



Agenda City of Sacramento Planning Commission

COMMISSION MEMBERS:

*Anna Molander
Jon Bagatelos
Michael Mendez, MCP
Philip Harvey*

*Jameel Pugh
Joseph Contreras
Michael Notestine, Chair
Rommel Declines*

*James Frayne
Joseph Yee, AIA, Vice Chair
Panama Bartholomy*

CITY STAFF:

*Gregory Bitter, Principal Planner
Sabina Gilbert, Senior Deputy City Attorney*

*New City Hall
915 I Street, 1st Floor – Council Chambers*

October 28, 2010 – 5:30 P.M.

The City Planning Commission was created by the City Council. Its powers and duties include: to develop and maintain the General Plan; to make recommendations to the City Council on amendments to the General Plan and the City's zoning code and on zoning changes; to act upon applications for tentative subdivision maps, special permits and variances; and to make environmental determinations associated with these actions.

NOTICE TO THE PUBLIC

You are welcomed and encouraged to participate in this meeting. Public comment is taken (3 minutes maximum) on items listed on the agenda when they are called. Public Comment on items not listed on the agenda will be heard at the end of the meeting as noted on the agenda. Comments on controversial items may be limited and large groups are encouraged to select 3-5 speakers to represent the opinion of the group.

Notice to Lobbyists: When addressing the Commission you must identify yourself as a lobbyist and announce the client/business/organization you are representing (City Code 2.15.160).

Speaker slips are located in the lobby of the hearing room and should be completed and submitted to the Commission Secretary.

Government Code 54950 (The Brown Act) requires that a brief description of each item to be transacted or discussed be posted at least 72 hours prior to a regular meeting. The City posts Agendas at City Hall as well as offsite meeting locations. The order of agenda items is for reference and may be taken in any order deemed appropriate by the legislative body. The agenda provides a general description and staff recommendations; however, the legislative body may take action other than what is recommended. Full staff reports are available for public review on the City's website and include all attachments and exhibits. Hard copies are available at the Community Development Department (10 cents per page). Live video streams and indexed archives of meetings are available via the internet. Visit http://sacramento.granicus.com/ViewPublisher.php?view_id=21.

Meeting facilities are accessible to persons with disabilities. If you require special assistance to participate in the meeting, notify the Office of the City Clerk at (916) 808-7200 at least 48 hours prior to the meeting.



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AGENDA

October 28, 2010

*New City Hall
915 I Street – 1st Floor, Council Chambers*

All items listed are heard and acted upon by the Planning Commission unless otherwise noted.

Call to Order – 5:30 p.m.

Roll Call

Consent Calendar

All items listed under the Consent Calendar are considered and acted upon by one motion. Anyone may request that an item be removed for separate consideration.

1. **Approval of Minutes for September 23, 2010**

Location: Citywide

Recommendation: Approve Commission Minutes from September 23, 2010.

Contact: Gregory Bitter, Principal Planner, 916-808-7816

Director's Report

2. **Director's Report (Oral)**

Location: Citywide

Recommendation: Receive and File- Status report on pending development applications and appeals; proposed amendments to Zoning Code, design standards, and other development-related regulations; Community Development Department organizational and operational changes, work program, and training program; and similar matters.

Contact: Gregory Bitter, Principal Planner, 916-808-7816

Public Hearings

Public hearings may be reordered by the Chair at the discretion of the Commission. If you challenge the decision of this Commission you may be limited to raising only those issues that are raised in this hearing or in written correspondence received by the Commission prior to the hearing.

3. **P10-051 Radiological Associates of Sacramento Sign Variance** (Noticed on 10/15/10) (Continued from 9/23/10)

Location: 2929 K Street, 007-0117-001-0000, District 3

Recommendation: Approve – Item A. Environmental Exempt (Per CEQA 15311); **Item B:** Request to construct a new exterior sign which exceeds 20-feet in height within 660-feet of a designated freeway, located within the Transportation Corridor Special Planning District (TC-SPD) Zone in the Alhambra Corridor Special Planning District.

Contact: Matthew Sites, Design Review Staff, 916-808-7646, Stacia Cosgrove, Senior Planner, 916-808-7110

4. **Z10-053 Appeal of Cornerstone Restaurant Parking Waiver** (Noticed on 10/15/10)
Location: 2301 K Street, 007-0095-021-0000, District 3
Recommendation: Approve - Item A: Environmental Exemption (Per CEQA 15301); **Item B:** Special Permit for Parking Reduction Waiver of required parking for the conversion of an existing structure into a restaurant; **Item C:** Staff Level Design Review, DR10-110.
Contact: Robert W. Williams, Associate Planner, 916-808-7686, Sandra Yope, Senior Planner, 916-808-7158
5. **P10-036 Township 9 Modifications** (Noticed on 10/15/10)
Location: 819 N 7th Street, 001-0020-003-0000, 001-0020-019-0000, 001-0020-034-0000, 001-0020-036-0000, 001-0020-041-0000, 001-0020-044-0000, 001-0020-045-0000, 001-0020-046-0000, 001-0200-012-0000, 001-0200-013-0000, 001-0200-034-0000, District 1
Recommendation: Approve –Item A. Environmental Determination: Exempt (Per CEQA Section 15305); **Item B:** PUD Design Guidelines Amendment to modify requirements regarding the location and shape of park paseos; **Item C:** Subdivision Modification to change the prior Tentative Map conditions to allow recreational easements in lieu of fee simple ownership. **Item D:** Subdivision Modification to adjust the right of way alignment for Vine Street.
Contact: Evan Compton, Associate Planner, 916-808-5260, Stacia Cosgrove, Senior Planner, 916-808-7110
6. **P10-038 City Life Church** (Noticed on 10/15/10)
Location: 2719 K Street, District 3, 007-0111-015-0000
Recommendation: Approve – Item A: Environmental Exemption (Per CEQA 15301); **Item B:** Special Permit – Church to establish a church use; **Item C:** Variance – Signs within 10 feet of a property line.
Contact: Kimberly Kaufman-Brisby, Associate Planner, 916-808-5260, Stacia Cosgrove, Senior Planner, (916) 808-7110
7. **P10-056 Clearwire on Auburn Blvd - PG&E** (Noticed on 10/15/10)
Location: Corner of Auburn Blvd & Connie Drive, 266-0111-009-0000, District 2
Recommendation: Approve – Item A: Environmental Exemption (Per CEQA 15301); **Item B:** Special Permit-Major Modification to add three panel antennas three microwave dishes, one GPS antenna, and an equipment cabinet on an existing PG&E lattice tower.
Contact: Elise Gumm, Associate Planner, 808-1927, Lindsey Alagozian, Senior Planner, 916-808-2659

8. **P10-057 Clearwire on Natoma Way - PG&E** (Noticed on 10/15/10)
Location: Southwest corner of Natoma Way & Roanoke, 252-0172-001-0000, District 2
Recommendation: Approve – Item A: A Environmental Exemption (Per CEQA 15301); **Item B:** Special Permit-Antennas/Wireless to install three panel antennas, three microwave dishes, one GPS, antenna, and an equipment cabinet on an existing PG&E lattice tower.
Contact: Elise Gumm, Associate Planner, 808-1927, Lindsey Alagozian, Senior Planner, 916-808-2659
9. **P10-064 Wolf Ranch Inclusionary Housing Amendment** (Noticed on 10/15/10)
Location: 7200 Jacinto Avenue, 117-0140-042-0000, District 8
Recommendation: Approve – Item A: A Environmental Exemption (Per CEQA 15301-Existing Facilities); **Item B:** Misc-Inclusionary Housing Plan to amend an approved inclusionary housing plan.
Contact: Elise Gumm, Associate Planner, 916-808-1927, Lindsey Alagozian, Senior Planner, 916-808-2659
10. **P10-065 Pell Circle Billboard Relocation** (Noticed on 10/15/10)
Location: 3961 Pell Circle, 237-0400-016-0000, District 2
Recommendation: Withdrawn, to be re-noticed
Contact: Antonio Ablog, Associate Planner, 916-808-7702, Lindsey Alagozian, Senior Planner, 916-808-2659
11. **LR10-005 Proposed Rezones and Land Use Designation and Text Changes to the 2030 General Plan** (Noticed on 10/15/10)
Location: City-wide
Recommendation: Forward Recommendations of Approval to City Council – Proposed text amendments to the 2030 General Plan and the North Natomas Community Plan include: 1) adding a policy to clarify how changes to the minimum floor-area-ratio may be consistent with the General Plan; 2) addition of a policy from the North Natomas Community Plan regarding infrastructure financing and removal of that policy from that plan; and 3) clarifying application of the density policy for the Traditional Neighborhood designation.
Contact: Teresa Haenggi, Associate Planner, (916) 808-7554, Jim McDonald, Senior Planner, 808-5273

Staff Reports

Staff reports include oral presentations including those recommending Receive and File.

None

Public Comments- Matters Not on the Agenda

12. To be announced.

Questions, Ideas and Announcements of Commission Members

13. To be announced.

Adjournment

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Minutes City of Sacramento Planning Commission

1

COMMISSION MEMBERS:

*Anna Molander
Jon Bagatelos
Michael Mendez, MCP
Philip Harvey*

*Jameel Pugh
Joseph Contreras
Michael Notestine, Chair
Rommel Declines*

*James Frayne
Joseph Yee, AIA, Vice Chair
Panama Bartholomy*

CITY STAFF:

*Gregory Bitter, Principal Planner
Sabina Gilbert, Senior Deputy City Attorney*

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September 23, 2010 – 5:30 P.M.

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MINUTES

September 23, 2010

*New City Hall
915 I Street – 1st Floor, Council Chambers*

All items listed are heard and acted upon by the Planning Commission unless otherwise noted.

Call to Order – 5:30 p.m.

Roll Call - All commissioners present except Pugh and Bartholomy. Commissioner Pugh arrived at 5:37 p.m.

Consent Calendar

All items listed under the Consent Calendar are considered and acted upon by one motion. Anyone may request that an item be removed for separate consideration.

1. Approval of Minutes for September 9, 2010

Location: Citywide

Recommendation: Approve Commission Minutes from September 9, 2010.

Contact: Gregory Bitter, Principal Planner, 916-808-7816

Action: Moved, seconded, and carried (Contreraz/Molander; 10:0:1, Absent-Bartholomy) to approve minutes including requested changes.

Director's Report

2. Director's Report (Oral)

Location: Citywide

Recommendation: Receive and File- Status report on pending development applications and appeals; proposed amendments to Zoning Code, design standards, and other development-related regulations; Community Development Department organizational and operational changes, work program, and training program; and similar matters.

Contact: Gregory Bitter, Principal Planner, 916-808-7816

Action: Received and Filed.

Public Hearings

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3. **LR07-008 Florin Road Corridor Plan Policies and Rezone** (Noticed on 9/13/10)
Location: The Florin Road Corridor from Tamoshanter Way to Franklin Boulevard, Districts 5 and 8

Recommendation: Forward Recommendations of Approval to City Council – Review 1) a **Resolution** approving environmental review of the actions implementing the Florin Road Corridor plan, 2) a **Resolution** amending the 2030 General Plan to add new policies to the South Area Community Plan chapter and to establish the Florin light rail station area as a Transit Village Development District under State law; 3) a **Resolution** amending the General Plan land use diagram to change the land use designation for parcel 049-0010-103-0000; and 4) an **Ordinance** rezoning various parcels on the Florin Road Corridor, and forward a recommendation of approval of these to Council.

Contact: Remi Mendoza, Assistant Planner, 916-808-5003, Desmond Parrington, AICP, Infill Coordinator, 808-5044

Public Comment made by Anita Carter.

Action: Moved, seconded, and carried (Harvey/Yee; 10:0:1, Absent- Bartholomy) to approve staff recommendation.

4. **P10-009 Highway 99 Billboard Relocation Agreement** (Noticed on 9/13/10)
Location: 7900 Stockton Boulevard, District 8, 117-0170-009-0000

Recommendation: Approve - Item A: Environmental Exemption (Per CEQA 15303); **Item B:** Variance-Signs to exceed the height allowed for a billboard within the General Commercial (C-2) zone; **Item C:** Relocation Agreement-Offsite Signs for the removal of three existing billboards in order to construct a new replacement billboard located within the General Commercial (C-2) zone.

Contact: Sandra Yope, Senior Planner, 916-808-7158, Joy Patterson, Principal Planner, 916-808-7507

Action: Moved, seconded, and carried (Molander/Mendez; 10:0:1, Absent- Bartholomy) to approve the environmental exemption and variance, and to forward the recommendation of approval of the Relocation agreement to City Council.

5. **P10-042 3820 Broadway** (Noticed on 9/13/10)
Location: 3820 Broadway, District 5, 014-0171-001-0000, 014-0171-017-0000, 014-0171-018-0000, 014-0171-019-0000, 014-0171-020-0000, 014-0171-025-0000

Recommendation: Approve – Item A: Environmental Determination: Exempt (per CEQA Guidelines Section 15332); **Item B:** Special Permit to construct a building over 20,000 square feet in size in the Limited Commercial (C-1-SPD) zone; **Item C:** Special Permit to reduce required parking for senior apartments; **Item D:** Variance to reduce the setback from 5' to 0' along Broadway and; **Item E:** Variance to reduce the setback from 20' to 0' along Martin Luther King Jr. Boulevard.

Contact: Matthew Sites, Assistant Architect, 916-808-7646, Lindsey Alagozian, Senior Planner, 916-808-2659

Public Comment made by Anita Carter, Bob Solorio, and Shirley Elmore, and eComment by Terrence Johnson.

Action: Moved, seconded, and carried (Molander/Pugh; 9:0:2, Recused – Notestine; Absent- Bartholomy) to approve staff recommendation with added condition to place sign at entrance to alley informing public of a dead end.

6. **P10-043 Sutter Medical Center Signage** (Noticed on 9/13/10)
Location: 2801 L Street, District 3, 007-0114-003-0000; 007-0118-001-0000; 007-0171-002-0000; 007-0171-003-0000; 007-0171-004-0000; 007-0171-005-0000; 007-0171-006-0000; 007-0171-008-0000; 007-0172-010-0000; 007-0172-013-0000; 007-0172-017-0000; 007-0173-001-0000; 007-0173-002-0000; 007-0173-003-0000; 007-0177-001-0000
- Recommendation: Approve – Item A:** Addendum to Environmental Impact Report; **Item B:** Special permit major modification to amend the existing sign program for a hospital campus with approximately 10.8 acres; **Item C:** Variance – Signs to exceed the allowed sign area of 200 square feet for signs with 660 feet of a freeway; **Item D:** Variance – Signs to exceed the allowed vertical sign height for attached signs within 660 feet of a freeway; **Item E:** Variance – Signs to exceed the allowed 6 foot height for detached signs within the Hospital (H) zone; **Item F:** Variance – Signs to place detached signs within 10 feet of a property line and/or within 5 feet of a driveway in the Hospital (H) zone; **Item G:** Variance – Signs to place detached signs within the public right-of-way; **Item H:** Variance – Signs to exceed the maximum allowed area of 300 square feet for attached signs in any zone.
- Contact:** Kimberly Kaufman-Brisby, Associate Planner, 916-808-5260, Stacia Cosgrove, Senior Planner, (916) 808-7110

Action: Moved, seconded, and carried (Harvey/Molander; 9:0:2, Recused – Bagatelos; Absent- Bartholomy) to approve staff recommendation.

7. **P10-051 Radiological Associates of Sacramento Sign Variance** (Noticed on 9/13/10)
Location: 2929 K Street, District 3, 007-0117-001-0000,
Recommendation: Continue to October 28, 2010
- Contact:** Matthew Sites, Assistant Architect, 916-808-7646, Stacia Cosgrove, Senior Planner, 916-808-7110

Action: Moved, seconded, and carried (Contreraz/Pugh; 10:0:1, Absent- Bartholomy) to approve staff recommendation.

Staff Reports

Staff reports include oral presentations including those recommending Receive and File.

None

Public Comments- Matters Not on the Agenda

8. None.

Questions, Ideas and Announcements of Commission Members

9. Staff member Gregory Bitter, announced that the October 14, 2010 Planning Commission has been canceled. Commissioner Yee asked if the General Plan rezone item would be coming before the Council at the October 28, 2010 meeting.

Staff informed him that it would be coming before the commission again on that date. Commissioner Yee requested that all interested parties for the rezone item be informed of the meeting date.

10. Commissioner Declines mentioned the e-mail that he had staff forward to the Commission regarding the community members that wished to form a neighborhood alliance to review land use and cell towers within the City. The alliance wouldn't be focused on any specific neighborhoods but wishes to review the City's policies regarding cell towers throughout the City.
11. Commissioner Molander reported back to the commission that the subcommittee has started reviewing the Delta water issues as well as the voting procedures, including reviewing other Planning Commissions procedures statewide. She stated that the subcommittee would be reporting back to the full commission soon.

Adjournment - 7:33 p.m.

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Oral Report

For

City of Sacramento

Planning Commission

Agenda Packet

For the Meeting of: October 28, 2010

Title: Director's Report - **Receive and File-** Status report on pending development applications and appeals; proposed amendments to Zoning Code, design standards, and other development-related regulations; Community Development Department organizational and operational changes, work program, and training program; and similar matters.

Contact Information: Gregory Bitter, Principal Planner, 916-808-7816

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REPORT TO PLANNING COMMISSION City of Sacramento

3

915 I Street, Sacramento, CA 95814-2671

PUBLIC HEARING
October 28, 2010

To: Members of the Planning Commission

Subject: RAS Sign Variance (P10-051) A request to construct a new exterior attached sign which exceeds 20 feet in height within 660-feet of a designated freeway, located within the Transportation Corridor Special Planning District (TC-SPD) Zone in the Alhambra Corridor Special Planning District.

- A.** Environmental Determination: Exempt per 15311, Infill Development;
- B.** Variance to construct a new exterior attached sign which exceeds 20 feet in height within 660-feet of a designated freeway, located within the Transportation Corridor Special Planning District (TC-SPD) Zone in the Alhambra Corridor Special Planning District.

Location/Council District:

2929 K Street, Sacramento, CA 95816

Assessor's Parcel Number: 007-0117-001-0000

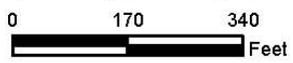
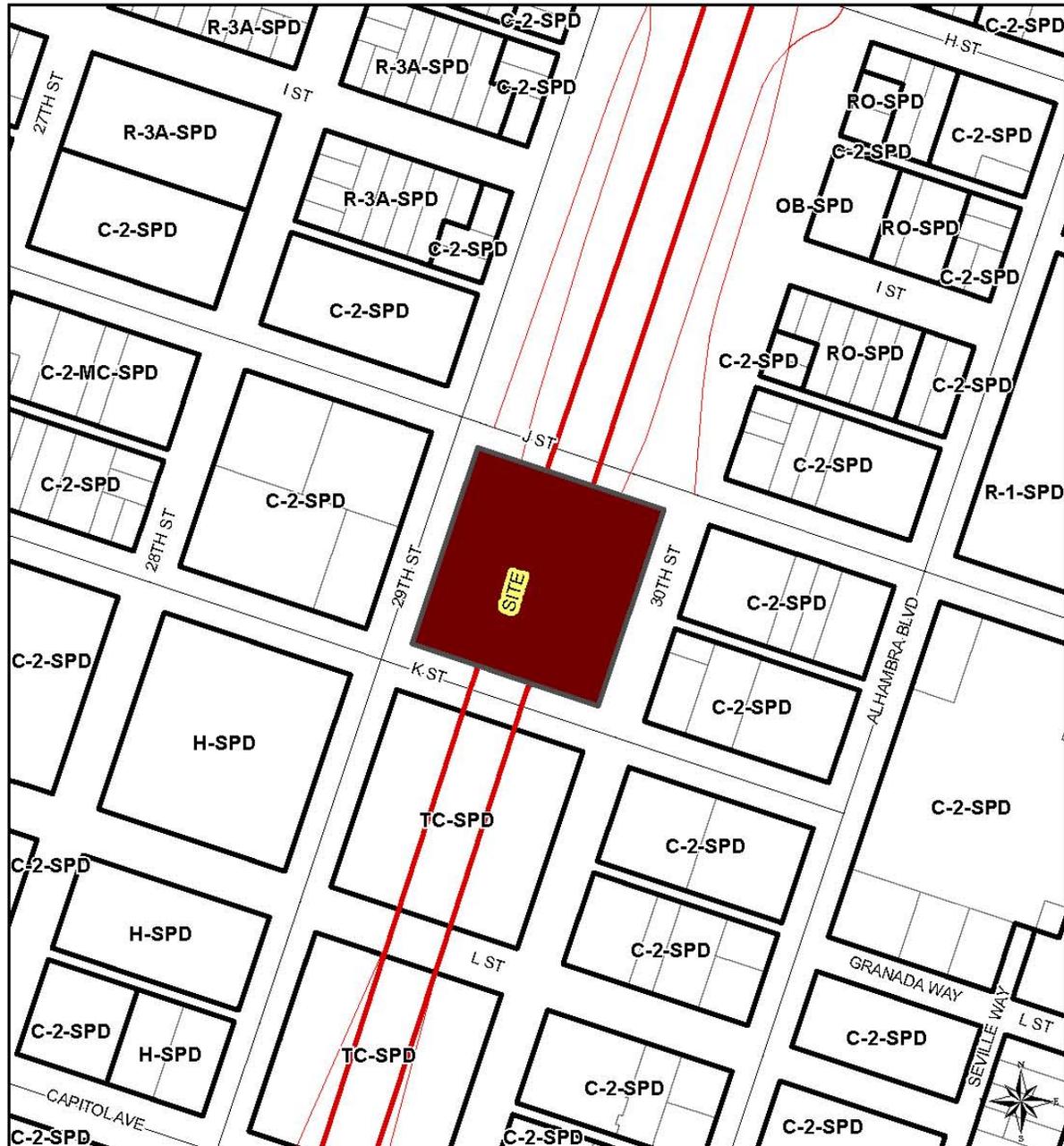
Council District 3

Recommendation: Staff recommends the Commission approve the request based on the findings and subject to the conditions listed in Attachment 1. The Commission has final approval authority over items A & B above, and its decision is appealable to City Council.

Contact: Matthew Sites, Associate AIA, LEED AP, Urban Design Staff, (916) 808-7646
Stacia Cosgrove, Senior Planner, (916) 808-7110

Applicant: Al Laehn, Radiological Associates, (916) 646-8450, 1500 Exposition Parkway, Sacramento, CA 95815

Owner: George Lenzi, Radiological Associates, (916) 646-8450, 1500 Exposition Parkway, Sacramento, CA 95815



October 28, 2010

P10-051
2929 K Street
007-0117-001-0000
TC-SPD zone



Summary: The Sutter Galleria's complex is a multi-tenant, commercial/office property located between 29th/30th Streets and J/K Street, beneath the Capitol City Freeway. Radiological Associates of Sacramento (RAS) has relocated to the upper two floors of the east building and is requesting a sign on the north side of the building for their current use. RAS, as the primary tenant in the east building is requesting a single sign for branding and wayfinding for regional customers that is complementary to existing signs and is respectful to the existing architecture.

The single attached sign is within 660 feet of the freeway, is located higher than 20 feet from grade, and would be visible from the freeway, and therefore is subject to the provisions of the sign ordinance section limiting the vertical location height and the area of signs within 660 feet of the freeway (Section 15.148.860). The proposed sign is 105 square feet at maximum dimension of 37'-4" in width and 4'-6" in height and located 40'-0" from grade.

The proposed sign variance is consistent with the land use designations and applicable policies of the 2030 General Plan. Staff notified all property owners within a 500 radius of the project campus regarding this public hearing and has received comments from a neighborhood group which have been attached. The project is not considered to be controversial.

Table 1: Project Information
General Plan designation: Public/Quasi-Public
Existing zoning of site: TC-SPD (Transportation Corridor & Alhambra Corridor Special Planning District)
Existing use of site: Retail and office
Property area: 2.5 +/- gross acres

Background Information: Currently, three buildings are located on the project site. Two of the buildings are mixed use with ground floor retail and second and third levels are offices, the third building is a parking structure located under the freeway. A sign program for the Sutter Galleria was established in 1988. The last application regarding revisions to the sign program was Z96-077 which revised the building signage from "Sutter Square" to "Galleria" and included new signage for "UC Davis Extension". File Z96-007 amended the original project (P84-164) which included Special Permits to construct a building over 75,000 s.f. and to develop a four-story retail office facility with parking in the TC-SPD zone.

Public/Neighborhood Outreach and Comments: Early project notification was sent to East Sacramento Improvement Association (ESIA), East Sacramento Preservation Task Force (ESPTF), McKinley East Sacramento Neighborhood Association (MENA), East Sacramento Chamber of Commerce, and the Midtown Business Association. Comments were received from the ESPTF and have been attached to this report (Attachment 3). An additional phone inquiry was made by a neighbor; after discussion with staff, the neighbor is in support of the proposed sign. No other comments or concerns have been received by staff. In response to the letter from ESPTF, RAS associates is requesting only one attached sign on the north side of the building, which staff thinks is reasonable, given that they are the primary tenant in the building. There are no requests for "billboard" signage at the site or at the Cannery Business Park site at 33rd and C Street.

Environmental Considerations: The project would allow the installation of a sign identifying the occupant of the building on which the sign would be attached. Section 15311 provides that the construction or placement of minor structures accessory to existing commercial facilities, including on-premise signs, is exempt from the California Environmental Quality Act. The project would not result in any significant effects on the environment.

Policy Considerations:

2030 General Plan

The 2030 General Plan Update was adopted by City Council on March 3, 2009. The 2030 General Plan's goals, policies, and implementation programs define a roadmap to achieving Sacramento's vision to be the most livable city in America. The 2030 General Plan Update designation of the subject site is Public, which provides for, "...a diversity of public and private community supportive uses that are located equitably throughout the city to support the needs of Sacramento's current and future residents and employees" Furthermore, the proposed project would promote the following Public policies:

- **Public/Quasi-Public.** Provide for governmental, utility, institutional, educational, cultural, religious, and social facilities and services that are located and designed to complement Sacramento's neighborhoods, centers, and corridors and to minimize incompatibility with neighborhoods and other sensitive uses. (Goal LU 8.1)
- **Adequate Community Supporting Uses.** The City shall seek to ensure that all manner of public and private community-supportive facilities and services are located throughout the city to provide places that serve the varied needs of the community, provide for community meeting places, and provide community and neighborhood landmark buildings and places. (Policy LU 8.1.2)

The proposed project meets the 2030 General Plan goals and policies related to the Public land use designation. The proposed sign will act as wayfinding signage for visitors to the site and its size and materials will be sensitive to surrounding uses.

Project Design:

Building design, signage and landscaping

The Galleria buildings have had many tenants over the past 26 years and the UC Davis Campus Extension is the most recent major tenant located in the west building on the parcel. UC Davis received approval for one sign on each building (Z96-077), yet only occupies the west building and has never occupied the east building per city records. The applicant proposes to provide a sign for Radiological Associates (RAS) on the north side of the building in addition to the UC Davis University Extension and Galleria signs on the south side of the building as RAS occupies the second and third floor of the K and 30th Street (east) building and UC Davis only occupies the larger west building located along 29th Street. The applicant will be providing comprehensive wayfinding signage which is allowed by right, in the parking garage below the freeway in addition to the proposed sign.

No changes are proposed to the existing building design with exception of the proposed attached sign along the J Street elevation. The proposed reverse pan channel halo illuminated sign will be located on the upper area of the north elevation. The sign will require a variance to be located above 20'-0" within 660 feet of the freeway as it will be located 40'-0" above street level. Due to the existing nature of the building and previously approved signage, staff feels that the additional attached sign on the north elevation is appropriate as the existing and proposed signs cannot be viewed at the same time (i.e. from both north and south bound freeway traffic) which is more common with signs oriented parallel to the freeway.

Variance: Within 660' of a designated freeway, the sign code normally allows an office building to have one (1) attached, freeway oriented sign. On this particular building, there is already one attached sign above 20 feet: the sign on the north elevation for UC Davis University Extension. UC Davis University Extension is the primary tenant in the west building, not this building (east building), therefore RAS must request an entitlement. In order to grant the variance, the Planning Commission must make the following findings per Section 15.148.1040 (A) (B) and (C) of the Sign Ordinance:

- A. That exceptional or extraordinary circumstances or conditions apply to the case referred to in the application that do not apply generally in the same district and the enforcement of the regulations of this article would have an unduly harsh result upon the utilization of the subject property.
 1. The variance would be appropriate for any property owner facing similar circumstances in that it is reasonable to expect that the primary tenant in the building should have an attached sign above 20-feet in height as provided by the sign code. In this case, there is a long existing attached sign that already fulfills that requirement, for a tenant that is not actually located in this building. Requiring removal of the existing, properly permitted sign is unreasonable, especially where an appropriate alternate location may be provided. In this case, an appropriate alternate location has been identified on the opposite side of the building.
 2. The sign will help to ensure that patients are able to directly access their destination. This sign is part of a wayfinding system for an office that routinely provides medical care to patients within the greater Sacramento area.
- B. That the variance will not result in a special privilege to one individual property owner and that the variance would be appropriate for any property owner facing similar circumstances.
 1. The variance would be appropriate for any property owner facing similar circumstances in that requiring removal of approved existing building signage on the same parcel, would inappropriately create a hardship for the existing tenant in order to provide signage for a new tenant, where a legitimate and non-controversial sign location may be provided for the main tenant of a building on the same property.
 2. The proposed sign offers visibility only from the south bound traffic lanes, and is discreetly designed to minimize any visual impact on the building and the freeway.

C. A variance must not be injurious to public welfare, nor to property in the vicinity of the applicant.

1. The sign placement and size respects the scale and character of the adjacent neighborhood through attention to views, illumination, font style, building scale and orientation.
2. This sign will provide wayfinding without the utilization of additional exterior building signage. The sign utilizes a reverse halo illumination which provides a soft glow of light behind the reverse pan channel letters with no direct illumination or hot spotting directed toward the freeway.
3. This sign does not represent a proliferation of attached signage on this building. Due to the orientation and attached locations of the existing and proposed freeway oriented signs, neither the southern or northern sign can be seen at the same time.

Conclusion:

Staff is in support of the proposal because the sign is tastefully design and respectful of the existing structure and signage. It will provide vehicular routing guidance now and into the future. Therefore, staff recommends the Planning Commission approve items A & B.

Respectfully submitted by:



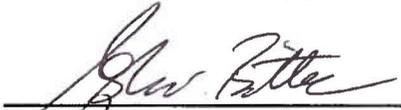
MATTHEW SITES, Associate AIA, LEED AP
Design Review Staff

Approved by:



STACIA COSGROVE
Senior Planner

Recommendation Approved:



GREGORY BITTER
Principle Planner

Attachments:

- | | |
|--------------|---|
| Attachment 1 | Recommended Findings of Fact and Conditions of Approval |
| Attachment 2 | Plans and Photos |
| Exhibit 1 | Sign Plan |
| Exhibit 2-4 | Photos |
| Attachment 3 | Letter from community group |
| Attachment 4 | Land Use & Zoning Map |

Attachment 1
Proposed Findings of Fact and Conditions of Approval
Project Name and (P10-051)
2929 K Street

Findings Of Fact

A. Environmental Determination: Exempt from CEQA pursuant to Section 15311 of the CEQA Guidelines

Based on the determination and recommendation of the City's Environmental Planning Services Manager and the oral and documentary evidence received at the hearing on the Project, the Planning Commission finds that the Project is exempt from review under Section 15311 of the California Environmental Quality Act Guidelines as follows:

The project would allow the installation of a sign identifying the occupant of the building on which the sign would be attached. Section 15311 provides that the construction or placement of minor structures accessory to existing commercial facilities, including on-premise signs, is exempt from the California Environmental Quality Act. The project would not result in any significant effects on the environment.

B. The Variance to exceed the allowed vertical location height for attached signs within 660 feet of a freeway is approved subject to the following Findings of Fact and Conditions of Approval:

1. That exceptional or extraordinary circumstances or conditions apply to the case referred to in the application that do not apply generally in the same district and the enforcement of the regulations of this article would have an unduly harsh result upon the utilization of the subject property.
 - a. The sign will help to ensure that patients are able to directly access their destination. This sign is part of a wayfinding system for an office that routinely provides medical care to patients within the greater Sacramento area.
2. That the variance will not result in a special privilege to one individual property owner and that the variance would be appropriate for any property owner facing similar circumstances.
 - a. The variance would be appropriate for any property owner facing similar circumstances in that building signage along the I-80 Business Route, have been well established as a single main building tenant sign located parallel to the freeway.
 - b. The proposed sign offers visibility only from the south bound traffic lanes, and is discreetly designed to minimize any visual impact on the building and the freeway.

