcast.net>, "kristinecelorio@yahoo.com" <kristinecelorio@yahoo.com>, "jhasko@att.net" <jhasko@att.net>, "reed.richerson@gmail.com" <reed.richerson@gmail.com>, "tguil75_@hotmail.com" <tguil75_@hotmail.com>, "suzmaast@yahoo.com" <suzmaast@yahoo.com>, "nikkicorbett@gmail.com" <nikkicorbett@gmail.com>, "chole531@gmail.com" <chole531@gmail.com>, "kari@sonic.net" <kari@sonic.net>, "ghostpony916@yahoo.com" <ghostpony916@yahoo.com>
Cc: "ron vrilakas" <Ron@VrilakasArchitects.com>, "Mark Groen" <mark@vrilakasarchitects.com>, "Ben Rosen" <bcrosen@gmail.com>, "ANDREA ROSEN" <andreaosen@sbcglobal.net>, "Lisa Nava" <LNava@cityofsacramento.org>, "Robert King Fong" <RKFong@cityofsacramento.org>
Sent: Wednesday, February 16, 2011 4:00:53 PM
Subject: Comments on Courtyard Condominiums (P10-089)

Dear Everyone,

For those who want to comment on any aspects of this project up to now, including those of you who have provided comments to me in the past, please submit your written comments by February 25th if you need me to include them in the staff report to the Planning Commission. You may still submit comments after that, up to the hearing date, but those will be forwarded as supplemental materials. Thanks for your attention.

David Hung
Associate Planner
Community Development Department
300 Richards Boulevard, 3rd Floor
Sacramento, CA 95811
Phone: (916)808-5530
E-mail: dhung@cityofsacramento.org

David Hung

From: John Hagar [hagar.john@gmail.com]
Sent: Wednesday, February 23, 2011 3:33 PM
To: David Hung
Cc: Robert King Fong
Subject: Objections to Proposed Development P10-089

Thank you for allowing comments concerning the proposal to construct condominiums at 24th and T Streets in mid-town Sacramento ("24th & T Courtyard Development" (P10-089)). We live on Q Street between 24th and 25th Streets in a brick "Tudor" style single-family dwelling. We see several fatal problems with the proposal, including the following:

1. We and our neighbors chose to live in the Newton Booth neighborhood because it is primarily comprised of single family homes, with a few scattered and small multi-unit apartments. At present, the City is working with our neighborhood concerning a number of traffic calming measure. Our neighborhood does not need the increase in density that will result from an additional six-unit condominium/apartment complex (assuming the complex is occupied), especially along narrow and already busy 24th Street.

2. The proposed design of this multi-unit building is not in any manner reflective of our neighborhood, and will clash with existing structures.

3. The proposal requests that the City approve far too many variances to critical construction standards, important regulations that should be rigorously enforced. For example, we understand that the developer seeks: (a) a variance that will allow her to build a multi-unit complex in a neighborhood of single family dwellings; (b) a variance to reduce the parking spaces normally required for a proposal of this nature (a variance which, if approved, will add to the already congested street parking on 24th Street); (c) a variance that will reduce the required setback for an accessory structure (in essence, a variance to further overcrowd the tiny .19 acre parcel on which the condominiums/apartments are to be placed); and (d) a variance waiving the trash enclosures required for multi-family development (which will create health and safety issues).

4. Finally, we question the timing and appropriateness of this project in terms of the City of Sacramento's vision for the future. The Newton Booth neighborhood is at present bordered by two other condominium projects. The project at 21st Street at T Street is only partially completed, leaving almost one entire block barren except for some foundation work. Additional units are not being constructed *because no one wants to buy them*. Another project, lofts along S Street between 21st and 22nd Streets, have also failed to sell and are now either short term rentals or sitting empty. Naked foundations and empty units are a blight, no matter how recently they were built. While this developer may want to build condominiums, there is no reason to believe that her condominiums will sell. Indeed, if the prior projects in the area are considered evidence, the 24th and T Courtyard condos will remain empty, or will be rented to short-term renters for a number of years into the future.

In terms of the realities of today's market, this project is the very opposite of what our City needs to move forward. Let's keep Sacramento a place where you can work, raise a family, and retire in a decent neighborhood. There is no reason to approve four major "Special Permits" to allow this developer to construct another empty multi-unit building in a quiet, established, and very livable neighborhood.

For all of these reasons we request that this project not be approved.

If I can provide other information, please do not hesitate to call at (415) 215-2400.

John Hagar

QuickTime™ and a
TIFF (Uncompressed) decompressor
are needed to see this picture.

David Hung
Associate Planner
Community Development Department
300 Richards Boulevard, 3rd Floor
Sacramento, CA 95811

Dear David,

Developer Andrea Rosen presented her initial plan to the Newton Booth Neighborhood Association at our October meeting. The board liked the design, but declined to write a letter to the City endorsing the project as the plans were not final and the board had not visited the site. The board was also astonished when the original structure was demolished within days. This was not mentioned at the meeting and seemed to undermine confidence in the developer's transparency.

The neighborhood has voiced unanimous disapproval to the scope and size and design of the project. It does not complement the neighboring structures and is of a higher density than existing parcels. The looming corner is an affront and the plan shows the ADA unit being the furthest from the garages (inadequate parking for the number of bedrooms planned). The many variances and permits speak to the inadequacy of the design to meld and dissonance this project is creating.

This design is well suited for an L Street or R Street loft environment.

I oppose this project in its current iteration. T/23rd Street has a bungalow home with three units that perfectly integrates itself into the neighborhood.

Thank you,

Alex Zabelin
NBNA, President
2023 23rd Street
Sacramento, CA 95818

Newton Booth Neighborhoods Association

24 Feb 2011

To: David Hung
City of Sacramento

I oppose this plan
as being not appropriate
for our neighborhood -

Sincerely,
Pat Melanby
2431 You St
Sacramento

768-7286

David Hung

From: Pamela Wade [peejwade1@att.net]
Sent: Friday, February 25, 2011 2:20 PM
To: David Hung
Cc: peejwade1@att.net
Subject: 24th & T Courtyard Development

Dear Mr. Hung:

I have lived on 24th and T for twenty-one years. I have watched this neighborhood transform from aging homes to beautiful restorations. Our neighborhood is proud of the 1920's architecture, and we have embraced the historical aspect as we restored our homes. I personally, with a business partner, have purchased 3 extremely run-down homes in the neighborhood and restored them to their original beauty. One such property is on a double lot close to Ms. Rosen's property.

I am outraged by Ms.Rosen's lack of consideration towards the homeowners in our neighborhood. She fails to acknowledge our desire to have her complex reflective of the craftsman bungalows and tudor-style homes that surround her. She likes ART-DECO and how dare us not WELCOME her taste in a neighborhood that has worked so hard to embrace the historical architecture of our homes.

I am concerned about the extreme density of her proposal, and the request for parking exemptions in a neighborhood that already struggles for adequate parking. I am concerned about the "cold" design of her courtyard, and how it contrasts with the open porches and welcoming feel of our homes. I feel that for Ms.Rosen, this is simply a "money-making" venture for her, and that our concerns are of NO CONSEQUENCE to her. Simply put, when she has made her money and moved on, our neighborhood will be forever saddled with a design that none of us like, and will have to live with until we move.

It doesn't seem fair in the least. Ms.Rosen is not concerned about any type of "good faith negotiation". I feel as if a "bully" has moved into the neighborhood, and I for one, am frustrated, angry and tired of feeling "pushed around" by her. Please support us in our desire to limit the density of this courtyard, and the blending of her design into the Craftsman, Bungalow and Tudor architecture of the neighborhood.

Our neighborhood is in the midst of an historical "revival", and I feel strongly that this construction would be an INSULT to our efforts.

Sincerely,

Pamela J. Wade

February 24, 2011

David Hung
Community Development Department
300 Richards Blvd, 3rd Floor
Sacramento, CA 95811

Re: P10-089

To Mr. Hung,

I am writing to express my opposition to the proposed Courtyard Development located on the northwest corner of 24th and T Streets. I live across T Street from the site with my husband and two children and we have invested too much in this neighborhood to see it undermined by Andrea Rosen's development. It absolutely inappropriate for the Newton Booth Neighborhood for its density and design and Ms. Rosen has demonstrated unwillingness to negotiate in good faith with the neighbors who have volunteered to represent the interests of the larger neighborhood.

The proposed density for this development includes six apartments of approximately 1200 – 1800 square feet. This many apartments of this size can easily house at least 12 people and as many as 24 people, somewhat comfortably. What existed at the site fewer than six months ago was a single-family home. The impact from the increase of a single-family home to six apartment units is simply too much for this neighborhood to sustain, as evidenced by the variances that Ms. Rosen has requested: a waiver from providing the number of parking units required for a multi-family dwelling and a waiver from providing the number of trash enclosures required for a multi-family dwelling. This development and these waivers will dramatically reduce available parking on T and 24th Streets. Additionally, the waiver for trash enclosures will result in clogging the 24th Street bike lane with 12 garbage and recycling cans once a week that may not be removed from the street in a timely fashion.

The design for this development is inconsistent with the existing design of the homes in the neighborhood. The Newton Booth Neighborhood is rife with examples of California Craftsman bungalows, brick style homes and Tudor-style homes. The lack of porches combined with casement windows and flat roofs are not at all reflected in the homes in several surrounding blocks. Our neighborhood already suffers too much from unsightly, inappropriately-designed apartment complexes built in the 1960's and 70's. Similarly, I do not believe that the current design will stand the test of time.

Though the neighbors in the Newton Booth and Poverty Ridge neighborhoods were never notified of the existing home's demolition nor consulted in the developing design of the complex, we organized a group of representatives to reach some sort of compromise on these issues with Ms. Rosen; no compromise has been reached as a result of Ms. Rosen's obstinacy.

My family and I have lived in our home, a Craftsman bungalow, for seven years. We have invested much time, money and energy in restoring the beauty of our home consistent with its original design. We greatly improved its curb appeal with new paint and landscaping. We have watched as seven homes within 300 feet of the proposed development do the same. The trend in our neighborhood is to buy an existing home and restore and beautify it – not demolition it and build the largest complex that can be squeezed into the lot size. Ms. Rosen's complex as proposed does not belong in our neighborhood.

