



REPORT TO PLANNING COMMISSION City of Sacramento

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915 I Street, Sacramento, CA 95814-2671

PUBLIC HEARING
December 9, 2010

To: Members of the Planning Commission

Subject: Bruceville Road Digital Billboard Variance (P10-074)

A request to approve a variance to allow the electronic billboard to encroach into the public right-of-way.

A. Environmental Determination: Mitigated Negative Declaration;

B. Mitigation Monitoring Plan; and

C. Variance to allow an electronic billboard to encroach approximately 11.5 feet into the public right-of-way adjacent to the 1.13 acre site in the Hospital-Review (H-R) zone.

Location/Council District: West side of HWY 99, S. of Mack Road across from Kaiser Hospital/ District 8

Assessor's Parcel Number: 117-0170-067

Recommendation: Staff recommends the Planning Commission approve the Variance based on the findings of fact. The Planning Commission has final approval authority over items A-C above, and its decision is appealable to City Council.

Contact: Sandra Yope, Senior Planner, 916-808-7158 and Joy Patterson, Principal Planner, 916-808-5607

Applicant: Michael Wagener for Clear Channel Outdoor; 1107 9th St, Ste. 500; Sacramento, CA 95814, 916-492-1309

Owner: City of Sacramento

Summary: The applicant is requesting approval of a variance for a previously approved and erected digital billboard that encroaches into the public right-of-way. As of the time of writing the report there were no comments or objections from any neighborhood group or adjacent property owners. **The project is not controversial.**

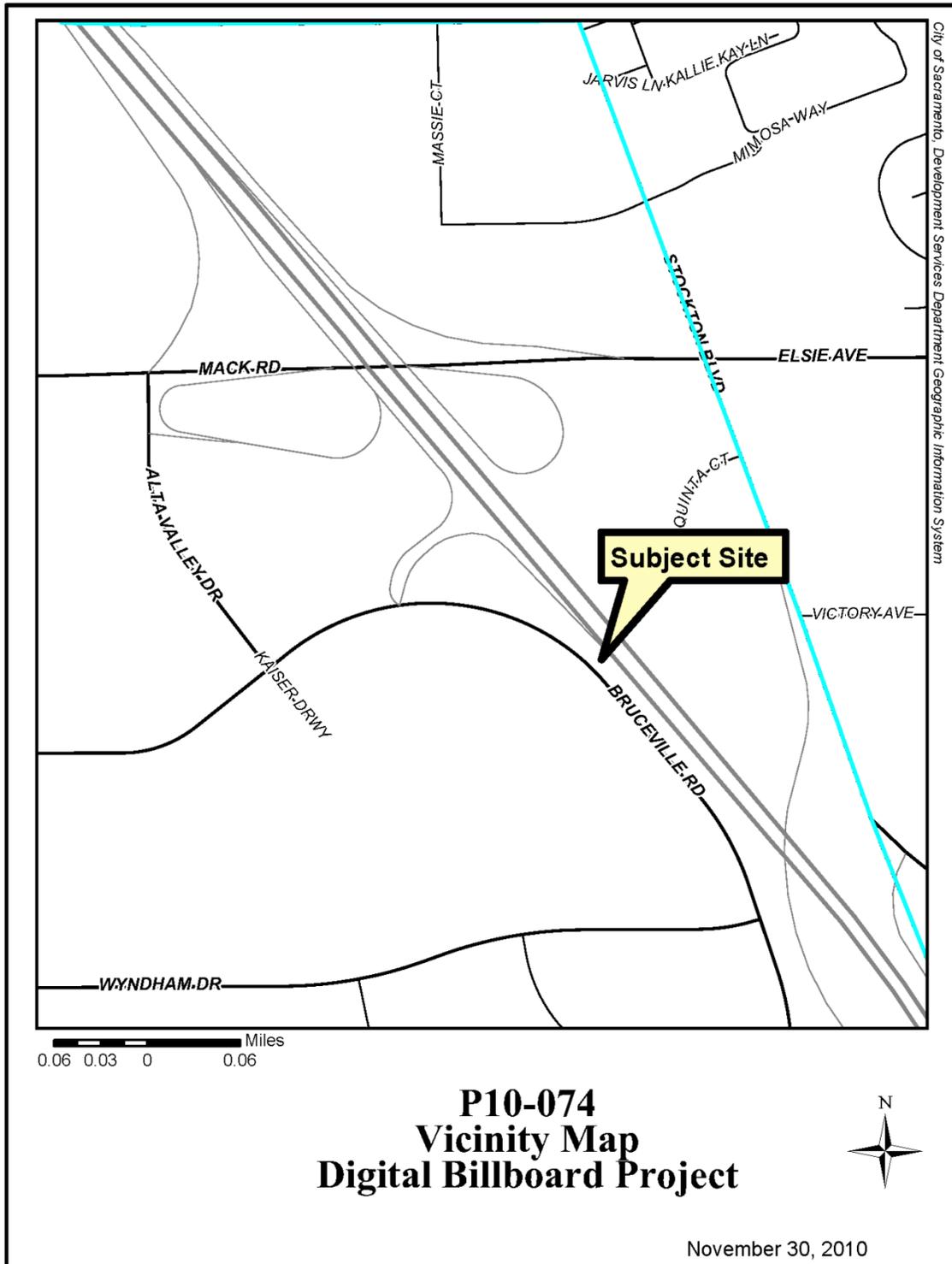


Table 1: Project Information
2030 General Plan designation: Suburban Corridor
Existing zoning of site: Hospital-Review (H-R)
Existing use of site: Digital Billboard
Property area: 1.13 acres

Background Information

On May 11, 2010, the Sacramento City Council approved the construction and operation of a new electronic (digital) billboard on City property through a billboard relocation agreement (P09-055) and a lease with the City that identified design and site requirements.

Environmental Considerations

A Mitigated Negative Declaration was prepared for the original project (M09-035 and P09-055). The City Council adopted the MND, and the Mitigation Monitoring Program, on May 11, 2010.

The current project would approve a re-location of the digital billboard located on the west side of Highway 99 to a location approximately eleven (11) feet south of the previously-identified site.

The relocation of the digital billboard would not substantially affect the environmental review conducted in the MND. The aesthetic and lighting issues discussed in the MND, as well as issues regarding traffic safety, would not be affected by the minor change in location. Nor would the relocation alter the analysis in the MND regarding effects on neighboring properties.

No change in circumstances has occurred since the original project approval that would result in a finding that the project would have any additional significant effects not identified and discussed in the MND, any increase in severity of impacts, or that mitigation measures identified in the MND would not be effective. No new mitigation measures that could reduce impacts are present.

The appropriate action is to re-adopt the MND prepared for the original project, and re-adopt the Mitigation Monitoring Program based on the MND. The applicant has signed and submitted the Mitigation Agreement and is bound by the provisions of mitigation identified in the MND.

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Project Setting

The site is a vacant 1.13 acre commercially zoned property owned by the City adjacent to Highway 99. The site is currently zoned Hospital-Review (H-R). The site is adjacent to the Highway 99 off ramp to the north, Highway 99 to the east, Kaiser Permanente Hospital to the south, and a commercial use to the west. The site belongs to the Department of Utilities.

Billboard Location, Design, Height

The applicant has constructed a new billboard on the southern most part of the site due to soil and easement issues. The new double face electronic billboard is 14 feet by 48 feet on each side. Each side has 672 square feet of sign area for a total of 1344 square feet of sign area. The billboard has a "V" design for the sign faces as in they are not parallel with each other. The billboard is 45 feet high with a five foot cap and supported by a single pole. The construction of the sign on the site resulted in the actual sign projecting 11.5 feet into the Bruceville Road street right-of-way. The bottom of the sign is 25 feet high. The Sign Code (Title 15.148) does not permit a sign to encroach into the public right-of-way. There is a provision to vary the code through the Planning Commission. The applicant is requesting variance for the encroachment. The Departments of Transportation and Utilities have already approved the encroachment.

Policy Considerations

The subject site is designated Suburban Corridor on the 2030 General Plan Land Use and Urban Form Diagram. The proposed sign location is consistent with the land use designation.

Conclusion

Staff recommends approval of the project since the City has approved a sign at this site and due to site constraints this is the only location on the site that the sign can be located. Additionally, the other City Departments have already approved the location and the sign will not impede vehicular traffic on Bruceville Road.

Respectfully submitted by: Sandra L. Yope
SANDRA YOPE
Senior Planner

Approved by:

Recommendation Approved:

Joy Patterson
JOY PATTERSON
Principal Planner

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**Proposed Record of Decision
Digital Billboard Variance (P10-074)**

Findings of Fact

A&B. Environmental Determination: Mitigated Negative Declaration and Mitigation Monitoring Program:

1. The Planning Commission of the City of Sacramento finds as follows:

a. On May 11, 2010, pursuant to the California Environmental Quality Act (Public Resources Code §21000 *et seq.* ("CEQA"), the CEQA Guidelines (14 California Code of Regulations §15000 *et seq.*), and the City of Sacramento environmental guidelines, the City Council adopted a mitigated negative declaration (MND) and a mitigation monitoring program and approved the Digital Billboard Project and Sign Code Amendment (M09-035 and P09-055) (Project).

b. The request for variance Modification [P# 10-074] (Project Modification) proposes to modify the previously approved Project as follows: the digital billboard location identified in the MND as Location 4, on the west side of southbound Highway 99, seeks to locate the billboard approximately eleven (11) feet to the south of the previously-identified location.

c. Staff has reviewed the Project Modification and has determined that the proposed changes to the original Project do not require the preparation of a subsequent environmental impact report or negative declaration, and that the previous MND adequately evaluates the potential environmental impacts of the Project Modification.

2. The Planning Commission has reviewed and considered the information contained in the previously adopted MND for the Project, and all oral and documentary evidence received during the hearing on the Project Modification. The Planning Commission has determined that the previously adopted MND constitutes an adequate, accurate, objective, and complete review of the proposed Project Modification and finds that no additional environmental review is required based on the reasons set forth below:

a. No substantial changes are proposed by the Project Modification that will require major revisions of the previously adopted MND due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;

b. No substantial changes have occurred with respect to the circumstances under which the Project Modification will be undertaken which will require major revisions to the previously adopted MND due to the involvement of new significant

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environmental effects or a substantial increase in the severity of previously identified significant effects;

c. No new information of substantial importance has been found that shows any of the following:

i. The Project Modification will have one or more significant effects not discussed in the previously adopted MND;

ii. Significant effects previously examined will be substantially more severe than shown in the previously adopted MND;

iii. Mitigation measures previously found to be infeasible would in fact be feasible and would substantially reduce one or more significant effects of the Project Modification; or

iv. Mitigation measures which are considerably different from those analyzed in the previously adopted MND would substantially reduce one or more significant effects on the environment.

3. Based on its review of the previously adopted MND for the Project, and all oral and documentary evidence received during the hearing on the Project Modification, the Planning Commission finds that the MND reflect the Planning Commission's independent judgment and analysis and adopts the MND for the Project Modification and readopts the findings of fact in support of the MND.

4. The mitigation monitoring program for the Project is adopted for the Project Modification, and the mitigation measures shall be implemented and monitored as set forth in the program, based on the following findings of fact:

1. The mitigation monitoring program has been adopted and implemented as part of the Project; and

2. The mitigation monitoring plan meets the requirements of CEQA section 21081.6 and CEQA Guidelines section 15074.

5. Upon approval of the Project, the City's Environmental Planning Services shall file or cause to be filed a Notice of Determination with the Sacramento County Clerk and, if the project requires a discretionary approval from any state agency, with the State Office of Planning and Research, pursuant to section 21152(a) of the Public Resources Code and the State EIR Guidelines adopted pursuant thereto.

6. The documents and other materials that constitute the record of proceedings upon which the Planning Commission has based its decision are located in the City of Sacramento Community Development Department, Environmental Planning Services, 300 Richards Boulevard, Sacramento, CA 95811-0218. The custodian of these

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documents and other materials is the Community Development Department, Environmental Planning Services.

C. The **Variance** to allow an electronic billboard to encroach approximately 11.5 feet into the public right-of-way within the H-R zone is approved based on the following Findings of Fact:

1. That exceptional or extraordinary circumstances or conditions apply to the proposed sign that do not apply generally in the same zoning district, and that the enforcement of the regulations of the Sign Code would have an unduly harsh result upon the utilization of the subject property in that the existing conditions are such that strict adherence to not allowing the billboard to encroach in the public right-of-way would result in an unusable site for the City's electronic billboard as the sign could not be constructed elsewhere on the site due to soil limitations, easements, and visibility of the billboard sign to the freeway.
2. That the variance will not result in a special privilege to one individual property owner, and that the variance would be appropriate for any property owner facing similar circumstances in that other variances for sign projection into the public right-of-way have been granted in similar situations.
3. That the requested variance will not materially and adversely affect the health and safety of persons residing or working in the neighborhood, and will not be materially detrimental to the public welfare or injurious to property and improvements in the neighborhood in that the proposed billboard is located adjacent to the freeway in an area that is predominantly comprised of commercial uses. Further, the lowest portion of the billboard will be 25 feet above the actual street.
4. Granting the variance does not constitute a use variance in that a relocation agreement to locate the billboard on the site was previously approved by the City Council.

**DIGITAL BILLBOARDS
MITIGATION MONITORING PROGRAM**

This Mitigation Monitoring Program (MMP) has been required by and prepared for the City of Sacramento Community Development Department, Environmental Planning Services, 300 Richards Boulevard, Sacramento, CA 95811, pursuant to CEQA Guidelines Section 21081.6.

SECTION 1: PROJECT IDENTIFICATION

Project Name / File Number: Digital Billboards

Owner/Developer- Name: Clear Channel Outdoor
Address: 1107 9th Street, Suite 500
Sacramento, CA 95814

Project Location:

Digital billboard structures are proposed for four locations. **Location 1** is near Interstate 80 and Northgate (potential APNs include: 237-0031-003, -031). The site is located immediately adjacent to Interstate 80 near the north end of the access road to Sump 157. Railroad tracks and the levees for the Natomas East Main Drainage Canal are located to the west; vacant commercially zoned land is located to the east on the south side, and industrial and warehouse buildings are nearby on the north side. **Location 2** is near Interstate 5 and Richards Boulevard (APN: 001-0190-015). The site is located on the west side of Jibboom Street, and adjoins the site of the PG&E Powerhouse to the south. The parcel is part of the Robert T. Matsui Waterfront Park. The Sacramento River and an abandoned water intake structure are located to the west, the Best Western Inn-Sandman to the north, and Jibboom Street and Interstate 5 to the east. **Location 3** is near Business 80 and Fulton (Raption Honda) (APN: 254-0310-002). The site is paved, and is part of the Raption Honda display lot immediately to the north of Business 80. The golf course is located to the east, Raption Honda to the north, and Business 80 and commercial businesses across the freeway to the south. **Location 4** is near Highway 99 and Mack Road (APN: 117-0170-067). The site is used for detention purposes by the City of Sacramento. Commercial uses are located to the northwest, Highway 99 and commercial uses to the north east, and hospital uses (Kaiser Permanente) to the south.

Project Description:

The **Digital Billboards** project consists of City actions that would result in the construction and operation of digital billboards on City-owned property along major freeways within the City limits. The project includes an amendment to City Code provisions regulating signage that would allow digital billboards on City-owned parcels; rezoning for one sign location to meet a state requirement that billboard locations may be located only on parcels zoned for commercial or industrial uses; approval of lease agreements with Clear Channel Outdoor (Clear Channel) that would allow access to City-owned property for the construction and operation of four (4) digital billboard structures, each with two display faces, on identified parcels; approval of relocation agreements by which Clear Channel would agree to remove a specified number of existing traditional billboards for each digital billboard structure installed under the lease agreements; and construction and operation of four digital billboard structures following issuance of building permits by the City of Sacramento..

SECTION 2: GENERAL INFORMATION

The Plan includes mitigation for Aesthetics, Biological Resources, Cultural Resources, Hazards and Transportation/Circulation. The intent of the Program is to prescribe and enforce a means for properly and successfully implementing the mitigation measures as identified within the Initial Study for this project. Unless otherwise noted, the cost of implementing the mitigation measures as prescribed by this Program shall be funded by the owner/developer identified above. This Mitigation Monitoring Program (MMP) is designed to aid the City of Sacramento in its implementation and monitoring of mitigation measures adopted for the proposed project.

The mitigation measures have been taken from the Initial Study and are assigned the same number they have in the document. The MMP describes the actions that must take place to implement each mitigation measure, the timing of those actions, and the entities responsible for implementing and monitoring the actions. The developer will be responsible for fully understanding and effectively implementing the mitigation measures contained within the MMP. The City of Sacramento will be responsible for ensuring compliance.

DIGITAL BILLBOARDS
MITIGATION MONITORING PROGRAM

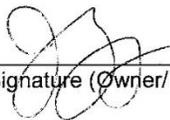
MITIGATION AGREEMENT

PROJECT NAME / FILE NUMBER: Digital Billboards
OWNER/DEVELOPER/APPLICANT: Michael Wagener / David M. McWalters
Clear Channel Outdoor
1107 9th Street, Suite 500
Sacramento, CA 95814

I, Jim Lyons (owner/developer/applicant), agree to amend the Digital Billboards project description to incorporate the mitigation measures identified in the Initial Study for the project. I understand that by agreeing to these mitigation measures, all identified potentially significant environmental impacts should be reduced to below a level of significance, thereby enabling the Environmental Coordinator to prepare a Mitigated Negative Declaration of environmental impact for the above referenced project.