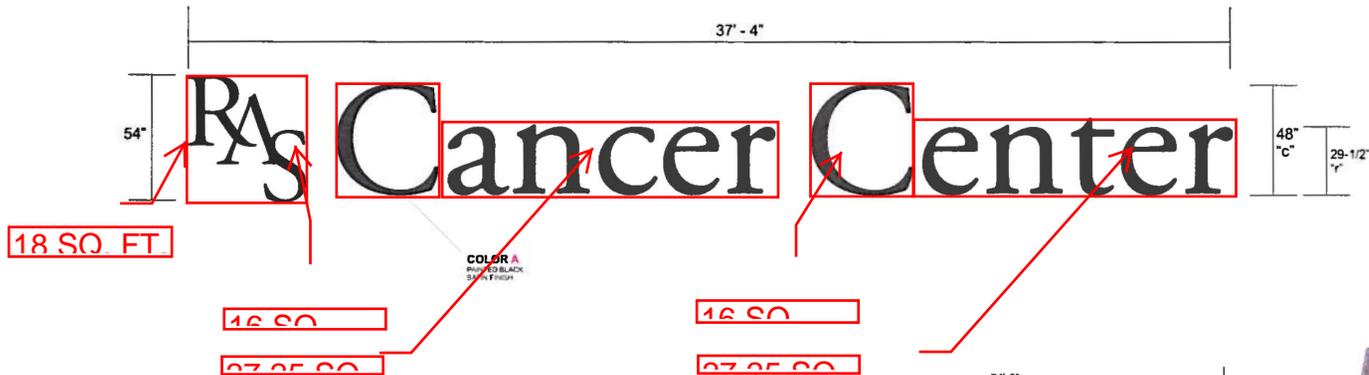
- c. The sign code recognizes the need for an office building to have a limited amount of freeway oriented signage, however, an attached sign already exists for a different tenant in the Galleria complex, but not the primary tenant of the east building. It is appropriate to provide reasonable attached signage for this primary tenant.
3. A variance must not be injurious to public welfare, nor to property in the vicinity of the applicant.
 - a. The sign placement and size respects the scale and character of the adjacent neighborhood through attention to views, illumination, font style, building scale and orientation.
 - b. This sign will provide wayfinding without the utilization of additional exterior building signage. The sign utilizes a reverse halo illumination which provides a soft glow of light behind the reverse pan channel letters with no direct illumination or hot spotting directed toward the freeway.
 - c. This sign does not represent a proliferation of attached signage on this building, and should not be constituted as such. Due to the orientation and attached locations of the existing and proposed freeway oriented signs, neither the southern or northern sign can be seen at the same time.

Conditions Of Approval

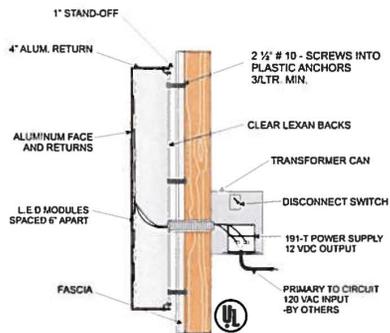
The **Variance** to exceed the allowed vertical location height for attached signs within 660 feet of a freeway is hereby approved subject to the following conditions:

1. Applicant shall obtain all necessary building and/or encroachment permits prior to commencing construction.
2. Any modification to the project shall be subject to review and approval by Planning Department staff prior to the issuance of building permits.
3. Unless modified by any condition herein, this project shall be developed and constructed in full compliance with the City Code.
4. A black satin finished aluminum reverse pan channel letters and channel logo with halo illumination and 1" standoffs shall be provided per approved plans.

SIGN OPTION 3
 REVERSE PAN CHANNEL LETTERS / LED ILLUMINATION
 QTY 1



Total sign area = 104.5 SQ.



DESCRIPTION exterior
 Reverse Pan Channel letters, 1/8" Aluminum faces, painted 4" painted returns, Mounted w/ 1" stand off. LED illumination - White

SCALE
 1/4" = 1'



Ellis & Ellis
 SIGN SYSTEMS
 Phone 916.924.1936 1111 Joellis Way
 Fax 916.924.3133 Sacramento, CA 95815
 Contractors License #545167

Project/ RAS Cancer Treatment Center
 Address/ 2929 K Street, Sacramento CA

All ideas, designs, arrangements and plans indicated or represented by this drawing are owned by and the property of Ellis & Ellis, and were created, revised and developed for use on and in connection with the specified project. None of such ideas, designs, arrangements or plans shall be used by or disclosed to any person, firm or corporation for any purpose whatsoever without the written permission of Ellis & Ellis.

Date/ 05.24.10

Revisions	Date	Description
1	06.01.10	07.13.10

Art# 8198

Designer/ KAS Sales Rep/ Brad
 Sales Rep. Approval for Production
 Date

Please check spelling and layouts. If approved, sign the layouts and return for production. If there are changes, please note them on the layout to which they pertain and send back for revision. Layouts must be returned no more than 72 hours after receipt, or a change order extending contract time will be issued. All signs are produced after the approval of shop drawings by client. Production time will be 6-8 weeks after layouts are received and signed without any changes. Any signs made incorrectly due to inaccurate client-approved drawings will not be the responsibility of Ellis & Ellis.

This document is submitted for review/approval of all aspects including; layout, spelling, context / content, grammar, graphics, etc. By signing, I acknowledge my review / approval and further acknowledge that any requested revisions may result in additional charges. Written dimensions on these drawing shall have precedence over scaled dimensions.

Electrical requirements client is responsible for:
 1) Electrical must be within 6' of sign.
 2) Must have bonded earth ground from electrical panel to sign location.
 3) N.E.C. requires dedicated circuits for signs.
 4) No roof penetrations.

ellises.com

P10-051
AUG 02, 2010







Matthew Sites

From: Will H. Green, M.D. [wgreen@surewest.net]
Sent: Monday, September 20, 2010 4:38 AM
To: Matthew Sites; 'noblep5@comcast.net'; 'mba@mbasac.com';
'nancycornelius@sbcglobal.com'; 'eastsacchamber@aol.com'
Cc: Janine Martindale; Stacia Cosgrove
Subject: Re: P10-051 Routing
Importance: High

Dear Matthew, why wouldn't RAS need/want signage in both freeway directions, North and South???

It is amazing the amount of competition that is happening in our medical communities.

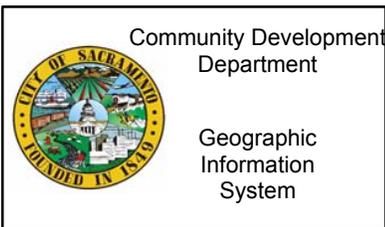
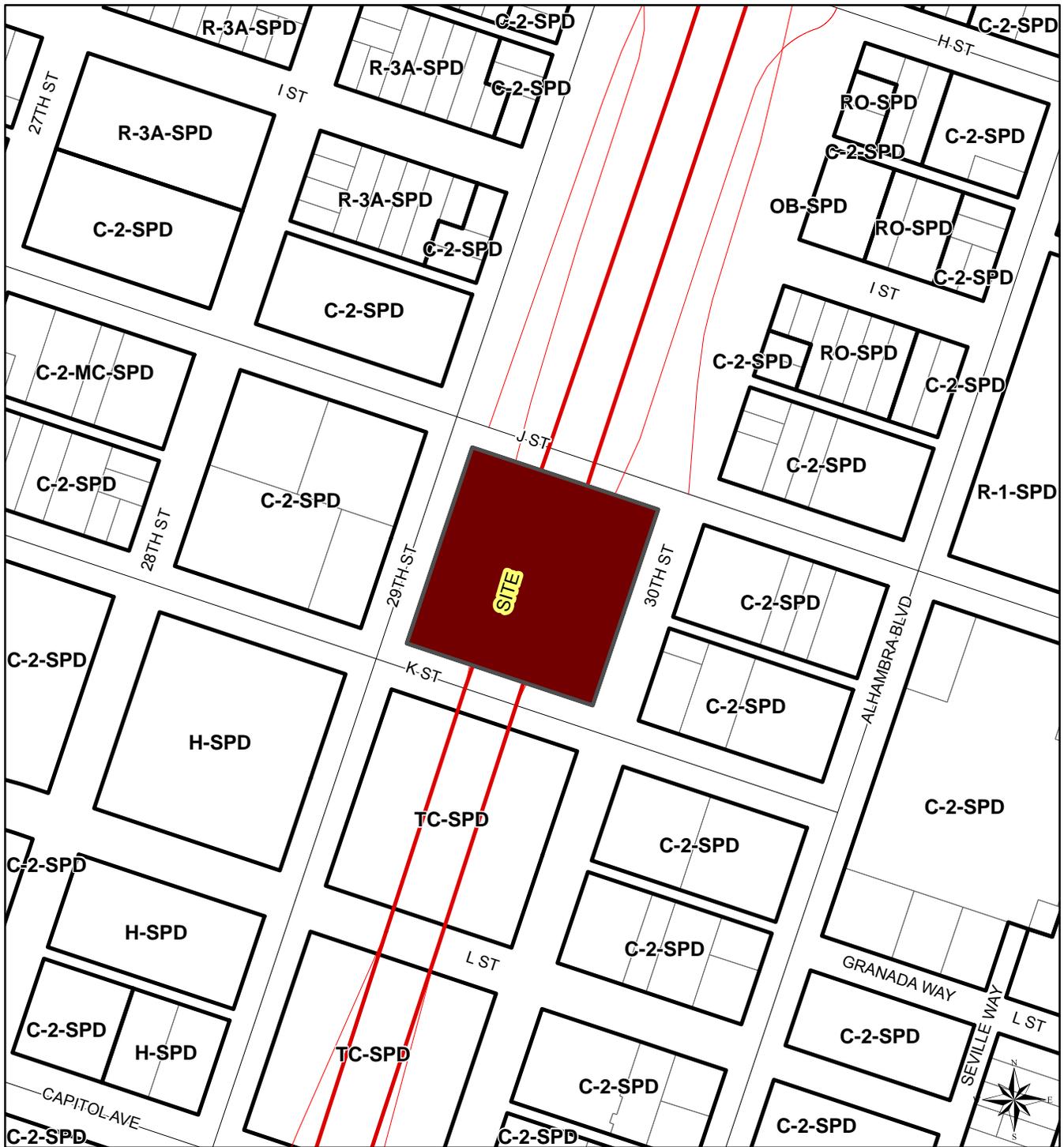
Mercy Cancer Center..at the Cannery... Will they need a billboard signage so it can be seen from the freeway???

RAS is builing a new Cancer Center on Blue Ravine Road in Folsom, on the right of the stop light where you would turn left into the CHW Folsom site.

UCDMC has a Cancer Center and is reportedly builing yet a new one...

When does this ever stop????

Will Green, President, ESPTF, Inc.



October 28, 2010

P10-051

2929 K Street
 007-0117-001-0000
 TC-SPD zone



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REPORT TO PLANNING COMMISSION City of Sacramento

4

915 I Street, Sacramento, CA 95814-2671

PUBLIC HEARING
October 28, 2010

To: Members of the Planning Commission

Subject: Appeal of Cornerstone Restaurant Parking Waiver (Z10-053)

A request to waive required parking in order to convert an existing office building (and former church) into a new restaurant, on a parcel of approximately 0.07 acres in the General Commercial, Neighborhood Corridor Overlay (C-2-NC) zone. The project also includes Staff Level Design Review for rehabilitation of an existing commercial structure.

- A. Environmental Determination: Exempt (CEQA 15301);
- B. Special Permit to waive required parking in order to convert an existing office building (and former church) into a new restaurant, on a parcel of approximately 0.07 acres in the General Commercial, Neighborhood Corridor Overlay (C-2-NC) zone;
- C. Staff Level Design Review for exterior renovations to an existing commercial structure that is located within the Central City Design Review District, file number DR10-110.

Location/Council District:

2301 K Street, Sacramento, CA 95816

Assessor's Parcel Number: 007-0095-021-0000

Council District 3

Recommendation: Staff recommends the Commission approve the request based on the findings and subject to the conditions listed in Attachment 1, thereby denying the appeal. The Commission has final approval authority over items A-C above. Its decision is not appealable to City Council, but is subject to call-up by the Mayor and the council member from District 3.

Contact: Robert W. Williams, Associate Planner, (916) 808-7686
Sandra Yope, Senior Planner, (916) 808-7158

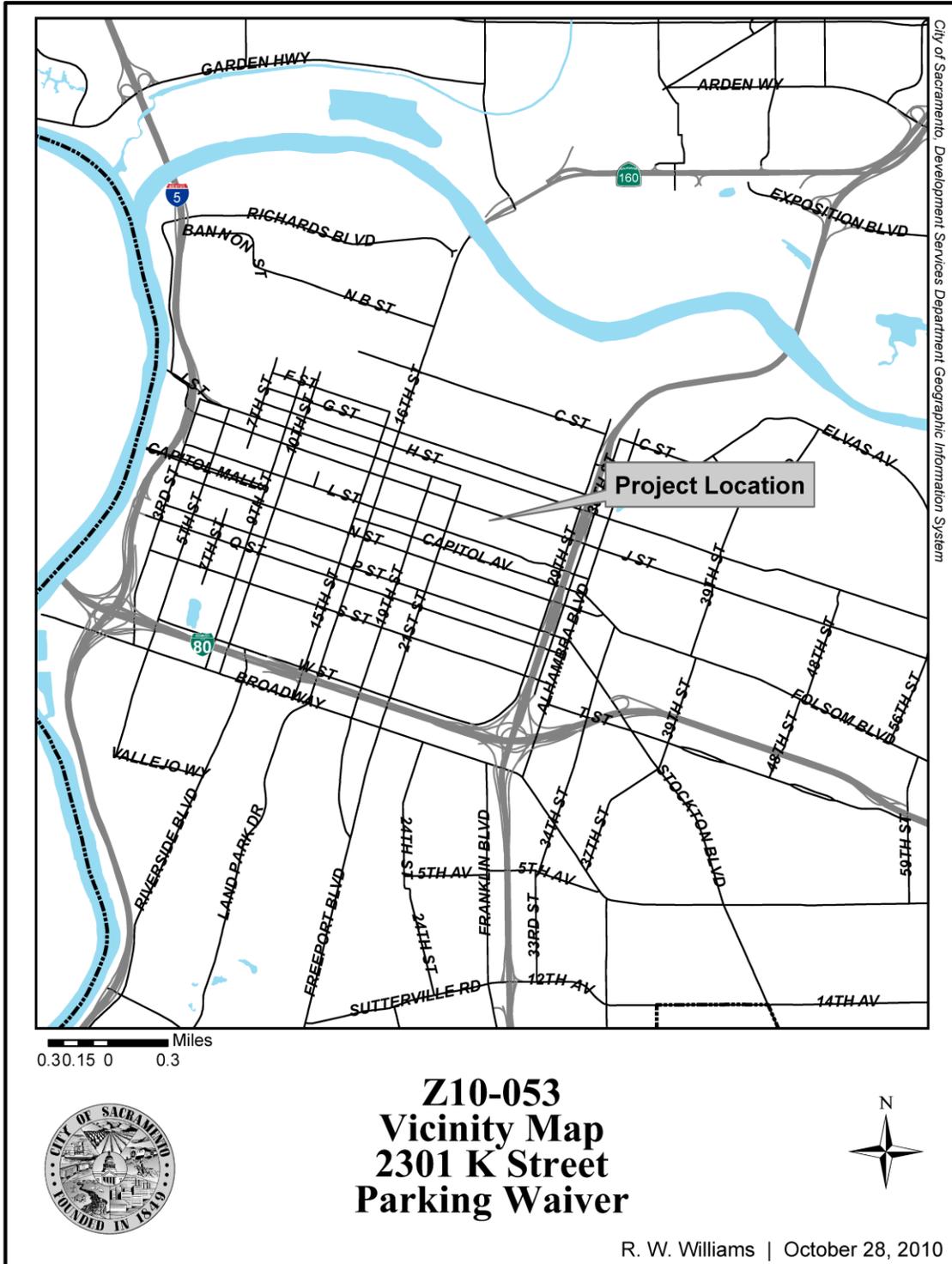
Applicant: Danny Lueng, PO Box 221633, Sacramento CA 95822

Owner: Same as Applicant

Subject: Appeal of Cornerstone Restaurant Parking Waiver (Z10-053) October 28, 2010

Appellant: Marilyn Shaffer, 2305 K Street, Sacramento CA 95816

Vicinity Map



Subject: Appeal of Cornerstone Restaurant Parking Waiver (Z10-053) October 28, 2010

Summary: The applicant is requesting a waiver of the on-site parking requirements in order to convert an existing commercial structure into a restaurant. A restaurant is an allowed use by right in the C-2-NC zone, however in order to meet the parking requirements in this area, a Special Permit to waive required parking is needed in order for the applicant to have more than thirty on-site seats. Because the building is 10,000 square feet or less, the Zoning Administrator has the authority to grant a Special Permit to waive the required parking. The structure is in poor shape and must be substantially renovated. Because the building is located within the Central City Design Review District, the exterior rehabilitation is subject to Design Review approval. The Design Review approval is staff level, however because of the concurrent Special Permit, the Staff Level Design Review is subject to approval by the Zoning Administrator.

The parking waiver request generated significant neighborhood opposition. Two public hearings on the parking waiver request were held before the Zoning Administrator on September 2, 2010 and September 16 2010. These hearings were attended by many neighbors and nearby property owners. At the second hearing, the Zoning Administrator approved the parking waiver subject to conditions. The project has been appealed to the Planning Commission by a neighboring property owner.

Table 1: Project Information
General Plan designation: Urban Corridor Low (UCORLOW)
Community Plan Area: Central City
Design Review Area: Central City
Existing zoning of site: C-2-NC (General Commercial, Neighborhood Corridor Overlay)
Existing use of site: Former Church/Office
Property area: 0.07 acres (3,200 square feet)

Background Information: The project site is 3,200 square foot lot that is located at the northeast corner of K Street and 23rd Street, within the midtown area of the Central City. The parcel contains a 3,577 square foot structure, which contains a 2,614 first story and a 963 square foot second floor on the rear portion of the building. The building dates from at least the early 1920's however there are indications that the north (rear) portion of the building could be much older. The building covers the entire rear (last 28 feet) of the property with no setbacks in this area. Along the remainder of the property, the side setbacks are between 4 to 6 feet from both sides and the front is setback approximately one foot from the property line.

The building was used by various church denominations until the early 1980's when it was converted into an office for a cable company. In 1985 a special permit was approved by the Planning Commission to reestablish a church use on the property (P85-418). This church use was discontinued prior to the late 1990's.

Subject: Appeal of Cornerstone Restaurant Parking Waiver (Z10-053) October 28, 2010

In 1998 an investigation was made by staff about the possibility of converting this building into a 75 seat restaurant (IR98-047). That investigation determined that the existing building had a parking credit of 10 spaces for the previous church use. Restaurants require 1 parking space per every 3 spaces therefore a parking waiver was required only if seating for more than 30 is provided. An application to waive parking was not submitted at that time, and the building has remained unused.

Now a new property owner is requesting to convert this building into a restaurant. They are currently proposing 57 on-site seats inside the building. This would require 19 on-site parking spaces if the building were to be constructed today. The parking credit for the prior church use is allowed to be applied, so the total parking waiver requested is 9 parking spaces. The plans show two dwellings on the small second floor which will be rehabilitated. Residential use is permitted as of right within the C-2 zone in the Central City subject to density requirements and development standards. The zoning code does not require any parking for up to two dwelling units on a parcel within the Central City that is 3200 square feet or less.

Public/Neighborhood Outreach and Comments:

Early Project notification was sent to the Midtown Neighborhood Association and the Midtown Business Association. No comments were received from the associations prior to the first Zoning Administrator hearing. Prior to the second hearing, and email was received from the Midtown Neighborhood Association stating that a parking waiver should not be granted without all cooperative use parking possibilities exhausted, with a minimum standard of provided sufficient parking for employees. The notice of the first Zoning Administrator hearing on September 03, 2010 was sent to all property owners within three hundred feet of the project site, as well as the community associations. Staff notified neighborhood groups, all property owners within a 300 foot radius, and all others who attended the previous Zoning Administrator hearings for this public hearing before the Planning Commission

Zoning Administrator Hearings:

When the property was first noticed for the hearing before the Zoning Administrator, staff received many calls concerning the project. Staff provided the project plans and other application components to neighbors for them to review. Many neighbors expressed concerns about the impact that the proposed parking waiver would have on their ability to find an on-street parking space nearby their place of residence or business. On-site parking requirements were not established until after many of the existing structures had already been constructed or converted multi-family dwellings, and other more intensive land uses.

Many neighbors expressed other concerns that were not directly before the Zoning Administrator. Some of the concerns expressed by the neighbors include allowing the property to be used for a restaurant, and the impacts that the restaurant use would have on the commercially zoned mixed-use neighborhood. These concerns include proposed

Subject: Appeal of Cornerstone Restaurant Parking Waiver (Z10-053) October 28, 2010

hours of the restaurant, the possible sale of alcohol at the restaurant, garbage storage and collection issues, and a possible future sidewalk café in the public right-of-way.

Almost all of the K Street and J Street corridors are commercially zoned. This particular area around the subject site has been zoned C-2 since at least the 1950's. The proposed restaurant is an allowed use by right on the subject site. The zoning code does not restrict the sale of alcohol for on-site consumption at restaurants. The issue of whether to allow alcohol sales at the restaurant will be regulated by the California Alcoholic Beverage Control (ABC), and will be subject to their restrictions and their review and notification process.

Plans that were originally submitted showed future sidewalk café seating within the public right-of-way. The sidewalk café would require a separate application and is subject to issuance of an encroachment permit that is issued by the Department of Transportation. Their review is a separate process that includes reviewing accessibility concerns and impacts to landscaping and city trees that are located in the public right of way. Subject to this review and approval, sidewalk cafes are allowed to be placed in the right of way next to restaurants. The zoning code also specifically exempts all sidewalk café seating in the right-of-way from any parking requirements, and it would not be subject to an existing or future parking waiver request. The applicant has not yet submitted a sidewalk café application to the Department of Transportation. The site plans have been revised to accurately reflect the existing project.

The only direct zoning code issue before the Zoning Administrator was the special permit for the parking waiver request and whether it should be granted, and if the special permit is granted what conditions should be placed on the project to help mitigate the impacts of the parking waiver request. The other issues of concern were investigated by staff and were discussed at the public hearings but they were not issues the Zoning Administrator had authority to take action on.

At the first hearing held before Zoning Administrator on September 2, 2010, the Zoning Administrator took testimony from the applicant and neighbors in attendance about the parking waiver request. There were many neighbors (approximately 20) who spoke about their concerns. After listening to testimony, the Zoning Administrator continued the item for two weeks in order to allow for additional investigation by the applicant and staff about the parking waiver request and other concerns. The applicant was directed to provide a parking survey to documents the availability of parking on surrounding blocks at various days and times in order to help determine the impact the parking waiver request.

Staff investigated the parking time restrictions in the neighborhood. The area along K Street and the number streets that are adjacent have free but limited time parking. Most of the area has a two hour limit, except for part of K Street between 22nd Street and 23rd Street, where there is a one hour time restriction. Residents are supplied with a Residential Parking Permit to allow them to park in many spaces without adhering to the

Subject: Appeal of Cornerstone Restaurant Parking Waiver (Z10-053) October 28, 2010

parking time limitations. On J Street nearby, there is paid parking, mostly limited to one hour. All of these parking time limitations end at 6 PM. Also one side of most the streets nearby are closed on various weekday mornings to provide for trash pickup and garden refuse services.

Shortly before the second hearing before the Zoning Administrator applicant did provide staff with a parking survey which showed that parking spaces were available at various times in the surrounding area. The most impacted area appeared to be along 23rd Street, between J Street and K Street. However there was usually some parking available within a block or two of the proposed restaurant. Staff cannot verify the accuracy of the parking survey.

Prior to the second hearing, staff forwarded additional concerns to the Zoning Administrator. At the second hearing on September 16, 2010, the Zoning Administrator took additional testimony from neighbors concerning the parking waiver request. Additional concerns about a proposed offsite evening parking lease were discussed. At the close of the public hearing, the Zoning Administrator approved the special permit for a parking waiver, subject conditions to help mitigate the parking waiver impacts. The conditions include limiting the hours of operation, reducing the total number of seats that are allowed in the restaurant, and further reducing the number of seats if an evening lease is not obtained. The specific conditions placed on the project are listed below in the conditions of approval.

Appeal: The third party appeal list several reasons for the appeal. Some of these statements that are listed as reasons for the appeal were not issues that were before the Zoning Administrator and are not before the Planning Commission. The Special Permit request is only for the parking waiver and not the land use. Staff cannot evaluate issues that are not directly associated with the parking waiver. Trash collection, deliveries and other operations issues must comply with all required regulations. Any future sidewalk café will require an application be submitted to the Department of Transportation for review. The zoning code specifically exempts sidewalk café seating from parking requirements, so it cannot be evaluated as part of the parking waiver. The sale of alcohol for on-site consumption at restaurants is allowed by the zoning code and is subject to the licensing by the California Department of Alcoholic Beverages (ABC).

The only issue before the Planning Commission in which neighbors have expressed any concern about is the Special Permit request to waive the required parking. If the parking waiver is granted, it can be conditioned to place restrictions on the hours of operation. If the waiver is denied, then no conditions can be placed on the restaurant if they choose to open. There could not be any restriction on the hours of operation.

Environmental Considerations: The Community Development Department, Environmental Planning Services Division has reviewed this project and determined that the project is exempt under the provisions of the California Environmental Quality Act (CEQA) Class 1, Section number 15301.

Policy Considerations:

2030 General Plan: The subject site is designated Urban Corridor Low (UCORLOW) in the 2030 General Plan. Urban Corridor Low includes street corridors that have multi-story structures and more-intense uses at major intersections, lower-intensity uses adjacent to neighborhoods, and access to transit service throughout. At major intersections, nodes of intense mixed-use development are bordered by lower-intensity single-use residential, retail, service, and office uses. Street-level frontage of mixed-use projects is developed with pedestrian-oriented uses. The streetscape is appointed with landscaping, lighting, public art, and other pedestrian amenities.

The General Plan also states that the City shall encourage the retention of existing structures and promote their adaptive reuse and renovation with green building technologies to retain the structures' embodied energy, increase energy efficiency, make it more energy efficient, and limit the generation of waste (LU 2.6.4). Lastly, the General Plan also states that the City shall reduce minimum parking standards over time to promote walkable neighborhoods and districts and to increase the use of transit and bicycles (M 6.1.2).

Special Permit to Waive Parking

The proposed conversion of this building into a restaurant with two dwellings is allowed by right and is consistent with the C-2-NC zoning of the property and the General Plan. The Zoning Code does not require a special permit to allow a restaurant on the property or to allow a bona fide restaurant sell alcohol, which is subject to ABC approval. The Special Permit before the Planning Commission is only a request to waive the required on-site parking.

This is a small parcel without any space to add parking on-site. Many existing buildings in the Central City were constructed or converted to a higher intensity use prior to current parking regulations. Most structures do not have adequate parking to meet current requirements. For many residents and businesses the only place to park is on the street.

The zoning code requires that any change of use of an existing structure that requires more parking than the prior use must be made up on-site or else obtain approval by special permit to waive required parking or by a special permit to provide exclusive (24 hours) parking on another parcel.

The Zoning Code allows for a credit to be applied for existing structures that were constructed prior to current parking requirements. The credit would be based upon the use of the building prior to current parking requirements. After this application staff determined that a prior parking credit of 10 parking spaces could be applied to any new use in the building.

The applicant's original plans showed 61 on-site (on the parcel) seats. This included 57 seats inside the restaurant and 4 (on the parcel) seats at tables that were adjacent to the west side of the building. A 61 seat restaurant would require 20 parking spaces if

Subject: Appeal of Cornerstone Restaurant Parking Waiver (Z10-053) October 28, 2010

constructed today (1 per 3 seats), but with the 10 space parking credit, the requested waiver before the Zoning Administrator was considered to be 10 spaces. The applicant has reduced the proposed on-site seating to 57 total seats. This would require 19 on-site spaces for new construction. With the preexisting credit of 10 parking spaces the current parking waiver request is for 9 spaces.

Table 2: Parking				
Use	Required Parking	Parking Credits	Proposed Parking	Difference
Restaurant with 57 seats. (1 per 3 seats)	19	10	0	9

The Zoning Administrator had conditioned the parking waiver to allow a maximum of 51 seats if either the restaurant closes by 5:30 PM or if an evening lease for at least 7 parking spaces is provided. Obtaining a lease for at least 7 parking spaces would allow the restaurant to stay open until 9:00 PM. If they cannot obtain a nearby evening lease then the restaurant is only allowed to have 41 seats in order to remain open past 5:30 PM, until a closing time of 9:00 PM: The reasoning behind the evening only lease is because most parking spaces on properties in the area are taken up during by the business that exists on those properties during the day. Leased parking spaces should be from nearby offices or similar uses that are not open during the evening. It is more difficult to find parking spaces that can be leased legally during the daytime.

The Zoning Administrator approved the special permit request to waive 9 spaces subject to conditions and based on findings of fact. Now that the project has been appealed, the Planning Commission has the authority to either approve or deny the parking waiver. In evaluating special permit proposals of this type, the Commission is required to make the following findings:

- A. A special permit shall be granted upon sound principles of land use.
In this case, staff finds that the restaurant is an allowed land use in the C-2-NC zone and project is located within a commercially zoned neighborhood with on-street parking and time limits to serve the proposed restaurant and ensure that parking spaces turn over for the businesses in the area.

- B. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.
There is no room to provide on-site parking and the proposed conditions are designed to minimize impacts of the parking waiver. Bicycle parking will be provided, and a large inside waiting area will be provided.

Subject: Appeal of Cornerstone Restaurant Parking Waiver (Z10-053) October 28, 2010

- C. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.

The project is consistent with the General Plan which designates the site as Urban Corridor Low (UCORLOW).

Staff recommends parking waiver be granted with the condition that the seating be reduced to a maximum of 51 seats (a 7 space waiver) if the applicant either obtains a nearby evening lease (for 7 spaces), or else is not open during the evening, as described in recommend condition number B4. If the applicant cannot obtain a nearby evening lease, and desires to be open in the evening staff recommends that the seating be reduced to 41 seats (a 3 space waiver). Staff recommends maximum hours of operations be set as in the recommended conditions of approval.

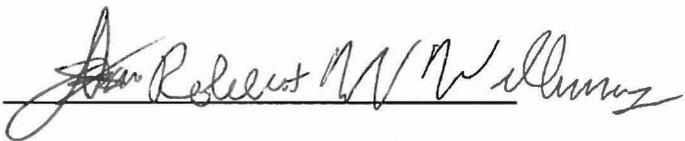
Building design, signage and landscaping

The existing building, originally constructed as a church, is being rehabilitated. There are no proposed additions. The exterior changes are, a new parapet to match the existing front parapet in order to hide mechanical equipment, new stairs to an existing second floor door (east side), window removal along the east side property line near the rear of the structure, a new canopy above the front entry, new exterior lights, new skylights, new 30 year compositional roofing material, and new railing on the existing ramp. The existing entry doors and window will be removed and replaced. All the other doors and windows are preexisting and they will remain. There is no proposed landscaping. Signage has not been submitted.

The exterior rehab would have only been subject to staff level Design Review approval, with no public hearing required. However, since there was a concurrent Special Permit request before the Zoning Administrator, the Zoning Administrator also took action to approve the Staff Level Design Review. The entire project is therefore subject to the appeal that is before the Planning Commission.

Design Review staff have reviewed the proposed renovations and have recommended the proposed Design Review conditions. None of the comments that have been received by staff are specific to the items that Design Review staff has reviewed and made recommendations for the project. Staff recommends the Planning Commission approve the Staff Level Design Review (DR10-110).

Subject: Appeal of Cornerstone Restaurant Parking Waiver (Z10-053) October 28, 2010

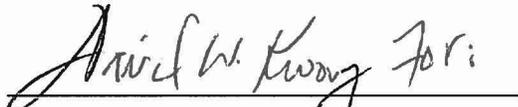
Respectfully submitted by: 

ROBERT W. WILLIAMS
Associate Planner

Approved by: 

SANDRA YOPE
Senior Planner

Recommendation Approved:


GREGORY BITTER, AICP
Principal Planner

Attachments:

Attachment 1 Recommended Findings of Fact and Conditions of Approval

Exhibit 1A Site Plan
Exhibit 1B First Floor Plan
Exhibit 1C Second Floor Plan
Exhibit 1D Elevations
Exhibit 1E Color Elevations
Exhibit 1F Appeal Letter
Exhibit 1H Applicant Parking Survey
Exhibit 1I Letters
Exhibit 1J Photos

Attachment 2 Aerials

Attachment 3 Land Use & Zoning Map

Attachment 1
Proposed Findings of Fact and Conditions of Approval
Appeal of Cornerstone Restaurant Parking Waiver (Z10-053)
2301 K Street

Findings Of Fact

- A. Environmental Determination:** Based on the determination and recommendation of the City's Environmental Planning Services Manager and the oral and documentary evidence received at the hearing on the Project, the Planning Commission finds that the Project is exempt from review under Section 15301, Existing Facilities of the California Environmental Quality Act Guidelines as follows: The project consists of the minor alteration of an existing private structure, involving negligible expansion of use beyond that existing.
- B. The Special Permit** to waive the required on-site parking spaces is approved subject to the following Findings of Fact and Conditions of Approval:
1. Granting the special permit is based upon sound principles of land use in that:
 - a. An restaurant is an allowed use in the in the General Commercial, Neighborhood Corridor overlay (C-2-NC) zone; and
 - b. The project is located in a commercial neighborhood with sufficient on-street parking and time limits to serve the proposed restaurant and ensure that parking spaces turn over for the businesses in the area.
 2. Granting the special permit would not be detrimental to the public welfare nor result in the creation of a public nuisance in that:
 - a. The project is conditioned to minimize impacts to surrounding properties; and
 - b. The existing commercial building covers most of the 3,200 square foot lot and there is no room to provide parking on the subject site; and
 - c. A waiting area for patrons is provide inside the building; and
 - d. Bicycle parking will be provided.
 3. The project is consistent with the General Plan which designates the site as Urban Corridor Low (UCORLOW).
 - a. The **Staff Level Design Review** request to rehabilitate an existing commercial structure is approved, subject to the following Findings of Fact and Conditions of Approval:
- C. The Staff Level Design Review (DR10-110)** request to rehabilitate an existing commercial structure (a former church) into a restaurant is approved, subject to the following Findings of Fact and Conditions of Approval:

Subject: Appeal of Cornerstone Restaurant Parking Waiver (Z10-053) October 28, 2010

1. The project is based upon sound principles of land use, in that the proposed use is allowed in the General Commercial, Neighborhood Corridor overlay (C-2-NC) zone; and
2. The proposed use will be consistent with the objectives of the City of Sacramento General Plan.
3. The project, as conditioned, will complement structures in the vicinity, and conforms to the design criteria set forth by the Design Commission.