The beloved single-family home that existed across the street from my house, within full view of my living room window will never return and we have accepted that. We cannot accept the current proposed development as a suitable dwelling in our neighborhood.

Sincerely,

Bridget Whitted
2314 T Street
Sacramento, CA 95816

October 10, 2010

Mr. David Hung
Associate Planner
Community Development Department
300 Richards Blvd., Third Floor
Sacramento, CA 95811

Dear Mr. Hung:

I'm writing to express my concern regarding proposed development on the northwest corner of the intersection of T street and 24th street. My wife and I bought our house across T street from the property in 2004 because we were attracted to the vibrancy and diversity of the neighborhood. We were drawn to the historic architecture, so we bought and remodeled our 1928 bungalow. Since then we have grown fond of many of our neighbors and have had two children whom we plan to raise in the neighborhood. We've invested in the neighborhood both financially and emotionally, so I was surprised and troubled to come home from work a couple of weeks ago to find that the house across the street had been reduced to rubble. My anxiety mounted as I watched a chain-link fence go up around what had become a dirt lot and I learned of a proposed six-unit apartment complex to be erected on the site. My frustration is two-fold: I am concerned about the impact such a project will have on my neighborhood and I object to the process by which my neighborhood has been drastically altered with no notification or community input.

I see the proposal as detrimental to the neighborhood for a number of reasons. Given that our neighborhood is already saturated with multi-unit apartment complexes, adding more will further tip the balance of renters to home owners, increasing the number of people who may not be invested in the neighborhood for the long term. The six proposed units would likely house 10 to 12 adults and their cars, putting more stress on an already tight parking environment. The architecture of the proposed building is modern and block-like, clashing with the surrounding cottages and bungalows both in size and style. All of these effects detract from the livability, charm and long-term stability of the neighborhood.

I am also disturbed that the site became a vacant lot surrounded by a chain-link fence with no notification of nearby residents and without an approved plan for development. I am not familiar with the guidelines for public notification of this kind of project, but my experience in the neighborhood is that such notification is necessary for making even small changes, so I'm surprised that I didn't receive any information nor did I see any posting at the property. Also, while the house and grounds that previously occupied the lot were neglected, they were certainly salvageable and preferable to the current dirt lot. By allowing demolition of those structures before any redevelopment plan was approved it seems that the city has tacitly approved the project before review. This undermines the review process and puts pressure on neighbors to accept any plan that will address the current blight.

Thank you for taking my concerns into consideration and I will be happy to discuss these issues with you at any point in the process. I would also like to request that I be notified of any changes or action taken with regard to this project.

Sincerely,

Steve Whitted
2314 T Street
Sacramento, CA 95816
(916)743-0889
sbwhitted@att.net

Steve Whitted
2314 T Street
Sacramento CA, 95816
(916) 743-0889
sbwhitted@att.net

February 24, 2011

Mr. David Hung
Associate Planner
Community Development Department
300 Richards Blvd., Third Floor
Sacramento, CA 95811

Dear Mr. Hung:

I am a resident across the street from the proposed development at the corner of 24th and T Streets. When the project was initially brought to my attention, I sent a letter expressing my apprehension regarding the proposal, and I'm sorry to report that despite efforts to work with the developer to address neighborhood objections, the project as it was finally submitted to the planning commission ignores concerns I share with many of my neighbors: the size of the buildings and the resulting population density.

Based on communication among neighbors, I understand that the developer, Andrea Rosen, insists that the project is in keeping with "smart growth" principles and that including six units in her proposal is fundamental to these principles. I applaud the idea of development that encourages alternative forms of transportation. Indeed, I choose to live in my neighborhood exactly because it is possible for me and my family to walk, ride our bikes and take light rail easily and safely. I also understand that increased population density is an important component of this kind of urban planning in that more people can live in comfortable proximity to work, entertainment and transportation. However development of this kind has to take established neighborhoods into account. I don't think anyone would advocate for a high rise apartment complex in an existing midtown neighborhood. So where do we strike the balance?

I agree with many of my neighbors that the proposed increase in density at 24th and T from one unit (pre demolition) to six is excessive. Three or four single story units would be much more in keeping with our neighborhood than the imposing buildings Ms. Rosen hopes to construct. The proposed buildings would be more at home on the R Street corridor where they would blend with existing light industry and office buildings than in our neighborhood of cottages and bungalows. I also take issue with the idea that fewer than six units would be irresponsible from a smart growth perspective. Certainly the lot in question can and should accommodate more than the one house that existed prior to demolition, but in a neighborhood of single family homes and small apartments, an increase from one to three seems more reasonable.

So I find Ms. Rosen's motive for insisting on six units dubious, and I believe that the only other possible motivation for her unwillingness to negotiate the number of units is financial gain. While I don't begrudge anyone's right to line their pockets, I do object to someone doing it to the

detriment of the neighborhood in which I hope to raise my family. I therefore urge the Planning Committee to reject the proposal in the hope that Ms. Rosen or someone else will find a way to develop the property in a way that balances "smart" growth with the concerns of our neighborhood. Thank you for taking the time to consider my input.

Sincerely,

Steve Whitted

David Hung

From: MARLENE RICE [marlenerice@sbcglobal.net]
Sent: Friday, February 25, 2011 10:10 AM
To: David Hung
Cc: Robert King Fong
Subject: RE: Proposed 24th and T Development (P10-089)

Dear Mr. Hung,

I would like to add my concerns regarding this development project. I was alarmed to discover that a single residence could still possibly be replaced by a 6 unit project. The increased density in an area of our neighborhood that already has many multiple unit residences creates an unfair increase in parking issues in an already impacted area.

I am also concerned about the height of the project overshadowing the closest properties. The project is attractive, but has no place at the current site. When a new project discounts the size and architecture of existing properties, it changes the quality of life and property values of existing residents.

I would like to see 3 units maximum with adequate onsite parking to accomodate the new residents and a height and architecture that blends with surrounding properties.

I hope that city representatives now recognize that we are strong and unified neighborhood, not just a development opportunity for an individual who does not have to live with the result of her intrusion.

Thank you for your consideration of my concerns.

Sincerely,

Marlene Rice
2225 T Street
(member of the Poverty Ridge Traffic Calming Committee)

David Hung

From: Linda McNamara [amaroo2@sbcglobal.net]
Sent: Friday, February 25, 2011 3:28 PM
To: David Hung
Subject: Proposed 24th & T Development (P10-089)

Dear Mr. Hung,

I would like to express my disappointment with Ms. Rosen's plan for the 24th & T project. I believe she has not listen to the neighborhood or tried to be reasonable with us. We do not oppose any development on the property but we would like a project that would compliment the neighborhood by matching our period stuctures.

My block which is the same block for the proposed structure. We have already two apartment buildings. I feel that my block has already fulfilled the city's desire for "in fill" quota. My desire would be for Ms. Rosen change her design to 3 to 4 units, single story and reflect the styles of the neighborhood.

Linda A. McNamara
1911 24th Street
Sacramento, CA 95816
916/42-3616

David Hung

From: Tim Gussner [timgussner@sbcglobal.net]
Sent: Friday, February 25, 2011 3:40 PM
To: David Hung
Subject: Proposed 24th & T Development (P1-089)

Dear Mr. Hung,

I am disappointed with Ms. Rosen's plan for the 24th & T project. She has not negotiated in good faith with our neighborhood or tried to be reasonable with us. We do not oppose any development on the property but we would like a project that would compliment the neighborhood by matching our period structures.

I live on the same block for the proposed structure. We have already two apartment buildings. I feel that my block has already fulfilled the city's desire for "in fill" quota. Ms. Rosen could change her design to 3 to 4 units, single story and reflect the styles of the neighborhood. This would not require any special clearances.

Any consideration in this matter will be deeply appreciated.

Timothy Gussner
1911 24th Street
Sacramento, CA 95816
916/42-3616

ALAN LoFASO
2001 24th Street
Sacramento, CA 95818
(916) 457-4322
ALoFaso@sbcglobal.net

February 25, 2011

David Hung
Associate Planner
Community Development Department
City of Sacramento
300 Richards Boulevard, 3rd Floor
Sacramento, CA 95811

RE: P10-089

Dear Mr. Hung:

I write to express serious concerns regarding the proposed 6-unit condominium development at the corner of T and 24th Streets. This proposed development anticipates two buildings of substantial mass employing an International/Moderne style of architecture that is out of character in a traditional neighborhood of architectural styles largely consisting of Craftsman, Bungalow, and Tudor style homes. The lot is now vacant as a result of the destruction of a historic home last September.

Consistent with many of my neighbors, I request that the City approve a project to be developed on the site comprising of three single-family homes, contained in three separate structures of no more than two stories; all units should provide off-street parking; and no additional height or setback variances should be allowed. Moreover, the architectural style of the development should be consistent with that of structures within the immediately surrounding area, which are generally although not exclusively Tudor cottages, Craftsman bungalows, and Depression-era brick duplexes. In keeping with these styles, and the general character of our neighborhood, the structures should not be too massive for the surrounding area, and they should employ porches and generally adhere to the principal of "eyes on the neighborhood."

This request is consistent with several planning principals enunciated in the City's general plan. For example, Goal LU 4.3.1 of the City's general plan requires that the "City shall protect the ... character of traditional neighborhoods, including ... architectural styles..." Poverty Ridge/Newton Booth is one of 13 traditional neighborhoods specifically mentioned in the general plan. Moreover, the general plan provides that the "City shall preserve the existing ... densities on each block of Traditional Neighborhoods." (See LU 4.3.2.) The general plan also provides that "[w]ithin the Traditional Neighborhood Medium Density designation, [a] development shall be allowed to reach 36 units per acre" only under limited conditions, including that the "development [must] maintain the character of Traditional Neighborhood Medium Density by presenting a façade of single family homes or duplexes..." (See LU 4.3.3.)

David Hung – February 25, 2011 – Page Two

The neighbors' request regarding this project is also consistent with developments in the surrounding area. For example, a nearby project recently approved by the City Planning Commission at 20th and S Streets (P10-069) would provide for nine single family homes, each as a separate structure, and built according to a Craftsman architectural style on a site approximately three times the size of the one at issue in this application. Three single family structures, in an appropriate architectural style, would be equivalent to nine units on a site three times as large. Moreover, this proposal would increase the number of units on the site threefold—well beyond the requirement to “preserve existing densities on each block.” Finally, the density of the proposed project at 20th and S Streets is approximately 24 DU/AC, within the mid-range of the general plan's density provisions for Traditional Neighborhood Medium Density.