I also understand that the City of Sacramento will adopt a Mitigation Monitoring Program (Program) for this project. This Program will be prepared by the Community Development Department, pursuant to the California Environmental Quality Act Guidelines Section 21081.6 and pursuant to Article III of the City's Local Administrative Procedures for the Preparation of Environmental Documents.

I acknowledge that this project, Digital Billboards, would be subject to this Program at the time the Program is adopted. This Program will establish responsibilities for the monitoring of my project by various City Departments and by other public agencies under the terms of the agreed upon mitigation measures. I understand that the mitigation measures adopted for my project may require the expenditure of owner/developer funds where necessary to comply with the provisions of said mitigation measures.



Signature (Owner/Developer/Applicant)
6-7

Title
12/9/09

Date

DIGITAL BILLBOARDS
MITIGATION MONITORING PROGRAM

Mitigation Measure	Monitoring Party	Compliance Milestone / Confirm Compliance
<p>1. TRANSPORTATION / CIRCULATION</p> <p>Transportation 1 The operation of digital billboards by the operator within the City of Sacramento shall comply with the following at all times:</p> <ul style="list-style-type: none"> a. No special visual effects that include moving or flashing lights shall accompany the transition between two successive messages, and no special visual effects shall accompany any message display; b. The operator shall report its intention of installing, implementing or using any technology that would allow interaction with drivers, vehicles or any device located in vehicles, including, but not limited to a radio frequency identification device, geographic positions system, or other device, in advance of such operation, in the annual report required in Mitigation Measure Transportation 2. <p>Transportation 2 The operator of any digital billboard operated within the City of Sacramento shall submit, within thirty days following June 30 of each year, a written report regarding operation of each digital billboard during the preceding period of July 1 to June 30. The operator may submit a combined report for all such digital billboards operated by such operator within the City limits. The report shall, when appropriate, identify incidents or facts that relate to specific digital billboards. The report shall be submitted to the Director of the Community Development Department, Director of Department of Transportation and the City Attorney, and shall include information relating to the following:</p> <ul style="list-style-type: none"> a. Status of the operator's license as required by California Business and Professions Code §§5300 et seq.; b. Status of the required permit for individual digital billboards, as required by California Business and Professions Code §§5350 et seq.; 	<p>City of Sacramento: Community Development Department; & Department of Transportation;</p>	<p>Provide annual written reports by July 30 each year detailing the information outlined in Mitigation Measure Transportation 2 to the Directors of the City of Sacramento Community Development Department and the Department of Transportation</p>

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MITIGATION MONITORING PROGRAM

Mitigation Measure	Monitoring Party	Compliance Milestone / Confirm Compliance
<ul style="list-style-type: none"> c. Compliance with the California Outdoor Advertising Act, California Business and Professions Code §§5200 and all regulations adopted pursuant to such Act; d. Compliance with California Vehicle Code §§21466.5 and 21467; e. Compliance with provisions of written agreements between the U.S. Department of Transportation and the California Department of Transportation pursuant to the federal Highway Beautification Act (23 U.S.C. §131); f. Compliance with mitigation measures identified in this Initial Study and in the Mitigated Negative Declaration adopted as part of project approval; g. Each written or oral complaint received by the operator, or conveyed to the operator by any government agency or any other person, regarding operation of digital billboards within the City of Sacramento; h. Each malfunction or failure of a digital billboard approved by the City of Sacramento and operated by the operator within the City of Sacramento, which shall include only those malfunctions or failures that are visible to the naked eye, including reason for the malfunction, duration and confirmation of repair; and i. Operating status of each digital billboard operated by the operator within the City of Sacramento, including estimated date of repair and return to normal operation of any digital billboard identified in the report as not operating in normal mode. 		
<p>2. <u>AESTHETICS</u> See Mitigation Measures Transportation 1 and 2</p>	See Mitigation Measures Transportation 1 and 2	See Mitigation Measures Transportation 1 and 2

DIGITAL BILLBOARDS
MITIGATION MONITORING PROGRAM

Mitigation Measure	Monitoring Party	Compliance Milestone / Confirm Compliance
<p>4. BIOLOGICAL RESOURCES</p>		
<p>BIO-1 (Location 2-Interstate 5 and Richards Boulevard) The following mitigation measures shall be implemented on the project site:</p> <ul style="list-style-type: none"> a. Fence and flag all areas to be avoided. Provide a minimum setback of at least 20 feet from the dripline of the elderberry plant located at the base of the electrical tower to the south. b. Brief contractors on the need to avoid damaging the elderberry plants and the possible penalties for not complying with these requirements. c. Put up signs every 50 feet along the edge of the avoidance areas with the following information: "This area is habitat of the valley elderberry longhorn beetle, a threatened species, and must not be disturbed. This species is protected by the Endangered Species Act of 1973, as amended. Violators are subject to prosecution, fines, and imprisonment." The signs should be clearly readable from a distance of 20 feet, and must be maintained for the duration of construction. Signs shall be removed following construction. d. Instruct work crews about the status of the beetle and the need to protect its elderberry host plant. 	<p>City of Sacramento Community Development Department</p>	<p>Measures shall be shown and confirmed on construction plans for Location 2.</p> <p>Prior to issuance of grading and/or building permits.</p>
<p>BIO-2 (Location 2-Interstate 5 and Richards Boulevard) If construction occurs during the breeding season (February 1–August 31), the developer will conduct CDFG-recommended protocol-level surveys within 0.8 kilometer (0.5 mile) of the project area prior to construction as required by the <i>Recommended Timing and Methodology for Swainson's Hawk Nesting Surveys in California's Central Valley</i> or as required by the CDFG in the future. If no active nests are identified during the survey, no additional mitigation is required.</p> <p>If active nests are found in the vicinity of the construction area, mitigation measures consistent with the <i>Staff Report Regarding Mitigation for Impacts to Swainson's Hawks (Buteo swainsoni) in the Central Valley of California</i> will be incorporated in the following manner or as directed by the CDFG.</p>	<p>City of Sacramento Community Development Department</p>	<p>Measures shall be shown and confirmed on construction plans.</p> <p>Provide written report verifying</p>

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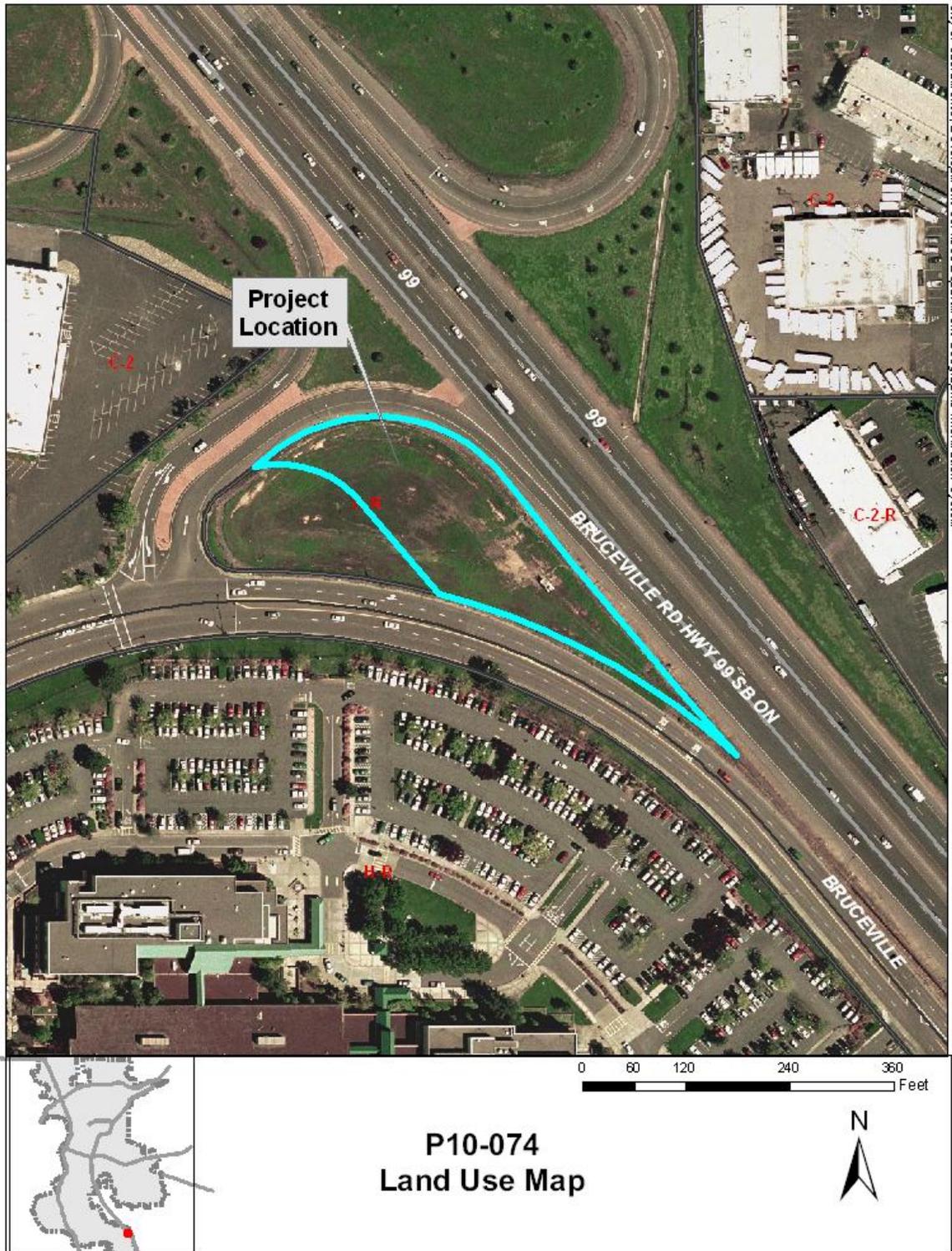
December 9, 2010

DIGITAL BILLBOARDS
MITIGATION MONITORING PROGRAM

Mitigation Measure	Monitoring Party	Compliance Milestone / Confirm Compliance
<p>a. If an active nest is found, no intensive new disturbances (e.g., construction activities that create sudden loud noises or vibrations) or other project-related activities that may cause nest abandonment or forced fledging, can be initiated within 200 yards (buffer zone) of an active nest between March 1 and September 15. The size of the buffer area may be adjusted if a qualified biologist and the CDFG determine it would not be likely to have adverse effects on the hawks. No project activity will commence within the buffer area until a qualified biologist confirms that the nest is no longer active.</p> <p>b. If construction or other project-related activities that may cause nest abandonment or forced fledging are necessary within the buffer zone, monitoring of the nest site (funded by the project proponent) by a qualified biologist will be required to determine if the nest is abandoned. If the nest is abandoned and if the nestlings are still alive, the project proponent will fund the recovery and hacking (controlled release of captive reared young) of the nestling(s).</p> <p>Routine disturbances, such as routine maintenance activities within 0.4 kilometer (0.25 mile) of an active nest, will not be prohibited unless consultation with the CDFG determines that these activities will affect the active nest.</p>		<p>compliance with BIO -2 for Location 2</p> <p>Prior to issuance of grading and/or building permits</p>
<p>5. CULTURAL RESOURCES</p> <p>CR-1 (Location 2-Interstate 5 and Richards Boulevard) For the proposed billboard sites located within archaeologically sensitive areas, as shown in the General Plan MEIR, Figure 6.4-1, the developer shall have test bores conducted by a qualified archaeologist in the location and to depth of the proposed excavation (drilling of the foundation holes). The archaeologist shall assess the significance of any finds in the test bores. The archaeologist shall coordinate the testing and provide written findings to the City's Preservation Director. If the find is determined to be significant by the archaeologist and the Preservation Director, the archaeologist and the Preservation Director shall coordinate to determine the appropriate course of action to be undertaken prior to initiation of construction.</p>	<p>City of Sacramento</p> <p>Community Development Department</p>	<p>Measures shall be shown and confirmed on construction plans.</p> <p>Provide a written report of the findings of testing to the</p>

DIGITAL BILLBOARDS
MITIGATION MONITORING PROGRAM

Mitigation Measure	Monitoring Party	Compliance Milestone / Confirm Compliance
<p>the remains are determined to be Native American, the coroner shall notify the Native American Heritage Commission, who shall notify the person most likely believed to be a descendant. The most likely descendant shall work with the contractor to develop a program for re-interment of the human remains and any associated artifacts. No additional work is to take place within the immediate vicinity of the find until the identified appropriate actions have taken place.</p>		
<p>7. HAZARDS</p> <p>HAZ -1 Prior to issuance of a building permit for the sites Location 2 (Interstate 5 and Richards Boulevard) and Location 3 (Raption Honda), the contractor shall submit a written report that either provides evidence that construction activities will not cause a release of contaminated soils or expose construction workers to contaminated soils prior to issuance of building or grading permits, or identifies the status of soils, plans for avoidance or remediation and the local or state agency that will have jurisdiction over site remediation.</p> <p>HAZ-2 The contractor shall comply with all requirements of the deed restrictions for the sites and coordinate with the applicable regulatory agencies overseeing the Operation and Maintenance Agreements for the affected sites.</p>	<p>City of Sacramento Community Development Department</p>	<p>Measures shall be shown and confirmed on construction plans.</p> <p>Provide written report verifying measures identified in HAZ – 1 for Location 3.</p> <p>Prior to issuance of any grading or building permit.</p>



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REPORT TO PLANNING COMMISSION City of Sacramento

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915 I Street, Sacramento, CA 95814-2671

STAFF REPORT
December 9, 2010

Honorable Members of the Planning Commission:

Subject: Northeast Line Implementation Plan (LR09-021)

Council District: 2

Recommendation: Review and comment.

Contact: Greg Sandlund, Associate Planner, (916) 808-8931; Jim McDonald AICP, Senior Planner, (916) 808-5723.

Presenters: Greg Sandlund, Associate Planner, (916) 808-8931

Department: Community Development

Division: Planning

Organization Number: 22001111

Description/ Analysis

Issue: The Northeast Line Implementation Plan is a planning effort to promote reinvestment, redevelopment, and revitalization along the light rail corridor that includes the Globe, Arden/Del Paso and Royal Oaks Stations. The Plan includes specific strategies to address housing, economic development, the strategic financing of infrastructure, public safety, and design needs along the light rail corridor.

The land use changes proposed are intended to better streamline uses that support an active and safe commercial corridor such as mixed use and mixed density housing as well as office and general commercial uses. Staff is also recommending that future infrastructure improvements be focused in key areas along the light rail corridor to encourage catalyst and near term development in the area.

Policy Considerations: The Northeast Line Implementation Plan is implementing the 2030 General Plan, which amended land use designations in key opportunity areas,

including light rail station areas and commercial corridors, to facilitate the revitalization of corridors and centers.

Environmental Considerations: At the time action is requested, staff will provide the appropriate discussion and findings to comply with the California Environmental Quality Act (CEQA).

Rationale for Recommendation: Staff would like any comments from the Commission prior the Commission taking formal action on the project on January 13th 2011.

Financial Considerations: None

Respectfully submitted by: 
 Greg Sandlund
 Associate Planner

Recommendation Approved:



 Jim McDonald AICP
 Senior Planner

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Background

Project Background

The Globe, Arden/Del Paso, and Royal Oaks Stations were built as part of the light rail starter line in 1987. Much of the land used for the starter line was existing right of way from freight rail lines. Therefore, most of the surrounding land uses were industrial or heavy commercial and not supportive of transit.

In 2002, Regional Transit and the City of Sacramento collaborated to identify land use and policy changes for areas within a 1/4 mile of transit stations to support transit. This planning effort was called Transit for Livable Communities (TLC).

As a follow up to the TLC planning effort, the Northeast Line Light Rail Stations Plan was approved by the City Council in 2007. This plan was predominately an urban design document that recommended, among other things: streetscape improvements, revisions to the North Sacramento Design Guidelines, rezones and urban design schemes for the Globe, Arden/Del Paso and Royal Oaks Station. The plan also analyzed the necessary infrastructure improvements to support 30 years of growth in project area.

The 2030 General Plan, adopted in 2009, amended land use designations in key opportunity areas, including light rail station areas and commercial corridors, to facilitate the revitalization of corridors and centers. The TLC and Northeast Line Light Rail Stations Plan informed the identification of the 2030 General Plan land use designations for this area.

Project Description

The Northeast Line Implementation Plan is an effort to implement the previous planning efforts mentioned above and includes the following actions:

- ◆ Rezone specified sites;
- ◆ Amend general plan land use designations;
- ◆ Expand the boundaries of the Del Paso Boulevard Special Planning District;
- ◆ Amend the Del Paso Boulevard Special Planning District;
- ◆ Amend the North Sacramento Design Guidelines;
- ◆ Amend the North Sacramento Community Plan to establish a transit village plan;
- ◆ Amend the RMX Zone;
- ◆ Establish phased infrastructure finance recommendations.

Rezones and General Plan Amendments:

The project includes rezoning sixteen parcels along Del Paso Boulevard to add the Transit Overlay Zone. This overlay zone will allow greater heights and densities than

the base General Commercial (C-2) Zone as well as allow for expedited application review for transit friendly development. These zoning designations are consistent with the 2030 General Plan which was adopted on March 3, 2009.

A single site would be rezoned from the Standard Single Family (R-1) Zone to the General Commercial (C-2) Zone. Until recently, this site was used a firehouse. The C-2 designation would be consistent with adjacent and nearby parcels along Del Paso Boulevard.

Twenty six parcels, located between Del Paso Boulevard and the Royal Oaks Station, are proposed to be rezoned from the Standard Single Family (R-1) Zone to the Residential Mixed Use (RMX) Zone. The RMX zone would allow for neighborhood and transit friendly commercial uses along Arden Way. It would also allow for future multi-family housing to be located nearby the Del Paso/Arden and Royal Oaks stations. Rezoning these parcels will require an amendment to the general plan land use designations, from Traditional Low Density Residential to Urban Corridor Low.