Conditions of Approval

The **Special Permit** to waive the required on-site parking spaces is approved subject to the following Conditions of Approval:

General

- B1. The proposed project shall conform to the submitted plans. Any changes to the approved site or building size or other plans are subject to additional Planning review and approval.
- B2. The applicant shall obtain all necessary permits before beginning construction.
- B3. The design of the project must conform to all design review conditions of approval for file DR10-110.
- B4. Restaurant seating and hours of operation are as follows:

Lease:	Hours: ^B	Maximum Seats: ^C
No	6:00 am – 5:30 pm	51
No	6:00 am – 9:00 pm	41
Yes ^A	6:00 am – 9:00 pm	51

- A. The applicant shall provide a lease for a minimum of seven (7) parking spaces within two (2) blocks of the restaurant between hours of 5:30 pm and 9:30 pm. The applicant shall provide staff with a final copy of the signed lease. If a lease is provided outside the two block radius, staff will review the lease location to determine the lease is acceptable for the parking waiver. If leased parking is provided the applicant shall place a sign in the restaurant and/or the menu indicating the availability of limited off-street parking.
- B. The restaurant shall not be open earlier than 6:00 AM and shall not be open later than 9:00 PM, seven days a week.
- C. Maximum number of seats allowed on the subject site. Does not include any seating in the public right-of-way that may be approved in the future.
- B5. The areas on the on the west and east sides of the main entry door facing K Street shall not be used for table service. They shall be designated for customer waiting areas only.

Subject: Appeal of Cornerstone Restaurant Parking Waiver (Z10-053) October 28, 2010

- B6. The applicant shall apply to the City Department of Transportation for a bicycle parking facility to be installed in front of the business in the public right-of-way within 30 days after the issuance of the Certificate of Occupancy. The applicant shall also install a bicycle parking facility on the west side of the building unless it interferes with ADA accessibility requirements.
- B7. The small office on the second floor shall not be separately leased, and can only be used by the restaurant or the residents of the property. No more than two dwelling units shall be allowed upstairs.
- B8. A signed copy of the Affidavit of Zoning Code Development Standards and each of the pages of this Record of Decision shall be scanned and inserted as a general sheet(s) in the plan set for any building permit submittal associated with this project.
- B9. An affidavit signed by the applicant that affirms the plans submitted for the Building Permit comply with all conditions of approval and approved exhibits shall be included on full-size sheets as part of the Building Permit plans submittals.

Department of Transportation:

- B10. Construct standard public improvements as noted in these conditions pursuant to Chapter 18 of the City Code. Improvements shall be designed to City Standards and assured as set forth in Chapter 18.04.130 of the City Code. All improvements shall be designed and constructed to the satisfaction of the Department of Transportation. Any public improvement not specifically noted in these conditions shall be designed and constructed to City Standards. This shall include street lighting and the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk fronting the property along K Street and 23rd Street per City standards to the satisfaction of the Department of Transportation.

The **Staff Level Design Review (DR10-110)** request to rehabilitate an existing commercial structure (a former church) into a restaurant is approved, subject to the following Findings of Fact and Conditions of Approval:

- C1. Existing cement plaster walls shall be retained and repainted per approved plans and color rendering.
- C2. A cement plaster mechanical well with a crenellated cornice shall be provided to match existing crenellated element at the front of the building.
- C3. Existing windows at east elevation shall be removed and replaced with cement plaster to match existing per approved plans.
- C4. Existing windows at north, east, south, and west elevations shall be retained and repainted per approved plans and color rendering.

Subject: Appeal of Cornerstone Restaurant Parking Waiver (Z10-053) October 28, 2010

- C5. Entry window and door shall be removed and replaced per approved plans. Final wood entry door with glazing shall be coordinated with Design Review staff during Building Plan Check review.
- C6. Entry canopy shall be provided per approved plans. Final suspension and canopy details shall be coordinated with Design Review staff during Building Plan Check review.
- C7. Trim shall be painted per approved plans and color rendering.
- C8. New stair at the east elevation shall be provided to meet City Standard stair detail, and final detailing shall be coordinated with Design Review staff during Building Plan Check review.
- C9. Wall sconce lighting shall be provided at 10' with final design and cut sheets provided and coordinated with Design Review staff during Building Plan Check review.
- C10. Skylights shall be provided per approved plans and elevations.
- C11. Existing Spanish tile roof shall be retained per approved plans.
- C12. Minimum 30-year laminated dimensional composition shingle with heavy ridge caps shall be provided per approved plans.
- C13. All mechanical systems shall be located behind and not exceed the height of the mechanical parapet per approved plans.
- C14. The sidewalk café shall be submitted to the Department of Transportation for review and approval. Design Review staff will review the submittal at that time.
- C15. The applicant shall consult with the Solid Waste Services Division of the Department of Utilities to arrange the required trash collection services. Any on-site trash cans and recycling containers shall be stored wholly within the building. No containers or similar objects shall be stored behind the building.
- C16. All proposed signage shall comply with the Sign Ordinance and Building Code, respectively, and shall be reviewed and approved by Design Review staff prior to issuance of required sign permits.
- C17. All other notes and drawings on the final plans as submitted by the applicant are deemed conditions of approval. Any changes to the final set of plans stamped by Design Review staff shall be subject to review and approval prior to any changes. Applicant shall comply with all current building code requirements.
- C18. The Conditions of Approval shall be scanned and inserted into the final set as a general sheet to be submitted for Building Permit.
- C19. A signed copy of the Affidavit of Zoning Code Development Standards shall be scanned and inserted into the final set as a general sheet to be submitted for building permit.

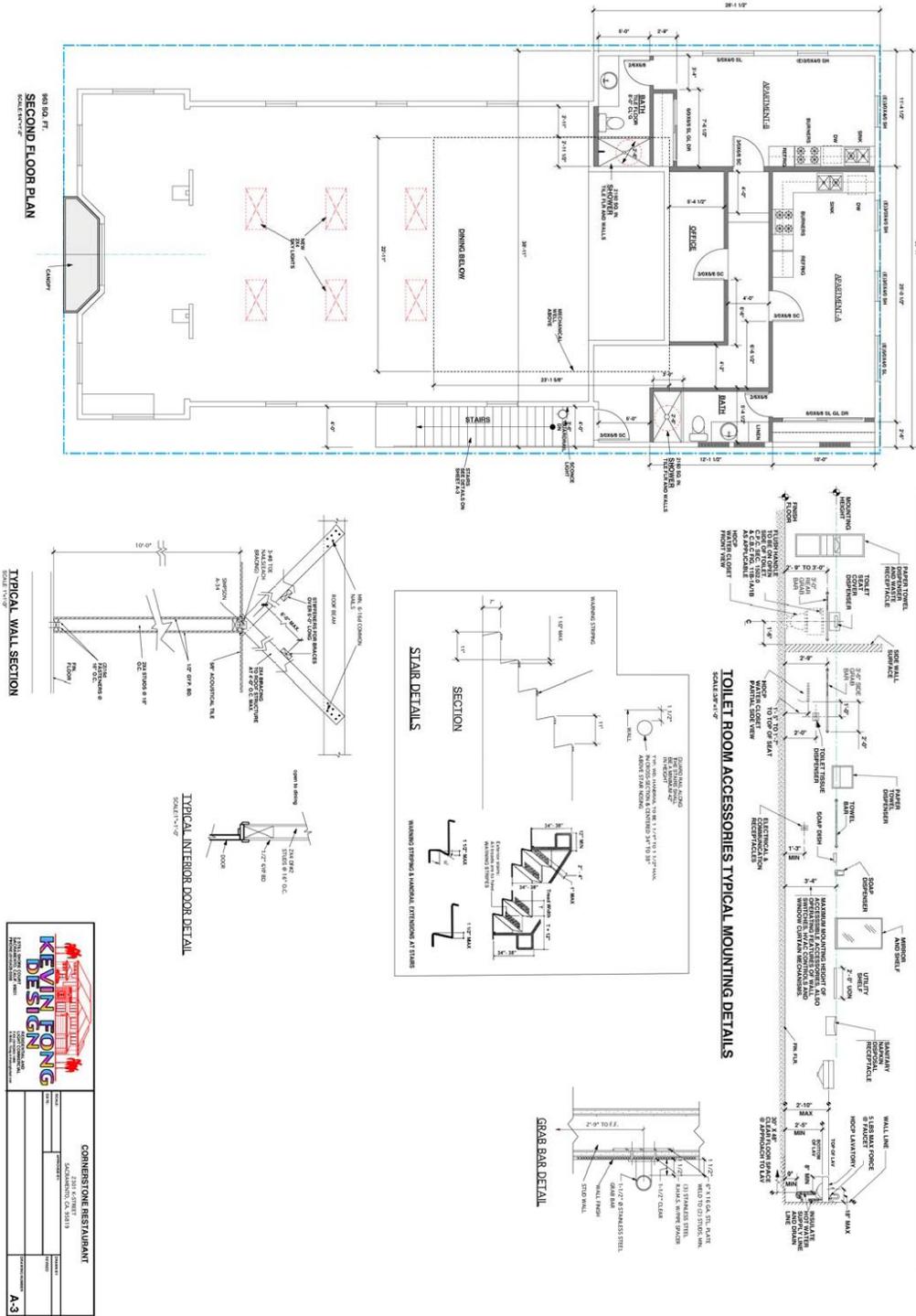
Subject: Appeal of Cornerstone Restaurant Parking Waiver (Z10-053) October 28, 2010

- C20. Design Review shall review the plans and cut sheets submitted for Building Permit and coordinate final details with applicant during the Building Plan Check cycle.

Advisory Notes:

1. *PLANNING*: A sidewalk café application is required to be reviewed and approved prior to establishing any seating in the public right-of-way.
2. *PLANNING*: If the applicant chooses to limit the on-site seating to 30 seats or less, then the parking waiver is not required. The Design Review conditions of DR10-110 will still remain applicable for the exterior renovations.
3. *BUILDING*: If there is outside seating, provide wheelchair seating at the outside dining area.
4. *BUILDING*: When door swings onto top landing, the landing width in the direction of egress travel shall have 42" + door width which equals to 6'-6" per CBC Figure 11B-39.
5. *BUILDING*: Provide bar counter with 60" long companion seats per DSA Guidelines.
6. *BUILDING*: Post maximum seating capacity in dining area.
7. *BUILDING*: Provide handrails & handrail extensions on ramp.
8. *BUILDING*: Provide emergency power supply system for means of egress illumination with battery back-up system for a duration of not less than 90 minutes per CBC section 1006.3.
9. *BUILDING*: Provide handrails on each side of stairway per CBC section 1012, & provide guardrail on the open-sided stairway per CBC section 1013.
10. *BUILDING*: Provide two-hour occupancy separation between A-2 & R per CBC Table 508.3.3.
11. *BUILDING*: Provide mix occupancies allowable area computations per CBC section 508.3.3.
12. *BUILDING*: Provide shear & overturning moment design calculations on west wall because of new openings.
13. *BUILDING*: Provide engineering & attachment details for the new awning.
14. *DEPARTMENT OF TRANSPORTATION*: The site plan shows a proposed café seating along the frontage of the building. The applicant shall apply for a revocable permit for the use of the public right-of-way for a café seating and must meet all the requirements for that permit. The applicant shall also be required to apply for an encroachment permit to allow for the construction of the Café seating with the Department of Transportation. Please contact Cesar Narvaez at (916) 808-7873 to inquire about the requirements of both permits.

Exhibit 1C Second Floor Plan

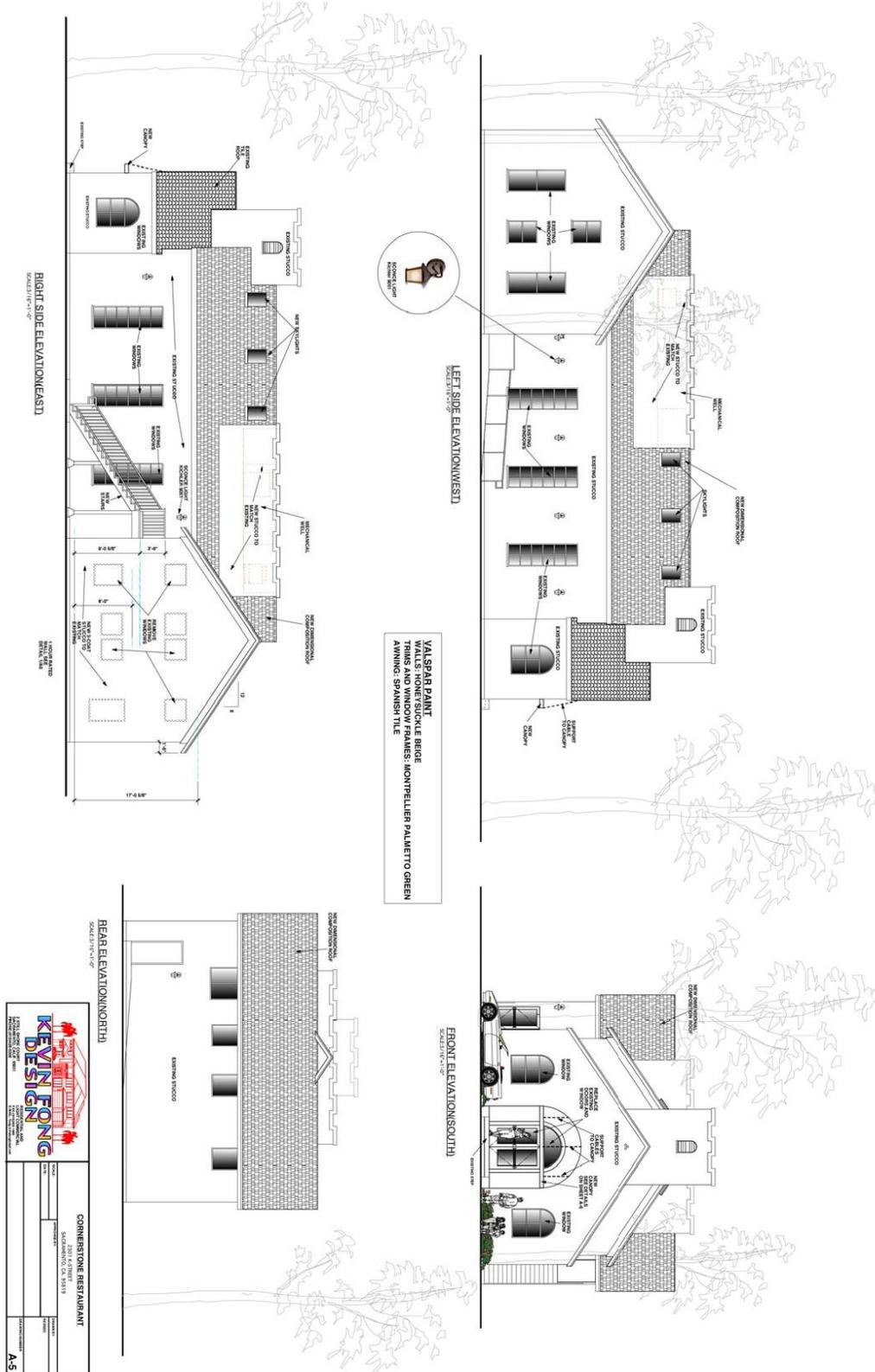


KEVIN FONG DESIGN
 ARCHITECTURAL DESIGN
 14111 WILSON BLVD, SUITE 100
 BELLFLOWER, CA 91706
 TEL: (909) 876-8888
 FAX: (909) 876-8889
 WWW.KFDESIGN.COM

CORNERSTONE RESTAURANT
 2201 CORNERSTONE
 SUITE 100
 ROCKLEDGE, CA 94981
 TEL: (415) 499-1111
 FAX: (415) 499-1112
 WWW.CORNERSTONE.COM

A-3

Exhibit 1D Elevations





FRONT ELEVATION(SOUTH)
SCALE: 3/16"=1'-0"

VALSPAR PAINT
WALLS: HONEYSUCKLE BEIGE
TRIMS AND WINDOW FRAMES: MONPELLIER PALMETTO GREEN
AWNING: SPANISH TILE



LEFT SIDE ELEVATION(WEST)
SCALE: 3/16"=1'-0"



	CORNERSTONE RESTAURANT 2301 K STREET SACRAMENTO, CA 95819	
	DATE: _____	PROJECT: _____
	BY: _____	REVISION: _____
	1. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CITY OF SACRAMENTO, CALIFORNIA.	DRAWN BY: _____ CHECKED BY: _____ DATE: _____

Grounds for Appeal to Zoning Administrators action

Date: September 27, 2010

Re: Location: Cornerstone proposal for restaurant at 2301 K Street

This appeal is being filed by a group of neighbors, business owners and property owners who live and work within a block of 2301 K Street, the proposed new location for Cornerstone Restaurant. Having been frequent patrons, we would like to see Cornerstone find a new location in our neighborhood where they can thrive. However, we feel that their proposal for a 83-seat restaurant at this particular site is too big in terms of parking needs and activity level.

We compliment Zoning Administrator Patterson for listening to our concerns and restricting the extent of the parking waiver and reducing the open hours.

However, we appeal the decision to the City of Sacramento Planning Commission and recommend that for the proposed project at this site:

1. No waiver of parking be granted
2. The hours be restricted to 6am-9pm on weekdays and 7am-9pm on weekends.
3. The trash storage be held wholly inside the building
4. Truck deliveries be restricted to the alley only

A. BACKGROUND

The mix of business and residents in Midtown is what we love the most and why we purchased property here. Balancing the needs of each is a challenge for the City. The Zoning Code and many City processes have been developed to keep the balance in check.

Our neighborhood is a C-2-NC Zone which is described below:

“The purpose of the neighborhood corridor (NC) overlay zone is to encourage the development of new housing in specified general commercial (C-2) zoned corridors located within low-to-medium density residential neighborhoods. At the same time, **some restrictions on commercial or office development are established in order to ensure compatibility with adjacent residential uses.** It is intended to allow a wider range of commercial uses than the RMX or C-1 zones, but at less intensity than the C-2 zone. Mixed-use projects, in which commercial or office uses are contained in the same building or on the same site as residential, are encouraged.”

We emphasize the residential component of the zone description because particularly at the corner of 23rd and K Streets, residences are the predominate land use. While this project proposes a large restaurant with two studios upstairs, it is the size of the restaurant that we object to, not the residential component.

B. PARKING WAIVER

Finding a parking space in this area is already difficult. One reason is due to the lack of on-site parking at nearby businesses, churches, restaurants and residences. In addition, there are a higher number of restaurants on our block than the surrounding blocks and we have discovered that most have obtained parking waivers from the City.

Parking allocations: the initial request for waiver of the entire parking requirement for the restaurant was for 20 parking spaces. City staff found that the former church had a parking credit of 10 spaces, resulting with a need for a waiver of 10 spaces. By reducing the seat count, the Zoning Administrator’s ruling waived 7 instead of 10. However, the project also described a total of 22 dining seats in the Street Right-Of-Way and these therefore are not included in the parking requirement. This waiver therefore results in an overall total of 24 parking spaces not provided for this project. There are only 25 parking spaces available on the entire block of K Street (both sides) to give you an idea of the size of impact this restaurant will have on the neighborhood parking.

Cornerstone parking survey results inaccurate: Cornerstone was asked to conduct a parking survey after the first hearing. The results of the parking survey were not shared with the hearing attendees. We believe that the results identified in the survey are inaccurate because any assessment of the current parking situation does not include future imminent impacts due to:

- a. The replacement restaurant at the former location at 2330 J St after Cornerstone closed their doors.
- b. The approved restaurant at 24th and K Street that is not yet constructed
- c. The vacant church at 24th & K.
- d. The vacant Hina’s Tea Shop
- e. The soon to be opened TexMex Restaurant
- f. The Beach Hut restaurant expansion requested waiver of parking (Z10-082)
- g. Other vacancies due to the current economy

Employee parking next door was provided at the former location and employee parking should be provided at this location too. However, the **evening only** lease of parking nearby does not mitigate the parking need at this location. The increased need for parking due to this restaurant will be especially acute at breakfast and lunch times in addition to the dinner hour. The parking lease for employees should be for the entire time that the employees are at work. Note that all of the businesses on this block provide at least some parking for their employees.

The impact of delivery trucks was not considered in the ruling. At the former location, this restaurant had deliveries at various times throughout the day on the alley. If the delivery trucks and trash retrieval trucks park anywhere on 23rd Street or K Street, the street will be blocked, significant parking will not be accessible, and the path for deliveries will be through vehicles, bike lanes, pedestrians and restaurant patrons, thus creating a safety hazard.

In summary, because parking in this area is already heavily impacted due to already existing parking waivers, the size of this project impact will jeopardize the viability of the nearby businesses and certainly inconvenience residents to the point of relocation. Therefore we believe the waiver of parking should NOT be approved and delivery and retrieval trucks should be restricted to parking in the alley in order to provide a safe environment.

C. HOURS OF OPERATION

The opening hours do not take into account weekend needs of the nearby residents. We recommend that the opening hours be set at 7 am per Zoning code 8.68.060 Exterior Noise Standards. The former location opened at 7 am on weekends.

The 9pm close time is a good compromise with the nearby residents. Of course we would prefer an even earlier close. However, we continue to have concerns about Cornerstone’s statement that they intend to renew their liquor license and about the offer during the hearing from the Zoning Administrator that the applicants may apply for a change of hours after 6 months time. We fear that they will request a later close time in the future.

Recommendation: Our recommendation is to slightly modify the Zoning Administrator’s ruling to open at 7 am on weekends.

D. WASTE STORAGE AND PUBLIC SAFETY

There is no outdoor space available on the property for waste storage. The option mentioned during the hearing was for a dumpster to be located on the alley. However, the alley consists of driveways for parking for the existing businesses. In addition, it is unlikely that any of the property owners on this block will agree to allow a dumpster to sit on their property or block their property access. This is especially true since those of us on this block are already familiar with Cornerstone’s previously poorly maintained dumpster. In addition, transporting trash from the restaurant around the corner to the alleyway will create a public safety hazard due to pedestrian conflicts, slips and falls from dropped bits of trash as well as rodent and pest issues.

Zoning Code 17.72.040 requires a trash enclosure be provided by “any commercial project when not wholly contained within the building.” No waiver of the trash enclosure was requested or permitted in this ruling so we believe that compliance is required. In addition, it would appear that recycling is also required and that the handling of grease should also be per City requirements.

Recommendation: Because there is no space available for trash storage on site, we recommend that all trash storage be contained wholly within the building.

E. OTHER NEIGHBORHOOD CONCERNS AND QUESTIONS:

1. The owners of the proposed Cornerstone Restaurant have not communicated with residents, churches, and other businesses in the neighborhood during the planning phases. This lack of coordination and collaboration is of concern to all of us. Their failure to develop relationships or initiate even the most basic communication with the neighbors could make it difficult to solve future problems. Lack of collaboration between the applicants with their neighbors could cost the City of Sacramento additional and preventable administrative costs in resolving problems in the short term and long run.
2. We believe that the statement in the first paragraph of "Additional Information" regarding there formerly being "two existing dwellings" in the building is not correct. Residents who have lived on this block since the mid 1970's have never seen anyone living in the building other than homeless campers. There are no bathrooms or other necessary facilities on the upper level of the building.
3. There is also a statement that there are "no setbacks in the rear of the property." If the rear property line is at the back wall, how can the back door exit onto another property? If there is a small setback (i.e. at the roof overhang) then is it possible to create a legal exit with only a 24" walkway on this property?
4. The Conditions of Approval # 5 is not clearly stated and could imply that the waiting area is allowed outside the building, thereby disrupting the public sidewalk and creating a public nuisance. We recommend that the word "inside" be added.
5. Some neighbors expressed concerns that the building will be overly-utilized which worsens current parking problems. While the area is zoned for commercial uses, the neighborhood is also concerned about over-saturation of food and alcohol establishments in the area which adversely impact residential property values and the overall safety and quality of life in the neighborhood.

F. DESIGN REVIEW

Exterior Design: The statement (page 4 first paragraph) "Staff did not receive any specific comments relating to the exterior design of the structure" is incorrect. Comments were made regarding landscaping, fences, door heights, windows, etc. through letters and testimony.

Design Review Conditions of Approval item 14 conflicts with 13. "East" should be deleted from 14.

Item 18: The head height of the exiting door from the 2nd floor will require roof modifications which are not shown or mentioned.

Advisory Notes Item 2: If the dining seats are reduced to 30, the other aspects of hours, trash, etc. should still apply in order to protect the public health and safety.

G. SUMMARY OF APPEAL TO CORNERSTONE RESTAURANT

By all appearances, this ill-conceived project overburdens the existing property and structure. Additionally it creates an unacceptable impact to the neighborhood businesses, residents and services required for its proposed use.

By filing this appeal, we are requesting that **the project simply comply with the City of Sacramento Zoning Ordinance** for parking and trash. Specifically:

1. No parking waiver be granted
2. The hours should be restricted to 6am-9pm on weekdays and 7am-9pm on weekends.
3. The trash storage be held wholly inside the building
4. Deliveries be restricted to the alley only

Sincerely,

Ann Bardy from church at 1017 23rd Street
Franc Blackbird owns building at 2320 J Street
Elizabeth Emerson from church at 1017 23rd Street
Nick Karas owns building at 2300 J Street
Anna Maria Marquez from church at 1017 23rd Street
Steve Pitsker from house at 1018 23rd Street
Randy Ruiz from church at 1017 23rd Street
Marilyn Shaffer from house at 2305 K Street
Lynn Whigham from church at 1017 23rd Street
Portia Zraggen from church at 1017 23rd Street

2301 K Street

Kevin Fong Design
 Parking Survey
 September 3, 2010

16 blocks surveyed September 14, 2010.		9:00AM		12:00PM		5:30PM		8:00PM	
Day	Total Spaces	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE
MON	446	138	308	233	213	272	174	214	232
TUES	446	128	318	234	212	228	218	193	253
WED	446	162	284	232	214	284	162	220	226
THUR	446	142	304	246	200	286	160	211	235
FRI	446	159	287	280	166	251	195	274	172
SAT	446	180	266	257	189	299	147	404	42
SUN	446	194	252	254	192	204	242	204	242

Kevin Fong Design
 2 Stillshore Court
 Sacramento, CA 95831
 (916) 428-5008

Exhibit 1H Applicant Parking Survey (cont.)

2301 K Street

Kevin Fong Design
 Parking Survey
 September 3, 2010

Monday Breakdown			9:00AM	12:00PM	5:30PM	8:00PM				
Area Street	Intersection	Total Spaces	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE		
1	L	22 & 23	12	14	14	12	10	16		
2	23	J & K	6	26	16	16	25	7		
3	K	23 & 24	8	17	19	6	16	9		
4	23	K & L	6	20	12	14	15	11		
5	23	L & CAPITAL	8	25	7	26	10	23		
6	L	22 & 23	12	12	10	14	14	10		
7	22	K & L	7	24	17	14	27	4		
8	K	21 & 22	14	9	15	8	17	6		
9	22	J & K	8	32	16	24	33	7		
10	J	22 & 23	3	20	22	1	8	15		
11	23	I & J	11	23	19	15	29	5		
12	J	23 & 24	5	17	7	15	7	15		
13	24	J & K	2	27	18	11	20	9		
14	K	24 & 25	14	11	20	5	17	8		
15	24	K & L	8	20	17	11	6	22		
16	L	23 & 24	14	11	4	21	18	7		
		SUBTOTAL	138	308	233	213	272	174	214	232

Kevin Fong Design
 2 Stillshore Court
 Sacramento, CA 95831
 (916) 428-5008

Exhibit 1H Applicant Parking Survey (cont.)

2301 K Street

Kevin Fong Design
 Parking Survey
 September 3, 2010

Tuesday Breakdown			9:00AM	12:00PM	5:30PM	8:00PM				
Area Street	Intersection	Total Spaces	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE		
1	L	22 & 23	5	21	15	11	8	18		
2	23	J & K	10	22	17	15	26	6		
3	K	23 & 24	4	21	15	10	15	10		
4	23	K & L	15	11	10	16	15	11		
5	23	L & CAPITAL	13	20	6	27	7	26		
6	L	22 & 23	2	22	11	13	6	18		
7	22	K & L	16	15	19	12	19	12		
8	K	21 & 22	2	21	17	6	12	11		
9	22	J & K	7	33	18	22	28	12		
10	J	22 & 23	0	23	22	1	5	18		
11	23	I & J	19	15	19	15	24	10		
12	J	23 & 24	2	20	7	15	4	18		
13	24	J & K	2	27	18	11	19	10		
14	K	24 & 25	6	19	15	10	18	7		
15	24	K & L	16	12	20	8	4	24		
16	L	23 & 24	9	16	5	20	18	7		
SUBTOTAL			128	318	234	212	228	218	193	253

Kevin Fong Design
 2 Stillshore Court
 Sacramento, CA 95831
 (916) 428-5008

Exhibit 1H Applicant Parking Survey (cont.)

2301 K Street

Kevin Fong Design
 Parking Survey
 September 3, 2010

Wednesday Breakdown			9:00AM	12:00PM	5:30PM	8:00PM			
Area Street	Intersection	Total Spaces	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE	
1	L	22 & 23	10	16	14	12	11	15	
2	23	J & K	7	25	17	15	23	9	
3	K	23 & 24	9	18	20	5	18	7	
4	23	K & L	8	18	8	18	16	10	
5	23	L & CAPITAL	10	23	6	27	12	21	
6	L	22 & 23	14	10	12	12	16	8	
7	22	K & L	7	24	18	13	28	3	
8	K	21 & 22	12	11	16	7	19	4	
9	22	J & K	9	31	14	26	35	5	
10	J	22 & 23	6	17	22	1	10	13	
11	23	I & J	15	19	19	15	26	8	
12	J	23 & 24	9	13	9	13	8	14	
13	24	J & K	4	25	16	13	22	7	
14	K	24 & 25	14	11	21	4	15	10	
15	24	K & L	12	16	16	12	7	21	
16	L	23 & 24	16	9	4	21	18	7	
		SUBTOTAL	162	284	232	214	284	162	220
								226	

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 2 Stillshore Court
 Sacramento, CA 95831
 (916) 428-5008

Exhibit 1H Applicant Parking Survey (cont.)

2301 K Street

Kevin Fong Design
 Parking Survey
 September 3, 2010

Thursday Breakdown		9:00AM		12:00PM		5:30PM		8:00PM		
Area Street	Intersection	OCUPIED	AVAILABLE	OCUPIED	AVAILABLE	OCUPIED	AVAILABLE	OCUPIED	AVAILABLE	
1	L	22 & 23	26	10	16	17	9	11	15	
2	23	J & K	32	17	15	20	12	30	2	
3	K	23 & 24	25	8	17	20	5	17	8	
4	23	K & L	26	9	17	11	15	18	8	
5	23	L & CAPITAL	33	5	28	5	28	11	22	
6	L	22 & 23	24	4	20	8	16	11	13	
7	22	K & L	31	9	22	19	12	29	2	
8	K	21 & 22	23	14	9	15	8	17	6	
9	22	J & K	40	12	28	19	21	34	6	
10	J	22 & 23	23	3	20	20	3	7	16	
11	23	I & J	34	14	20	17	17	28	6	
12	J	23 & 24	22	3	19	9	13	5	17	
13	24	J & K	29	11	18	21	8	22	7	
14	K	24 & 25	25	4	21	22	3	20	5	
15	24	K & L	28	15	13	20	8	6	22	
16	L	23 & 24	25	4	21	3	22	20	5	
SUBTOTAL			142	304	246	200	286	160	211	235

Kevin Fong Design
 2 Stillshore Court
 Sacramento, CA 95831
 (916) 428-5008

Exhibit 1H Applicant Parking Survey (cont.)

2301 K Street

Kevin Fong Design
 Parking Survey
 September 3, 2010

Friday Breakdown			9:00AM	12:00PM	5:30PM	8:00PM				
Area Street	Intersection	Total Spaces	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE		
1	L	22 & 23	12	14	20	6	7	19		
2	23	J & K	20	12	25	7	20	12		
3	K	23 & 24	8	17	20	5	22	3		
4	23	K & L	7	19	12	14	14	16		
5	23	L & CAPITAL	6	27	7	26	9	24		
6	L	22 & 23	4	20	11	13	14	10		
7	22	K & L	7	24	26	5	21	10		
8	K	21 & 22	18	5	16	7	20	3		
9	22	J & K	15	25	28	12	22	18		
10	J	22 & 23	5	18	18	5	12	11		
11	23	I & J	18	16	20	14	21	13		
12	J	23 & 24	2	20	7	15	5	17		
13	24	J & K	15	14	23	6	16	13		
14	K	24 & 25	5	20	22	3	23	2		
15	24	K & L	15	13	23	5	19	9		
16	L	23 & 24	2	23	2	23	6	19		
SUBTOTAL			159	287	280	166	251	195	274	172

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 2 Stillshore Court
 Sacramento, CA 95831
 (916) 428-5008

Exhibit 1H Applicant Parking Survey (cont.)