Poverty Ridge/Newton Booth is a unique, historic Sacramento neighborhood with a character that varies even from block to block. It is important to preserve the character of this neighborhood. The surrounding area was the victim of many poor development choices during the 1960s and 1970s. These examples should not be the justification of additional developments out of character with the immediate surrounding area. Moreover, even an architectural style of greater quality that might be appropriate in a part of downtown or midtown Sacramento comprising of more recent infill developments characterized by a more modern style is not appropriate for this particular traditional neighborhood.

City planners have a variety of choices to meet the City's general plan requirements, including policies supporting appropriate infill development and smart growth. My neighbors and I support these principles. However, this application as currently proposed, is not an appropriate approach to meeting these policy goals. There are other alternatives, and the neighborhood is poised to work constructively within these policy goals in a manner that will preserve the traditional character of the Poverty Ridge/Newton Booth neighborhood.

Thank you for your consideration of my concerns.

Sincerely,

/s/

Alan LoFaso

Cc: Honorable Robert King Fong, Council Member, Fourth District
Lisa Nava, District Director, Office of Council Member Fong

Claire Pomeroy, MD, MBA
2009 23rd Street
Sacramento, CA 95818

February 23, 2010

RE: Proposed 24th and T project

Dear Planning Commission members,

I write to ask that you help our diverse and thriving neighborhood achieve our dream of rejuvenation with urban renewal and infill projects designed to create a strong community. Specifically, I ask that you do this by requiring that the 24th and T project be limited to 3 units and constructed in a style consistent with the character of our neighborhood.

We are a re-emerging city neighborhood that strives to be an important part of the future of Midtown Sacramento. We celebrate our neighbors - people from all walks of life, the elderly and the young, renters and owners, families and singles - who have discovered the joy of living in this special part of the city. We celebrate the combination of stately homes, historic bungalows, garden cottages, and affordable apartments. As we walk through our neighborhood, we see the pride of our neighbors reflected in well-kept gardens; we meet each other to chat as we walk our dogs; and we embrace new families when they move in to build their lives here.

Our neighborhood has a personality - one that has emerged over the past several years. We want to ensure that as we grow and improve, we maintain that personality, so that we become a vibrant part of the future of our city.

That is why I write to you today to ask that you honor and support our neighborhood's wish that the housing project proposed for 24th and T not radically diverge from the previous density of housing at that address. I ask that you respect the neighborhood's suggestion to limit the number of units to 3 (an increase in density from the previous single family home that is acceptable to the neighbors) and that the style, height, size and landscaping be consistent with the neighborhood.

I want to emphasize that we believe in urban neighborhoods (that's why we live here!) and we believe in in-fill projects. What we are asking is that the in-fill projects respect the character of our community. We believe that the community's acceptance of an increase from 1 unit to 3 units is a thoughtful and reasonable position on the part of those of us who will welcome the inhabitants as new neighbors. We believe that this is a responsible use of the land that would represent a compromise on both sides. It is unfortunate that the current developer's request to put 6 units on the property appears to be driven by a desire to maximize business profits, not to help create a dynamic city neighborhood. By building an oversized structure with 6 units, we fear the project will skew the neighborhood away from a balance of single family homes and

apartment complexes and risk a “tipping point” that could destroy the neighborhood feel of our community.

A group of neighbors has diligently tried to discuss reasonable compromises with the developer, Andrea Rosen. I am disappointed that she has maintained that the number of units in the project is not an issue that she is willing to discuss. I do not see good faith or evidence of caring about our neighborhood in this negotiation stance. She appears unwilling to think beyond “cash flow” to consider the “impact on our neighborhood”.

Our Newton Booth Neighborhood Association has also expressed their lack of support for the project as currently proposed.

Therefore, I ask the Planning Commission to require project plans that limit the density increase on the 24th and T project site to 3 units and ensure that the style, height, size, and landscaping are consistent with the overall character of our neighborhood.

Thank you for your consideration,

Claire Pomeroy, MD, MBA
Vice Chancellor and Dean, UC Davis Health System*
Resident - 2009 23rd Street, Sacramento, California

*This title is used for identification purposes only, per UC policy.

cc: Mayor Kevin Johnson

cc: Councilman Rob Fong

David Hung

From: Bill Robertson [w.preston.robertson@gmail.com]
Sent: Monday, February 28, 2011 12:10 AM
To: David Hung
Subject: AR letter to CP
Attachments: AR letter to CP.pdf

David--

Attached please find a .pdf of the letter that Ms. Rosen hand-delivered to my wife, Dr. Claire Pomeroy, at the UC Davis Health System, where she is Vice Chancellor and Dean. The letter speaks for itself in its inappropriateness and as a representation of the challenges our neighborhood has faced in its efforts to negotiate, just as, I am sure, Ms. Rosen has had her own perceived challenges with us.

I would note only that Ms. Rosen's claim in the letter of her development's "health benefits" is contradicted by her own admitted haste to demolish the existing 1940's house on the lot in order to avoid impending lead paint abatement guidelines set to take effect.

Please include both the attached .pdf and this email explaining it inclusion.

Thanks,

Bill Robertson

--

William P. Robertson
2009 23rd Street
Sacramento, CA 95818
916-607-2405

February 9, 2011

Dr. Claire Pomeroy
Chief Executive Officer, UC Davis Health System
UC Davis Vice Chancellor for Human Health Sciences
Dean, School of Medicine
4610 X Street
Sacramento, CA 95817

HAND DELIVERED

RECEIVED
SCHOOL OF MEDICINE
OFFICE OF THE DEAN
11 FEB 11 PM 3:22

RE: Your Position Regarding Two-Triplex Residential Infill Development at 24th and T Streets (Courtyard Condominiums)

Dear Dr. Pomeroy:

I am writing to you in your capacity as a regional health leader in Sacramento. I am asking you to re-evaluate your position on the above-named residential development project in light of the public health benefits of this type of residential development.

This Smart Growth residential infill development brings important short term and long term health benefits to the region and to its residents - current and future. This project contributes to a reduction in Sacramento's air pollution- a continuing public health challenge to the region.

I am asking you to join other regional health leaders who are supporting this project in large part due its health benefits. Please see the attached letters from:

- Larry Greene, Sacramento's Air Pollution Control Officer
- Mike McKeever, Sacramento Area Council of Governments Executive Director
- Teri Duarte MPH, Sacramento Design 4 Health Chair*

These leaders recognize the health benefits of 24th and T Courtyard Condominiums and its contribution to regional health improvement by making high quality residential housing available to future Midtown residents who will be able to choose a sustainable lifestyle that includes less or no reliance on the automobile. High quality housing built within close walking distance to light rail increases ridership thereby reducing Vehicle Miles Travelled (VMT), thereby reducing air pollution which improves individual and community public health.

Infill such as 24th and T Courtyard Condominiums provides attractive housing options for individuals who choose to prioritize walking and cycling as a lifestyle choice by making these travel options easier and therefore more likely to be used. This project, in particular, is pedestrian-oriented and thus contributes to a healthy lifestyle by its residents.

I encourage you to read the support letters from these important health leaders. I have also attached a bibliography of articles documenting the health effects of air pollution and the health benefits of Transit Oriented Development. 24th and T Courtyard Condominiums is a perfect example of a Transit Oriented Development and has been recognized as such.

I know you are very busy, but health and our built environment is an important regional health concern that merits your attention. Community support for Smart Growth residential infill is a priority for Sacramento. (See Sacramento General Plan adopted in March 2009).

I request an opportunity to review this project including its health benefits with you at your earliest opportunity. Please contact me at 916.508-6721 to set up a time to meet.

Dr. Pomeroy. You could use this project as a vehicle to educate the neighborhood and community about the health benefits of Smart Growth residential infill. There are MANY UC experts on this topic who would be happy to work with you to organize such a forum. As a doctor, educator and health leader, I urge you to support the requested residential density at this critically important site.

Respectfully,



Andrea Rosen MPH, JD

Enclosed:
Support Letters for 24th and T Courtyard Housing as described
Bibliography

*This group includes doctors from UCD who will be testifying publicly in support of the health benefits of this project.

David Hung

From: trostel4@comcast.net
Sent: Sunday, February 27, 2011 9:33 PM
To: David Hung
Cc: Robert King Fong
Subject: Proposed 24th & T st development

Mr Hung,

I apologize for the timing of this email but I have been out of town for approximately one week and just arrived home to find I could voice my family and I concerns with the proposed project on the corner of 24th and T st.

We believe the specifications of the project would bring numerous negatives to the neighborhood.

My wife and I bought our house on the corner of 24th and S st approximately 3 yrs ago. We bought in this particular neighborhood because it encapsulated the quintessential midtown neighborhood with the old victorian architecture and family atmosphere all in one. My wife now runs a successful childcare facility out of our home in which she has multiple children each day.

Although we love our neighborhood it is not without it's problems. Because we have numerous businesses and a bar in the neighborhood we constantly struggle with parking and oversaturation of traffic on our streets. With two young children this makes it difficult for them to play anywhere in the neighborhood outside the boundaries of our fences as well as the difficulty of finding parking for us when we arrive home. There is simply too many cars and not enough parking in the neighborhood. It is my understanding that the developer of the project in question is asking for variances on the parking that is essential for the size of the project. This is of great concern to my family and this will greatly effect the neighborhood in an extremely negative way.