Approximately 110 parcels located northwest of Del Paso Boulevard are proposed to have amended general plan designations. Ten of the 110 parcels would have land use designations changed from Urban Corridor Low to Employment Center Low Rise. The rest of the 110 parcels would have land use designations changed from Urban Low Density Residential to Employment Center Low Rise. The purpose of these land use amendments is to continue to allow viable industrial uses to operate and allow for a more gradual transition of the area from a predominantly an industrial area to one of a more commercial/residential nature.

Amend and Expand the Del Paso Boulevard Special Planning District:

The project includes an expansion of the Del Paso Boulevard Special Planning District to include parcels, one block deep, located along the north side of Arden Way as well as the parcels immediately south of the Royal Oaks Station. These parcels are proposed to be included in the Special Planning District (SPD) because of their location along a busy corridor and their close proximity to light rail stations. The expansion of the SPD into Arden Way will change to name of the SPD to the Del Paso/Arden Special Planning District.

Additionally, one parcel on the southwest edge of the SPD and twelve parcels north of Del Paso Boulevard, fronting El Monte Avenue, would be included in the SPD. These parcels are proposed to be included in the district because of their current non-residential uses and their close proximity to the commercial corridor.

The amendments to the Special Planning District will help to facilitate a more flexible and expedited planning application process for uses that support the commercial corridor. Additionally, residential mixed use developments would be allowed with a plan review, as opposed to a special permit. The specific changes to the SPD are listed in Attachment 4. :

Design Review Guidelines Amendments: The project includes amendments to the North Sacramento Design Review Guidelines that incorporate design guidelines from

the Northeast Light Rail Stations Plan. These new design guidelines would enhance the existing residential and commercial guidelines and also give specific guidance on transit friendly housing such as live-work lofts, town houses/row houses, and residential mixed use developments.

North Sacramento Community Plan Amendments: The project includes amending the North Sacramento Community Plan to include new policies resulting from the Northeast Line Implementation Plan effort as well as policies from the Northeast Light Rail Stations Plan. These policies are consistent with the existing 2030 General Plan policies. Policy additions include: those that designate the Northeast Line section of the North Sacramento Community Plan as a transit village plan; and the addition of a new map showing the Northeast Line station area.

The new section in the North Sacramento Community Plan would include the designation of the Globe, Arden/Del Paso, and Royal Oaks stations a transit village districts per the California Transit Village Development Planning Act of 1994 (Section 65460 *et al* of the State of California Government Code). Under State law, a transit village plan shall include land within $\frac{1}{4}$ mile from the station; should encourage development in close proximity to the transit station; should offer intermodal service; should include a mix of uses and housing types; and provide a number of benefits such as increased infill, greater transit ridership and live-travel opportunities. A transit village plan shall be prepared, adopted, and amended in the same manner as a general plan. The City's General Plan was adopted by City Council resolution and this transit village plan will be adopted through a resolution.

Amendments to the RMX Zone: Staff is recommending that parcels zoned RMX along Arden Way be able to have up to 100% commercial uses with a zoning administrator's special permit. After initially considering this provision to be applied only in the special planning district, staff reasoned that such a provision should be applied citywide. The amendments to the RMX zone will allow for greater flexibility in permitting neighborhood supporting commercial uses while still emphasizing residential mixed use.

Phased Infrastructure Finance Recommendations: The infrastructure finance strategy will include specific recommendations for the public/private financing of prioritized infrastructure improvements in the study area. The recommendations will be for near term improvements that will help facilitate catalyst development in the area.

DRAFT ORDINANCE NO.

Adopted by the Sacramento City Council

**AMENDING TITLE 17 OF THE SACRAMENTO CITY CODE
(THE ZONING CODE) BY REZONING VARIOUS PARCELS
OF REAL PROPERTY AS PART OF THE NORTHEAST LINE
IMPLEMENTATION PLAN (LR09-021)**

BE IT ENACTED BY THE COUNCIL OF THE CITY OF SACRAMENTO:

Section 1. Title 17 of the Sacramento City Code (the Zoning Code) is amended by rezoning the properties depicted in the attached Exhibit A and identified by APN and address in the attached Exhibit B, from the existing zone to the proposed zone as set forth in Exhibit B.

Section 2. Rezoning of the property shown in the attached Exhibit A, by the adoption of this Ordinance, will be considered to be in compliance with the requirements for the rezoning of property described in the Zoning Code, as amended, as those procedures have been affected by recent court decisions.

Section 3. The City Clerk of the City of Sacramento is directed to amend the official zoning maps, which are part of the Zoning Code, to conform to the provisions of this Ordinance.

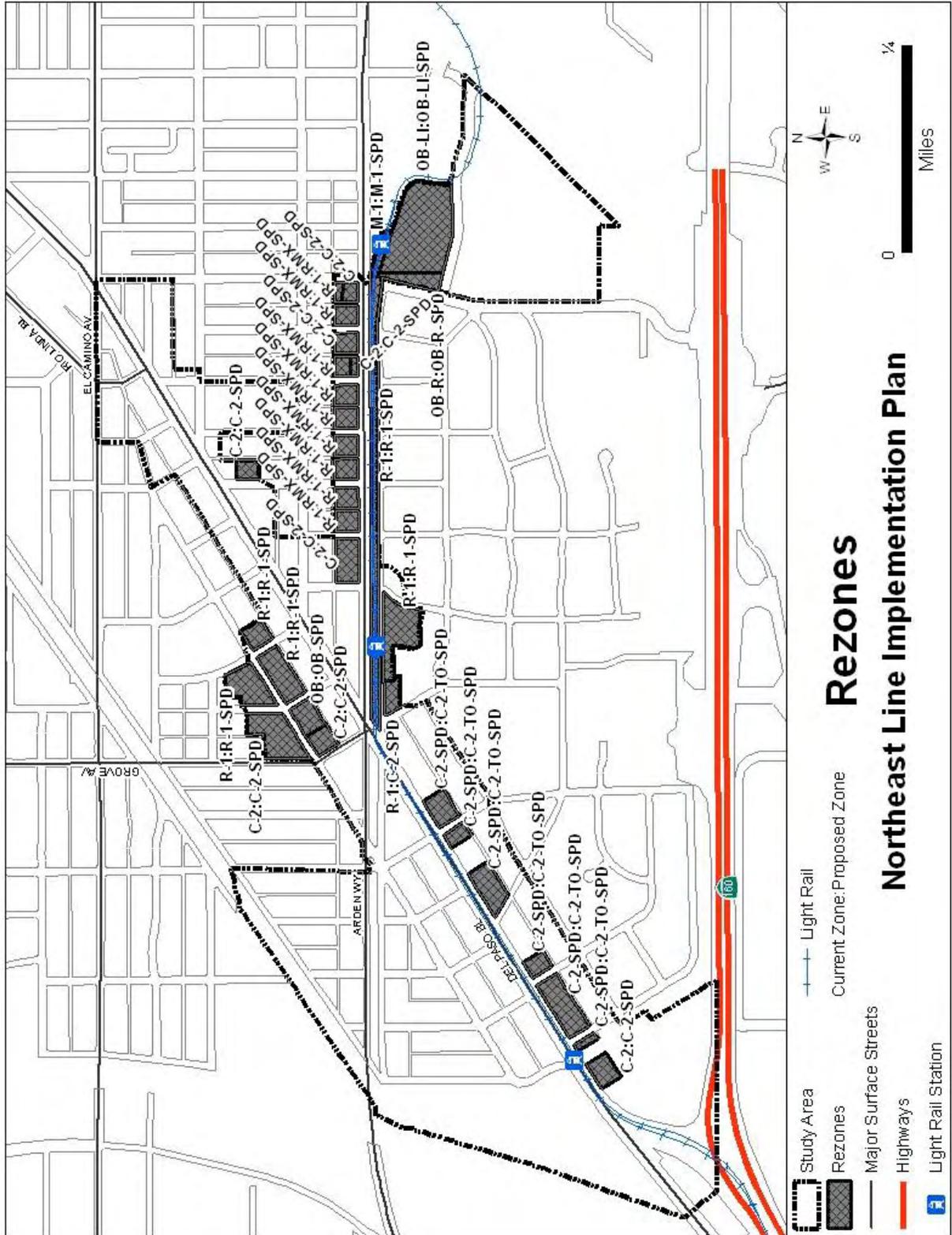
Section 4. Exhibits A and B are a part of this Ordinance.

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Exhibit A – Rezone Maps

Exhibit B – List of Rezone Properties

Exhibit A



Rezones
Northeast Line Implementation Plan

- Study Area
- Rezones
- Major Surface Streets
- Highways
- Light Rail Station
- Light Rail
- Current Zone: Proposed Zone

Exhibit B

[Property List to be Generated Prior to the Hearing]

DRAFT RESOLUTION NO.

Adopted by the Sacramento City Council

AMENDING THE 2030 GENERAL PLAN LAND USE AND URBAN FORM DIAGRAM RELATING TO THE NORTHEAST LINE IMPLEMENTATION PLAN (LR09-21)

BACKGROUND

- A. On October 15, 2002, the City Council accepted the Transit for Livable Communities (TLC) recommendations, which provided recommendations and strategies for transit-supportive development proximate to existing and future light rail stations.
- B. On July 24, 2007, the City Council accepted the Northeast Line Light Rail Stations Plan as the guiding vision for development within the quarter mile radius around the Globe, Arden/Del Paso, and Royal Oaks light rail stations. This plan consisted of design guidelines, recommended land use changes and an infrastructure assessment.
- C. On March 3, 2009, the City Council adopted the 2030 General Plan, which includes land use and policy direction to promote infill development in key opportunity areas, including commercial corridors and areas served by transit, such as the Northeast Line Light Rail Corridor.
- D. The 2030 General Plan Urban Corridor Low and Urban Neighborhood Low land use designation for the area known as the El Monte Triangle have been re-evaluated and found to not acknowledge the many viable industrial uses in the area. The Employment Center Low Rise general plan land use designation is consistent with the current heavy commercial uses as well as future urban uses, including office, retail, and housing.
- E. On January 13, 2011, the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve proposed amendments to the 2030 General Plan Land Use and Urban Form Diagram consistent with the Northeast Line Implementation Plan.
- F. On _____, the City Council conducted a public hearing, for which notice was given pursuant Sacramento City Code Section 17.200.010(C)(1)(a) (publication).

BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:

Section 1. Environmental Determination: The City Council has approved the environmental review of the Project as being within the scope of the 2030 General Plan Master EIR by Resolution No. ____.

Section 2. Based on the verbal and documentary evidence received at the hearing, the City Council approves the 2030 General Plan Land Use and Urban Form Diagram Amendment as set forth in Exhibits A and B.

Section 3. Exhibits A and B are a part of this Resolution.

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EXHIBIT A: Land Use Diagram Changes Maps

EXHIBIT B: Land Use Changes Property List

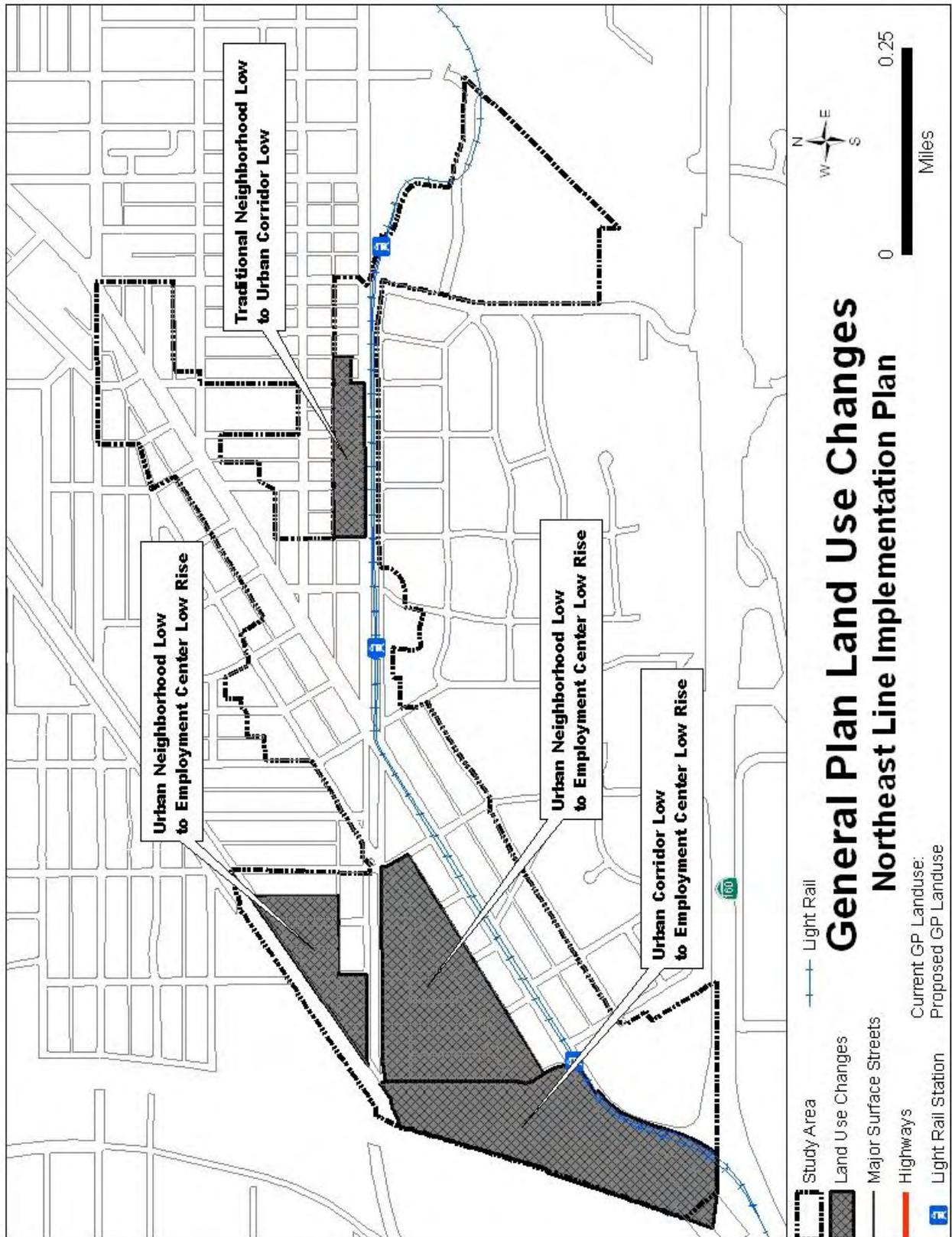


Exhibit B

[Property List will be Generated Prior to Hearing]

Attachment 4**Highlighted Draft Amendments to the
Del Paso Blvd Special Planning District**

- Change the name to Del Paso Boulevard/Arden Way Special Planning District in section 17.20.030 and throughout 17.108
- Allow apartments in the General Commercial (C-2) Zone with a zoning administrators plan review (instead of a zoning administrators special permit)
- Set the maximum allowable density for residential uses in the General Commercial (C-2) Zone to be 60 dwelling units per net acre
- Require that new residential development of 12 dwelling units per net acre include the following open space standards:
 - A minimum of fifty (50) square feet of usable common open space per unit is required. This open space area may include courtyards, gardens, recreation areas, and similar areas.
 - A minimum of fifty (50) square feet of usable private open space per unit is required. This area is for the exclusive use of the unit and may include decks, balconies and patios. Private useable open space shall be directly accessible from the unit.
 - For each square foot of usable private open space over fifty (50) square feet that is provided, the required fifty (50) square feet of usable common open space may be reduced by one square foot.
- Require that manufacturing uses fronting Del Paso Boulevard in the General Commercial (C-2) Zone have an office or other active commercial use facing the street
- Allow produce stands with a zoning administrator's special permit
- Require setbacks in the General Commercial (C-2) zone but allow them to be waived when appropriate through the design review process
- Continue to allow temporary parking lots with the same process as downtown
- Allow up to 50% residential uses in the Office (OB) Zone with a zoning administrators plan review
- Allow office-supporting uses in the Office (OB) Zone with a zoning administrator's plan review

DRAFT RESOLUTION NO. 2010- ____

Adopted by the Sacramento City Council

AMENDING THE NORTH SACRAMENTO COMMUNITY PLAN CHAPTER OF THE 2030 GENERAL PLAN TO ADD POLICIES FOR THE NORTHEAST LINE TRANSIT VILLAGES AND TO ESTABLISH THE NORTHEAST LINE TRANSIT VILLAGE DEVELOPMENT DISTRICTS FOR THE GLOBE, ARDEN/DEL PASO, AND ROYAL OAKS LIGHT RAIL STATIONS (LR09-021)

BACKGROUND

- A. On October 15, 2002, the City Council accepted the Transit for Livable Communities (TLC) recommendations, which provided recommendations and strategies for transit-supportive development proximate to existing and future light rail stations.
- B. On July 24, 2007, the City Council accepted the Northeast Line Light Rail Stations Plan as the guiding vision for development within the quarter mile radius around the Globe, Arden/Del Paso, and Royal Oaks light rail stations. This plan consisted of design guidelines, recommended land use changes and an infrastructure assessment.
- C. On March 3, 2009, the City Council adopted the 2030 General Plan, which includes land use and policy direction to promote infill development in key opportunity areas, including commercial corridors and areas served by transit, such as the Northeast Line Light Rail Corridor.
- D. On October 6, 2009, the City Council designated the Northeast Line Light Rail Corridor as a Tier 2, shovel-ready area in order to promote reinvestment efforts in the area and to prepare the area for new development that would fulfill the vision of the 2030 General Plan and other past planning efforts.
- E. The policies in Exhibit A, comprising the Northeast Line Transit Village Plan for the Globe, Arden/Del Paso, and Royal Oaks stations, are consistent with the goals and policies of the North Sacramento Community Plan and the 2030 General Plan.
- F. The policies included in Exhibit A of this resolution support the City's vision for the Northeast Line Light Rail Corridor and were drafted in accordance with the provisions of the State Transit Village Development Act (Government Code section 65460 et seq.), which encourages mixed-use development at higher

densities around transit stations.

