



REPORT TO PLANNING COMMISSION City of Sacramento

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915 I Street, Sacramento, CA 95814-2671

PUBLIC HEARING
September 23, 2010

To: Members of the Planning Commission

Subject: 3820 Broadway (P10-042) Request to construct a 53,000 square foot mixed use project including 56 units of senior apartments and 4,000 square feet of retail/commercial space on approximately 1.2 acres in the Limited Commercial, Broadway/Stockton Special Planning District (C-1-SPD) zone. The project requires the following entitlements:

- A. Environmental Determination:** Exempt (per CEQA Guidelines Section 15332, Infill Development);
- B. Special Permit** to construct a building over 20,000 square feet in size in the Limited Commercial (C-1-SPD) zone;
- C. Special Permit** to reduce required parking for senior apartments;
- D. Variance** to reduce the setback from 5' to 0' along Broadway and;
- E. Variance** to reduce the setback from 20' to 0' along Martin Luther King Jr. Boulevard.

Location/Council District:

3820 Broadway, Sacramento, CA

Assessor's Parcel Number: 014-0171-001, -017, -018, -019, -020, -025

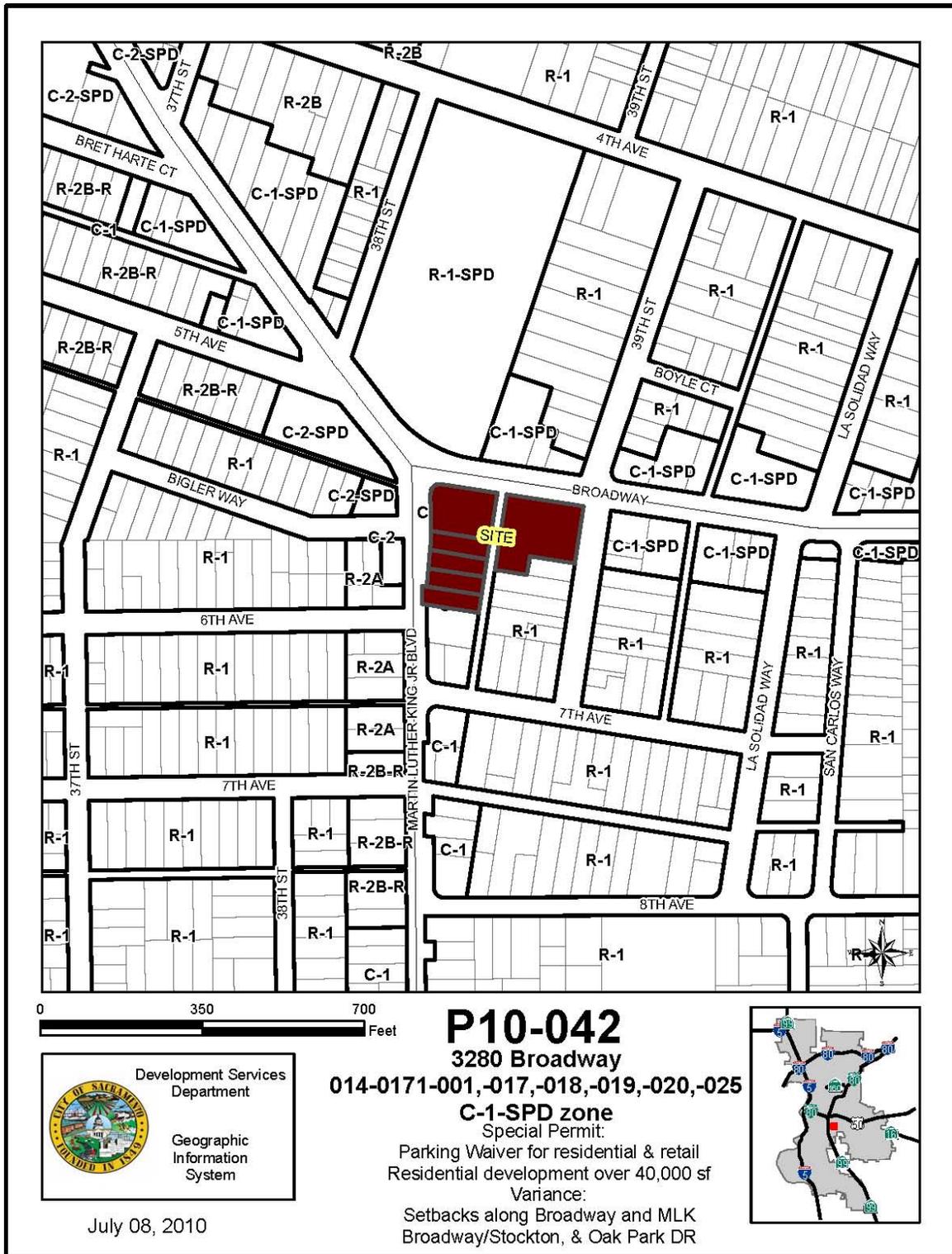
Council District 5

Recommendation: Staff recommends the Commission approve the request based on the findings and subject to the conditions listed in Attachment 1. The Commission has final approval authority over items A-E above, and its decision is appealable to City Council. At the time of writing this report, staff is not aware of any outstanding neighborhood issues.

Contact: Matthew Sites, Associate AIA, LEED AP, Urban Design Staff, (916) 808-7646
Lindsey Alagozian, Senior Planner, (916) 808-2659

Applicant: Steven Oh, Related Companies, (949) 660-7272, 18201 Von Carmen Avenue, Suite 900, Irvine, CA 92612.

Owner: Redevelopment Agency Of The City Of Sacramento, (916) 444-9210, 801 12th Street, Sacramento, CA 95814.



0 350 700 Feet



Development Services
Department

Geographic
Information
System

July 08, 2010

P10-042
3820 Broadway
014-0171-001,-017,-018,-019,-020,-025
C-1-SPD zone
 Special Permit:
 Parking Waiver for residential & retail
 Residential development over 40,000 sf
 Variance:
 Setbacks along Broadway and MLK
 Broadway/Stockton, & Oak Park DR



Summary: The applicant is proposing to develop an approximately 53,000 square foot mixed use project including 56 units of senior apartments and 4,000 square feet of retail/commercial space on approximately 1.2 acres in the Limited Commercial, Broadway/Stockton Special Planning District (C-1-SPD) zone. The project requires two Special Permits and two Variances. Staff supports the project as it provides a mix of uses including affordable housing for seniors. The proposed senior housing development with ground floor retail, which is a lower generator of vehicular trips supports a parking reduction, as it is within walking distance of commercial services. Staff notified all property owners within 500 feet of the site for this public hearing and received no opposition from the surrounding neighborhood.

Table 1: Project Information
General Plan designation: Urban Corridor Low (20-110 dwelling units per net acre), FAR: 0.3- 3.0
Existing zoning of site: C-1-SPD; Limited Commercial, Broadway Stockton Special Planning District Zone
Existing use of site: Vacant
Property area: Approximately 1.2 acres

Background Information: The applicant is requesting entitlements to develop a 53,000 square foot mixed use project including 56 senior apartments on approximately 1.2 net acres within the Broadway/Stockton Special Planning District. This project has been through several iterations over the past several years, and the current developer and owner (SHRA) have acquired an appropriate scope and budget for this project. This building is located at a prominent corner in the Oak Park neighborhood and the proposed uses complement those of the surrounding area.

A previous planning application P06-159, was submitted in 2006 for a mixed use development consisting of retail, townhouses, and residential apartments, but was later withdrawn in 2008. The project became infeasible due to the traffic mitigations and density requirements and was ultimately withdrawn. Additional information was requested in IR09-348 regarding traffic mitigation, acoustic study, arborist survey, and a review of the Phase 1 report.

Public/Neighborhood Outreach and Comments: Staff routed the proposal to the Oak Park Neighborhood Association, Oak Park Business Association, Oak Park Redevelopment Advisory Committee and property owners within a five-hundred foot radius of the project site. Staff received comments from the Oak Park Redevelopment Advisory Committee (RAC) see Attachment 4. Comments were expressed by individual RAC members regarding the design of the project. Since the RAC meeting city staff, the applicant, and SHRA staff have been working closely to resolve the design concerns for the project. At this time, staff believes that the design of the project will be resolved before the Design Commission hearing in October, 2010.

Environmental Considerations: The Community Development Department, Environmental Planning Services Division has reviewed this project and determined that this is exempt from the provisions of the California Environmental Quality Act (CEQA) Section 15332, In-fill Development Projects. The project consists of the construction of a building that occurs in an urban area served by utilities and public services, on a site that is less than 5 acres, has no habitat value, is consistent with all applicable land uses, and would not result in any significant effects to traffic, noise, air, or water quality.

Policy Considerations:

2030 General Plan

The 2030 General Plan Update was adopted by City Council on March 3, 2009. The 2030 General Plan's goals, policies, and implementation programs define a roadmap to achieving Sacramento's vision to be the most livable city in America. The 2030 General Plan Update designation of the subject site is Urban Corridor Low, which provides for, "...A development pattern with moderate lot coverage, limited side yard setbacks, and buildings cited up to the corridor ... large-scale development should include a mix of nonresidential and residential uses with more intense development near major intersections." Furthermore, the proposed project would promote the following Urban Corridor Low policies:

- **Transformed Corridors.** The City shall facilitate the transformation of major thoroughfares dominated by auto-oriented strip commercial uses to include a broader mix of uses, both horizontal and vertical, that provides opportunities for medium- and higher-density housing, while also addressing local and citywide demand for retail and services. (Policy LU 6.1.2)
- **Efficient Parcel Utilization.** The City shall promote the aggregation of small and irregular shaped parcels along corridors into larger development sites to facilitate their redevelopment. (Policy LU 6.1.4)
- **Conversion to Residential.** The City shall support proposals to convert nonresidential properties along mixed-use corridors, between major intersections, to residential or mixed-use residential uses. (Policy LU 6.1.7)

The proposed project meets the 2030 General Plan goals and policies related to the Urban Corridor Low land use designation.

Project Design:

Land Use

The applicant is proposing to develop an approximately 53,000 square foot mixed use project including 56 units of senior apartments and 4,000 square feet of retail/commercial space on approximately 1.2 acres in the Limited Commercial, Broadway/Stockton

Special Planning District (C-1-SPD) zone. The project requires a Special Permit to allow construction of a building over 20,000 square feet in size in the Limited Commercial C-1-SPD zone. The proposed project also requires a Special Permit to reduce the number of parking stalls for senior apartments. Variances are requested to reduce the setback from 5' to 0' along Broadway and from 20' to 0' along Martin Luther King Jr. Boulevard.

This project includes a parcel merger of six parcels into a single 1.2 acre parcel, and proposed partial alley abandonment off Broadway between Martin Luther King, Jr. Blvd and 39th Street. The parcel merger request has been reviewed and has been determined to be consistent with zoning regulations. A condition of approval is included to require that the applicant complete the parcel merger prior to commencing construction of the project. The alley abandonment is proceeding along a separate application with the Department of Transportation which ultimately requires City Council approval. The alley abandonment request is expected to be presented to City Council on October 5, 2010.

Special Permit to allow a building over 20,000 square feet in size.

According to the zoning code, Section 17.60-030 (15) a special permit is required for any building of 20,000 square feet or larger in the Limited Commercial (C-1) zone. In evaluating special permit proposals of this type, the Planning Commission is required to make the following findings:

A. Sound Principles of Land Use. A special permit shall be granted upon sound principles of land use.

In this case, staff finds that the proposed project is appropriate because it provides a housing type not found in the area and will help ensure that the City meets its objectives to provide more affordable ownership housing opportunities. Staff finds that approval of the Special Permit is appropriate due to compatibility of the proposed mixed use building with a vacant parcel zoned for limited commercial and residential uses to the east, a mini-mart and residential to the south, retail and residential uses to the west, and institutional and retail used to the north, as the retail use will serve and complement the population in the immediate area of the site. In addition, the site is surrounded by existing development and existing infrastructure.

B. Not Injurious. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.

The approval of the Special Permit for a 53,000 square foot mixed-use project will not be detrimental to the public welfare and will not result in the creation of a public nuisance in that the uses are compatible with the surrounding community and will provide additional commercial uses for the surrounding single family community. The building has been designed on the property in such a way that minimizes any impacts to the residential uses to the south. The additional square footage allocated to the building allows for a quality mixed-use development with parking and landscaping located to the rear of the property.

C. Must Relate to a Plan. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.

The proposed project is consistent with the commercial land use and is consistent with the General Plan designation of Urban Corridor Low for the subject site.

Access, Circulation and Parking

The proposed project includes driveway access points along the east side of Martin Luther King, Jr. Boulevard. Emergency vehicle access is provided on the east side of the property along 39th Street, along the south property line at the alley, and in the parking area at the west. The proposed vehicular entries requires the construction of a median in Martin Luther King, Jr. Boulevard to prevent left in and left out maneuvers. The applicant has agreed to constructing the median and staff has included a condition of approval. The proposed development is consistent with the zoning regulations in that the safe and proper functioning of the entries will be achieved with the Department of Transportation traffic measures. A vehicular drop-off area is provided along Broadway and is located adjacent to a proposed pedestrian connection at the entry of the residential portion of the building. The applicant has the option to provide this element and will be conditioned accordingly. Staff supports the proposed drop off as it will provide greater support for the reduced number of parking spaces.

Pedestrian circulation is provided with the inclusion of walkways throughout the project site. A main pedestrian connection is proposed off Broadway and extends throughout the project, culminating at the pergola adjacent to the fountain and gathering space. In addition, ample walkways are provided throughout the site which is consistent with the Multi-Family Residential Design Guidelines. The following tables identify the total number of vehicular parking and bicycle parking spaces for the project:

Table 2: Parking			
Use	Required Parking	Proposed Parking	Difference
Apartments	88 (based on 1.5 space per unit + 1 guest space per 15 units)	31	-53
Retail, etc.	10	10	0

Table 2a: Bicycle Parking			
Total parking provided	Required bicycle parking	Provided bicycle parking	Difference
41	5	6	+1

The project is proposing a total of 41 parking spaces, including compact spaces and accessible spaces. Ten parking spaces are required for the retail uses and are provided on the street side of the fence/gate off Martin Luther King, Jr. Blvd. A metal security gate and fence is provided for the remainder of the parking spaces for 24 hour secured parking for senior residents. According to the Zoning Code, apartment uses typically require 88 parking spaces for 56 units based upon the 1.5 parking per unit ratio plus one guest space per 15 units. The project provides a total of 31 parking spaces for the senior apartments, which requires the approval of a Special Permit to reduce the total number of parking spaces. A discussion of this entitlement is located in the subsequent section of this staff report.

The project is consistent with Multi-Family Residential Design Guidelines regarding parking in that: the surface parking areas are located away from the adjacent public roadways; and parking and vehicle access is located away from street corners.

According to the Sacramento City Code (Section 17.64.050), one bicycle parking facility is required for every twenty (20) off-street parking spaces required. Since 88 parking spaces are required, a minimum of five bicycle facilities are required. Fifty (50) percent of the required bicycle parking facilities shall be Class I; the remaining facilities may be Class I, Class II, or Class III. According to the Multi-Family Residential Design Guidelines, bicycle parking should be located close to, and with direct access to, residential buildings. The project will be conditioned such that bicycle parking spaces are located in close proximity to the building.

Special Permit for Parking Reduction

The applicant provides a total of 31 parking spaces for 56 units, and ten parking spaces for the retail uses. The project provides the required number of parking spaces for the retail uses, but does not provide the required number of parking spaces for the residential component. Typically, apartment projects provide 1.5 spaces per dwelling unit and one guest space per 15 units and the applicant provides senior residential parking at a ratio of 0.5.

Staff is in support of reduced the number of parking spaces for the project. Staff has extensively evaluated the subject site and believes that the project is feasible based on the following reasons: 1) the site is located adjacent to existing retail and commercial uses which helps to reduce trip generations; 2) the proposed high density use will be next to several Regional Transit (RT) bus routes and will support transit ridership; 3) a drop-off area (if constructed) can be provided to reduce the need for additional parking spaces for those that do not drive; 4) senior housing typically requires less parking than non-senior housing and therefore a reduction in parking is supported; 5) the project provides indoor community space and ample outdoor amenities for use by its residents; and 6) staff has supported reduced parking for senior housing projects throughout the city based on the reduced need for parking.

