



REPORT TO PLANNING COMMISSION City of Sacramento

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915 I Street, Sacramento, CA 95814-2671

PUBLIC HEARING
May 27, 2010

To: Members of the Planning Commission

Subject: Greyhound Bus Terminal (P10-020)

A request to construct a bus terminal on 1.74± acres in the Office Building (OB-PUD SPD) zone and located in the Discovery Centre Planned Unit Development and Richards Boulevard Special Planning District.

- A. Environmental Determination: Subsequent Mitigated Negative Declaration;
- B. Mitigation Monitoring Plan;
- C. PUD Guidelines Amendment to allow a bus terminal use in the Discovery Centre Planned Unit Development (PUD);
- D. Special Permit to construct a bus terminal in the OB-PUD SPD zone.

Location/Council District:

420 Richards Boulevard, Sacramento, CA 95811

Assessor's Parcel Number 001-0210-045, -046, -047, -048, -049, -053

Council District 1

Recommendation: Staff recommends the Planning Commission approve the request based on the findings and subject to the conditions listed in Attachment 1. The Commission has final approval authority over items A-D above, and its decision is appealable to City Council.

Contact: Evan Compton, Associate Planner, 916-808-5260 and Stacia Cosgrove, Senior Planner, 916-808-7110.

Applicant: Craig Stradley, Mogavero Notestine Associates, 2012 K Street, Sacramento, CA 95811

Owner: City of Sacramento (Kirk Thompson), 5730 24th Street, Building 4, Sacramento, CA 95822



City of Sacramento, Development Services Department Geographic Information System

Summary: The applicant is requesting entitlements to allow for the development of a bus terminal on 1.74± acres in the Office Building (OB-PUD SPD) zone and located in the Discovery Centre Planned Unit Development and Richards Boulevard Special Planning District. The PUD Guidelines require an amendment to depict a 10,000 square foot bus terminal on the site where a 400,000 square foot, eight-story office building was contemplated as part of Phase IV of the Discovery Centre PUD.

At the time of writing the report, there were no outstanding issues or concerns regarding the project. **The project is considered to be non-controversial.**

Table 1: Project Information
General Plan designation: Urban Center High
Existing zoning of site: OB-PUD-SPD
Existing use of site: Vacant
Property area: 1.74 ± acres

Background Information: The Greyhound Bus Terminal project was formally submitted on March 9, 2010. Although the subject site is owned by the City of Sacramento, the bus terminal tenant is a private user which requires the project to obtain planning entitlements. Greyhound is currently located at 715 L Street and is requesting to relocate to 420 Richards Boulevard.

The Planning Commission may grant an amendment of PUD Guidelines provided that the proposed amendments do not increase the intensity of the land uses by more than ten percent otherwise City Council approval is required. Staff finds the proposal will not intensify land uses in the Discovery Centre PUD, therefore the Planning Commission has final approval authority over the requested entitlements unless the project is appealed or called up.

Entitlement History: The Discovery Centre PUD (P97-037) was adopted by City Council on November 5, 1998 (Resolution 98-544). The approved PUD consisted of approximately 990,000 square feet of office and a hotel to be developed in four phases. Only Phase I (consisting of a 150,000 square foot office building) has been completed at this time. The Discovery Centre PUD (P01-059) was modified on July 12, 2001 to allow an increase in the light pole height from 18 feet to 25 feet maximum. The Discovery Centre PUD (P01-066) was modified on October 30, 2001 to allow additional parking as part of Phase I and II and lower the intensity of the proposed hotel. On February 24, 2009, the City Council approved a resolution allowing the execution of a lease with Greyhound Lines, Inc. for the bus terminal at the site.

Public/Neighborhood Outreach and Comments: Staff notified the River District Association. In addition, staff notified property owners within 500 feet of the subject property. Walk Sacramento requested a pedestrian walkway be added along the

eastern edge of the parcel from the Richards Boulevard sidewalk to a crossing of the driveway near the northeast corner of the building. The applicant reviewed the request and will install additional signage to direct pedestrians to the optimal access points. The signage will minimize potential safety issues with pedestrian and bus/vehicle conflicts at the driveway entrance.

The Sacramento Area Bicycle Advocates (SABA) requested that the project include bike parking in addition to vehicle parking. They requested secure, long-term bike parking for employees and travelers. They also requested some short-term bike parking. The applicant has committed to providing a minimum of two bicycle facilities on the site (which includes at least one bicycle locker) on the northeast corner of the proposed building which satisfies the zoning code requirement.

At the Design Director hearing where the design of the project was approved with conditions, representatives from the Calvada Sales Company, located at 450 Richards directly to the east of the project site, presented three concerns: a) Drainage: The existing slope of the site partially drained on the neighbor's parcel and has the potential to create foundation issues for the neighboring building; b) Pollutants: There was concern the bus terminal would have refueling or maintenance operations that could contaminate adjacent property; c) Fencing: The Greyhound site will be fenced with wrought iron fencing with the exception of a small portion on the rear of the site which has existing chain link. The neighbor has issues with chain link because it can be easily cut which allows transients to use the vacant area for camping. The request is to have the entire site fenced with wrought iron. The applicant has reviewed and responded to each concern from the neighbor at 450 Richards Boulevard: a) Drainage: The site will be graded to ensure there is no water intrusion on the neighboring parcels; b) Pollutants: The Greyhound site is a bus terminal only and there will be no onsite refueling or maintenance operations; c) The applicant stated they are using wrought iron along all street frontages and they will replace the existing chain link fence with wrought iron fencing on the portion of the property to the east of the detention basin if it is financially feasible.

No other comments have been received at the time of writing this report.

Environmental Considerations: On February 24, 2009, the City Council approved the Greyhound Terminal Relocation Mitigated Negative Declaration (MND) and Mitigation Monitoring Plan by Resolution 2009-115, with the execution of the building lease. Following adoption of the MND, the project has been modified to include minor changes in building alignment and interior traffic circulation, and to extend Sequoia Pacific Boulevard south to intersect with Bannon Street, in lieu of a cul-de-sac access road, which would improve circulation within this area of the River District. Section 15162 of the CEQA Guidelines provides that a subsequent Mitigated Negative Declaration should be prepared if changes are proposed in the project after approval of a Mitigated Negative Declaration that are considered substantial, or if new information of substantial importance to the project becomes known or available. The Environmental Services

Manager determined that a Subsequent MND was required because the proposed modifications to the project were considered to be significant new information. The Subsequent Mitigated Negative Declaration was circulated for a 20-day public review period from April 21, 2010 to May 10, 2010. No comments were received.

Adjacent Properties

The parcel to the west is zoned Office Building (OB-PUD) and is currently used for the City of Sacramento Community Development offices and the Police Department. The parcels to the north (I-5 Furniture Warehouse) and east (Calvada Warehouse and Food Sales) of the subject site are zoned Heavy Industrial (M-2 SPD Central) and there are existing industrial buildings. On the south of the subject site, there is the Union Gospel Mission which is a residential care facility in the Heavy Industrial (M-2 SPD West) zone. The CHP headquarters is located nearby at Richards Blvd and North 7th Street.

Policy Considerations:

General Plan: The subject site is designated Urban Center High on the General Plan Land Use and Urban Form Diagram. The proposal is consistent with the following General Plan policies:

1. *Inter-City Bus Service. The City shall promote the continued operation of private inter-city bus service. (M 3.3.1)* Staff finds that the current proposal allows for the continued operation of the bus terminal in the Central City while the intermodal site is under development.
2. *Taxi Service. The City shall promote the continued operation of taxi service, including the provision of dedicated, on-street loading spaces where appropriate, incremental improvements in gas mileage, and improved access for passengers with disabilities. (M 3.3.2)* Staff finds that proposal allows for taxi queuing on Sequoia Pacific Boulevard with public sidewalks and a direct pedestrian connection to the main entrance of the building.
3. *Screening of Off-street Parking. The City shall reduce the visual prominence of parking within the public realm by requiring most off-street parking to be located behind or within structures or otherwise fully or partially screened from public view. (LU 2.7.8)* Staff finds that the proposal locates the employee parking lot behind the building and only a single row of parking is located along the Richards Boulevard street frontage.
4. *General Plan Density Regulations for Mixed-Density Development Projects. Where a developer proposes a multi-parcel development project with more than one Floor Area Ratio (FAR), the applicable FAR range of the General Plan Land Use Designation shall be applied to the net developable area of the entire project site. Some parcels may be zoned for intensities that exceed the maximum allowed intensity of the project site's Land Use Designation, provided the net density of the project as a whole is within the allowed range. (LU 2.1.4)* Staff

finds that the General Plan designation for Urban Center High requires a minimum Floor Area Ratio (FAR) of 1.75. The FAR for the current bus terminal proposal is .17. Although this is less than the minimum requirements, the proposal is still consistent with the General Plan because: a) the 2030 General Plan and the Discovery Centre PUD anticipate a level of intensification for the surrounding area that will be consistent with the Urban Center High designation when looked at from the view of a multi-parcel development; and b) the bus terminal is an interim use and when the Intermodal facility is completed in the Railyards, this use will move and the site could be redeveloped.

Urban Center High Designation (Page 2-74): This designation provides for a balanced mix of high intensity single-use commercial or high density residential development or horizontal and vertical mixed use. The designation includes major transportation hubs accessible by public transit, major highways and local arterials, and pedestrian travel.

Key components of the urban form in the Urban Center High designation include:

- A mix of low- and mid-rise buildings creating a varied and defined skyline
- Lot coverage generally not exceeding 90 percent
- Buildings sited to positively define the public streetscape and public spaces
- Building facades and entrances directly addressing the street and having a high degree of transparency
- An interconnected street system providing greater distribution of traffic and route flexibility
- Minimal or no curb cuts along primary street facades, with side or rear access to parking and service functions
- Broad sidewalks appointed with appropriate pedestrian amenities / facilities
- Street design integrating safe pedestrian, bicycle, transit and vehicular use and incorporates traffic-calming features and on-street parking
- Consistent planting of street trees providing shade and enhance character and identity

Staff finds that the proposal is consistent with the intent of the components of the urban form listed above.

Zoning Code

A bus/transit terminal is allowed in the Office Building (OB) zone subject to the granting of a Planning Commission Special Permit. A bus and transit terminal is defined in the Zoning Code as an enclosed building which provides transportation services including,

but not limited to: passenger waiting, loading, and unloading. A bus terminal is different from a bus and transit maintenance facility which allows for general or specialized maintenance services for buses and other transit vehicles. The proposal for the bus terminal at 420 Richards Boulevard does not include any maintenance services onsite. The maintenance and storage of buses will be conducted at 1874 South River Road, West Sacramento, CA.

Consistency with the Richards Boulevard Special Planning District (SPD)

The proposal for a bus terminal use is consistent with the Richards Boulevard Special Planning District because one of the goals for the SPD is to provide for the intensification of commercial and office uses within close proximity to the future intermodal transportation terminal and planned light rail extensions. Construction of the "Green Line" light rail extension to the planned station on Richards Blvd, between North 5th and North 7th Streets, at Township 9 is underway. The new Greyhound Terminal would be about two blocks from this new light rail station. Furthermore, the proposal is also consistent with the policies of the Richards Boulevard Area Plan which allows for the development of a diverse mixture of uses within the Richards area which will complement Sacramento's downtown district and facilitate the enhancement and revitalization of the Richards Boulevard area. (Land Use Objective 1) The new terminal is located next to the City Police Department offices and the parking lot and building entrances are visible from Richards Blvd.

Modifications to the Discovery Centre Planned Unit Development (PUD)

The project site is located in the Discovery Centre Planned Unit Development. The designated PUD includes a Schematic Plan and a set of development guidelines. The applicant is requesting to amend the existing PUD guidelines to accommodate the proposed development. The PUD Schematic Plan is consistent with the zoning designations and PUD Guidelines for the project site.

PUD Guidelines

A redlined version of the changes to the PUD Guidelines is attached. (See Attachment 7) The guidelines have been modified to facilitate a bus terminal use in the Discovery Centre PUD. The table below shows the proposed change to Phase IV of the PUD. The PUD Guidelines have been updated to refer to a Planning Director Plan Review which is the standard process for developments within most PUDs. The PUD Guidelines also contained specific Utility conditions that applied to the proposed office building but would not apply to the proposed bus terminal use. As a result, the modifications to the PUD Guidelines also clarify that in the event of a conflict with prior PUD conditions, the Utilities conditions in Attachment 1 shall apply for the development of the bus terminal use.

Phase	Allowed Use	Height	Current Use
Phase I	150,000 square foot office building	44 feet (3 stories)	Office currently used by Police and Community Development
Phase II	224 room hotel	85 feet (8 stories)	Surface parking
Phase III	240,000 square foot office building and a 2 story parking structure	65 feet (6 stories)	Surface parking
<i>Phase IV</i>	<i>400,000 square foot office building</i>	<i>140 feet (8 stories)</i>	<i>Vacant site</i>
<i>Modified Phase IV</i>	<i>10,000 square foot commercial building (bus terminal)</i>	<i>20 feet</i>	<i>Vacant site</i>

Staff supports the PUD Guidelines amendments above because the proposal will allow the temporary bus terminal use until it is relocated to the Sacramento Intermodal Transportation Facility in the Railyards. Furthermore, the amendments do not preclude future office development as originally depicted in the original approval.

Height, Bulk and Setbacks

The following height and setback standards are defined in the Discovery Centre PUD Guidelines and apply to the proposed bus terminal. As shown in the chart below, the project meets all the height and setback requirements.

Standard	Required	Proposed	Deviation?
Height	140 feet maximum	20 feet	no
Setback: Richards Blvd	10 feet	97 feet	no
Setback: East property line	0 feet	42 feet	no
Setback: Sequoia Pacific	5 feet	27 feet	no
Setback: Future Bannon Street	15 feet	200 feet	no

Building Design:

The Design Director approved the project at a public hearing on April 15, 2010. Although the project site is located in a PUD which typically does not require design review, the PUD Guidelines did not address the design of a bus terminal and all city funded building projects are subject to a Review and Comment hearing by the Design Director. A copy of the conditions of approval have been attached. (Attachment 4)

Signage

Signage has not been reviewed as a part of this approval. The Discovery Centre PUD allows one detached monument sign per parcel not exceeding 24 square feet. The monument sign shall not exceed 6 foot in height and shall be located 10 feet from any property line and 10 feet from any driveway in order to provide a clear vision area. Two attached signs are allowed per building not to exceed 20 square feet each. Each attached sign shall be located on different faces of the building. The applicant has indicated they will meet all the signage standards.

Traffic Circulation and Parking

Buses and the public will enter the site by a driveway located off of Richards Boulevard. Vehicles will exit the site by the extension of Sequoia Pacific Boulevard and may exit either north to Richards Boulevard or south to Bannon Street. Buses will not exit the site by utilizing Bannon Street. All bus traffic will use Sequoia Pacific Boulevard and then turn onto Richards Boulevard to access the I-5 freeway.

There is no specific parking requirement for a bus terminal in the Zoning Code; therefore the parking requirement is subject to the approval of the Planning Commission. Currently the Greyhound site downtown does not have any onsite parking for the public.

	Required Parking	Proposed Parking	Difference
Commercial (10,000 sqft)	26 spaces*	39 spaces	no

*The zoning code for new commercial in the Office Building (OB-PUD-SPD) zone in the Richards Boulevard SPD requires one space per 400 square feet for the first 9,600 square feet and then 1 space per 250 square feet for the remainder. Using this ratio, the proposal would require 26 onsite spaces and the proposal includes 20 parking spaces for employees only and 19 public parking spaces for a total of 39 onsite parking spaces. Staff supports this amount of parking because it will provide adequate customer and employee parking without sacrificing a pedestrian-friendly site layout, since parking is limited to only one row adjacent to Richards Boulevard to strengthen the building's presence for pedestrians and the motoring public along the Richards Boulevard Corridor.

Table 4a: Bicycle Parking			
Total parking provided	Required bicycle parking	Provided bicycle parking	Difference
39	1 bicycle facility minimum*	2	no

*Calculation of bicycle parking facilities was not specified in the Discovery Centre PUD for commercial development however, the Richards Boulevard SPD states that one Class I bicycle facility is required for a building under 12,500 square feet. A Class I facility is an enclosed box or compartment with a locking door; or a stationary rack designed to secure the frame and both wheels of the bicycle where the bicyclist supplies only a padlock, and which is located in an area completely enclosed and covered and where entry is secured by a locking door.