- G. On January 13, 2011 the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve the components of the Northeast Line Implementation Plan, including the amendments to the North Sacramento Community Plan chapter of the 2030 General Plan as set forth in Exhibit A (LR09-021).
- H. On _____, the City Council conducted a public hearing, for which notice was given pursuant Sacramento City Code Sections 17.200.010(C)(1) (a) and (c) (publication and mail (500 feet)), and received and considered evidence concerning the Northeast Line Implementation Plan, including the amendments to the North Sacramento Community Plan chapter of the 2030 General Plan as set forth in Exhibit A (LR09-021).

BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:

Section 1. Environmental Determination: The City Council has approved the environmental review of the Project as being within the scope of the 2030 General Plan Master EIR by Resolution No. ____.

Section 1. The North Sacramento Community Plan chapter of the 2030 General Plan is hereby amended to add the language and policies related to urban development in the Northeast Line Corridor as identified in Exhibit A.

Section 2. All that land within the North Sacramento Community Plan that is not more than a quarter mile from the Globe light rail station is hereby designated the Globe Transit Village Development District pursuant to the Transit Village Development Planning Act of 1994 (Government Code section 65460 et seq.). The Northeast Line Transit Village Plan set forth in Exhibit A, supporting policies have been prepared and are adopted as the transit village plan for the district.

Section 3. All that land within the North Sacramento Community Plan that is not more than a quarter mile from the Arden/Del Paso light rail station is hereby designated the Arden/Del Paso Transit Village Development District pursuant to the Transit Village Development Planning Act of 1994 (Government Code section 65460 et seq.). The Northeast Line Transit Village Plan set forth in Exhibit A, supporting policies have been prepared and are adopted as the transit village plan for the district.

Section 4. All that land within the North Sacramento Community Plan that is not more than a quarter mile from the Royal Oaks light rail station is hereby designated the Royal Oaks Transit Village Development District pursuant to the Transit Village Development Planning Act of 1994 (Government Code section 65460 et seq.). The Northeast Line Transit Village Plan set forth in Exhibit A, supporting policies have been prepared and are adopted as the transit village plan for the district.

Section 5. Exhibit A is a part of this Resolution.

Table of Contents:

Exhibit A – North Sacramento Community Plan Amendment Language and Figures

North Sacramento Community Plan Amendment Language and Figures

[To be inserted after the infrastructure challenges discussion on page 3-NS-17 of the North Sacramento Community Plan chapter of the City's 2030 General Plan.]

Policies for the Northeast Line Transit Village Plan

In order to promote reinvestment and the long-term success of the Northeast Line Light Rail Corridor, the City prepared the Northeast Line Implementation Plan (2011), a planning effort to promote new housing, economic development, the strategic financing of infrastructure, public safety, and design needs along the light rail corridor that includes the Globe, Arden/Del Paso, and Royal Oaks stations. The Plan is based on previous planning efforts, including the Northeast Line Light Rail Stations Plan (2007) and Transit for Livable Communities (2002).

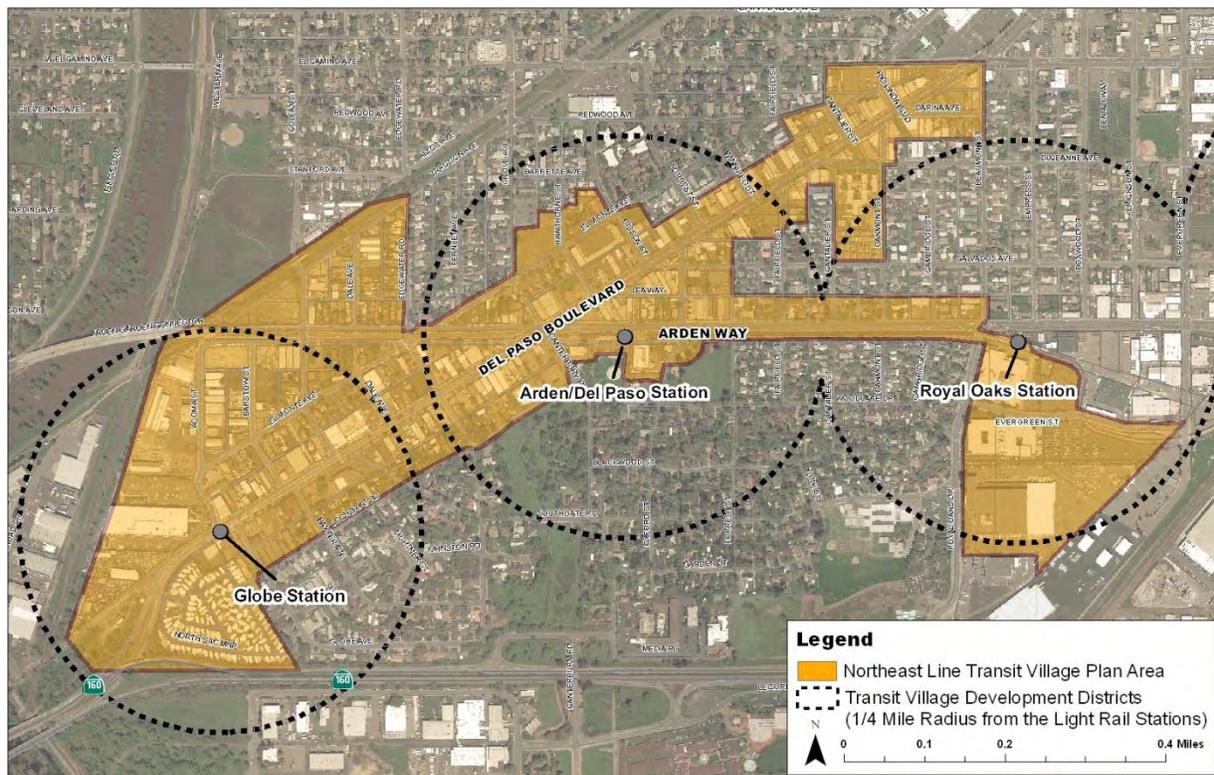


Figure NS-NELTV 1: Northeast Line Transit Village Plan Area (Pursuant to the Transit Village Development Act of 1994 [Government Code section 65460 et seq.]

The Northeast Line Transit Village shown in Figure NS-NELTV 1 above has three transit village development districts, which are encompassed by land within a ¼ mile radius of the Globe, Arden/Del Paso and Royal Oaks Stations. These three separate transit village development districts are each subject to polices of the overall Transit Village Plan Area where the transit village development districts overlap

the Northeast Line Transit Village Plan area. The Northeast Line Transit Village Plan as well as the Globe, Arden/Del Paso, and Royal Oaks Transit Village Development Districts have been adopted pursuant to State law and embody both the State and City’s vision of intensified development near transit and mixed-use activity centers, which in turn will lead to increased walking and reduced automobile use.

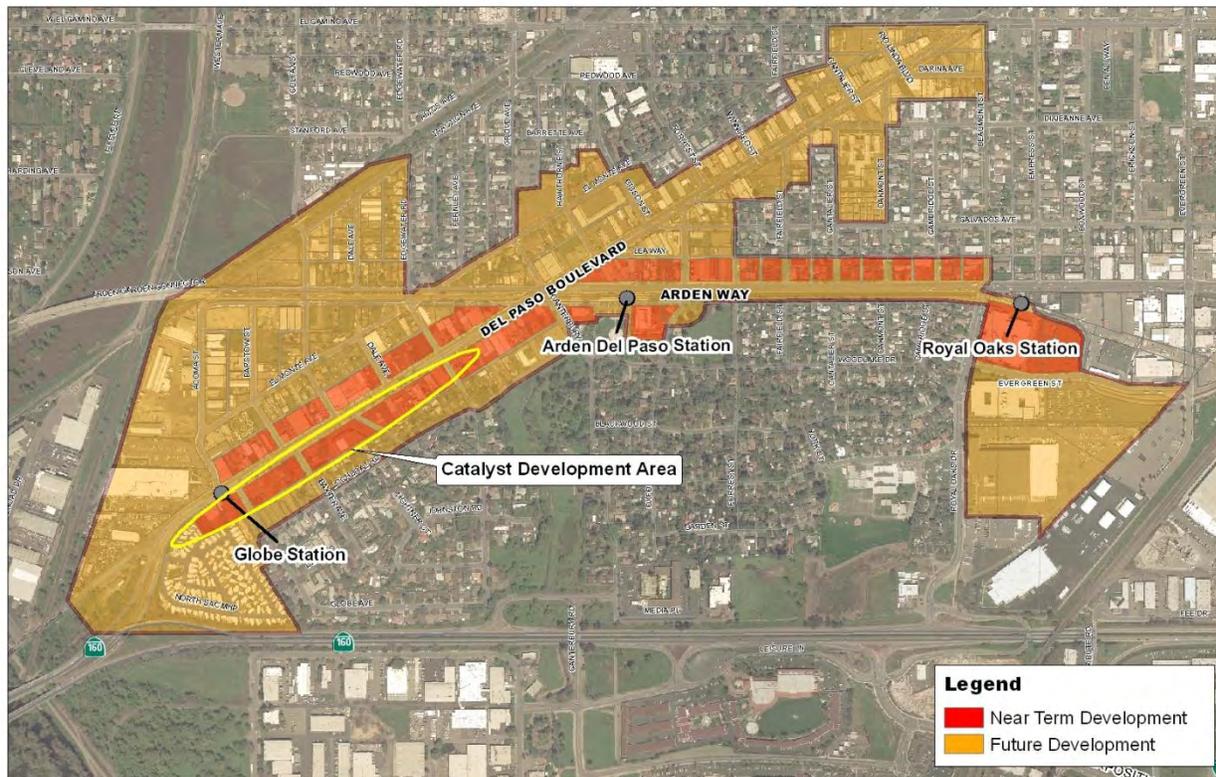


Figure NS-NELTV-2 – Policy Area for the Northeast Line Transit Village

The policies included in this section will help to shape a transit village that efficiently utilizes the land around each light rail station and provides a mix of uses that benefit the surrounding community. The areas that will accommodate catalyst development and near term development are shown in Figure NS-NELTV-2, above. Specific infrastructure improvements to facilitate development in these areas have been identified in the 2011 report entitled “Northeast Line Light Rail Stations Plan Phased Infrastructure Recommendations.” Parking facilities shall be developed when on street parking is required to promote economic development.

NS.NELTV 1.1 Active Ground Level Uses. The City shall require larger residential mixed use projects along Del Paso Boulevard to have active ground level uses built up to the right of way in order to provide strong street definition and an active edge along the sidewalk. (RDR)

NS.NELTV 1.2 Prioritized Infrastructure Improvements. The City shall prioritize infrastructure improvements to support the catalyst development indicated in Figure NS-NELTV-2, above. (SO)

- NS.NELTV 1.3** **Street Walls.** The City shall ensure that each block along Del Paso Boulevard has a predominant street wall. The street wall shall have a consistent height, be composed of contiguous buildings, and have upper stories stepped back when necessary. (RDR)
- NS.NELTV 1.4** **Sensitivity to Adjacent Neighborhood Scale.** The City shall ensure that development along Del Paso Boulevard and Arden Way is sensitive to adjacent neighborhood scale and provide a height and mass transition to the medium to higher density development at the corridor. (RDR)
- NS.NELTV 1.5** **Existing Industrial and Service Oriented Uses.** The City shall allow for the retention and continued operation of existing light industrial and service oriented uses, while providing for a comfortable coexistence with future new residential and commercial development. (RDR)
- NS.NELTV 1.6** **Ground Floor Visibility.** The City shall require windows to be provided on the street level of new buildings in the Northeast Line Transit Village as a visual link between business and pedestrians. Ground-floor commercial facades facing streets, sidewalks, pedestrian routes and public plazas shall have non-reflective, transparent windows. (RDR)
- NS.NELTV 1.7** **Parking.** The City shall support reduced parking ratios for transit oriented residential or commercial development in the transit village area while promoting the efficient design and use of parking, including curbside parking, shared parking, and the use of parking structures for higher density development and park-and-ride areas. (RDR)
- NS.NELTV 1.8** **Temporary Parking Facilities along Del Paso Boulevard.** The City shall work with the Sacramento Housing and Redevelopment Agency to provide temporary parking facilities along Del Paso Boulevard when necessary. (IGC)

DRAFT ORDINANCE NO.

Adopted by the Sacramento Council

**AMENDING SECTION 17.28.030 OF TITLE 17
OF THE SACRAMENTO CITY CODE (THE ZONING
CODE) RELATING TO THE RESIDENTIAL MIXED USE
ZONE (LR09-021)**

BE IT ENACTED BY THE COUNCIL OF THE CITY OF SACRAMENTO:

Section 1. Section 17.28.030 of Title 17 of the Sacramento City Code (the Zoning Code) is amended as follows:

A. Subsection A of Section 17.28.030 is amended to read as follows:

A. Nonresidential Development Limitations.

1. For new development in the RMX zone, commercial and office uses are limited to the ground floor only and may occupy up to a maximum of fifty (50) percent of the building square footage; provided, that

a. On lots that are less than or equal to three acres in size, the percentage of commercial or office use may be increased up to 100% of the building square footage, subject to approval of a zoning administrator’s special permit;

b. On lots that are greater than 3 acres in size, the percentage of commercial or office use may be increased up to 100% of the building square footage, subject to approval of a planning commission special permit.

2. The design of the proposed commercial or office development shall conform to the commercial corridor design principles adopted under Section 17.132.180 as they may be amended from time to time. The commercial corridor design principles shall be applied in addition to the design guidelines applicable under Chapter 17.132, Design Review, if any. In the event of a conflict, the design guidelines applicable under Chapter 17.132 shall take precedence over the commercial corridor design principles.

3. An architecturally or historically significant structure of any size may be converted entirely to commercial or office uses, subject to approval of a zoning administrator’s special permit, in order to ensure preservation and maintenance of the structure. The intent of this provision is to make structural repair and restoration economically viable, and ensure the community’s continued benefit from the preservation of the significant structure.,

4. The percentage of nonresidential use may be increased up to one hundred (100) percent of the building square footage if the building is occupied by a community or neighborhood-based nonprofit organization, subject to approval of a zoning administrator's special permit.

B. Except as specifically amended by the amendments to subsection A, Section 17.28.030 remains unchanged and in full force and effect.

DRAFT RESOLUTION NO.

Adopted by the Sacramento City Council

**AMENDING THE NORTH SACRAMENTO DESIGN GUIDELINES AS
PART OF THE NORTHEAST LINE IMPLEMENTATION PLAN (LR09-21)****BACKGROUND**

- A. On October 15, 2002, the City Council accepted the Transit for Livable Communities (TLC) recommendations, which provided recommendations and strategies for transit-supportive development proximate to existing and future light rail stations.
- B. On July 24, 2007, the City Council accepted the Northeast Line Light Rail Stations Plan as the guiding vision for development within the quarter mile radius around the Globe, Arden/Del Paso, and Royal Oaks light rail stations. This plan consisted of design guidelines, recommended land use changes and an infrastructure assessment.
- C. On March 3, 2009, the City Council adopted the 2030 General Plan, which includes land use and policy direction to promote infill development in key opportunity areas, including commercial corridors and areas served by transit, such as the Northeast Line Light Rail Corridor.
- D. Design guidelines from the Northeast Line Light Rail Stations Plan will augment the North Sacramento Design Guidelines and give specific design direction for housing types that will occupy the urban corridor.
- E. On January 19, 2011 the City Design Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve the proposed amendments to the North Sacramento Design Guidelines, for which notice was given pursuant to Sacramento City Code Section 17.200.010(C)(2)(a) (publication).
- F. On _____, the City Council conducted a public hearing, for which notice was given pursuant to Sacramento City Code Section 17.200.010(C)(2)(a) (publication).

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL
RESOLVES AS FOLLOWS:**

Section 1. Environmental Determination: The City Council has approved the environmental review of the Project as being within the scope of the 2030 General Plan Master EIR by Resolution No. ____.

Section 2. Based on the verbal and documentary evidence received at the hearing, the City Council approves the amendments to the North Sacramento Design Guidelines as set forth in Exhibit A.

Section 3. Exhibit A is a part of this Resolution.

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EXHIBIT A: Amended North Sacramento Design Guidelines

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Town House and Row House

Town houses and Row houses are defined as multi-story single-family residential units and are currently the most market-friendly building prototype. Row houses generally front public streets, while town houses are often located along internal pedestrian pathways and mews.

Development can also be designed to have more of a multi-family character. Depending on the intended character of the development, staff and the applicant can refer either to the single family section of these guidelines or the multi-family section for further design guidance.



Row houses that face the street create an attractive environment.

Town House and Row House

SITE DESIGN

This section addresses the location of row houses and town house on their lots, its overall layout relative to the site, its orientation toward the street and adjacent buildings, and the location of parking and utilities. Good site design of row house and town house structures, should:

- complement the scale, massing and setbacks of existing detached homes on the block;
- structures located in or near a commercial corridor may have smaller setbacks similar to the guidelines for new commercial buildings;
- provide an entry facing the street to create a welcoming appearance and to give homes “curb appeal”;
- guest parking areas, utilities, and service facilities should be located toward the interior of the site;
- common spaces should be toward the interior of the site.