The applicant has completed a parking analysis for the project and is included in the staff report as Attachment 3. Within this analysis an extensive comparison with other senior housing projects is provided demonstrating that the proposed 0.5 parking ratio is consistent with other senior housing projects in existing urban areas of Sacramento.

Staff reviewed several Senior Living projects that the Planning Commission and the Zoning Administrator approved and found that a range of 0.50 to 0.75 spaces per unit was acceptable in higher density areas, and areas with no local amenities required 1.0 to 1.25 spaces per unit. As there are a significant amount of amenities surrounding this property staff supports the parking reduction. The reduction in parking is also appropriate due to the shared use as the retail space will generally serve the residences. The applicant has indicated that the gated parking will be assigned and the rest of the parking stalls will be unmarked to be used for both retail and residential guests. The applicant will submit a Transportation Management Plan for review and approval by the City which may address alternative transportation modes such as public transit and shuttles. In conclusion, staff supports the requested parking reduction and finds the proposed parking to provide adequate parking for retail, residents and guests.

The approval of a Special Permit is required to reduce the parking for the apartment use. In evaluating special permit proposals of this type, the Planning Commission is required to make the following findings:

A. A special permit shall be granted upon sound principles of land use.

Staff finds that the parking reduction is appropriate for the project since senior housing typically requires less parking for the residents and because the site is adjacent to a Regional Transit Bus Route and commercial establishments that can be easily accessed by residents.

B. Not Injurious. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.

Staff finds that the proposed parking will be adequate for users within the senior housing development and that the reduction will not be detrimental to the safety and welfare of its residents. The proposed parking ratio is consistent with other senior living projects in urban areas of Sacramento.

C. Must Relate to a Plan. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.

The proposed project, in providing higher density development adjacent to a Regional Transit Bus Route is consistent with the residential land use policies and density requirements of the General Plan.

Height, Bulk and Setbacks

Under the Special Permit to allow a building over 20,000 square feet in size, the Planning Commission has the authority to vary the setbacks. The Zoning Code requires that mixed use projects meet the required Limited Commercial (C-1) street side setbacks (5 feet) and rear setback (20 feet). The proposal requests reduced setbacks on Broadway and Martin Luther King, Jr. Boulevard to allow the building to be constructed with a strong street wall and establish an urban identity. The parcels if not merged, would have a zero foot setback along Broadway and Martin Luther King, Jr. Boulevard. The project meets the intent of the Broadway / Stockton Special Planning District which requires that buildings be located close to the public street; however, this project is constrained due to the odd configuration of the merged lots and necessitates a variance. Therefore staff supports the request to vary the setbacks as noted in the chart below.

Standard	Required	Proposed	Deviation?
Height*	35'	33.5'	no
Front setback (39 th Street)	0'	5'	no
Side setback	5'	27'1.5"	no
Street side setback (Broadway)	5'	0'	Yes - 5'
Rear setback (MLK)	20'	0'	Yes -20'
Courtyard	n/a	n/a	n/a
Lot coverage	70% max.	30%	no
Density	20-110 d.u./n.a.	47 d.u./n.a.	no

*The height of the proposed office building is measured to the plate line. The Zoning Code allows additional height for elevator towers and mechanical appurtenances as long as the variation in height is not greater than 20% and does not cover more than 50% of the roof area.

As indicated above, the project will request entitlements to deviate from the required setback requirements. The proposed project will exceed the setback requirement by 5 feet along Broadway and 20 feet along Martin Luther King, Jr. Boulevard. Staff supports the reduction of the setbacks as it will provide a strong corner element and massing that erodes toward single family residences.

Variance to reduce setbacks

The project involves merging six lots into one resulting in a parcel that is bordered by three public streets: Broadway, Martin Luther King, Jr. Blvd, and 39th Street. According to the Zoning Code, the shortest side of the lot adjacent to a public street is considered the front which is 39th Street. The Broadway elevation is considered the street side yard setback and Martin Luther King, Jr. Boulevard is considered the rear, for setback purposes only. The Broadway / Stockton Special Planning District encourages that buildings be located close to the major commercial corridor, which is Broadway in this case. Due to the odd configuration of the proposed merged lots, variances are required for this type of development. Therefore, the applicant is requesting approval of two setback variances along Broadway and Martin Luther King, Jr. Boulevard. According to the Zoning Code, the approval of a variance is required to reduce the required setback distance for the project building as proposed. In order to grant a variance, the Planning Commission must make the following findings:

A. A variance cannot be a special privilege extended to one individual property owner. The circumstances must be such that the same variance would be appropriate for any property owner facing similar circumstances.

No special privilege is being extended and the variance would be appropriate for any property owner, in that the parcels if not merged, would have a zero foot setback along Broadway and Martin Luther King, Jr. Boulevard. The project meets the intent of the Broadway / Stockton Special Planning District which requires that buildings be located close to the public street; however, this project is constrained due to the odd configuration of the merged lots and necessitates a variance.

B. The consideration of "use variances" is specifically prohibited. These are variances which request approval to locate a use in a zone from which it is prohibited by ordinance.

A use variance is not requested; the proposed uses are permitted subject to the approval of a special permit and compliance with City development standards.

C. A variance must not be injurious to public welfare, nor to property in the vicinity of the applicant.

The variance will not be injurious to public welfare, nor to property in the vicinity of the applicant, in that the building design utilizes zero foot setbacks at the corner of Broadway and Martin Luther King, Jr. Boulevard, and is reduced at the street edges as the building returns toward the single family residential uses both east and south of the intersection. The Zoning Code and the Broadway/Stockton Special Planning District requires activation of public streets and allowing visually accessible retail at street level, the location of the retail entry should provide transparency from one street frontage to another. **A variance must be in harmony with the general purpose and intent of the zoning code. It must not adversely affect the general plan or specific plans of the city, or the open space zoning regulations.**

The proposed development is otherwise consistent with the zoning regulations in that the safety and aesthetic of the area is not impaired. The proposed variance does not violate any applicable general plan policies, as it promotes the aggregation of small and irregular shaped parcels along corridors into larger development sites to facilitate their redevelopment.

Because of this unique circumstance and the lack of harm to public safety, staff has no objection to the variances to reduce the setback from 5' to 0' along Broadway and to reduce the setback from 20' to 0' along Martin Luther King Jr. Boulevard.

Building design, signage and landscaping

The applicant is proposing construction of an approximately 53,000 square foot building containing 4,000 square feet of ground floor retail / commercial space and 49,000 square feet of senior apartments. The building is designed such that the commercial space is located at the corner of Broadway and Martin Luther King, Jr. Boulevard. The remaining first floor of the building is comprised of common area, access to outdoor gathering and landscaped spaces, and single bedroom apartments. The common spaces include: lounge, multi-purpose room, fitness room, and a computer room. The outdoor spaces are located to the rear of the property and include the following amenities: Rose Walk, water feature, Getaway Garden, and an outdoor seating area with an outdoor kitchen and barbeque. The parking for the project is located behind the building along Martin Luther King, Jr. Boulevard.

A total of 56 apartment units are being proposed for senior housing. 55 of the units will be single bedroom units of approximately 587 square feet. One of the units, which is projected to be for the on-site manager will be a 2-bedroom unit of approximately 720 square feet in size. The apartments are located on all three floors of the building. Bollards are located at the corner of Broadway and Martin Luther King, Jr. Blvd and the building is slightly recessed in response to the city's concerns for traffic accidents at this corner. Staff believes that the proposed bollards and location of the building at the corner will address the safety concern. As required by the Zoning Code, a six foot high CMU solid masonry wall is required along the southern and western property lines which abut residential zones and uses.

The proposed building will be three stories in height which is consistent with the goals of the general plan for Urban Corridor Low and the Limited Commercial zoning. The massing and scale of the building is appropriate for the location and is well articulated through layering of the façade and materials reducing the potential of a monolithic mass. The building design was sited to reduce localized issues of traffic views, noise from the streets, and maximize protection and ambiance of the outdoor green space at the south of the building. The building orientation also provides access to the delta breeze and southern sun exposure in the outdoor green space creating a more inviting amenity.

The building finishes include smooth finished cement plaster with reveals utilized to articulate the plaster, fiber cement horizontal lap siding, and thin brick veneer. Balconies

have been provided at several of the second and third floor units, as well as some screened outdoor patio space at the ground level along Broadway to create further articulation and interest in the façades. A recessed open outdoor space adjacent to the retail space provides punctuation of the Broadway and Martin Luther King, Jr. Boulevard corner and the colonnade has been brought close to the street facilitating the pedestrian connection. This project requires review and approval by the Design Commission as it is located in the Broadway/Stockton and Oak Park Design Review Districts. Design Review staff supports the proposed project and will present this project to the Design Commission, this meeting is tentatively scheduled for October 13, 2010.

The building shown is schematic in design and will require refining prior to construction. The project will be conditioned at the Design Commission Hearing for staff level design review approval prior to construction. Although the building is schematic in design it does exhibit many characteristics consistent with the goals of the Design Review Districts. The buildings have been designed to integrate with the surrounding context in terms of function, scale, and massing.

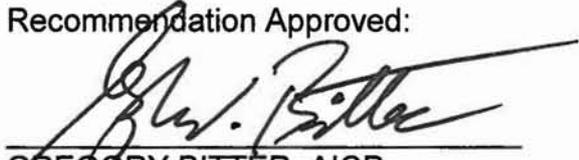
Blade signs have been proposed on the elevations as a placeholder during this phase of work. Any signs proposed shall meet the City Code and Design Review standards prior to Building Permit submittal.

Conclusion:

Staff is in support of the proposal and recommends the Planning Commission approve items A through E.

Respectfully submitted by: 
MATTHEW SITES, Associate AIA, LEED AP
Urban Design Staff

Approved by: 
LINDSEY ALAGOZIAN
Senior Planner

Recommendation Approved:

GREGORY BITTER, AICP
Principle Planner

Attachments:

Attachment 1	Recommended Findings of Fact and Conditions of Approval Project Plans
Attachment 2	
Exhibit 1	Illustrative Site Plan
Exhibit 2	District Content
Exhibit 3	Site Analysis
Exhibit 4	Project Context Photos
Exhibit 5	Building Elevations
Exhibit 6	Enlarged Elevations – Option A
Exhibit 7	Enlarged Elevations – Option B
Exhibit 8	Enlarged Elevations – Building Section
Exhibit 9	Annotated Site Plan
Exhibit 10	First Floor Plan
Exhibit 11	Second Floor Plan
Exhibit 12	Third Floor Plan
Exhibit 13	Roof Plan
Exhibit 14	Prototypical Unit Plan
Exhibit 15	Preliminary Landscape Plan
Exhibit 16	Setbacks & Easements
Attachment 3	Parking Studies as provided by applicant
Attachment 4	Comments from Oak Park RAC
Attachment 5	Land Use & Zoning Map

**Attachment 1
Proposed Findings of Fact and Conditions of Approval
3820 Broadway and P10-042**

Findings Of Fact

- A. Environmental Determination:** Exemption - Based on the determination and recommendation of the City's Environmental Planning Services Manager and the oral and documentary evidence received at the hearing on the Project, the Planning Commission finds that the Project is exempt from review under Section **15332, In-Fill Development Projects** of the California Environmental Quality Act (CEQA) Guidelines as follows:

The project consists of the construction of a building that occurs in an urban area served by utilities and public services, on a site that is less than 5 acres, has no habitat value, is consistent with all applicable land uses, and would not result in any significant effects to traffic, noise, air, or water quality.

- B.** The **Special Permit** to allow residential development within Limited Commercial C1-SPD that is over 20,000 sf **is approved** subject to the following Findings of Fact:
- a. Granting the Special Permit is based upon sound principles of land use in that the proposed project is compatible with the surrounding area of commercial and residential and adds to the balance of housing types in the downtown neighborhood. The proposed development promotes the goals and policies of the General Plan designation of Urban Corridor Low, which provides a development with moderate lot coverage, limited side yard setbacks, and building cited up to the corridor as well as including a mix of nonresidential and residential uses with more intense development near major intersections.
 - b. Granting the Special Permit will not be detrimental to the public welfare nor result in the creation of a public nuisance in that the project will develop residential units that are oriented to provide eyes on the street and internal parking area, and provides balconies and terraces for outdoor space, and;
 - c. The proposed project is consistent with the proposed General Plan designation of Urban Corridor Low, and the proposed Limited Commercial (C-1) zone which housing is allowed by right. The project is also consistent with the General Plan policies which encourage infill development and will promote alternative modes of transportation such as bus, bike, and walking which helps air quality and reduces urban sprawl.
- C.** The **Special Permit** to reduce required parking for senior housing **is approved** subject to the following Findings of Fact:

- a. Staff finds that the parking reduction is appropriate for the project since senior housing typically requires less parking for the residents and because the site is adjacent to a Region Transit Bus Route and retail establishments that can be easily accessed by residents., and that proposed parking ratio is consistent with other senior living projects in urban areas of Sacramento;
- b. Staff finds that the proposed parking will be adequate for users within the senior housing development and that the reduction will not be detrimental to the safety and welfare of its residents; and
- c. The proposed project, in providing high density development adjacent to existing RT Bus Routes, is consistent with the residential land use policies and density requirements of the General Plan and Broadway/Stockton Special Planning District.

D & E. The **Variiances** to reduce the setbacks from 5' to 0' along Broadway and to reduce the setback from 20' to 0' along Martin Luther King Jr. **is approved** subject to the following Findings of Fact:

- a. Granting the variance does not constitute a special privilege extended to an individual property owner in that variances would be granted to other property owners facing similar circumstances where the parcels if not merged, would have a zero foot front setback along Broadway and Martin Luther King, Jr. Boulevard;
- b. No use variance is requested; the proposed use is permitted subject to the approval of a special permit and City development standards;
- c. Granting the variance will not be injurious to public welfare, nor to property in the vicinity of the applicant, in that the building design utilizes zero foot setbacks at the corner of Broadway and Martin Luther King, Jr. Boulevard, and is reduced at the street edges as the building returns toward the single family residential uses both east and south. The use of bollards and the recently completed Capital Improvement Project of the Broadway and Martin Luther King, Jr. Boulevard intersection should provide enough protection for public welfare at the corner.
- d. The proposed development is otherwise consistent with the zoning regulations in that the safety and aesthetic of the area is not impaired. The proposed variance does not violate any applicable general plan policies, as it promotes the aggregation of small and irregular shaped parcels along corridors into larger development sites to facilitate their redevelopment.

Conditions Of Approval

B. The **Special Permit** to allow construction of a building greater than 20,000 square

feet within the Limited Commercial C-1-SPD zone **is approved** subject to the following conditions:

Planning

- B1. This approval is for the construction of 56 apartment units and 4,000 square feet of retail shown on attached exhibits. Development of this site shall be in compliance with the attached exhibits except as conditioned. Final plans shall be submitted to Current Planning for review prior to the issuance of building permits.
- B2. The applicant shall obtain all necessary building permits prior to commencement of construction; any modification to the project shall be subject to review by Current Planning staff prior to the issuance of building permits. Any significant modifications to the project may require subsequent entitlements.
- B3. The third story plate line shall not exceed 35 feet in height per the report and exhibits. The building and mechanical parapet shall not exceed the height as provided in the report and exhibits.
- B4. Landscaping, Walls and Fencing:
 - a. The project shall comply with the City's Tree Shading Ordinance which requires 50 percent shading of the parking area within 15 years.
 - b. All landscaping and planting shall conform to City standards for sight line requirements at intersections and driveways.
 - c. All mechanical equipment shall be located within enclosed cabinets or screened by landscaping and/or screening/fencing and meet Design Review Conditions of Approval.
 - d. Continuous concrete curbing shall be provided around all planter areas within or adjacent to parking lots and driveways.
 - e. Landscaping plans shall be submitted to the Building Division – Landscape Architecture Section of the Parks & Recreation Department. The scope of the review shall include plant species selection, landscape materials, irrigation system, and calculation to ensure that the 50% shading requirement is met.
 - f. Walls and fences shall be consistent with the Broadway/Stockton Special Planning District guidelines and shall conform to City standards for sight line requirements at intersections and driveways.
 - g. Fencing shall be decorative wrought iron or tubular steel painted to complement the buildings.