Alternative Modes

The first phase of the future Downtown-Natomas-Airport light rail transit is under construction along North 7th Street and then will turn west on Richards Boulevard and terminate at the Township 9 site. The subsequent phase will continue the light rail down Richards Boulevard to turn right on Sequoia Pacific Boulevard and cross the American River to connect to Truxel Boulevard and eventually will connect to the Sacramento International Airport.

The future light rail station at the Township 9 project (north side of Richards Boulevard between North 5th and North 7th Street) is scheduled to be open in 2011 and this light rail station will be within 1,320 feet (1/4 mile) of the subject site. A future light rail station is also planned on Sequoia Pacific Boulevard to the north of Richards Boulevard which is within 660 feet (1/8 mile) of the subject site.

According to the Regional Transit website, there are also three bus routes in the Richards Boulevard Area: Route 11, 15, and 33. There is an existing bus stop on Richards Boulevard directly in front of the proposed Greyhound site.

Tree Shading Requirements

The parking lot tree shading ordinance requires that all new parking lots include tree plantings designed to result in 50 percent shading of parking lot surface areas within 15 years. The shading requirements apply to all new impervious surfacing on which a vehicle can drive including parking stalls, all drives within the property line regardless of length, and all maneuvering areas regardless of depth. However, there are exceptions to the requirement which include truck maneuvering areas unconnected to and exclusive of any vehicle parking and areas under covered canopies. After a discussion with Urban Forest Services division, it was confirmed that the bus maneuvering area and bus loading areas would not be required to meet the 50% tree shading requirement. The remainder of the site shall meet the 50% tree shading requirement which includes the public and employee parking lot areas.

Fencing

The Discovery Centre PUD states that fencing for the security of the site may include open ornamental steel or iron to a maximum of 6 feet in height and unobtrusive in color. Solid perimeter walls are discouraged. Staff finds the proposed fencing is consistent with the Zoning Code and PUD guidelines and encourages the applicant to consider eliminating the existing chain link fencing on interior property lines to the east of the detention basin and replacing it with wrought iron fencing. (See Advisory Condition #2)

Trash Enclosure

The Discovery Centre PUD requires that the trash enclosure be concealed by a screening wall and located in an inconspicuous area of the site. Furthermore, it requires the facility to relate appropriately to the building and not detract from the design theme.

The Design Director has conditioned the project to incorporate design elements of the building on the trash enclosure. The trash enclosure is also required by the Zoning Code to have a six foot high enclosure. Staff supports the location on the site because it is close to the kitchen and food service area and is also easily accessible for trash collection pickup.

Removal of Existing Billboard

There is an existing billboard on the eastern portion of the property oriented to Richards Boulevard. This billboard will be removed before construction of the bus terminal.

Conclusion:

Staff recommends approval of the project since the proposal: a) is consistent with the 2030 General Plan and the Office Building (OB) zoning; b) allows for the continued operation of a private bus service in the Central City; c) provides an interim use on a vacant site until the bus terminal is ultimately moved to the completed intermodal station; d) locates the bus terminal use in an area near law enforcement agencies for enhanced security; e) allows a bus terminal on a site well served by existing and future transit including Interstate 5 and Highway 160, three bus routes, and two future light rail stations located within ¼ mile.

Respectfully submitted by: 
EVAN COMPTON
Associate Planner

Approved by:


STACIA COSGROVE
Senior Planner

Recommendation Approved:


GREG BITTER, AICP
Principal Planner

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Attachment 1
Proposed Findings of Fact and Conditions of Approval
Greyhound Bus Terminal (P10-020)
420 Richards Boulevard

1. Findings of Fact

A. and B. Environmental Determination: Subsequent Mitigated Negative Declaration and Mitigation Monitoring Plan: The Mitigated Negative Declaration for the original project was approved by the City Council on February 24, 2009. The Project initial study identified potentially significant effects of the Project. Revisions to the Project made by or agreed to by the Project applicant before the proposed mitigated negative declaration and initial study were released for public review were determined by City's Environmental Planning Services to avoid or reduce the potentially significant effects to a less than significant level, and, therefore, there was no substantial evidence that the Project as revised and conditioned may have a significant effect on the environment. A Mitigated Negative Declaration (MND) for the original Project was then completed, noticed and circulated in accordance with the requirements of the California Environmental Quality Act (CEQA), the State CEQA Guidelines and the Sacramento Local Environmental Procedures.

Following approval of the Mitigated Negative Declaration, the Project was modified to include minor changes in the location of the terminal building on the site and internal traffic circulation, and to extend Sequoia Pacific Boulevard south to intersect with Bannon Street. These changes are substantial, and required the preparation of a Subsequent Mitigated Negative Declaration. The Subsequent Mitigated Negative Declaration was circulated for public comment as follows:

a. On April 21, 2010 a Notice of Intent to Adopt the Subsequent MND (NOI) dated April 21, 2010 was circulated for public comments for 20 days. The NOI was sent to those public agencies that have jurisdiction by law with respect to the proposed project and to other interested parties and agencies, including property owners within 500 feet of the boundaries of the proposed project. The comments of such persons and agencies were sought.

b. On April 21, 2010 the project site was posted with the NOI, the NOI was published in the Daily Recorder, a newspaper of general circulation, and the NOI was posted in the office of the Sacramento County Clerk.

2. The Planning Commission has reviewed and considered the information contained in the Subsequent MND, including the initial study, MND, the revisions and conditions incorporated into the Project, and the comments received during the public review process and the hearing on the Project. The Planning Commission has determined that the Subsequent MND constitutes an adequate, accurate, objective and complete review of the environmental effects of the proposed project.

6. Based on its review of the Subsequent MND and on the basis of the whole record, the Planning Commission finds that the Subsequent MND reflects the Planning Commission's independent judgment and analysis and that there is no substantial evidence that the Project, as revised with the incorporation of the mitigation measures, will have a significant effect on the environment.
7. The Planning Commission adopts the Subsequent MND for the Project.
8. Pursuant to CEQA section 21081.6 and CEQA Guidelines section 15074, and in support of its approval of the Project, the Planning Commission adopts the Mitigation Monitoring Program to require all reasonably feasible mitigation measures be implemented by means of Project conditions, agreements, or other measures, as set forth in the Mitigation Monitoring Program.
9. Upon approval of the Project, the City's Environmental Planning Services shall file or cause to be filed a Notice of Determination with the Sacramento County Clerk and, if the project requires a discretionary approval from any state agency, with the State Office of Planning and Research, pursuant to section 21152(a) of the Public Resources Code and the State EIR Guidelines adopted pursuant thereto.
10. The documents and other materials that constitute the record of proceedings upon which the Planning Commission has based its decision are located in the City of Sacramento Community Development Department, Environmental Planning Services, 300 Richards Boulevard, Sacramento, CA 95811-0218. The custodian of these documents and other materials is the Community Development Department, Environmental Planning Services.

C. PUD Guidelines Amendment: The PUD Guidelines Amendment to allow a bus terminal use in the Discovery Centre PUD is **approved** based on the following Findings of Fact:

1. The PUD amendment conforms to the 2030 General Plan and the Richards Boulevard Area Plan; and
2. The PUD amendments meet the purposes and criteria stated in the City Zoning Ordinance in that the PUD encourages mixed use that is healthy and of long-lasting benefit to the community and the City of Sacramento; and
3. The PUD Amendments will not be injurious to the public welfare, nor to other property in the vicinity of the development and will be in harmony with the general purposes and intent of the Zoning Ordinance in that the PUD establishes minimum development standards to facilitate new development which will revitalize the Richards Boulevard area.

D. Special Permit: The Special Permit to allow a bus terminal in the Office Building (OB-PUD SPD) zone and located in the Discovery Centre PUD and Richards Boulevard SPD is **approved** based on the following Findings of Fact:

1. The project is based upon sound principles of land use in that a bus terminal is compatible with the surrounding properties in the neighborhood, since the area is currently in transition from industrial to mixed use and within the Richards Boulevard area there is a strong law enforcement presence given the current locations of the City of Sacramento Police Department and California Highway Patrol Campus to provide enhanced surveillance and discourage criminal activity on the site; and
2. The proposed use will not be detrimental to the public health, safety and welfare, nor result in a public nuisance in that the site will be well served by existing and future transit including Interstate 5 and Highway 160, three bus routes, and two future light rail stations located within ¼ mile; and
3. The project is consistent with the 2030 General Plan and Richards Boulevard Area Plan (RBAP) policies related to encouraging the continued operation of a private bus service within the city limits and allowing for a diverse mix of uses to further the revitalization of the Richards Boulevard area.

2. Conditions of Approval

D. Special Permit: The Special Permit to allow a bus terminal in the Office Building (OB-PUD SPD) zone and located in the Discovery Centre PUD and Richards Boulevard SPD is **approved** subject to the following conditions of approval:

PLANNING:

- D1. The project shall conform to the attached plans. Any changes to the project shall require additional planning review and approval.
- D2. The applicant shall obtain all required building and/or encroachment permits prior to commencing construction.
- D3. A sign permit shall be obtained prior to construction or installation of any attached or detached sign.
- D4. The project shall meet the conditions of the Design Director (DR10-047).
- D5. A sign indicating a 24-hour emergency phone number and contact person shall be kept current and posted on the building.

- D6. A minimum of 2 bicycle facilities shall be provided. A minimum of one bicycle facility shall be Class I.
- D7. The applicant shall comply with the requirements of the Mitigation Monitoring Plan developed by and kept on file in the Community Development Department (P10-020.)
- D8. All parking spaces and maneuvering area (with the exception of the bus maneuvering and loading areas as noted in this staff report) shall meet the 50% tree shading requirements.
- D9. The designated spaces for employee parking shall be noted with striping or signage.
- D10. The project shall be LEED Certified Silver or equivalent unless the City Council approves a different standard.

UTILITIES

- D11. Any new domestic water services shall be metered. Only one domestic water service is allowed per parcel or lot. Excess services shall be abandoned to the satisfaction of the DOU. All water connections shall comply with the City of Sacramento's Cross Connection Control Policy. The existing water tap and backflow preventer serving 300 Richards Blvd. shall be relocated or modified such that water services do not cross property lines.
- D12. Provide a separate street tap for a metered irrigation service.
- D13. Per City Code, the point of service for water, sewer and storm drain service is located at the back of curb for separated sidewalks and at the back of sidewalk for attached sidewalks. The onsite water, sewer and storm drain systems shall be private systems maintained by the property owner with either a cleanout, manhole or backflow preventer installed at the point of service.
- D14. No trees or permanent structures (with the exception of a small corner of the proposed building as approved by the DOU) shall be located within a 20' wide area centered along the entire length of the existing 15" City Sewer Main that runs through the property.
- D15. Construct a 12" water main extension from the intersection of Richards and Sequoia Blvd., south in the proposed Sequoia Blvd. to the satisfaction of the DOU.
- D16. A drainage inlet or inlets (as needed) connected to the city drainage system shall be installed on the southeast corner of the intersection of Richards Blvd. and Sequoia Blvd. to the satisfaction of the DOU.

- D17. If the project requires a drainage connection to the city system at some point along the southern portion of the property a drainage main extension from Bannon Street shall be required to the satisfaction of the DOU.
- D18. The building pad elevation shall be approved by the DOU and shall be a minimum of 1.5 feet above the local controlling overland release elevation or a minimum of 1.2 feet above the highest adjoining back of sidewalk elevation, whichever is higher, or as approved by the Department of Utilities.
- D19. This project is served by the Combined Sewer System (CSS). Therefore, the developer/property owner will be required to pay the Combined Sewer System Development Fee prior to the issuance of building permit. The Combined Sewer System fee at time of building permit is estimated to be \$792.89 plus any increases to the fee due to inflation. The fee will be used for improvements to the CSS.
- D20. The applicant shall comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinance. This ordinance will require the applicant to prepare erosion and sediment control plans for both during and after construction of the proposed project, prepare preliminary and final grading plans, and prepare plans to control urban runoff pollution from the project site during construction.
- D21. Post construction, stormwater quality control measures shall be incorporated into the development to minimize the increase of urban runoff pollution caused by development of the area. Since the project is not served by an existing regional water quality control facility, both source control and on-site treatment control measures (e.g., stormwater planters, detention basin, infiltration basin and/or trench, media filters (Austin Sand Filter), multi-functional drainage corridors, vegetated filter strips and/or swales, and proprietary devices) are required. A maintenance agreement is required for all on-site treatment control measures. Contact DOU for a list of accepted proprietary devices if considered for treatment control. Specific source controls are required for (1) vehicle and equipment fueling areas, (2) loading/unloading areas, (3) outdoor storage areas, (4) outdoor work areas, (5) vehicle/equipment wash, repair and maintenance areas, (6) waste management areas and (7) Storm drain inlet (markings). Improvement plans must include the source controls and on-site treatment control measures selected for the site. Refer to the latest edition of the "Stormwater Quality Design Manual for the Sacramento and South Placer Regions (May 2007)" for appropriate source control measures. Runoff reduction measures (e.g. porous pavement) are optional control measures. Refer to the Runoff Reduction Credit Worksheet in the above Manual for porous pavement design.
- D22. This project is greater than 1 acre in size; therefore, the project is required to comply with the State "NPDES General Permit for Stormwater Discharges Associated with Construction Activity" (State Permit). To comply with the State Permit, the applicant will need to file a Notice of Intent (NOI) with the State Water

Resources Control Board (SWRCB) and prepare a Stormwater Pollution Prevention Plan (SWPPP) prior to construction. A copy of the State Permit and NOI may be obtained from www.swrcb.ca.gov/stormstr/construction.html. The SWPPP will be reviewed by the DOU prior to issuing a grading permit. The following items shall be included in the SWPPP: (1) vicinity map, (2) site map, (3) list of potential pollutant sources, (4) type and location of erosion and sediment BMP's, (5) name and phone number of person responsible for SWPPP and (6) certification by property owner or authorized representative.

FIRE

- D23. All turning radii for fire access shall be designed as 35' inside and 55' outside.
- D24. Roads used for Fire Department access shall have an unobstructed width of not less than 20' and unobstructed vertical clearance of 13'6" or more.
- D25. Fire Apparatus access roads shall be designed and maintained to support the imposed loads of fire apparatus and shall be surfaced so as to provide all-weather driving capabilities. CFC 503.2.3
- D26. Provide the required fire hydrants in accordance with CFC 508 and Appendix C, Section C105.
- D27. Timing and Installation. When fire protection, including fire apparatus access roads and water supplies for fire protection, is required to be installed, such protection shall be installed and made serviceable prior to and during the time of construction.
- D28. Provide a water flow test. (Make arrangements at the Permit Center walk-in counter: 300 Richards Blvd, Sacramento, CA 95814). CFC 508.4
- D29. Provide appropriate Knox access for site
- D30. Roads used for Fire Department access that are less than 28 feet in width shall be marked "No Parking Fire Lane" on both sides; roads less than 36 feet in width shall be marked on one side.
- D31. An automatic fire sprinkler system shall be installed in any portion of a building when the floor area of the building exceeds 3,599 square feet.
- D32. Locate and identify Fire Department Connections (FDCs) on address side of building no further than 50 feet and no closer than 15 feet from a fire hydrant.
- D33. An approved fire control room shall be provided for all buildings protected by an automatic fire extinguishing system. Fire control rooms shall be located within the building at a location approved by the Chief, and shall be provided with a means to access the room directly from the exterior. Durable signage shall be

provided on the exterior side of the access door to identify the fire control room.
CFC 903.8

- D34. Vehicle gates shall be provided with a minimum 20' clear access. Gates shall be AC powered and provided with Key override switch (Knox) and Radio operated controller (Click2Enter).