Town House and Row House

39 Relationship to the Street

Design Principle

Development should present a facade that encourages interaction with the street by including entry features, windows, and landscaping along the street side of the building.

Rationale

Development adjacent to a public street should encourage residents to actively engage with that street through a variety of design elements. In addition to improving the visual quality of the streetscape, design elements should allow residents to see and be seen from the street, enhancing neighborhood interaction, improving safety and providing "eyes on the street."

Design Guidelines

- 39-1 Maximize the number of units and building entries fronting the street to allow maximum "eyes on the street".
- 39-2 Configure residential developments so that the majority of the units minimize exposure to the south-west and west sun while still allowing plenty of light and ventilation from at least two sides in each unit.
- 39-3 Provide parking in the rear of the lots accessed by existing alleys and new minimum 20 feet wide driveways.
- 39-4 Ensure adequate (5-20 ft) setbacks for each unit to allow for open spaces for gardening, barbecuing, etc.
- 39-5 Where possible, provide variation in front facade depth to enrich the pedestrian experience.
- 39-6 Stepback upper floors to create opportunities for balconies.



Maximize the number of units and building entries fronting the street to allow maximum "eyes on the street".

Town House and Row House



Design front setbacks to allow maximum opportunities for interaction between residents and neighbors.



This development has setbacks similar to those of surrounding single-family homes.



This development has smaller setbacks that are similar to those of adjacent commercial buildings.

40 Setbacks

Design Principle

Setbacks of structures should reflect the appropriate commercial or residential context.

Rationale

When development is placed on busy commercial streets, smaller setbacks that locate the building closer to the street are preferred. Development constructed near single-family residential neighborhoods should reflect the larger setbacks typically found in those areas.

Design Principles

- 40-1 Development should be designed with varied setbacks that contribute to an interesting streetscape and avoid a monotonous streetwall. Continuous lines of buildings with the same setback should be avoided.
- 40-2 Individual buildings can also be designed with an articulated front, with porches closer to the street.
- 40-3 In residential neighborhoods, row house and town house should adopt the predominant setback, but should also vary the building facade to relieve the appearance of mass.
- 40-4 In residential neighborhoods, design front setbacks to allow maximum opportunities for interaction between residents and neighbors.
- 40-5 In commercial areas, setbacks that locate buildings close to the street are preferred.

Town House and Row House

41 Scale and Mass

Design Principle

Development should be compatible with the scale and mass of existing structures in the vicinity.

Rationale

Development should use design and construction methods that minimize the appearance of mass with multiple rooflines, articulated facades, and architectural detailing that break up the facade.

Design Guidelines

- 41-1 Development that is constructed as infill near an existing single-family residential neighborhood should provide a streetside facade that is complementary to these single-family homes in style and massing.
- 41-2 Encourage two- to four-story buildings.
- 41-3 Setback upper floors to create opportunities for balconies.
- 41-4 Multi-story structures should be articulated to break up the facade and minimize massing.
- 41-5 Two-story structures should have multiple rooflines with corresponding gables that are consistent in style and materials with the overall structure.
- 41-6 Architectural detailing, such as dormer and other types of decorative windows, complementary trim, porch details, decorative shutters, color and wainscoting, should vary from unit to unit to reduce the appearance of bulk and mass by providing visual interest.



This three-story development sets the third floor back and has a facade that is complementary to nearby single-family homes.

Town House and Row House

42 Circulation

Design Principle

A network of public streets, internal streets, driveways, and paseos should be used throughout the development to enhance circulation within the site and connectivity to the adjacent neighborhood.

Rationale

Good site design of streets, driveways, and paseos enhances the interaction between pedestrians and motorists. A hierarchy of circulation options will promote safety and add to the character of the development.

Design Guidelines

- 42-1 A network of public streets, internal streets, driveways, paseos etc. is encouraged, when feasible.
- 42-2 Driveways should be designed to be accessible and safe for both pedestrians and motorists.
- 42-3 Internal paths such as paseos should be designed to improve pedestrian circulation and connections throughout the site.
- 42-4 Pedestrian connections to adjacent existing or future retail developments is encouraged.

Town House and Row House

43 Interior Common Spaces

Design Principle

Development should provide interior common spaces that are easily accessible. Individual units adjacent to common spaces should have facades with entry features and windows that open onto those common spaces.

Rationale

Interior common spaces should ideally foster a sense of community. This can be facilitated by building facades that allow residents to see and easily use common spaces. Common spaces should offer amenities that invite use, such as seating, shade, and tot lots.

Design Guidelines

- 43-1 Units should have doorways that open onto interior common spaces.
- 43-2 All units that overlook interior common spaces should have windows that allow residents to easily see these areas.
- 43-3 Common amenities, such as tot lots, seating areas, and swimming pools, should be provided that cater to all age ranges, from small children to the elderly, as appropriate.
- 43-4 Common open space should be designed as a visible, accessible transition between the street and individual units.



Development with doors and windows that face out on the common open space area.



This development has a common area with amenities such as play equipment.

Town House and Row House



The garages are located at the rear of this row house development.



Access to these garages is at the rear of each unit.

44 Garages

Design Principle

Row house garages should be located in the rear of the unit and accessed by an internal street or alley. Town house garages should be located at the front of the unit.

Rationale

To minimize the visual prominence of garages row house and town house garages should be designed to blend into the structure.

Design Guidelines

- 44-1 Row house developments should use tuck-under or below grade garages.
- 44-2 Town house developments are encouraged to use two car tandem garages rather than traditional two car garages to reduce the visual impact of large garage doors, when feasible.
- 44-3 Garage doors should have small opaque or transparent windows, to allow light into the garage and to reduce the visual prominence of the door.

Town House and Row House

45 Guest Parking

Design Principle

Guest parking should be located on internal streets throughout the site. Parking lots that face the street or are on the side of row house and town house should be minimized.

Rationale

Development should encourage residents to have an active relationship with the street(s) adjacent to the development. To this end, guest parking should be located in the interior of the development so as not to interfere with access to the street or interior common spaces.

Design Guidelines

- 45-1 Parking lots shall conform to City Municipal Code Section 17.64.030, "development standards for parking facilities," which specifies stall size and design.
- 45-2 Smaller, scattered lots will provide better access to residents and be less visually obtrusive than a single large lot.
- 45-3 Parking areas should be screened from adjacent structures with landscaping strips. However, screening should not exceed 4 feet in height, and should be permeable so that areas can be viewed by passing pedestrians and vehicles.
- 45-4 Underground parking in private or shared garages accessible from the street is acceptable if it does not interfere with pedestrian access to the street.
- 45-5 Provide parking in the rear of lots accessed by side streets or alleyways.

Town House and Row House

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Lofts and Live Work Units

Lofts and live-work units allow for flexible spaces that can be used for both residential and non-residential purposes. This building prototype is well suited for the largely industrial sections of North Sacramento as the transit stations area transition into non-industrial mixed use residential neighborhoods.

For further design guidance please refer to the multi-family section of these guidelines.



Live-work lofts.

Lofts and Live Work Units

46 Orientation and Layout

Design Principle

Lofts and live work units should be oriented towards public streets to increase pedestrian interaction and facilitate activity between residential and non-residential building uses.

Rationale

Proper building orientation can promote pedestrian friendly design and energy efficiency.

Design Guidelines

- 46-1 Orient the flexible space component of the unit towards the public realm of streets and pedestrian pathways to optimize business visibility.
- 46-2 Facades with large amounts of glazing should be oriented towards the north to minimize glare and reduce heat gain.



Live work units flex space oriented towards public realm.

Lofts and Live Work Units

47 Massing & Setbacks

Design Principle

Maintain an industrial nature of the building while signaling the human, residential elements of the use. Building massing and setbacks should occur at a human scale and promote connectivity to streets.

Rationale

By incorporating industrial and residential massing and designs, new uses will transition smoothly from predominantly industrial uses that surround the property.

Design Guidelines

- 47-1 Encourage floor-to-floor heights of about fifteen feet.
- 47-2 Allow five to fifteen foot wide front setbacks to provide privacy. Allow the setbacks to accommodate architectural elements, including colonnades and awnings.
- 47-3 Encourage the street facing facades to be vertical with little or no setbacks.



Loft and live work structure with industrial character and appropriate massing and setbacks which actively engage the street.

Lofts and Live Work Units



Live-work lofts articulated with large windows and awnings.

48 Building Articulation

Design Principle

The facades of structures should be visually interesting and emphasize the industrial nature of the project.

Rationale

The visual interest of the non-residential aspect of the live-work lofts will promote an interesting environment for residents.

Design Guidelines

- 48-1 Design the front façade of live work units to reflect the simple and functional, yet edgy, character of industrial buildings.
- 48-2 Articulate the front facades with big double height windows, awnings, saw tooth roofs, etc.
- 48-3 Allow upper story balconies to protrude four to six feet from the building edge.

Lofts and Live Work Units

49 Private Realm

Design Principle

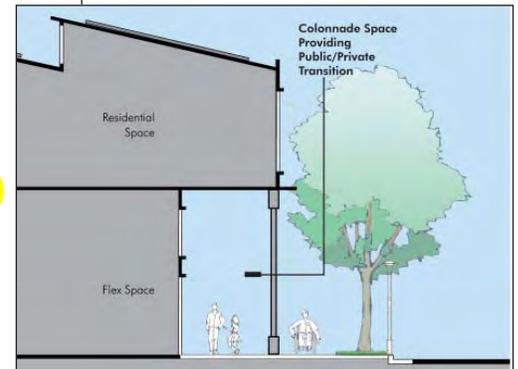
The “private realm” refers to the buildings and land that are on privately-owned lots and parcels. The private realm should consist of private and semi-private transitional spaces between the public realm and buildings, that serve to enhance the vitality of the community.

Rationale

The design of the private realm can have a significant impact on the quality of the public realm, as private buildings typically provide the edges to streets and open spaces. The guidelines serve to guide those aspects of the private realm that have a direct affect on the surrounding public context.

Design Guidelines

- 49-1 Accommodate elements, such as colonnades in the front setbacks, that provide flexibility to be used as residential oriented porches or business entry alcoves, whichever best suites the use of the live-work unit.
- 49-2 Allow awnings and signage to protrude within front setbacks.



Example of colonnades in front setback of live-work unit.

Commercial

50 Building Orientation, Setbacks, and Build-to Lines

Design Principle

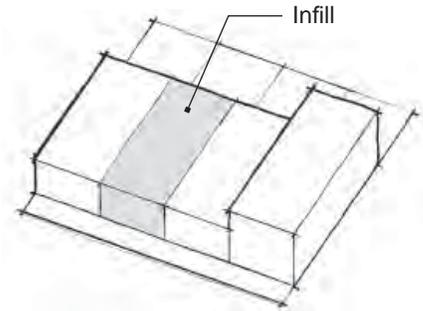
Buildings should be constructed to the front of the property line behind the sidewalk, with allowable variation in the setback to provide for café seating, plazas, and other additions to the public realm.

Rationale

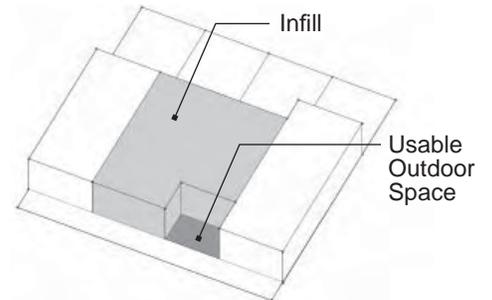
Commercial buildings in urban areas have typically been built to the front of the property line behind the sidewalk, creating a line of buildings with a consistent “streetwall” that supports a strong relationship between the building, the sidewalk, and the street. This streetwall should be reinforced by new construction and additions. The streetwall may be varied to create usable public spaces such as outdoor café dining and small plazas with seating.

Design Guidelines

- 50-1 Buildings should be constructed to the front of the property line and from side property line to side property line.
- 50-2 Facades that front onto a public street should be built parallel or nearly parallel to the public right-of-way.
- 50-3 A portion of the front setback may be increased by as much as 15 feet, if that setback is used as public space, such as outdoor restaurant seating or a courtyard with public access. A minimum of 60% of the front facade should be constructed up to the front setback.
- 50-4 Buildings at corners may be set back to create corner entries or “chamfered” entries.
- 50-5 New buildings should provide an appropriate setback to allow rear- and side-yard facing windows on existing buildings to have access to light, air, and usable space between buildings.



New construction and additions should be built to the back of the sidewalk or at the front of the property line.



New construction and additions may increase a portion of the front setback if designed as usable outdoor space.



Many buildings on Del Paso Boulevard are built to the property line.

Commercial

50-6 The ground floor of buildings within or near transit-oriented development areas should be oriented toward the street, adjacent plazas, or parks.

50-7 Orient buildings such that the primary active building facades and key pedestrian entries of the buildings face the street.

50-8 Encourage corner buildings to actively address both streets with pedestrian friendly entries.

50-9 Encourage maximum building edges and open spaces, such as front yards and outdoor restaurant seating, to front onto sidewalks to encourage pedestrian activity.

50-10 Orient new buildings so as to minimize exposure to the southwest and west sun to minimize undesirable heat gain of buildings.

50-11 Explore new developments, especially residential and mixed-use residential developments that are configured around open to sky courtyards that are contextually appropriate to the Sacramento climate.

50-12 Encourage buildings, especially individual residential units, to have access to sun and air on at least two sides to encourage adequate light and ventilation.

50-13 Incorporate pedestrian friendly elements including balconies and front porches within front setbacks.

Commercial

ARCHITECTURAL ELEMENTS

Architectural design guidelines address the exterior of buildings and their relationship to the surrounding built context. It is paramount to ensure that the design of the building complements the community setting and character and contributes to the public realm. Architectural design should promote commercial buildings that are:

- visually welcoming from the primary pedestrian street;
- similar in mass and scale to other commercial buildings in the area; and
- constructed of high-quality materials that will contribute to the longevity of the building.

Respect the past Art/Streamline Modern architectural style along Del Paso Boulevard by not replicating or imitating the architecture, but continuing its essence, which was inspired by technology and the emerging love affair America had with machines. Simple and functional architecture that highlights the juxtaposition of strong architectural elements, such as contrasting strong horizontal and vertical lines with curving forms and complimenting subdued earthy base building colors with bright and dark colored trims.

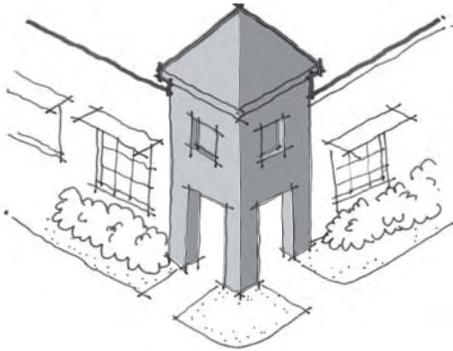


High quality materials and creative design on the Plaza del Paso building



The Limn furniture store references traditional local architectural elements with its small round windows and entry feature, while the building's signage and sculptures display cutting-edge architectural design.

Commercial



Building entries at corners should address both sides.

52 Building Height, Massing, and Scale

Design Principle

The size and scale of commercial buildings should be compatible with existing development in commercial districts.

Rationale

To ensure compatibility with existing development, new development should appear similar in massing and scale, and the heights of new buildings should generally fall within the height range of existing buildings on the block. Corner sites offer a special opportunity for providing additional building height and can serve as anchor sites for a block.

Design Guidelines

- 52-1 New, higher buildings can reinforce the established building heights along a block by stepping back upper floors that are above the average building height along the street.
- 52-2 A building that is larger than the average of buildings on the same block should break up the mass of the structure with articulation of the structure into smaller components and the creation of multiple surfaces.

Commercial

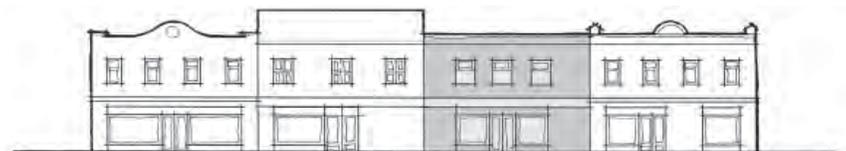
- 52-3 Appropriately scaled doors, windows, awnings, and detailing can reduce the appearance of mass.
- 52-4 Buildings on corner lots provide an opportunity for structures that exceed the average height on the block and can serve as anchor points.
- 52-5 Building heights should not block important view corridors in the neighborhood.
- 52-6 The floor-to-floor height used in older, established buildings should be maintained in new construction.
- 52-7 Encourage larger scale buildings along major arterial roads like Del Paso Boulevard and Arden Way and finer grain buildings along local streets like Canterbury Road and Boxwood.
- 52-8 Respect the adjoining residential developments with the massing and scale of new developments.

Sustainability Guidelines

- 52-9 Massing design should provide opportunities for daylighting and solar panels. Glazing should be located predominantly on the north and south sides of the structure, with glazing on the west side of the structure minimized unless the west side is the street side.



New construction and additions that deviate from the typical proportions of height, width, and depth may appear out of scale with existing buildings.



New construction and additions should respect the typical proportions of height, width, and depth.

Commercial

53 Building Facades

Design Principle

Building facades should be designed to create visually interesting buildings that offer variety along the commercial street.

Rationale

Building facades provide the interface between the built environment and the public realm. Historically, commercial districts have consisted of buildings that are one or two stories in height and cover entire lots. This pattern creates a regular rhythm of building mass and streetwalls. A streetwall of varied building facades is visually appealing and enhances the pedestrian environment. Blank walls at the ground floor level are unattractive and uninviting and should be avoided. Instead, elements should be used to create visual interest, including windows, doors, awnings and canopies, trellises, detailed parapets, or arcades.