2301 K Street

Kevin Fong Design
 Parking Survey
 September 3, 2010

Saturday Breakdown			9:00AM	12:00PM	5:30PM	8:00PM				
Area Street	Intersection	Total Spaces	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE		
1	L	22 & 23	8	18	18	8	20	6		
2	23	J & K	24	8	26	6	21	11		
3	K	23 & 24	11	14	14	11	13	12		
4	23	K & L	19	7	21	5	18	8		
5	23	L & CAPITAL	12	21	15	18	25	8		
6	L	22 & 23	17	7	16	8	17	7		
7	22	K & L	8	23	12	19	19	12		
8	K	21 & 22	1	22	9	14	15	8		
9	22	J & K	14	26	19	21	23	17		
10	J	22 & 23	1	22	8	15	14	9		
11	23	I & J	25	9	28	6	28	6		
12	J	23 & 24	2	20	11	11	15	7		
13	24	J & K	10	19	15	14	19	10		
14	K	24 & 25	8	17	20	5	21	4		
15	24	K & L	14	14	20	8	22	6		
16	L	23 & 24	6	19	5	20	9	16		
SUBTOTAL			180	266	257	189	299	147	404	42

Kevin Fong Design
 2 Stillshore Court
 Sacramento, CA 95831
 (916) 428-5008

Exhibit 1H Applicant Parking Survey (cont.)

2301 K Street

Kevin Fong Design
 Parking Survey
 September 3, 2010

Sunday Breakdown			9:00AM	12:00PM	5:30PM	8:00PM				
Area	Street Intersection	Total Spaces	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE		
1	L	22 & 23	9	17	10	16	7	19		
2	23	J & K	21	11	26	6	14	18		
3	K	23 & 24	16	9	18	7	16	9		
4	23	K & L	12	14	15	11	11	12		
5	23	L & CAPITAL	10	23	26	7	8	25		
6	L	22 & 23	12	12	21	3	12	17		
7	22	K & L	21	10	24	7	20	11		
8	K	21 & 22	11	12	10	13	6	17		
9	22	J & K	12	28	14	26	18	22		
10	J	22 & 23	7	16	10	13	9	14		
11	23	I & J	15	19	14	20	24	10		
12	J	23 & 24	5	17	17	5	7	15		
13	24	J & K	11	18	14	15	9	20		
14	K	24 & 25	12	13	13	12	19	6		
15	24	K & L	11	17	10	18	17	11		
16	L	23 & 24	9	16	12	13	7	18		
SUBTOTAL			194	252	254	192	204	242	204	242

Kevin Fong Design
 2 Stillshore Court
 Sacramento, CA 95831
 (916) 428-5008



September 16, 2010

Robert Williams
City of Sacramento Community Development Department
300 Richards Blvd, 3rd Floor
Sacramento, CA 95811

Project: Z10-053 Cornerstone Restaurant at 2301 K Street
Subject: Proposed Special permit to waive parking

Dear Robert:

After hearing the give and take at the September 2 hearing on the proposed subject project, I continue to have major concerns. While the Cornerstone owners have attempted to address the issues raised in my previous letter, their response is inadequate and indeed raises new issues.

PARKING

Since the restaurant will be open from 6 am until 2 am, renting 7 or less parking spaces in the evening for employees will not help very much. Providing 10 valet spaces during lunch and dinner helps, but not only does it not solve the breakfast rush, but it won't be effective since it is unlikely for Cornerstone customers to use valet service (Ella maybe, Cornerstone no).

It is true that some customers have biked and walked to the restaurant in the past. However, the existing credit for the 10 parking spaces already compensates for this.

The reason that midtown and downtowns in general are successful is the balance of commercial and residential uses. Fortunately, the City of Sacramento has recognized that keeping and encouraging residential development in these areas will keep midtown and downtown viable and energized. Parking decisions need to favor the residents because theirs is a 24/7 commitment, while commercial is much less. The corner of 23 and K Streets currently is predominantly residential. Even without the waiver, the project will impact residential parking not only on this corner but in the surrounding blocks. With the waiver, the situation will be terrible.

ARCHITECTURE AND
PROJECT MANAGEMENT

■ 2320 J STREET, SACRAMENTO, CALIFORNIA 95816
TEL 916.446.6227 FAX 916.446.7877

Robert Williams

September 16, 2010

Page 2

TRASH

I was shocked to learn that the City of Sacramento will even contemplate allowing anyone to place a dumpster on the alley behind someone else's property. When I renovated my property on J Street, not only was I required to provide a dumpster on my property, but I was required at great expense to build an enclosure around it.

Because of Cornerstone's history with poor trash maintenance, they should be required to store their trash inside their building. This would eliminate any risk of trash spilling on the public sidewalk along 23 Street due to transport as well as in the alley.

SUMMARY

As a property owner on the block and newly concerned midtown resident, I recommend against the waiver of parking for the Cornerstone project. In addition I recommend that the design be modified to provide trash storage within the building.

Sincerely,



Franc Blackbird, AIA



August 27, 2010

Robert Williams
City of Sacramento Community Development Department
300 Richards Blvd, 3rd Floor
Sacramento, CA 95811

Project: Z10-053 Cornerstone Restaurant at 2301 K Street
Subject: Proposed Special permit to waive parking

Dear Robert:

I am a long standing customer and neighbor of Cornerstone Restaurant and own the property at 2320 J Street. In addition, I am an architect and am familiar with the City of Sacramento zoning and design review requirements. For that reason I would like to share my concerns about the proposed project at 2301 K Street.

PARKING

What is the compelling reason that this project should not conform to the City of Sacramento parking requirements? Since there was no justification given in the notice, I can only surmise that it is simply a cost saving measure for the building owners and tenants. The other restaurants in the neighborhood (Chicago Fire, Thai Basil, etc.) had to comply, why not Cornerstone? When I moved my architectural business to this location, I had to comply with the regulations. All of the projects that I have processed through the City have had to comply with all of the zoning requirements. Why not this one? It seems to me that all projects should be treated the same and if the rules are too strict, then the City should change the rules, not allow "special permits" on an arbitrary basis instead.

Even though the restaurant is only moving around the block, the parking impact to the neighborhood residents will be much greater than the current situation, simply because there are more residents located near the new corner. I agree with encouraging the mixture of residential and commercial functions in Midtown, but believe that we should be especially sensitive to the needs of the residents, since they spend more time here than those of us who just work here.

ARCHITECTURE AND
PROJECT MANAGEMENT

2320 J STREET, SACRAMENTO, CALIFORNIA 95816
TEL 916.446.6227 FAX 916.446.7877

Robert Williams

August 27, 2010

Page 2

TRASH

The proposed design has no accommodation for a trash enclosure , a dumpster for the restaurant or trash receptacles for the two apartments. Again, this is a zoning requirement that should be addressed. I would like to share my personal experience with Cornerstone's trash history. Over the years, the walkway between the current restaurant and the former Taco Loco has been an area of loose trash, eggshells, spilled liquids and foods to the point that it is too disgusting to walk through. I believe that the majority if not all of the debris is from the Cornerstone restaurant.

In the past, the two restaurants shared a 3 yard dumpster located next to my (zoning compliant) trash enclosure. The dumpster was emptied once per day. However, it was continuously overflowing, smelly, attracted rodents and bugs and was an eyesore to the alley. It was so often overfilled that when it was emptied, more trash would fall in the alley and get spread throughout the neighborhood. More recently Taco Loco closed and Cornerstone now has its own 3 yard dumpster which has moved closer to the restaurant. This has been a great improvement to the alley safety and appearance.

However, it is clear that there MUST be an enclosed space established for at least a 3 yard dumpster for the restaurant or the unattractive, unhealthy, unsafe, smelly, rodent attracting condition will now be on the public street sidewalk instead of just the alley.

FENCING

I would recommend that this project provide security fencing at the 2nd level exit stair and the rear walkway so that overnighters won't move in. The walkway in particular has been a chronic location for messy and unsanitary campers in the past years.

HOURS

The hours noted in the application say 6 am to 2 am. The late hour is much different than the current Cornerstone schedule and implies that not only will dinner be served but very likely alcohol. I believe that permitting a bar at this location should be processed in a much different way.

SUMMARY

The neighborhood will benefit from converting the vacant and deteriorating church building into a viable function. However, the City's zoning requirements regarding parking, trash enclosures and safety should be followed by the new tenant.

Sincerely,



Franc Blackbird, AIA

Subject: Appeal of Cornerstone Restaurant Parking Waiver (Z10-053) October 28, 2010

Exhibit 1I Letters (cont.)

City of Sacramento
Community Development Dept.
300 Richards Blvd.
Sacramento, CA 95811

September 2, 2010

Re: Project # Z10-053 (Testimony at 9/2/10 hearing)

To Whom It May Concern:

My wife and I have owned the four-unit apartment building at **2225 K Street** for 35 years. Our building is located one lot (40') away from the corner of 23rd and K Streets. We bought it in rundown condition and have spent decades upgrading it to a quality structure that has been home to doctors, interior designers, lobbyists, authors, radio personalities and, even a Pulitzer Prize winning newspaper reporter.

During our decades of ownership, our limited resources required us to provide virtually all of the thousands of hours of labor to upgrade and maintain the property. In recent years it has come to provide a significant income stream upon which I (a retired school district employee) and my family rely for retirement income.

In the early years, renting the units was effortless because of the location, the size and quality of the units, and the availability of on-street parking in the immediate area (our small 40'x80' corner lot has no alley access and no open area available for parking behind the structure). Over the years, however, providing even minimal parking has been an ever-increasing challenge.

In the past 15 years or so there has been a determined effort on the part of the city to restrict or eliminate access of tenants (ours and others) to parking within a safe and practical distance of their homes. **At present, we are on the verge of no longer being able to assure prospective tenants that parking will be available should they rent in our building.** At present, we are still able to rent units because tenants can still find a parking space or two on 23rd Street provided they apply for and obtain the required "K" permit. (The length of K Street in front of our building is now "off-limits" to tenants. It is signed "One-Hour Parking" with no permits in spite of the fact that five of the six structures located there are multiple-residential!)

Should the Cornerstone Restaurant move into the structure at the corner of 23rd and K Streets, it appears painfully obvious to me that the few remaining parking spaces currently available to my tenants would cease to exist making my units virtually unrentable and forcing my lifelong family business . . . out of business. The city has been quick to point out that there are no alternative uses for my existing building in spite of the area's commercial zoning. The loss of our retirement income would be devastating. In addition, our fully-renovated, fully-rented apartment building would quickly become a "deserted" (chronically and completely vacant) structure and, therefore, a very attractive target for undesirables. It was already nearly burned to the ground by a serial arsonist . . . while it was occupied! I should add that the four-unit residential structure between us and the corner of 23rd and K is in the identical situation. This move could potentially dispossess almost one resident for every one parking space being requested by the Cornerstone Restaurant.

I am keenly aware of the many efforts (in-fill, high density, condos, etc.) being undertaken to provide desperately needed housing for Sacramento's residents. We have worked tirelessly for over 30 years to help meet that need and to ensure that we would be able to "fund" our retirement without imposing on others. Please help us to continue to live our dream.

Sincerely,


Larry R. Crabbe



August 26th, 2010

City of Sacramento, Community Development Department
300 Richards Blvd., 3rd Floor
Sacramento, CA 95811-0

**Proposal for Cornerstone Café Relocation
#007-0095-021-0000**

Dear Mr. Williams:

It was a pleasure to hear that the relocation of the Cornerstone Café is progressing. I am a long-time nearby K Street commercial property owner and business owner in the neighborhood and have watched this section of K Street develop for over 20 years. I am hopeful that the new location can be an upgrade from the current location. I support the Cornerstone's new location but have a few very important issues that I would strongly request that the City require the owner's to conduct their business differently than in their current location. These are similar conditions that the City placed on the relocation of the former Truelove Coffee House to K Street. These concerns are the following:

1. Mandate professional regular cleaning of all visible sidewalks where outdoor table service is planned to remove grease and other stains on a regular basis.
2. Prohibit white/green plastic or molded resin (Walmart/Target) low-cost seating and tables and require upgraded seating of powder coated metal to convey a more historic and tailored streetscape image.
3. Require that any sidewalk fencing for outdoor seating be architecturally designed to accent, support and enhance the style and character of the old church's historic architecture.
4. Require that any exterior planters, pots, bike racks or containers for trash, plants or newspaper racks be organized, designed and of such a character to accent, support and enhance the style and character of the old church's historic architecture.
5. Require that the planter strip in front and adjacent to the building and streets be maintained, planted/irrigated and/or covered in a material such as decomposed gravel.
6. Require any window awnings, interior window blinds and/or other covers to be appropriate to accent, support and enhance the style and character of the old church's historic architecture.
7. Prohibit umbrellas that advertise food and beverage manufacturers and require color coordinated fabrics that accent, support and enhance the style and character of the old church's historic architecture.
8. Require a 12-bike rack to be installed as a basic trade for no parking supplied on-site. The design and placement of the rack should accent, support and enhance the style and character of the old church's historic architecture.

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MAIN OFFICE 2319 K STREET, 2ND FLOOR
SACRAMENTO, CA 95816
TEL 916.441.4500 FAX 916.438.1553
BAY AREA TEL 925.587.4200 / 408.371.4200 / 415.482.0300

E-MAIL design@orrdesign.com
ONLINE www.ORRdesign.com
www.ORRdreamhouse.com

Subject: Appeal of Cornerstone Restaurant Parking Waiver (Z10-053) October 28, 2010

Exhibit 1I Letters (cont.)

I feel that the above eight suggestions are a reasonable measure considering that they operators have no expenses related to on-site parking. These measures will support an enhanced image of the K Street 23rd, 24th block and corridor.

Please keep me informed. I will be unable to attend the meeting but would request that the Zoning Administrator address these comments at the meeting.

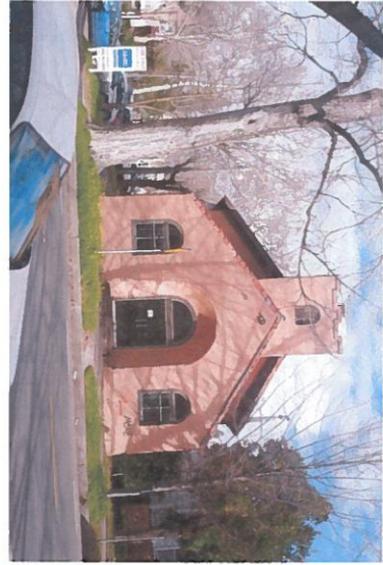
Thank You

A handwritten signature in black ink, appearing to read "Gary Orr". The signature is fluid and cursive, with a large initial "G" and "O".

Gary Orr, Principal and Director of Design

Owner- ORR Design Office
2319 K Street, Suite 200
Sacramento, CA 95816

Exhibit 1J Photos



FRONT(SOUTH)



FRONT(SOUTH) AND ADJACENT BUILDING



FRONT(SOUTH) AND ADJACENT BUILDING



RIGHT SIDE(EAST)



RIGHT SIDE (EAST)

CORNERSTONE RESTAURANT

2301 K-STREET
SACRAMENTO, CA. 95819



LEFT SIDE(WEST)



LEFT SIDE(WEST)AND REAR(NORTH)



LEFT SIDE(WEST) AND ADJACENT BUILDING

CORNERSTONE RESTAURANT
2301 K-STREET
SACRAMENTO, CA. 95819



BUILDING ON THE SOUTH



BUILDING ON THE WEST

CORNERSTONE RESTAURANT
2301 K-STREET
SACRAMENTO, CA. 95819

City of Sacramento
Community Development
Department
Current Planning
Division

Z10-053
& DR10-110

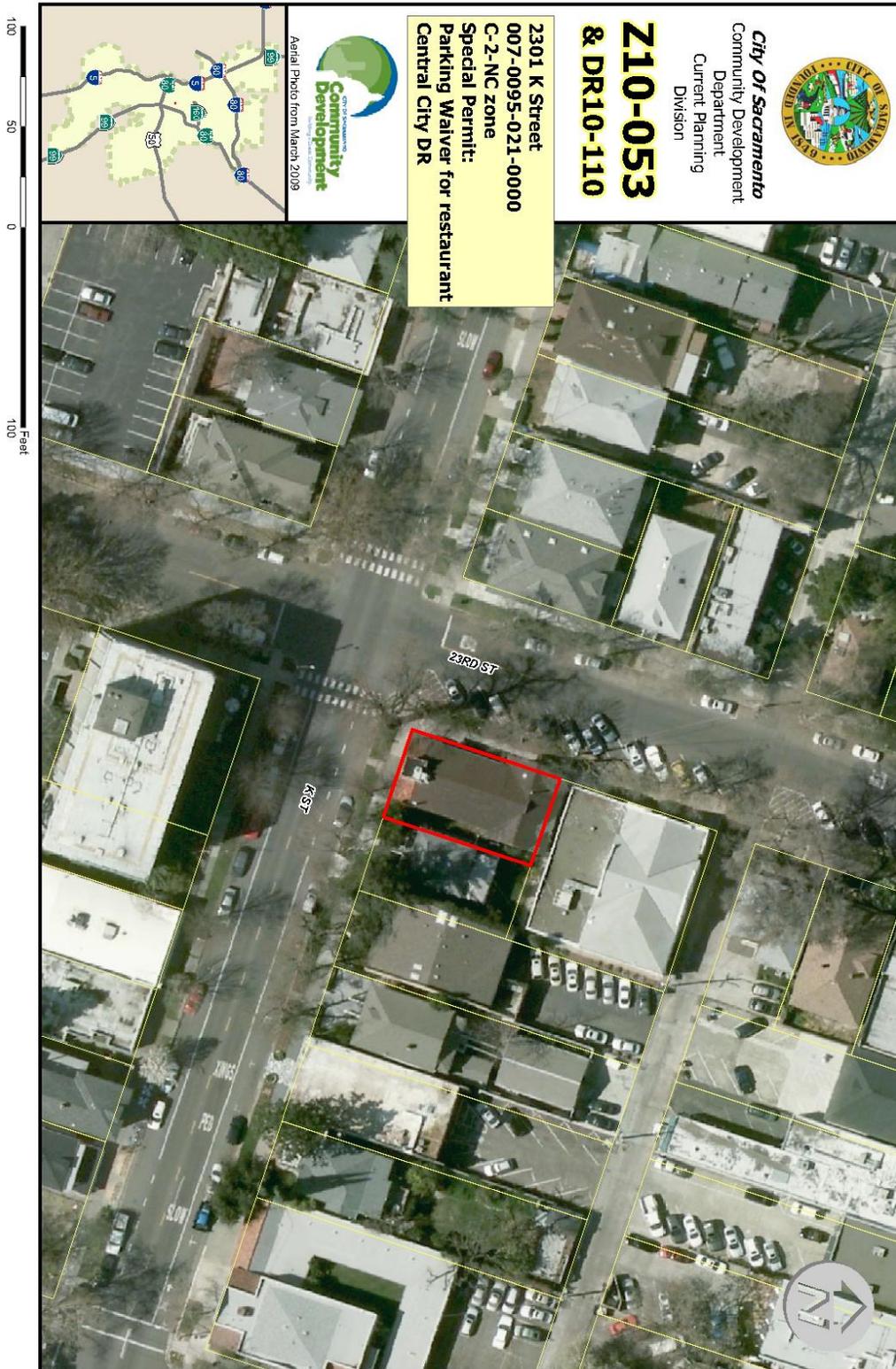
2301 K Street
007-0095-021-0000
C-2-NC zone
Special Permit:
Parking Waiver for restaurant
Central City DR

City of Sacramento
Community Development

Aerial Photo from March 2009

100 50 0 100 Feet

22ND ST
23RD ST
24TH ST
K ST



City of Sacramento
Community Development
Department
Current Planning
Division

Z10-053
& DR10-110

2301 K Street
007-0095-021-0000
C-2-NC zone
Special Permit:
Parking Waiver for restaurant
Central City DR

City of Sacramento
COMMUNITY DEVELOPMENT
DEPARTMENT
CURRENT PLANNING
DIVISION

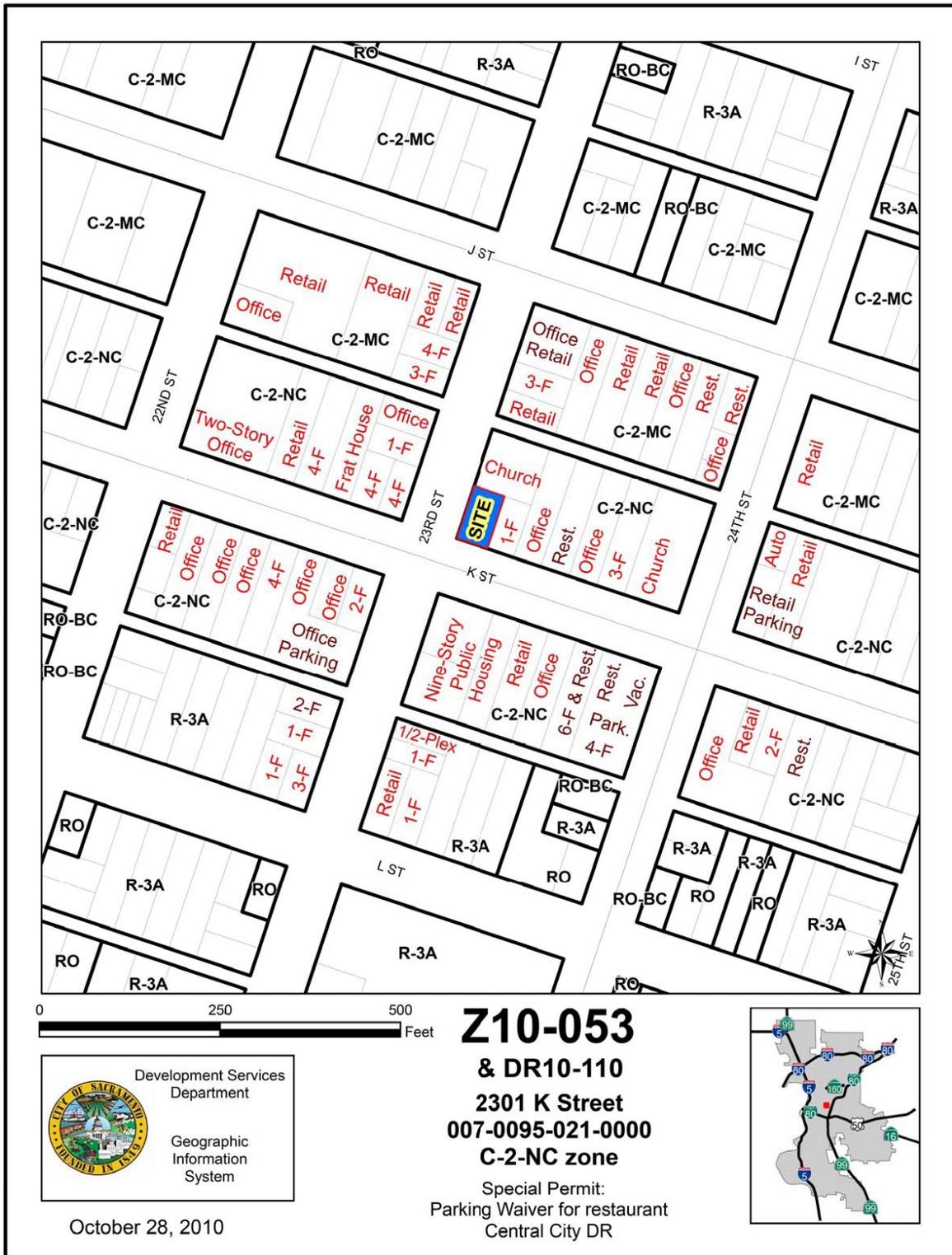
Community Development
City of Sacramento

Aerial Photo from March 2009

100
50
0
100 Feet

K ST
23RD ST

Attachment 3 Land Use & Zoning Map



0 250 500 Feet



October 28, 2010

Z10-053
& DR10-110
2301 K Street
007-0095-021-0000
C-2-NC zone

Special Permit:
 Parking Waiver for restaurant
 Central City DR





REPORT TO PLANNING COMMISSION City of Sacramento

5

915 I Street, Sacramento, CA 95814-2671

PUBLIC HEARING
October 28, 2010

To: Members of the Planning Commission

Subject: Township 9 Modification (P10-036)

A request to modify the previously approved Township 9 project by adjusting the Vine Street right of way and changing prior Tentative Map conditions on 47.71± acres in the Office Building (OB-PUD SPD), Residential Mixed Use (RMX-PUD SPD), and Agricultural Open Space (A-OS-PUD SPD) zone and located in the Township 9 Planned Unit Development and Richards Boulevard Special Planning District.

- A.** Environmental Determination: Exempt under CEQA Guidelines Section 15305;
- B.** PUD Design Guidelines Amendment to modify requirements regarding the location and shape of park paseos;
- C.** Subdivision Modification to change the prior Tentative Map conditions to allow recreational easements in lieu of fee simple ownership;
- D.** Subdivision Modification to adjust the right of way alignment for Vine Street.

Location/Council District:

424 North 7th Street, Sacramento, CA 95811

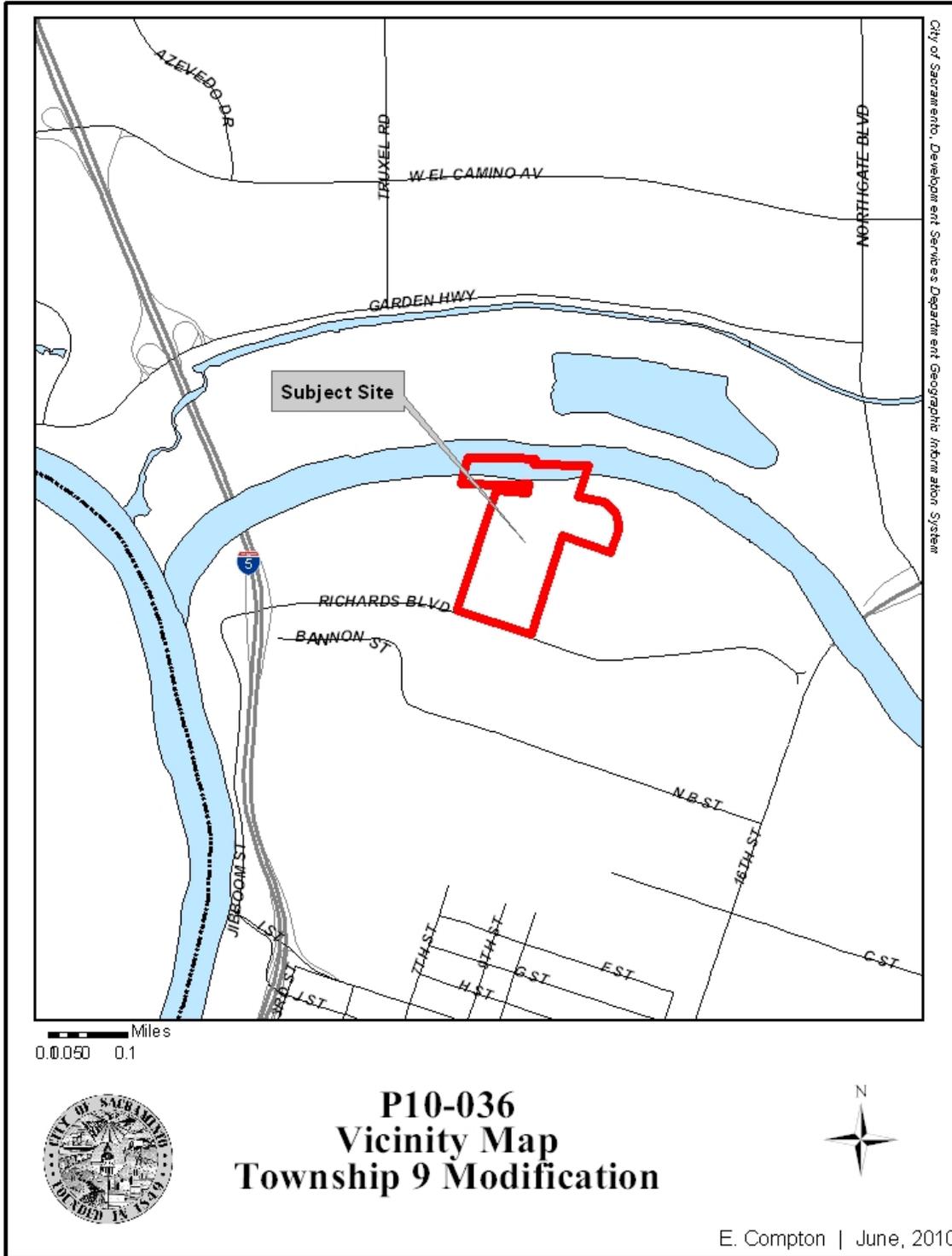
Assessor's Parcel Numbers: 001-0020-003, -019, -034, -036, -041, -044, -045, -046, and 001-0200-012, -013, -034

Council District 1

Recommendation: Staff recommends the Planning Commission approve the request based on the findings and subject to the conditions listed in Attachment 1. The Commission has final approval authority over items A-D above, and its decision is appealable to City Council.

Contact: Evan Compton, Associate Planner, 916-808-5260 and Stacia Cosgrove, Senior Planner, 916-808-7110.

Applicant: Al Esquivel, Capitol Station 65 LLC, 640 Bercut Drive, Suite C, Sacramento, CA 95811 (916-482-7900)



Subject: Township 9 Modification (P10-036)

Owner: Steve Goodwin, Capitol Station 65 LLC, 640 Bercut Drive, Suite C, Sacramento, CA 95811 (916-482-7900)

Summary: The applicant is requesting entitlements to modify the previously approved Township 9 project by adjusting the Vine Street right of way and modifying conditions relating to the park paseos on 47.71± acres in the Office Building (OB-PUD SPD), Residential Mixed Use (RMX-PUD SPD), and Agricultural Open Space (A-OS-PUD SPD) zone and located in the Township 9 Planned Unit Development and Richards Boulevard Special Planning District.

At the time of writing the report, there were no outstanding issues or concerns regarding the project. **The project is considered to be non-controversial.**

Table 1: Project Information
General Plan designation: Urban Center Low
Existing zoning of site: OB-PUD-SPD, RMX-PUD-SPD, and A-OS-PUD SPD
Existing use of site: Buildings Currently Under Demolition
Property area: 47.71 ± net acres

Background Information: The Township 9 project (P06-047) was approved by the City Council on August 28, 2007. (Resolution 2007-644) The project allowed for a high density mixed use development of approximately 2,350 residential units, 840,000 square feet of office, and 146,000 square feet of retail uses on 65± gross acres.

Entitlement History: On February 24, 2010, the Planning Director approved a Plan Review for the Township 9 Light Rail Station and Scale House relocation projects (P09-054). On February 24, 2010, the Preservation Director approved a request to move and remodel a historic structure (Scale House) in preparation for the new light rail station (PB09-081).

Public/Neighborhood Outreach and Comments: Staff notified the River District Association. In addition, staff notified property owners within 500 feet of the subject property. At the time of writing this report, staff had not received any comments.

Environmental Considerations: The Community Development Department, Environmental Planning Services Division has reviewed this project and determined that this is exempt from the provisions of the California Environmental Quality Act (CEQA) under CEQA Guidelines Section 15305, Minor Alterations in Land Use Limitations. The project consists of minor alterations in land use limitations in areas with an average slope of less than 20%, which do not result in any changes in land use or density.

Township 9 Planned Unit Development (PUD) Design Guidelines

The project site is located in the Township 9 Planned Unit Development. The designated PUD includes a Schematic Plan and a set of development guidelines. Staff finds that the proposed modifications do require amendments to the PUD Design Guidelines because Section 4.8 (Open Space/Parks) states that “[t]hree linear, landscaped greenways shall be developed to transverse the live/work/townhouse area in Township 9.” The guidelines dictated the linear shape and location of the greenways and therefore an amendment is necessary. A redlined copy of all the text amendments has been included in this report as Attachment 4.

The Planning Commission may approve the amendment of PUD Design Guidelines provided that the proposed amendments do not change the intensity of land uses by more than ten percent. Staff finds that the text amendments are minor in nature and do not intensify the land uses and therefore the entitlement may be handled at the Planning Commission level.

Parks and Open Space

The Tentative Map contemplated a series of small parks located throughout the project. Condition 75 of the approved Tentative Map requires title to Lots 7B, 8B, 9, 11B, 12B, 15B, 20, and 21 be provided in fee simple ownership. The applicant is requesting to modify the condition to allow the parcels to be dedicated as recreational easements. This change would allow the applicant to have greater flexibility in planning future buildings which include underground parking. The proposal for underground parking is problematic with the existing Parks condition because the park dedication must be completed with fee simple ownership. Therefore staff supports the modification which would allow a recreational easement to facilitate underground parking and ultimately encourages active ground floor uses facing the street.

Traffic Circulation

The irrevocable offer of dedication (IOD) for the Vine Street right of way is proposed to be relocated 35 feet to the north of the previously approved alignment. The original street alignment required 35 feet along the CHP Headquarters site and another 35 feet along the Township 9 site for a total of 70 feet. The modification removes the 35 foot requirement on the CHP site and realigns the 70 feet to be provided on the Township 9 site. Traffic Engineering staff have reviewed the realignment request and have no objections to the request.

Tentative Map

The Subdivision Review Committee (SRC) heard this item on October 6, 2010 and recommended that the Planning Commission approve the request based on the conditions of approval found in Attachment 1. These conditions will replace the previous conditions of approval from the original project (P06-047).

Adjacent Properties

To the north of the subject site is the American River. The parcels to the east are currently used for the California Highway Patrol Headquarters. On the west of the subject site are FedEx, the State Department of Communications, and the Sheriff Work Release Facility. The State Printing Plant and other industrial buildings are located to the south.