DEVELOPMENT ENGINEERING:

- D35. Construct standard improvements as noted in these conditions pursuant to section 16.48.110 of the City Code. Improvements shall be designed and constructed to City standards in place at the time that the Building Permit is issued. All improvements shall be designed and constructed to the satisfaction of the Department of Transportation. Any public improvement not specifically noted in these conditions shall be designed and constructed to City Standards. This shall include street lighting and the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk fronting the property along Richards Boulevard per City standards and to the satisfaction of the Department of Transportation;
- D36. All new driveways shall be designed and constructed to City Standards and to the satisfaction of the Department of Transportation. All site driveways shall be sized enough to accommodate a turning radius of the largest Greyhound bus in use at this terminal;
- D37. The site plan shall conform to A.D.A. requirements in all respects. This shall include the replacement of any curb ramp that does not meet current A.D.A. standards at the south-east corner of the intersection of Sequoia Boulevard and Richards Boulevard;
- D38. The site plan shall conform to the parking requirements set forth in chapter 17 of City Code (Zoning Ordinance);
- D39. The design of walls fences and signage near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height at maturity. The area of exclusion shall be determined by the Department of Transportation;
- D40. The applicant shall dedicate and construct a bus turnout along Richards Boulevard as shown on the site plan (dated 3-9-2010) to the satisfaction of the Department of Transportation;
- D41. **Sequoia Pacific Boulevard / Bannon Street intersection.** The applicants shall dedicate and construct the intersection of Sequoia Pacific Boulevard and Bannon

Street as depicted on the site plan (dated 3-9-2010). The applicant shall not be responsible for constructing the eastern leg of Bannon Street (shown on the site plan). This shall include the construction of the required roadway section, street lights, round corners and curb ramps to the satisfaction of the Department of Transportation.

- D42. **Richards Boulevard / Sequoia Pacific Boulevard intersection.** The applicant shall dedicate and construct the expanded intersection as shown on the site plan (dated 3-9-2010). This shall include any required modifications or relocations of any signal equipment (if needed), round corner reconstruction, etc. to the satisfaction of the Department of Transportation.

URBAN FOREST

- D43. The applicant shall coordinate with the Urban Forest Services Department to finalize the species selection of trees located within the public right-of-way.
- D44. The applicant shall ensure all onsite trees are consistent with the "Parking Lot Shading Design and Maintenance Guidelines." These guidelines list City approved tree species in which the Landscape Architect (Owner) can choose from for on-site use. The applicant has proposed tree species in the original landscape exhibit. The applicant shall provide further clarification of their intent while working with the Urban Forest Services Department.
- D45. All trees shall be planted in a gradual mound 2 inches to 3 inches higher than the surrounding grade and mulched w/ wood chips (playground fiber or coarser) to a depth of approximately 3 inches.
- D46. There shall be no understory planting within 4 feet of any tree trunk (5 feet for large canopy species trees).

ENGINEERING SERVICES – ELECTRICAL SECTION:

- D47. This project shall require street lighting. There is an existing street lighting system around this project area. Improvements of right-of-way may require modification to the existing system. Electrical equipment shall be protected and remain functional during construction.

ADVISORY NOTES ONLY:

Planning

- ADV1. If the site is transferred to another private user in the future who reuses the building and converts the bus loading and maneuvering area to vehicle parking, the exemption shall no longer apply and the project shall be required to meet the 50% tree shading requirements.

- ADV2. The applicant shall explore the feasibility of fencing the entire site with wrought iron fencing and if the budget allows for a fencing upgrade, the existing chain link fence along the interior property line shall be removed and replaced with wrought iron fencing.

Development Engineering

- ADV3. Richards Boulevard / Sequoia Pacific Boulevard intersection. (Future 2035 scenario) The applicant shall reserve sufficient right of way and reconstruct the expanded intersection to add an additional left turn pocket as shown on the 2035 site plan. This shall include the reconstruction of any round corners, curb ramps, relocation (if needed) of any signal equipment, and any signal timing changes to the satisfaction of the Department of Transportation.

Urban Forest Services

- ADV4. Trees should be ordered well in advance of anticipated planting date to ensure species availability.

Utilities

- ADV5. The proposed project is located in the Flood zone designated as a shaded X zone on the Federal Emergency Management Agency (FEMA) Federal Insurance Rate Maps (FIRMs), dated December 8th, 2008. Within the X zone, there are no requirements to elevate or flood proof.
- ADV6. Many projects within the City of Sacramento require on-site booster pumps for fire suppression and domestic water systems. Prior to design of the subject project, the DOU suggests that the applicant request a water supply test to determine what pressure and flows the surrounding public water distribution system can provide to the site. This information can then be used to assist the engineers in the design of the on-site fire suppression.
- ADV7. A looped 12" water main in Sequoia Blvd. is the preferred situation from both a water supply and water quality point of view. It is recommended that the main extension in Sequoia Blvd. be connected to the existing city water system both in Richards Blvd. as well as Bannon Street. If a looped system is not provided, it would be beneficial in respect to water quality to have the irrigation service tap for the project located at the end of the dead end main in Sequoia Boulevard.
- ADV8. The project must meet the Recycling and Solid Waste Disposal Regulations outlined in City Code Chapter 17.72 (http://www.qcode.us/codes/sacramento/view.php?topic=17-iii-17_72&frames=off).

Parks

- ADV9. As per City Code, the applicant will be responsible to meet his/her obligations regarding Title 18, 18.44 Park Development Impact Fee, due at the time of issuance of building permit. The Park Development Impact Fee due for this project is estimated at \$1,697. This is based on 9,980 sq. ft at the Specified Infill Retail/ Commercial Services/Other rate of \$ 0.17 per sq. ft. Any change in these factors will change the amount of the PIF due. The fee is calculated using factors at the time that the project is submitted for building permit.

Building

- ADV10. Provide accessible path of travel from bus stop to the entry of bus terminal per CBC section 1127B.1
- ADV11. Provide international symbol of accessibility sign along or leading to an accessible route of travel to entrance of facility per CBC section 1127B.3.
- ADV12. Provide accessible path of travel from employee parking stall to the bus terminal, and also provide detectable warning sign when crossing a vehicular way per CBC section 1133B.8.5
- ADV13. Provide building construction type & building occupancy groups, & specify separated occupancies or non-separated occupancies per CBC section 508 (mixed use & occupancy)
- ADV14. Provide occupant load in dining area, lobby area, waiting area, & ticket queuing area.
- ADV15. If occupancy separation is required, comply with CBC Table 508.3.
- ADV16. Provide 18" minimum on the strike side of exit access door in employee break room, & employee hallway area.
- ADV17. Provide panic hardware at lobby exit doors when occupant load of 50 or more.
- ADV18. Provide mixed occupancies allowable area computations per CBC section 508.3.3.2



MOCAYERO
INCORPORATED
ASSOCIATES
11011 STREET
SUSANVILLE
CA 95911
530-401-0200
530-401-0200
MOCAYERO

DATE: 05/27/2010 10:58 AM

PROJECT TITLE

Greyhound Terminal

420 W. HANLEY BLVD
SUSANVILLE, CA
95958

SHEET NUMBER

SHE

PROJECT NUMBER

7796 Design/Drawings

DATE: 05/27/2010

PROJECT NAME

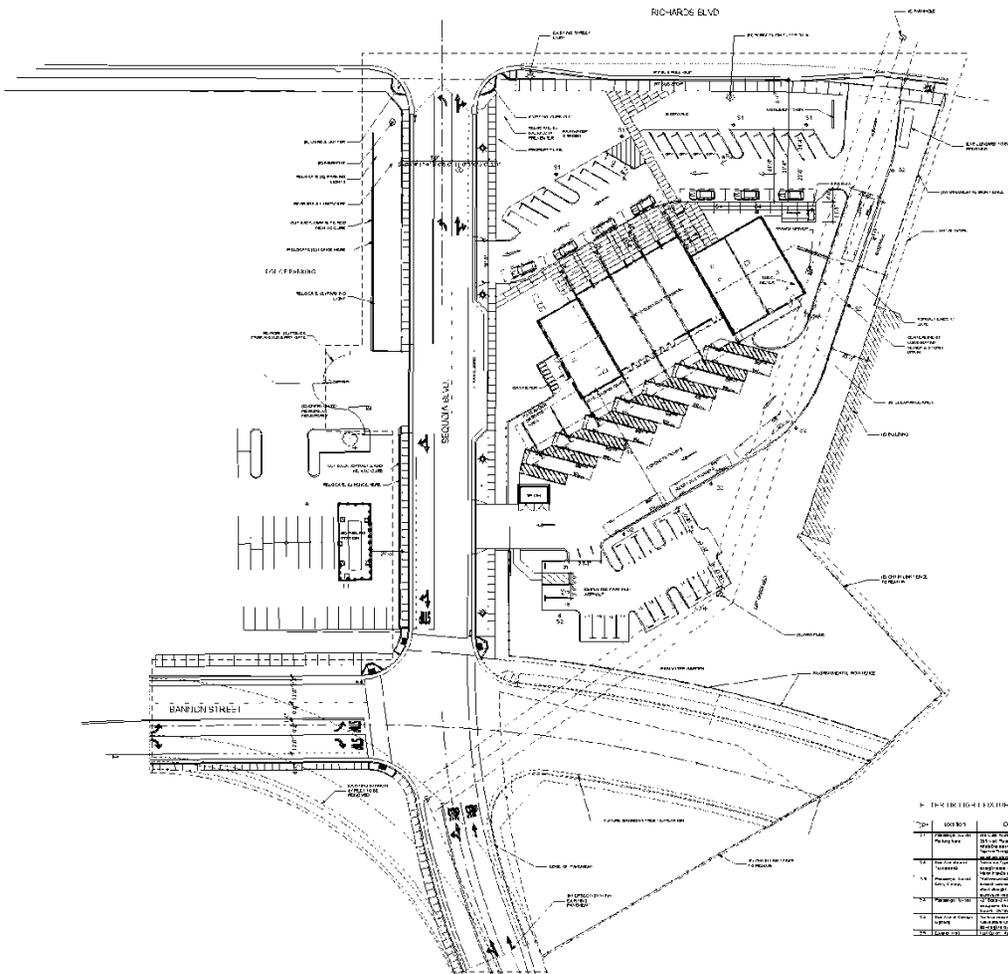
PROJECT ADDRESS

SHEET NUMBER

A1.1

PROJECT NUMBER

PROJECT ADDRESS



REVISIONS

NO.	DATE	DESCRIPTION	BY	CHKD BY
1	05/27/10	ISSUED FOR PERMITS	MM	MM
2	05/27/10	ISSUED FOR PERMITS	MM	MM
3	05/27/10	ISSUED FOR PERMITS	MM	MM
4	05/27/10	ISSUED FOR PERMITS	MM	MM
5	05/27/10	ISSUED FOR PERMITS	MM	MM
6	05/27/10	ISSUED FOR PERMITS	MM	MM
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8	05/27/10	ISSUED FOR PERMITS	MM	MM
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10	05/27/10	ISSUED FOR PERMITS	MM	MM
11	05/27/10	ISSUED FOR PERMITS	MM	MM
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14	05/27/10	ISSUED FOR PERMITS	MM	MM
15	05/27/10	ISSUED FOR PERMITS	MM	MM
16	05/27/10	ISSUED FOR PERMITS	MM	MM
17	05/27/10	ISSUED FOR PERMITS	MM	MM
18	05/27/10	ISSUED FOR PERMITS	MM	MM
19	05/27/10	ISSUED FOR PERMITS	MM	MM
20	05/27/10	ISSUED FOR PERMITS	MM	MM

SCALE: 1" = 30'



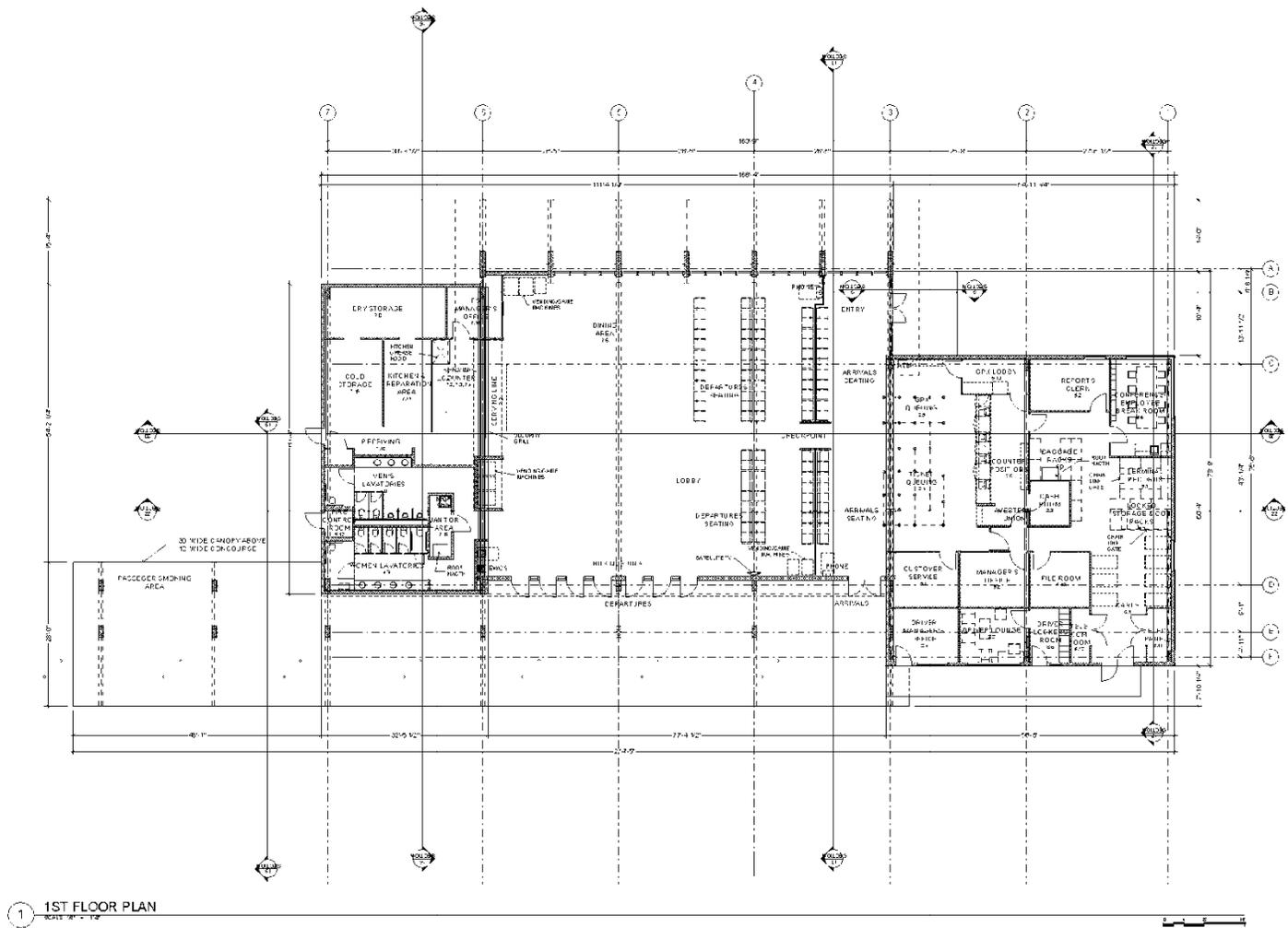
4 1/2" x 11 1/2" (Imag. Area) Edge

PROJECT TITLE
Greyhound Terminal
400 PINEHURST BLVD.
FAYETTEVILLE, NC
28404

SHEET NUMBER
1st FLOOR PLAN

DATE: 05-27-10
75% Design Drawings

DATE: 05-27-10



1 1ST FLOOR PLAN
SCALE: 1/8" = 1'-0"