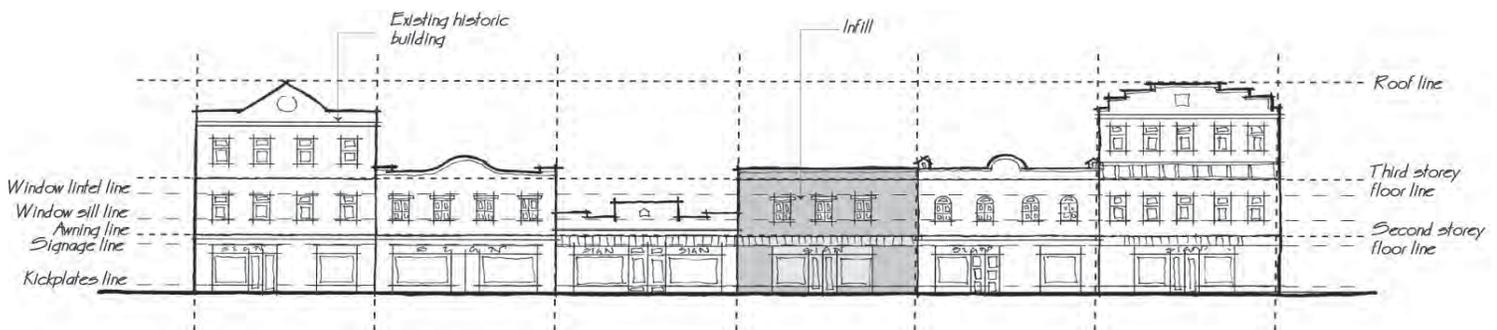
In recent decades, new buildings have increased in size and scale, creating greater challenges to creating human-scaled commercial environments. Therefore, appropriate architectural elements, such as window openings, commercial displays, frequent building entries, ornamentation, awnings and canopies, contribute to a pleasant urban streetscape.

Design Guidelines

- 53-1 Doors, windows, floor heights, cornice lines, signage, and awnings should be appropriately scaled to reduce the mass of buildings as they are experienced at the street level.
- 53-2 The primary facade of a building must face a public street and include an entry that is accessible from that street.
- 53-3 The main entrance of a building without street edge facades should open directly onto a publicly accessible walkway. This walkway should connect directly to an adjacent street sidewalk.



Avoid expansive blank walls along streets.



New construction, additions, and alterations should draw from existing architectural features.

Commercial

- 53-4 Building facades facing streets should be lined with windows, entries, and openings that provide indoor and outdoor views to the public rights-of-way and sidewalks. Continuous blank wall surfaces are not allowed.
- 53-5 Architectural features, such as display windows, pilasters, lattices, and alcoves for product display, can provide visual relief on buildings that cannot achieve continuous openings along the street and sidewalk.
- 53-6 Facades can also be articulated with insets, partial setbacks, and small pedestrian plazas, (see Section 39, "Building Orientation").
- 53-7 Solid roll-down security grates should not be used on the exterior of the building; however, they may be placed on the interior of storefront glazing or entry doors.
- 53-8 Highly reflective or dark tinted glass should be avoided.
- 53-9 Street facades of commercial buildings in areas of predominantly older buildings must have a ground floor base of a durable material, such as stone, tile, or certain types of finished concrete, where feasible.



Renovated corner entry on Del Paso Boulevard



This commercial structure is a contemporary interpretation of traditional design.

Commercial

53-10 Building facades should be designed to create a recognizable “base” and “top.” Building bases and tops can be created with variations in:

building wall thickness;

- use of special materials;
- changes in colors and materials on window trim;
- cornice treatments;
- roof overhangs with brackets; and
- use of ornamental building lines.

53-11 Utilize building elements such as cornices, lintels, sills, balconies, awnings, porches, stoops, etc to enhance building facades.

53-12 Incorporate vertical and horizontal architectural elements to mitigate long unbroken building facades.

53-13 When windows face southwest and west, frame them with protruding vertical and horizontal shading elements such as lintels, sills, etc to provide required protection from glare.

53-14 Interpret key signature elements of the Art/Streamline moderne style in modern 21st Century building context, to create extremely pedestrian friendly and arresting building facades, by grouping windows to create strong horizontal lines, using doors made of large plate glass, and incorporating materials in innovative ways.

53-15 Break the mass of some of the long and larger commercial buildings with architectural design including vertical elements and minor setbacks.

53-16 If possible, provide opportunities for seating and gathering within the building façade, minor building setback and sidewalks adjacent to the building.



New construction and additions are encouraged to use horizontal elements to create a “top” and “base” that give definition to the building and break down its elements to a more human scale.

Commercial

55 Roof Forms

Design Principle

The roof forms of new development should reflect the rooflines of established commercial structures.

Rationale

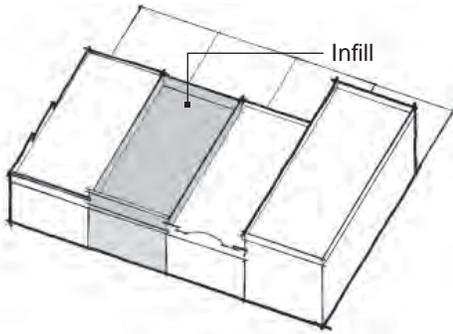
Flat rooflines are typical of much established commercial development. New commercial development should try to emulate this existing form to maintain the character of the neighborhood. However, variation in roof shapes can be desirable if compatible with existing buildings on the block.

Design Guidelines

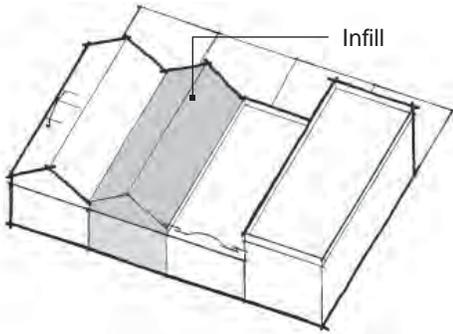
- 55-1 Articulated facade surfaces with multiple rooflines are encouraged for taller buildings to avoid an appearance of mass and to add interest.
- 55-2 Roof parapets may be used to add visual interest to flat roof lines.
- 55-3 One-story buildings should avoid the use of exaggerated, sloped roof forms.
- 55-4 Special roof forms on corner buildings are encouraged to help accentuate the corner location.
- 55-5 Mark key commercial and mixed use buildings with unique roof silhouettes to celebrate their importance and establish them as landmarks along the corridor.
- 55-6 Articulate and accentuate roofs of key residential buildings, especially at street corners and entry of developments.

Sustainability Guidelines

- 55-7 The addition of photovoltaic solar panels is encouraged to reduce energy use.
- 55-8 The use of “cool roof” materials and or “green” roofs is encouraged to reduce energy use, heat transmission, and stormwater runoff and improve the water quality of stormwater runoff.
- 55-9 Roofing options that include recycled content are encouraged.



Infill project with a flat roof similar to other nearby existing structures.



An infill project with a pitched roof in areas where nearby buildings have pitched roofs is acceptable.



Typical flat commercial roof

Commercial

56 Entry Features

Design Principle

Entry features of commercial buildings should be clearly visible to pedestrians, with a defined relationship to the street and sidewalk.

Rationale

A recessed entry helps to break up the massing of a building and makes the threshold immediately apparent to pedestrians. Decorative features, such as awnings, canopies, lighting, and signage, can also be used to clearly define and articulate an entryway.

Design Guidelines

- 56-1 Primary entries should be located on major sidewalks to provide clearly visible pedestrian access.
- 56-2 The size of the entry should be proportional to the building.
- 56-3 Secondary entries may be located at the side or rear of the building to provide access from parking areas.
- 56-4 Entries should be clearly defined with signage and architectural details.
- 56-5 In mixed-use buildings, the entrance to residential uses on the second story should be clearly defined and easily approachable from a public street or sidewalk.
- 56-6 Buildings near transit centers should provide clear pedestrian access and entry features oriented toward the transit center.
- 56-7 Maximize the building entries along the primary street façade. Emphasize the primary entry of buildings.



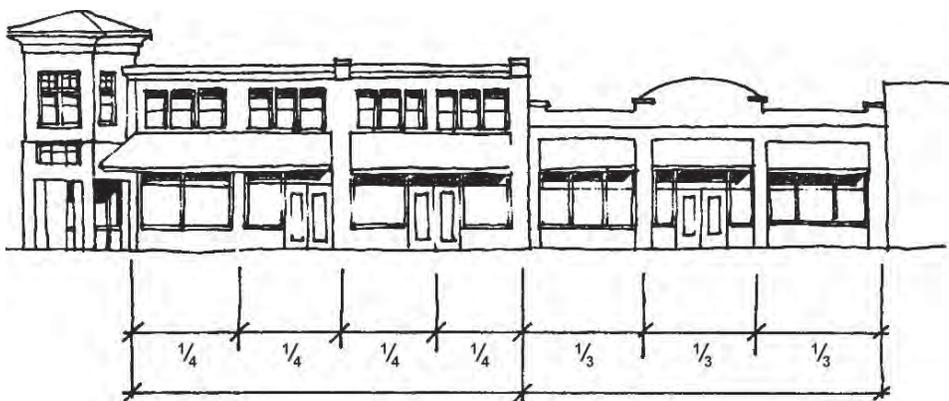
This recessed entry on the public library is typical of many older buildings on Del Paso Boulevard.



The Supper Club has a more contemporary recessed entry and door.



New Faze on Del Paso Boulevard has a dramatic corner feature with a street level entry opening onto the pedestrian way.



Building openings should maintain the proportions and spacing of other openings on the block.

Commercial



Landscaped areas add to the beauty of commercial districts.

66 Landscape Elements

Design Principle

Landscape elements should be used to foster an attractive and comfortable commercial environment.

Rationale

Parks, plazas, and town squares should be developed as the focus of commercial areas, with commercial development opening directly onto these spaces. Parks, plazas and town squares should include landscape elements, such as ornamental plants and water features, to create visual interest and an attractive, appealing environment.

Design Guidelines

- 66-1 Landscaping shall conform to all relevant City of Sacramento regulations and guidelines, including the City of Sacramento Municipal Code, "Landscaping and Paving Regulations," Chapter 124.625.
- 66-2 Plant species should be suitable for the Sacramento climate. Low-water landscaping materials are encouraged.
- 66-3 High-maintenance annuals and perennials should be used only as smaller landscape elements.
- 66-4 Anticipate the full growth of landscaping materials so that trees and shrubs do not conflict with lighting and roofs.
- 66-5 Landscaped areas are preferred over impermeable paved surfaces.
- 66-6 An automatic irrigation system must be installed to provide consistent coverage of all landscaped areas. Automatic controllers with rain shut-off valves will allow for greater water conservation. Irrigation controls should be screened from view by landscaping or other attractive site materials.
- 66-7 Turf and groundcover are more effectively irrigated with a conventional spray system. Head-to-head spray coverage is recommended. Avoid overspray onto adjacent areas.
- 66-8 A drip irrigation system is recommended for shrubs and trees to provide deeper, more even watering. Drip irrigation permits greater water conservation than a conventional spray system.
- 66-9 Bare soil should be planted or mulched to minimize run-off.
- 66-10 Include tree planting along the alley to screen and soften the impact of new multi-story buildings on existing residential, and to create a more pedestrian-friendly environment along alleyways.

Commercial

Sustainability Guidelines

- 66-11 Deciduous shade trees and shrubs should be planted, where appropriate, to shade the west and south sides of buildings and all paved areas to reduce heat transmission.
- 66-12 New planting strips located between the sidewalk and street should be a minimum of 6 feet wide to promote the health of shade trees.

Mixed-Use Development

Mixed-Use buildings are defined as multi-story residential developments with ground floor retail and commercial and condominiums and/or apartments above. Primarily located along major arterial roads and collector streets, mixed-use developments are vital in creating walk-able transit friendly communities.

Mixed-use development combines commercial development with other uses, such as office and residential. When mixed-use development is vertical in form, the commercial and office professional uses should be on the first story, with residential above. The first story should be designed as described in Guideline 42-4, with a large percentage of windows, doors, and other transparent surfaces. Upper stories should have a larger percentage of opaque surface, which can be articulated with windows, balconies, and patios.

Additional design guidelines from the multifamily and commercial chapters should be referenced as well.



Mixed-use building with ground floor retail and residential above, Orenco Station, Oregon



This mixed use building has a strong corner treatment, a clearly defined base, and an articulated facade.

Mixed-Use Development

68 Orientation & Layout

Design Principle

Mixed-Use buildings should be constructed to the front of the property line behind the sidewalk, with allowable variation in the setback to provide public amenities.

Rationale

Mixed-Use buildings in urban areas have typically been built to the front of the property line behind the sidewalk, creating a line of buildings with a consistent "streetwall" that supports a strong relationship between the building, the sidewalk, and the street. This streetwall should be reinforced by new construction and additions. The streetwall may be varied to create usable public spaces such as outdoor café dining and small plazas with seating.

Design Guidelines

- 68-1 Create a strong building edge along the street to maximize visibility of the commercial uses, which in turn provides eyes on the street.
- 68-2 Provide parking in the rear of the lot, preferably accessed by side roads, and existing alleys and new minimum 20 feet wide driveways.
- 68-3 Articulate driveways and parking lots with special paving and trees. Setback building three to five feet beyond the minimum width of the driveway to allow for small canopy trees.



Mixed-use building built to the street edge with ground floor retail and residential above.

Mixed-Use Development

69 Massing & Setbacks

Design Principle

The size and scale of mixed-use buildings should be compatible with existing development in commercial districts.

Rationale

To ensure compatibility with existing development, mixed-use buildings should appear similar in massing and scale, and the heights of new buildings should generally fall within the height range of existing buildings on the block. Corner sites offer a special opportunity for providing additional building height and can serve as anchor sites for a block.

Design Guidelines

- 69-1 Locate the majority of the building façade and commercial building uses of the building along the edge of sidewalk.
- 69-2 Stepback the massing of the building development such that it is at its highest intensity along major streets, and at its lowest when adjacent to existing residential development.



Mixed-use building with varied setbacks and massing .

Mixed-Use Development



Ground floor commercial uses should have larger windows to differentiate from the residential above.

70 Building Articulation

Design Principle

Buildings should include ground floor design details and features that provide a significant contribution to the streetwall and overall pedestrian experience.

Rationale

Public access and greater visibility will make the development financially successful but privacy will help to retain long term employees and residents.

Design Guidelines

- 70-1 Maximize the number of building entries, especially of office and retail businesses, along the façade fronting the major street. Emphasize primary entry of buildings (e.g. entrance lobby) with vertical elements.
- 70-2 Where possible, locate pedestrian-oriented entries of the upper floor residential units along the street facing façade.
- 70-3 Articulate the front facades with rhythm of windows, both along the ground floor and upper residential floors.
- 70-4 Ensure that ground floor is as transparent as possible to connect the pedestrians and the building users.



This mixed-use building has a clearly defined base, and an articulated facade.

Mixed-Use Development

71 Private Realm

Design Principle

The “private realm” refers to the buildings and land that are on privately-owned lots and parcels. The private realm should consist of private and semi-private transitional spaces between the public realm and buildings, that serve to enhance the vitality of the community.

Rationale

The design of the private realm can have a significant impact on the quality of the public realm, as private buildings typically provide the edges to streets and open spaces. The guidelines serve to guide those aspects of the private realm that have a direct affect on the surrounding public context.

Design Guidelines

- 59-5 Allow residential balconies and commercial awnings and signage to protrude four to six feet from the building edge into the sidewalk realm.
- 59-6 Landscape front setbacks of the street facing ground floor residential component of the mixed-use buildings.
- 59-7 Provide privacy for first floor office and residential units by allowing them to be three feet above the sidewalk level.

DRAFT RESOLUTION NO.

Adopted by the Sacramento City Council

**RESOLUTION APPROVING INFRASTRUCTURE
RECOMMENDATIONS CONTAINED IN THE REPORT ENTITLED “THE
NORTHEAST LINE LIGHT RAIL STATIONS PLAN PHASED
INFRASTRUCTURE RECOMMENDATIONS” AS PART OF THE
NORTHEAST LINE IMPLEMENTATION PLAN (LR09-21)**

BACKGROUND

- A. On October 15, 2002, the City Council accepted the Transit for Livable Communities (TLC) recommendations, which provided recommendations and strategies for transit-supportive development proximate to existing and future light rail stations.
- B. On July 24, 2007, the City Council accepted the Northeast Line Light Rail Stations Plan as the guiding vision for development within the quarter mile radius around the Globe, Arden/Del Paso, and Royal Oaks light rail stations. This plan consisted of design guidelines, recommended land use changes and an infrastructure assessment.
- C. On March 3, 2009, the City Council adopted the 2030 General Plan, which includes land use and policy direction to promote infill development in key opportunity areas, including commercial corridors and areas served by transit, such as the Northeast Line Light Rail Corridor.
- D. The infrastructure assessment from the Northeast Line Light Rail Stations plan has been revised to identify affordable, phased, and prioritized infrastructure improvements that will facilitate initial catalyst development and near term growth consistent with the 2030 General Plan’s growth projections for the plan area.
- E. On December 9, 2010, the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve the recommendations for future infrastructure improvements along the Northeast Line Light Rail Corridor.

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL
RESOLVES AS FOLLOWS:**

Section 1. Environmental Determination: The City Council has approved the environmental review of the Project as being within the scope of the 2030 General Plan Master EIR by Resolution No. ____.