Policy Considerations:

General Plan: The subject site is designated Urban Center Low on the General Plan Land Use and Urban Form Diagram. The proposal is consistent with the following General Plan policies:

1. *Screening of Off-street Parking. The City shall reduce the visual prominence of parking within the public realm by requiring most off-street parking to be located behind or within structures or otherwise fully or partially screened from public view. (LU 2.7.8)* Staff finds that the proposal for underground parking is problematic with the existing Parks condition which requires the park dedication be completed with fee simple ownership. Therefore staff supports the modification which would allow a recreational easement to facilitate underground parking and ultimately encourages active ground floor uses facing the street.
2. *Small Public Spaces for New Development. The City shall allow new development to provide small plazas, pocket parks, civic spaces, and other gathering places that are available to the public, particularly in infill areas, to help meet recreational demands. (ERC 2.2.9)* Staff finds that the blocks in Township 9 will have recreational easements to provide necessary open space amenities.
3. *Adequate Rights-of-Way. The City shall ensure that all new roadway projects and major reconstruction projects provide appropriate and adequate rights-of-way for all users including bicyclists, pedestrians, transit riders, and motorists except where pedestrians and bicyclists are prohibited by law from using a given facility. (M 4.2.1)* Staff finds that although the alignment of Vine Street is being modified, it will maintain the same width and therefore staff supports the requested realignment.

Urban Center Low Designation (Page 2-72): This designation provides for smaller urban areas throughout the city. Each center includes employment-intensive uses, a mix of housing, and a wide variety of retail uses.

Key components of the urban form in the Urban Center Low designation include:

- Blocks are small and rectangular, allowing for convenient pedestrian access from adjacent areas
- Relatively small and narrow lots, providing a fine-grained development pattern

Subject: Township 9 Modification (P10-036)

- Building heights generally ranging from two to seven stories (taller heights are acceptable if supported by context and market)
- Building heights stepping down to not more than one story higher at the property line than permitted in the adjacent neighborhood unless separated by a roadway, rail corridor, or other setback or buffer
- Lot coverage generally not exceeding 80 percent
- Buildings sited at or near the sidewalk along the primary street frontage and typically abut one another with limited side yard setbacks
- Building entrances set at the sidewalk along the primary street frontage
- Rear alleys and secondary streets providing vehicular and service access, with limited driveways and curb cuts on primary streets
- Parking provided on-street as well as in individual or shared lots at the rear of structures, or in screened parking structures
- Transparent building frontages with pedestrian-scaled articulation and detailing
- Moderately wide sidewalks (e.g., 6 to 10 feet), furnished with street trees, public seating areas, and other amenities that create inviting streetscapes
- Public streetscapes serving as the area's primary open space, complemented by plazas, courtyards, and sidewalk dining areas

Staff finds that the proposal is consistent with the intent of the components of the urban form listed above.

Subdivision Code

The Planning Commission may approve a subdivision modification to modify the conditions of approval of an approved Tentative Map. Approval of the modification to the conditions of approval must be based on the same findings of fact needed for approval of the subdivision map:

1. None of the conditions described in Government Code Section 66474, subsection (a) through (e), inclusive, exist with respect to the proposed subdivision as follows:
 - a. The proposed subdivision, together with the provisions for its design and improvement, is consistent with the City's 2030 General Plan and Title 16 of the City Code, which is a specific plan of the City;
 - b. The site is physically suitable for the type of development proposed and suited for the proposed density;

Subject: Township 9 Modification (P10-036)

- c. The design of the subdivision and the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife habitat since the site is located in an urban area and historically was developed with an industrial use;
 - d. The design of the subdivision and the type of improvements are not likely to cause serious public health problems since the proposed residential, retail, and office uses will not generate smoke or toxics;
 - e. The design of the subdivision and the type of improvements will not conflict with easements, acquired by the public at large, for access through or use, of, property within the proposed subdivision.
2. The proposed subdivision, together with the provisions for its design and improvement, is consistent with the City General Plan and Title 16 Subdivisions of the City Code, which is a specific plan of the City (Gov. Code §66473.5);
 3. The discharge of waste from the proposed subdivision into the existing community sewer system will not result in a violation of the applicable waste discharge requirements prescribed by the California Regional Water Quality Board, Central Valley Region, in that existing treatment plants have a design capacity adequate to service the proposed subdivision (Gov. code §66474.6);
 4. The design of the proposed subdivision provides, to the extent feasible, for future passive or natural heating and cooling opportunities (Gov. Code §66473.1);
 5. The Planning Commission has considered the effect of the approval of this tentative subdivision map on the housing needs of the region and has balanced these needs against the public service needs of its residents and available fiscal and environmental resources (Gov. Code §66412.3).

Alternative Modes

The first phase of the future Downtown-Natomas-Airport light rail transit is under construction along North 7th Street and then will turn west on Richards Boulevard and terminate at the Township 9 site. The subsequent phase will continue the light rail down Richards Boulevard to turn north on Sequoia Pacific Boulevard and cross the American River to connect to Truxel Boulevard and eventually will connect to the Sacramento International Airport. The future light rail station at the Township 9 project (north side of Richards Boulevard between North 5th and North 7th Street) is scheduled to be open in 2011. According to the Regional Transit website, there are also three bus routes in the Richards Boulevard Area: Route 11, 15, and 33.

Conclusion:

Staff recommends approval of the project since the proposal: a) is consistent with the 2030 General Plan, Central City Community Plan, and the Office Building (OB),

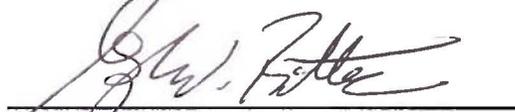
Residential Mixed Use (RMX), and Agricultural Open Space (A-OS) zoning; b) facilitates underground parking by allowing recreational easements in lieu of fee simple title; and c) continues to provide adequate right-of-way with the proposed realignment of the future Vine Street.

Respectfully submitted by: 
EVAN COMPTON
Associate Planner

Approved by:


STACIA COSGROVE
Senior Planner

Recommendation Approved:


GREG BITTER, AICP
Principal Planner

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**Attachment 1
Proposed Findings of Fact and Conditions of Approval
Township 9 Modification (P10-036)
424 North 7th Street**

1. Findings of Fact

A. Environmental Determination: Categorical Exemption per CEQA Guidelines 15305

Based on the determination and recommendation of the City's Environmental Planning Services Manager and the oral and documentary evidence received at the hearing on the Project, the Planning Commission finds that the Project is exempt pursuant to CEQA Guidelines Section 15305 (Minor Alterations in Land Use Limitations) of the California Environmental Quality Act Guidelines as follows:

The project consists of minor alterations in land use limitations in areas with an average slope of less than 20%, which do not result in any changes in land use or density.

B. PUD Design Guidelines Amendment: The amendment to the Township 9 PUD Design Guidelines to modify requirements regarding the location and shape of park paseos is **approved** based on the following Findings of Fact:

1. The PUD Design Guidelines amendment conforms to the General Plan and the Central City Community Plan;
2. The PUD Design Guidelines amendment facilitates the construction of subterranean parking by removing regulations dictating the linear shape and location of paseos, which allows screened parking and future opportunities for active ground floor uses; and
3. The PUD Amendments will not be injurious to the public welfare, nor to other property in the vicinity of the development and will be in harmony with the general purposes and intent of the Zoning Ordinance in that the PUD ensures the development will be well-designed.

C. and D. Subdivision Modifications: The Subdivision Modifications to allow the realignment of Vine Street along the Township 9 project area and revisions of approved Tentative Map conditions are **approved** based on the following Findings of Fact:

1. None of the conditions described in Government Code Section 66474, subsection (a) through (e), inclusive, exist with respect to the proposed subdivision as follows:

Subject: Township 9 Modification (P10-036)

- a. The proposed subdivision, together with the provisions for its design and improvement, is consistent with the City’s 2030 General Plan and Title 16 of the City Code, which is a specific plan of the City;
- b. The site is physically suitable for the type of development proposed and suited for the proposed density;
- c. The design of the subdivision and the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife habitat since the site is located in an urban area and historically was developed with an industrial use;
- d. The design of the subdivision and the type of improvements are not likely to cause serious public health problems since the proposed residential, retail, and office uses will not generate smoke or toxics;
- e. The design of the subdivision and the type of improvements will not conflict with easements, acquired by the public at large, for access through or use, of, property within the proposed subdivision.

2. The proposed subdivision, together with the provisions for its design and improvement, is consistent with the City General Plan and Title 16 Subdivisions of the City Code, which is a specific plan of the City (Gov. Code §66473.5);

3. The discharge of waste from the proposed subdivision into the existing community sewer system will not result in a violation of the applicable waste discharge requirements prescribed by the California Regional Water Quality Board, Central Valley Region, in that existing treatment plants have a design capacity adequate to service the proposed subdivision (Gov. code §66474.6);

4. The design of the proposed subdivision provides, to the extent feasible, for future passive or natural heating and cooling opportunities (Gov. Code §66473.1);

5. The Planning Commission has considered the effect of the approval of this tentative subdivision map on the housing needs of the region and has balanced these needs against the public service needs of its residents and available fiscal and environmental resources (Gov. Code §66412.3).

2. Conditions of Approval

C. and D. Subdivision Modifications: The Subdivision Modifications to allow the realignment of Vine Street along the Township 9 project area and to revise the Parks conditions on the Tentative Map are **approved** subject to the following conditions of approval:

CONDITIONS: Tentative Map

NOTE: These conditions shall supersede any contradictory information shown on the Tentative Map or any contradictory provisions in the PUD Design guidelines approved for this project (P06-047). The design of any improvement not covered by these conditions or the Design Guidelines shall be to City standard.

The applicant shall satisfy each of the following conditions prior to filing the Final Map unless a different time for compliance is specifically stated in these conditions. Any condition requiring an improvement that has already been designed and secured under a City Approved improvement agreement may be considered satisfied at the discretion of the Department of Transportation.

The City strongly encourages the applicant to thoroughly discuss the conditions of approval for the project with their Engineer/Land Surveyor consultants prior to City Planning Commission approval. The improvements required of a Tentative Map can be costly and are completely dependent upon the condition of the existing improvements. Careful evaluation of the potential cost of the improvements required by the City will enable the applicant to ask questions of the City prior to project approval and will result in a smoother plan check process after project approval:

GENERAL: All Projects

1. Pay off existing assessments, or file the necessary segregation requests and fees to segregate existing assessments;
2. Comply with requirements included in the Mitigation Monitoring Plan developed by, and kept on file in, the Planning Division Office (P06-047);
3. Meet all conditions of the Design Guidelines established for this project "Township 9" (P06-047) unless it is superseded by a Tentative Map condition;
4. Show all continuing and proposed/required easements on the Final Map;
5. If unusual amounts of bone, stone, or artifacts are uncovered, work within 50 meters of the area will cease immediately and a qualified archaeologist shall be consulted to develop, if necessary, further mitigation measures to reduce any archaeological impact to a less than significant effect before construction resumes. A note shall be placed on the final improvement plans referencing this condition;
6. Multiple Final Maps may be recorded. Prior to recordation of any Final Map all infrastructure/improvements necessary for the respective Final Map must be in place to the satisfaction of the Departments of Utilities, the Department of Transportation and Development Services.

7. Riverfront Drive shall be set back a minimum of 50 feet from the Two Rivers Bicycle Trail. This setback shall be measured from the southern edge of pavement of the Two Rivers Bicycle Trail to the back of curb along the northern boundary of Riverfront Drive.

Engineering: Streets General

8. Submit a Geotechnical Analysis prepared by a registered engineer to be used in street design. The analysis shall identify and recommend solutions for groundwater related problems, which may occur within both the subdivision lots and public right-of-way. Construct appropriate facilities to alleviate those problems. As a result of the analysis street sections shall be designed to provide for stabilized subgrades and pavement sections under high groundwater conditions. (See also condition 41 under Porous Concrete);
9. Construct standard subdivision improvements as noted in these conditions pursuant to section 16.48.110 of the City Code. All improvements shall be designed and constructed to the design guidelines and to the satisfaction of the Department of Transportation. Improvements required shall be determined by the city. The City shall determine improvements required for each phase prior to recordation of each phase. Any public improvement not specifically noted in these conditions or on the Tentative Map shall be designed and constructed to City standards. This shall include the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk per City standards, the adopted design guidelines and to the satisfaction of the Department of Transportation;
10. All right-of-way and street improvement transitions that result from changing the right-of-way of any street shall be located, designed and constructed to the satisfaction of the Department of Transportation. The center lines of such streets shall be aligned.
11. Construct bulb-outs at locations specified within the design guidelines or as directed by the City.
12. All crosswalks shall be disability access compliant; ramps shall be installed and/or relocated as determined necessary by the City.
13. The design and placement of walls, fences, signs and Landscaping near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance

14. Street lights shall be provided as prescribed by the design guidelines and must be acceptable to the City.

Streets

15. Prior to first map being finalized and with the initial improvement plan set, the applicant shall provide an overall layout and phasing plan for Richards Blvd from 300 feet west of 5th Street to 300 feet east of 7th street. At a minimum this plan set shall include the proposed future location of curb, gutter, and sidewalk improvements, street light locations and striping plans, existing right-of-way and any future right-of-way needs. This plan set shall include the intersections of 5th and 7th along Richards and 300 feet of each intersection leg or as determined acceptable by the City.
16. Dedicate and construct the following streets as development occurs within the map boundary, for each phase of development the adjacent streets shall be improved to the right-of-way and easement standards established by the Design Guidelines and shown on this map, applicable City Standards, and to the satisfaction of Development Services engineers authorized to accept plans on behalf of the City;
17. Dedicate and construct the north side of Richards Blvd per the right-of-way width and easement standards set forth in the design guidelines, depicted on the tentative map, the city standards for street construction, and to the satisfaction of Department of Transportation;
18. Dedicate and construct Riverfront Drive per the right-of-way width standards set forth in the design guidelines, depicted on the tentative map, the city standards for street construction and to the satisfaction of Department of Transportation;
19. Dedicate and construct the east side of 5th Street per the right-of-way width standards set forth in the design guidelines, depicted on the tentative map, the city standards for street construction and to the satisfaction of Department of Transportation;
20. Dedicate and construct 7th Street per the right-of-way width and easement standards set forth in the design guidelines, depicted on the tentative map, the city standards for street construction and to the satisfaction of Department of

Transportation, the median shall have a mountable curb and all weather surface to meet fire access requirements or provide other modification acceptable to the Fire Department (see condition 99);

21. Dedicate and construct Park Boulevard per the right-of-way width and easement standards set forth in the design guidelines, depicted on the tentative map, the city standards for street construction and to the satisfaction of Department of Transportation, the median shall have a mountable curb and all weather surface to meet fire access requirements or provide other modification acceptable to the Fire Department (see condition 99);
22. Dedicate and construct Signature Street per the right-of-way width and easement standards set forth in the design guidelines, depicted on the tentative map, the city standards for street construction and to the satisfaction of Department of Transportation;
23. Dedicate and construct New Street "A" per the right-of-way width and easement standards set forth in the design guidelines, depicted on the tentative map, the city standards for street construction and to the satisfaction of Department of Transportation; New Street "A" shall be extended east of 7th Street with the development of Lot 1B to provide vehicle access. Note: The subdivision modification resulted in a change in the alignment of New Street "A" East of 7th street (Extension of Vine Street) as shown on the revised Tentative Map submitted on 8-20-2010. The new Street "A" is located entirely within the township 9 project boundary.
24. Dedicate and construct New Street "B" per the right-of-way width and easement standards set forth in the design guidelines, depicted on the tentative map, the city standards for street construction and to the satisfaction of Department of Transportation;
25. Dedicate and construct New Street "C" per the right-of-way width and easement standards set forth in the design guidelines, depicted on the tentative map, the city standards for street construction and to the satisfaction of Department of Transportation;
26. Dedicate and construct New Street "D" per the right-of-way width and easement standards set forth in the design guidelines, depicted on the tentative map, the city standards for street construction and to the satisfaction of Department of Transportation;

27. Dedicate and construct New Street "E" per the right-of-way width and easement standards set forth in the design guidelines, depicted on the tentative map, the city standards for street construction and to the satisfaction of Department of Transportation;
28. Dedicate and construct New Street "F" per the right-of-way width and easement standards set forth in the design guidelines, depicted on the tentative map, the city standards for street construction and to the satisfaction of Department of Transportation;
29. Dedicate and construct New Street "G" per the right-of-way width and easement standards set forth in the design guidelines, depicted on the tentative map, the city standards for street construction and to the satisfaction of Department of Transportation.

Signalized Intersections

30. With the improvement plan sets for signalized intersections, the applicant shall submit a signal design concept report (SCDR) per section 15.18 of the Cities Design and Procedures Manual to the Department of Transportation for review and approval prior to the submittal of any improvement plans involving traffic signal work. The SCDR provides crucial geometric information for signal design and should be started as early as possible to avoid delays during the plan check process. Signal Design concept reports shall be submitted for the construction and/or modification of the existing traffic signals located at the following intersections:
 - a. The modification of the signalized intersection of Richards Blvd and North 5th Street.
 - b. The modification of the signalized intersection of Richards Blvd and North 7th Street
 - c. The construction of a signal at the intersection of North 7th Street and "Signature" Street.
31. Prior to the construction of 1/3rd of the project trip generation or 1000 of the dwelling units are constructed and as when determined necessary by the city of Sacramento, the intersection of Richards Blvd and North 5th Street shall be reconstructed to allow for eastbound dual left turn lanes and/or other modifications. The applicant shall use best efforts to obtain an easement from the adjacent property owner(s) for necessary rights-of-way along Richards Blvd if not already dedicated. To the extent necessary and at its discretion, the City

32. The applicant shall dedicate sufficient rights-of-way for an expanded intersection to the City of Sacramento Street Standards and shall construct modifications to 7th Street for the southbound approach at Richards Boulevard. These modifications to the southbound approach shall be to provide one left-turn lane, one through lane, and two right-turn lanes or as determined acceptable by the city of Sacramento. As necessary during the design of the intersection, the applicant shall use best efforts to obtain an easement from the adjacent property owner(s) for necessary rights-of-way along Richards Blvd and 7th street if not already dedicated for appurtenances and necessary alignment of the intersection. To the extent necessary and at its discretion, the City may use its eminent domain authority as provided by Government Code Section 66462.5 to acquire the easement at the applicant's expense.
33. The applicant shall construct a signal at the intersection of North 7th Street and "Signature" Street to the satisfaction of the city of Sacramento.

Roundabout Intersections

34. Dedicate and construct the roundabout at "Signature" Street and New Street "C" per the recommendations provided by the W-trans analysis for this intersection or other acceptable design, this analysis is on file with the City of Sacramento Department of Transportation Traffic Study section. Roundabouts shall be designed and constructed to the satisfaction of the city of Sacramento.
35. Dedicate and construct the roundabout at the intersection of 7th Street and New Street "A" per the recommendations provided by the W-trans analysis or other acceptable design, this analysis is on file with the City of Sacramento Department of Transportation Traffic Study section. Roundabouts shall be designed and constructed to the satisfaction of the city of Sacramento. Note: The subdivision modification resulted in a change in the alignment of New Street "A" East of 7th street (Extension of Vine Street) as shown on the revised Tentative Map submitted on 8-20-2010. The applicant shall provide sufficient right-of-way (as needed) to accommodate the roundabout design to the satisfaction of the Department of Transportation.
36. Dedicate and construct the roundabout at the intersection of 'Park' Blvd, New Street 'B' and New Street "C" per the recommendations provided by the W-trans analysis for this intersection or other acceptable design, this analysis is on file with the City of Sacramento Department of Transportation Traffic Study section.

Roundabouts shall be designed and constructed to the satisfaction of the city of Sacramento.

37. Prior to construction, the roundabout designs shall return to the Disability Advisory Commission for final review and comment. The commission is chaired by Ron Brown and the meeting coordinator is Obi Agha (916-808-8426) and Rocky Burks (916-808-5521).

Regional Transit

38. The applicant shall make provisions for bus stops, shelters, transit centers, etc. to the satisfaction of Regional Transit.
39. The applicant shall dedicate (if necessary) and construct bus turn-outs for all bus stops adjacent to the subject site to the satisfaction of the Department of Transportation. Bus stop turnouts shall be constructed with reinforced concrete pads.
40. Applicant shall enter into agreement with Regional Transit to provide right-of-way for light rail tracks, a light rail station platform and a bus turnout in the form of an Irrevocable Offer of Dedication (IOD). The 60-foot easement over the south edge of lots 13, 14 and 17 mentioned on Page 2-14 of the DEIR will be sufficient for the tracks, platform and bus turnout if the train route continues along Richards Boulevard.
41. The developer shall coordinate with RT regarding the placement of the outlet of New Street "C" onto Richards Boulevard, which crosses the future rail tracks.

Porous Concrete – Street Maintenance (See also condition 67 City Utilities)

42. The city of Sacramento is not convinced that porous concrete within the street will function adequately or be maintainable in the long term. Some additional information will need to be provided to show that it is feasible as a storm water treatment measure including a recommendation from a Geotechnical Engineer that the street section will continue to function under long term use by passenger and truck traffic. City staff will need a demonstration of its maintainability and its ability to treat storm water run-off. In addition, because of the potential for failure of the pavement section there must be a funding source to reconstruct and/or replace the pavement if it fails. This may include a contingency in the development agreement, the Community Financing District and bonding of the improvements for 3years to demonstrate that a porous pavement in a street functions and is maintainable.

- a. A Geotechnical Soils report must verify that storm water will drain 10 feet below the surface at the rate required for water quality. The water table in all of the proposed drainage areas must be identified before approval for this method. Seasonal groundwater levels for the area must be reported.
- b. A Geotechnical Engineer must submit a pavement design to provide an adequate structural section.
- c. Surface drainage will need to be accommodated for safety considering the underground storage capacity of the porous surface areas is unknown.
- d. Cutoff walls may be needed to keep water separate from the structural section supporting the vehicle travel ways.

CITY UTILITIES

43. Provide standard subdivision improvements per Section 16.48.110 of the City Code. Construct water, sewer, and drainage facilities to the satisfaction of the Department of Utilities (DOU). Off-site main extensions may be required.
44. All existing easements and all existing right-of-ways shall be shown on the Final Map.
45. If required by the DOU, any parcels that are subject to additional water quality treatment measures shall be identified on the Final Map.
46. Dedicate all necessary easements, IOD easements right-of-way, fee title property, or IOD in fee title property on the final map as required to implement the approved drainage, water and sewer studies, per each approving agency requirements.
47. If required by the Department of Utilities (DOU), the applicant shall enter into and record an Agreement for Conveyance of Easements with the City, in a form acceptable to the City Attorney, requiring that private easements be granted, as needed, for drainage, water and sanitary sewer at no cost at the time of sale or other conveyance of any lot. A note stating the following shall be placed on the Final Map: "The lots created by this map shall be developed in accordance with recorded agreement for conveyance of easements in Book____, O.R. Page____."
48. Design and construct water, sewer, and drainage pipe systems and appurtenances in all existing and proposed streets in accordance with the approved sewer, water, and drainage studies.

49. Provide all weather roadway and concrete driveway access to sump 111 to the satisfaction of DOU.
50. Unless otherwise approved by the DOU, all public water, sanitary sewer and storm drain pipelines shall be placed within the asphalt concrete (AC) section of public-right-of-ways and easements.
51. Public streets, with publicly maintained water, sanitary sewer and storm drain systems shall have a minimum paved AC width of 25 feet from lip of gutter to lip of gutter. Drain inlets, curbs and gutters shall be constructed to City Standards for all public streets. Any parallel underground dry utilities proposed within in the public right-of-way (or within the required 25-foot AC public section) shall be designed, constructed, and placed to the satisfaction of the City.
52. Along all streets with separated curb and sidewalk, place minimum 2-inch diameter sleeves under the sidewalk for irrigation of the landscape planter. The irrigation sleeves shall be placed prior to construction of sidewalks. In situations where separated sidewalks are located along the perimeter of parks or other common lots/parcels, irrigation sleeves shall be placed at maximum 200-foot intervals under the sidewalks.
53. Any decorative paving which is removed by the City while repairing, maintaining and/or replacing surface and subsurface water, drainage and sanitary sewer facilities will be repaved with asphalt concrete (AC). The owner(s) shall form or annex to a community facility district to finance the replacement of said decorative paving at no cost to the City.
54. The developer(s)/owner(s) shall complete a drainage master plan for this site. The 10-year and 100-year HGL's for this study shall be calculated using the City's SWMM model. This project may require a storm water detention basin, based on the approved SWMM model and/or increasing the pump capacity of basin sump 111, which serves this project. The drainage study shall also include an overland flow release map for the proposed project. Sufficient off-site and on-site spot elevations shall be provided in the drainage study to determine the direction of storm drain runoff.
55. The DOU shall approve the drainage master plan and any phasing plan, if appropriate, included with the master plan for drainage infrastructure.
56. The developer(s)/owner(s) shall be responsible to obtain all the necessary permits from the Army Corps of Engineering, Fish and Game, U.S. Fish and

Wildlife Services, SAFCA, American River Flood Control District, and/or any other applicable agency.

57. The applicant/owner shall also coordinate with SAFCA to determine any potential flooding problems or related issues for this area that may have a significant impact to the design of this project.
58. If the approved drainage study requires a project detention facility, the applicant shall construct the detention facility to the satisfaction of the DOU. The detention facility shall be designed for dual purpose (flood control and water quality) and utilized as joint use facility (public amenities) whenever possible.
59. Provide a seepage study prepared by a registered engineer. The study shall identify and recommend solutions to groundwater related problems that may occur within both the subdivision lots and the public streets. Appropriate facilities shall be constructed to alleviate those problems. The DOU and SAFCA shall approve this study.
60. If the approved drainage study and/or comprehensive storm-water quality plan requires the construction of a detention basin facility for the purpose of flood control, storm-water quality treatment, and/or recreational facility, the owner(s) shall Dedicate to the City said property as an IOD in fee title, at no cost to the City.
61. Per City Code, the Subdivider may not develop the project in any way that obstructs, impedes, or interferes with the natural flow of existing off-site drainage that crosses the property. Furthermore, all lots shall be graded so that drainage does not cross lot or property lines. The project shall construct the required public and/or private infrastructure to handle runoff to the satisfaction of the DOU. If private infrastructure is constructed to handle runoff, the applicant shall dedicate the required private easements and/or, at the discretion of the DOU, the applicant shall enter into and record an Agreement for Maintenance of Drainage with the City, in a form acceptable to the City Attorney.
62. A grading plan showing existing and proposed elevations is required. All lots and/or parcels shall be graded so that drainage does not cross property lines or private drainage easements shall be dedicated. Adjacent off-site topography shall also be shown to the extent necessary to determine impacts to existing surface drainage paths. At a minimum, one-foot off-site contours within 100 feet of the project boundary are required. No grading shall occur until the grading plan has been reviewed and approved by the DOU.

63. The developer(s)/owner(s) shall mitigate the storm water quality impacts generated by this development by implementing comprehensive storm water quality control measure(s) for the entire 65 acres. The DOU shall approve the comprehensive water quality control plan (measures). These measures shall be implemented prior to the recordation of the respective final map(s) or otherwise as approved by the DOU.
64. This project is greater than 1 acre in size; therefore, the project is required to comply with the State “NPDES General Permit for Stormwater Discharges Associated with Construction Activity” (State Permit). To comply with the State Permit, the applicant will need to file a Notice of Intent (NOI) with the State Water Resources Control Board (SWRCB) and prepare a Stormwater Pollution Prevention Plan (SWPPP) prior to construction. A copy of the State Permit and NOI may be obtained from www.swrcb.ca.gov/stormstr/construction.html. The SWPPP will be reviewed by the DOU prior to issuing a grading permit. The following items shall be included in the SWPPP: (1) vicinity map, (2) site map, (3) list of potential pollutant sources, (4) type and location of erosion and sediment BMP’s, (5) name and phone number of person responsible for SWPPP and (6) certification by property owner or authorized representative.
65. Post construction, storm-water quality control measures shall be incorporated into the development to minimize the increase of urban runoff pollution caused by development of the project. Since the project is not served by a regional water quality control facility, both source control and on-site treatment control measures (e.g. storm-water planters, detention basin, infiltration basin and/or trench, media filters (Austin Sand Filter), vegetated filter strips and/or swales, and pre-approved proprietary devices) are required.
66. If a water quality facility such as a detention basin or any publicly maintained feature is required for the development of this project, the developer(s)/owner(s) will be required to form a maintenance district and/or participate in the existing regional maintenance district, which is created to incorporate storm-water quality measures through “extraordinary maintenance procedures”. This maintenance district shall be formed to the satisfaction of the DOU. The extraordinary maintenance procedures are implemented to meet post construction, storm-water quality control measures to minimize the increase of urban runoff caused by development of the area. Acceptance of the required landscaping, irrigation, drainage structures, and other features (Detention/Water Quality Facility) by the City into the proposed financing mechanism shall be coordinated with the Development Services Department (Special Districts); Parks Planning, Design, & Development Department; and the DOU. The developer shall maintain the

Detention/Water Quality Facility for a period of two (2) years or until acceptance by the City into the District, whichever is less. The two-year period shall begin following the issuance of a notice of completion by the City for the Water Quality Facility. At the time of acceptance by the City, the developer shall remove any sediment or debris that has accumulated prior to acceptance.

67. If require by the DOU, the owner(s) shall enter into and record maintenance agreement, for all storm water quality treatment measures, with the city in a form acceptable to the City Attorney.
68. Any storm water quality treatment features proposed within the public right away (e.g. storm-water planters, porous pavements, vegetated filter strips and/or swales, etc.) shall be reviewed and approved by the DOU, the Department of Transportation (DOT) and the Department of Parks and recreations prior to their implementation and prior to the recordation of the respective final Map(s). At a minimum, said proposed features shall consider the following requirements:
 - a. Any infiltration features that utilize natural or imported soil to treat the storm water runoff shall provide a minimum separation between the ground water table and the bottom of said feature of 10 feet. The developer(s)/owner(s) shall provide groundwater elevation data to the DOU confirming this requirement will be met at all times of the year. The groundwater data collection period and locations shall be approved by the DOU prior to collecting the data.
 - b. Infiltration rate of the soil will need to be established. Since most soils around this area have very poor infiltration rates, any roadway pavement adjacent to these features must be designed so that the structural integrity of the pavement is not compromised. Since roadways are designed and constructed to withstand significant traffic loads (vertical loads), stem walls may have to be incorporated as part of the design of the roadway infrastructure.
 - c. Maintenance cost is a major factor in the implementation of these features. Since the city has not approved this as part of the roadway design criteria, no maintenance costs have been assessed for such features. Thus, the developer(s)/owner(s) shall bond for any feature maintenance and/or replacement of this pavement structures, to the satisfaction of the DOT and DOU. If these features were to prove deficient and since these features' purpose is to treat storm water run-off the developer(s)/owner(s) will need to provide alternatives to this treatment feature in case they fail.
 - d. A comprehensive and detail geotechnical report by a register geotechnical engineer will need to be provided for review and approval by the DOT, DOU,

and the Department of Transportation Section prior to the design of these features.

NOTE: Please be advised that the requirements mentioned above are not the only issues that need be considered. Subsequent requirements may be required when and if the information is provided.

69. Prior to the submittal of improvement plans, prepare a project specific water study for review and approval by the DOU. The water distribution system shall be designed to satisfy the more critical of the two following conditions: (1) at maximum day peak hour demand, the operating or "residual" pressure at all water service connections shall be at least 30 pounds per square inch, (2) at average maximum day demand plus fire flow, the operating or "residual" pressure in the area of the fire shall not be less than 20 pounds per square inch. The water study shall determine if the existing and proposed water distribution system is adequate to supply fire flow demands for the project. A water supply test may be required for this project. Contact the DOU for the pressure boundary conditions to be used in the water study.
70. All water connections shall comply with the City of Sacramento's Cross Connection Control Policy.
71. Two points of service for the water distribution system for this subdivision or any phase of this subdivision are required. All public water mains shall be placed within the asphalt section of public right-of-ways or dedicated easements as per the City's Design and Procedures Manual.
72. Current city records indicate an existing private water system for this area. Properly abandon the existing private water system. Prior to abandoning said private system, provide to the adjacent parcels that are dependent of this system and that are not part of this development with adequate water services to the satisfaction of the DOU. Any utility services that cross property lines to serve any existing building-that are not part of this project- shall be relocated to the satisfaction of the Department of Utilities.
73. A sanitary sewer study described in Section 9.9 of the City Design and Procedures Manual is required. This study and shed map shall be approved by the DOU.

PG & E

74. The developer may need to reserve space with a width of approximately 20 feet by 40 for a future easement to be granted to Pacific Gas and Electric Company.

This space would consist of gas regulator station to supply the development with such a large capacity. This will need to be coordinated with PG&E early on in the design stages to decide the best location for the regulator lot's placement if needed. The developer should contact PG&E's Service Planning Department at (916) 386-5112 as soon as possible to coordinate construction so as not to delay the project.