A2.1

DATE: 05-27-10

Exhibit C: Building Elevations and Trash Enclosure Details

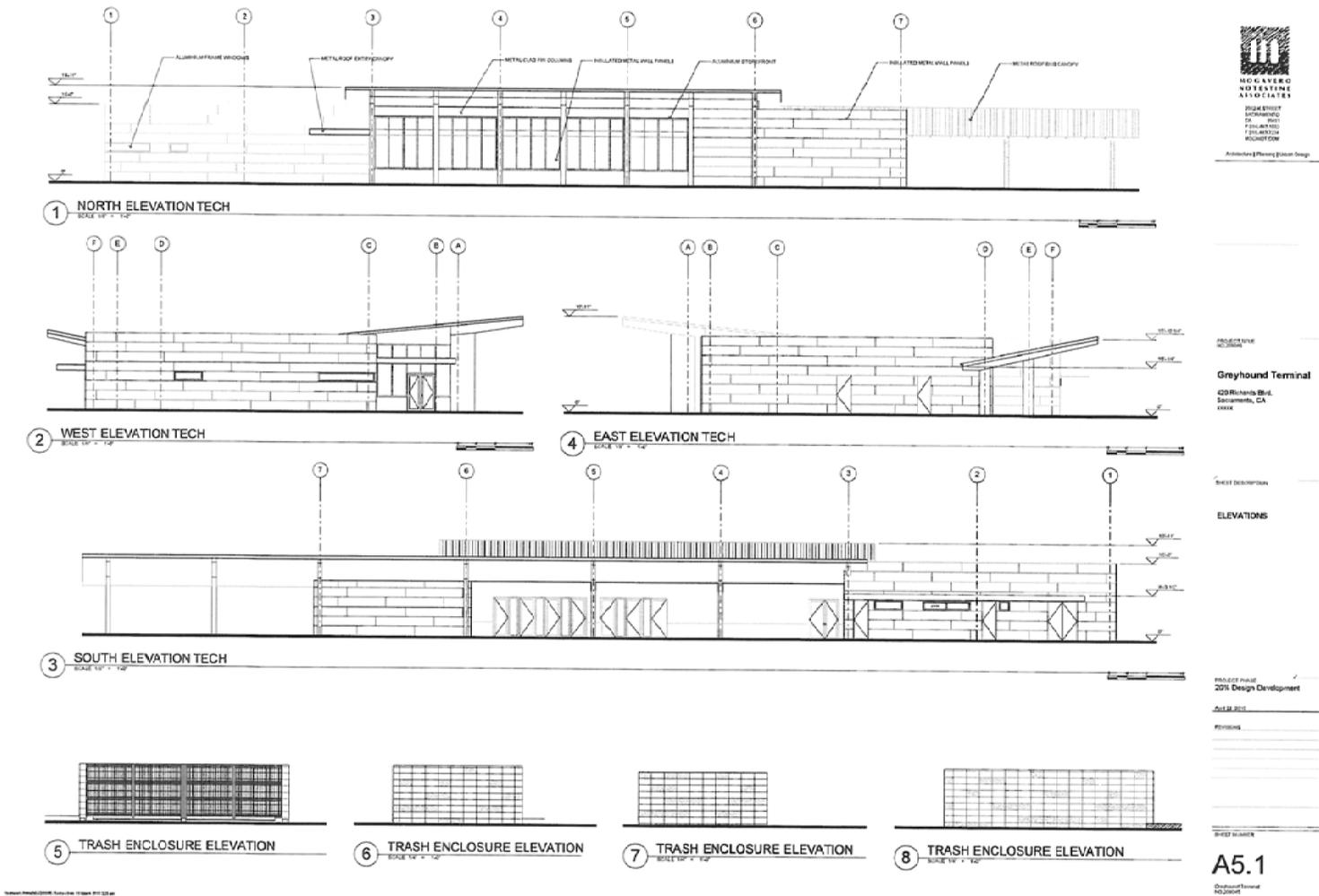


Exhibit E: North and South Perspectives



ENTRY VIEW FROM RICHARDS BLVD.



VIEW OF BUS ENTRY

GREYHOUND TERMINAL
420 RICHARDS BOULEVARD SACRAMENTO, CA

EXTERIOR VIEWS
2010.03.08



Attachment 2: Mitigated Negative Declaration

A copy of the Mitigated Negative Declaration and Subsequent Mitigated Negative Declaration may be found at the following link:

<http://www.cityofsacramento.org/dsd/planning/environmental-review/eirs/>

Attachment 3: Mitigated Monitoring Plan (MMP)

REVISED GREYHOUND BUS TERMINAL RELOCATION PROJECT
(P10-020)
MITIGATION MONITORING PLAN

Revised Greyhound Bus Terminal Relocation Project (P10-20)
Mitigation Monitoring Plan
Mitigation Agreement

PROJECT NAME / FILE NUMBER: Revised Greyhound Bus Terminal Relocation Project (P10-020)

OWNER/DEVELOPER/APPLICANT: Kirk Thompson
Department of General Services
City of Sacramento
5730 24th Street, Building 4
Sacramento, California 95822
(916) 808-8431

I, Reina J. Schwartz (owner/developer/applicant), agree to amend the Revised Greyhound Bus Terminal Relocation project application to incorporate the attached mitigation measures as identified in the Initial Study for the project. I understand that by agreeing to these mitigation measures, all identified potentially significant environmental impacts should be reduced to below a level of significance, thereby enabling the Environmental Coordinator to prepare a Subsequent Mitigated Negative Declaration of environmental impact for the above referenced project.

I also understand that the City of Sacramento will adopt a Mitigation Monitoring Plan (Plan) for this project. This Plan will be prepared by the Community Development Department, pursuant to the California Environmental Quality Act Guidelines Section 21081.6 and pursuant to Article III of the City's Local Administrative Procedures for the Preparation of Environmental Documents.

I acknowledge that the Revised Greyhound Bus Terminal Relocation project, would be subject to this Plan at the time the Plan is adopted. This Plan will establish responsibilities for the monitoring of my project by various City Departments and by other public agencies under the terms of the agreed upon mitigation measures. I understand that the mitigation measures adopted for my project may require the expenditure of owner/developer funds where necessary to comply with the provisions of said mitigation measures.

Reina J. Schwartz
Signature (Owner/Developer/Applicant)

Director, Dept. of General Services
Title

4/21/10
Date

**REVISED GREYHOUND BUS TERMINAL PROJECT
(P10-020)
MITIGATION MONITORING PLAN**

This Mitigation Monitoring Plan (MMP) has been required by and prepared for the City of Sacramento Community Development Department, Environmental Planning Services, 300 Richards Boulevard, Sacramento, CA 95811, pursuant to Public Resources Code of California, Statute, 21081.6.

SECTION I – PROJECT IDENTIFICATION

Project Name: Revised Greyhound Bus Project (P10-020)

Owner/Developer/Applicant: Kirk Thompson
Department of General Services
City of Sacramento
5730 24th Street, Building 4
Sacramento, California 95822
(916) 808-8431

Redevelopment Project Manager: Rachel Hazlewood
City of Sacramento
Economic Development Department
Downtown Development Group
New City Hall, 915 I Street, 3rd Floor
Sacramento, California 95814
(916) 808-8645

City of Sacramento Contact: Dana Allen, Associate Planner
Environmental Planning Services
Community Development Department
300 Richards Boulevard
Sacramento, CA 95811
Phone: (916) 808-2762

Project Location: The project site is located at 420 Richards Boulevard. It is east of Interstate 5 (I-5), west of North 7th Street, on the south side of Richards Boulevard and north of Bannon Street (APNs: 001-0210-047).

Project Components: The project would develop an approximately 10,000 square foot building in the Discovery Centre Planned Unit Development on approximately 1.74 acres. As part of the revised project, the City would modify the design and location of the terminal, extend Sequoia Boulevard southerly to Bannon Street and make minor changes in internal circulation of the proposed terminal. The relocation is an interim facility until the permanent Greyhound Terminal is constructed within the Railyards Redevelopment Plan Area.

**GREYHOUND BUS TERMINAL PROJECT
MITIGATION MONITORING PLAN**

The project has been revised since the adoption of the Mitigated Negative Declaration. As part of the revised project, the City would modify the design and location of the terminal, extend Sequoia Boulevard southerly to Bannon Street and make minor changes in internal traffic circulation on the project site.

Requested Entitlements: Specific entitlements include: a) Special Permit to locate a bus terminal in the OB-PUD zone; and b) Planned Unit Development Guidelines Amendment to allow a bus terminal in the Discovery Centre PUD.

**GREYHOUND BUS TERMINAL PROJECT
MITIGATION MONITORING PLAN**

SECTION II – GENERAL INFORMATION

The Mitigation Monitoring Plan (MMP) includes mitigation for Transportation, Seismicity, Soils, and Geology; Air Quality; and Cultural Resources. The intent of the Plan is to prescribe and enforce a means for properly and successfully implementing the mitigation measures as identified within the Mitigated Negative Declaration/Initial Study and Subsequent Mitigated Negative Declaration for this project. Unless otherwise noted, the cost of implementing the mitigation measures as prescribed by this Plan shall be funded by the owner/developer/applicant identified above; in this case, the City. This MMP is designed to aid the City in its implementation and monitoring of mitigation measures adopted for the proposed project.

The mitigation measures have been taken verbatim from the Mitigated Negative Declaration/Initial Study and Subsequent Mitigated Negative Declaration and are assigned the same number they have in the document. The MMP describes the actions that must take place to implement each mitigation measure, the timing of those actions, and the entities responsible for implementing and monitoring the actions. The City will be responsible for fully understanding and effectively implementing the mitigation measures contained within the MMP. The City, along with other applicable local, state, or federal agencies, will be responsible for ensuring compliance.

REVISED GREYHOUND BUS TERMINAL RELOCATION PROJECT
(P10-020)
MITIGATION MONITORING PLAN

Impact	Mitigation Measure	Action	Implementing Party	Timing	Monitoring Party
3. Seismicity, Soils, and Geology					
Seismic MM-1: Seismic Impacts	Seismic MM-1: Prior to construction, site-specific geotechnical evaluations shall be performed by an appropriately licensed professional engineer qualified to assess seismic conditions including probability associated with liquefaction, settlement, and lateral spreading using a maximum probable and credible earthquake. The evaluation shall identify specific geotechnical recommendations for development foundation design to mitigate for seismically induced hazards, as well as recommendations for adequate building design including excavation and fill requirements for any identified soil constraints.	Conduct preconstruction site-specific geotechnical evaluation by licensed professional engineer	Project Proponent Department	Prior to approval of construction documents	City Development Services Department - Building Division
Seismic MM-2: Erosion	Seismic MM-2: A comprehensive erosion control plan shall be prepared by a registered civil engineer or a registered professional hydrologist prior to submittal of the final map to protect water resources from impacts due to siltation and sedimentation generated by project construction in the Planning Area. The plan shall be prepared in coordination with the Central Valley Regional Water Quality Control Board and the City of Sacramento to assure	Comprehensive erosion control plan shall be prepared by a registered civil engineer or a registered professional hydrologist	Project Proponent Economic Development Department Central Valley Regional Water Quality Control Board	Prior to approval of construction documents	City Development Services Department - Building Division

GREYHOUND BUS TERMINAL PROJECT
MITIGATION MONITORING PLAN

Impact	Mitigation Measure	Action	Implementing Party	Timing	Monitoring Party
	<p>compliance with applicable NPDES permit requirements for construction activities. The plan shall include a combination of the following Best Management Practices (BMPs) or equally effective measures, or any other measures required by local codes and ordinances.</p> <ul style="list-style-type: none"> a. If feasible project construction periods should be limited to the dry months of the year (May through October). b. If project construction does occur during the rainy season (November through April), sediment traps barriers covers or other methods shall be used to reduce erosion. c. Slopes both cut and fill shall not be steeper than those recommended by the detailed geotechnical report for the Planning Area see Mitigation Measure 4.11- 1(a). d. Sediment basins sediment traps or similar sediment control Best Management Practices (BMPs) shall be installed before extensive ground alteration operations begin. e. Temporary mulching seeding or other suitable stabilization 				

REVISED GREYHOUND BUS TERMINAL RELOCATION PROJECT
(P10-020)
MITIGATION MONITORING PLAN

Impact	Mitigation Measure	Action	Implementing Party	Timing	Monitoring Party
	<p>measures shall be used to protect exposed areas during construction activities.</p> <p>f. Excavated materials shall not be deposited or stored where the material could be washed away by storm water runoff.</p>				
Seismic MM-3: Erosion	<p>Seismic MM-3: Use the following best management practices (BMPs) or equally effective measures:</p> <p>a. Develop and implement a program to safely store and handle cement materials, paints and solvents, fuels and lubricating oils, pesticides, and herbicides, and other hazardous materials.</p> <p>b. Develop and implement a hazardous materials spill prevention, control, and cleanup program.</p> <p>c. Or develop and implement other measures as determined by the Utilities Department.</p>	Incorporate BMPs into contractor specifications	<p>Project Proponent</p> <p>Economic Development Department</p> <p>Contractor</p>	Before and during, project construction	City Development Services Department - Building Division
Seismic MM-4: Erosion	<p>Seismic MM-4: A comprehensive runoff control plan shall be prepared by a registered civil engineer or registered professional hydrologist to protect water resources from impacts due to urban and landscape runoff generated by the project. The plan</p>	Comprehensive runoff control plan shall be prepared by a registered civil engineer or registered professional	<p>Project Proponent</p> <p>Economic Development Department</p> <p>Central Valley Regional Water Quality Control</p>	Prior to approval of construction documents	City Development Services Department - Building Division

GREYHOUND BUS TERMINAL PROJ.
MITIGATION MONITORING PLAN

Impact	Mitigation Measure	Action	Implementing Party	Timing	Monitoring Party
	<p>shall be prepared in coordination with the Central Valley Regional Water Quality Control Board and the City of Sacramento to assure compliance with applicable NPDES permit requirements for new developments. The plan shall include a combination of the following BMPs or equally effective measures:</p> <ul style="list-style-type: none"> a. Oil and grease separators shall be used to control roadway and parking lot contaminants. b. Streets and parking lots shall be cleaned and swept on a regular basis. c. Peak flow reduction and infiltration practices such as grass swales infiltration trenches and grass filter strips and detention and retention basins shall be incorporated. d. Landscape areas including borders and medians shall use low water-using plants wherever feasible. e. Plants of similar water use shall be grouped to reduce over-irrigation of low water-using plants. f. Mulch shall be used in all non-lawn landscaped areas to a minimum depth of two (2) 	hydrologist	Board		

REVISED GREYHOUND BUS TERMINAL RELOCATION PROJECT
(P10-020)
MITIGATION MONITORING PLAN

Impact	Mitigation Measure	Action	Implementing Party	Timing	Monitoring Party
	<p>inches. Mulch applied on top of the soil will improve the water-holding capacity and reduce runoff.</p> <p>g. Existing trees and shrubs shall be preserved and protected where feasible because established plants are often adapted to low water-using conditions.</p> <p>h. Efficient irrigation systems shall be installed to minimize runoff and evaporation and maximize the water that will reach the plant roots such as drip irrigation soil moisture sensors and automatic irrigation systems.</p> <p>i. Seasonal, climatical, and dosage fertilizer application restrictions shall be followed as recommended by manufacturer.</p> <p>j. Slow release fertilizers shall be used.</p> <p>k. Where feasible landscape areas shall be limited to 4:1 slopes to reduce runoff unless such slopes form landscape berms, which are required to mitigate aesthetic and noise impacts.</p>				

GREYHOUND BUS TERMINAL PROJECT
MITIGATION MONITORING PLAN

Impact	Mitigation Measure	Action	Implementing Party	Timing	Monitoring Party
	The use of plastic or other impervious materials to control weed growth in landscaped areas shall not be permitted.				
5. Air Quality					
AQ MM-1: Project Operations	<p>AQ MM-1: To ensure that construction mitigation is used, final approval shall not be given until the developer submits a construction dust mitigation plan satisfactory to the City. This plan should specify the methods of control that will be used to control dust and particulate matter demonstrate the availability of needed equipment and personnel and identify a responsible individual who if needed can authorize the implementation of additional measures.</p> <p>The construction dust mitigation plan should at a minimum include the following:</p> <ol style="list-style-type: none"> a. Suspend earthmoving or other dust producing activities during periods of high winds when dust control measures are unable to prevent visible dust plumes of a significant size. b. Provide equipment and staffing for watering of all exposed or disturbed soil surfaces at least twice daily including weekends 	<p>Prepare and submit construction dust mitigation plan.</p> <p>Incorporate measures into construction specifications.</p>	<p>Project Proponent Economic Development Department Contractor</p>	<p>Prior to approval of demolition, grading, and construction permits</p>	<p>City Development Services Department - Building Division</p>

REVISED GREYHOUND BUS TERMINAL RELOCATION PROJECT
(P10-020)
MITIGATION MONITORING PLAN

Impact	Mitigation Measure	Action	Implementing Party	Timing	Monitoring Party
	<p>and holidays. An appropriate dust palliative or suppressant added to water before application should be used.</p> <p>c. Water or cover stockpiles of debris soil sand or other materials that can be blown by the wind.</p> <p>d. Sweep the active construction area and adjacent streets of all mud and debris on a regular basis since this material can be pulverized and later re-suspended by vehicle traffic.</p> <p>e. Limit the speed of all construction vehicles to 15 miles per hour while on-site.</p> <p>f. All materials transported by truck will be covered or wetted down.</p> <p>g. All inactive portions of the site will be watered with an appropriate dust suppressant covered or seeded.</p> <p>h. Trucks shall maintain freeboard (i.e., the distance between the top of the load and the top of the truck bed sides).</p> <p>i. Truck wheel washers shall be installed before the roadway entrance at construction sites.</p>				