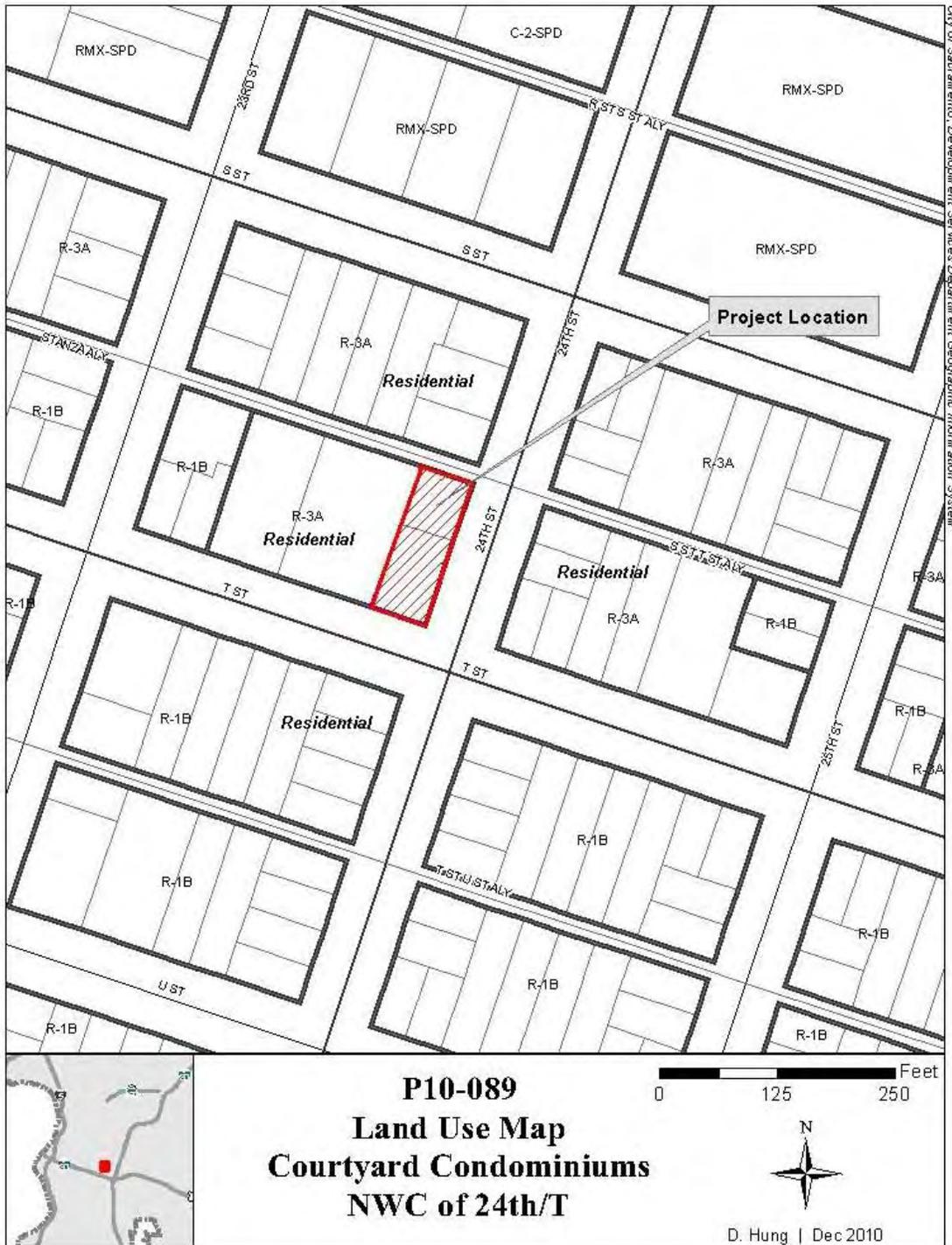
Another concern for my family is the overall size of the project. My wife and I could have moved anywhere but we chose to buy in Sacramento because of it's beautiful victorian architecture as well as it's small city feel. Similar to the brownstones of Brooklyn New York or the row housing of Baltimore, Midtown Sacramento is known for it's early century architecture and that is something that has always attracted people such as myself to this beautiful city. We feel that something in the same theme of the existing neighborhood would be in order with no more than 3 units going into 2 very small lots.

This letter is not a protest against change. My family and I welcome positive changes for the neighborhood, like restaurants shops and even housing. The issues we have with the proposed design is the sheer size and potential problems this project would bring to the neighborhood.

We thank you for hearing our concerns and once again I apologize for the timing of this letter.

Thank you,
Michael Trostel
2400 S st
Sacramento Ca 95816
530-400-1515

Attachment 4 – Land Use & Zoning Map



City of Sacramento, Development Services Department Geographic Information System

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REPORT TO DESIGN COMMISSION City of Sacramento

4

915 I Street, Sacramento, CA 95814-2671

STAFF REPORT
March 16, 2011

To: Members of the Design Commission

Subject: **The Gateway on Fremont Park (IR11-041)** Located at 1601 16th Street, the applicant is requesting to develop a five (5) story, 30 residential unit project with approximately 5,847 square feet of ground floor retail on .44 +/- acres in the C-2 (General Commercial) zone.

- A. Design review and comment on Capitol Area Development Authority (CADA) development, request to develop a five (5) story, 30 residential unit project with approximately 5,847 square feet of ground floor retail.

Location:

Address: 1601 16th Street, Sacramento, CA
Assessor's Parcel Number: 006-0293-001, -002, and -026
Council District 3
Central Core Design Review District

Recommendation: Staff recommends the Design Commission **review and forward advisory conditions of approval to Capital Area Development Authority (CADA)** for item A.

Contact: Evan Compton, Associate Planner, (916) 808-5260
Luis R. Sanchez, AIA, LEED AP, Senior Architect (916) 808-5957

Applicant: John Leonard, Sukna Global Holdings, Inc., (916) 443-8300, 2210 K Street, Suite 101, Sacramento, CA 95816.

Owner: Tom Kigar, Capital Area Development Authority (CADA), (916) 322-2114, 1522 14th Street, Sacramento, CA 95814.

SUMMARY: The project is before the Design Commission for the first time. The development consists of a five story mixed-use building at the southeast corner of 16th and P Streets. This project includes demolishing an existing motel. The proposed building will accommodate 5,847 square feet of commercial space and 30 residential units. The proposed 30 units would include 11 one bedroom units and 19 two bedroom units ranging in size from 925 square feet to 1,535 square feet.

CADA is exempt from requiring Planning and Design Review entitlements from the City, but is requesting a review and comment in the form of Advisory Conditions of Approval.

Table 1: Project Information
Existing zoning of site: C-2 (General Commercial)
Existing use of site: Motel
Property dimensions/area: 120 feet by 160 feet or 19,200 square feet (.44± acres)
Building square footage: 56,272 ± square feet
Building height: 60'6" to top of parapet, 68' to top of photovoltaic panel system.
Exterior building materials: Cement plaster and brick veneer, precast concrete veneer, aluminum storefront systems, steel railings, and steel awnings.

PUBLIC/NEIGHBORHOOD OUTREACH AND COMMENTS: Staff notified the adjacent property owners within 500 feet of the subject site and neighborhood associations for the March 16, 2011 Design Commission Hearing. The following organizations were noticed: Capitol Area R Street Association, Downtown Partnership, Richmond Grove Neighborhood Association, Midtown Neighborhood Association, Midtown Business Association, and the Newton Booth Neighborhood Association.

The applicant has contacted property owners and neighborhood associations during the initial planning and design phase. No comments have been received by staff at the time of writing this report.

ENVIRONMENTAL CONSIDERATIONS: CADA prepared and certified an environmental impact report that included analysis of development on the project site. CADA has indicated that additional CEQA analysis may be required. The City does not have discretionary authority for the project, but would review and submit any appropriate comments if additional environmental review is completed by CADA.

SUSTAINABILITY CONSIDERATIONS: The City has adopted a Sustainability Master Plan to complement the City's General Plan. This was done to ensure that the City set the standard for the practices of sustainability within its own organization as well as becoming a model for any construction projects within the City. Projects should consider the following goals adopted by the City as projects are proposed within the City: reduce consumption of materials, encourage the reuse and local recycling of materials, reduce the use of toxic materials; establish and continuously improve "green" building standards for both residential and commercial development--new and remodeled, reduce dependence on the private automobile by working with community partners to provide efficient and accessible public transit and transit supportive land uses, reduce long commutes by providing a wide array of transportation and housing choices near jobs for a balanced, healthy city; improve the health of residents through access to a diverse mix of wellness activities and locally produced food, promote "greening" and "gardening" within the City, create "Healthy Urban Environments" through Restorative Redevelopment, and maintain and expand the urban forest.

The Gateway on Fremont Park (IR11-041)

Staff recommends that the applicant introduce sustainable practices during the construction of the proposed project. Staff recommends the use of a Construction Waste Management Plan, energy efficient designs, and the use of local materials as a minimum standard for this project.

POLICY CONSIDERATIONS: The 2030 General Plan Update was adopted by City Council on March 3, 2009. The 2030 General Plan's goals, policies, and implementation programs define a roadmap to achieving Sacramento's vision to be the most livable city in America. The 2030 General Plan Update designation of the subject site is Urban Corridor High. As stated in the General Plan, this designation is in urbanized areas and includes multistory structures and highly developed transit service. New development along the corridor contributes to a more compact and consistent pattern that relocates parking primarily to structures and to the rear of buildings. Street level frontages are lined with retail and other pedestrian-oriented uses. The streetscape is appointed with pedestrian amenities that support and enhance pedestrian activity.

In addition, the 2030 General Plan has identified goals and policies under the Land Use and Urban Design Element. Some of the goals and policies supported by this project are:

- *Redeveloping Automobile-Oriented Corridors.* The City shall promote redevelopment of existing automobile corridors and the upgrading of existing commercial development to create vibrant, mixed use boulevards that balance efficient movement of motor vehicles with the creation of attractive pedestrian-friendly districts that serve the adjoining neighborhoods as well as passing motorists. (Policy LU 6.1.3)
- *Neighborhood Enhancement.* The City shall promote infill development, redevelopment, rehabilitation, and reuse efforts that contribute positively (e.g., architectural design) to existing neighborhoods and surrounding areas. (LU 2.1.6)
- *Reduce Minimum Parking Standards.* The City shall reduce minimum parking standards over time to promote walkable neighborhoods and districts and to increase the use of transit and bicycles. (M 6.1.2)
- *Transitions in Scale.* The City shall require that the scale and massing of new development in higher-density centers and corridors provide appropriate transitions in building height and bulk that are sensitive to the physical and visual character of adjoining neighborhoods that have lower development intensities and building heights. (Policy LU 2.7.3)
- *Buildings that Engage the Street.* The City shall require buildings to be oriented to and actively engage and complete the public realm through such features as building orientation, build-to and setback lines, façade articulation, ground-floor transparency, and location of parking. (Policy LU 2.7.7)
- *Screening of Off-street Parking.* The City shall reduce the visual prominence of parking within the public realm by requiring most off-street parking to be located behind or within structures or otherwise fully or partially screened from public view. (Policy LU 2.7.8)

Central City Community Plan Policies:

- *Mixed-Use Buildings.* The City shall provide the opportunity for mixture of housing with other uses in the same building or on the same site at selected locations to capitalize on the advantages of close-in living. (CC.H 1.1)

This proposed project is also consistent with the Sacramento Central Core Design Review District and Urban Design Plan. Staff is supportive of the design and recommends Design Commission review and discuss the following items to assist the applicant with further project development.