Section 2. Based on the verbal and documentary evidence received at the hearing, the City Council approves infrastructure recommendations contained in the report entitled "The Northeast Line Light Rail Stations Plan Phased Infrastructure Recommendations" which is attached as Exhibit A of this Resolution.

Section 3. Exhibit A is a part of this Resolution.

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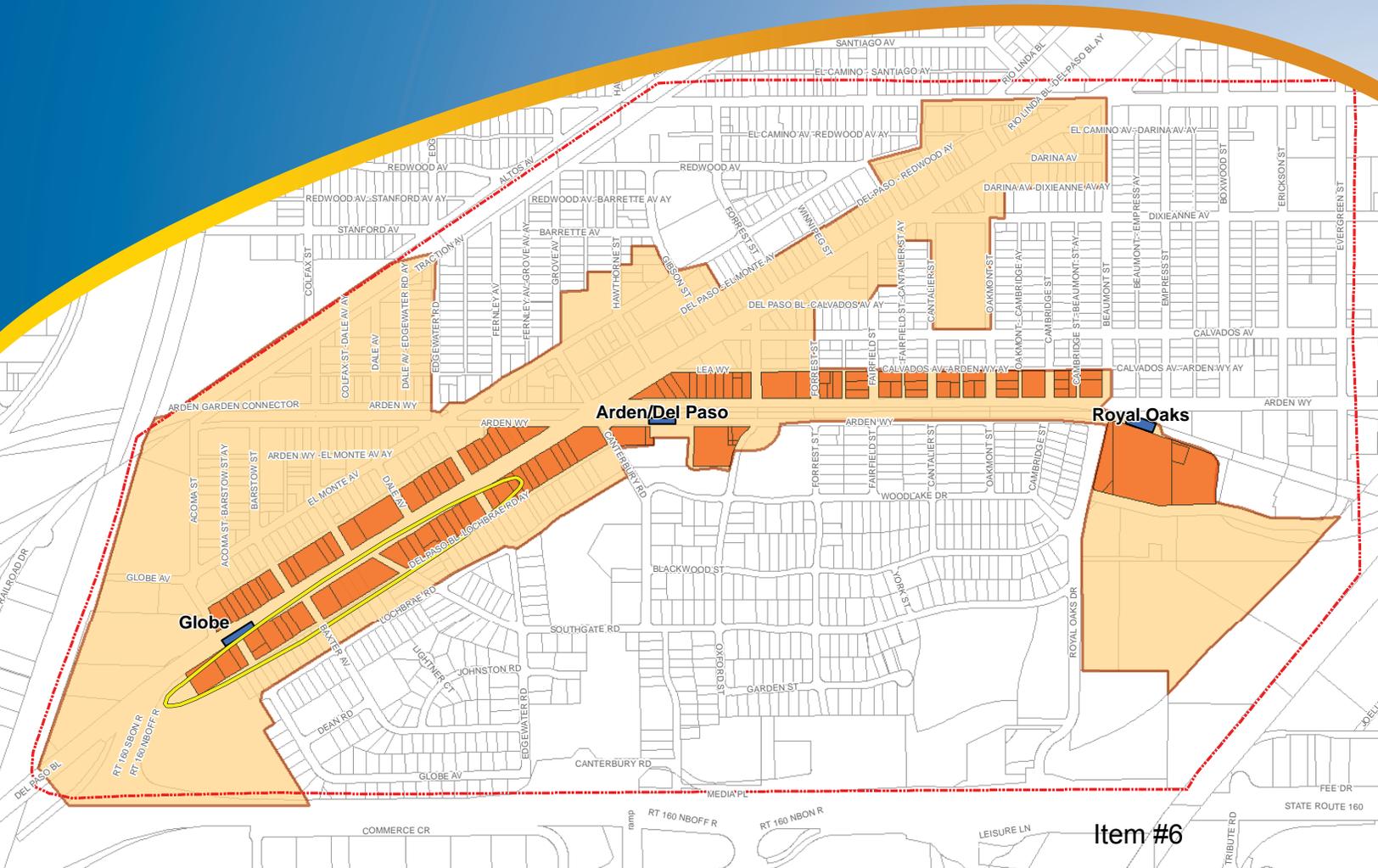
EXHIBIT A: Northeast Line Light Rail Stations Plan Phased Infrastructure Recommendations

Northeast Line Light Rail Stations Plan Exhibit A Phased Infrastructure Recommendations

DRAFT

SUBMITTED TO THE
City of Sacramento

January 2011





**FINAL REPORT FOR THE
NORTHEAST LINE IMPLEMENTATION PLAN
PHASED INFRASTRUCTURE RECOMMENDATIONS**

City Agreement #2010-0434

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JANUARY 2011 - FINAL



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Appendix A – Land Use Calculations.....	A-1 to A-5



INTRODUCTION

Project Description

The Northeast Line Light Rail Stations Plan (The Plan) was adopted by the City Council in December 2007. The Plan set forth the vision of an active, thriving transit-oriented residential and commercial neighborhood to maximize the advantages of the proximity to the existing three Light Rail Stations – Globe, Del Paso/Arden, and Royal Oaks. The Plan established proposed mixed land uses, goals, and policies that will guide future development.

The Plan study area encompassed a study impact area of roughly 570 acres, with a development focus within a quarter mile radius surrounding each of the existing three light rail stations. Newly envisioned land uses for these areas will present added infrastructure demands. Existing sanitary sewer, storm drainage, water, electrical power, telecommunications, natural gas and street improvement infrastructure capacity was analyzed and modifications proposed to adequately serve these new demands.

This report is being prepared with the goal to revisit the previously prepared infrastructure study for The Plan (dated March 2007) prepared by Nolte Associates, Inc. as a member of the Moore Iacofano & Goltman (MIG) Team. The report performs an analysis of the basic infrastructure needs and associated costs to support a realistic projection of growth by 2030 consisting of approximately 1,384 dwelling units and 112,950 square feet of commercial development. This reduced growth is located in a narrower Core Development Area focused on the Del Paso Boulevard Corridor and the Arden Way Corridor. This analysis relies on the previous infrastructure study with a focus on just the essential improvements necessary for the proposed development in the near term. The focus of the report is to identify key infrastructure investments that can be made at minimal cost to maximize development in the near term.

If the recommended infrastructure improvements specified in this report cannot be made in a timely manner, this report can serve as a guide for developers to determine which sites have the least infrastructure constraints. For such sites, there is a greater chance that infrastructure improvements can be realistically made on a project by project basis.



EXECUTIVE SUMMARY

Tier I - Catalyst Sites

There are a total of 13 parcels grouped together in four areas consisting of a total of 3.15 acres that are considered the catalyst sites for the near term development. The Sacramento Housing and Redevelopment Agency owns 8 of the parcels, Sacramento Regional Transit District owns 1, and the remaining 4 are privately owned. The anticipated development of the combined catalyst sites is a total of 189 residential dwelling units together with a total of 54,960 square feet of non-residential (ground floor commercial) development.

For the development of these catalyst sites, it is recommended to upgrade the existing water main in the Del Paso/Lochbrae Alley and reconstruct the pavement of the alley with concrete pavement. The following is a summary of the estimated cost of construction for the Tier I infrastructure improvements.

TIER I - CATALYST SITES	
A. STREETWORK	
Streetscape Improvements	\$0
Del Paso Alleys	\$346,300
B. SEWER SYSTEM	
East	\$0
West	\$0
C. DRAINAGE SYSTEM	
Shed 151 East	\$0
D. WATER DISTRIBUTION SYSTEM	
Del Paso Alley	\$477,056
TOTAL TIER I CONSTRUCTION (A-D)	\$823,356



City of Sacramento

Northeast Line Light Rail Stations Plan – Phased Infrastructure Recommendations

Tier II – Near Term Development

The remainder of the Del Paso/Arden Way Corridor area is anticipated to have potential development in the near term to selected opportunity sites along the Del Paso and Arden Way Corridors. The anticipated development of all of the Tier II areas totaling 16.10 acres is 834 residential dwelling units together with a total of 285,601 square feet of non-residential development. Significant improvements are needed for the existing drainage system to allow development near the Royal Oaks Station. Upsizing of the existing sanitary sewer system on Edgewater Road is required for the added development along Del Paso Boulevard. The following is a summary of the estimated cost of construction for the Tier II infrastructure improvements.

TIER II - DEVELOPMENT SITES

A. STREETWORK	
Streetscape Improvements	\$0
Del Paso Alleys	\$268,088
B. SEWER SYSTEM	
East	\$273,139
West	\$783,641
C. DRAINAGE SYSTEM	
Shed 151 East*	\$5,663,908
D. WATER DISTRIBUTION SYSTEM	
Del Paso Alleys	\$347,625
TOTAL TIER II CONSTRUCTION (A-D)	\$7,336,401

*The drainage system improvement necessary for the Tier II development in the vicinity of the Royal Oaks Station area assumes full construction of the piping and detention system downstream of Arden Way. Alternative mitigations and/or offsite improvement strategies (that achieve City performance requirements) of this system may be allowed on a case by case basis with approval of the City’s Department of Utilities.



City of Sacramento

Northeast Line Light Rail Stations Plan – Phased Infrastructure Recommendations

Tier III – Full Buildout

Tier III is considered the full buildout of the Northeast Line Light Rail Stations Plan area. The original infrastructure study prepared in March 2007 details the anticipated growth projection and associated infrastructure costs for the full buildout of the Plan area. The following is the cost estimate summary table from the original infrastructure study. The costs estimates are inclusive of the Tier I and Tier II estimates above. The costs provides for major street beautification on Del Paso and Arden Way and major drainage improvements as well as the improvements necessary for the additional growth capacity. For brevity, the full detail of these estimates is not included with this focused study.

A. STREETWORK	\$19,569,360
B. SEWER SYSTEM	
East	\$273,139
West	\$1,234,617
C. DRAINAGE SYSTEM	
Shed 151 East	\$7,559,047
Shed 151 West	\$4,301,480
Shed 153	\$2,337,660
D. WATER DISTRIBUTION SYSTEM	
Globe Station Area	\$1,507,359
Arden - Del Paso Station Area	\$1,466,859
Royal Oaks Station Area	\$2,715,188
TOTAL CONSTRUCTION (A-D)	\$40,964,708



LAND USE

A proposed development intensity land use analysis was prepared for the original Plan Area by the project planners Moore, Iacofano & Goltsman, Inc. (MIG). The land use analysis proposed higher intensity land uses for selected parcels surrounding the general area of each of the three existing light rail stations - Globe, Del Paso/Arden, and Royal Oaks.

It is envisioned that the sites will develop as either multi-family residential or mixed use multi-family residential/non-residential (commercial). The land use analysis proposed five different levels of development intensities (A-E) for the selected parcels. Each of the five development intensities were given a “Low” and “High” range for expected density of multi-family residential dwelling units per acre (DU/AC) and commercial floor area ratio (FAR). The following summarizes the assumptions used in the original Northeast Line Light Rail Plan analysis:

Development Intensity A:	Residential - Low = 40 DU/AC, High = 60 DU/AC Non-Residential - Low = 0.3 FAR, High = 0.4 FAR
Development Intensity B:	Residential - Low = 40 DU/AC, High = 60 DU/AC Non-Residential – None Proposed
Development Intensity C:	Residential - Low = 25 DU/AC, High = 40 DU/AC Non-Residential – None Proposed
Development Intensity D:	Residential - Low = 15 DU/AC, High = 25 DU/AC Non-Residential - Low = 0.45 FAR, High = 0.6 FAR
Development Intensity E:	Residential - Low = 25 DU/AC, High = 40 DU/AC Non-Residential - Low = 0.3 FAR, High = 0.4 FAR

Projections of the number of multi-family residential units and the gross square feet of non-residential by land use were developed. Table A-1 in Appendix A presents the results of the original land use development intensity analysis. For the purposes of the original infrastructure analysis, the Technical Advisory Committee asked that only the “High” range be analyzed.

TIER I - CATALYST SITES

For the purposes of this report, the core development area has been narrowed to encompass approximately 24.1 acres immediately adjacent to the main roadway corridors of Del Paso Boulevard and Arden Way. Within this core development area, there are a total of 13 parcels grouped together in four areas consisting of a total of 3.15 acres that are considered the catalyst sites for the near term development. The Sacramento Housing and Redevelopment Agency owns 8 of the parcels, Sacramento Regional Transit District owns 1, and the remaining 4 are privately owned.



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Northeast Line Light Rail Stations Plan – Focus Study

The four groups of lots are 0.35, 0.43, 1.00, and 1.38 in size located on the southerly side of Del Paso Boulevard between Globe Avenue and Edgewater Road. Using the assumed High level of development intensity “A” from the original study (High : Residential = 60 DU/acre & Non-Residential = FAR 0.4), this would yield a total of 189 residential dwelling units together with a total of 54,960 square feet of non-residential (ground floor commercial) development over the 3.15 acres of the catalyst sites.

TIER II – NEAR TERM DEVELOPMENT SITES

The remainder of the Del Paso Boulevard Corridor area is anticipated to have a potential of development in the near term to selected opportunity sites. The original Land Use Plan prepared by MIG identified opportunity sites along the Corridor. In addition to the sites identified above in the Tier I – Catalyst Sites, there is an additional 4.84 acres of development anticipated in these opportunity sites. Using the assumed High level of development intensity “A” from the original study (High : Residential = 60 DU/acre & Non-Residential = FAR 0.4), this would yield a total of 299 residential dwelling units together with a total of 84,410 square feet of non-residential development.

At the intersection of Del Paso and Arden Way there are three sites with a total area of 3.93 acres identified as opportunity sites. The two sites on the north side of Arden Way were assumed with a High level of development intensity “D” (High : Residential = 25 DU/acre & Non-Residential = FAR 0.6). The one larger site on the south side of Arden Way was assumed with a High level of development intensity “A” noted above. Using these densities would yield a total of 242 residential dwelling units together with a total of 73,685 square feet of non-residential development.

Near the Globe Station area on Arden Way there are three sites with a total of 7.32 acres identified as opportunity sites. The two sites on the north side of Arden Way were assumed with a High level of development intensity “E” (High : Residential = 40 DU/acre & Non-Residential = FAR 0.4). Using these densities would yield a total of 293 residential dwelling units together with a total of 127,506 square feet of non-residential development.

The total anticipated development of all of these three Tier II areas totaling 16.10 acres is 834 residential dwelling units together with a total of 285,601 square feet of non-residential development.

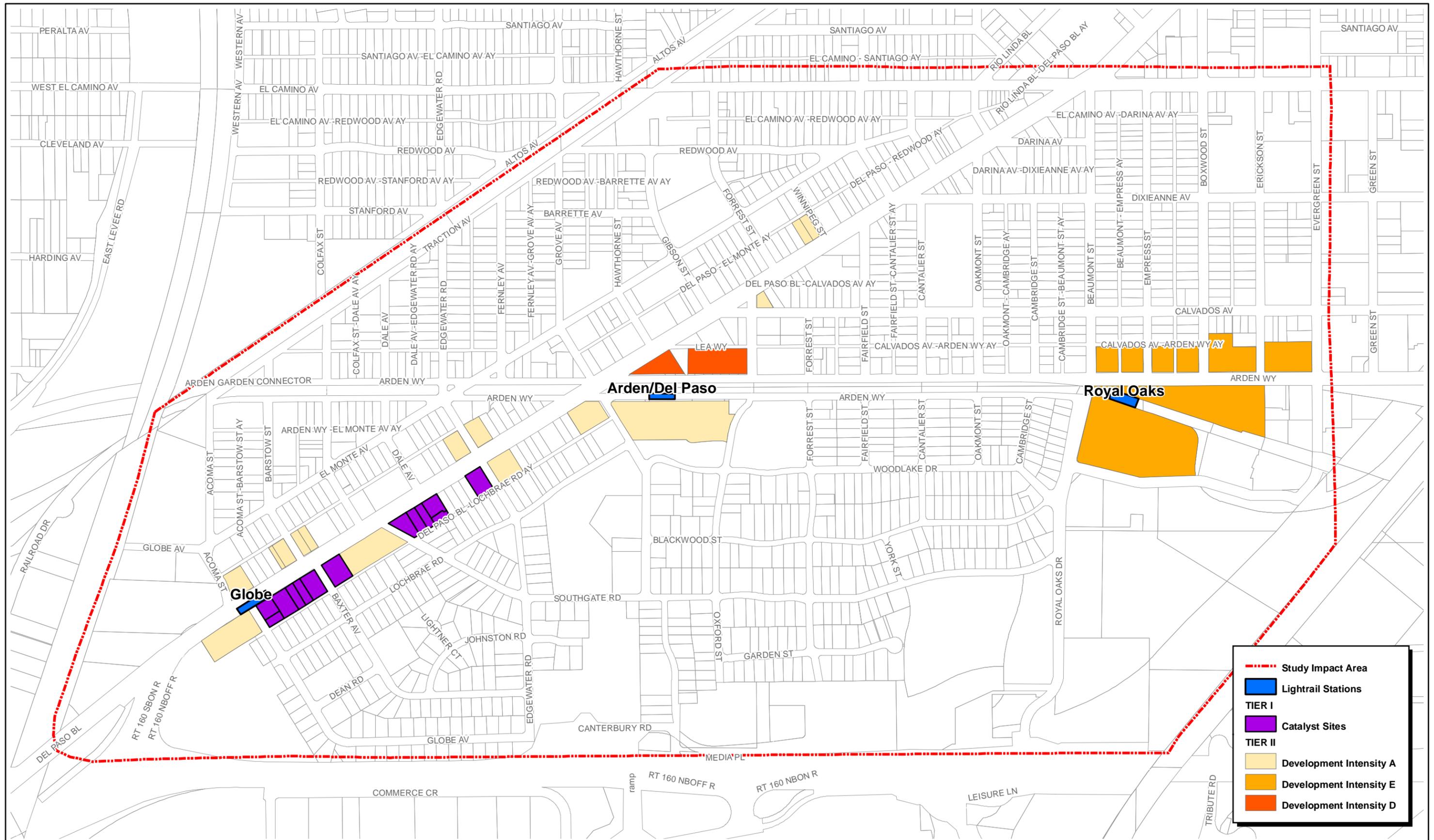
The Community Development Department (CDD) has estimated the total anticipated realistic growth projection of development within the year 2030 in the Plan area is approximately 1,384 residential dwelling units and 112,950 square feet of commercial development. This is somewhat less than the combined Tier I and Tier II projections of 1023 (= 299 + 834) for residential dwelling units.