**GREYHOUND BUS TERMINAL PROJECT
MITIGATION MONITORING PLAN**

Impact	Mitigation Measure	Action	Implementing Party	Timing	Monitoring Party
	<ul style="list-style-type: none"> j. Tarps shall be used on trucks carrying dirt. k. Dust hoods shall be used on drilling and blasting equipment. 				
	<p>AQ MM-2: To the extent feasible, the following measures are required during construction:</p> <ul style="list-style-type: none"> a. Use low emission fuels for pile drivers such as methanol or low sulfur fuels. b. Use construction equipment that has catalytic converters for gasoline powered equipment. c. Prevent trucks from idling for more than two minutes. d. Discontinue operations during second stage smog alerts. 	Incorporate measures into construction specifications.	Project Proponent Contractor	Prior to approval of demolition, grading, and construction permits	City Development Services Department - Building Division
14. Cultural Resources					
Cultural MM-1: Impact to paleontological, prehistoric-period, or historic-period resources	Cultural MM-1: In the event that any prehistoric subsurface archeological features or deposits, including locally darkened soil ("midden"), that could conceal cultural deposits, animal bone, obsidian and/or mortars are discovered during construction related earth-moving activities, all work within 50 meters of the resources shall be halted, and the City shall consult with a qualified archeologist to assess the significance of the find.	<p>Mitigation measures shall be used and monitored during construction activities.</p> <p>Incorporate measures into construction specifications.</p>	Project Proponent Contractor Preservation Director	During construction	City Development Services Department - Building Division Preservation Director

REVISED GREYHOUND BUS TERMINAL RELOCATION PROJECT
 (P10-020)
 MITIGATION MONITORING PLAN

Impact	Mitigation Measure	Action	Implementing Party	Timing	Monitoring Party
	<p>Archeological test excavations shall be conducted by a qualified archeologist to aid in determining the nature and integrity of the find. If the find is determined to be significant by the qualified archeologist, representatives of the City and the qualified archeologist shall coordinate to determine the appropriate course of action. All significant cultural materials recovered shall be subject to scientific analysis and professional museum curation. In addition, a report shall be prepared by the qualified archeologist according to current professional standards.</p>				
<p>Cultural MM-2: Impact to paleontological, prehistoric-period, or historic-period resources</p>	<p>Cultural MM-2: If a Native American site is discovered, the evaluation process shall include consultation with the appropriate Native American representatives.</p> <p>If Native American archeological, ethnographic, or spiritual resources are involved, all identification and treatment shall be conducted by qualified archeologists, who are certified by the Society of Professional Archeologists (SOPA) and/or meet the federal standards as stated in the Code of Federal Regulations (36 CFR 61), and Native American representatives, who are approved by the local Native American community as</p>	<p>Mitigation measures shall be used and monitored during construction activities.</p> <p>Incorporate measures into construction specifications.</p>	<p>Project Proponent Contractor Preservation Director</p>	<p>During construction</p>	<p>City Development Services Department - Building Division Preservation Director</p>

GREYHOUND BUS TERMINAL PROJECT
MITIGATION MONITORING PLAN

Impact	Mitigation Measure	Action	Implementing Party	Timing	Monitoring Party
	<p>scholars of the cultural traditions.</p> <p>In the event that no such Native American is available, persons who represent tribal governments and/or organizations in the locale in which resources could be affected shall be consulted. If historic archeological sites are involved, all identified treatment is to be carried out by qualified historical archeologists, who shall meet either Register of Professional Archeologists (RPA), or 36 CFR 61 requirements.</p>				
Cultural MM-3: Impact to paleontological, prehistoric-period, or historic-period resources	<p>Cultural MM-3: If a human bone or bone of unknown origin is found during construction, all work shall stop in the vicinity of the find, and the County Coroner shall be contacted immediately. If the remains are determined to be Native American, the coroner shall notify the Native American Heritage Commission, who shall notify the person most likely believed to be a descendant. The most likely descendant shall work with the contractor to develop a program for re-interment of the human remains and any associated artifacts. No additional work is to take place within the immediate vicinity of the find until the identified appropriate actions have taken place.</p>	<p>Mitigation measures shall be used and monitored during construction activities.</p> <p>Incorporate measures into construction specifications.</p>	<p>Project Proponent Contractor Preservation Director County Coroner</p>	During construction	<p>City Development Services Department - Building Division Preservation Director</p>

REVISED GREYHOUND BUS TERMINAL RELOCATION PROJECT
(P10-020)
MITIGATION MONITORING PLAN

Impact	Mitigation Measure	Action	Implementing Party	Timing	Monitoring Party
Transportation and Circulation					
Trans-1: Cumulative Impacts	<p>Trans-1: At the Sequoia Pacific Boulevard / Richards Boulevard intersection, provide two northbound left-turn lanes, and one through-right turn lane; add one westbound right-turn lane with overlap signal phasing, to provide one left-turn, two through lanes, and one right-turn lane; monitor and adjust the signal timing when needed. The project shall be required to dedicate/ reserve the right of way needed to implement this mitigation measure in the future year, 2035.</p> <p>With implementation of this mitigation measure, the level of service would be improved to an acceptable LOS E (78.7 seconds delay) in the a.m. peak hour, and would be improved to LOS E (74.2 seconds delay) in the p.m. peak hour.</p>	Incorporate measures into construction specifications.	<p>Department of General Services</p> <p>Department of Transportation</p>	During Construction	Department of Transportation

Attachment 4: Design Director Approval



COMMUNITY DEVELOPMENT
DEPARTMENT

CITY OF SACRAMENTO
CALIFORNIA

915 I STREET, 3rd floor
SACRAMENTO, CA
95814-2998

DESIGN DIRECTOR LEVEL PROJECT REVIEW

DR Number: DR10-047
Address: 420 Richards Boulevard
Description: **Greyhound Bus Terminal**
APN: 001-0210-047

Applicant: Craig Stradley
Date Filed: March 9, 2010
Date Approved: April 15, 2010
Staff Contact: Evan Compton, 808-5260

DESIGN DIRECTOR ADVISORY CONDITIONS OF APPROVAL:

This project subject to Design Director level review has been deemed to be in substantial compliance with appropriate guidelines and is therefore approved with the following conditions of approval:

Site Conditions:

The design of the site (see plans attached) is hereby approved subject to the following conditions. These conditions must be met prior to the issuance of a building permit:

1. The buildings shall be sited as indicated on the exhibits.
2. The project shall have building setbacks and entries as indicated in the exhibits.
3. The project shall meet all planning conditions of approval (P10-020).
4. Auto access and site layout shall be as indicated in the exhibits.
5. Any site mechanical equipment proposed shall be screened as necessary to fit in with the design of the new building. Backflow prevention devices, SMUD boxes, etc., shall be placed where not visible from street views and screened from pedestrian view. The applicant shall submit final mechanical locations and screening to Design Review staff for review and approval prior to Building Permit issuance.
6. Provide colored concrete for the crosswalk (instead of just striping) to connect the special paving areas from the front steps along the public sidewalk to the building entrance area.
7. The applicant shall incorporate design elements from the building for the trash enclosure since it is located in an area that is visible from the street.

Building Conditions:

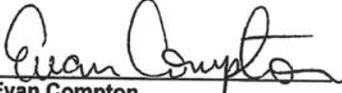
The design of the building (see plans attached) is hereby approved subject to the following conditions:

8. The design of the building shall be as indicated on the final plans and color and material board.
9. Final heights and massing shall be as indicated on the plans. The building elevations shall have a consistency of detail and quality as indicated on the plans.
10. Metal overhang shall be provided over the entry as shown on the attached plans.
11. Scoring lines shall be provided on the metal fins on the building front elevation drawings.

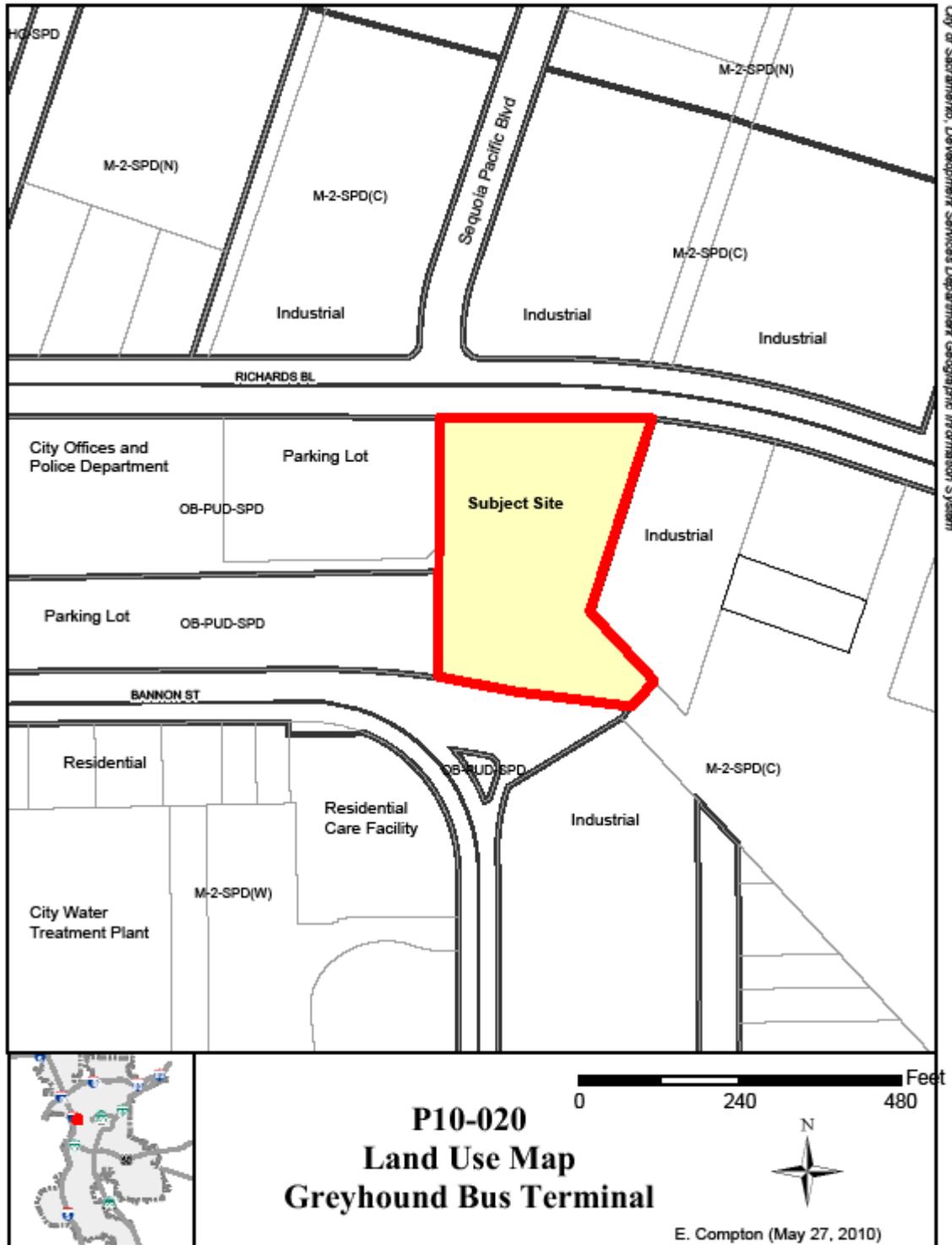
12. An aluminum framed storefront system shall be provided on the elevation facing Richards Boulevard.
13. The storefront system shall be modified to maintain clear views along the front elevation that faces Richards Boulevard. This will require the relocation of vending machines currently placed against the glass inside the building. Applicant shall consider moving the vending area to the side, and maintain the mullion pattern with translucent glass to screen the K brace and vending machine. Final revised elevation shall be provided for review by Design Review staff.
14. The applicant shall consider additional windows for the FS Manager's office, customer service, and GPX lobby.
15. Clear glazing shall be used on all windows.
16. Wall panels shall be constructed as shown on the attached plans with multiple colors as indicated for added visual interest.
17. All rooftop equipment shall be screened from the street.
18. The applicant shall submit a final exterior lighting plan to Design Review staff for review and approval prior to Building Permit issuance. Provide lowest possible height for front parking lot pole lighting.

General Conditions:

19. No building permit shall be issued until the expiration of the ten (10) business day request for reconsideration period. If reconsideration is requested, no permit shall be issued until final approval is received.
20. Any person dissatisfied with the staff action has the right to request reconsideration by the Design Director. Requests for reconsideration must be received within ten (10) business days of the staff action. The decision of the Design Director after reconsideration shall be final and shall not be subject to appeal.
21. All other notes and drawings on the final plans as submitted by the applicant are deemed conditions of approval. Any changes to the final set of plans stamped by Design Review staff shall be subject to review and approval prior to any changes. Applicant shall comply with all current building code requirements.
22. The approval shall be deemed automatically revoked unless required permits have been issued and construction begun within three years of the date of the approval. Prior to expiration, an extension of time may be granted by the Director upon written request of the applicant.
23. The Conditions of Approval shall be scanned and inserted into the final set as a general sheet to be submitted for building permit.

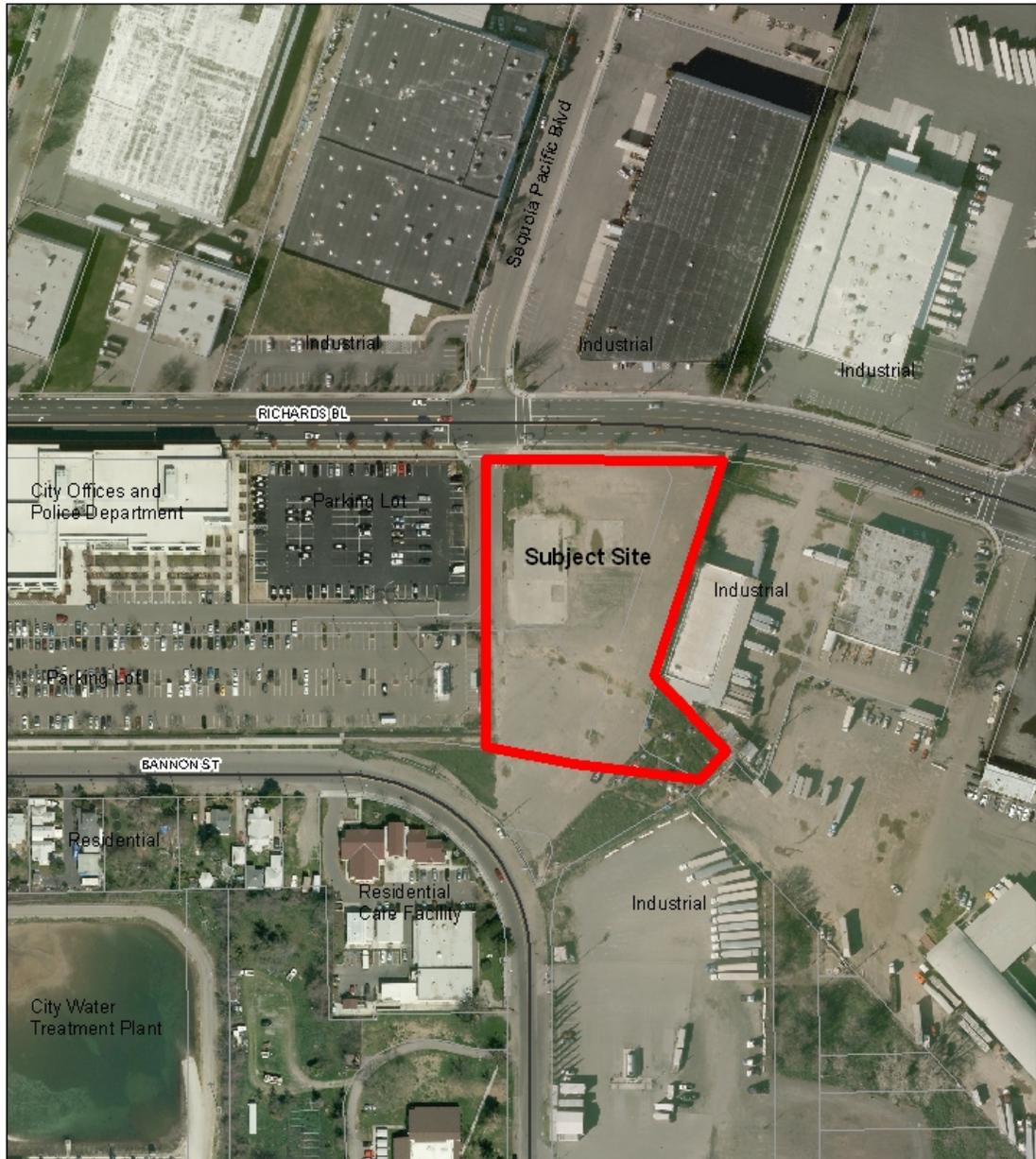

Evan Compton
Associate Planner

Attachment 5: Land Use Map

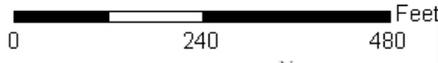


City of Sacramento, Development Services Department Geographic Information System

Attachment 6: Aerial Map



City of Sacramento, Development Services Department Geographic Information System



**P10-020
Aerial Map
Greyhound Bus Terminal**

E. Compton (May 27, 2010)

Attachment 7: Redline of Discovery Centre PUD Guidelines

DISCOVERY CENTRE PUD

DEVELOPMENT GUIDELINES
AND
SCHEMATIC PLAN

Adopted: November 5, 1998
Resolution No. 98-544

DISCOVERY CENTRE PUD

***DISCOVERY CENTRE PUD
DEVELOPMENT GUIDELINES***

I. PURPOSE AND INTENT

Discovery Centre is a Planned Unit Development (PUD) consisting of approximately 990,000 square feet of office and hotel use to be located at 300 Richards Boulevard in the Richards Boulevard Planning Area of the Central City. The Discovery Centre Planned Unit Development is a four phase project. However, not all of the phases may be developed and may not be developed sequentially.