Design Policy Considerations: Promote creative architectural solutions that acknowledge contextual design issues. Complement the architectural character of the Sacramento area and promote harmony in the visual relationships and transitions between new and older buildings. Relate the bulk of the new structure to the scale or context of existing area to avoid an overwhelming or dominating appearance. Enhance the pedestrian experience. Promote efforts to utilize high-quality building materials, detailing and landscaping.

Design Guidelines Considerations: Enhance the building base, street wall height, and mechanical parapet. Provide building step backs to further articulate façade. Relate the building's massing to the neighborhood. *"How does the building complement adjacent buildings?"* Enhance the design of fenestration and rhythm of the building. Promote building articulation through the use of offsets, insets, and reveals. Promote the ground level pedestrian experience and protection. Retain and enhance landscaping, sidewalks and curbs. Provide project lighting that complements the character of the neighborhood and design. Integrate Mechanical, Service, and Recycling/Trash collection areas into the building design.

STAFF RECOMMENDATIONS TO COMMISSION: Staff is very supportive of this project and provided the design team a list of comments attached at the end of this report. Staff feels the massing of the proposed structure is respectful of the neighboring properties. The massing as shown on the plans will complement the Fremont Building to the north but is designed as a standalone project. The building steps down in height on the alley (south property line) with a pavilion and respects the lower height at 1617 16th Street. The existing building at 1612 P Street (east property line) is also smaller in scale, and the proposed project references the existing datum line with the ground floor brick veneer on the new structure. The building feels substantial at both the northeast and southeast corners along 16th Street which anchors the building for pedestrians, bicyclists, and the motoring public along the commercial corridor. The alley will also be activated with the open pavilion area and outdoor restaurant seating.

The project has multiple building planes and incorporates inset balconies for the residential units. The metal railings on the balconies facilitate clear views of the adjacent park and provide visual interest to the building. A mixture of materials has been proposed and each elevation (including the alley and interior facing facade) is articulated and includes appropriate finishes.

Staff appreciates the variety and placement of the building fenestration. On the eastern elevation, the windows look over a tenant courtyard area. The courtyard utilizes areas of both hardscape and softscape and also provides vertical planter strips to maximize planting opportunities.

Staff requests that the Design Commission review and comment on the proposed project design as well as the following points.

Site Comments:

1. Staff supports the proposed tenant courtyard, but requests review and comment by the Design Commission regarding the proposed mix of hardscape and softscape.
2. Staff requests additional information on the proposed pavilion area including paving material, proposed café railings, and details on the raised planter areas.

March 16, 2011

The Gateway on Fremont Park (IR11-041)

3. Staff requests additional consideration regarding the size of the proposed trash and recycling enclosure. The project site should provide adequate space for trash to avoid placing cans on the alley.

Building Comments:

4. Staff requests a color and materials board be provided at the Design Commission Hearing.
5. Staff requests further information about the steel frame supporting the rooftop photovoltaic panels and how the structure will be designed to be substantial and incorporated into the building design.
6. Staff supports sustainable elements proposed such as solar panels, greenhouses, and fish harvesting areas on the rooftop. Staff recommends additional details of these elements be provided at the Design Commission Hearing.

Signage Comments:

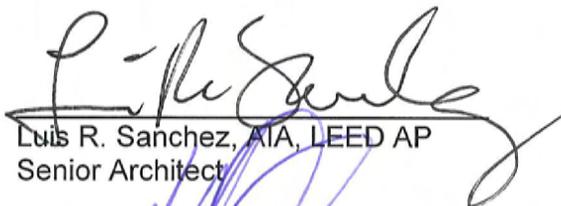
7. Staff recommends high quality signage with design and materials that complements the architecture. Staff recommends pedestrian oriented signage at street level either through signs suspended from the awnings, wall mounted plaques, or small graphics on entry door glazing.

Respectfully submitted by:



Evan Compton
Associate Planner

Recommendation Approved:



Luis R. Sanchez, AIA, LEED AP
Senior Architect



William Crouch, AIA, FRAIA, NCARB, LEED AP
Urban Design Manager

The Gateway on Fremont Park (IR11-041)

Attachments:

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Attachment 2		
Page 11		Vicinity Map
Attachment 3		
Page 12		Project Plans
		Exhibit A: Coversheet
		Exhibit B: Design Narrative
		Exhibit C: Rendering South from 16 th Street
		Exhibit D: Corner Entry at 16 th and P Street
		Exhibit E: View from P and 16 th Street
		Exhibit F: Restaurant Corner at 16 th Street and Alley
		Exhibit G: Ground Floor Plan
		Exhibit H: Second Floor Plan
		Exhibit I: Third and Fourth Floor Plan
		Exhibit J: Fifth Floor Plan
		Exhibit K: Roof Plan
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		Exhibit N: South and West Elevations
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**Attachment 1
Proposed Findings of Fact and Advisory Conditions of Approval
The Gateway on Fremont Park (IR11-041)
1601 16th Street**

Findings Of Fact

- A.** The **Design Review** request to develop a five (5) story, 30 residential unit project with ground floor retail is approved, subject to the following Findings of Fact:
1. The project, as conditioned, enhances the surrounding neighborhood.
 2. The project, as conditioned, will complement structures in the vicinity, and conforms to the Design Commission's design criteria.
 3. The proposed use will be consistent with the objectives of the City of Sacramento General Plan.
 4. The project is based upon sound principles of land use in that the proposed use is allowed in the General Commercial (C-2) and includes conditions addressing building design, site design and signage.

Advisory Conditions Of Approval

The Design Review request to develop a five (5) story, 30 residential unit project with ground floor retail is hereby approved subject to the following advisory conditions.

- A.** **The design of the site (see plans attached) is hereby approved subject to the following advisory conditions. These conditions must be met prior to the issuance of a building permit:**
1. The building shall be sited as indicated in the report and exhibits. The Applicant shall coordinate with the appropriate City agencies regarding alley improvements associated with the overall project
 2. Auto access and site layout shall be as indicated in the report and exhibits.
 3. The project shall have building setbacks as indicated in the exhibits.
 4. The project shall have building entries as indicated in the exhibits. Further development of all project entry elements, including building sign standards and lighting cut sheets, shall be reviewed and approved by Design Review staff prior to Building Permit submittal.
 5. The applicant shall work with Design Review Staff on special paving for the outdoor seating area on the alley and the City standard paving in the public right of way. Final paving shall be reviewed and approved by Design Review Staff prior to issuance of Building Permit.

The Gateway on Fremont Park (IR11-041)

6. The applicant shall work with Development Engineering, Urban Forest, and Design Review staff on the removal and pruning of existing street trees, as well as selection of new trees. All landscaping shall have automatic irrigation.
7. Exterior lighting style and design shall be compatible and consistent with the building design, and the site should be adequately illuminated for safety and security with a minimum 1.0 foot candle throughout. Appropriate lighting should light up wall surfaces or landscape areas. The applicant shall submit all site light fixtures cut sheets and plan locations for review and approval by Design Review staff prior to submitting for Building Permit. Street pole lights shall be energy efficient with cutoff devices included in the acorn style fixtures.
8. Site mechanical equipment and utility vaults shall be incorporated into the project site. Backflow prevention devices, SMUD boxes, etc., shall be placed in vaults or incorporated into building structure where not visible from street views, and screened from any pedestrian view. The applicant shall submit final mechanical locations for review and approval by Design Review staff prior to Building Permit submittal.
9. The integrated trash enclosure details and construction shall be reviewed and approved by Design Review staff prior to Building Permit submittal. All final material, finish, and color, shall match with the project's material and color scheme, and shall be reviewed and approved by Design Review staff prior to Building Permit submittal.
10. The project shall include landscaping elements as indicated in the report and exhibits, and final landscape plans (including hardscape and softscape) shall be reviewed and approved by Design Review staff prior to Building Permit submittal. Final tree species shall be coordinated with Urban Forest staff. Automatic irrigation shall be provided for all planting and landscaping. Drought tolerant vegetation shall be provided at the street tree planters. **Street planters and sidewalk shall be provided per Development Engineering standards and reviewed by Development Engineering, Urban Forest, and Design Review staff prior to Building Permit submittal.**

B. The design of the new building (see plans attached) is hereby approved subject to the following advisory conditions:

11. The design of the building shall be as indicated in the report and exhibits with final conditions as approved by the Design Commission.
12. The building elevations shall have a consistency of detail and quality.
13. All the final details for the steel frame structure supporting the rooftop photovoltaic panels shall be reviewed and approved by Design Review staff prior to Building Permit submittal.
14. The exterior materials provided shall be precast concrete veneer, full brick veneer, and cement plaster with an imperfect smooth finish. Design Review Staff to review final colors and materials palette prior to issuance of Building Permit.
15. Single hung windows shall be provided per approved plans. Aluminum storefront system and other window design and placement shall be provided per approved plans.
16. Steel awnings and eye-brow projections shall be provided per approved plans.

17. Ensure green roofs and courtyards have sufficient depth to sustain landscaping. Final plans shall be provided to Design Review Staff for review and approval prior to Building Permit submittal.
18. Exterior lighting style and design shall be compatible and complementary to the building design. **Final building lighting plans and light fixture cut sheets shall be reviewed and approved by Design Review staff prior to Building Permit submittal.**
19. Final mechanical penthouse shall be integrated into the final elevations through the use of materials and design. Final roof plan and mechanical penthouse elevations with mechanical equipment locations shall be reviewed and approved by Design Review staff prior to Building Permit submittal.