- B11. Prior to the issuance of any building permits, the applicant shall provide the City with a copy of the certificate of payment of school fees for the applicable school district(s).
- B12. The project shall reasonably maintain the buildings and landscaping.
- B13. The community amenities shall be used by tenants and their guests only.
- B14. The project shall meet all of the Design Review conditions of P10-042 as approved by the Design Commission.
- B15. Historic Acorn street lighting shall be required for all new street lights.

Development Engineering

- B16. Construct standard improvements as noted in these conditions pursuant to chapter 18 of the City Code. Improvements shall be designed and constructed to City standards in place at the time that the Building Permit is issued. All improvements shall be designed and constructed to the satisfaction of the Department of Transportation. Any public improvement not specifically noted in these conditions shall be designed and constructed to City Standards. This shall include street lighting and the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk fronting the property along Martin Luther King, Broadway and 39th Street per City standards and to the satisfaction of the Department of Transportation;
- B17. The applicant shall dedicate (If necessary) and construct full frontage improvements along the site's frontage on Martin Luther King JR. Boulevard. Improvements along MLK shall consist of a separated sidewalks, street lights and repair/reconstruction of the existing curb and gutter as shown on the site plan per City standards and to the satisfaction of the Department of Transportation;
- B18. The site plan shall conform to A.D.A. requirements in all respects. This shall include the replacement of any curb ramp that does not meet current A.D.A. standards. The affected ramps are located along the site's frontage at Broadway and 39th and MLK Jr. Boulevard;
- B19. The applicant shall repair/reconstruct any deteriorated portions of the existing alley fronting the property per City standards (in Concrete) and to the satisfaction of the Department of Transportation;
- B20. The design of walls fences and signage near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters.

Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height at maturity. The area of exclusion shall be determined by the Department of Transportation;

- B21. The project design is contingent upon the City Council's approval of the abandonment of the northern portion of the existing alley;
- B22. A parcel merger is required to facilitate the current project design. The applicant shall apply for a parcel merger prior to obtaining any building permits;
- B23. A revocable encroachment permit will be required for any doors, canopies, awnings, or any projections into the right of way. The projections cannot be permanent in nature and should be removable within a 24-hour notice to accommodate any City service work within the right-of-way;

Police

- B24. A 180 degree viewing device, such as a peephole, shall be installed in:
 - a. All solid doors.
 - b. Any office which contains a safe.
- B25. There shall be no video/arcade machines maintained upon the premises at any time.
- B26. Building Security
 - a. Alarms
 - 1. All alarm plans shall be approved by The Sacramento Police Department's Alarm Unit.
 - 2. Immediate access to detailed building and floor plans shall be made available to the Sacramento Police Department.
 - b. Surveillance Cameras
 - 1. Closed-circuit color video cameras shall be employed to monitor high risk areas such as the entrances and parking lot. Parking lot cameras often make an immense difference during the investigation of a crime. Additional cameras should also be placed to monitor patio areas.
 - 2. The recording device shall be a digital video recorder (DVR) capable of storing a minimum of 7 days worth of activity. (A DVR capable of storing 30 days worth of activity is preferable.) The camera recording system with monitors shall be controlled in the buildings fire control room.
- B27. Adequate signage shall be installed prohibiting trespassing, loitering, and noise in accordance with Section 602(k) of the California Penal Code and Section 9.16.140 of the Sacramento City Code.
 - a. The property should be posted for "No Trespassing" and sign an

agreement with the Police Department to prosecute all violators.

- b. This agreement shall be kept on file on the premises and in the Police Department.

Fire

- B28. All turning radii for fire access shall be designed as 35' inside and 55' outside. The turning radius shall apply to driveways located off MLK. The alley isn't required for an access point; however, the entrance from the alley shall be kept clear and accessible.
- B29. Provide the required fire hydrants in accordance with CFC 508 and Appendix C, Section C105.
- B30. Timing and Installation. When fire protection, including fire apparatus access roads and water supplies for fire protection, is required to be installed, such protection shall be installed and made serviceable prior to and during the time of construction.
- B31. Provide a water flow test. (Make arrangements at the Permit Center walk-in counter: 300 Richards Blvd, Sacramento, CA 95814). CFC 508.4
- B32. The furthest projection of the exterior wall of a building shall be accessible from within 150 ft of an approved Fire Department access road and water supply as measured by an unobstructed route around the exterior of the building. (CFC 503.1.1) This condition must be met by providing clear access from the parking lots, MLK, Broadway and 39th Streets. Man gates with access walkways will be required from all access points. In addition, dry standpipes may be required to meet the 150' hose pull.
- B33. Provide appropriate Knox access for site
- B34. Roads used for Fire Department access that are less than 28 feet in width shall be marked "No Parking Fire Lane" on both sides; roads less than 36 feet in width shall be marked on one side.
- B35. An automatic fire sprinkler system shall be installed in any portion of a building when the floor area of the building exceeds 3,599 square feet.
- B36. Locate and identify Fire Department Connections (FDCs) on address side of building no further than 50 feet and no closer than 15 feet from a fire hydrant.
- B37. Per 2007 California Fire Code Amendments; City Code 15.36.050 section 903.8 Fire control room, an approved fire control room shall be provided for all buildings protected by an automatic fire extinguishing system. Fire control rooms shall be located within the building at a location approved by the Chief, and shall be provided with a means to access the room directly from the exterior. Durable

signage shall be provided on the exterior side of the access door to identify the fire control room. CFC 903.8.

- B38. Provide at least 5 foot setback for second story and 8 foot setback for 3 third story bedroom windows to allow for fire ladder rescue operations. Provide clear access to buildings openings, free to landscaping and other obstructions. Exterior doors and openings required by this code or the Building Code shall be maintained readily accessible for emergency access by the Fire Department. CFC 504.1 This shall apply to all emergency escape windows for bedrooms no matter the location. These areas shall be kept clear of air conditioning units, shrubs on any other devices that may impede ladder access. This doesn't apply to actual street frontage property lines as long as clear access is available.
- B39. Minimum vehicle gate width shall provide 20 foot clear access. Gates across roads serving residential occupancies shall be AC powered operated and provided with Key override switch Knox and Radio Operated controller Click2Enter. Pedestrian gates shall be provided with a 4 foot minimum width and be capable of use with the Sacramento Fire Department Key Box key. Use of a key box to provide a key is acceptable. In addition, a pedestrian gate with a minimum 4' wide sidewalk shall also be provided on 39th for emergency access. Placement of this gate and access path shall be to the satisfaction of the Fire Department.

Utilities

- B40. Per City Code 13.04.070 and the Departments current Tap Policy, commercial lots may have more than one (1) domestic tap. All domestic water taps shall be metered. Excess services shall be abandoned to the satisfaction of the Department of Utilities.
- B41. Common area landscaping shall have a minimum of one (1) separate tap from the public distribution system for a metered irrigation service.
- B42. Multiple fire services are allowed per parcel and may be required.
- B43. Per City Code, the point of service for water, sewer and storm drain service is located at the back of curb for separated sidewalks or at the back of sidewalk for attached sidewalks. The onsite water, sewer and storm drain systems shall be private systems maintained by the association.
- B44. There is an existing sewer main in Martin Luther King Blvd and 39th St Alley. The sewer main in the alley shall be relocated to the satisfaction of the DOU. No service connections will be allowed to the relocated sewer main. Existing sewer services shall be re-connected to a public sewer main in Broadway or Martin Luther King Blvd.
- B45. The applicant shall provide a minimum of 20-foot wide easement for the new

sewer main to the satisfaction of the DOU.

- B46. If required by the DOU, the applicant shall enter into and record a Hold Harmless Agreement, in a form acceptable to the City Attorney, for any permanent structure (including without limitation concrete slabs, fences, decorative pavement, and similar structures) that will be constructed on top of the combined sewer system.
- B47. This project is served by the Combined Sewer System (CSS). Therefore, the developer/property owner will be required to pay the Combined Sewer System Development Fee (per City Council Resolution 2005-162) prior to the issuance of any building permit. The impact to the CSS due to the Equivalent Single-Family Dwelling (ESD) unit is estimated to be 44 ESD. The Combined Sewer System fee at time of building permit is estimated to be \$56,536.39 plus any increases to the fee due to inflation and credit for existing sanitary sewer flows from the site. The fee will be used for improvements to the CSS.
- B48. Prior to or concurrent with the submittal of improvement plans, a combined sanitary sewer study is required and shall be approved by the DOU. The study shall provide an analysis of the pre and post development condition of both the sewer and drainage flow that is contributing to the combine system. Based on the analysis, the applicant may be required to provide onsite storage or upsize the existing 8" combined sanitary main in the Alley. Sufficient off-site and on-site spot elevations shall be provided in the study to determine the direction of storm drain runoff.
- B49. Onsite sewer and drainage mains shall be separate systems.
- B50. If the paved area is greater than 6,000 square feet, then an onsite surface drainage system is required and shall be connected to the street drainage system by means of a storm drain service tap. All onsite systems shall be designed to the standard for private storm drainage systems (per Section 11.12 of the Design and Procedures Manual) (Note: Onsite drainage systems for lots fronting Martin Luther King Blvd. (MLK Blvd.), should connect to the existing drainage main in MLK Blvd.)
- B51. Lot pad elevations shall be a minimum of 1.5 feet above the controlling overland release elevation and a minimum of 1.2 feet above the highest adjoining back of sidewalk elevation. Finished lot pad elevations shall be accepted by the Department of Utilities.
- B52. The applicant may not develop the project in anyway that obstructs, impedes, or interferes with the natural flow of existing off-site drainage that crosses the property. The project shall construct the required public and/or private infrastructure to handle off-site runoff to the satisfaction of the DOU. If private infrastructure is constructed to handle off-site runoff, the applicant shall dedicate the required private easements and/or, at the discretion of the DOU, the

applicant shall enter into and record and Agreement for Maintenance of Drainage with the City, in a form acceptable to the City Attorney.

- B53. A grading plan showing existing and proposed elevations is required. Adjacent off-site topography shall also be shown to the extent necessary to determine impacts to existing surface drainage paths. No grading shall occur until the grading plan has been reviewed and approved by the Department of Utilities.
- B54. The applicant must comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinance. This ordinance requires the applicant to prepare erosion and sediment control plans for both during and after construction of the proposed project, prepare preliminary and final grading plans, and prepare plans to control urban runoff pollution from the project site during construction.
- B55. Post construction, storm water quality control measures shall be incorporated into the development to minimize the increase of urban runoff pollution caused by development of the area. Since the project is in the combined sewer system area, only source control measures are required. Storm drain message is required at all drain inlets. Improvement plans must include the source controls measures selected for the site. Refer to the latest edition of the "Stormwater Quality Design Manual", dated May 2007 for appropriate source control measures.

Advisory Notes for the Project:

Utilities

- B56. The applicant shall comply with the abandonment process.
- B57. If there is a gate for this development, then the DOU may require the applicant to provide an access key for maintenance and repair of the existing underground utilities.
- B58. Many projects within the City of Sacramento require on-site booster pumps for fire suppression and domestic water system. Prior to design of the subject project, the Department of Utilities suggests that the applicant request a water supply test to determine what pressure and flows the surrounding public water distribution system can provide to the site. This information can then be used to assist the engineers in the design of the on-site domestic, irrigation and fire suppression systems.
- B59. The applicant is responsible for the protection and repair of the existing City combined sanitary sewer and water mains in Alley during construction of the proposed structure. Contact Underground Service Alert at 1-800-642-2444, 48 hours before work is to begin.
- B60. The proposed project is located in the 100-year floodplain, designated as Shaded X zone on the Federal Emergency Management Agency (FEMA)

Federal Insurance Rate Maps (FIRMs). Within the Shaded X zone, there are no requirements to elevate or flood proof.

Building

- B61. It appears that the building will need to be of Type V-A construction.
- B62. Provide an elevator lobby per CBC Section 707.14.1.
- B63. The central stair is open to more than two stories. It must comply with CBC Sections 1020 and 707.2 which require an exit enclosure with limited exception.
- B64. It appears that the top landing at the Southwest stair is too small. Please review and revise accordingly.
- B65. Provide accessibility per CBC Chapter 11A. This will require all of the units to be adaptable and all public and common use areas to be accessible. Identify the accessible path of travel on the plans. It doesn't appear that the rose walk is currently accessible. Please review and revise accordingly.
- B66. The plans are not currently detailed sufficiently to provide complete comments.

Parks

- B67. As per City Code, the applicant will be responsible to meet his/her obligations regarding Title 18, 18.44 Park Development Impact Fee (PIF), due at the time of issuance of building permit. The Park Development Impact Fee due for this project is estimated at \$80,372. This is based on 56 multifamily residential units at the specified infill rate of \$1,425 per unit and 3,362 square feet of commercial retail at the specified infill rate of \$0.17 per square foot. Any change in these factors will change the amount of the PIF due. The fee is calculated using factors at the time that the project is submitted for building permit.

C. The Special Permit to reduce required parking for senior housing is approved subject to the following conditions:

Planning

- C1. Prior to issuance of a Building Permit the applicant will submit a Transportation Management Plan for review and approval by the City's Alternate Modes Coordinator and Planning Director or show compliance with an existing AQTMP.
- C2. Vehicular parking shall be provided per section 17.64.030 of the Zoning Ordinance. A total of 41 parking spaces shall be provided per the report and exhibits.

- C3. Bicycle parking facilities shall be provided per section 17.64.050 of the Zoning Ordinance. A total of 5 bicycle facilities are required for this project and shall be located in close proximity to each of the apartment buildings and the community building.
- C4. The applicant shall provide a minimum of two striped turn around parking spaces along the street side of the fence and automatic gate.
- C5. Auto repairs are not permitted at any time, except in emergencies, such as flat tire or dead battery.
- C6. Visitor parking shall be strictly enforced.

Development Engineering

- C7. The applicant shall construct a raised median along Martin Luther King Boulevard to restrict the two proposed driveways movements to right-in and right-out only as shown on the site plan per City standards and to the satisfaction of the Department of Transportation. This shall include any needed restriping of MLK Jr. Boulevard to accommodate the median;
- C8. The applicant may construct a curb cut-out along the building's frontage on Broadway to act as an optional drop-off area for the residents. The drop off area shall be a minimum length of 35-feet and shall have a curb ramp in the middle to be ADA compliant per City standards and to the satisfaction of the Department of Transportation;
- C9. The applicant shall provide appropriate signage to restrict parking for a minimum distance of 20-feet south of the southern-most driveway along Martin Luther King Boulevard to facilitate driveway movements to the satisfaction of the Department of Transportation;
- C10. All proposed vehicular gates along Martin Luther King Jr. Boulevard must be located a minimum of 20-feet behind the right-of-way and shall be equipped with a Knox lock for Fire Department access. The applicant shall hatch out one parking space immediately adjacent to the gate in the southern most parking lot to act as a turn around to prevent vehicles from backing out onto MLK Jr. Boulevard;
- C11. The site plan shall conform to the parking requirements set forth in chapter 17.64.030 of City Code (Zoning Ordinance). If the applicant proposes any off-site parking, an **ADA compliant path** shall be provided between the two sites to the satisfaction of the Department of Transportation;
- C12. All new and existing driveways shall be designed and constructed to City Standards and to the satisfaction of the Department of Transportation;

Fire

- C13. Roads used for Fire Department access shall have an unobstructed width of not less than 20' and unobstructed vertical clearance of 13'6" or more.
- C14. Fire Apparatus access roads shall be designed and maintained to support the imposed loads of fire apparatus and shall be surfaced so as to provide all-weather driving capabilities. CFC 503.2.3

D & E. The **Variiances** to reduce the setbacks from 5' to 0' along Broadway and to reduce the setback from 20' to 0' along Martin Luther King Jr. are **approved** subject to the following conditions:

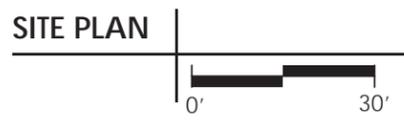
Planning

- D&E1. The setback shall be reduced from 5' to 0' along Broadway per the report and exhibits to be consistent with the Broadway/Stockton Special Planning District guidelines.
- D&E2. The setback shall be reduced from 20' to 0' along Martin Luther King, Jr. Boulevard per the report and exhibits to allow for greater outdoor amenities.