Phase I will consist of an office building of approximately 150,000 square feet. Phase II will consist of a hotel of approximately 224 rooms or approximately 200,000 square feet. Phase III consists of an office building of approximately 240,000 square feet, and Phase IV consists of either an office building of approximately 400,000 square feet or a commercial building of approximately 10,000 square feet which could serve as an interium bus terminal. Parking for Phases I and II shall be accommodated with surface parking. Phase III will include the construction of a two-story, above grade parking structure that will may be expanded in Phase IV. The parking structure will contain all the parking for the office and some parking for the hotel. The hotel will also include on their site a number of parking spaces to accommodate their use. The Discovery Centre office buildings are intended to meet the specific market demands for both large floor plate and multi-tenant office users. As such, all buildings have an anticipated floor plate area of between 35,000 and 50,000 gross square feet or larger and they will be designed to accommodate public agency and private office users. The Discovery Centre PUD is well suited to any office user seeking the efficiency, flexibility and cost-effectiveness of large floor plate design in an attractive urban office campus setting.

These PUD Guidelines, as approved by the City of Sacramento City Council, shall govern the development of the 300 Richards Boulevard site and are intended to promote the following project objectives:

1. To provide large "back office" type floor space for private and public sector users wishing to locate in the Central City;
2. To house office users seeking the efficiency, flexibility and cost effectiveness of large floor plate designs in an attractive urban office campus setting;
3. To contribute to and enhance the operational efficiency and functional qualities of the existing tenants in the Richards Boulevard Redevelopment Area which

DISCOVERY CENTRE PUD

are seeking to expand their office space without the necessity of relocating from the area;

4. To provide a signature mixed use office and hotel urban campus to anchor the west end of the Richards Boulevard corridor and establish a design standard for the "lower cost, support office space" targeted by the Richards Boulevard Area Plan (RBAP) for this Central City location.
5. To contribute to financing of the critical first phase of infrastructure for the Railyards Specific Plan (RSP) and Richards Boulevard Area Plan;
6. To provide a logical extension of downtown employment activity, commerce and trade; and
7. To reinforce the use of major transit improvements planned for the area including the Light Rail Transit (LRT) Station to be located across from the Discovery Centre site on Richards Boulevard.

II. FORCE AND EFFECT

These Guidelines are intended to act as a supplement to existing City codes and ordinances and shall control when more restrictive than such codes and ordinances or when inconsistencies arise between the provisions contained herein and the provisions of such codes and ordinances, in so far as such inconsistencies do not in the opinion of the City, threaten the public health and safety. Any amendments hereto shall be subject to the procedures established in Section 180 Chapter 17.180 of the Zoning Ordinance Code (Title 17 of the City Code).

The Architectural Guidelines Section is proposed to supplement the design guidelines of the Richards Boulevard Area Plan and shall govern when more restrictive or when inconsistencies arise between these provisions and those contained within the Richards Boulevard Area Plan.

III. SCHEMATIC DEVELOPMENT PLAN

These development guidelines incorporate the Schematic Plan of the Discovery Centre PUD approved by the Sacramento City Council by Resolution No. 98-544. The conceptual Schematic Plan showing land uses, building locations, parking layout, access and site circulation is attached as Exhibit A. A special permit ~~planning director's plan review~~ is required for development in the PUD, except where the proposed use requires a special permit under the -only if the Zoning Code requires a special permit for such use or development project. ~~Otherwise, all development would be subject to a~~

DISCOVERY CENTRE PUD

~~Planning Director's Plan Review per the PUD regulations in Section 180 of the Zoning Code. The special permit and/or plan review allows the Planning Commission and staff or Planning Director to address specific issues and develop conditions of approval for items such as, but not limited to, throat distance, driveway locations, site circulation, parking layout, hotel pick up/drop off areas, loading areas, and other items necessary of the development of specific buildings. Minor modifications may be made to the Schematic Plan if the Planning Director determines that the requested modification is consistent with the objectives and intent of these PUD Guidelines. Amendments to the Schematic Plan are subject to the procedures established in Section 180 Chapter 17.180 of the Zoning Ordinance Code.~~

IV. PROCEDURES FOR APPROVAL

~~Development within the Discovery Centre office campus is subject to either a special permit approval by the City Planning Commission or plan review by the Planning Director. Such permit or plan review shall include and design review approval by the Design Review Board. Special permit and all plan review development plans for each phase of the PUD shall be prepared in conformance with the Schematic Plan and these PUD Guidelines.~~

~~A preliminary review of special permit and plan review applications may be required when the City determines that such review, by City, County, State and other agencies, is essential to a thorough review.~~

~~The following information shall be submitted with a special permit or plan review application:~~

- ~~1. Names and address of builder, tenant, developer, and architect.~~
- ~~2. Project site plan with dimensions taken from signed recorded plat.~~
- ~~3. All submissions must include topography showing existing grades and proposed grades at one foot intervals with spot elevations as required to clarify drawings; also show building corner elevations and floor landscaping.~~
- ~~4. Landscape plans, including shading calculations~~
- ~~5. Retaining walls.~~
- ~~6. Locations and details of temporary and permanent signs, including dimensions.~~

DISCOVERY CENTRE PUD

If the specific signage program is not known, the applicant shall designate a zone or alternative zones on the building facade(s) on which attached signage may be located and the location or alternative locations of detached signage. The Planning Commission or Planning Director shall approve the acceptable location(s) or zone(s) as part of the Special-
special Permit-permit or Pplan Rreview.

7. Temporary and permanent fences and fence material.
8. Front, side and rear setbacks from building to property lines.
9. Easements and rights-of-way, existing and proposed.
10. Pipes, berms, ditches, swales.
11. Driveways, parking areas, pathways and lighting, existing and proposed.
12. Locations and details of open space, including patios, seating areas, water features, pathways, etc.
13. Exterior storage and screening devices for trash, mechanical and communications equipment, and meters.
14. Location of light poles and transformers, with height and type indicated.
15. Utility plan, including services stubbed to the site.
16. Mailboxes, if known.
17. Roof projections and/or roof plan and screening treatment.
18. Land use distribution, percent and square footage of site used for building pads, surface parking and any other paved area, and landscaping (includes on-site open space, private sidewalks, and patios).
19. Building elevations for all sides and height to top plate and top of roof.
20. Location of existing and proposed buildings.
21. Street names and right-of-way widths and driveway locations adjacent to and across the street from the site.

DISCOVERY CENTRE PUD

22. Cross sections of structures indicating relationship to adjacent buildings and roadways.
23. Dimensions for typical parking stalls and maneuvering areas, including setbacks of buildings and building separation.
24. Bar scales (standard engineer's scale) on all plans.
25. Phasing scheme and proposed timing schedule for buildout.
26. Location and details regarding any on-site storage of hazardous or toxic material regulated by any applicable City Ordinance ~~No. 82-097~~.
27. Statement of Recycling Information regulated by ~~Section 34 of the Zoning Ordinance~~ City Code.
28. Show existing and proposed roadways and related facilities, including but not limited to curbs, gutters, sidewalks, medians, striping signals, street lights, drainage facilities, utility poles, etc.

V. LAND USE SUMMARY

The Discovery Centre PUD consists of six parcels with development proposed on four parcels to be developed in phases. Phase I consist of the development of an office building on Parcel 2, approximately 4.81 net acres, with 300 parking spaces developed on site. The Phase II hotel building will be located on Parcel 1, approximately 2.88 net acres, one hundred seventy (170) parking spaces will be developed on site for the use. An additional one hundred (100) parking spaces will be provided for the hotel use on Parcel 2, for a total of 270 parking spaces for the hotel use. The Phase III office building will be located on Parcel 3, approximately 1.74 net acres. The Phase IV office or commercial building will be located on Parcel 4, approximately 1.74 net acres. Parking for ~~all four p~~ Phases I, II and III will be provided on Parcel 2; a parking structure will be constructed during Phases III & IV, providing a total of 890 parking spaces, if additional parking is required for Phase III and if Phase IV is developed for office use. Parcel 5, approximately 0.19 net acres and Parcel 6, approximately 0.11 net acres are remnant parcels, created with the dedication of North 5th Street, that are ~~currently not proposed for~~ may be developed as part of Phase II or IV, and will require amendments to the PUD if development is proposed. Parcels 5 & 6 are not currently served by utilities.

The ~~proposed~~ current zoning of the Discovery Centre PUD site is Office Building Planned Unit Development/Special Planning District (OB PUD/SPD). The total maximum allowable area by the Richards Boulevard Redevelopment Plan for the site allows for a Floor Area Ratio (FAR) of 3 or approximately 1,460,000 square feet. This FAR is based on Parcels 1-4 net acreage

DISCOVERY CENTRE PUD

| and building development on these parcels. The ~~current~~adopted plan for the Discovery Centre PUD proposes a total of 990,000 square feet of gross building development with the following development in the phases listed. Adjustments to the phases to allow for the development of the site to the allowable maximum square footage (1,460,000 g.s.f.) requires an amendment | to these PUD Guidelines and approval by the Planning Commission and City Council, since the EIR was prepared assuming 990,000 square feet of allowable building development.

DISCOVERY CENTRE PUD

TABLE I
Land Use Summary

<i>Phase / Bldg. Type</i>	<i>Parcel / Size</i>	<i>Approx. Bldg. Size</i>
Office		
Phase I Office Building	2 / 4.81 net ac.	150,000± sf
Phase III Office Building	3 / 1.74 net ac.	240,000± sf
<u>Phase IV Office Building</u>	<u>4 / 1.74 net ac.</u>	<u>400,000± sf</u>
Total Office	8.29 net ac.	790,000± sf
Commercial		
<u>Phase IV Commercial Building</u>	<u>4/1.74 net ac.</u>	<u>10,000± sf</u>
Hotel		
<u>Phase II Hotel Building</u>	<u>1 / 2.88 net ac.</u>	<u>200,000± sf</u>
Total Hotel	2.88 net ac.	200,000± sf
Total Office and Hotel	11.17 net ac.	990,000± sf
Parking		
Phase II - Surfaced	1	170 spaces
<u>Phase III & IV - Structured</u>	<u>2</u>	<u>890 spaces</u>
Total Parking		1,060 spaces

VI. BUILDING STANDARDS

- A. Allowable Development:
 - 1. The overall building square footage (net of structured parking) in the Discovery Centre PUD shall not exceed a total of 990,000 gross square feet in the office and hotel buildings.
- B. For the purpose of calculating the Floor Area Ratio (FAR) for the Discovery Centre PUD, the area of the site shall include the aggregate area of all four Parcels for a total of 11.17 net acres.
- C. Permitted Uses:
 - 1. Office
 - 2. Hotel/Motel

DISCOVERY CENTRE PUD

- 3. Child Care Facilities
- 4. Ground floor commercial, retail or service uses as follows:
 - a. art gallery
 - b. appliance repair shop
 - c. bank, savings and loan
 - d. barber, beauty shop
 - e. book, stationery store
 - f. cleaners, laundry agency
 - g. clothing store
 - h. florist
 - j. laundromat
 - k. restaurant or cafeteria
 - l. retail
 - m. photography studio
 - n. martial arts training
 - o. video rental stores

D. Conditional Uses:

- 1. Temporary Parking Lot - a temporary lot is acceptable which addresses on an interim basis parking deficiencies in the downtown area created by new downtown development. The approval of the parking lot shall be consistent with the goals and policies of the City, including interim parking plans to assist in alleviating short term parking impacts created by the Federal Courthouse, CalEPA Office Building, Convention Center, Convention Center Hotel and other approved projects that assist in making Old Sacramento and the Downtown Business area a destination point . A temporary parking lot may be provided if properly designed and tailored to accommodate visitors, employees and patrons of the downtown area especially around the depot and Federal Courthouse.
 - a. Special Permit: A Zoning Administrator's Special Permit is required to establish a temporary parking lot no greater than 120 parking spaces. For lots larger than 120 spaces a Planning Commission special permit is required. A special permit may be granted subject to the following development standards:
 - i. A temporary parking lot shall have a term of five years.
 - ii. A shuttle shall be provided from/to the parking lot to/from the downtown area.

DISCOVERY CENTRE PUD

- iii. Clearly defined on-site pick-up and drop-off area for the shuttle bus.
- iv. A six foot planter shall be constructed along Richards Boulevard and Bannon Street. The planter shall be planted, irrigated and maintained with sod, flowers, bushes, shrubbery and trees. Bushes and shrubs shall be maintained at a maximum height of 30 inches and trees maintained at a minimum distance of six feet from lowest branch to ground.
- v. Interior shading is encouraged but not required. Shading may be provided in the form of trees or shade structures (e.g. photovoltaic, car ports etc.)
- vi. Exterior fencing, if provide, shall be open and decorative to a maximum of six feet in height, unobtrusive in color. Chain link, or vinyl clad chain link is not allowed if visible from any street frontages.
- vii. Lighting shall be provided at 1.5 footcandles of minimum maintained illumination per square foot of parking space between the hours of dusk and sunrise.
- viii. Upon conclusion of five year term, the parking lot shall be removed, or access to the lot shall be eliminated by closing the driveway, to the satisfaction of the Public Works Department, or by restricting access to the parking lots by bollards, curbing or fencing to the satisfaction of the Planning Division.

2. Bus Terminal. A Planning Commission special permit is required to establish such use.

- E. Ground floor retail or personal service business users along Richards Boulevard
 - i. Phase I: Ground floor retail is encouraged but not mandatory, however, the building shall be designed to accommodate future ground floor retail space.
 - ii. Phase III & IV - 25% of the street level of the office buildings frontingage on Richards Boulevard shall be occupied by retail, ~~or~~ personal services or other commercial uses.

DISCOVERY CENTRE PUD

- F. Height: building height shall be measured from finished lot grade at the front of the building to top plate line.
 - 1. Phases I through III maximum height shall be 85 feet.
 - 2. Phase IV maximum height shall be 140 feet.
 - 3. Penthouse for mechanical equipment, if provided, shall be permitted up to an additional 15 feet.

- G. Setbacks: The Discovery Centre project site is bounded by Bannon Street/proposed Richards Boulevard East to the south, Richards Boulevard to the north and the proposed 5th Street extension to the east.

Setbacks shall be measured from property lines of future rights-of-way, unless otherwise noted. Setback areas shall be landscaped and shall be in the widths set forth below:

- 1. Bannon Street (south) - 15 ft.
- 2. West Parcel line - 15 ft
- 3. Richards Boulevard (north) - Phase I - 0 ft.
Phase II - IV - 10 ft.
- 4. 5th Street (east) - 0 ft.

Setbacks may be required by the City to provide adequate sight distance at driveways and corners.