C. The design of the signage is hereby approved subject to the following conditions:

20. High quality signage with a design and materials that complements the architecture is required and shall meet the sign ordinance and Design Review Guidelines for the Central Core Design Review District. **Signage criteria for the project shall be submitted to Design Review staff and the City Sign Coordinator for review and approval prior to building permit submittal. Signage criteria requirements are locations of signage (elevations) including general size, potential illumination, and materials. Final sign designs shall be reviewed at time of tenant improvement Building Permit submittal.**

D. General conditions:

21. All final details affecting the exterior building design that are not determined at the time of the Design Commission's final review shall be reviewed and approved by Design Review staff prior to Building Permit submittal.
22. All other notes and drawings on the final plans as submitted by the applicant are deemed conditions of approval. Any changes to the final set of plans stamped by Design Review staff shall be subject to review and approval prior to Building Permit submittal. Applicant shall comply with all current building code requirements.
23. Any major revisions to the final approved design are subject to review and approval by the Design Commission.
24. **All required new and revised plans shall be submitted for review and approval by Design Review staff prior to building permit submittal. A set of the appropriate plans (reduced to 11 x 17 set) along with a Letter of Compliance indicating how the project is in compliance with each Condition of Approval with detailed sheet references shall be submitted directly to Design Review Staff 1-2 weeks prior to Building Permit submittal. A final 3D model (SketchUp preferred) shall be provided to Design Review staff prior to building permit submittal. All necessary planning entitlements shall have been approved by the Planning Commission prior to final Design Review sign-off of plans.**
25. The approval shall be deemed automatically revoked unless required permits have been issued and construction begun within three years of the date of the approval. Prior to

expiration, an extension of time may be granted by the Design Commission upon written request of the applicant.

26. **The Design Commission decision may be appealed to City Council. Appeals must be filed within 10 calendar days of written notice of the Design Commission action.**
27. Building permits shall not be issued until the expiration of the 10 day appeal period. If an appeal is filed, no permit shall be issued until final approval is received.
28. Final occupancy shall be subject to approval by Design Review Staff and shall involve an on-site inspection by Design Review Staff.
29. **The Record of Decision shall be scanned and inserted into the final set as a general sheet to be submitted for building permit.**

Attachment 2: Vicinity Map

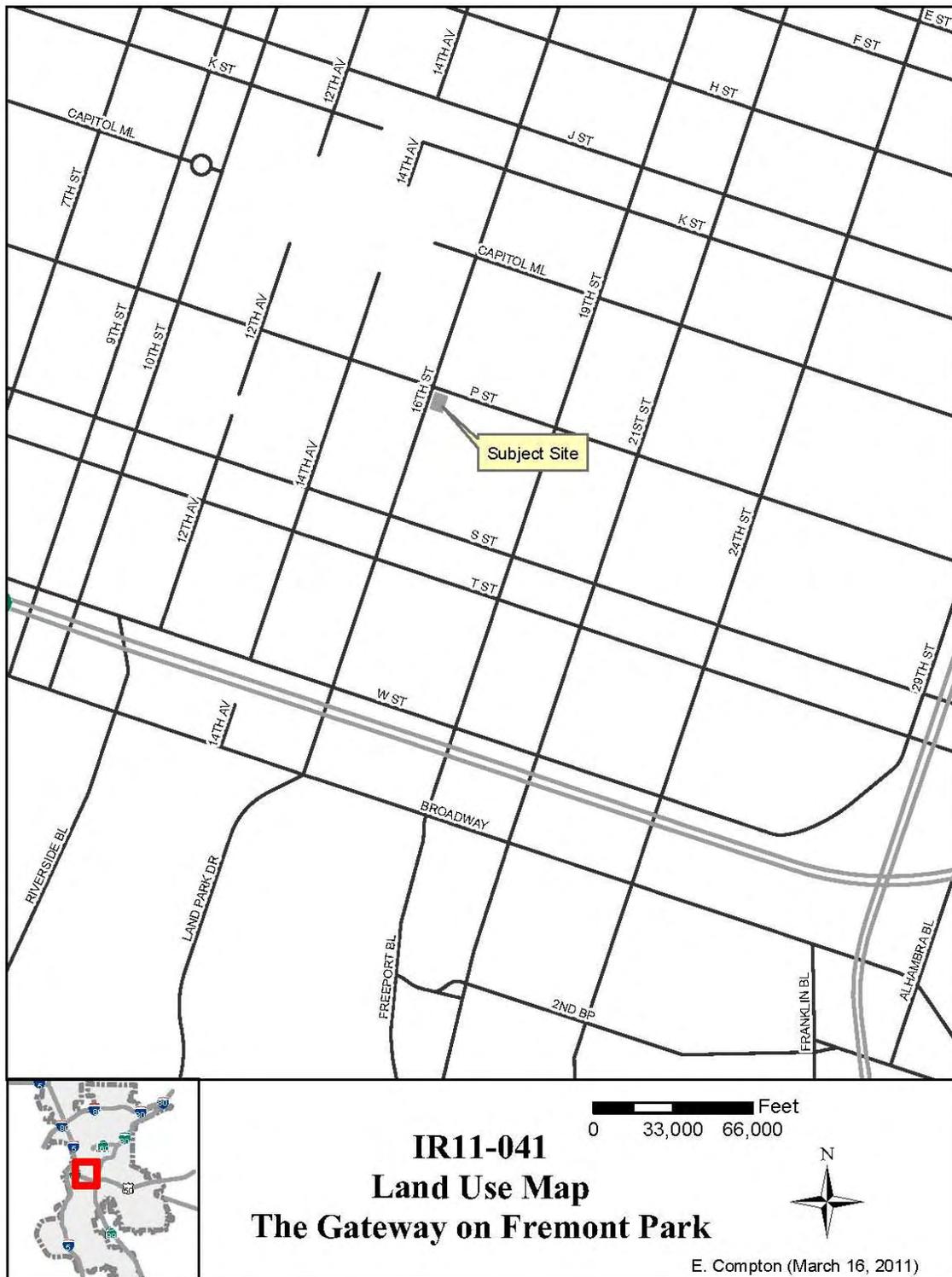


Exhibit B: Design Narrative

The Gateway on Fremont Park / Design Narrative

With expansive frontage along the east side of Fremont Park, our design team has endeavored to create a building that is a landmark in its own right. The project is a multi-story, multi-unit residential development. The project consists of a five-story mixed-use building with street level restaurant and retail space. The design responds to its context with a blend of traditional and modern materials, including a detailed brick facade emphasizing structure, a glazing system highlighting the significance of its corner, and stepped back building massing providing views from the majority of its residential units. The project will endeavor to be a model of sustainability, employing an array of technologies which will provide its residents net-zero energy use.

The project also presents an opportunity to advance many of the goals of the Capital Area Development Authority, and the evolving 18th Street Corridor. Guided by a process that emphasizes sustainability and excellence in design, *The Gateway* development has the potential to become a project that foreshadows a new future of growth in the Sacramento region.

Our design concept seeks to implement the following objectives:

Gateway Presence

Each day thousands pass Site 4, often traveling north on 18th Street into the city. Our design recognizes that for those passing in cars, the alley corner of the site becomes the "primary corner." We have designed the building to have significant features at the corner of the alley as well as the corner of F Street, to both welcome the motorcade and to create a positive impression of the development and community.

Park Views

With flat topography and a no ocean frontage, Sacramento is a city largely absent views. Parks provides an exception, and our project takes advantage of its long frontage facing Fremont Park by providing park views in 22 of the 30 units, each with private balconies on the park side. Our project is "single-loaded," meaning all units are oriented to streetfacing, and will be entered from a common courtyard space on the interior side of the site.

Solar Orientation

While the park views require windows to the west, our design solution wings many of the windows around the corner to the south also, enhancing views while significantly improving the ability to utilize passive solar shading to slow solar gain in the winter, and full shades in the summer months.

Alley Activation

With its retail edge and primary focal point abutting the alley, the site presents an excellent opportunity to build on the alley activation movement that has been enjoying success in Midtown neighborhoods. We propose to improve infrastructure and introduce pedestrian and bike friendly details into the alley, with café seating and landscaping spilling to its edge.

Courtyard Common Area

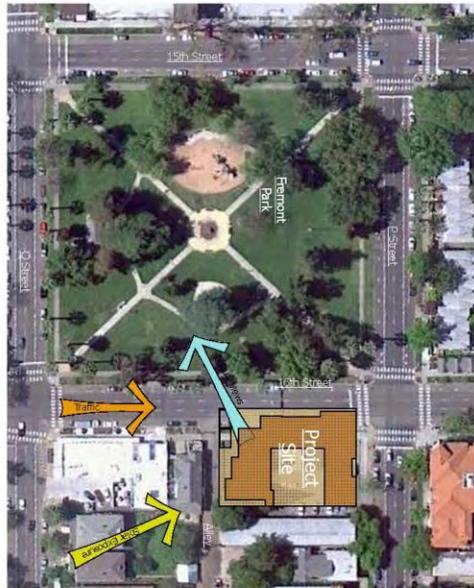
At the center of the project design is an expansive open courtyard on the podium level, providing an outdoor amenity space for residents and an alternate building form for architectural articulation. Each unit will enjoy cross-ventilation with windows into the courtyard for natural cooling, and each second level unit will have a semi-private entrance patio on the courtyard.

Redwood Tree Re-Use

The existing redwood trees on the site will be milled and utilized as a finish wall material in interior common spaces such as the lobby, for details and outdoor benches in the courtyard, and in a few locations as exposed decorative columns.

Rooftop Solar

A significant commitment to solar energy capture is evidenced at the roof level of design, which has accommodated 11,000 sq ft of photovoltaic panels on a steel frame elevated over the podium, producing a substantial percentage of building energy needs.



Site Plan

Multi-Modal Transportation

The project design seeks to balance between the myriad of transportation options. While providing open parking space per residential unit (50 total), the project is designed to engage the pedestrian experience with its active edges and proximity to transit, shops, and jobs. Class A bicycle facilities will also be provided.

Sustainability

Enhancing both density and quality living spaces, the project design will reduce vehicle miles traveled by providing for and attracting many to an urban environment. The building massing, orientation, and unit design has been executed utilizing passive solar considerations. And lastly, state of the art sustainable systems will be employed throughout the development, creating a project that will be a showcase of green technology.