PROJECT INFORMATION

56	DWELLING UNITS
42	PARKING SPACES
3,000 - 4,000 SF	RETAIL/COMMERCIAL



DRAWING INDEX

01	ILLUSTRATIVE SITE PLAN
02	DISTRICT CONTENT
03	SITE ANALYSIS
04	PROJECT CONTEXT PHOTOS
05	BUILDING ELEVATIONS
06	ANNOTATED SITE PLAN
07	FIRST FLOOR PLAN
08	SECOND FLOOR PLAN
09	THIRD FLOOR PLAN
10	PRELIMINARY LANDSCAPE PLAN
11	REQUIRED SETBACKS & EASMENTS



BROADWAY AND STOCKTON BLVD. SHOPPING DISTRICT/GROCERY STORE/PHARMACY
SACRAMENTO COUNTY PRIMARY CARE CLINIC
UCD MEDICAL CENTER

HISTORIC OAK PARK SHOPPING AND BUISNESS DISTRICT

CURTIS PARK

AMERICAN LEGION HIGH SCHOOL

BANK OF AMERICA

MCCLATCHY PARK

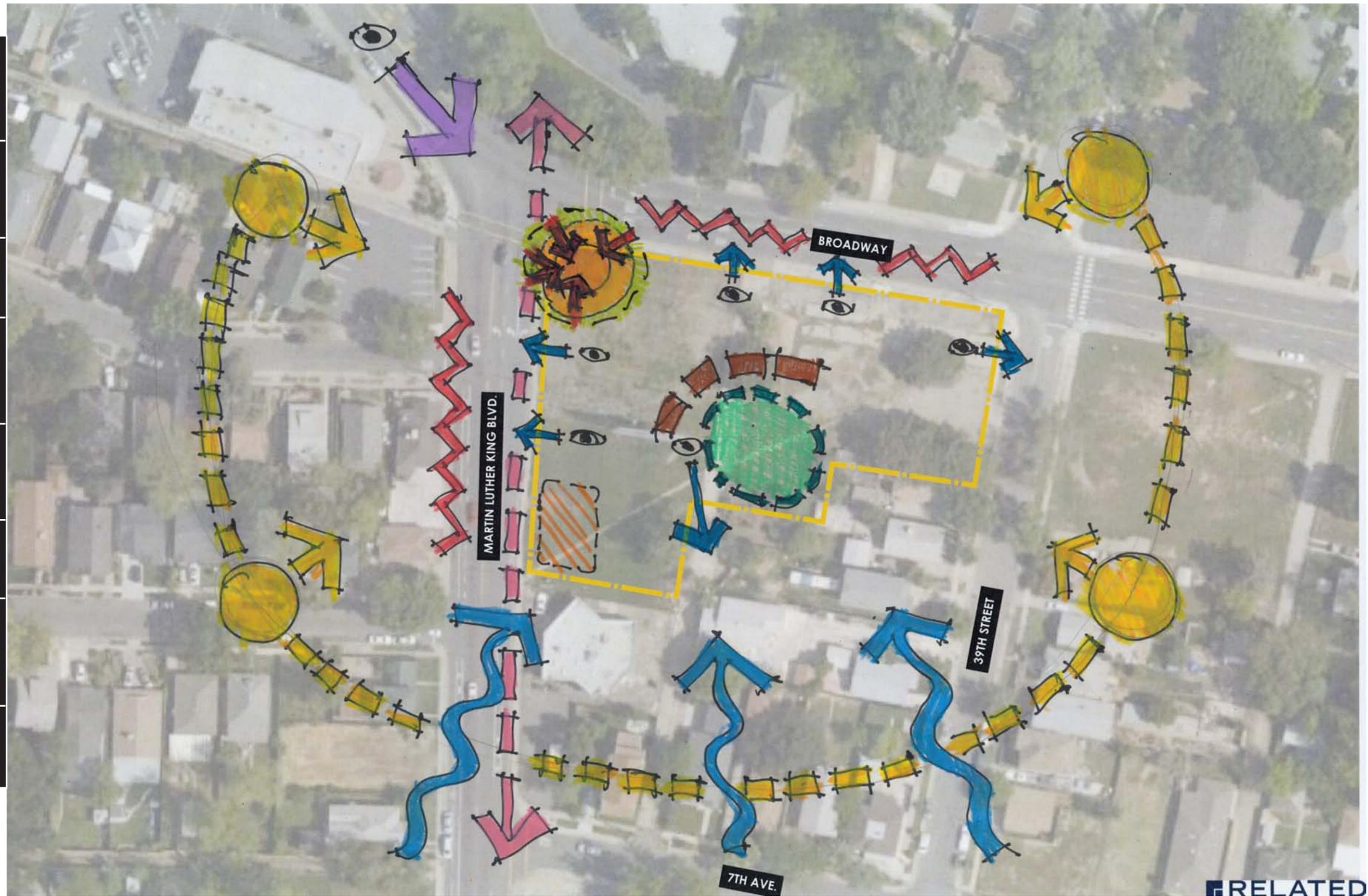
OAK PARK CENTER



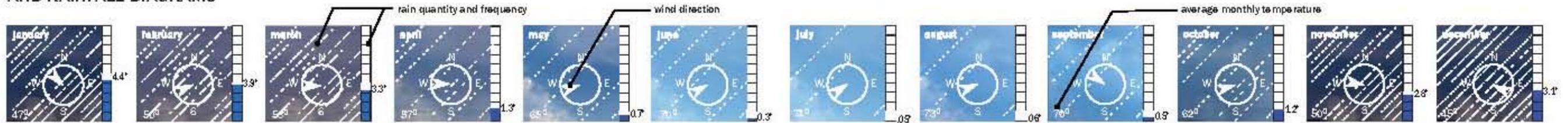


LEGEND

	MAJOR AUTOMOTIVE VIEW
	AUTOMOBILE PATH OF TRAVEL
	NOISE FROM STREET
	PRIVATE ENCLOSED GREEN SPACE
	PUBLIC PLAZA
	VIEW TO THE STREET
	PATH OF SUN
	COOLING DELTA BREEZE



AVERAGE TEMPERATURE WIND AND RAINFALL DIAGRAMS





1 MLK looking north from 7th Ave.



2 MLK looking north from 7th Avenue



3 Bigler Way looking east to project site



4 From project site looking west to Bigler Way



5 From project site looking north down Broadway



6 From project site looking north to Americal Legion High School (on the left)



7 Broadway looking southeast to corner of project site



8 American Legion High School looking south to site



9 View south to center of project site



10 39th Street and Broadway looking east



11 39th Street looking southeast to the project site



12 39th Street and Broadway looking northwest



13 39th Street looking south to project site



14 Broadway at mid-site looking east



15 39th Street and Broadway looking southwest to project site



16 39th Street looking east



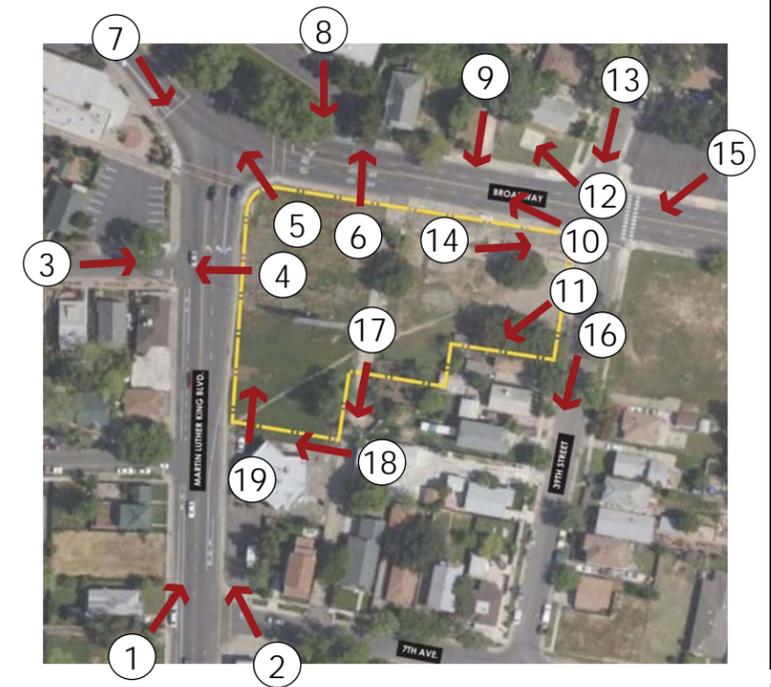
17 View down Alley looking south



18 South property line looking west



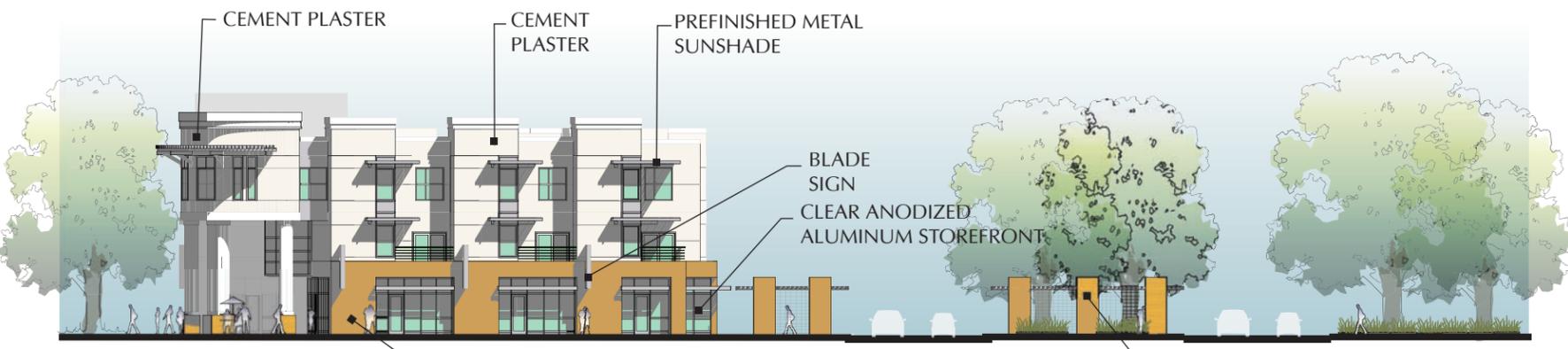
19 South corner of site looking north on MLK



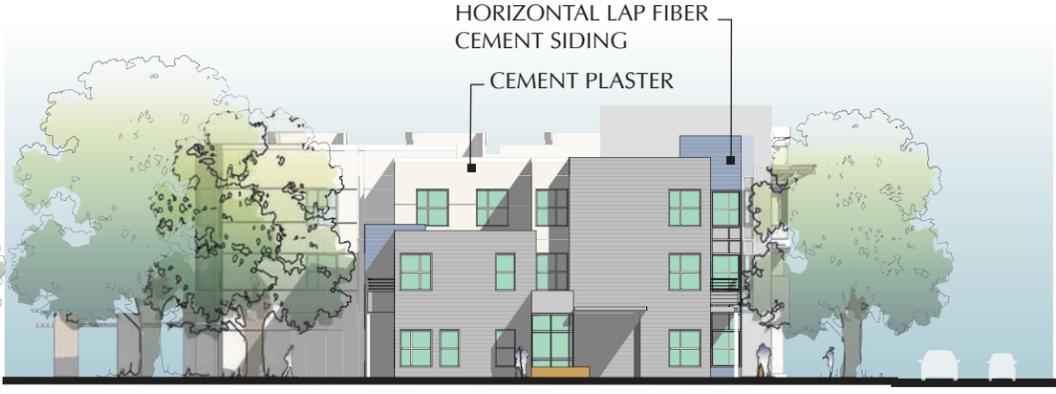


BROADWAY ELEVATION

NOTE:
ALUMINUM STOREFRONT WILL
BE USED AT RETAIL AND
RESIDENTIAL COMMON AREAS
ONLY; THE REST WILL BE VINYL
SINGLE-HUNG RESIDENTIAL
WINDOWS.