- H. Vehicle Parking: Parking for the office buildings in Phases I, III, and IV shall be provided on Parcel 2. The Phase III and IV office buildings will provide a phased parking structure on Parcel 2 to complete the parking requirements of the project. The Phase II hotel building will include surface parking on Parcel 1 and additional parking on the Parcel 2, as surface parking or within the parking structure when built. Private reciprocal easements for ingress, egress, parking and maneuvering shall be dedicated with the sale or lease of any parcel for ingress and egress of all driveways and shared use of all parking areas.

The parking structure shall be constructed to include outlets for electrical vehicle recharging stations. Two (2%) percent of all structured vehicle parking spaces shall be equipped with electrical outlets for electric vehicles. This will include adequate power supply (120/240v.40amp) service to each parking stall.

- 1. Office

DISCOVERY CENTRE PUD

- a. Off-street parking shall be developed at a ratio of one vehicle parking spaces for every 500 gross square feet of floor area.
 - b. Parking may be reduce to above the 1:500 ratio up to a minimum amount of off-street vehicle parking of one space for every 1,000 gross square feet of floor area, provided that additional TSM measures, beyond those mandated by the Developer TSM Ordinance (35%), shall be implemented in order to support the reduction in parking. A developer Transportation Management Plan (TMP) shall be submitted concurrently with the special permit for development that provides less than one space per 500 gross square feet to justify the requested parking reduction.
2. Restaurant - Vehicle parking shall not be required for a restaurant use that is a component of the office project provided that the restaurant use does not exceed 6,000 gross square feet per building.

Exception - vehicle parking shall not be required for a state operated cafeteria, not exceeding 14,500 net square feet, that serves the office building and operates during the hours the office building is open.

- a. The minimum off-street vehicle parking spaces for a restaurant use, exceeding 6,000 gross square feet, provided such use is a component of an office project is one space per 450 gross square feet of floor area. The maximum parking ratio is one space per 100 gross square feet of floor area.
3. Commercial/retail or service uses:
- a. Vehicle parking shall not be required for a commercial or retail use provided that such use is a component of an office project and provided that such use does not exceed 5,000 square feet of building occupancy per tenant.
 - b. One vehicle parking space per 450 gross square feet shall be required for a commercial or retail use provided such use is a component of an office project and provided that such use does not exceed 9,600 gross square feet of building occupancy per tenant.
 - c. One vehicle parking space per 400 gross square feet shall be required for a commercial or retail use provided such use is a component of an

DISCOVERY CENTRE PUD

office project and provided that such use exceeds 9,600 gross square feet of building occupancy per tenant. The maximum amount of off-street vehicle parking permitted shall be one space per 300 gross square feet of floor area for commercial or retail use.

- 4. Hotel: Parking for the hotel can be accommodated on the hotel site, in the parking structure or surface parking on Parcel 2, or in a combination of both.
 - a. A minimum of one vehicle per every two guest rooms plus one parking space for the hotel manager.
- I. Bicycle Parking:
 - 1. Office: One bicycle parking facility shall be provided for every 6,000 gross square feet of building area (net of structured parking). Fifty percent (50%) of the required bicycle parking facilities shall be Class I facilities. The remaining facilities may be Class I, II, or III.
 - 2. Hotel: One bicycle parking facility shall be provided for every 50 off-street vehicle parking spaces required. Twenty-five percent of the required bicycle parking facilities shall be Class I. The remaining facilities may be Class I, II, or III.
- J. Open Space: Areas specifically designed for active and passive enjoyment of the outdoors are required. A mixture of recreational and passive spaces shall be incorporated into the open space.
 - 1. Office and Hotel Use:
 - a. The Discovery Centre PUD shall provide open space at a ratio of 1 square foot of open space per 10 square feet of development (1:10). The open space shall be satisfied on a cumulative basis for the entire PUD. The open space requirement may be placed on a single parcel or on individual parcels. Reciprocal access shall be provided to the open space if provided on an individual parcel.
 - b. A minimum of 20% of the open space shall be provided on site.
 - i. Provisions shall be made for the balance, if any, of the open space obligation attributable to the Discovery Centre project either through dedication/conveyance to the City, at no cost to the City, of an off-site open space location acceptable to the City and within the Richards Boulevard Planning Area.

DISCOVERY CENTRE PUD

- ii. Dedication/conveyance of off-site open space or an in lieu fee shall be made by a Development Agreement or other similar agreement or arrangement by the City.
 - c. On-site open space shall be in the form of courtyards and public plazas. Courtyards and plaza areas shall be a minimum 2,500 square foot and no larger than 85,000 square feet. To the extent consistent with considerations of security, courtyards and plaza areas shall be oriented towards pedestrian linkages or located adjacent to people oriented uses (eg. restaurants and retail uses).
 - d. The on-site open space shall incorporate a minimum of one active space area as provided in the list below (e).
 - e. The on-site open space area, shall incorporate, but not be limited to, one or more of the following features:
 - i. Landscaping (i.e. turf, trees, flower gardens, etc.)
 - ii. Decorative paving (i.e. tile, cobblestone, colored concrete, etc.)
 - iii. Public art pieces
 - iv. Water features (i.e. ponds, reflecting pools, etc)
 - v. Outdoor sport areas (i.e., basketball & volleyball courts, soccer areas, softball fields, etc)
 - vi. Seating areas
 - vii. Canopies
 - viii. Lighting
 - f. The on-site open space features shall be complimentary to the building architecture. Design elements, material, colors and lighting should be contextual with the proposed building or existing adjacent buildings.
- K. Wall/Fencing Requirements:
 - 1. Solid perimeter walls shall be discouraged unless required for noise mitigation,

DISCOVERY CENTRE PUD

or security. In all cases requiring a solid screen wall, landscaping along the wall shall be planted to minimize the visual effect of the wall from both internal view and external view.

2. Fencing necessary for the security of the site shall be of either textured solid masonry such as split face concrete block or open ornamental steel or iron to a maximum of six feet in height, unobtrusive in color, .
3. Existing chain link fencing may be retained for undeveloped parcels, provided that it is green or black vinyl clad, and if landscaping is provided the landscaping shall be maintained.
4. All walls and/or fences shall comply with City requirements for visibility, sight distance and gate design and location.

L. Temporary Structures:

1. Temporary structures, including but not limited to trailers, mobile homes and other structures not affixed to the ground, are permitted only during construction and shall be removed promptly upon completion of the construction activity.
2. Such temporary structures shall be as inconspicuous as practicable and shall cause no inconvenience to the general public.

VII. INFRASTRUCTURE

Discovery Centre PUD will be developed in phases. With the development of the PUD in phases the infrastructure will also be developed in phases. Since it is difficult to project the exact phasing, the infrastructure improvements required by subdividing the property will be implemented in three phases, the recordation of the final parcel map, first and second building permits. As envisioned the infrastructure phasing will occur as follows, but does not include improvements required by the MMP:

Phase I: Prior to recordation of Final Parcel Map: Design and construct improvements (including but not limited to curb, gutter, sidewalk and landscape strip) adjacent to Richards Boulevard (entire length);

Phase II: prior to or concurrent with the issuance of the a building permit for the development of the 150,000 square foot office building on Parcel 2, design and construct Bannon Street improvements adjacent to Parcel 2 (improvements include but not limited to pavement section, curb, gutter, sidewalk, landscape strip and reconstruction of existing street to centerline);

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Phase III: prior to or concurrent with issuance of the second building permit for development in the PUD on any parcel, design and construct remaining Bannon Street improvements adjacent to Parcel 1 and Parcel 4 and design and construct North 5th Street improvements.

Should development not occur as proposed, the phasing of the infrastructure may be adjusted at the time of special permit or plan review for each parcel to provide for the orderly development of the infrastructure. Furthermore, should the Final Tentative Parcel Map not be recorded, each special permit or plan review shall be conditioned to require the final map to be recorded.

The City can require the construction of any of the adjacent infrastructure prior to issuance of any building permits, if the City deems them necessary for public health and safety or if the improvements are a necessary prerequisite to the orderly development of the surrounding area.

The issuance of a building permit for a temporary parking lot will not trigger any of the improvements below, with the exception of utilities which may be necessary to ensure proper drainage of the property.

A. Infrastructure Phasing:

1. Prior to recordation of final Parcel Map:
 - a. Dedicate eight (8) feet of right-of-way adjacent to Richards Boulevard.
 - b. Design and construct a landscaped planter and new sidewalk adjacent to Richards Boulevard in accordance with City of Sacramento standards and consistent with the Railyards/Richards Boulevard Facility Element and to the satisfaction of Public Works.
 - c. Dedicate an additional 18 feet of right-of-way adjacent to Bannon Street (48 foot half -street). Bannon is proposed as a 96 foot right-of-way.
 - d. Dedicate the future North 5th Street from Bannon Street to Richards Boulevard, and dedicate the intersection of future North 5th Street with the future Bannon Street.
2. Prior to or concurrent with the issuance of a building permit for the

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150,000 square foot office building on Parcel 2, design and construct Bannon Street adjacent to Parcel 2 to a 48 foot half street in accordance to City of Sacramento standards and consistent with the Railyards/Richards Boulevard Facility Element and to the satisfaction of Public Works. Improvements shall include but not be limited to pavement section, curb, gutter, landscaped planter, sidewalk and reconstruction of existing street to centerline.

3. Prior to or concurrent with the issuance of a second building permit for the Discovery Center PUD
 - a. design and construct Bannon Street to a 48 foot half street adjacent to Parcel 1 and Parcel 4 in accordance to City of Sacramento standards and consistent with the Railyards/Richards Boulevard Facility Element and to the satisfaction of Public Works. Improvements shall include but not be limited to pavement section, curb, gutter, landscaped planter, sidewalk and reconstruction of existing street to centerline.
 - b. Design and construct the intersection of future North 5th Street with the future Bannon Street in accordance to City of Sacramento standards and consistent with the Railyards/Richards Boulevard Facility Element and to the satisfaction of Public Works. Improvements shall include but not be limited to pavement section, curb, gutter, landscaped planter, sidewalk.
 - c. Design and construct future North 5th Street to the centerline, from Richards Boulevard south to existing Bannon Street, (south property line including City property) in accordance to City of Sacramento standards and consistent with the Railyards/Richards Boulevard Facility Element and to the satisfaction of Public Works. Improvements shall include but not be limited to pavement section, curb, gutter, landscaped planter, sidewalk..
 - d. Design and construct necessary traffic signal conduits and appurtenances to the satisfaction of Public Works at the intersection of future North 5th Street and Richards Boulevard and the intersection of North 5th Street and Bannon Street.
4. Prior to or concurrent with the issuance of any building permit, design

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and construct a 15 inch drainage main in Bannon Street from manhole #403 as show on page Z14 in the Department of Utilities Drainage/Sewer Book to a new manhole in the proposed intersection of Bannon Street and future North 5th Street with an 18-inch drainage main stub for a future connection of an 18-inch drainage main to manhole #803 also shown on Page Z14 and to the satisfaction of the Department of Utilities.

The tentative map conditions and advisory notes (P97-037) provide additional information as to the specific details of the improvements required. The exact limits of construction for required improvements will be determined at the time of building permit based on sound engineering principles, traffic safety and development plans in the area.

A building permit for a temporary parking lot on Parcel 3 or 4 shall not trigger the infrastructure improvements. However, drainage, water and sewer improvements may be required. Coordination with the Department of Utilities is required prior to issuance of any building permits for a temporary parking lot.

- B. Construction of a driveway on Bannon Street, to the satisfaction of Public Works, is required at the earlier of:
 - 1. the third special permit or plan review.
 - 2. the first special permit or plan review following the programming/funding of Bannon Street from I-5 to North 7th Street.
 - 3. the first building permit following construction of Bannon Street from I-5 to North 7th Street.
 - 4. The designation of Bannon as a one-way street.
- C. A special permit or plan review for the Phase 3 or 4 building shall require the conversion of the easterly driveway to the fourth leg of a standard intersection, modification of the traffic signal may be required.
- D. The intense use of the site will necessitate provision of adequate on-site stacking at the site driveways. This stacking area should be 200 feet, measured along the driveway into the site. Parking aisles and stalls need to be outside of this stacking area.
- E. The location of the hotel registration parking area and drop-off/pick-up

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should be relocated and be wide enough to accommodate side-by side vehicle parking and maneuvering to prevent vehicles from backing up into the site driveway.

- F. The Facility Element identifies a local street on the west side of the property. The Schematic Plan does not propose dedication or improvement of this street. Public Works has identified potential operational problems with the development of this street and the proposed improvements to the I-5/Richards ramp. Therefore, Public Works does not oppose the elimination of this proposed street.
- G. In December, 1993, the City Council certified a program EIR for the Railyards/Richards Boulevard planning area. In December, 1994, the City Council certified a Supplemental EIR (SEIR) and adopted the Railyards Specific Plan and the Richards Boulevard Area Plan. The SEIR addressed the development of 870,000 square feet of office space within the Richards Boulevard area and 3 million square feet of office area in the Railyards prior to implementation of all Phase I infrastructure improvements (identified in the planning documents). In September, 1997, the City Council adopted a Finance Plan for the Richards Boulevard, Railyards and Downtown area to facilitate the construction of infrastructure within the Richards and Railyards planning area. Under the Financing Plan, development fees collected at the building permit stage are used to construct major infrastructure. If insufficient fees have been generated to construct infrastructure improvements required by a development project, the developer shall construct the required improvements and be reimbursed from fees generated by subsequent projects.

The SEIR does not identify the infrastructure improvements required if development in the Richards area exceeds 870,000 square feet prior to completion of all Phase I infrastructure improvements. Future special permits requested for the Discovery Centre PUD (as well as other special permits in the area) will be required to evaluate the level of development in place (completed, under construction + proposed project). If the level of development is greater than 870,000 square feet, the special permit may require additional traffic analysis to determine compliance with the SEIR, other program level EIRs certified for the area, and/or identify additional mitigation measures. This is to ensure that the proposed development complies with the SEIR requirement that all Phase I infrastructure improvements listed in the SEIR are constructed at the time development in the Richards Boulevard area exceeds 870,000 square feet of office space or to identify other appropriate mitigation and, if appropriate, amend the land use plans accordingly.

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H. Utilities -

1. The overall project is greater than 5 acres (13.6±), therefore the project is required to comply with the State "NPDES General Permit for Stormwater Discharges Associated with Construction Activity" (State Permit). For each individual project the developer will be required to comply with the State Permit. To comply with the State Permit, the developer will need to file a Notice of Intent (NOI) with the State Water Resources Control Board (SWRCB) and prepare a Stormwater Pollution Prevention Plan (SWPPP) prior to issuance of building permit. A copy of the State Permit and NOI may be obtained from the Department of Utilities by calling (916)433-6318.
2. GROUNDWATER DISCHARGE PERMIT - All new groundwater discharges to the Combined or Separated Sewers must be regulated and monitored by the Department of Utilities (City Council Resolution 92-439). Groundwater discharges to the City's sewer system are defined as follows:
 - a. Construction dewatering discharges.
 - b. Treated or untreated contaminated groundwater cleanup discharges.
 - c. Uncontaminated groundwater discharges

Foundation or basement dewatering discharges to the CSS will not be allowed. The CSS does not have adequate capacity to allow for dewatering discharges for foundations or basements. Foundations and basements shall be designed without the need for dewatering.

Groundwater discharges may contain toxic and/or explosive chemicals that could be harmful to the environment and to service workers working in the City's sewer system. Groundwater discharges to the sewer system go beyond the original design of the City's system, thus removing existing sewer capacity from other system users and potentially causing overflows or restricting development. The additional water from groundwater discharges must be conveyed and pumped by the City's existing facilities. The additional volume of water increases the City's operations and maintenance costs through increased capacity, power, and maintenance costs.

Currently, two types of groundwater discharges are recognized by the Department of Utilities; limited discharges and long-term discharges.

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these types of discharges are described as follows:

- a. "limited discharges" are short groundwater discharges of 7-days duration or less. Limited discharges must be approved through the Department of Utilities by acceptance letter.
- b. "long-term discharges" are groundwater discharges of greater duration than 7-days. Long-term discharge must be approved through the Department of Utilities and the City Manager through a Memorandum of Understanding (MOU) process.