SPECIAL DISTRICTS: Assessment Districts

75. Dedicate to the City those areas identified on the Tentative Subdivision Map as Landscape Corridors, and Open Space areas. Annex the project area to the appropriate Landscape Maintenance District, or other financing mechanism acceptable to the City, prior to recordation of the Final (Parcel) Map. Design and construct landscaping, irrigation and masonry walls (or wood fences) in dedicated easements or rights of way, to the satisfaction of the Development Services Department, Parks Planning and Development Services (PPDS). Acceptance of the required landscaping, irrigation and walls or fences by the City into the Landscape Maintenance District shall be coordinated with the Department of Transportation (Special Districts and Development Services) and PPDS. The Developer shall maintain the landscaping, irrigation and walls for two years or until acceptance by the City into the District (whichever is less). The two year period shall begin following the issuance of a notice of completion by the City for the landscaping, irrigation and walls or fences;

PPDS: Parks

76. Park Dedication – IOD: Pursuant to Sacramento City Code Chapter 16.64 (Parkland Dedication) the applicant shall provide on City's form an irrevocable offer of dedication in fee (IOD) of the parks sites identified on the approved tentative map as Lots 2, 9 and 18. The applicant shall also provide on City's form an irrevocable offer of dedication of exclusive recreation easements in the North 7th Street median between Signature Street and Riverfront Drive (excepting the roundabout at New Street "A", and the Park Boulevard Median, excepting the roundabout at the intersections of Park Boulevard, New Street "C" and New Street "B". In addition, exclusive recreation easements shall be provided for the transit plaza, situated between Lots 13 and 14, the Mew between Lots 3 and 4, and mid-block paseos between Lots 7A/7C, 8A/8C, 12A/12C, 15A/15C, 16A/16C and in the southeast corner of Lot 11 as reflected on the Tentative Subdivision Modification Map, submitted 08/20/2010 (see condition below in "Park Site" section). At the time of delivery of the IOD, the applicant shall (1) provide to City a title report demonstrating that it holds full and clear title to Lots, including all interests necessary for maintenance and access;

(2) provide a Phase 1 environmental site assessment of Lot(s) 2, 9, 18 and the areas subject to the recreation easements; (3) if the environmental site assessment identifies any physical conditions or defects in Lots 2, 9, or 18, or the areas subject to the recreation easements which would interfere with its intended use as a park, as determined by PPDS in its sole discretion, applicant shall complete a supplemental assessment and remedy any such physical condition or defect, to the satisfaction of PPDS; and (4) take all actions necessary to ensure that Lots 2, 9, 18, and the areas subject to the recreation easements are free and clear of any wetland mitigation, endangered or threatened animal or plant species (except for Lot 18), sensitive habitat or other development restrictions. The applicant shall be solely responsible, and at its sole cost, for any required mitigation costs or measures associated with Lots 2, 9, 18, (except for Lot 18) and the areas subject to the recreation easements;

77. Prior to acceptance of recreation easements for the new, transit plaza and paseos, Applicant shall enter into Agreement with City to address matters including, without limitation, (1) waiver of City's liability for damages to Applicant's parking structure which may be caused by the park improvements and/or maintenance of such improvements, (2) Applicant's agreement to hold City harmless and to defend and indemnify City for any claims of injury or damage to persons or property located with a parking garage or other improvements which may be caused by or arise from City's maintenance of the park improvements;

78. Applicant shall be responsible for maintenance of IOD Lots 2, 9 and 18, and all of the areas subject to the recreation easements until the time that the City records acceptance of the IOD. Maintenance of Lot 18 shall be consistent with the maintenance standard for the American River Parkway and be coordinated with the Sacramento County Department of Regional Parks;

79. The square footage of each paseo shall remain intact within each block within which it is located; however, each paseo's location and shape may be reconfigured subject to approval of PPDS and approval of a subdivision modification, as needed;

80. Applicant shall provide an exhibit that shows Lot 18 overlaid on the 2001 aerial provided by the City of Sacramento. The exhibit shall indicate the land acreage that is not submerged and that land that is submerged. Only the land acreage (net) that is not submerged shall be eligible for park land dedication credit pursuant to the Sacramento City Code Chapter 16.64;

81. The following shall not be eligible for park land dedication requirements and/or park development impact fee (PIF) credits pursuant to the Sacramento City Code Chapters 16.64 and/or 18.44:
- a. The roundabouts because they are not safely accessible by the public.
 - b. Any portion of the public right-of-way area that may be used for vehicular use, excepting the 3 foot wide turf block or stamped concrete walkway adjacent to North 7th Street.
 - c. Any area that serves as a water detention or water quality feature unless otherwise approved by PPDS.
 - d. Any area that is submerged under the American River as shown on the 2001 aerials provided by the City.
 - e. The associated costs of the realignment and/or replacement of Two Rivers Trail.
 - f. Any rough grading or off-site improvements outlined in Condition 95 below.

82. Payment of In-lieu Park Fee: Pursuant to Sacramento City Code Chapter 16.64 (Parkland Dedication) the applicant shall pay to City an in-lieu park fee in the amount determined under SCC §§16.64.040 and 16.64.050 equal to the value of land prescribed for dedication under 16.64.030 and not satisfied by dedication. If the final map is phased, each phase must fulfill its parkland dedication obligation;

Park Sites

83. All park sites and recreation easements and their net acreage shall be shown on the final subdivision map. The amount of acreage to be applied towards the park land requirements pursuant to Sacramento City Code Chapter 16.64 (Parkland Dedication) will be determined and approved by PPDS;

84. The exclusive recreation easements (paseos) located on blocks containing Lots 7A/7C, 8A/8C, 12A/12C, 15A/15C, and 16A/16C shall be at least 40 ft wide, not including the building setbacks. The square footage of each paseo shall remain intact within each block within which it is located; however, each paseo’s location and shape may be reconfigured subject to approval of PPDS and approval of a subdivision modification, as needed;

85. Lot 7A and 7C shall provide a 16 ft. wide public access easement for pedestrian and bicycle use connecting the exclusive recreation easement with New Street “D” and New Street “B “;

86. The blocks containing Lots 12A/12C and 16A/16C, shall provide a 16 ft. wide public access easement for pedestrian and bicycle use connecting the exclusive recreation easement between Lots 12A and 12C, and 16A and 16C, and connecting New Street "D" and Signature Street;
87. The applicant shall dedicate a recreation easement for the 20 ft. portion of the 40 ft. wide median along North 7th Street between "Signature" Street and the roundabout at New Street "A", and a 40 ft. wide median between "Riverfront" Drive and the roundabout at New Street "A" that falls within the project boundaries, excluding the roundabout. The easement shall not include curb, gutters, and sidewalks (except for sidewalks within median), on North 7th Street. The easement and the associated net acreage shall be shown on the final subdivision map;
88. The applicant shall dedicate an exclusive recreation easement for the 70 ft. wide 'transit plaza' (located between Lot 13 and Lot 14), excluding an area 28+ ft. by 66+ ft. to allow for the placement of the former scale house to be developed for a retail use;
89. The applicant shall dedicate the 40 ft. wide median on Park Blvd. as a recreation easement between the roundabout at the intersection of New Street "B", New Street "C" and "Park" Boulevard, and New Street "A" excluding the intersections and the roundabout. The easement shall not include curb, gutters, and sidewalks (except for walkways within median) along Park Blvd. The easement and the associated net acreage shall be shown on the subdivision map;

Park Master Planning and Development

90. Applicant shall prepare a park master plan(s) for all parks and all recreation easements. The park master plan(s) shall be prepared to the satisfaction of PPDS and shall be submitted for review and shall be approved by the PPDS, Parks and Recreation Commission and City Council. The park master plan(s) shall be designed to the appropriate neighborhood or community park standard as outlined in Table 18 of the City of Sacramento Parks and Recreation Master Plan 2005-2010 and as determined by PPDS. Park design shall comply with Crime Prevention through Environmental Design (CPTED) principles;
91. Applicant shall be responsible for the design and construction of all park sites and all recreation easements. Applicant shall enter a standard City credit / reimbursement agreement to construct the park improvements to the satisfaction of the City's PPDS. The credit / reimbursement agreement shall address (1) the

92. Each paseo, mew and/or transit plaza shall be developed as one complete park space (not in phases) and shall be constructed concurrently with the first development that occurs within the block within which the paseo, mew and/or transit plaza is located so that the public improvements will be available for use no later than when the housing units or other development on that block are ready for occupancy;
93. Public park improvements in the North 7th Street median and Park Boulevard median shall occur when the street improvements are constructed. The street improvements cannot be accepted until the public median improvements are completed;
94. Site Plan: The applicant shall submit a site plan and electronic file showing the location of all utilities on all park sites and recreation easements to the PPDS for review and approval ;
95. Improvements: The applicant shall construct the following public improvements on all park lots and recreation easements prior to and as a condition of City's acceptance of the park and recreation easement sites:
- a. Full street improvements where adjacent to streets, including but not limited to curbs, gutters, accessible ramps, street paving, streetlights, and sidewalks; and improved surface drainage through the site.
 - b. A concrete sidewalk and vertical curb along all street frontages, unless otherwise approved by PPDS. The sidewalk shall be contiguous to the curb unless otherwise approved by PPDS.
 - c. PPDS to approve rough grading plan for the sites as required by City Code to provide positive drainage as approved by PPDS.
 - d. A twelve inch (12") storm drain stub and six inch (6") sanitary sewer stub to the back of sidewalk on all parks and recreation easements or as sized and located per approved park master plan for future service. Number of

stubs and locations to be approved by PPDS. Storm drain and sewer stubs are to be marked with a 3' high, white 4" x 4" post indicating stub or service location.

- e. One water tap for irrigation, one water tap for domestic water, and electrical and telephone service to all parks and recreation easements. The irrigation water tap shall be 4 inches for parkland 4 acres and larger, and 2-1/2 inches for parkland less than 4 acres, or as approved by PPDS; and the domestic water tap shall be 1 inch. Water taps and telephone and electrical services shall be marked with a 3' high, white 4" x 4" post indicating stub or service location.
 - f. A ten foot (10') driveway into each park and recreation easement at a location approved by PPDS in order to provide future maintenance access to the public spaces.
96. Design Coordination for PUE's and Facilities: If a 12.5 foot public utility easement (PUE) for underground facilities and appurtenances currently exists or is required to be dedicated adjacent to a public street right-of-way contiguous to Lots 2, 3, 4, 7A/7C, 8A/8C, 9, 11, 12A/12C, 13, 14, 15A/15C, 16A/16C, 18 or the North 7th Street or Park Blvd. median, the applicant shall coordinate with PPDS and SMUD regarding the location of appurtenances within the PUE to minimize visual obstruction in relation to the parks and to best accommodate future park improvements. The applicant shall facilitate meetings with SMUD and PPDS prior to SMUD's facilities coordinating meeting for the project;
97. Multi-Use Trail: Any realignment or replacement of Two Rivers Trail or associated access ramps shall be designed to PPDS specifications. PPDS shall approve the alignment and design of the trail prior to submitting improvement plans for the trail;

Any realignment of the North 5th Street trailhead to Two Rivers Trail shall be at Applicant's expense and shall be subject to the review and approval of PPDS and the California Resources Agency.

An access to the Two Rivers Trail shall be provided at North 7th Street. PPDS shall approve the alignment and design of the access.

Vehicular access controls shall be placed at the entrance to all access points to the trail (refer to PPDS details and specifications for approved designs).

Finance Plan and Park Maintenance

98. Finance Plan: The Applicant shall provide a Finance Plan for the project prior to first final subdivision map approval that identifies all funding sources and includes the development of all designated park facilities, trails, open space/parkway or other open space areas anticipated to be maintained by the City of Sacramento Department of Parks and Recreation. The Plan shall include all improvement costs associated with the designated park facilities, trails, open space/parkway or other open space areas along with ongoing maintenance, operations, and replacement costs for these facilities in perpetuity. The Finance Plan shall reference the Applicant’s obligation to pay Park Development Impact Fees, construct turnkey parks, realign or replace Two River Trail, maintain park improvements until dedication is accepted by City, and Applicant’s obligation to form into an assessment district to fund on-going park maintenance as described below;

99. Landscaping and Lighting Maintenance and/or Mello-Roos District: Prior to recording the first final subdivision map, Applicant shall have completed the proceedings to create a Landscaping and Lighting District in accordance with Streets and Highway Code Section 22500 et seq. to fully fund the maintenance of all public improvements under Streets and Highways Code Section 22525, and/or a Mello Roos District in accordance with Government Code Section 53322 (Community Facilities Act) for all park improvements.

The City will be responsible for maintenance of City-owned or controlled property only .

100. The foregoing maintenance district (Lighting and Landscaping or Mello-Roos special tax assessment district), shall encompass the full costs of park maintenance, operations and replacement. The applicant shall pay all city fees for formation of a new assessment or Mello Roos district.

MISCELLANEOUS

101. Meet all conditions of the development agreement;

102. Form a Homeowner's Association or other financial funding mechanism such as a Community Facility District for the maintenance of special features proposed in the design guidelines. CC&R's or other financial funding mechanism shall be approved by the City and recorded assuring maintenance of roadway(s) and/or landscaping. The Homeowner's Association or Community Facility District shall fund the maintenance of all streets, lights, sewers, drains and water systems unless determined otherwise by the City;

Fire Department

103. All turning radii for fire access shall be designed as 35' inside and 55' outside.
104. Roads used for Fire Department access shall have an unobstructed width of not less than 20' and unobstructed vertical clearance of 13'6" or more. Street sections C-C 7th Street North and Section H-H Park Boulevard don't meet this requirement. Therefore, make provisions for emergency use of medians by providing rolled or mountable curbs and surface capable of supporting fire apparatus.
105. Provide the required fire hydrants in accordance with CFC 508 and Appendix C, Section C105.
106. Fire Apparatus access roads shall be designed and maintained to support the imposed loads of fire apparatus and shall be surfaced so as to provide all-weather driving capabilities. CFC 503.2.3.

SMUD

107. The owner/developer must disclose to future /potential owners the existing 21 kV electrical facilities. This whole area is both overhead and underground 21 kV.
108. Dedicate a 12.5-foot public utility easement for underground facilities and appurtenances adjacent to all public street rights of way.
109. Dedicate a 12.5-foot public utility easement for underground and overhead facilities and appurtenances adjacent to Richards Boulevard, 7th Street and 5th Street.

ADVISORY NOTES:

The following advisory notes are informational in nature and are not a requirement of this Tentative Map:

- A. Prior to the issuance of any building permits, provide the City with a copy of the certificate of payment of any school fees for the applicable school district(s);
- B. City may enter into a reimbursement agreement for over-width pavement construction on Richards Blvd;

Note: Subdivider shall notify future property owners within this subdivision that they will be required to maintain the sidewalks and landscaping between the curbs and sidewalks;

Utilities

- C. The applicant is responsible for the protection and repair of the City sanitary sewer and water mains during construction of the proposed structures. Contact Underground Service Alert at 1-800-642-2444, 48 hours before work is to begin.
- D. Sewer development fees are estimated at 5.16 million dollars.
- E. Many projects within the City of Sacramento require an on-site booster pump system for fire suppression and/or domestic water systems. DOU suggest that the future developers request a water supply test for available flows and pressures prior to the design of any buildings.

Regional Transit

- F. Transit information shall be displayed in prominent locations in the residential sales/rental office, through a homeowner's association, or with real estate transactions and for employees and patrons.
- G. For the station block (New Street "C" to 7th Street), the south platform may be constructed within the 11' to 18' City easement, and the track centers may be reduced from 16' to 14', and incorporating the 5' sidewalk north of the light rail tracks into the station platform area.
- H. For the non-station block (5th Street to New Street 'C'), RT proposes leaving the sidewalk north of the tracks but replacing the 16' wide platform areas with a 4' RT maintenance walkway (and making the other changes above).
- I. Bicycle parking facilities should be provided at building entrances.
- J. The applicant shall join the Sacramento Transportation Management Association (TMA).

Solid Waste

- K. Recycling capacity be met or exceeded.

- L. A recycling program shall be established. The developer should send the name of the service provider, the frequency of service, and the processing facility to the Solid Waste Division to verify that service has been established.

- M. This project shall divert construction waste. The project proponent should plan to target cardboard, wood waste, scrap metal, brick, concrete, asphalt, and dry wall for recovery. The developer should submit the following information to the Solid Waste Division (For questions contact Tyler Stratton at 916-808-4927):
 - Method of recovery
 - Hauler information
 - Disposal facility
 - Diversion percentage
 - Weigh tickets documenting disposal and diversion

Parks

- N. A Development Agreement exists for this project which allows parkland dedication credit for lands that do not meet the standards and formulas for dedication of land as established in Sacramento City Code Chapter 16.64, the City’s Quimby Ordinance. A Second Amendment to the Agreement allows paseos, the transit plaza and mew to be dedicated as exclusive recreation easements instead of dedicated in fee title; requires landowner to indemnify the City against liability for damages that may occur as a result of the easement’s improvements or maintenance; and requires that the paseos remain intact within each block and the timing of the development of the paseos

- O. If the project is phased, each phase shall meet its parkland dedication obligation through the dedication of land or payment of in lieu fees or a combination of the two at the discretion of the PPDS prior to approval of each phased final map.

- P. As per City Code, the applicant will be responsible to meet his/her obligations regarding:
 - 1) Title 16, 16.64 Park Dedication / In Lieu (Quimby) Fees, due prior to approval of the final map.

 - 2) Title 18, 18.44 Park Development Impact Fee (PIF), due at the time of issuance of building permit. The Park Development Impact Fee due for this project is

estimated at \$7,799,702. This is based on 2,396 multi-family residential units at the rate of \$3,058 per unit, 839,628 square feet of office at the rate of \$0.50 per square foot and 147,000 square feet of retail space at the rate of \$0.36 per square foot. Any change in these factors will change the amount of the PIF due. The fee is calculated using factors at the time that the project is submitted for building permit. The project does not qualify for the City's 'specified infill' rate because the project as a whole exceeds the size constraints as defined by 'specified infill'.

- 3) Creation of a new maintenance district rather than Community Facilities District 2002-02, Neighborhood Park Maintenance CFD Annexation .

Q. The Developer shall be responsible for maintenance (weed abatement) of IOD Lot(s) 2, 9, 18, and all recreation easements until the time that the City records acceptance of the IOD.

R. Private Facility Credits: City Code Chapter 16.64, Sections 16.64.100, 110 and 120 address granting of private recreation facility credits. The city may grant credits for privately owned and maintained open space or local recreation facilities, or both, in planned developments as defined in Section 11003 of the Business and Professions Code, condominiums as defined in Section 783 of the Civil Code, and other common interest developments. Such credit, if granted in acres, or comparable in lieu fees, shall not exceed twenty-five (25) percent of the dedication or fees, or both, otherwise required under this chapter and no more than five percent per category of open space or recreational facilities described in City Code Section 16.64.100. The request for credit shall be made in writing before recordation of the final map and the eligibility for credit shall be at the sole discretion of PPDS.

S. The City of Sacramento (City) is responsible for providing local sewer service for the subject property. The City collector pipelines will convey the sewage to the Sacramento Regional Wastewater Treatment Plant (SRWTP) via the 96-inch City Inceptor.

Developing this property will require the payment of SRCSD sewer impact fees. Impact fees shall be paid prior to the issuance of building permits. Applicant should contact the Sewer Fee Quote Desk at (916) 876-6100 for sewer impact fee information.

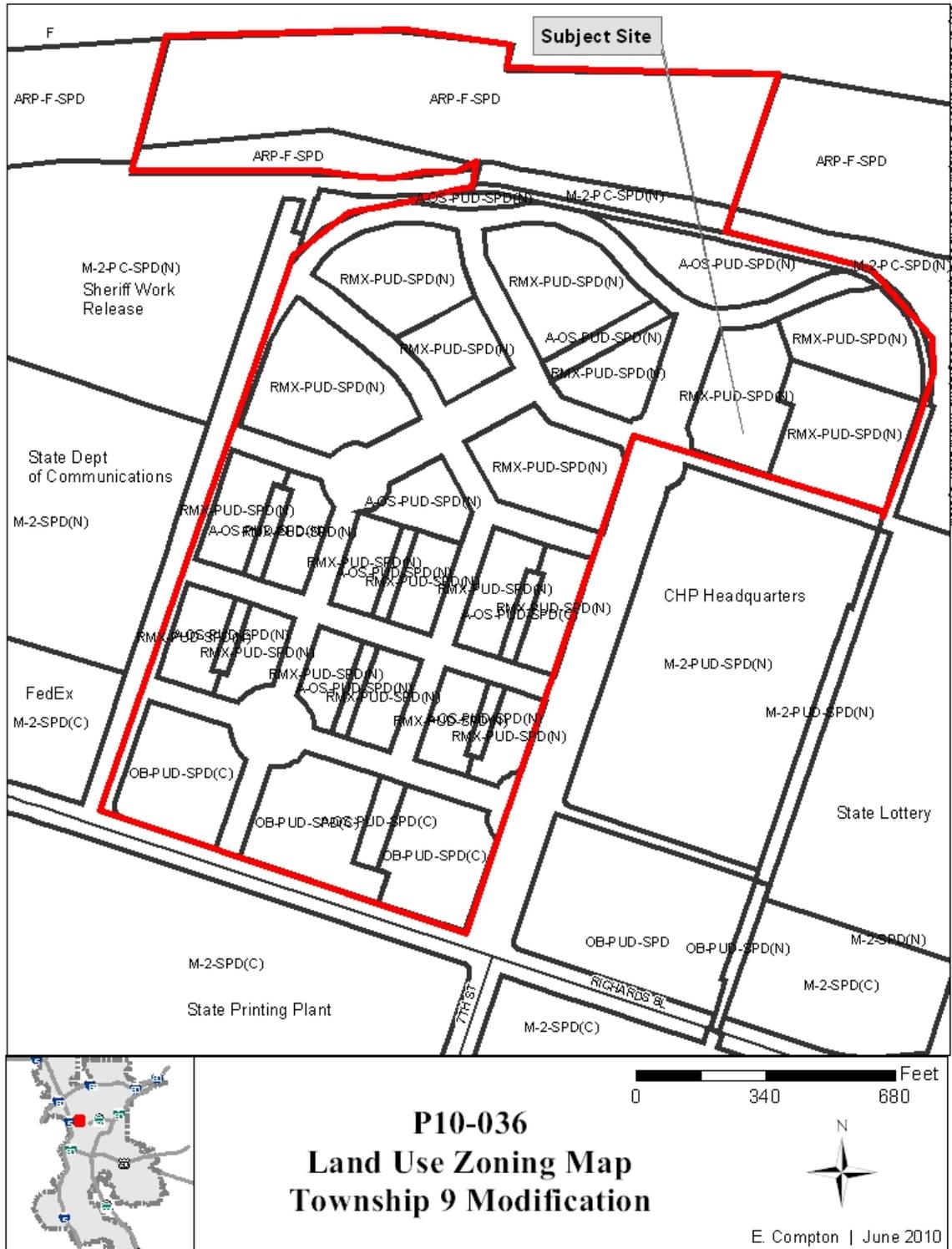
SRCSD will issue sewer permits to connect to the system if it is determined the capacity is available and that the property has met all requirements for service.

Subject: Township 9 Modification (P10-036)

This process is "*first come, first served.*" There is no guarantee that capacity will be available when the actual request for sewer is made. Once connected, the property has the entitlement to use the system. However, its entitlement is limited to the capacity accounted for by the payment of the appropriate SRCSD fees.

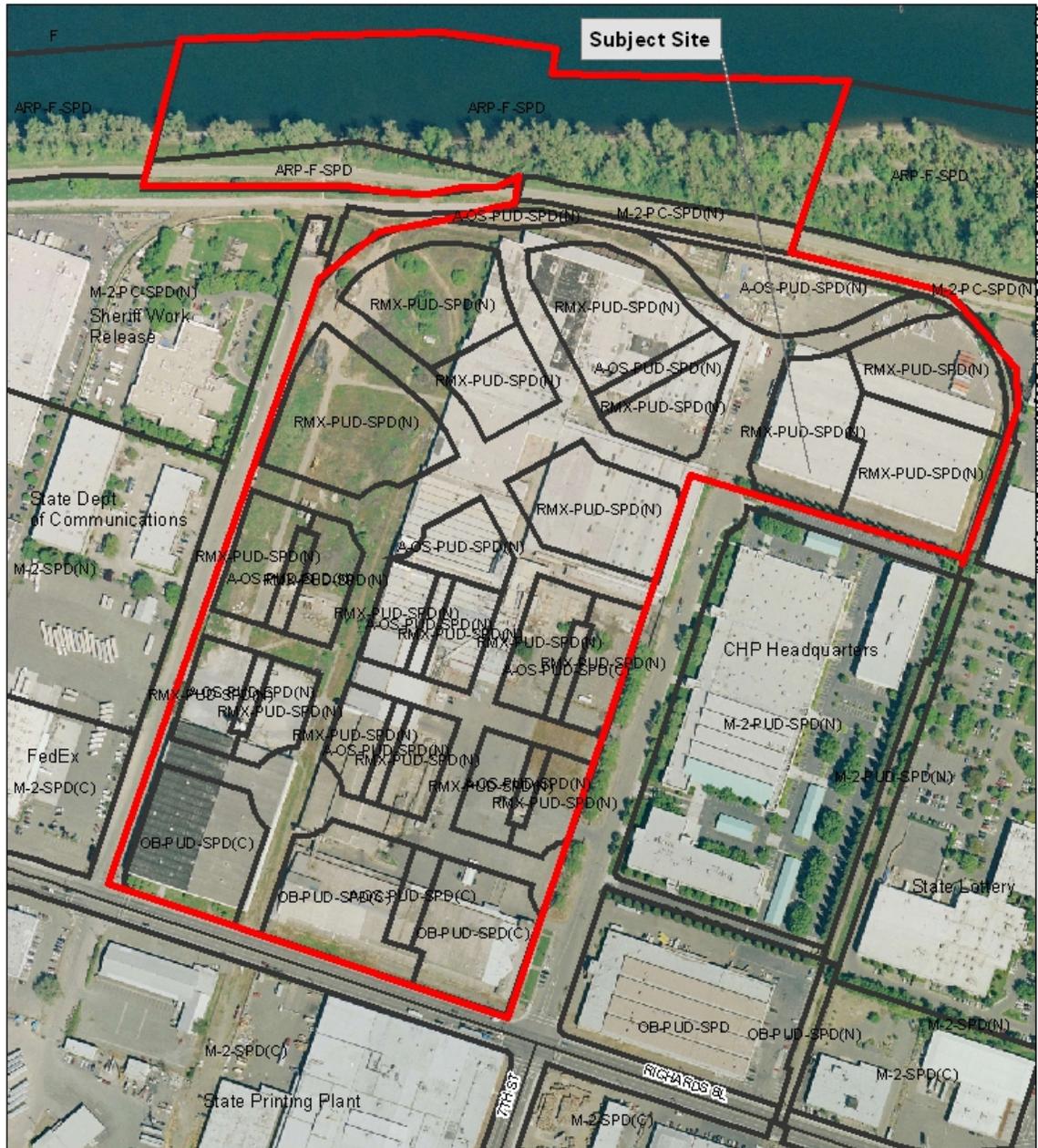
Due to the existing and future growth proposed within the City of Sacramento, it appears that the sewage flow being routed to the City Inceptor may be exceeding or close to exceeding the allotted 108.5 MGD indicated within the Operating and Maintenance (O&M) Agreement between the City of Sacramento, the County of Sacramento, and SRCSD.

Attachment 2: Land Use Map



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Attachment 3: Aerial Map



City of Sacramento Development Services Department Geographic Information System



**P10-036
Aerial Map
Township 9 Modification**

E. Compton | June 2010

Attachment 4: Redline of PUD Design Guidelines Text Amendments

Township 9

Sacramento, California

PUD Design Guidelines

Applicant:

Capitol Station 65, LLC

Consultant:

Carter & Burgess, Inc.

September 7, 2007

Amended October 28, 2010

Section 1: Purpose and Intent

1.1 *Vision for Community*

Township 9 is a mixed-use, urban infill development located on the south bank of the American River in the River District of Sacramento, California. The 65-acre site is generally bounded by the American River to the north, North 5th Street to the west, Richards Boulevard to the south, and North 7th Street to the east. Access to the region is provided via Interstate 5 (a half-mile to the west) State Route 160, (a half-mile to the east) and a future light rail line between downtown Sacramento and the Sacramento International Airport. Direct access to the Central Business District (CBD)—Downtown and Midtown—is provided via 7th Street. The CBD can also be accessed via Jibboom Street and 12th Street. Not only is the CBD within walking distance, alternatives to automobile also include future light rail and bus service and regional bicycle network access via Two Rivers Trail along the American River.

The vision for Township 9 is a vibrant mixed-use neighborhood developed to a human scale in accordance with principles of the Sacramento Blueprint. The plan also incorporates New Urbanism and Smart Growth principles with neighborhood parks, paseos, and tree-lined streets to encourage walking and neighborhood interaction. The pedestrian environment and activated streets will be enhanced by a unified lighting concept, attractive site furnishings and pedestrian-scaled architecture.

1.2 *Purpose and Effect of Development Guidelines*

The Township 9 Design Guidelines have been adopted by the Sacramento City Council under authority of Chapter 17.180 of Title 17 of the Sacramento City Code (the Zoning Code) to implement the Township 9 Planned Unit Development (PUD) and the Richards Boulevard Area Plan (RBAP). These Guidelines contain the development standards applicable to development within the Township 9 PUD and contain specific details, elements, conditions, and restrictions that the City Council determined to be necessary and appropriate to carry out the vision of the Township 9 PUD.

As authorized under Chapter 17.180 of the Zoning Code, certain zoning regulations related to height, setbacks, area requirements, and other development standards otherwise applicable to the property may have been modified in these Guidelines for the Township 9 PUD. Therefore, to the extent that development standards contained in these Guidelines conflict with provisions of the Zoning Code, the development standards contained in these Guidelines shall apply. City of Sacramento staff will review project applications for conformance to these Guidelines as described below in Section 1.3.

The Township 9 Design Guidelines incorporate by reference a portion of the Central City Urban Design Plan design standards. To the extent that the same subject matter regarding a design standard is not included in these Township 9 Design Standards, then the applicable requirement in the Central City Urban Design Plan shall apply to the Township 9 PUD. As of the date of approval of these Township 9 Design Guidelines, the Central City Urban Design Plan was in draft form. Therefore, the adopted version of the Central City Urban Design Plan, and as it may be amended in the future, will be applicable to the Township 9 PUD to the limited extent as described above.

These Guidelines incorporate both mandates and recommendations. *Where the word "shall" is used it is intended to be a mandate and where the words "should" or "encouraged" are used it is intended to be a recommended guideline.* The mandates are treated as standards with little room for variation whereas the recommendations are subject to some interpretation and have room for minor variations.

These Guidelines have been organized for ease of use by planners, design reviewers, City staff and builders. Section 2: Development Areas describes the overall land use plan and types of uses and buildings envisioned for the project. Section 3: Development Standards establishes design standards for buildings, roadways, and individual parcel development. Section 4: Development Guidelines provides recommended design criteria for architecture, site plans, common use areas and other community elements.

1.3 Procedures for Approval

All development in the Township 9 PUD is subject to Planning Director Plan Review to ensure conformance with the PUD Schematic Plan and these Guidelines. The Plan Review application shall include information required on the standard City application.

The Township 9 Development Agreement provides that the Township 9 PUD is exempt from the special permit requirement for large buildings, as set out in Section 17.60.030 (9) of the Zoning Code. If a proposed use requires a special permit, then review for conformity with the PUD Schematic Plan and these Guidelines would be part of the special permit process as provided in Chapter 17.220 in lieu of the Planning Director's Plan Review process.

Because the Township 9 Project is a PUD, development is not subject to City design review under Chapter 17.132 of the Zoning Code that otherwise is required for all development in the Richards Boulevard SPD (Project applications will also be subject to review by the township 9 Design Review Committee which standards and review may exceed these Guidelines). Section 17.120.020 exempts properties within a planned unit development from design review because the PUD project approval process incorporates design review. The Planning Director's Plan Review process will ensure that projects within the Township 9 PUD comply with these Guidelines. Appeal of a Planning Director's decision regarding Plan Review for a project is set out in Chapter 17.220 of the zoning code.

Amendments to these Guidelines and development project entitlements approved in the Township 9 PUD are subject to review and approval of the City as required by the Zoning Code, and subject to the provisions of the Township 9 Development Agreement.

Section 2: Development Areas

Township 9 is comprised of four distinct development areas linked by a network of pedestrian-friendly streets and open spaces. The design vernacular is "urban mixed-use" where elements of the former cannery building can mix with contemporary mixed-use design elements typically found in warehouse conversion developments. The **Transit Area** is a high density mix of office and/or high-density residential and retail uses adjacent to a future light rail station on Richards Boulevard. The **Central Mixed-Use Area** provides a medium density transition from the intensity of the transit area to a central residential neighborhood. The **Live-Work / Townhouse Area** provides an enclave of primarily residential uses. The **Riverfront Area** is a collection of mid-rise residential buildings with ground floor retail that embrace the new Riverfront Drive along the American River.

2.1 Transit Area

The transit area incorporates a future light rail station and fronts Richards Boulevard, the arterial connecting

the project to highways east and west of the site. The Transit Area will also have frontage on Signature Street, the “main street” of Township 9, located parallel to Richards Boulevard and one block north. The Transit Area will be the front door to Township 9 and will have the highest activity, highest densities and tallest buildings. It is planned either for mixed use combinations of multi-family residential or office with ground floor retail. It is anticipated that buildings in this area will have underground parking with the ability to have shared parking. Dramatic downtown views will be provided to users of buildings on these sites.

The transit station at Township 9 presents a unique opportunity to preserve the history of this site as one of the prominent canneries in California. The station is envisioned as a partially enclosed warehouse structure built from the trusses and bricks recovered from the largest of the historic cannery buildings on the site.

2.2 *Central Mixed Use Area*

The Central Mixed-Use Area provides a transition from the higher density transit area to a lower density residential area in the middle blocks of the project. It is intended to provide an active neighborhood edge and street life while keeping a pedestrian scale. Buildings will be 3 to 5 stories in this area. Retail is expected to serve the surrounding residential neighborhood, except along Signature Street where community or regional uses may occur. Many parcels in this area are within two development areas—the Central Mixed Use and the Live-Work/ Townhouse Area that are discussed below. There is no fixed or defined boundary between the two development areas on any given parcel; however, the Central Mixed-Use Area is intended to have 50’ to 100’ lot depths fronting major streets, such as Signature Street and North 7th Street, or the area fronting a public open space.

2.3 *Live-Work / Townhouse Area*

This area is planned primarily for multi-family residential use. It will be developed at a pedestrian scale with building story heights limited to low-rise construction. Two to four-story attached residences are expected. The area will have a network of local driveways and pedestrian passages connecting the Transit Area to the Riverfront Area.

Buildings and sites in the Live-Work / Townhouse Area are limited to Multi-family Residential or any non-residential accessory use directly related to Live-Work occupancies that would otherwise be allowed in comparable zones elsewhere in the City and as stipulated by the existing Richards Boulevard Area Plan.