MARTIN LUTHER KING ELEVATION



39TH STREET ELEVATION



COURTYARD ELEVATION



BROADWAY ENLARGED ELEVATION - CORNER PLAZA | OPTION A



MARTIN LUTHER KING ENLARGED ELEVATION - CORNER PLAZA | OPTION A



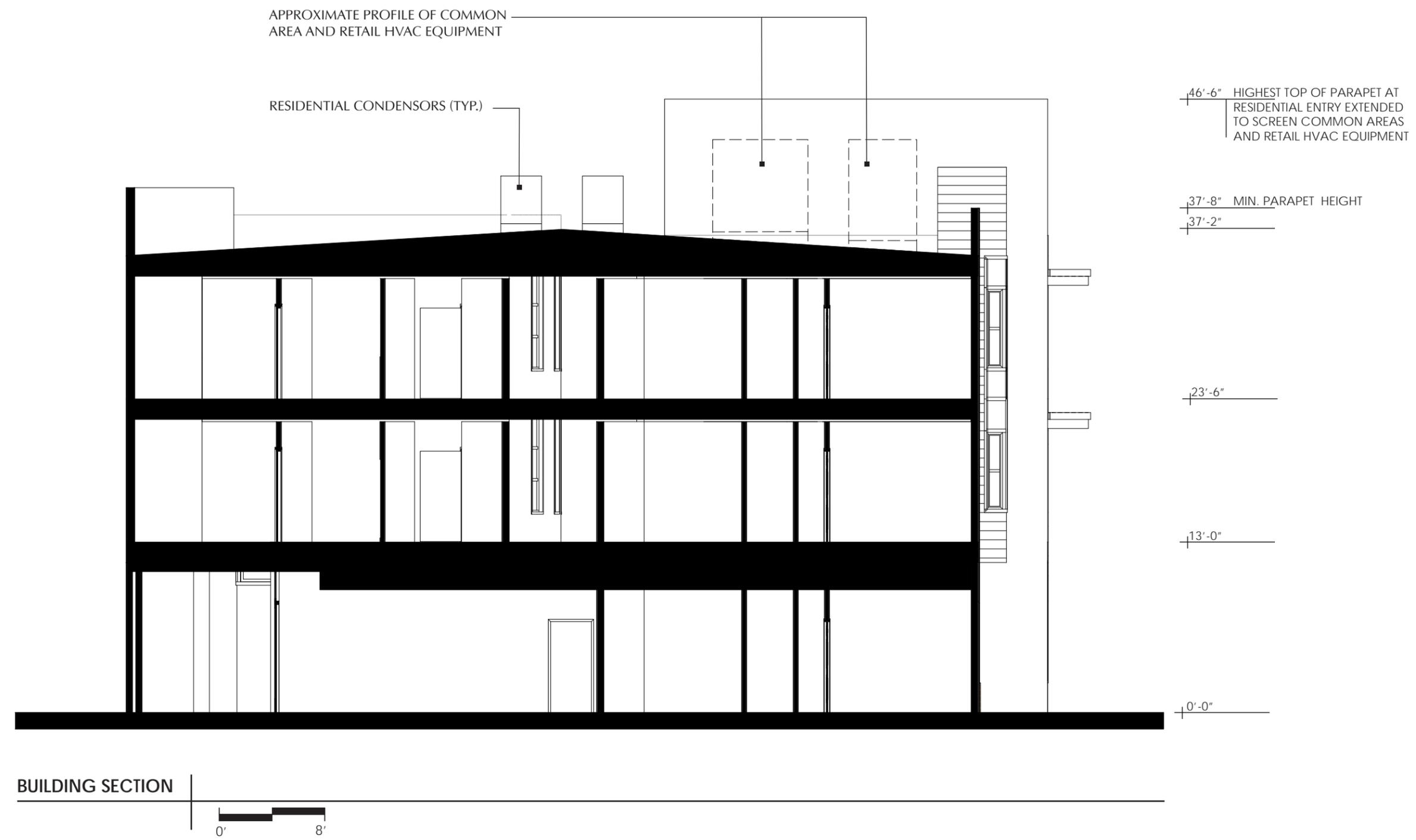


BROADWAY ENLARGED ELEVATION - CORNER PLAZA | OPTION B



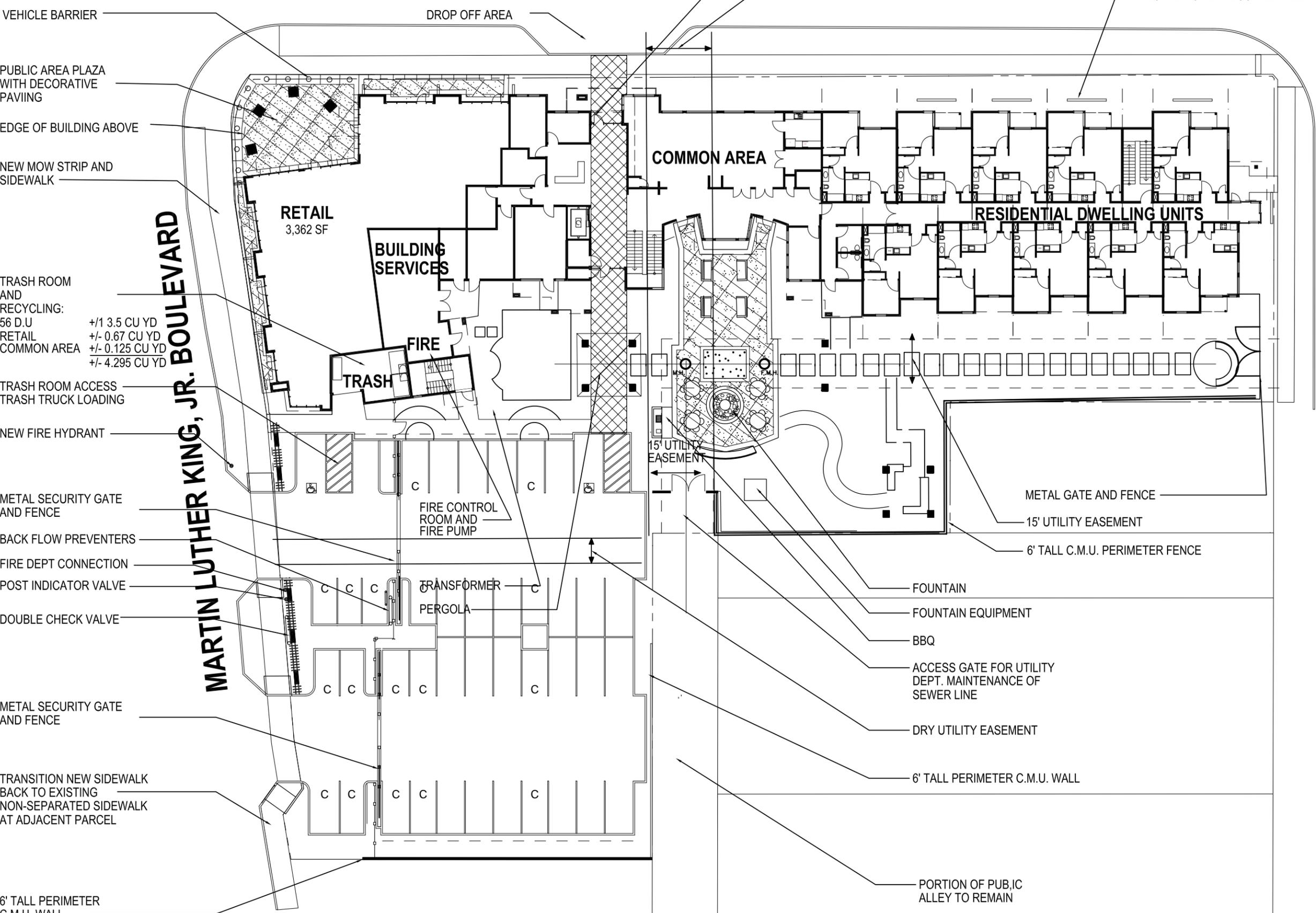
MARTIN LUTHER KING ENLARGED ELEVATION - CORNER PLAZA | OPTION B







BROADWAY



- VEHICLE BARRIER
- PUBLIC AREA PLAZA WITH DECORATIVE PAVING
- EDGE OF BUILDING ABOVE
- NEW MOW STRIP AND SIDEWALK
- TRASH ROOM AND RECYCLING: 56 D.U.
- TRASH ROOM ACCESS TRASH TRUCK LOADING
- NEW FIRE HYDRANT
- METAL SECURITY GATE AND FENCE
- BACK FLOW PREVENTERS
- FIRE DEPT CONNECTION
- POST INDICATOR VALVE
- DOUBLE CHECK VALVE
- METAL SECURITY GATE AND FENCE
- TRANSITION NEW SIDEWALK BACK TO EXISTING NON-SEPARATED SIDEWALK AT ADJACENT PARCEL
- 6' TALL PERIMETER C.M.U. WALL