The Gateway on Fremont Park will be a catalyst for a more vibrant, urban, pedestrian friendly, transit-oriented 18th Street Corridor. With an emphasis on urban design principles, we will endeavor to create a mixed-use, net-zero project that will be inspiring to both the surrounding community and the passing commuter alike.

Exhibit D: Corner Entry at 16th and P Street



The Gateway on Fremont Park (IR11-041)

Exhibit E: View from P and 16th Street

10/10/10 11:47 AM 10/10/10 11:47 AM 10/10/10 11:47 AM

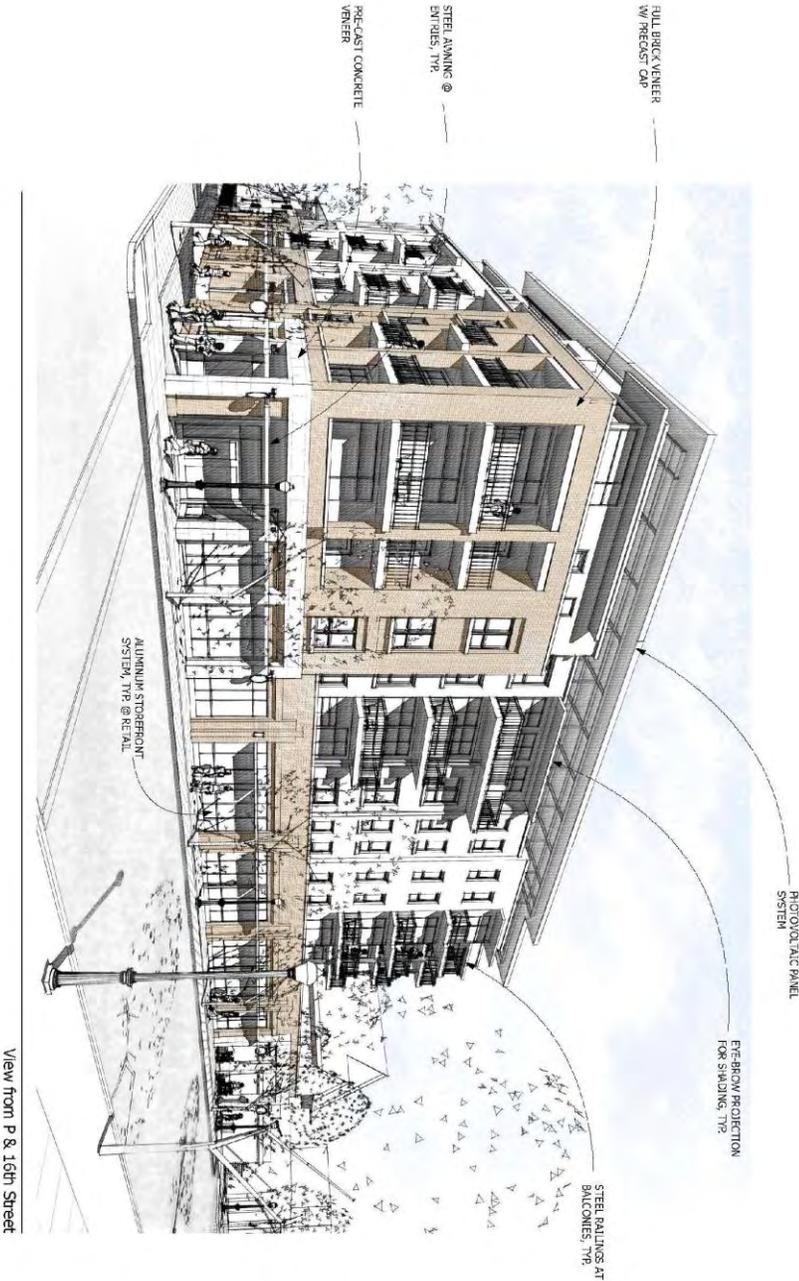


Exhibit F: Restaurant Corner at 16th Street and Alley

11/16/11 10:11 AM C:\PROJECTS\GATEWAY\11-041\11-041-001-001.dwg (11/16/11 10:11 AM)



Exhibit I: Third and Fourth Floor Plan

V:\110 1101 ST - ICA\Architectural\1101041 03.dwg

THIRD FLOOR & FOURTH FLOOR PLAN

0 5 10 15 20
1" = 10'



Exhibit K: Roof Plan

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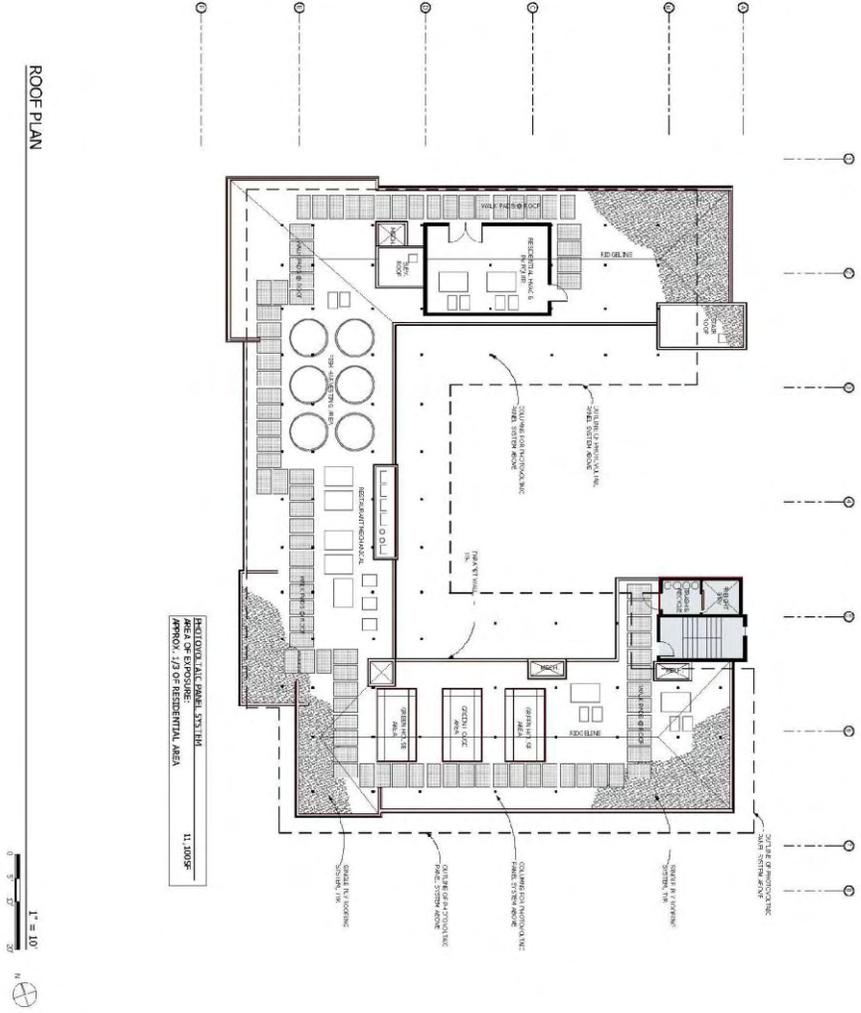


Exhibit L: Typical Units

VRILAKAS ARCHITECTS

WEST UNIT 05, TYPICAL
SCALE 3/16" = 1'-0"
1145 SF



NORTHEAST CORNER UNIT 01, TYPICAL
SCALE 3/16" = 1'-0"
1112 SF



SOUTH UNIT 07, TYPICAL
SCALE 3/16" = 1'-0"
925 SF



NORTHWEST CORNER UNIT 03, TYPICAL
SCALE 3/16" = 1'-0"
1248 SF



Exhibit M: Penthouse Units

\\FD11B112\CAD\Drawings\2011\11-041\11-041.dwg

WEST PENTHOUSE UNIT 504
SCALE: 3/16" = 1'-0"

1335 SF



CORNER PENTHOUSE UNIT 505
SCALE: 3/16" = 1'-0"