2.4 *Riverfront Area*

The Riverfront Area encompasses the parcels adjacent to the American River. This is the most unique district because of the visual and physical access to the River. The grade within this district will be elevated to meet the top of the existing levee, thereby eliminating the barrier normally created by a levee. This will benefit users of the Two Rivers Trail by providing access and improved safety while also creating much needed open space and recreational opportunities for residents of Township 9 and the citizens of Sacramento. Riverfront Drive (as prescribed by the Richards Boulevard Specific Plan) is incorporated into this district as a meandering roadway with open space along the north edge and a tree-lined pedestrian walkway and active ground floor buildings to the south (Figure 9).

Buildings in the Riverfront District are primarily high density multi-family residential with ground floor retail. Buildings along Riverfront Drive are limited to 4 stories with front facades and major entrances oriented towards the river to accentuate the prominence of this unique adjacency. Buildings shall be oriented to minimize the visual impact from the river view while also allowing views of the river and parkway from

individual buildings.

To minimize the visual impact of buildings from the river the following guidelines shall be incorporated into each parcel along Riverfront Drive.

- Buildings in this district must adhere to setback standards as defined in Section 3 of this document.
- Street trees shall be planted along Riverfront Drive per the streetscape guidelines in Section 4.7 of this document.
- Building facades visible from the river shall conform to the architectural guidelines in Section 4.2 of this document.
- Building bulk, massing and towers shall conform to the architectural guidelines in Section 3.6 & 3.7 of this document.

Section 3: Development Standards

3.1 *Land Use and Zoning*

Purpose & Intent: Provide zoning that allows mixed-use development within the PUD. Encourage active ground floor uses within the mixed-use buildings.

The Tentative Map of Township 9 consists of multiple parcels on 65 acres of land, and it is anticipated that multiple final maps will be recorded and further subdivision of the property will occur in the future. The land uses for the development parcels are a combination of residential, retail, office and commercial. There are also several open space parcels to accommodate parks and public spaces. The PUD incorporates flexibility in uses on individual parcels to facilitate changes in market conditions and to encourage creativity in development. Many parcels have a range of potential uses and most are encouraged to have ground floor retail. The specific land uses and zoning by parcel are shown in the following plan and table.

3.2 *Allowable Development*

Purpose & Intent: Provide mixed-use development with maximum flexibility to transfer uses and density within the PUD.

The specific allowable development area by parcel is shown in the following table. Transfers of density and uses are permitted within the PUD provided the total density and square footage does not exceed the totals shown in the table.

3.3 *Permitted Uses*

Purpose & Intent: Promote a dynamic mix of uses within the PUD that serves the daily needs of local residents while also creating destination oriented retail and commercial uses. Maximize the potential of the transit station access through TOD oriented retail and commercial uses. Note that area boundaries do not necessarily correspond with parcel boundaries.

Transit Area Permitted Uses

Buildings and sites in the Transit Area (parcels 13, 14 and 17) may be used for the following:

- Office

- Multi-Family Residential
- Child Care Facilities
- Ground floor commercial, retail or service uses as follows:
 - Art gallery
 - Bank, savings and loan
 - Barber, beauty shop
 - Book, stationary store
 - Cleaners, laundry agency
 - Clothing store
 - Florist
 - Food store, bakery, delicatessen
 - Gym
 - Laundromat
 - Restaurant, café
 - Retail
 - Photography studio
 - Martial arts training
 - Video rental store
- Education
- Hotels
- Parking, general and parking structure
- Open space
- Transit stations and rail facilities
- Other uses as approved by the Planning Director

Central Mixed Use Area Permitted Uses

Buildings and sites in the Central Mixed Use Area (parcels 6, 7, 8, 10, 11, 12, 15 and 16) may be used for the following:

- Office
- Multi-Family Residential
- Child Care Facilities
- Ground floor commercial, retail or service uses as follows:
 - Art gallery
 - Bank, savings and loan
 - Barber, beauty shop
 - Book, stationary store
 - Cleaners, laundry agency
 - Clothing store
 - Florist
 - Food store, bakery, delicatessen
 - Gym
 - Laundromat
 - Restaurant, café
 - Retail
 - Photography studio
 - Martial arts training

- Video rental store
- Parking, general and parking structure
- Open space
- Other uses as approved by the Planning Director

Live-Work and Townhouse Area Permitted Uses

Buildings and sites in the Live-Work and Townhouse Area (parcels 7, 8, 11, 12, 15 and 16) may be used for the following:

- Multi-Family Residential
- Child Care Facilities
- Ground floor commercial, retail or service uses as follows:
 - Art gallery
 - Bank, savings and loan
 - Barber, beauty shop
 - Book, stationary store
 - Cleaners, laundry agency
 - Clothing store
 - Florist
 - Food store, bakery, delicatessen
 - Gym
 - Laundromat
 - Restaurant, café
 - Retail
 - Photography studio
- Parking, general and parking structure
- Open space
- Other uses as approved by the Planning Director

Riverfront Area Permitted Uses

Buildings and sites in the Riverfront Area (parcels 1, 3, 4, 5 and 6) may be used for the following:

- Multi-Family Residential
- Child Care Facilities
- Ground floor commercial, retail or service uses as follows:
 - Art gallery
 - Bank, savings and loan
 - Barber, beauty shop
 - Book, stationary store
 - Cleaners, laundry agency
 - Clothing store
 - Florist
 - Food store, bakery, delicatessen
 - Gym
 - Laundromat
 - Restaurant, café
 - Retail
 - Photography studio

- Hotels
- Parking, general and parking structure
- Performing Arts Center
- Open space
- Other uses as approved by the Planning Director

3.4 Density and Height Standards

Purpose & Intent: Provide an urban standard for development that supports Transit Oriented Design objectives established by the Sacramento Blueprint. Create a pedestrian oriented urban environment for mixed-use development.

Guidelines for residential density are provided here with the intent of allowing development flexibility while retaining the overall character of each development area as it is presented in the conceptual plan. Residential density ranges are approximately those found in the concept plan. Building heights shall be limited to the lower limit either by stories or height in feet. Building heights take into account that the ground floor of a mixed-use building may be higher than a standard residential floor.

The guidelines rely on the definition and measurement method of building height in the City Zoning Ordinance. Exceptions to building heights that are allowed elsewhere by Ordinance would be allowed here. Buildings with sloping roofs and/or limited architectural features of buildings may exceed height limits as approved during Design Review.

3.5 Building Setback Standards

Purpose & Intent: Provide an urban standard that is similar to the Central Business District and urban areas of Midtown Sacramento. Provide an urban edge to all streets with interesting ground floor building massing.

The building setbacks shown in table 5A are based upon the adjacent sidewalk and are intended to establish an urban relationship between building and street.

Additional riverfront setback criteria regulate parcels 3 and 5B as follows:

Riverfront Building Setbacks:

The building setback criteria for those buildings located on parcels 3 and 5B are unique due to their proximity to the American River Parkway. Buildings located here are subject to "Parkway Setbacks" as follows:

- 400' minimum setback from the American River Waterline for buildings exceeding 4 stories in height.
- The American River Waterline for this PUD shall be defined per Nolte survey July 2007 and aerial photo (dated 2001).

3.6 Building Step-back Standards

Purpose & Intent: Provide articulation for the upper levels of buildings exceeding 4 stories in height to enhance the scale of the building at street level for pedestrians, allow more sunlight to reach the street level and to make the buildings more architecturally interesting at the upper levels.

The City of Sacramento Urban Design Plan (UDP) requires a building bulk reduction of 10% for the upper 20% of the tower elements (greater than 8 stories) within the Central Business District. This PUD incorporates the PUD criteria as a standard and adds additional "step-back" criteria to address the numerous buildings within the PUD that are between 8 and 12 stories in height. Because these buildings are allowed to have a ground floor plan length of 200', the step-back criteria helps to articulate the upper levels of the buildings to achieve the purpose and intent statement above.

The step-back criteria are unique to the adjacent street frontage to reinforce the scale of that particular street. Wide streets require less step-back than narrow streets. The standards are identified in Table 5B.

3.7 *Building Bulk Standards*

Purpose & Intent: Define spatial standards that will create a visually cohesive yet lively community image for the diverse building types of Township 9. Establish architecture as community building blocks whose collective effect in creating the urban form is greater than the individual buildings alone.

The City of Sacramento Urban Design Plan (UDP) requires a building bulk standard for buildings exceeding 8 stories in height that limits the plan dimensions to 90' length with a 120' diagonal. This PUD supercedes this standard by allowing buildings between 8 and 12 stories to use a maximum plan dimension of 200' length with a 225' diagonal. Refer to figure 15.

3.8 *Parking Standards*

Purpose & Intent: Provide parking commensurate with the density and variety of uses within the PUD while allowing reduced parking requirements for parcels in close proximity to the future light rail station.

The minimum parking standards for the PUD are based upon the Richards Boulevard Area Plan (RBAP) Special Planning District (zoning code 17.120.020). These standards are identified in Table 6 below. There are several additional standards that supersede the RBAP parking requirements as follows:

1. Parcels 12, 13, 14, 15 and 17 will be allowed a reduced parking requirement of 1 space / 1,000 square feet of office space and 1 space / unit with no guest parking space for residential units. This reduction may require an interim off-site parking solution prior to the operation of the light rail station subject to approval by the Planning Director.
2. Adjacent on-street public parking spaces will be counted towards the required retail and guest parking within the PUD. This will be limited to the parking directly adjacent to the public ROW fronting the building and excludes office, hotel and general commercial requirements.

Reciprocal parking is encouraged on a project basis where adjacent buildings and/or parcels have compatible shared use opportunities. General guidelines for reciprocal parking are provided in the appendix.

3.9 *Private Open Space Standards*

Purpose & Intent: Provide private open space for residential buildings within the PUD that complements the public open space areas throughout the project. Allow high density building types to waive individual private open space where common open space is provided.

A. Office:

- Office buildings shall provide open space at a ratio of one square foot of open space per 10 square feet

of site development (1:10) area.

- The open space area shall incorporate, but not be limited to, one or more of the following features:
 - Landscaping (i.e., turf, trees, flower gardens, etc.)
 - Decorative paving (i.e., tile, cobblestone, colored concrete, etc.)
 - Public art pieces
 - Water features (i.e., ponds, reflecting pools, etc.)
 - Seating areas
 - Canopies
 - Lighting
- Open space features shall be complementary to the building architecture. Design elements, material, colors and lighting should be contextual with the proposed building or existing adjacent buildings.

B. Residential:

Multi-family residential housing:

- A minimum of 80 square feet of common usable open space (e.g., for the use of two or more units) per unit is required. Such areas may include courtyards, gardens and recreational areas.
- A minimum of 50 square feet of private usable open space (e.g., for the exclusive use of one unit) per unit is required. Such areas may include decks, balconies and patios. Private usable open space should be directly accessible from the associated unit.

Within the RMX zone, the Richards Boulevard Special Planning District includes an open space requirement, as set out in Section 17.120.020 (3)(g) of the City Code, that specifies that a minimum of eighty (80) square feet of common usable open space per unit and a minimum of fifty (50) square feet of private (exclusive) usable open space per unit that is directly accessible from the unit. Landowner shall be allowed to meet this combined 130 square foot on-site open space per unit requirement by a combination of common and private open spaces. This means that if the amount of common open space provided exceeds eighty (80) square feet per unit, the excess common open space may counted towards meeting the minimum of fifty (50) square feet of private open space per unit, as long as each unit has some amount of private open space (e.g., a Juliette balcony). In addition, all or part of the common open space for one parcel may be provided on the immediately adjacent parcel of land if the common open space is accessible to the residents in both of the adjoining parcels. Landscaped setback areas may be counted towards meeting the common open space requirement if the setback area is designed to be an integral part of a larger common open space area.

The PUD acknowledges that the City anticipates adopting a new Central City Urban Design Plan that makes recommendations regarding changes to Central City design guidelines and development standards, including open space requirements for residential developments. If and when the Urban Design Plan is adopted and the Richards Boulevard Special Planning District is amended to change the open space requirements for residential developments, the Project shall be subject to those open space requirements if such requirements provide for a reduction in the total amount of common and private open space.

Live/Work housing:

- Common usable open space is encouraged to be developed as courtyards for the use of all units. There

is no minimum standard.

- New live/work developments are encouraged to provide a gallery space at ground level, to be used as exhibition space.

Section 4: Development Guidelines

4.1 *Building Orientation Standards*

Purpose & Intent: Provide guidelines that encourage thoughtful placement of individual buildings that contribute to the overall fabric of the PUD.

To support the creation of a community of buildings that are pedestrian-oriented and that promote convenient access to the street and/or public spaces, the following guidelines will apply to architectural development in all areas of Township 9:

- Building Orientation - all building main entrances shall be oriented to streets and public spaces.
- Sidewalk entries - shall be located to accommodate ease of pedestrian movement along primary streets. In general, applicants will be required to distribute sidewalk entries as frequently as necessary to demonstrate acceptable pedestrian-focused design.
- Micro-climatic considerations – should be considered when locating entrances and placing buildings on the site. Passive solar orientation, wind-tunnel effects, shadows cast by the building and other influences should be evaluated by individual projects within the context of the adjacent streets, buildings and overall project development.
- View corridor considerations – should be considered when locating buildings within the PUD to ensure that extended views from within the PUD towards the American River Parkway are maximized. This is particularly important for parcels located within the “modified grid” of the Riverfront Area where angled buildings and angled streets occur. Similarly, views from the parkway looking towards the PUD should also be considered such that visual penetration between buildings is maximized.
- Awnings and/or canopies – Building entrances and pedestrian paths in front of retail uses shall be provided with shelter from sun or inclement weather by means of awnings or canopies. These elements may encroach into right of way to cover sidewalks. Structural supports for these may occur in sidewalks as long as public access is not impeded and travel clearances required elsewhere are maintained such as for barrier-free access. Continuous coverage of pedestrian paths for up to 75% of street frontages are encouraged at key locations in the plan (Figure 16) to unify and reinforce important pedestrian oriented places.
- Street Terminuses - all buildings shall be sited / oriented to serve as terminuses to primary streets. No parking or unarticulated building wall shall terminate any street corridor.
- Community Service Uses - all community-serving building uses within residential projects are strongly encouraged to be oriented to face the street or other adjacent public space (these uses may include leasing offices, fitness centers, community centers, etc.)

4.2 *Building Design Elements*

Purpose & Intent: Building design throughout Township 9 should promote visual interest and diversity

through use of architectural detail and massing changes where appropriate.

Building design within the PUD is to reflect the warehouse character of the cannery while also creating an edgy architectural style similar to the Pearl District in Portland, Oregon. Brick, masonry, wood trusses, and other industrial materials are encouraged. Modern design features are also welcomed. The following general design elements shall also be considered.

- **Building Articulation** - Unbroken facades shall be limited to 120 feet in length with articulation based upon multiples of 40 feet maximum. Fenestration will be required on approximately 50% of each building façade. Un-articulated walls facing streets or public spaces will not be allowed on any building façade in Township 9.
- **Building Facades** – buildings should be clearly organized to have a Base Course (bottom), Street Wall (middle), and Cornice (top).
- **Building Base Course** – should have visual interest and variety since it defines the street experience within the project. The base course should be scaled for the pedestrian with rich materials, texture and detailing. Durable materials such as stone and masonry are encouraged. Additional details such as arcades, colonnades, awnings and other changes in the vertical plane are encouraged.
- **Building Street Wall** – this element creates the urban massing of the community. Windows and balconies provide visual interest to this element. Patterns should reflect the urban character of the community and avoid relentless grids and repetitive patterns. Reveals, step-backs, moldings are encouraged to create shadow lines and visual interest. This fenestration should differ from the base course and the cornice.
- **Building Cornice** – parapets and roof elements should be designed with decorative treatments that provide a clearly defined top to the building. Variations for entries, setbacks, and corners are encouraged. Roof top aesthetics, as viewed from adjacent buildings, should also be incorporated.
- **Building Entries** – entries should be clearly defined and oriented to the public street frontage. Multiple entries at corner locations are encouraged to activate both street frontages. Canopies, awnings and other features are encouraged with distinctive lighting for safety and effect. Service entries should be located away from the main entrance where possible. Main entrances should be elevated whenever practical especially for individual residential units facing a street or other public or semi-public space. This provides territorial reinforcement and exposure to abnormal users.
- **Building Corners** – Building corners shall be designed to support increased pedestrian activity and way finding needs and are encouraged to emphasize street termini and intersections. Buildings shall be designed to accommodate City of Sacramento required visibility triangles without compromising the corner design.
- **Tower Elements** – encouraged at key corners or terminus locations. The tower element should be integrated with lower elements of the building incorporating the same family of materials and interconnecting the base course, street wall and cornice features.
- **Clearstory Windows** – Many of the warehouse buildings associated with the Cannery Facility had north facing clearstory windows to vent heat and provide natural lighting. This detail is encouraged for buildings where practical.
- **Roof Mechanical Equipment** – Roof mounted mechanical equipment shall be screened from public view through use of parapet walls or continuous partial roofs.

4.3 *Riverfront Drive Guidelines*

Purpose & Intent: Provide guidelines for buildings along Riverfront Drive that promote architectural solutions that minimize visual impact to the users of the adjacent American River Parkway.

Building Materials:

- Natural colors (medium and dark earth tones) found along the American River corridor shall be incorporated into the building facades where visible from the river.
- Transparent and/or low reflectivity glass shall be incorporated into the building facades where visible from the river.
- Non-reflective surfaces are required to minimize glare towards the river.
- Natural materials such as stone and wood are required within the building facades as accents or ground floor features.

Building Massing:

- Building accents shall emphasize the horizontal features of the parkway (rather than vertical).
- Building elements (exceeding 4 story height) must adhere to setback and massing standards established in section 3 of this document. They should be oriented at an angle towards the river to preclude a monotonous wall of towers and to allow visual penetration between towers as viewed from the parkway.
- Building facades along Riverfront Drive should have breaks and variations to avoid a monotonous urban edge.

Building Landscaping & Lighting:

- Street Trees shall be installed along the South edge of Riverfront Drive as defined in the Landscape Guidelines).
- Landscaping is encouraged at ground level and elsewhere on the building where practical (balconies, terraces, outdoor areas) to provide a vegetation buffer and to screen the building form the river view.
- Lighting of the building shall be minimal along the waterfront. Shields and directional louvers are required to ensure minimal spillage across Riverfront Drive and into the river.
- Where commercial and/or retail uses occur along Riverfront Drive the signage and lighting shall minimize bright lights, flashing lights, neon and other highly intrusive light sources that could be visible from the river.

4.4 *Streetscape and Circulation*

Purpose & Intent: Provide a cohesive streetscape that ensures a walkable community where pedestrians are comfortable, safe and interested in walking to daily services and transit facilities. Establish an urban tree canopy on all streets.

The development of Township 9 as a vibrant, sustainable urban center will require the implementation of a hierarchical framework of sidewalks, streets, lighting, parks, greenways, plazas, and other public facilities – the design of which will be critical in establishing a dynamic, pedestrian-oriented environment.

The streetscape and circulation network of Township 9 is seen as a hierarchy of public spaces that create a

strong sense of place and contribute to the social vibrance of the community. Streets, intersections, and their related streetscapes are designed not only to move vehicular and pedestrian traffic, but also to enrich the public realm by encouraging street life, a sense of community, and promoting pedestrian activity and social interaction throughout day and evening hours alike. The key design features that will be used to achieve the objectives are:

- Continuity of street parking, planters, sidewalks, and building setbacks along each street
- Consistent and generous width of sidewalks
- Consistent tree canopy and spacing
- Consistent light spacing
- Bulb-outs/neck-downs and wide, well marked crosswalks at intersections

Landscape Easements

The rights-of-way for streets will vary, but the landscape easement (LSE) shall be defined from the back of curb to the adjacent building. The landscape easement will be used to ensure consistent design, use, and maintenance of the public realm at the edge of the street. The maintenance of the LSE will be the responsibility of either a community facilities district (CFD), homeowners' association (HOA) or landscape and lighting district (LLD) to be established by the Township 9 developer. The LSE encompasses two or three parallel streetscape zones, all of which have unique characteristics and uses as defined below.

Zone A – Street Tree Planter Zone

This zone occurs on all streets throughout the PUD and is usually 8' wide (back of curb to sidewalk). The width increases to 10' on Riverfront Drive. The primary use is to provide planting area for street trees, which are to be spaced uniformly at approximately 30' on center in order to ensure a consistent and extensive shade canopy. Tree species shall be the same along the length of streets within the PUD as specified by the Street Tree Map. To avoid monoculture, no one species should be used on more than two streets. On the ground, turf should be the predominant planting to allow pedestrian access. Other low ornamental planting may be allowed occasionally.

Paving in Zone A can be at corners (bulb-outs) and at certain, relatively small stretches to allow for building entrances, outdoor dining areas, etc. to connect to the curb. These paved areas shall be carefully located so as to not interrupt the regular spacing of street trees, which have priority within Zone A. Site furniture (benches, trash receptacles, bike racks, etc.) are allowed within Zone A on paved surfaces only. Lighting shall be located midway between alternating tree locations (approximately 60' on center).

It is a goal of this document to ensure that the streets of Township 9 have a full and continuous tree canopy and a consistent rhythm of vertical elements (trees and lights) along their edges. Site plans for individual parcels should maintain the prescribed spacing (30' for trees, 60' or 120' for lights) by locating driveways in between tree and light locations. Applicants are encouraged to split wider driveways into two or more driveways and use tree grates or tree wells when necessary. They should design patios, plazas and other hardscape features and overhead structures around light locations (which may occur in hardscape). Utilities should be placed around trees and lights, not vice versa. These measures help to give priority to the visual public framework (shade, trees, lights), making it an unbroken urban platform from which private developments can build variety and architectural character.

Zone B – Pedestrian Zone

Sidewalks are a minimum of 8' wide on all streets and increase to 10' at Riverfront Drive. They vary in size on

Richards Boulevard (depending on the light rail right-of-way). Streets and sidewalks may have coverings for protection from rain and sunshine. These coverings combined with a consistently full tree canopy over streets and sidewalks and the ample width of sidewalk pavement, will help to provide a pleasant pedestrian experience. To ensure consistent design throughout the project, sidewalks should have similar color and scoring pattern (see subsequent discussion on Paving Design). Sidewalks should generally remain free of permanent obstructions; however columns (for sidewalk coverings or similar structures) may be located at the edges of sidewalks.

Zone C – Buffer Zone

The third zone from the street is the buffer zone and is a more flexible space that may be used in different ways depending upon uses in the adjacent buildings. The width varies from 8' to 10' where it occurs (it does not occur on local streets or Richards Boulevard). Zone C should be considered the "front yard" of the adjacent ground floor building uses and should be designed in such a way to compliment them. Outdoor dining patios, small plazas, seating areas, or other enhanced paving areas are encouraged. A minimum of 15% (square footage) of Zone C shall be landscaped with planting areas or potted plants. Full width continuous planters in Zone C are discouraged.

This zone may be covered by awnings or canopies which extend from the building. Architectural elements such as bays, or columns may encroach and raised planters can be used to provide seating. The design of this area is to be unique to the adjacent building and need not match any particular standards, other than minimum width and percent of landscaping. In special cases such as building entrances or large outdoor dining areas, the paving layout and planting design may extend beyond Zone C, into the pedestrian and tree planter zones. In such cases, street tree spacing and a minimum sidewalk width of 5' shall be maintained (tree grates are allowed).

Street Furniture

Minimum street furnishings are identified in this document. Individual parcels may propose additional furnishings so long as they are coordinated with adjacent parcels and support a consistent approach for that particular city block and / or street. Street furniture shall be placed in logical relationship to primary pedestrian movement with higher concentrations at retail buildings and open spaces/plazas. Recommended locations and/or placement intervals for street furniture in all zones of development shall be as follows:

Benches, trash cans, and bicycle racks should be constructed in a manner consistent with crime prevention strategies and placed in highly visible locations.

- **Benches** - Benches shall be installed at a one (1) per 100' average. Owners may place benches individually or in groupings, but near street lamps, street intersections, and building entries as necessary. Seat walls around raised planters are acceptable substitutions for benches.

Benches should be designed to discourage sleeping by utilizing partitions or circular designs that wrap around trees or poles. In areas such as bus and transit stops where loitering is not desirable, benches should also be designed to deliver minimal comfort while still providing a temporary resting place for legitimate customers. Wrought iron benches are desirable because they provide a fireproof design that is difficult to damage and is easily secured to the ground.

- **Light Standards** – Street light standards shall be located within Zone A and/or Zone C and spaced at the mid-point of adjacent street trees. The lights shall be decorative features within the streetscape and are

defined in the street furniture appendix of this document.

- **Bollards** – Post bollards and/or light bollards shall be installed as needed in drop-off and plaza areas.
- **Trash Receptacles** - Trash receptacles shall be installed at a one (1) per two (2) benches or group of benches. One per block minimum.
Trash receptacles should be designed to be vandal resistant. Wrought iron designs are fireproof, can be easily secured to the ground and cannot be easily broken and utilized as a weapon or projectile.
- **Bicycle Racks** - Bicycle racks shall be placed near building entrances in Zone A or C only.
- **Pet Waste Stations** – Pet waste stations are to be located so as to conveniently serve pedestrian use. Logically, residential streets and open space will require the greatest concentration, and applicants shall place at least one (1) station per City block.

Street Trees

Township 9 is designed to facilitate a dense urban street tree canopy. Each street within the project site has provisions for street trees. In most cases the tree will be located within the planter strip located between the curb and the sidewalk, but there are some streets where the tree is in an individual tree planter or tree grate. Trees are also located in medians and adjacent open space areas.

Each street within the project has been assigned a particular tree species per the figure below. This map identifies the primary tree to be located on each street but is not intended to be the only tree species allowed on that street.

Paving Design

While building designs will be allowed to have their own style, building materials, forms, etc., the wider framework of streets and sidewalks should be unified in its design in order to create some cohesiveness for Township 9. The paving design of Township 9 will be an important factor in unifying the public realm by providing visual continuity. There are four categories of paving found throughout the site that will be influenced by these design guidelines: sidewalks, crosswalks, special vehicular paving (at intersections and Signature Street), and Riverfront Promenade/Two Rivers Trail.

- **Sidewalks** shall be built with score lines at 3.5' feet apart along their length and width (as illustrated in Figure 26 – Typical Sidewalk Paving Design). Since most sidewalks are 8' wide, the scoring pattern will have a score line down the center, leaving a 6" strip at each edge of the sidewalk. This pattern is found on many historical sidewalks in Sacramento. At 10' sidewalks, the same pattern should be applied, with a line down the middle and 18" strips on the sides. The color shall be a standard medium gray to be applied as a dust on powder (for consistency over several pours and patches). Paseo walkways, bulbed-out street corners, and small extensions of the sidewalks into the planter strip should all have the same color and pattern as the regular street sidewalks. As previously noted, specialty paving that is associated with a building may interrupt the standard sidewalk color and pattern at certain special locations such as building entries.
- **Crosswalks** - Specialty paved crosswalks in Township 9 shall be 15' in width and shall consist of brick pavers in a running bond (or similar decorative) pattern with a 2' wide colored concrete band or soldier courses of pavers (2' width) at the crosswalk edge. To promote universal accessibility for all residents, employees, and patrons, barrier free ramps complying with ADA standards shall be installed at each intersection crossing.

- **Special Vehicular Paving** is defined as street pavement (not including crosswalks) within enhanced intersections, traffic plazas, Signature Street, and Park Boulevard north. Materials may consist of stamped and colored concrete or asphalt, concrete unit pavers, or a befitting combination of each. For consistency and continuity, complementary design treatments shall be used at all special vehicular paving within Township 9.

Street Sections

Subject to phasing as market and traffic conditions allow, the Master Developer will be responsible for installing improvements to the major streets within the project—Richards Boulevard, North 7th Street, Riverfront Drive, North 5th Street, Park Boulevard and Signature Street. Builders of buildings on individual parcels shall be responsible for installing improvements for local streets adjacent to their parcels. The streets within Township 9 are defined as follows:

Richards Boulevard - The main roadway for this part of Sacramento will maintain a similar cross section to that which exists currently from curb to curb except that median landscaping will be installed in place of the existing dual left turn lane. North of Richards, light rail tracks and a station will parallel the roadway, separated by a planting strip. To the north of the light rail, a 10' sidewalk will provide circulation and access to buildings.

North 7th Street - North 7th Street serves as the gateway to Township 9 from Downtown and terminates at Riverfront Park and the adjacent American River Parkway. The central feature of the North 7th Street cross section is a usable linear park within the median. This median starts one block north of Richards Boulevard (signalized intersection at Signature Street) and continues to Terminus Park (intersection at Riverfront Drive). One block south of the Riverfront Drive, the "grand boulevard" will be punctuated by the Parkway Tower, a 150' tall sculptural icon located within the traffic circle and on axis with the intersection of Vine Street and will begin gradually increasing in elevation to meet the grade of Riverfront Drive.

Riverfront Drive - Riverfront Drive at the north edge of project Township 9 is intended to connect to future developments east and west along the river. The elevation of the road will be the top of the American River levee providing a strong visual connection to the river. The meandering alignment will provide park space at the river's edge on top of the levee. Riverfront Drive features gateway intersections at North 7th and North 5th Streets and a secondary intersection at a local street.

North 5th Street - The existing North 5th Street at the western boundary of the Project connects Richards Boulevard to the south. The terminus of North 5th Street will be reconstructed to slope up to and connect to Riverfront Drive. Frontage improvements will also be installed along the Project boundary. North 5th Street features a Gateway Intersection at Signature Street.

Signature Street - This will be the main retail street of Township 9. It will be close to the light rail station and Richards Boulevard edge, making it highly accessible. Being entirely internal to the Project site, Signature Street is envisioned as a slower speed, pedestrian-friendly, narrow street more suitable for shopping and other urban activities. Signature Street will feature major amenities such as specialty paving throughout and a large traffic plaza with the center island serving as a public square. It is envisioned to connect to Richards Boulevard and the light rail station via two linear plazas.

Park Boulevard - Park Boulevard is the main diagonal street on the project, stretching from the Parkway Tower on the north to a traffic plaza adjacent to the urban park on the south. This street is set apart by the

fact that it has two distinct sections. For two blocks it contains a wide median and a traffic plaza whose center island can be accessed by pedestrians. For one block it becomes more of an urban plaza with specialty pavement that can be driven on or closed off for special occasions. This portion of Park Boulevard will slope up at no more than 5% to the north where it will meet the elevated grade of Riverfront Drive. Park Boulevard features three Secondary Intersections.

Local Streets - Local streets are largely residential in their land use and have no building setbacks from the back of sidewalk. Their design will follow the pattern established by the major streets. Local streets shall be built with enhanced intersections as required by these guidelines.

4.5 Intersections

Purpose & Intent: Celebrate key intersections and traffic circles as design opportunities that enrich the pedestrian experience and calm automobile traffic.

In order to reinforce an experiential transportation framework for both pedestrian and vehicular traffic in Township 9, special consideration should be afforded to design treatments at all intersections. Adding punctuation and accentuation to such crossroads will offer the Township 9 traffic environment an opportunity to celebrate and engage significant crossing nodes/points of entry, rather than simply pass through them. In addition, the design treatments of infrastructure elements in any development are ultimately decisive communicators of the overall development image and should be designed in order to communicate success and thoughtful design vision to residents and patrons alike.

Gateway Intersections

Gateway intersections in Township 9 are located at significant points of entry into the development. The "main gateway" intersection is 7th Street and Signature Street. The remaining gateway intersections are located at 7th Street and Riverfront Drive, and at 5th Street and Signature Street. These intersections should be viewed as first impressions and front doors of Township 9, and should be designed to reflect a strong, inviting project image. The gateway enhancements are:

- **Monumentation** –Visually significant landmark monuments will be incorporated into the gateway intersections. Monuments will be placed at highly visible locations and outside of standard visibility triangles and shall be related in style and form to the historic cannery buildings.
- **Special Vehicular Paving** – Paving enhancement of the roadway surface will occur at all gateway intersections within the area bounded by the crosswalks. Paving materials may consist of stamped and colored concrete or asphalt, concrete unit pavers, or a befitting combination of each.

Secondary Intersections, Traffic Circles, and Traffic Calming

In addition to the Gateway Intersections there are several roadway enhancements that contribute to the unique design of Township 9. These are defined as secondary intersections, traffic circles and traffic calming features.

- **Secondary Intersections** – Paving enhancement of the roadway surface will occur at secondary intersections within the area bounded by the crosswalks. Paving materials may consist of stamped and colored concrete or asphalt, concrete unit pavers, or a befitting combination of each.
- **Traffic Circles** – The traffic circles provide visual interest, improved traffic flow and a unique design feature

within the project. They are designed to have a landscape island in the center of the intersection where sculpture, fountains or enhanced landscape can be added. The perimeter will have enhanced paving within the roadway and pedestrian surfaces. The perimeter will have public open space areas that complement adjacent buildings and provide shade, seating and circulation.

Traffic Calming – There are several options for traffic calming that can be incorporated into Township 9. These can be used in combination with each other and in addition to the enhanced intersections discussed above. Final design must consider visibility triangles, ADA codes and other criteria prescribed by the City.

Crosswalks

Specialty paved crosswalks in Township 9 should be 10'-15' in width, and shall consist of brick pavers in a running bond (or similar decorative) pattern with a 1'-2' wide colored concrete band or soldier courses of pavers at the crosswalk edge. To promote universal accessibility for all residents, employees and patrons, barrier free ramps complying with ADA standards shall be installed at each intersection crossing.