MARTIN LUTHER KING, JR. BOULEVARD

RETAIL
3,362 SF

BUILDING SERVICES

TRASH

FIRE

FIRE CONTROL ROOM AND FIRE PUMP

TRANSFORMER PERGOLA

COMMON AREA

RESIDENTIAL DWELLING UNITS

15' UTILITY EASEMENT

METAL GATE AND FENCE

15' UTILITY EASEMENT

6' TALL C.M.U. PERIMETER FENCE

FOUNTAIN

FOUNTAIN EQUIPMENT

BBQ

ACCESS GATE FOR UTILITY DEPT. MAINTENANCE OF SEWER LINE

DRY UTILITY EASEMENT

6' TALL PERIMETER C.M.U. WALL

PORTION OF PUBLIC ALLEY TO REMAIN

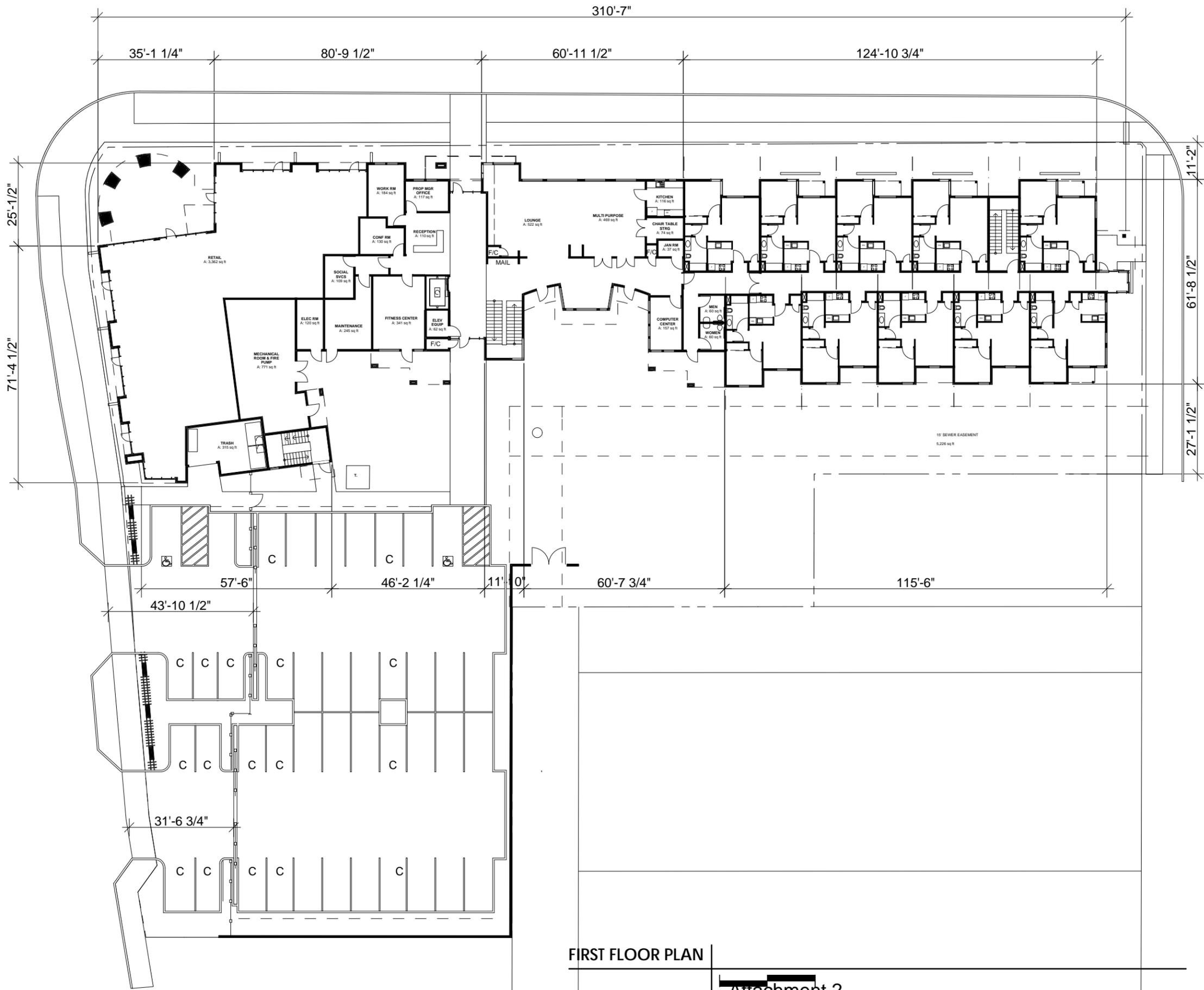
ALLEY TO BE ABANDONED

LOW BRICK LANDSCAPE WALL

39TH STREET

ANNOTATED SITE PLAN



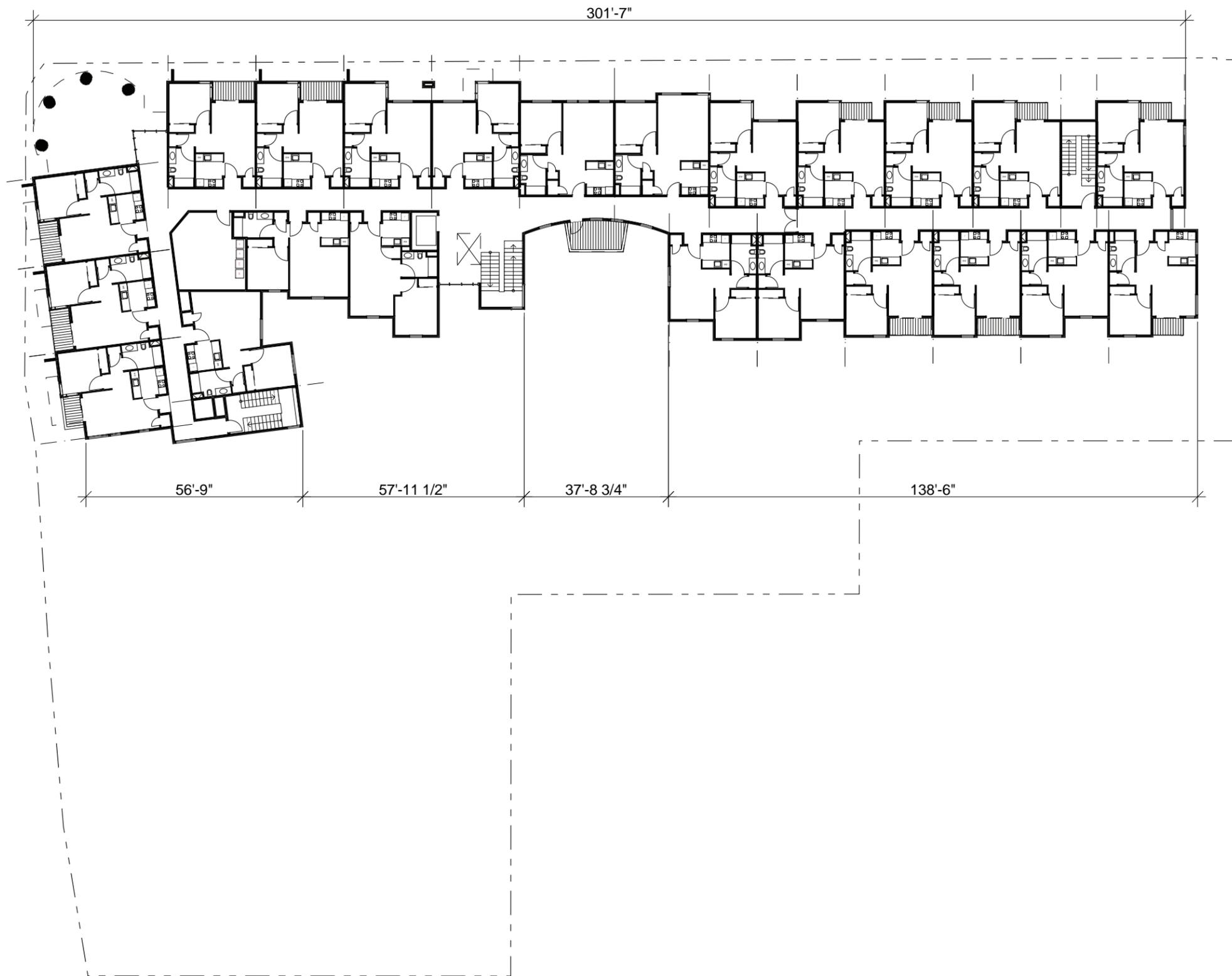


FIRST FLOOR PLAN

Attachment 2
Exhibit 10



Item #5



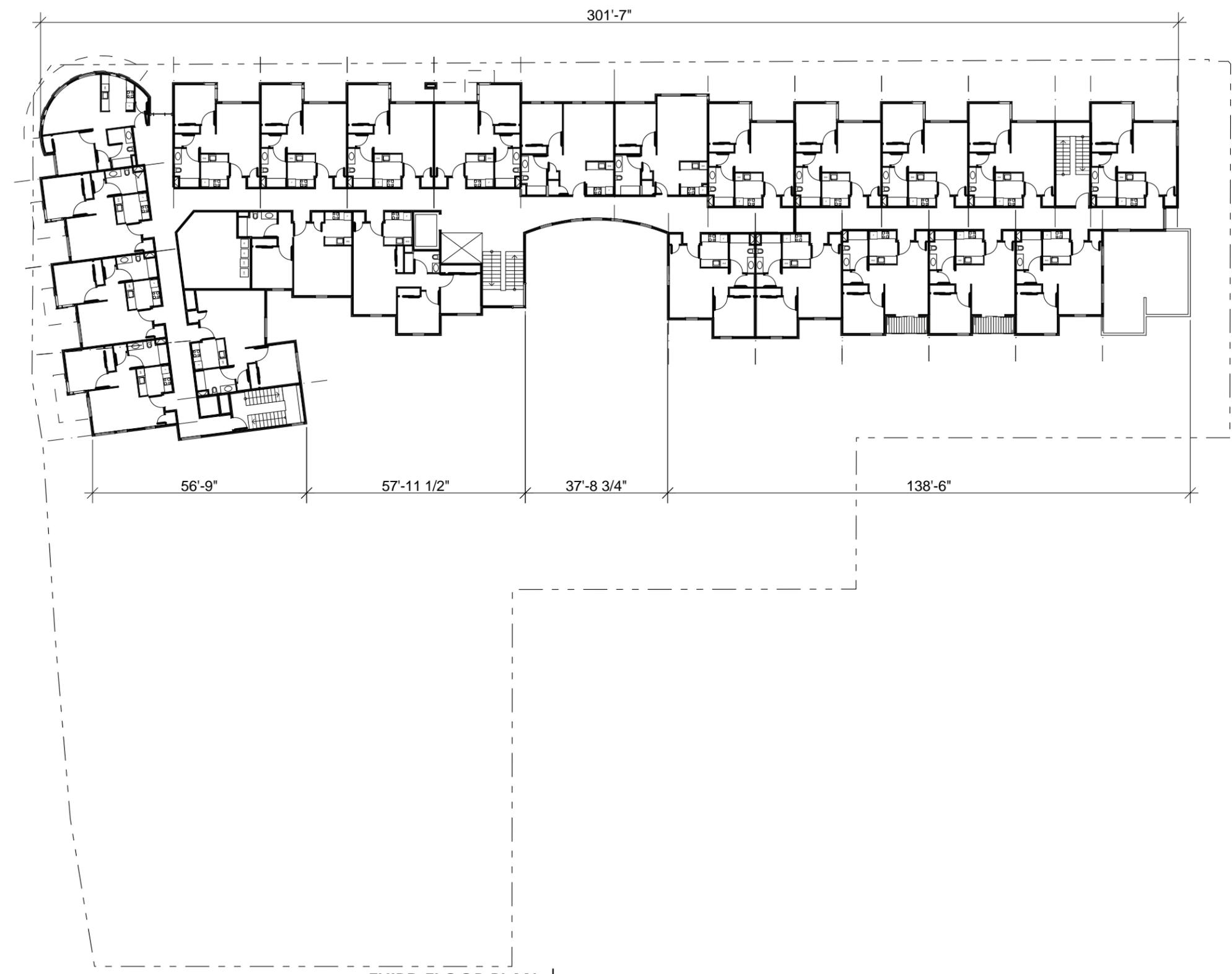
SECOND FLOOR PLAN

Attachment 2
Exhibit 11



Item #5



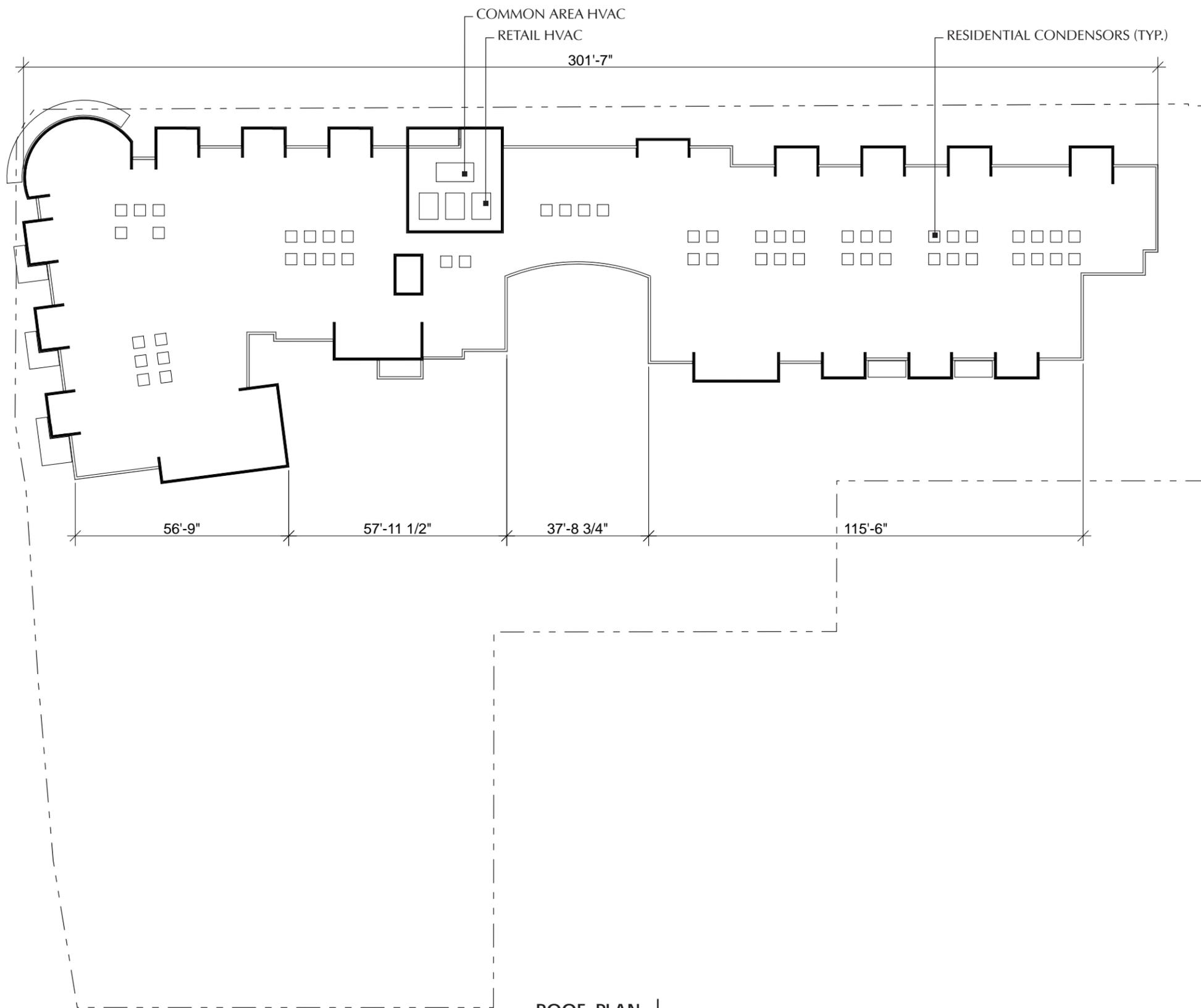


THIRD FLOOR PLAN



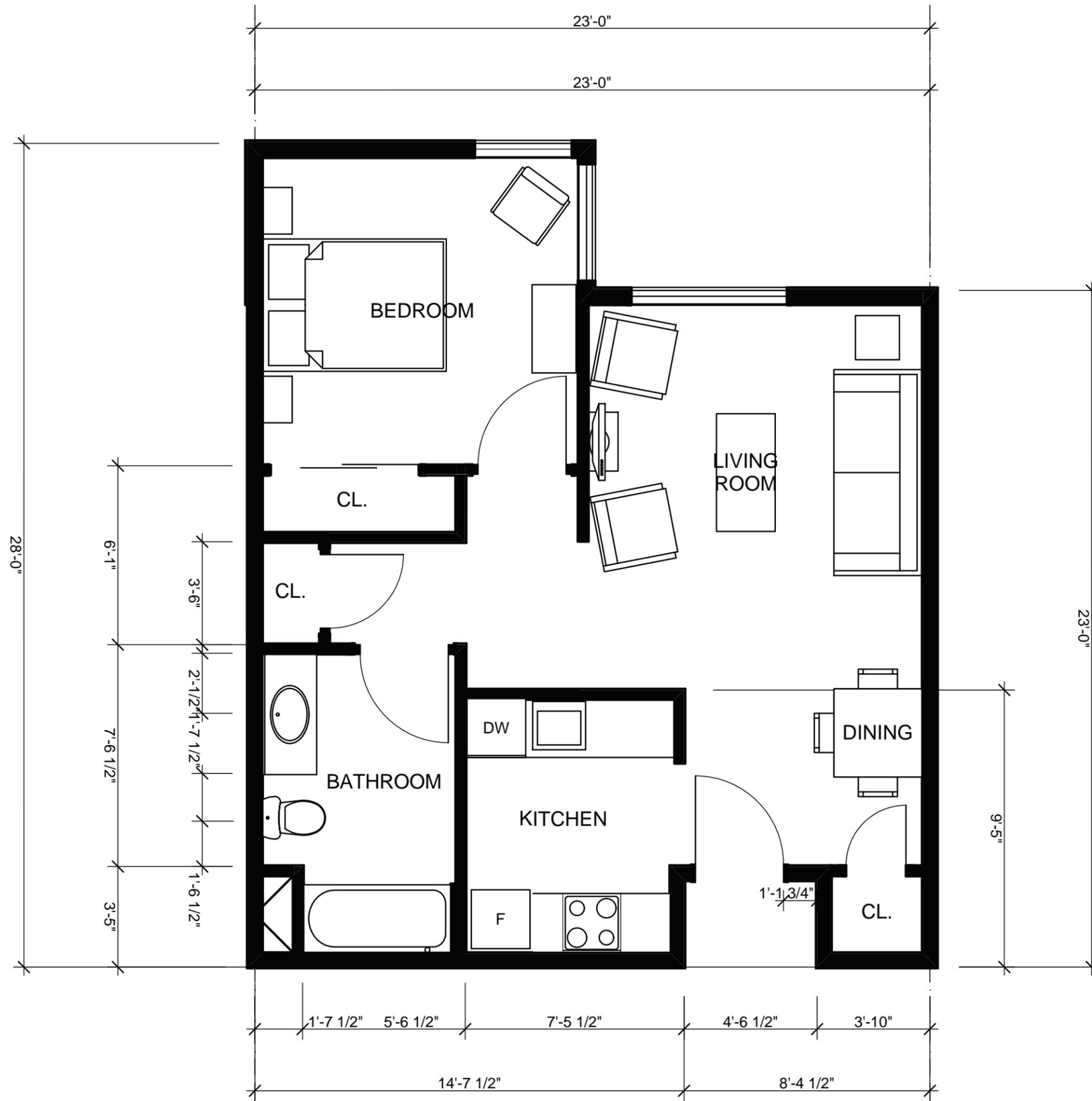
Attachment 2
Exhibit 12





ROOF PLAN

0' 30'
Attachment 2
Exhibit 13



PROTOTYPICAL UNIT



Attachment 2
Exhibit 14



Item #5



POTENTIAL PLANT LIST

- TREES**
- VERTICAL TREE ALLE' OVER CARPET ROSES
 - 15 GAL. CB CARRINUS BETULUS 'COLUMNARIIS', COLUMNAR EUROPEAN HORNBREAM
 - 15 GAL. CB CARRINUS BETULUS 'COLUMNARIIS', COLUMNAR EUROPEAN HORNBREAM
 - STREET TREE IN LAWN STRIP
 - 15 GAL. PL. PLATANUS ACERIFOLIA 'YARWOOD' SYCAMORE OR
 - 15 GAL. PL. QUERCUS MACROCARPA, BURK OAK
 - PARKING SHADE TREE
 - 24" box PS PISTACHIA CHINENSIS, CHINESE PISTACHE OR
 - 24" box ZS ZELKOVA SERRATA, ZELKOVA
 - PATIO TREE IN TREE GRATES OVER PAVING
 - 15 GAL. PK PYRUS KARAKAMI, EVERGREEN PEAR
 - 15 GAL.
 - SMALL ACCENT TREE
 - 15 GAL. AC ACER PALMATUM PURPUREUM, PURPLE LEAF JAPANESE MAPLE
 - 15 GAL. RM RAPHIOLEPIS 'MAJESTIC BEAUTY' - STANDARD
 - 15 GAL. CB CARRINUS BETULUS 'COLUMNARIIS', COLUMNAR EUROPEAN HORNBREAM
- SHRUBS**
- 5 GAL. AZ AZALEA SOUTHERN INDICA - 'FELDERS WHITE'
 - 2 GAL. BG BERGENIA CORDIFOLIA
 - 5 GAL. CA CAMELLIA JAPONICA 'OLEOPATRA'
 - 5 GAL. OR GARDENIA VICTORIS
 - 5 GAL. LO LOROPETALUM CHINENSE 'PURPUREA'
 - 5 GAL. PK PACHYRIZOM TENUIS 'TOM THUMB' FLAX
 - 2 GAL. RC ROSA, PINK, RED, YELLOW AND WHITE CARPET ROSE
 - 5 GAL. RA RAPHIOLEPIS INDICA 'ENCHANTRESS' INDIAN HAWTHORNE
 - 5 GAL. RT ROSEMARINUS 'TUSCAN BLUE, ROSEMARY'
 - 5 GAL. SY SYRINGA VILGANS, LILAC 'LAVENDER LADY'
 - 5 GAL. VT VIBURNUM THUNIS, DWARF LAURESTINUS
- GROUNDCOVERS**
- 1 gal @ 30" o.c. M MYOPORIUM PARVIFOLIUM
 - 1 gal @ 18" o.c. V VINCA MINOR
- LAWN AREA**
- 500 L 95% 'SHORT STOP' TURF TYPE - 5% MERT KENTUCKY BLUE GRASS

WATER USE CALCULATIONS

Estimated areas of water usage, for compliance with Water Conservation Ordinance Requirements.

Water Use	Area	Subtotal	Total
Low	15,250 S.F.	X .4 =	6,100 S.F.
MEDIUM	3,400 S.F.	X .6 =	2,040 S.F.
HIGH	4,950 S.F.	X 1 =	4,950 S.F.
Total:	23,600 S.F.		13,090 S.F.

(Site area greater than)

SHADE CALCULATIONS for less than 50 car parking lot

Total Paving:	12,360 S.F.
Paving Requiring Shade:	6,180 S.F.
Minimum 50% Shading Req:	6,252 S.F. (50.5%)
Plan Shows:	6,252 S.F.

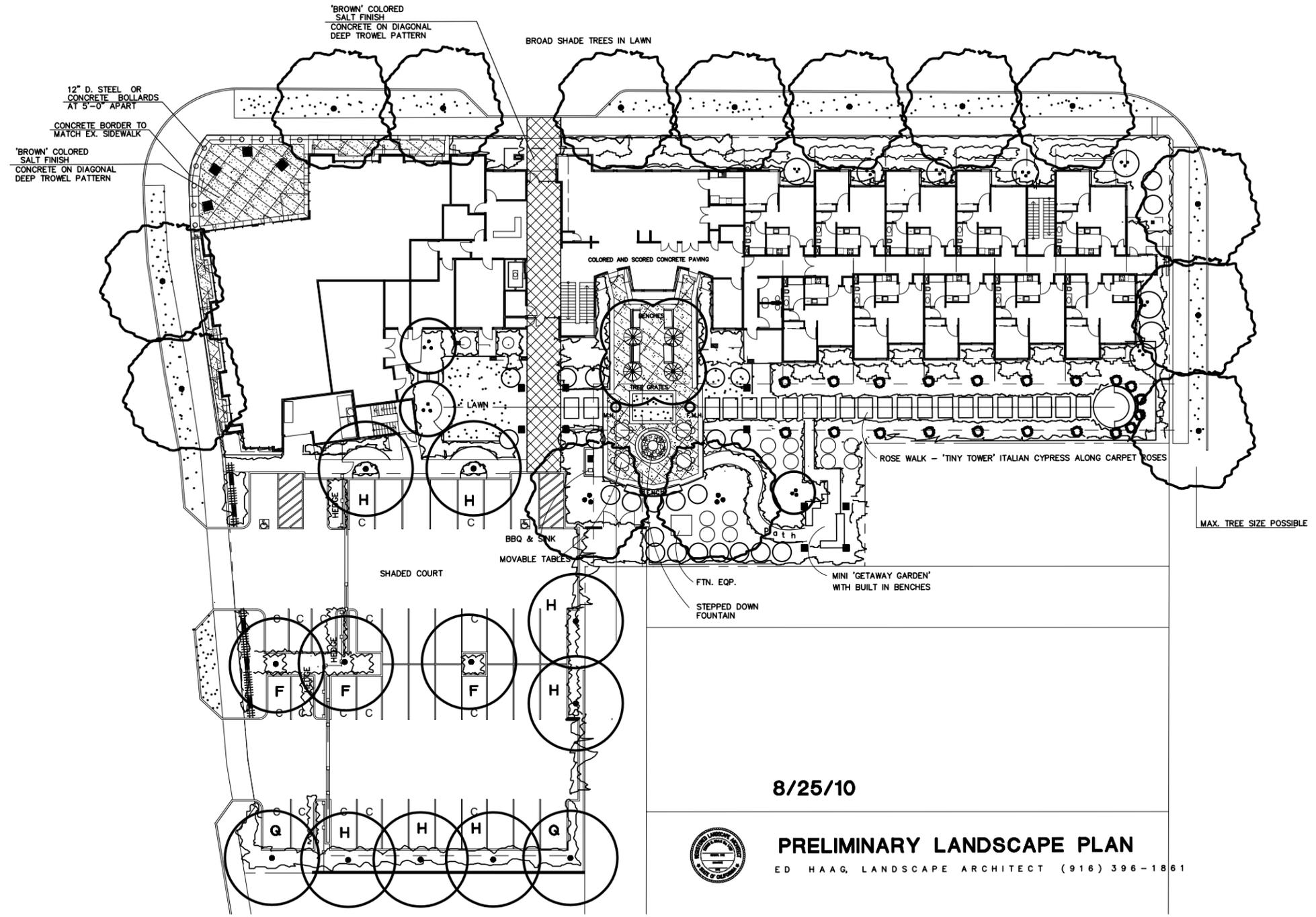
PISTACHE OR ZELKOVA

3 F @ 962 S.F.	=	2,886 S.F.
6 H @ 481 S.F.	=	2,886 S.F.
2 Q @ 240 S.F.	=	480 S.F.
TOTAL	=	6,252 S.F.

PLANTING NOTES

- An automatic irrigation system shall be fully operational, and coverage approved prior to installation of planting.
- Finish grade at edge of all planting areas shall be 2" below top of adjacent curbs, walls or paving.
- All shrub and groundcover areas shall receive 2" layer of shredded bark mulch topdressing as specified.
- Fertilize individual plants after installation with Osmocote time release formula granulated fertilizer, per manufacturer's instructions.
- Pre-planting soil preparation shall include cultivating all planted areas down to 12 inch depth, and incorporating the following amendments:

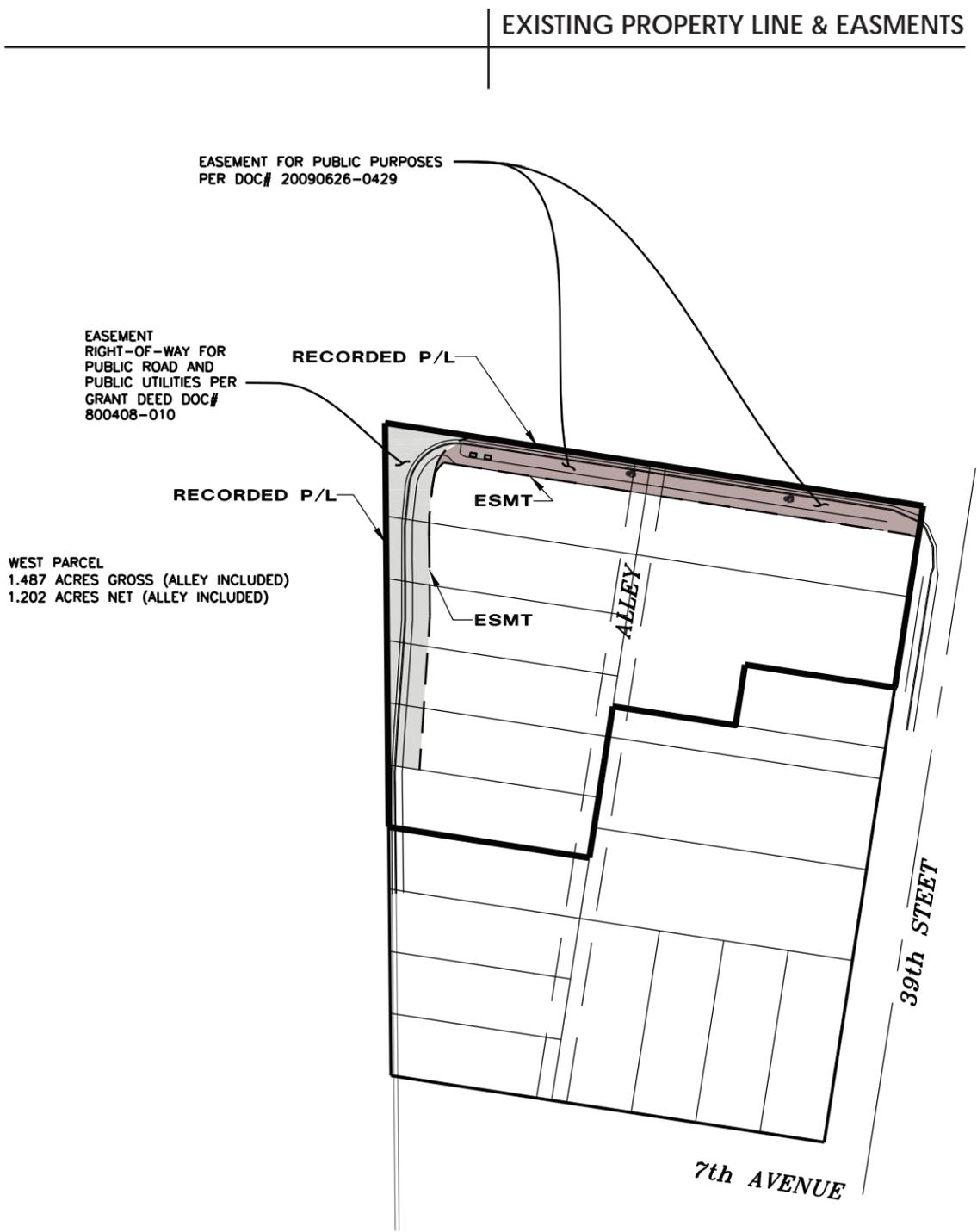
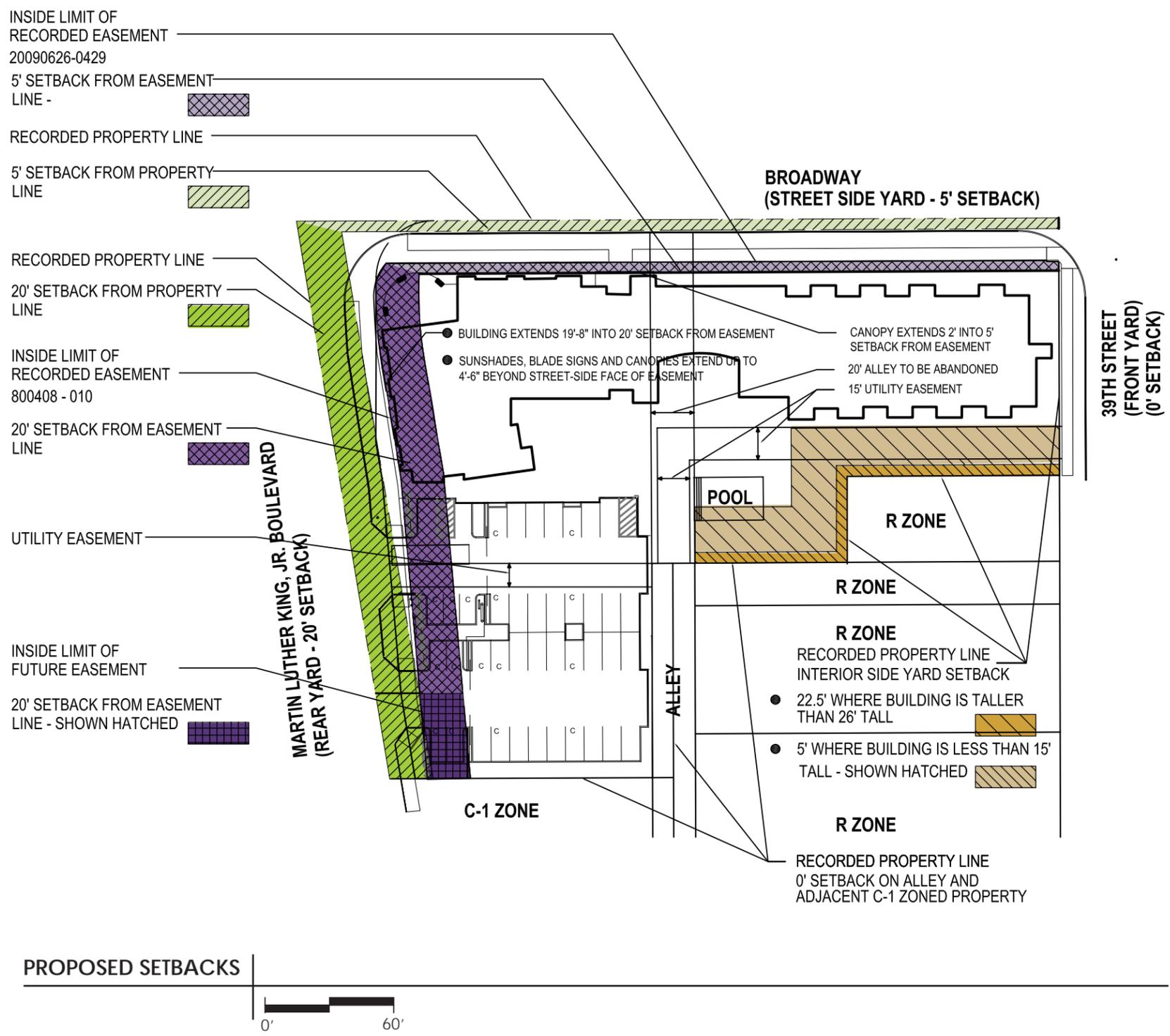
Amendment or fertilizer	Application Rate
Nitrogen stabilized compost	4% vol. per 1,000 s.f.
Ammonium Phosphate (6-20-0)	10 lbs. per 1,000 s.f.
- GUARANTEE PRODUCTS AND INSTALLATION OF PLANTING AND IRRIGATION FOR A PERIOD OF ONE YEAR AFTER ACCEPTANCE OF THE 90 DAY MAINTENANCE PERIOD.



8/25/10

PRELIMINARY LANDSCAPE PLAN
ED HAAG, LANDSCAPE ARCHITECT (916) 396-1861





August 30, 2010

Lindsey Alagozian
City of Sacramento
300 Richards Blvd., 3rd Floor
Sacramento, CA 95811

RE: Supplemental Information in Support of Retail Parking Reduction
Oak Park Mixed Use Senior Development

Dear Lindsey:

Per our conversation on July 28th, I am submitting this supplemental information in support of my request for a reduction in the retail parking requirements at the Oak Park Mixed-Use Development. The information within this document augments the parking reduction application that was submitted to you previously, dated May 19th, 2010.

The Project is requesting that the retail parking requirements be reduced to 1 parking space per 1,000 square feet of net rentable retail space. This request is based upon the following salient facts:

1. Retail is Neighborhood Serving

Given the Project's location and the types of retail within the vicinity of the project, the retail at the Project is anticipated to be neighborhood service (vs. destination retail) and the vehicular demands are expected to be significantly reduced. Most of the customers to the retail will be predominantly neighbors residing within a brisk walk of the Project. Attracting customers from much farther than a quarter mile radius is improbable.

2. Market Competitiveness of Project's Proposed Levels of Parking

A survey of the existing retail within close vicinity of the project shows that the vast majority of the existing retail in the area doesn't have dedicated retail parking and relies almost entirely on street parking. Examples of this include:

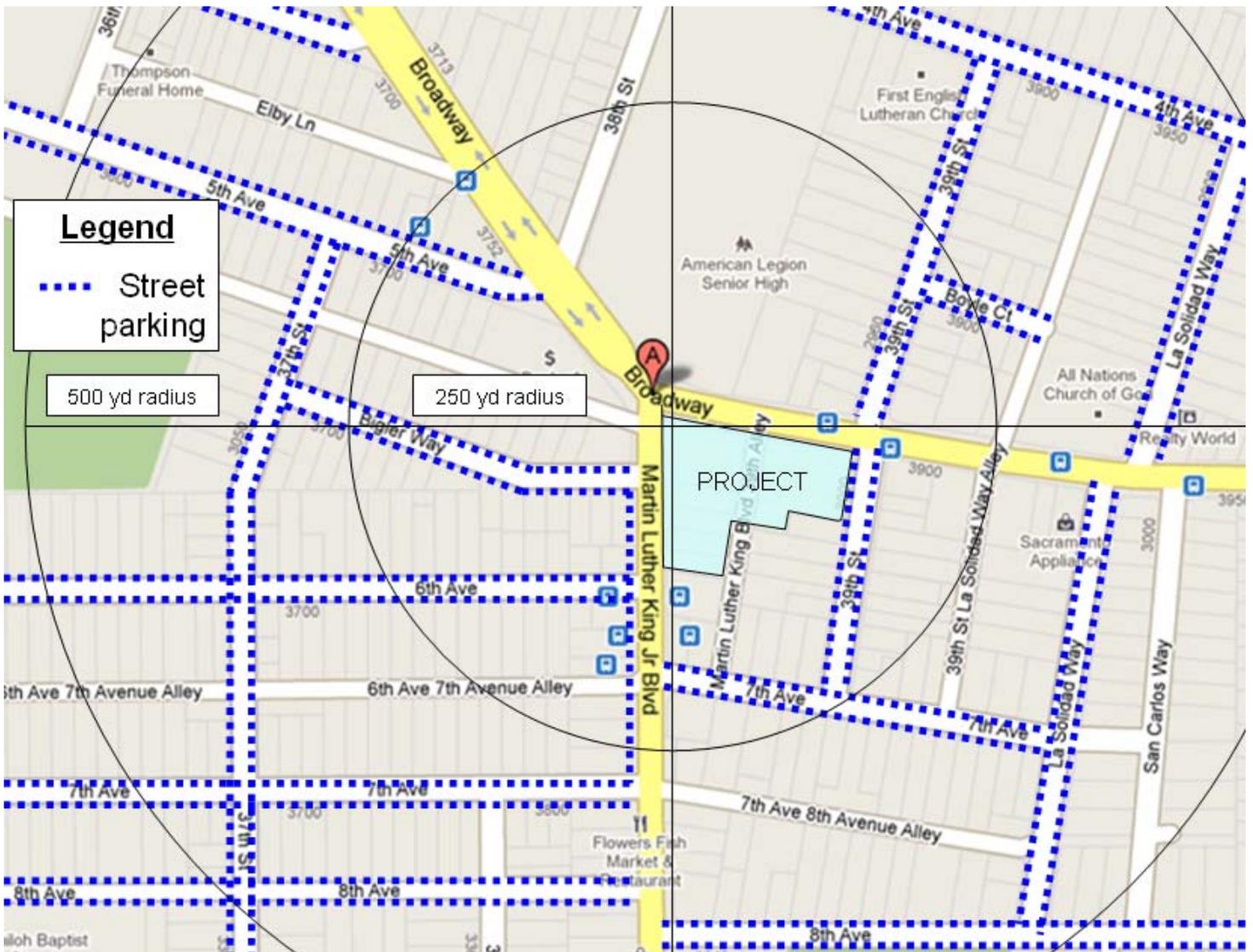
- Old Soul Café at 3456 3rd Avenue has no dedicated off-street retail parking.
- Broadway Soul Food at 3515 Broadway has no dedicated off-street retail parking.
- BFD at 3528 Broadway has no dedicated off-street retail parking.
- US Bank at 3418 Broadway has no dedicated off-street retail parking.
- Underground Books at 2814 35th street has no dedicated off-street retail parking.
- The Guild Theater at 2828 35th street has no dedicated off-street parking.
- Smoochum's BBQ at 3060 Martin Luther King has no dedicated off-street parking.
- Dry Clean Today at 3519 Broadway has no dedicated off-street parking.
- Thom's Cyclery at 3401 2nd Avenue has no dedicated off-street parking.
- Robin's Nest at 3230 Broadway has no dedicated off-street parking.

The Project's positioning with dedicated parking at 1 stall per 1,000sf is above and beyond what other retailers offer in this local market and provides our property a competitive advantage. Discussions with retail brokers indicate that our project will be well positioned (from a parking standpoint) to attract the retail tenants we are seeking with parking provided at 1 stall per 1,000sf we are proposing.

3. Abundant and Convenient Street Parking

In addition to the proposed dedicated parking on our Project site, the Project's retailers also benefit from an abundance of easily accessible and convenient street parking around the project. The location of where street parking is available nearby the project is shown in the figure below. Most notably, street parking is available on:

- **39th Street.** Street parking is available on both sides of 39th street.
- **Martin Luther King Jr.** Street parking is available on the west side of Martin Luther King Jr. Blvd, between Bigler Way and 7th Avenue, immediately across the street from the Project. This stretch of parking is approximately 500 linear feet long.
- **Bigler Way.** Street parking is available on both sides of Bigler Way.
- **5th Avenue.** Street parking is available on both sides of 5th Avenue.
- **6th Avenue.** Street parking is available on both sides of 6th Avenue.
- **7th Avenue.** Street parking is available on both sides of 7th Avenue.
- **38th Street.** Street parking is available on both sides of 38th street.



4. Planning District Objective - Pedestrian and Bicycle Oriented Retail

One of the Project's objectives is to encourage pedestrian oriented and bicycle oriented retail. This goal is consistent with the Stockton Broadway Planning District's own objectives which state:

- "Encourage reduction of motor vehicle parking requirements for new commercial development as allowed by this title as a means of attracting desirable businesses"
- "Enhance the bike and pedestrian connections and accessibility between the commercial uses on Broadway and Stockton Boulevard and the surrounding residential neighborhoods

Bicycle stalls are planned as part of the Project. Please refer to our letter dated May 19th, 2010 for a detailed description of the number of bicycle stalls we are proposing.

5. Public Transportation

The proposed project has great access to public transit. Bus route 51 stops directly in front of the project on Broadway. This bus lines run along Broadway and connects to the Light Rail Stop at Broadway and 19th. The accessibility of public transportation will reduce the parking demand on the Project.