The Groundwater MOU has a term of one year and requires the discharger to:

- a. Provide a description of the groundwater discharge,
 - b. Obtain a Regional Sanitation District permit,
 - c. Obtain approval from the Regional Water Quality Board if discharge is part of groundwater cleanup or contains contaminants above MCLs,
 - d. Pay fees based on flow amounts when a fee schedule is established by ordinance,
 - e. Comply with any new pertinent laws,
 - f. Assess and repair sewer lines if the discharge exceeds MCLs,
 - g. Suspend discharges during storm events or at City request,
 - h. Provide shut-off switches accessible to the City, and
 - i. Indemnify the City against all claims related to the MOU.
3. The developer shall comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinance. This ordinance requires the applicant to show erosion and sediment control methods on the improvement plans. These plans shall also show the methods to control urban runoff pollution from the project site during construction.
 4. Post construction, stormwater quality control measures shall be incorporated into the development to minimize the increase of urban runoff caused by development of the area. Since the project is not served by a regional water quality control facility and is greater than one acre, both source controls and on-site treatment control measures are required. On-site treatment control measures may affect site design and site configuration and therefore, should be considered during the early planning stages. Improvement plans must include on-site treatment control measures. Refer to the draft "Manual of Standards for Design of New Development On-site

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Stormwater Quality Control Measures", dated January 23, 1995, for appropriate source control measures and recommended on-site control measures.

5. An on-site surface drainage system is required and shall be connected to the street drainage system by means of a storm drain service tap. All on-site systems shall be designed to the standard for private storm drainage systems (per Section 11.12 of the Design and Procedures Manual).
6. Sanitary sewer flows to the combined sewer system (CSS) from this project have been determined to increase the street flooding adjacent to this site. this impact to the CSS has also been determined to be significant and must be mitigated. If mitigation of impacts is not practical, the developer may enter into an impact fee agreement with the City.
7. The developer shall provide a study to the satisfaction of the Department of Utilities demonstrating that adequate water pressure is available for fire prevention and domestic water service. If water pressure is not adequate a water distribution main extension may be required in Bannon Street.
8. In the event the PUD conditions conflict with the Utilities conditions of the bus terminal project (P10-020), the conditions of the bus terminal project shall apply.

VIII. ARCHITECTURAL TREATMENT AND MATERIALS.

- A. Discovery Centre buildings shall be designed to provide a signature building to anchor the western approach into the Richards Boulevard area and establish a design standard for the lower cost, support office space targeted by the Richards Boulevard Area Plan for this location.
 1. Building Facades. The design of building facades should be clearly organized to distinguish between the building base (up to 40 feet), the upper level streetwall facades (up to 85 feet) and any tower element that is above 85 feet in height.
 2. Building Bases. The building base should be open in nature and designed in a way that creates visual interest and variety along the street frontage of Richards Boulevard and the future North 5th Street both from the perspective of the pedestrian and the passing motorist. Architectural features such as setbacks,

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colonnades, arcades, changes in materials and changes in wall planes are encouraged to provide visual relief and clearly articulate the building base from the upper level portions of the building. Because of the proximity of the building base to pedestrian level activity, special attention should be given to architectural detail including placement of joint lines, use of premium cladding at the pedestrian level and location of ventilation louvers for mechanical equipment away from points of pedestrian activity. Although the building base should employ transparent design features such as colonnades, areas of solidity should also be incorporated as the base elevation is articulated.

3. **Building Entries.** The building entry design should reflect a principal building orientation towards Richards Boulevard. Entries should be clearly defined with integrated canopies for weather protection and with adequate and distinctive lighting for safety and effect. Service entries should be located away from the major pedestrian activity points.
4. **Fenestration.** The fenestration above the building base, up to 85 feet in height, is intended to articulate the different masses of the building by use of curtain walls, ribbon windows, punched openings and sun protection devices such as overhangs, trellises, and canopies to provide an extra level of texture for different window types. Window openings should provide for a hierarchy of horizontal and vertical expression and should be recessed to offer shade and complexity to the building elevation. Patterns should reflect changes in building form, stepbacks and proportions, and should avoid relentless grids and repetitive patterns. The solid portion of the wall should incorporate reveals, moldings, and changes in plane that reinforce the compositional design of the facade and offer shadow, detail and variety. This fenestration should contrast in appearance from the fenestration of the tower element.
5. **Tower Element.** The tower element should have a higher proportion of glass to wall to create a lighter appearance; fenestration may be flush with the building wall to reinforce this lightness. Glass curtain walls may be used if integrated with other design features to lighten the building mass. The tower element should emphasize the corner of 5th Street and Richards Boulevard by providing visual interest through special architectural features such as fenestration, massing, and expression of building structure. The design of the tower should be highly integrated with lower portions of the building, incorporating the same family of materials and creating a strong "interlock" between the base and the tower shaft. The tower should step back in profile toward the top and incorporate a distinctive roof treatment that creates a pleasing silhouette against the sky. Additional stepbacks in the tower element need not be employed. The combined effect of the design orientation and architectural features discussed above should be to acknowledge the tower element as the

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intersection of Richards Boulevard and 5th Street.

6. **Roof Treatment.** Parapets and roofs should be designed with decorative treatments such as cornices and loggias to provide horizontal datum lines and add articulation and interest to the facade. Roof and/or parapet lines should be broken at intervals to provide for variation; such changes in treatment should correspond to building entries, corners and/or other compositional elements of the facade. All mechanical equipment shall be screened from view and architecturally integrated within the roof treatment of the building. Satellite dishes, communications equipment and helicopter pad requirements should also be integrated into the roof design.
7. **Building Materials.** Building materials shall convey solidity, durability and substance and be appropriate to the commercial function of the area. Recommended materials for the upper portions of the building and the tower element include precast concrete, glass-fiber reinforced concrete (GFRC), metal panels or other equivalent high quality materials. Highly reflective mirrored or excessively dark glass should be avoided as a primary building material for the facade.

IX. ENVIRONMENTAL STANDARDS

A. Purpose and Intent

The purpose and intent of this section is 1) to encourage the creative and innovative use of materials and methods of construction, and 2) to prevent indiscriminate and insensitive use of materials and design. All buildings, structures, paved areas and building materials, color schemes, and landscape elements shall be designed and constructed so as to create a desirable environment for the intended use and relate harmoniously to the surrounding community.

B. Landscaping

1. **Landscape Plans.** Detailed landscape and irrigation plans shall be approved by the Neighborhoods, Planning and Development Services Department prior to issuance of a building permit. The detailed landscape and irrigation plans shall include a tree shading diagram for any on-site surface parking.
2. **General Landscaping.** Groundcovers interspersed with tree plantings will be employed to enhance the aesthetics of the Discovery Centre PUD development. All landscaping referred to in this section shall be maintained in a healthy, neat and orderly fashion. Shrubbery and bushes adjacent to public right-of-ways and parking area shall be maintained at a maximum height of 30

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inches. Trees adjacent to public right-of-way and adjacent to or within the parking lot shall be maintained at a minimum distance of 6 feet from lowest branch to ground.

3. Planting types. A plant list for the Discovery Centre PUD shall be approved by the Neighborhoods, Planning and Development Services Department prior to the issuance of a building permit for the project. All trees, shrubs, and groundcover planting types shall conform to the Discovery Centre approved plant list (attached as Exhibit B) unless an alternate type is approved by the Neighborhoods, Planning and Development Services Department.
 4. Irrigation. All landscaped areas shall be irrigated with timed permanent automatic systems.
 5. Setback Areas. Landscaping in these areas shall consist of an effective combination of trees, lawns, groundcover and shrubbery. Backflow preventers located in the setback areas shall be attractively screened. Landscaping near corners and driveways shall comply with City standards for visibility and sight distance, including maintaining shrubbery to a height of not more than three feet and trimming trees so that branches are not lower than six feet.
 6. Unpaved Areas. All unpaved areas shall be fenced or other physical barrier provided to prevent parking and storage and maintained in a weed free condition. If landscaped, the landscaping shall include groundcover and/or shrubbery and tree material and be maintained.
 7. Pedestrian-Oriented Amenities. The inclusion of pedestrian-oriented amenities such as benches in the landscaped areas is encouraged.
 8. Installation of Landscaping. Prior to the issuance of any temporary or final occupancy permits, the project landscaping, including permanent automatic irrigation systems, shall either be installed or a security, in a form satisfactory to the City, shall be posted to insure installation as soon as climatically possible after occupancy.
- C. Walkways and Courtyards

Walkways shall be designed indicating a relationship with street access, transit stops, parking areas, commercial and retail space and adjacent structures. Walkways shall be designed with pedestrian health and safety in mind. Pedestrian walkways shall be landscaped to provide shade in the summer.

Walkways and courtyard materials shall be compatible with the exterior wall materials

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of the buildings. Layout and design shall provide comfort and safety to pedestrians and should include pedestrian-oriented amenities such as benches, water fountains, etc.

D. Treatment of Parking Areas

1. Landscaping. All landscaping shall be installed and maintained with the objective of maximizing the growth potential of the plant material. Parking areas should be planted with trees which moderate the effects of the climate. Trees shall be planted and maintained throughout the parking lot area so as to ensure that, within 15 years after construction, at least 50% of the parking area will be shaded at noon on August 21st.
2. Lighting. Lighting shall be designed to contribute to the public safety. There must be a minimum of 1.5 footcandles of maintained illumination per square foot of parking space and 0.25 footcandles per square foot of surface on walkways, alcoves, passageways, etc. Light standards in parking areas shall be no higher than 25 feet. The configuration of light fixtures are to have reflective heads in order to prevent spillover and to minimize glare on adjacent streets and properties. Light fixtures shall be offset from trees so as not to be obscured by overhanging limbs.
3. Curbs, walls, decorative fences with effective landscaping or similar screening devices shall be located along the perimeter of parking lots and enclosed storage areas except at entrances and exits indicated on approved plans. Such barriers shall be designated and located to prevent parked vehicles from extending beyond property lines of parking lots or into landscaped spaces where parking is prohibited and to protect public right-of-way and adjoining properties from damaging effects of surface drainage from parking lots. Minimum stall dimensions shall correspond to standards provided in the City Zoning Ordinance except that the front two feet of all stalls (the area into which the vehicle bumper overhangs) may be incorporated into the adjacent landscape or walkway improvements, provided that the adjacent landscape or walkway improvements are a minimum of six (6) feet in depth, resulting in a net decrease of two (2) feet of the required surface depth of the parking stall. A continuous six (6) inch raised concrete curb shall be provided along all landscaped areas and walkways abutting parking or drives. Curbs and drives shall be constructed in accordance with the latest requirements of the City of Sacramento.

E. Exterior Lighting

1. Pedestrian areas around the buildings shall have pedestrian scaled lights that

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shall be no higher than 16 to 18 feet.

2. Lighting shall be designed in such a manner as to provide safety and comfort for occupants of the development and the general public.
3. Lighting design shall be such as not to produce hazardous and annoying glare to motorists and building occupants, nearby residents, or the general public.
4. Lighting shall be oriented away from the properties adjacent to Discovery Centre PUD.
5. Exterior lighting fixtures shall be similar and compatible throughout the Discovery Centre PUD.

F. Loading and Service Areas

Loading and service areas should generally be located behind buildings and away from primary roadways. In locating these areas, primary consideration should be given to the adjoining land uses. Design mechanisms such as walls and landscaping should be utilized to conceal the loading and storage activities from public view.

G. Energy Conservation

1. The Discovery Centre buildings shall be designed to meet current state and federal energy requirements at the time of construction.
2. Developers, builders and architects shall work with relevant utilities during the initial stages of project development to maximize energy efficiency and reduce infrastructure needs and developer cost. The purpose of this coordination is to ensure that energy efficiency and load management measures are considered feasible for all developments in order to reduce overall energy use in the community.
3. Site Design. Buildings should be oriented to provide appropriate solar access/exposure and shading for improved efficiency. Natural ventilation and day lighting should also be considered to reduce cooling and heating loads and to provide alternatives to more energy-intensive mechanical/electrical systems.
4. Building Design. Construction should maximize energy efficiency potential through the utilization of: exterior building materials that are well insulated and shaded where appropriate; window shading strategies/devices and effective shading coefficients; the use of light colored paints and building materials for interior and exterior surfaces; and the use of efficient lighting systems and day lighting louvers

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and controls/sensors to reduce dependence on artificial lighting and to reduce cooling loads. In addition, water consumption features as well as efficient heating, ventilating and air conditioning systems, pumps, motors and energy management systems should be employed where economically feasible. Opportunities for passive and active solar design (photovoltaics, solar domestic hot water) and the inclusion of alternative and renewable energy systems should be explored.

5. Landscaping shall be designed to shade walks, streets, drives and parking areas so as to minimize surface heat gain and shall, at a minimum, comply with the Water Conservation Landscape Requirements and all other current City of Sacramento standards.
 6. Outdoor lighting should be designed to provide the minimum level of site lighting commensurate with site security.
- H. Garbage Services and Trash Enclosures
1. The Discovery Centre development shall participate fully in a comprehensive solid waste management program designed so that on the average 50 percent of the solid waste generated within the PUD is diverted from the landfill to recycling, re-use or other disposal means such as cogeneration. All recycling and source reduction programs shall comply with the City of Sacramento Recycling and Solid Waste Reduction Ordinance.
 2. These facilities shall not create a nuisance and shall be inconspicuously located. Trash enclosures shall meet City design requirements and shall be located to the satisfaction of Public Works..
 3. All exterior garbage, refuse and recycling areas shall be concealed by a screening wall.
 4. Such facilities shall relate appropriately to the buildings they serve and shall not be obtrusive or detract from the building design theme.
- I. Utility Connections, Mechanical Equipment and Communications Equipment
1. Mechanical and communications equipment, utility meter and storage tanks shall not be visible at ground level.
 2. If concealment within the building is not possible, then such utility elements shall be concealed by screen walls, which shall be appropriately landscaped.
 3. All utility lines providing electric service directly to the site shall be underground.

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- 4. All mechanical equipment shall be located so as not to cause a nuisance or discomfort from noise, fumes, odors, etc.
 - 5. Penthouse and mechanical and communications equipment screening shall be of a design and material compatible with the building siding, with colors to match the roof or building.
 - 6. Placement of mechanical and communications equipment, utility meters, backflow preventers, substations, and storage tanks shall minimize their visibility particularly from the public rights-of-way. Visual barriers such as walls or landscaping shall be used when such equipment cannot be placed out of view, provided such barriers do not significantly impede the proper functioning of the equipment.
- J. On-site Drainage
- Adequate drainage facilities shall be provided in accordance with City of Sacramento standards.
- K. Hazardous Materials
- All buildings or structures containing hazardous materials shall be labeled at all doorways with easy to read signs. The signs shall provide emergency response teams with information on the hazardous contents of the building or structure and proper containment procedures. Labeling should be based on existing systems (such as the National Fire Protection Association 704 system) and approved by the City Fire Department.

X. SIGN CRITERIA

- A. A sign program for the Discovery Centre development shall be submitted to the Department of Neighborhoods, Planning and Development Services subsequent to the City Planning Commission special permit hearing or the Planning Director plan review determination and shall be approved prior to the installation of any signage. The sign program is intended to assure that all Discovery Centre signage is harmonious with the Discovery Centre building and site. The sign program shall set forth the locations and details of temporary and permanent signage, including sign dimensions and comply with the following requirements:
- 1. One detached sign per Parcel not to exceed 24 square feet.
 - a. All detached signs shall be monument type signs not to exceed 24 square

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feet each. The height of the monument sign shall not exceed six feet. All detached signs shall be located ten feet from any property line or driveway in order to provide a clear vision area.

2. Two attached signs per building not to exceed 20 square feet each.
 - a. Attached signs shall be placed flat against a building or designed as part of an architectural feature. No height limit is specified for signs placed flat against the wall of a building.
 - b. Attached signs are to be located on different faces of the building. No two signs are to be on the same face of the building.

Attachment 8: List of Matrix Team Members

Department	Contact Person	Telephone	Email
Current Planning	Evan Compton	808-5260	ecompton@cityofsacramento.org
Team Leader	Ron Yasui	808-1937	ryasui@cityofsacramento.org
Parks	Raymond Costantino	808-8826	rcostantino@cityofsacramento.org
Fire	King Tunson	808-1358	ktunson@sfd.cityofsacramento.org
Dev. Engineering	Anis Ghobril	808-5367	aghobril@cityofsacramento.org
Utilities	Neal Joyce	808-1912	njoyce@cityofsacramento.org
Urban Forest	Duane Goosen	808-4996	dgoosen@cityofsacramento.org
Environmental	Dana Allen	808-2762	dallen@cityofsacramento.org