Mid-Block Crossings

As a minimum, there shall be three mid-block crosswalks, with the same special paving as intersection crosswalks, along the paseos where they intersect local street D (see tentative map for location). Their purpose is to make pedestrian circulation along the paseos easy and efficient. The crosswalks shall be 10'-15' in width, and shall consist of brick pavers in a running bond (or similar decorative) pattern with a 1'-2' wide colored concrete band or soldier courses of pavers at the crosswalk edge. To promote universal accessibility for all residents, employees and patrons, barrier free ramps complying with ADA standards shall be installed at each intersection crossing. Mid-block pedestrian crosswalks will be required where retail, housing, and transportation locations are separated by busy streets

Bulb-Outs

Street corners within Township 9 (where feasible) shall have bulb-outs. The purpose of these is to reduce the length a pedestrian has to travel to cross a street and to calm traffic. The bulb-outs effectively move the curb location 7' (the width of a parking lane) closer to the center of the street. They do not encroach within the travel lanes (or bike lanes), leaving a 20'- 24' curb-to-curb width in most situations. There should be no bulb-outs into Park Boulevard in order to maintain a larger street width for emergency vehicle access. There should also be no bulb-outs at Richards Boulevard because it has no parking lanes.

Bicycle Circulation

Bicycles are accommodated within Township 9 on a combination of on-street Class II and III facilities and one off-street Class I facility. The Class I facility is located on the American River levee known as the Two Rivers Trail. The Class II facilities occur along Richards Boulevard, 5th and 7th Streets. The remaining streets within the PUD are Class III facilities and will be equipped with "share the road" signage.

4.6 Parking Design Criteria

Purpose & Intent: Provide design criteria that ensures that structured parking is located away from public streets or designed such that they do not appear to be parking structures at the ground level.

Structured Parking Design

In order to ensure the safety and security of all parking structures, the following guidelines shall apply.

- Parking structures with continuous floor transitions are preferable to ramp design structures (because they cause fewer pedestrian/vehicle conflicts).
- Structures should be engineered with as many open interior and exterior walls as practical.
- Staircases should be very open by design, or they should have glass/polycarbonate walls.
- Elevators should be designed with mirrors, video surveillance, and glass/polycarbonate back walls whenever possible.
- Larger parking structures include space on one or more floors for small retail operations that can bring legitimate activity to the area.
- Larger structures should also use private security.

Transit Area Parking

With the exception of on-street parking, parking areas in the Transit Area will be limited to structured or below-grade parking. Applicants are required to design these structures to be within or below buildings, in a manner that conceals them from public view and that does not interrupt the continuity of the pedestrian environment. Parking garages shall be exempt from height restrictions, provided they are not taller than the development they serve and are not visible from streets or any public open spaces.

Occupied space shall be developed between above ground parking garages and street level for a minimum of 80% of the garage wall facing a street. Should the necessity arise to expose one façade of the garage to a street front, the façade shall comply with all architectural guidelines and restrictions as defined in this document, including building articulation and accent features. In such cases, the incorporation of planters, decorative screens and/or trellis elements is strongly encouraged to bring variation and interest to the façade design.

To promote a secure, visually continuous streetscape environment, street level entryways and stairways should be located along the street edge, and should be easily distinguishable and well lit (with elements from the streetscape family of lighting elements). Upper level openings of garages should be designed as defined window openings rather than a continuous horizontal slit between structural columns.

Central Mixed Use Area Parking

Freestanding parking structures as well as surface parking lots will be permitted in the Central Mixed-Use Area. Parking structures should be positioned along key vehicular corridors to intercept traffic and reduce the potential for traffic congestion at peak traffic times. Parking structures should also be positioned to encourage shared use by retail shops / restaurants and public activities during non-peak hours. Sloping floor elevations designed to be viewable from, or adjacent to, public streets will not be allowed. As in the Transit Area, all parking structure openings should be designed as windows rather than continuous strips.

Surface parking lots are permitted in this area. When surface lots are adjacent to any primary street, parking should be screened from public view and where practical, in concert with crime prevention guidelines. All surface lots shall be developed with landscaping, lighting, and shade trees per City ordinance.

Live-Work / Townhouse Area Parking

Same as Transit Area, only that above ground parking garages may have one wall facing a local street with the same stipulation that the garage wall receives architectural treatment as described above. Surface

parking lots are permitted in this area subject to the same development requirements as the Central Mixed-Use Area above.

Riverfront Area Parking

Riverfront Area Parking is the same as shown in the Central Mixed Use Area above.

Reciprocal Parking

Reciprocal parking is encouraged on a project basis where adjacent buildings and/or parcels have compatible shared use opportunities. General guidelines for reciprocal parking are provided in appendix 5.6.

Parking and Delivery Screening

Functional service areas in Township 9, while necessary, are not intended to be viewable from primary streets, neighboring properties, or any public space. Materials and elements used in screening shall be complementary to the architecture and streetscape design of the development zones in which they fall. The following guidelines will function to conceal objectionable areas / activities from public view and are intended for application in all development zones of Township 9.

- **Delivery, Loading and Other Service Areas** - Loading and service areas must be screened by masonry walls at least 6' in height, or screened by a solid evergreen planting reaching at least 6' in height at maturity. Plantings shall be a minimum of 36" in height at time of planting and must reach mature height within three years of installation.
- **Garbage Collection** – Applicants should make efforts to design garbage collection areas to be enclosed in a building envelope. These collection areas should be screened by a solid metal gate and should not face any primary street or civic space. In the Transit Area, this configuration will be required for design approval by the Design Review Committee.

In alternative cases, garbage collection areas shall be enclosed by a masonry wall 6' high, or 1' higher than the container it screens – whichever is greater, and will have a solid, metal lockable gate to screen the opening. A double row of evergreen shrubs (5-gallon, 18" height minimum size at installation) shall be planted in a triangular spacing pattern, 36" o.c., at the base of the screen wall.

Masonry walls should incorporate murals and/or other types of graffiti reducing public art. Screen plantings/shrubs should be a hostile variety to reduce habitation by transients.

4.7 Landscaping

Purpose & Intent: Provide shade and aesthetic value through thoughtful selections of trees, shrubs and ground cover plantings. Ensure the long term viability of an urban street tree canopy.

In all zones of Township 9, applicants shall incorporate suitable landscape design that will enhance the architectural character of the building it serves, and create a cozy, urban atmosphere that promotes a comfortable pedestrian experience. Spaces and plantings shall be designed at a human-scale and be harmonious to the rest of the development in form and expression. To enhance public safety, it is imperative that the landscaping plan is coordinated with the lighting plan to ensure proper illumination is maintained through the maturity of the trees and shrubs.

- **Street Trees** – Installation of a consistent network of street trees along Township 9 travel corridors will

establish shade, beauty, and a human scale element to the development as a whole. Trees are to be planted at approximately 30' o.c. which allows for mature tree growth and room for lights and other street elements to be located on street edges. As previously noted, this tree spacing (which works in tandem with street light spacing) should be constant and not violated by sidewalks, utilities, or other ground plane objects. Even driveways are encouraged to be located or reconfigured so as to not change the street tree and light pattern.

Street trees planted along the length of any street, in medians as well as on the sides, must be of the same genus and species to promote an ordered visual structure to the street (as specified by the Street Trees Map, Figure 25). To avoid monoculture, no one species should be used on more than two streets. Street trees shall be installed at a minimum container size of 24" boxes. Regardless, all trees shall be nursery container-grown and planted in the center of the 8' (minimum) planter strip. Planter strip should generally be planted with lawn, although minor variations may be allowable.

To denote a special condition, street trees adjacent to plazas, dining patios or other similar conditions may be planted in tree wells or tree grates, especially to maintain the continuous 30' tree spacing pattern. Each tree grate or well should measure 5' x 5' minimum.

- **Site Landscaping** - Property owners shall provide appropriate landscaping for all structures erected in Township 9. Landscape designs shall reinforce the urban street front character and should introduce an interesting human scale element to the base of buildings. The use of window boxes, planting pockets, and the like, are encouraged.

All landscapes shall include an automatic, underground irrigation system. Developers shall attempt to locate controllers, valve boxes, and other irrigation/electrical equipment requiring access out of public view from walks, drives, and buildings, as best as physically and practically possible.

- **Plant Material Guide** - Planting design should always be incorporated into thoughtful landscape planning by a qualified landscape designer. The massing and composition of proposed planting areas should enhance the overall style of the architecture and should not detract or become obtrusive.

Plant material should be chosen to create visual interest by incorporating a well-balanced variety of forms, textures, and colors. The use of seasonal color plantings is encouraged, especially at building entries, civic/public spaces, and other special points of interest.

4.8 Open Space / Parks

Purpose & Intent: Provide a variety of open space & park amenities that offer a variety of passive and active urban experiences. Provide public access to the American River Waterfront while protecting the natural environments of the American River Parkway.

Individual project applicants and designers shall give special attention to appropriate design regarding scale, safety, maintainability, and visual/physical connectivity when designing parks and open space in Township 9. Each applicant should design with the principle that successful open space locations and linkages are critical in creating a vibrant, enjoyable community. To ensure Township 9 residents and patrons are afforded convenient access to a variety of high-quality open spaces, a full spectrum of parks is provided in Township 9.

The American River Waterfront

The American River Waterfront within the project site is envisioned as a contiguous open space feature

along the South side of the American River and the most prominent open space feature in Township 9. Located on the north boundary of Township 9, along the levee that defines the American River, it will provide an important regional amenity for the entire Sacramento community by bringing the city to the river and celebrating it. The American River Waterfront will ultimately be part of a larger linear open space/parkway system that extends along the south side of the American River levee throughout the entire Richards Boulevard area.

A critical design element of the waterfront (and the urban interface along the south side of Riverfront Drive) is raising the Township 9 grade (elevation) up to the same elevation as the top of the existing levee. This provides a direct visual and physical connection with the river rarely found in Sacramento. The grade will vary along the waterfront based upon the Riverfront Drive centerline profile. The new earthen fill will be transitioned back to existing grade through subtle slopes (2-4%) and in some locations with retaining walls incorporated into the foundation of adjacent buildings.

Other important objectives of the waterfront design are to enhance and compliment the native vegetation of the American River Parkway, screen views of buildings from the river and maintain flood protection.

The American River Waterfront is defined by three distinct landscape areas within Township 9 that ensure a balance between the urban uses, recreation uses and open space and preservation uses.

American River Parkway

The natural landscape area located within the American River Parkway (north of the levee) will be preserved and enhanced as an open space and riparian preserve. This area is subject to flooding and is monitored by numerous agencies to ensure that the vegetation supports the flood protection, water quality and habitat objectives for the corridor. Township 9 respects this landscape by restricting access to it, planting non-invasive species adjacent to it, controlling irrigation, fertilizer and surface run-off into it and collaborating with the operating agencies to clear invasive species, protect suitable species and re-vegetate where appropriate to ensure that this landscape area remains healthy.

Riverfront Park

Riverfront Park is the linear park located between the American River Parkway (open space and riparian preserve) and Riverfront Drive. The park varies in width due to the meandering alignment of Riverfront Drive creating a variety of passive recreational opportunities. The primary recreational facility is the Two Rivers Trail which passes through the Township 9 site and connects into a regional multi-use trail system along the existing levee adjacent to the American River Parkway. Other passive recreational features may include a promenade, picnic areas, and open lawn areas.

Riverfront Park will primarily be landscaped as an Oak Woodland with native Oak species and grasses. The landscape will include native Oak tree species throughout, but the ground plane planting (ground cover) will transition from native grasses to ornamental grasses (turf) along a clear edge such as a walkway or the Two Rivers Trail. (The approved Riverfront Park plant list is included in the appendix). This transition of grasses provides a balance between the natural and urban landscapes and offers an opportunity for interpretive and / or educational elements within the Riverfront Park. The south edge of the park is defined by Riverfront Drive where California Sycamores are planted as street trees to complement the Sacramento riparian landscape.

Terminus Park / Riverfront Plaza

Terminus Park and Riverfront Plaza are located at the terminus of North 7th Street at Riverfront Drive. They are located within the Riverfront Park and are the jewel of the Township 9 waterfront. Terminus Park is a large turf area with informally planted shade trees that creates an active gathering place for outdoor festivals, fairs and passive recreation. Riverfront Plaza provides Sacramento with a formal gathering place along the American River where public events such as live performances and special ceremonies can celebrate the beauty of the waterfront and the excitement of the mixed use community in Township 9.

The plaza is designed hardscape, lawn, and a participatory fountain and related park amenities. Within the plaza there could be an interpretive feature that highlights the significance of the American River within the hydrological, ecological and geographical systems of California.

A linear water feature starts at Riverfront Plaza and flows downhill through the 7th Street median. Along with the design of the plaza, the water feature shall convey the meaning and importance of the hydrological link between the Sierra Nevada Mountains and the Sacramento metropolitan area via the American River. A tower feature within a small pool in the traffic circle island at the intersection of North Seventh and new street A makes up the end of the water feature. Within this pool rises a tower feature (maximum 150') that serves as a community identity maker. ~~The tower is envisioned to be lit with neon (or similar) lights that can only be seen from the south.~~

Riverfront plaza is designed to minimize light and noise pollution into the adjacent American River Parkway. Programming will have limited night time hours to avoid conflicts with adjacent human and wildlife populations.

7th Street Linear Parkway – 7th Street is envisioned as a **linear park** with 2 lanes of traffic ~~on either side moving through it.~~ These lanes are divided by a 40' wide median which contains an interpretive walkway. The sides of the street contain wide planting areas and sidewalks and a 10' building setback which makes room for outdoor dining, plazas and more planting space. The width and length of the street are unified by a strong boulevard tree planting pattern which is interrupted at intersections by focal nodes with accent trees.

The interpretive walkway provides an opportunity to describe Sacramento's ecology, history and/or culture. The paving design of the walkway could be derived from the American River, abstracting the form of the river as it begins in the high mountains and descends through different ecological and urban environs on the way to the Pacific Ocean. The pavement should consist of specialty materials, for example colored concrete with decorative stone exposed aggregate or special art pieces embedded into the surface of the walk. The design of the walkway could portray uses of the river: gold mining, irrigation, transportation, recreation, etc. These literal ideas and facts should thoughtfully be expressed into the designed landscape in the form of sculptural pieces, special plantings, or the shape, color & texture of the pavement of the walkway. Interpretive signs that explain the ideas being portrayed in the design should occur at regular intervals.

~~In addition to the linear parkway interpretive walk there are two nodes along the walk that create opportunity for historical interpretation. These are the shelters located within the median (at the intersections of 7th with local streets) that provide an opportunity for a mini-museum for interpreting the cannery operation (or other cultural or historical items) and a rest area for pedestrians. One of these shelters should be the historic scale house which exists on-site currently. Its size and the fact that it served a function for the historical use of the property make the building a good choice for an interpretive building. The scale house will need to be relocated into the new location within the median as well as structurally modified to~~

~~serve its new use. Changes should include making the structure somewhat open to the outside in order to keep it safe, while maintaining the spirit, intent and most of the material of the original building.~~

~~The other structure could likewise interpret or reuse the building materials from the site, including brick for the columns, wood trusses and galvanized metal roofing. Historically accurate lights and other accessories could also be used. Shelters could house remnants of the historic cannery machinery, historical artifacts, ecological exhibits and other appropriate displays along with interpretive signage and benches.~~

Other signage and entry features should be located along the sides of 7th Street, especially near the south end to mark the arrival from Richards Blvd into Township 9. For the construction of all of these features, here again, the materials used should be taken from the demolished cannery and warehouses that are on site, especially bricks and hollow clay units.

Paseos - ~~Three-Six linear, mid-block~~ landscaped ~~greenways-greenspaces~~ shall be developed ~~to transverse in~~ the live-work/townhouse area in Township 9. These ~~greenbelts-greenspaces~~ shall be a minimum of 40' wide with an 8' wide concrete walkway and should be designed to complement development occurring on its edges. The paseos should link the central mixed-use area on their north end to Signature Street on the south. The eastern and western paseos may pass through breezeways under larger buildings to retain their linear continuity. Breezeways shall be designed to enhance safety. The paseo design should reflect the surrounding development and the paseos may be relocated to another location within the same block if first agreed to by Parks Department staff. ~~However,~~ The central paseo should be designed to be more important since it connects to the urban park and the middle of Signature Street.

Mews – This is a linear plaza that is intended to achieve the effect of a narrow street. It is situated as the logical extension of Park Boulevard and should be designed to accommodate traffic, although only an occasional vehicle (emergency, ~~delivery,~~ etc) would be permitted. It will slope gently up to the level of Riverfront Drive and contain two rows of ~~palm~~-trees which continue south into the Park Boulevard median. The ~~palm~~-trees also serve to accentuate the view from Riverfront Plaza to the traffic circle at the end of Park Boulevard.

Urban Parks - To serve the needs of residents/employees of Township 9 and to reduce the effects of compact development in the higher-density areas of development, incorporation of additional open space will be critical in the creation of an enjoyable living and working environment. With that in mind, an urban park located in the center of the project provides relief from urban conditions and serves as a space for recreational, civic, or commercial activities. This space is intended to provide a sense of definition to sub-areas within Township 9, and to provide linkages between activity centers.

Urban Plazas- There are two public plazas conceived for Township 9. Two of them are located on the southernmost edge of the site, connecting the Richards Boulevard frontage to Signature Street.

Each plaza is somewhat linear in shape and is partially intended to convey pedestrians - transitional spaces, rather than gathering spaces. They should be wide enough to give ample room for pedestrians and seating and planting areas. Specialty pavement colors and patterns are encouraged.

Traffic Circles –The traffic circles within Township 9 are considered key design features that create a unique identity for the project. These circles are intended to be pedestrian oriented spaces that emphasize the

outdoor seating, walkways, and adjacent ground floor uses to create exciting gathering places. The roadway will be designed with enhanced paving, bollards and landscaping to minimize the visual and physical presence of automobiles. The traffic circle at Signature Street is shown here as an example of these design features. The other circles will be designed with a similar approach.

4.9 Lighting

Purpose & Intent: Provide safe and interesting streets, plazas and parks through decorative lighting solutions. Shield glare to adjacent properties especially the American River Parkway. Establish solutions that meet the principles of "dark sky" lighting.

Lighting in Township 9 shall be used as a tool to: 1) illuminate roadways, pedestrian spaces, and architecture while enhancing their safety component as well as aesthetic qualities and 2) serve as repetitive elements that reinforce the image of Township 9. Lighting elements shall blend attractively into the environment by day and perform effectively at night to promote a comfortable, visually continuous condition. Lighting shall be directed and controlled so not to disturb residences, and to respect "dark sky" principles of lighting design. All light fixtures shall have incandescent, halogen, or metal halide light sources.

General Guidelines

- Lighting should minimally meet the Illuminating Engineering Society of North America (IESNA) standards. In troubled areas, consideration should be given to doubling or tripling the foot candle output. Lighting must also be uniform and efforts made to avoid glare and light trespass.
- Fixtures should be vandal resistant. Full cut off wall packs and shoebox fixtures are recommended for parking lots, walkways and around buildings. These help eliminate glare and light trespass.
- For exterior commercial lighting (e.g., parking lots, paths, parks, plazas, etc.), either Metal Halide or Induction lighting is recommended. This lighting provides a clear white light that allows for true color rendition and the ability to better recognize potential threats. For some applications such as apartment doorways, a compact fluorescent white bulb can be used.
- In addition to lighting levels being uniform (should have a uniformity average to minimum ratio of 4:1), the lighting plan should be made part of the landscaping plan. The lighting plan should be capable of meeting the lighting standards from the time of planting up through the time of landscaping maturity.
- The lighting plan should address issues such as shadows that will be created by awnings and/or canopies that are planned to shade business windows. Lighting solutions under these structures should be implemented and utilize the same types of lighting listed above.

Street Lighting

Along major roadways in Township 9, roadway scale illumination located within medians will be required at intervals designed to promote safety, visual continuity, and community identity. The general height for light poles is to be 16'-24' for street lights and they should be located in the center of the planting strip. These parameters in combination with the spacing intervals and target foot-candle intensities listed below are intended to provide adequate lighting for automobile and pedestrian as well as an aesthetically pleasing pedestrian scale. Lights may contain Township 9 logos suspended from a cross arm at special locations.

Pedestrian Lighting

On most streets, it is strongly encouraged that applicants include pedestrian-scale lighting in the design of

all streetscapes and public spaces in Township 9. Pedestrian scale illumination promotes visual continuity, safety, and night activity in any community. This may consist of lighting that is suspended from adjacent buildings. In situations away from buildings (paseos, parks, plazas, etc.) 12' tall pedestrian lights (otherwise being of the same design as a 16' street light) are required along walks or other high traffic corridors. Bollard lighting is also encouraged as a way to provide lower level lighting.

Architectural Accent Lighting

Exterior illumination, designed to highlight and accent architectural features of buildings, will be required on all building facades facing streets, sidewalks, parking areas, and other public spaces in Township 9. This may include pedestrian-scale lamps, bollards, landscape lighting, and/or step lighting that is complementary in design to the family of light elements described by these guidelines.

In general, lighting plans shall be designed to appropriately accent architecture / landscape elements, and shall not include ostentatious shows of light such as wall washes or multi-colored displays. By exception, festive light displays in retail or entertainment areas will be reviewed on an individual basis for approval by the Development Review Committee.

Sign Lighting – In all zones of development, lighting of signs shall be from a relatively concealed light source that is not intrusive to vehicular traffic, pedestrians, or neighboring properties. Encouraged sign lighting treatments are as follows:

- Snorkel lighting
- Hooded spotlights
- Lighting recessed at the base or side of the sign
- Lights concealed within relief lettering, illuminating the background
- Appropriately sized and directed floodlights

Internally lit, plastic box type signs, in which the light source is not visible, are not allowable in any development zone in Township 9. Sign lights that flash on and off intermittently are also not allowable. All sign lighting design will be subject to review and approval by the Development Review Committee prior to installation.

Because of the narrow width of the streets, these smaller scale lights will also provide lighting for the roadway surface. The light should be located within the center of the street planter strip, able to throw light on both the sidewalk area and street surface. They are usually placed exactly midway between tree locations. Certain streets will receive a special non-City standard light fixture. See Table 6 for pedestrian lighting intervals, height, and style.

4.10 Signage

Purpose & Intent: Encourage unique solutions for signage while meeting the existing City codes.

Signage serves a dual purpose within any urban space – it not only orients and directs users, but with thoughtful design, it can enhance the aesthetic environment of a streetscape while adding to its identity and sense of place.

Lack of way-finding can lead to fear, confusion and possibly exposure to crime. It is important to incorporate clear way-finding measures in parks and public plaza areas because it increases public safety.

Following are general guidelines regarding different types of signage design that may be appropriate in all development areas of Township 9. All signage design shall be subject to review and approval by the Design Review Committee prior to installation.

Regulatory Signage – All regulatory signage design shall include painted back surfaces, and sign trim of color that is coordinated to other streetscape elements. Where possible, regulatory signs shall be consolidated on light standard poles.

Projecting Signage – In pedestrian-oriented communities such as Township 9, projecting signs have an advantage over traditional fascia signs in that they can be viewed by pedestrians at right angles to the building façade, as well as from a distance. In general, projecting signage features will be encouraged, provided they comply with the following guidelines. Projecting signage, in all zones of development in Township 9, should:

- Identify the name and business of the occupant
- Not be of unusual size or shape when compared to human scale or adjacent buildings (maximum dimensions equal 24"-36" in width by 18"-24" in height)
- Be oriented at right angles to primary pedestrian movement
- Not project above the roof line of a building
- Not block or detract from adjacent property
- Preserve the architectural quality and atmosphere of Township 9

Awning Signage – Awnings over building entrances add color, visual interest, and environmental protection to the streetscape environment, and are encouraged as design accents in Township 9. Awning signs are typically non-illuminated displays that are painted on or permanently attached to an awning. These treatments are encouraged for signage design in Township 9, provided they are:

- Permanently attached designs
- Not of unusual size so as to detract from the visual continuity of the streetscape
- Appropriate in scale when compared to the building/business which they serve
- Simple in text / logo design so easily read and not detracting from the overall streetscape

Ground Signage – Ground signage is an acceptable alternative to pole-mounted or billboard-type signage that often defaces the community image of neighborhoods. Use of ground signage is allowable only in the Transit Area of Township 9, and should be:

- Of complimentary architectural design and material when compared to the building it serves
- Integrated into the landscaping
- Simple in text design so to be easily read at typical driving speeds
- Creatively lit from a concealed light source that is not intrusive to vehicular traffic, pedestrians, or neighboring properties

Window Signage – Window signs may be etched or painted onto the glass surface (as permitted by owners), and must maintain clear visibility through the window or display case. Symbols, logos, or decorative elements comprising a window sign should be subtle in size / design, and should cover no more than approximately 15% - 30% of display window surface.

Fascia Signage – Fascia signs, signboards, or wall-mounted signs are allowable in Township 9 provided they

are designed to accentuate the vertical architectural elements of the building they serve, and that the sign size, shape, or scale does not detract from the overall character of the business.

Rooftop Signage – The historic “Richards Bercutt Cannery Company” had a rooftop sign that was unique to this site and since been removed. The proposed transit station shelter provides an opportunity to re-interpret this historical form of signage. Rooftop signage will be permitted on this rooftop subject to review by the City zoning administrator.

4.11 *Historic Preservation*

Purpose & Intent: Respect the unique history of the cannery buildings through selective re-interpretation and re-use of materials and forms in both architectural features and site features where practical.

The construction of Township 9 will remove a historically significant cannery and industrial site. The historical report states, “It is important that the property’s historic significance be conveyed to future residents and visitors to the Township 9 property.” To achieve this and to satisfy mitigation measures required by the EIR, portions of the existing buildings could be used to replicate the cannery building architectural vernacular in shelters for transit users and visitors to the site. Additionally, project entry signage that is to occur along seventh and in other site entry points could be constructed of historic materials from the site. The warehouse identified as the Fruit Cocktail Building (building #3) in the Historical Report is the best inspiration for historical reuse and interpretation. The brick columns, wood trusses and galvanized metal roofing from this building could all be closely (if not in fact literally) recreated at the interpretive shelters. One of the walls of the shelter could even contain a metal framed window from the original warehouse. Lights and other accessories should also be reused if possible, but historically accurate fixtures may be substituted if necessary.

Transit Station Shelter – The Sacramento Rapid Transit (RT) station located along Richards Boulevard is the most promising location to preserve the history of the cannery facility. This location would establish this historical identity for thousands of RT users as they pass through the station. This location creates a distinct sense of place for the entire Township 9 community while interpreting history for residents and visitors to Sacramento.

The shelter uses traditional warehouse construction of clay brick, scissor trusses and factory glass (windows) to create a functional enclosure for the transit platform that recalls the cannery vernacular of this historic site.

Interpretation Shelters - Two shelters are planned within the 7th Street linear park that provide an opportunity for a “mini-museum” for interpreting the cannery operation (or other cultural or historical items) and a rest area for pedestrians. Beneath the shelter, a remnant of the historic cannery machinery could be displayed with other valuable historical artifacts or resources, along with cultural or ecological exhibits. Interpretive signage can be used to explain the interpretive display of the site’s history and the exhibits displayed underneath the shelter. Other amenities that should be located under the shelter are benches, drinking fountains and trash receptacles.

Entry Monuments - The entry monuments planned for Township 9 is an excellent opportunity to reuse materials from the site. Obelisks, pedestrian arches, seat walls, and other structures that were not part of the historical shelters could be used to mark the arrival into the project and provide a sense of place. Materials used for these structures would be primarily salvaged brick. Other materials could be hollow clay tiles

(units) and possibly galvanized metal roofing. Interpretation of these materials could be done through small signs affixed directly to the monuments or signs. There could be direct access to the entry monuments via the interpretive walkway and sidewalks.

4.12 *Public Utilities*

Purpose & Intent: Encourage careful placement of utility appurtenances to minimize visual impact within the streetscape and other public spaces.

Above grade utilities, including telephone boxes, air conditioning units, meters, irrigation controllers, and the like, are to be placed away from public view or views from neighboring properties where possible. Below ground vaults are encouraged within the public ROW where practical and should be located away from building entrances and public features such as paseos, mews, traffic circles, etc. Architectural niches or offsets should be designed to accommodate mechanical equipment. Green screens are allowable provided they accomplish solid screening at initial installation, not at assumed plant maturity.

4.13 *Stormwater Quality Treatment Control*

Purpose and intent: To comply with the City's Municipal NPDES Permit that require both source control and treatment control measures to minimize the increase of urban runoff pollution caused by the project.

Off-site treatment control measures: The streets planned for the Township 9 shall incorporate both runoff reduction measures and treatment control measures. All streets in the project shall have separated sidewalk and continuous planting of trees. Local streets shall have stormwater planters alongside of them when necessary. The control measures shall include stormwater planters, porous pavement, vegetated swales or other appropriate treatment control measures. Any selected measures shall meet the design criteria of the City's current stormwater quality design manual in effect at the time of the development. Attached schematic drawings show the conceptual stormwater quality treatment measures for the project, but the measures have not been verified meeting the criteria or suitable for intended use.

On-site treatment control measures: Some of the off-site treatment measures can be designed to include treatment for the abutting parcels. Most parcels will be able to be fully treated by the off-site treatment measures. Parcels 1, 3, 4, 5, 12, 14, 15 and 16 have been identified as those parcels not meeting the one hundred percent treatment in the preliminary Water Quality Plan, dated June 2007. Additional parcels may be added to this list if field conditions limit the implementation of some of the proposals. Those parcels that do not meet the one hundred percent treatment shall provide additional on-site treatment controls. The exact measures can not be predetermined; however, the measures shall include porous pavement, stormwater planters, vegetative swale, proprietary treatment devices, sand filters and eco roof, as appropriate. Any selected measures shall meet the design criteria of the City's current stormwater quality design manual in effect at the time of the development. Attached drawing shows the conceptual on-site stormwater quality treatment measures. The measures have not been verified if they have met the design criteria or if they are suitable for intended use.

Section 5: Appendices

5.1 *Approved Tree List*

5.2 Recommended Plant List (Non Trees)

5.3 Riverfront Park Plant List

5.4 Township 9 Street Furniture Guidelines

Note that the following furniture examples are suggestions based on preliminary design concepts in this document. Changes may be made as the design is further developed.

5.5 Sustainability Guidelines

Purpose and Intent: Sustainable developments: promote livable developments, reduce Sacramento's environmental footprint, and sustain economic viability for Sacramento businesses and general population.

The following guideline is a summarization of elements that encompass sustainable communities. These are not all encompassing and general and nature.

Sustainable Sites

- Plan building lot orientation to take into account the path of the sun and design for passive solar strategies (solar heating and cooling). Allow for the installation of photovoltaic installations that convert sunlight to electricity.
- Consider natural breezes and utilize thermal mass in the building interior for cooling effect in the summer.
- Plant trees that sequester carbon, shade and cool the environment and reduce the urban heat island effect (thermal gradient differences between developed and undeveloped areas). Do not block solar access to the rooftops of designated solar projects.
- Consider cool roofs and/or green roofs to reduce urban heat island effect.
- Reduce pollution and land development impacts from single occupancy vehicle use by sizing parking capacity appropriately and providing preferred parking for carpools, van pools, car-share services, bicycles, etc.
- Develop infrastructure and provide for low emission, fuel efficient and alternative fuel vehicles, (i.e.: electric, hybrids, fuel cell).

Water Efficiency

- Plan site for natural drainage, increase on-site infiltration and manage stormwater runoff. Provide pervious (vs. impervious) landscaped and parking surfaces and provide on-site planting, bioswales, constructed wetlands and vegetated filters to allow water to return naturally to the aquifer and pretreat it before it enters the storm drainage system. Utilize drought resistant and water efficient planting and irrigation efficiency.
- Reduce generation of wastewater and potable water demand. Capture and re-use rainwater, provide water-conserving fixtures and consider the use of "greywater" for landscaping and other uses.

- Maximize water efficiency within buildings (water closets, urinals, lavatory faucets, showers, kitchen sinks) by utilizing high efficiency fixtures and consume less of our community's water.

Energy & Atmosphere

- Verify that building's energy related systems are installed, calibrated and perform according to project requirements.
- Optimize and increase energy performance above standards to reduce the environmental and economic impacts associated with excessive energy use.
- Minimize condenser and refrigerant driven mechanical systems and utilize clean air renewable energy sources on-site which reduce carbon emissions and maximize energy efficiency (i.e.: photovoltaics, indirect-direct evaporative cooling, wind power, co-generation, District Heating & Cooling, etc.).
- Include mechanical systems that utilize fresh air intake and the best ventilation and filtration technology.
- Support building envelope design that appropriately insulates homes and address their orientation to the sun with the use of sun shades, light shelves, high performance glass, roof slopes, cool roofs, green roofs and solar photovoltaics. Integrate photovoltaics into the building envelope.
- Encourage the use of Green Energy programs as provided by local utilities or third party providers.
- Address daylighting strategies to improve the indoor environmental quality and productivity of building occupants.

Materials & Resources

- Reuse and recycle building materials and products in order to reduce demand for virgin materials. Use salvaged, refurbished or reused materials in construction. Use building products that incorporate recycled content materials. When possible, use rapidly renewable building materials and products (made from plants that are typically harvested within ten-year cycle or shorter) in order to reduce the use and depletion of finite raw materials.
- Utilize regional building materials and products whenever possible, thereby supporting the local economy and reducing the environmental impacts resulting from transportation. Utilize low emitting building materials and minimize or avoid the use of materials and interior finishes that utilize urea-formaldehyde and other volatile organic compounds in their production, and are irritating and/or harmful to the comfort and well-being of installers and occupants (adhesives, caulking, paints, finishes, sealers, carpet systems)

Other Innovative Design Strategies

- Utilize LEED (Leadership in Energy & Environmental Design) and/or Build It Green rating systems for your project.

5.6 Reciprocal Parking Guidelines

(to be inserted)

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