We believe the salient points highlighted above provide solid grounds for our request to reduce the parking requirements. Thank you Lindsey, please do not hesitate to contact me should you have any questions.

Respectfully,



Steven S. Oh
The Related Companies of California

May 19, 2010

Lindsey Alagozian
City of Sacramento
300 Richards Blvd., 3rd Floor
Sacramento, CA 95811

RE: Request for Parking Reduction
Oak Park Mixed Use Senior Development

Dear Lindsey:

For your consideration and response, we are submitting this request for staff's determination of our application to reduce the required parking ratios for the Oak Park Mixed-Use Senior Project (the "Project") – located at the southeast corner of Broadway Avenue and Martin Luther King Jr. Boulevard.

The proposed project is a mixed-use development consisting of affordable senior rentals and retail. We are requesting that the parking requirements be reduced to the following:

- Residential Rental Units: 0.5 parking spaces per unit
- Visitors: 1 parking space per 15 units
- Commercial: 1 parking space per 1,000 square feet of net rentable retail space.

In summary, our request is based upon the following salient facts:

1. Affordable Senior Tenant Demographics

The proposed residential parking ratios reflect the anticipated residential usage by the senior tenants (55 years and older) with incomes below 50% of Area Median Income.

2. Proximity and Accessibility to Amenities and Services

The Project's proximity and accessibility to amenities and services support the tenant's reduced need for cars and the project's application for reduced parking ratios.

3. Market Data

A survey of other local, affordable, senior projects clearly demonstrates that a reduced parking ratio of 0.5 spaces per unit is appropriate.

4. Planning District Objectives

The Stockton Broadway Special Planning District has an identified goal to reduce motor vehicle parking as a means of attracting desirable businesses. Our project's request to reduce the required parking levels both for retail and residential is consistent with the Special Planning District's objective.

We believe that this request for reduced motor vehicle parking is appropriate based on the tenant demographics, project location, City planning objectives and market data.



We would appreciate a timely response from Staff to this request and will call tomorrow to discuss a timeframe. Thank you Lindsey, please do not hesitate to contact me should you have any questions.

Respectfully,

A handwritten signature in black ink, appearing to read 'Steven S. Oh'.

Steven S. Oh
The Related Companies of California

INTRODUCTION

The Oak Park Mixed-Use Senior Development will contain approximately 53 one-bedroom rental units and 1 two-bedroom manager’s unit. The development also contains a community room, fitness center, theater and approximately 3,000 to 4,000 square feet of pedestrian-oriented retail. The residential rental units in the Project shall be restricted to tenant with incomes no greater than 50% of Area Median Income. Funding requirements will ensure that the facility remains affordable to seniors (age 55 and older) for 55 years.

The Project is located on the corner of Broadway and Martin Luther King, Jr. Boulevard within the Broadway/Stockton Special Planning District (SPD) (City Code Title 17.94.010).

PARKING REDUCTION REQUEST

The following tables show the required and our proposed parking ratios for this Project.

Use	Vehicular Parking			
	Current Requirements		Project Proposal	
Residential	1.5 spaces/DU	81.0 spaces	0.5 spaces/DU	27 spaces
Visitors	1 space /15 DU	3.6 spaces	1 space /15 DU	3.6 spaces
Retail	1 space per 250 gross sf	12 spaces	1 space per 1,000 gross sf	3 spaces
Total		97 spaces		34 spaces

Use	Bicycle Parking			
	Current Requirements	Proposed Bicycle Parking		
			Class 1	Class 2,3
Residential	1 bicycle for every 20 vehicular parking stalls	4 spaces	2 spaces	2 spaces
Visitors		1 space	-	1 space
Retail		1 space	1 space	0 spaces
Total		6 spaces	3 spaces	3 spaces

Our request for a reduction is based upon the following salient facts:

1. Affordable Senior Tenant Demographics

The proposed residential parking ratios reflect the anticipated residential usage by the senior tenants (55 years and older) with incomes below 50% of Area Median Income. The combination of the tenant age and income profiles, a residential parking ratio of 0.5 spaces per unit is appropriate.

2. Proximity and Accessibility to Amenities and Services

The proposed project is located within close proximity to many amenities and services including supermarkets, banks, clinics, hospitals, parks and community centers. Additionally, the project has great access to public transit. A detailed list of key amenities and services follows:

▪ **Public Transportation Access**

Bus route 51 stops directly in front of the project on Broadway and connects to the Light Rail Stop at Broadway and 19th. Bus 51 also travels to Broadway and Stockton where supermarkets/pharmacies and other services are located.



- **Park**
McClatchy Park is one block away from the Project.

- **Hospital and Clinics**
UC Davis Medical Center is within 1/2-mile from the site.

Oak Park Community center and the Community Health Center is located within 1/4-mile from the site on Martin Luther King Jr. Boulevard.

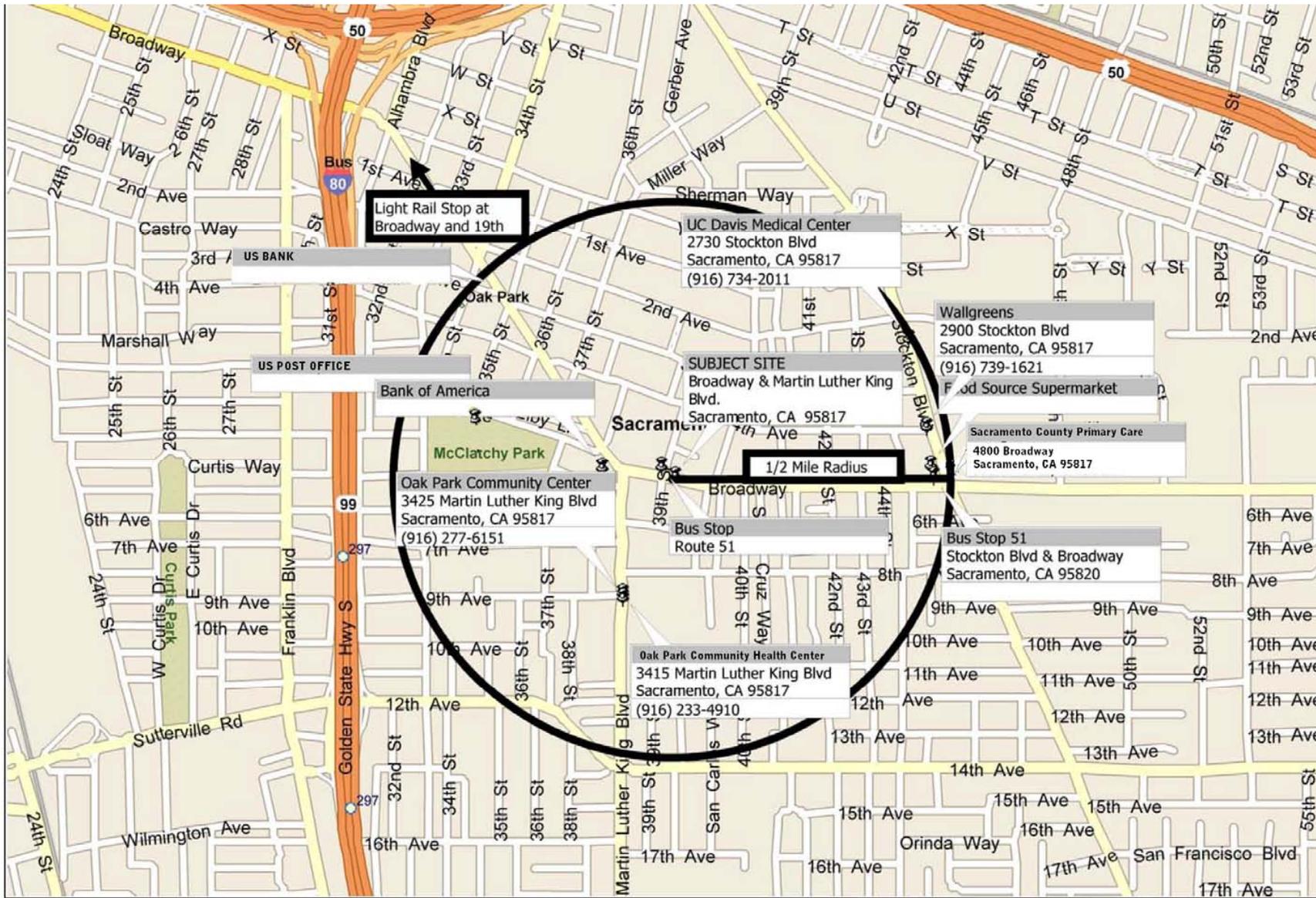
Sacramento Primary Care Center is located at the intersection of Stockton Boulevard and Broadway, 1/2-mile from the project site and on Bus Route 51.

- **Pharmacy**
Walgreens is located at the intersection of Stockton Boulevard and Broadway, 1/2-mile from the project site and on Bus Route 51.

- **Groceries, Restaurants and Other Retail**
A full-service supermarket (Food Source), restaurants, a café, and other retail services are located at the intersection of Stockton Boulevard and Broadway, 1/2-mile from the project site and on Bus Route 51.

- **Financial Services**
Bank of America is located directly across Martin Luther King Jr. Boulevard from the project site.

The project's proximity to this array of amenities and services, in addition to the Project's access to multiple forms of public transit, greatly reduces the tenant's need for cars. Below is a map showing the Project's location with respect to the aforementioned amenities and services.



Amenities and Services Proximity Map

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 18201 Von Karman Avenue, Suite 900, Irvine, CA 92612 • (949) 660-7272 phone • (949) 660-7273 fax • www.related.com

3. Market Data

A survey of the parking usage at other local, affordable senior projects clearly demonstrates that a reduced parking ratio of 0.5 spaces per unit is appropriate. The survey data is show below. Please note that the ratios presented below are inclusive of guest parking.

- **The Terraces**

Address: 1615 O Street, Sacramento
 # of units: 59 units
 % of units ≤ 50% AMI: 100%
 Occupancy: 100%
 # of parking spaces: 23 spaces (0.39 spaces/DU)
 Current usage: 21 spaces used by tenants (0.36 spaces/DU)

- **Globe Mills**

Address: 1131 C Street, Sacramento
 # of units: 112 units
 % of units ≤ 50% AMI: 81%
 Occupancy: 93%
 # of parking spaces: 54 spaces (0.48 spaces/DU)
 Current usage: 54 spaces used by tenants (0.48 spaces/DU)

- **Pioneer Tower**

Address: 515 P Street, Sacramento
 # of units: 198 units
 % of units ≤ 50% AMI: 100%
 Occupancy: n/a
 # of parking spaces: 50 spaces (0.25 spaces/DU)
 Current usage: 50 spaces used by tenants.
 Manager reports they could use 10 more parking spaces to accommodate guest and staff parking for a total of 60 spaces (0.30 spaces/DU)

- **Sky Parkway Terrace**

Address: 5414 Sky Parkway, Sacramento
 # of units: 58 units
 % of units ≤ 50% AMI: 98%
 Occupancy: 100%
 # of parking spaces: 58 spaces (1.0 spaces/DU)
 Current usage: 30 spaces used by tenants (0.52 spaces/DU)

- **Los Victorianas**

Address: 505 10th Street, Sacramento
 # of units: 40 units
 % of units ≤ 50% AMI: not applicable
 Occupancy: 100%
 # of parking spaces: 24 spaces (0.6 spaces/DU)
 Current usage: 15 spaces. Manager quotes usage is “never at capacity.”
 Property reports no additional demand for parking. There are no residents on the parking waitlist.
 (0.375 space/DU)

- **Riverview Plaza**

Address: 600 I Street, Sacramento
of units: 123 units
% of units ≤ 50% AMI: 99%
Occupancy: 95.1%
of parking spaces: 52 spaces (0.42 spaces/DU)
Current usage: 52 spaces.
Manager reports property could use 10 more spaces for residents and guests for total of 62 spaces including guest parking. (0.50 spaces/DU)

- **Greenfair Towers**

Address: 701-702 Fairgrounds Dr., Sacramento
of units: 388 units
% of units ≤ 50% AMI: 99%
Occupancy: 100%
of parking spaces: 143 spaces (0.37 spaces/DU)
Current usage: 143 spaces.
Property report that there are 10 people on the waitlist for parking. Manager reports property could use 70 more spaces for guest and residents. (0.55 space/DU)

4. **Planning District Objectives**

The Stockton Broadway Special Planning District has an identified goal to reduce motor vehicle parking as a means of attracting desirable businesses. The Broadway/Stockton SPD states:

“Encourage reduction of motor vehicle parking requirements for new commercial development as allowed by this title as a means of attracting desirable businesses.”

“Enhance the bike and pedestrian connections and accessibility between the commercial uses on Broadway and Stockton Boulevard and the surrounding residential neighborhoods.”

“Encourage the use of bicycle parking in accordance with designs preferred by the Sacramento city/county bicycle advisory committee.”

The project is requesting a reduction for the retail motor vehicle parking. The intention is to provide parking primarily for Business owners and Employees only. This strategy would increase pedestrian activity to enliven the area and encourage pedestrian friendly retail tenants. This would also serve to implement the Zoning Code goal of “Encouraging reduction of motor vehicle parking requirements for new commercial development as allowed by this title as a means of attracting desirable businesses.” The Project’s request to reduce the required parking levels both for retail and residential is consistent with the Special Planning District’s objective.

Conclusion

We believe that this request for reduced motor vehicle parking is appropriate based on the tenant demographics, project location, City planning objectives and market data.

We would appreciate a timely response from Staff to this request and will call tomorrow to discuss a timeframe. Thank you Lindsey, please do not hesitate to contact me should you have any questions.

Comments from The Oak Park RAC from the July RAC Meeting.

From: Matt Hertel [mailto:mhertel@shra.org]
Sent: Tuesday, September 14, 2010 11:52 AM
To: Matthew Sites; Lindsey Alagozian
Cc: Celia Yniguez
Subject: RE: Oak Park Redevelopment Advisory Committee Comments on Broadway/MLK Project

Hello Matt and Lindsey,

At the July 14th, 2010 Oak Park RAC meeting, the developer and architect presented the proposed design and site plan for the project. Comments were made by individual RAC members or members of the public in attendance of the meeting. These individual comments made by RAC members or residents were forwarded to Matt Sites on July 15th are reflected in the list below. SHRA staff has reviewed the September 15, 2010 Report to Design Commission regarding the proposed development at 3820 Broadway (P10-042). The Design Commission and Design Review staff recommendations as outlined in the staff report adequately address the comments made at the July 14, 2010 Oak Park Redevelopment Advisory Committee meeting. Additionally, the parking studies conducted by the developer and the adequate amount of on street parking also address the question about sufficient parking.

Thank you,

Matt Hertel
Associate Redevelopment Planner
SHRA
801 12TH Street
Sacramento, CA 95814
Phone (916) 449-6234
Fax (916) 447-2261
mhertel@shra.org
www.shra.org

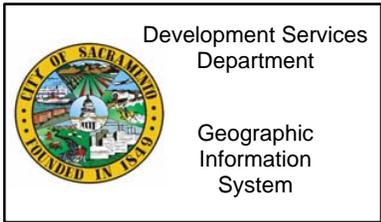
From: Matt Hertel [mailto:mhertel@shra.org]
Sent: Thursday, July 15, 2010 11:18 AM
To: Matthew Sites
Cc: Yniguez, Celia; Marsh, Richard
Subject: Oak Park Redevelopment Advisory Committee Comments on Broadway/MLK Project

Hello Matt,

Here are the comments RAC and Community members had about the design and site plan of the project.

- 1) Explore possibility of a drop-off area on Broadway in front of the project
- 2) Concern about the number of retail parking spaces
- 3) Continue to examine whether more parking spaces are required for the residents
- 4) Supportive of bollard component at corner near retail
- 5) Continue to explore consistency of materials
- 6) Corner entrance way over hang is too high

Celia and Richard please let me know if I missed anything.



July 08, 2010

P10-042
3280 Broadway
014-0171-001,-017,-018,-019,-020,-025

C-1-SPD zone
 Special Permit:
 Parking Waiver for residential & retail
 Residential development over 40,000 sf
 Variance:
 Setbacks along Broadway and MLK
 Broadway/Stockton, & Oak Park DR