1335 SF



Exhibit N: South and West Elevations

VRILAKAS ARCHITECTS 1001 15TH AVENUE, SUITE 1000, DENVER, CO 80202 TEL: 303.733.1100 FAX: 303.733.1101

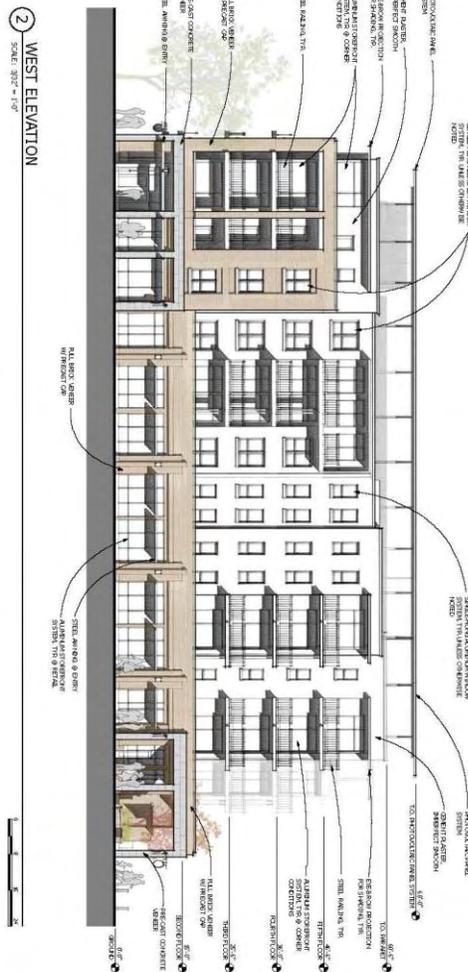


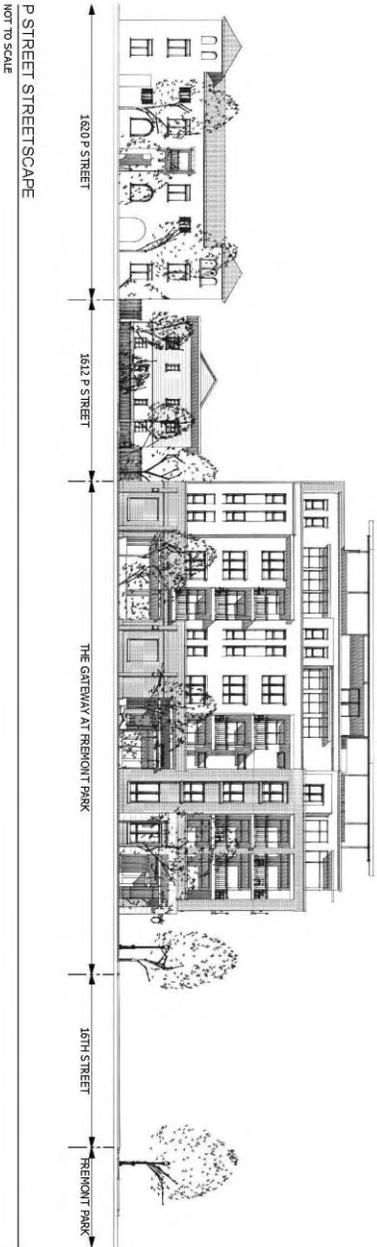
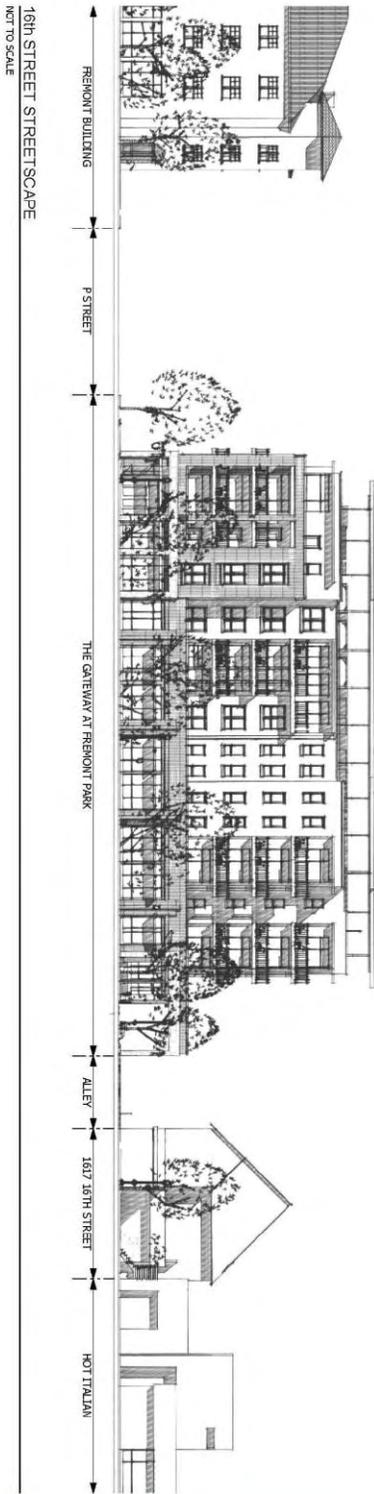
Exhibit P: Material Palette

THE GATEWAY ON FREMONT PARK (IR11-041) - 03/16/2011



Exhibit S: Streetscape Drawings

\\SRV12\BNA\PROJECTS\IR11-041\Drawings\110317\110317_1001.dwg (11/03/11)



Attachment 4: Design Review Narrative

THE GATEWAY ON FREMONT PARK

A MIXED-USE DEVELOPMENT AT 16TH & P

VRILAKAS
architects

Design Narrative

With expansive frontage along the east side of Fremont Park, our design team has endeavored to create a functional, environmentally responsive, and architecturally inspiring mixed-use project for CADA' s Gateway Site 4. Envisioned as a catalyst urban infill development along the historic 16th Street Corridor, this sustainable project consists of a five-story mixed-use building with street level restaurant and retail space. The design responds to its context with a blend of traditional and modern materials, including a detailed brick façade emphasizing structure; a glazing system highlighting the significance of its corners; and stepped back building massing providing views from the majority of its residential units. The project will endeavor to be a model of sustainability, employing an array of technologies which will provide its residents net-zero energy use.

The project also presents an opportunity to advance many of the goals of the Capitol Area Development Authority, and the evolving 16th Street Corridor. Guided by a process that emphasizes sustainability and excellence in design, *The Gateway* development has the potential to become a project that foreshadows a new future of growth in the Sacramento region.

Our design concept seeks to implement the following objectives:

Gateway Presence

Each day thousands pass Site 4, often traveling north on 16th Street into the city. Our design recognizes that for those passing in cars, the alley corner of the site becomes the " primary corner" . We have designed the building to have significant features at the corner of the alley as well as the corner of P Street, to both welcome the motorists and to create a positive impression of the development and community.

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www.vrilakasarchitects.com

VRILAKAS
architects

Park Views

With flat topography and a no ocean frontage, Sacramento is a city largely absent views. Parks provide an exception, and our project takes advantage of its long frontage facing Fremont Park by providing park views in 22 of the 30 units, each with private balconies on the park side. Our project is "single-loaded", meaning all units are oriented to street sides, and will be entered from a common courtyard space on the interior side of the site.

Solar Orientation

While the park views require windows to the west, our design solution wraps many of the windows around the corner to the south also, enhancing views while significantly improving the ability to utilize passive solar shading to allow solar gain in the winter, and full shade in the summer months.

Alley Activation

With its retail edge and primary focal point abutting the alley, the site presents an excellent opportunity to build on the alley activation movement that has been enjoying success in Midtown neighborhoods. We propose to improve infrastructure and introduce pedestrian and bike friendly details into the alley, with café seating and landscaping spilling to its edge.

Courtyard Common Area

At the center of the project design is an expansive open courtyard on the podium level, providing common green space for residents, and an atrium feeling space for primary circulation to individual units. Each unit will enjoy cross-ventilation with windows into the courtyard for natural cooling, and each second level unit will have a semi-private entrance patio on the courtyard.

Redwood Tree Re-Use

The existing redwood trees on the site will be milled and utilized as a finish wall wainscot in interior common spaces such as the lobby, for details and outdoor benches in the courtyard, and in a few locations as exposed decorative columns.

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Rooftop Solar

A significant commitment to solar energy capture is evidenced at the roof level design, which has accommodated 11,000 sf of photovoltaic panel on a steel frame elevated over the parapet, producing a substantial percentage of building energy needs.

Multi- Modal Transportation

The project design seeks a balance between the myriad of transportation options. While providing one parking space per residential unit (30 total), the project is designed to engage the pedestrian experience with its active edges and proximity to transit, shops, and jobs. Class A bicycle facilities will also be provided.

Sustainability

Emphasizing both density and quality living spaces, the project design will reduce vehicle miles traveled by providing for and attracting many to an urban environment. The building massing, orientation, and unit design has been executed utilizing passive solar considerations. And lastly, state of the art sustainable systems will be employed throughout the development, creating a project that will be a showcase of green technology.

The Gateway project will be a catalyst for a more vibrant, urban, pedestrian friendly, transit-oriented 16th Street Corridor. With an emphasis on urban design principles, we will endeavor to create a mixed-use, net-zero project that will be inspiring to both the surrounding community and the passing commuter alike.

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THE GATEWAY ON FREMONT PARK

A MIXED-USE DEVELOPMENT AT 16TH & P

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architects

Parking Narrative

The Gateway Mixed-Use project is located in the heart of the central city, two blocks from light-rail, along numerous bus lines, and within a walk of major employment centers. Parking for the residential units within the project are provided, consistent with City of Sacramento parking standards (however guest space is not provided).

Bicycle parking will exceed City of Sacramento requirements.

In order to add to the vitality and quality of life of the neighborhood, restaurant space is provided along the sidewalk edge of the ground floor. The storefronts will also serve to screen the onsite parking from the public way, and restaurant patrons will enjoy a view of Fremont Park (and the park will enjoy enhanced security with the addition of the active use).

We are not proposing to provide parking for the restaurant uses, due to the impractical nature of providing on-site parking, not uncommon for an urban project. Restaurant operators are expected to provide valet service, as necessary, typical for restaurants in the vicinity. While this site is within the Central City, it is a few blocks south of the designated Central Business SPD- the significance being that within the Central Business SPD, restaurants are not required to provide parking. The Gateway project area is, however, within a mixed-use, multi-modal community which exhibits all of the characteristics found in the Central Business SPD, two blocks to its north.

Due to its proximity and qualities, we are proposing no on-site parking for the restaurant uses.

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The Gateway on Fremont Park

Alley Partial-closure and Alternative Use Plan

The "Gateway on Fremont Park" mixed-use residential condominium project proposed for the CADA East End Gateway 4 site at the SEC of 16th and P Streets contemplates an upscale "farm to table" dining experience sited directly across the street from Fremont Park. In an effort to maximize the outside dining experience, the developer, Sukna Global Holdings, and architect Ron Vrillakas propose a partial alley closure (see attached graphics and rendering of proposed partial closure area, as well as images of other currently operated partial alley closures) from 10:00 am to 10:00 pm daily, with immediate decorative bollard removal capability by safety crews at any time.

This plan, while acknowledging the ongoing use and need of the alley, and the critical access needs for fire and life safety vehicles, proposes a creative and expanded alternate use for the alleyway: an outdoor patio dining area for the proposed restaurant use. This area, protected from traffic by temporary and removable bollards, would enable the maximum use of this area, and provide direct views of the neighborhood's key visual amenity, Fremont Park.

The proposed improvements of the alley, currently unfinished gravel, could include paving stones, or colored and stamped decorative concrete, properly graded and drained per city standard, as well as decorative and removable bollards at the edge of the sidewalk. These improvements will help address the projected increase in foot traffic generated by the project's new pedestrian oriented uses.

This is a new urbanism concept currently embraced by numerous other cities such as San Francisco, Portland, and Seattle, to name a few. It is a concept already in place in several other alleys throughout midtown Sacramento. It will increase the vibrancy, visibility and capacity of the restaurant, while adding to the new fabric of the neighborhood. This is the future of alternate, and part time uses for our urban alleyways, while still maintaining the critical ingress/egress and access for safety vehicles, and off-business hour access for commercial service vehicles such as waste removal and materials delivery.

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Oral Report

For

City of Sacramento

Agenda Packet

For the Meeting of: March 16, 2011

Title: Recognition of Chair

Recommendation: Recognition of outgoing chair, Brian Sehnert.

Contact Information: William R. Crouch, AIA, NCARB, LEED AP, Urban Design Manager, 916-808-8013

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