City of Sacramento

Northeast Line Light Rail Stations Plan – Focus Study

The combined projection for non-residential of 340,561 (= 84,410 + 285,601) square feet is considerably more than the CDD's 2030 growth projection for the area. However, the non-residential uses do not have as significant of an impact on the utility system as the residential uses. The difference between the two estimates in non-residential development is roughly equivalent to only 60 multi-family residential units.



NORTHEAST LINE LIGHT RAIL STATIONS PLAN - FOCUSED LAND USE PLAN

FIGURE II - 1

January, 2011 - FINAL



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STREETSCAPE

The Circulation and Pedestrian Access portion of the Northeast Line Light Rail Stations Plan (The Plan) was prepared by Moore Iacofano & Goltsman, Inc. (MIG). Working directly with the City of Sacramento Planning and Transportation staff as well as the Northeast Line Light Rail Stations Technical Steering Committee, MIG developed a streetscape master plan for the Plan area together with a set of illustrative typical plan and sections for each of the proposed modifications to the existing streets. For the original infrastructure study, the typical street sections developed by MIG were used to develop conceptual cost estimates for The Plan.

For the purposes of this focused study, the street modifications are limited to the Del Paso Boulevard and the Arden Way modifications. The following is a discussion of the proposed improvements for each of these two Corridors.

Del Paso Boulevard: The City of Sacramento Transportation Department is currently under contract with a consultant for the design of improvements to Del Paso Boulevard within the Plan area from Highway 160 to Arden Way. The design of the improvements is being funded through a mixture of funding sources including City of Sacramento, Sacramento Area Council of Governments (SACOG), and Sacramento Housing and Redevelopment Agency (SHRA). The construction of these improvements will be funded through a mixture of sources including SACOG and Federal Grants.

The project is designed to improve the aesthetic and travel experience along Del Paso Boulevard. The improvements will largely follow the design principles set forth in the original Northeast Line Light Rail Stations Plan streetscape guidelines with a focus on the bulbout, on-street parking, tree well modifications, high visibility crosswalks, and sidewalk areas. A new traffic signal is planned at the Colfax/Southgate intersection. Underground utility work is limited to storm drainage modifications necessary to support the bulbout design. The plans do not include the Globe Light Rail Station decorative streetscape plan originally envisioned in The Plan. The total project cost is estimated at \$3.3 million with construction of the project scheduled for 2011. This project will greatly enhance the development potential of the Del Paso Boulevard Corridor portion of the Study Area by providing frontage improvements for the parcels facing the street.

Arden Way: The City's 2008 Transportation Programming Guide (TPG) has identified three projects along Arden Way within The Plan area. The following is a brief description of each project:

Arden Way - Del Paso Boulevard to Royal Oaks Drive: This is a streetscape project designed to improve both the aesthetics and travel experience along Arden Way. The project is listed as 15th on the Streetscape Enhancements (Other Corridors) list contained in the TPG.

City of Sacramento

Northeast Line Light Rail Stations Plan – Phased Infrastructure Recommendations



Arden Way - Royal Oaks Drive to Evergreen Street: This is a streetscape project designed to improve both the aesthetics and travel experience along Arden Way. The project is listed as 17th on the Streetscape Enhancements (Other Corridors) list contained in the TPG.

Arden Way - Beaumont Street to Evergreen Street: This is a project to install curb, gutter, and sidewalk improvements. The project is listed as 9th on the Pedestrian Improvements list contained in the TPG.

While all of the above three projects are contained in the TPG, none of these projects are currently funded. Conceptual cost estimates for these three projects are not available. As funding is made available, the projects will be implemented based upon their TPG rankings. Due to the significant costs of these projects, this focused study does not recommend improvements to Arden Way be included as a key infrastructure investment for the immediate needs of the Focus Study Area.

Del Paso Boulevard Alleys: While not a focus of the original infrastructure study improvements, the existing Alleys parallel to Del Paso Boulevard (El Monte/Del Paso Alley on the north and the Del Paso/Lochbrae Alley on the south) have been identified by this focused study as a potential catalyst to development along the corridor. The majority of the existing alleys are a mixture of gravel and/or deteriorated asphalt paving, with limited areas of recently paved asphalt, and a small section of concrete paving. Two sections of the existing alleys have asphalt pavement in good condition, the Del Paso/El Monte Alley between Colfax Street and Dale Avenue, and the Del Paso/Lochbrae Alley between Edgewater Road and Canterbury Road.

With development along the Corridor, access to the developing parcels will primarily be provided at the rear of the frontage lots by utilizing the existing alleys. The alley must be fully improved if it is used as the main vehicular access to a project. The development of a single parcel in the middle of a block would trigger the need to improve the pavement of the full length of the alley access to the main connecting side street. These alley improvements can be cost prohibitive to a single developing parcel in the middle of a block that would need improvements to the entire alley length out to the main street.

The City's standard for alley improvements is 6-inch concrete paving (per Design and Procedures Manual, Section 15, Plate 15-14). The concrete paving is a requirement because the typical standard 20 foot alley does not meet the minimum requirements for street width for Federal roadway maintenance funds. The concrete paving provides a longer lasting surface; however, the initial construction costs are considerably more expensive.

However, the City has allowed the use of asphalt pavement on alleys in selected areas within the City. The use of asphalt paving in the Study area may be allowed for a project on a case by case basis with approval from the City's Department of Transportation. For the purposes of this study, concrete paving has been used to provide a conservative estimate for the cost of alley pavement reconstruction.



FUTURE ACTION/RECOMMENDATION

Improvement of the alley pavement (possibly in conjunction with watermain upsizing improvements) would be a significant benefit to individual parcel development along the Del Paso Boulevard Corridor. Therefore, this study recommends reconstruction and concrete pavement of the alleys as a key infrastructure investment to serve the immediate needs of the core development area.



NORTHEAST LINE LIGHT RAIL STATIONS PLAN - STREETSCAPE PLAN

FIGURE III-1

January, 2011 - FINAL

Scale: 0 100 200 Feet

North Arrow

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Item #6



SANITARY SEWER

The Northeast Line Light Rail Stations Plan (The Plan) project area is primarily served by two separate Sewerage Collection Basins, Basins G304 & G305. The Basins are generally divided through the project area following Canterbury Road, Woodlake Drive, Cambridge Street, Beaumont Street and El Camino Avenue/Darina Avenue Alley.

For this focused study, the two main development areas along the Del Paso Boulevard and Arden Way Corridors were examined. The following is a description of the sewer improvements for each area.

Del Paso Boulevard Corridor: This area is served by the G304 collection system with the existing 10 inch main line located in Edgewater Road, the Del Paso Road/Lochbrae Alley, and the El Monte/Del Paso Alley. As identified in the original infrastructure study, the full development of this area will require significant sewer improvements to the downstream collection system. However, this included the impacts from the full development of the El Monte Triangle area.

The original study also noted that a portion of the Globe Station/Del Paso Station areas could be developed by utilizing the existing excess capacity of the existing collection system. An estimate of the existing flow rates in the system was made at the junction of the collection system pipelines at the intersection of Edgewater and Del Paso/Lochbrae. It was found that the main collection pipeline had an excess capacity at this point of approximately 207 ESDs (Equivalent Single Family Dwelling Units with an average flow rate of 400 gallons per day per unit). Using a multi-family rate of 0.75 ESDs per unit, this would potentially allow up to 276 multi-family units to be constructed before this pipeline would need to be upsized.

The total of the Tier I catalyst sites in this focus study area along the Del Paso Corridor are estimated to have 189 multi-family residential units and 54,960 square feet of non residential development. Using the above sewer generation rates, this would be a total of 153 ESDs ($= 0.75 \times 189 + 0.2/1000 \times 54,960$). This is well within the additional estimated capacity of the existing sewer system of 207 ESDs as noted above.

Based on the opportunity sites and associated land use densities presented in the Land Use Plan from the original Northwest Light Rail Stations Plan by MIG, a total of 408 multi-family residential units and 91,598 square feet of commercial development are anticipated for the Del Paso Boulevard Corridor. Note the boundary of these development estimates are limited to the area southwest of Canterbury Lane and do not include the development along Arden Way immediately east of the Del Paso/Arden intersection. Using a factor of 0.75 ESDs per multi-family unit and 0.2 ESDs per 100 square feet of commercial, this equates to a total of 324 ESDs. This means that approximately 64% ($=207/324$) of this focused study area of the Del Paso Corridor can be developed before the upgrades to the downstream system are necessary.



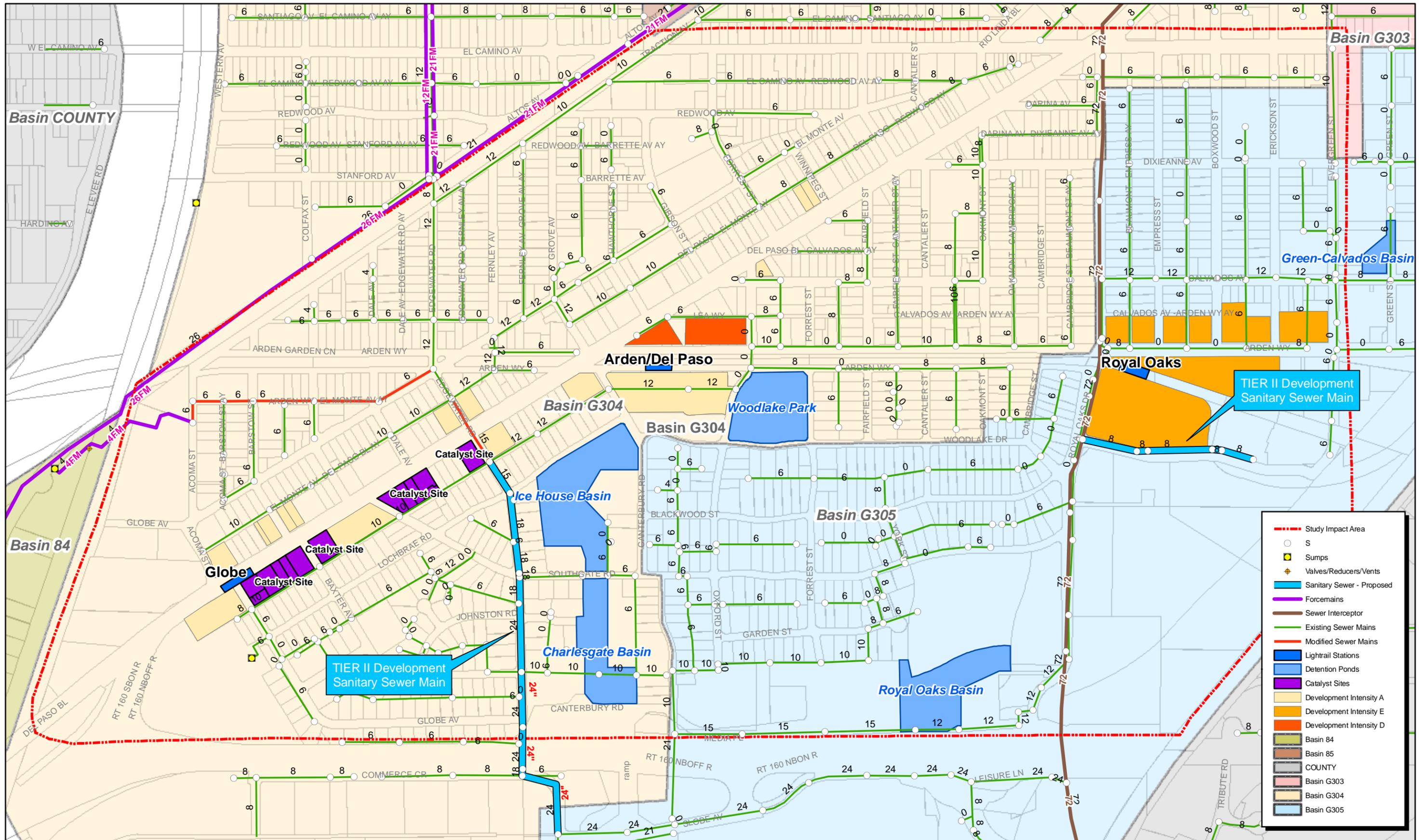
FUTURE ACTION/RECOMMENDATION

Upgrades to the downstream system are anticipated to be necessary with approximately 64% of the anticipated development along the focused study area of the Del Paso Corridor. Impact fees should be collected from both the Tier I and Tier II development to pay a fair share of the future system upgrades.

Arden Way Corridor: This area is served by the G305 collection system. As noted in the original infrastructure study, the main 12 inch collection pipeline located in Royal Oaks Drive does not have sufficient capacity for the increased flows from the proposed development around the Royal Oaks Station. Rather than upsize the entire length of the main pipeline from the Royal Oaks Drive / Evergreen Street intersection all the way to where it leaves The Plan area at Canterbury Road at Highway 160, it was recommended to create a new direct connection to the 72 inch interceptor at the Royal Oaks Drive / Evergreen Street intersection. The existing 12 inch pipeline north of the intersection and the proposed 15 inch pipeline in Evergreen Street would both be connected directly to the 72 inch interceptor at this point. This will eliminate the need to upsize a considerable length of pipeline. It will also reduce the flows into the downstream system thus allowing the G304 system modifications as noted in the original infrastructure study.

FUTURE ACTION/RECOMMENDATION

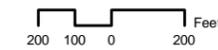
The direct connection of the existing system and the construction of the new 15 inch pipeline in Evergreen Street would be a key infrastructure investment to serve the needs of this focused study area.



NORTHEAST LINE LIGHT RAIL STATIONS PLAN- SEWER PLAN

FIGURE IV - 1

January, 2011 - FINAL



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STORM DRAINAGE

In general, the majority of the Northeast Line Light Rail Stations Plan (The Plan) area drainage system is more than 40 years old. There have been numerous reported instances of street flooding within The Plan project area. Modeling studies indicate that there will likely be localized structure flooding during the projected 100-year storm event.

The Plan project area is located primarily within two separate Drainage Basin Areas, Basins 151 and 153. These two Basins are generally divided along the Del Paso Boulevard corridor. The following is a description of the drainage improvements for each area.

Del Paso Boulevard Corridor: The Del Paso Boulevard Corridor generally drains northwesterly into the Basin 153 system to Sump 153 located near the western end of Stanford Avenue which pumps into the Natomas East Main Drainage Canal. Minor improvements to the collection system inlets are proposed with the Del Paso Boulevard Streetscape Project (Highway 160 to Arden Way).

The system improvements envisioned in the original infrastructure study were to upsize the collection system. The study utilized the Hydrology Standards contained in the Sacramento City/County Drainage Manual (December 1996) for this analysis. The peak 10-year storm flow rates were determined utilizing the 10-Year Peak Flow rates from the Sacramento Method Rainfall Zone 2 (Figure 2-14), an assumed imperviousness of 80%, and the basin sub-shed areas. Proposed pipe sizes were determined using Manning's Equation and a minimum flow rate of two feet per second in the pipe. A detailed topographic survey of the Plan Area was considered beyond the scope of the work, and therefore the pipe sizes will need to be verified when more accurate information is available during the detail design of the system.

The proposed development of this focused study is limited to the parcels immediately adjacent to the Del Paso Boulevard Corridor. The majority of these parcels are highly impervious with either existing structures or paving. Therefore the drainage characteristics are not expected to change significantly.

The 100-year flooding is limited in this Corridor to a few parcels at the northeasterly end near the Canterbury/Lochbrae intersection. It is expected that development of parcels in this area will require floodproofing of the proposed structures.

Arden Way Corridor: The Arden Way Corridor generally drains southerly into the Basin 151 system to Sump 151 located east of Lathrop Way which drains into the American River. The original infrastructure study divided the Basin 151 improvements into two basic areas, West and East. The majority of the improvements identified in the original infrastructure study for the Basin 151 East area affect the anticipated development of this focused study for the area surrounding the Royal Oaks Station. This area has significant drainage capacity and floodplain issues. Upsizing of the existing main drainage pipeline system will be very expensive. In



addition, upstream pipeline and detention improvements within the Swanston Station area are also necessary.

FUTURE ACTION/RECOMENDATION

For this focused study, we have included an estimate of the costs for the main drainage pipeline system improvements for the Basin 151 East shed from Arden Way south to the detention basin. These improvements are considered necessary for unrestricted development of this area.

Funding for these drainage improvements has not been identified at this time. The City does not currently have funds available for drainage system Capital Improvement Projects (CIPs), but is hoping to implement a City wide drainage fee to fund projects in the future.

Development in the Royal Oaks Station area may be able to provide alternative solutions to mitigate the drainage impacts. Through a more detailed hydraulic study of the system and the project impacts (considered beyond the scope of this focused study), it may be possible to provide on-site/off-site storage, piping improvements, or combination of the two that can effectively mitigate the project impacts at a reduced cost. These improvements would be reviewed and approved by the Department of Utilities on a case by case basis.

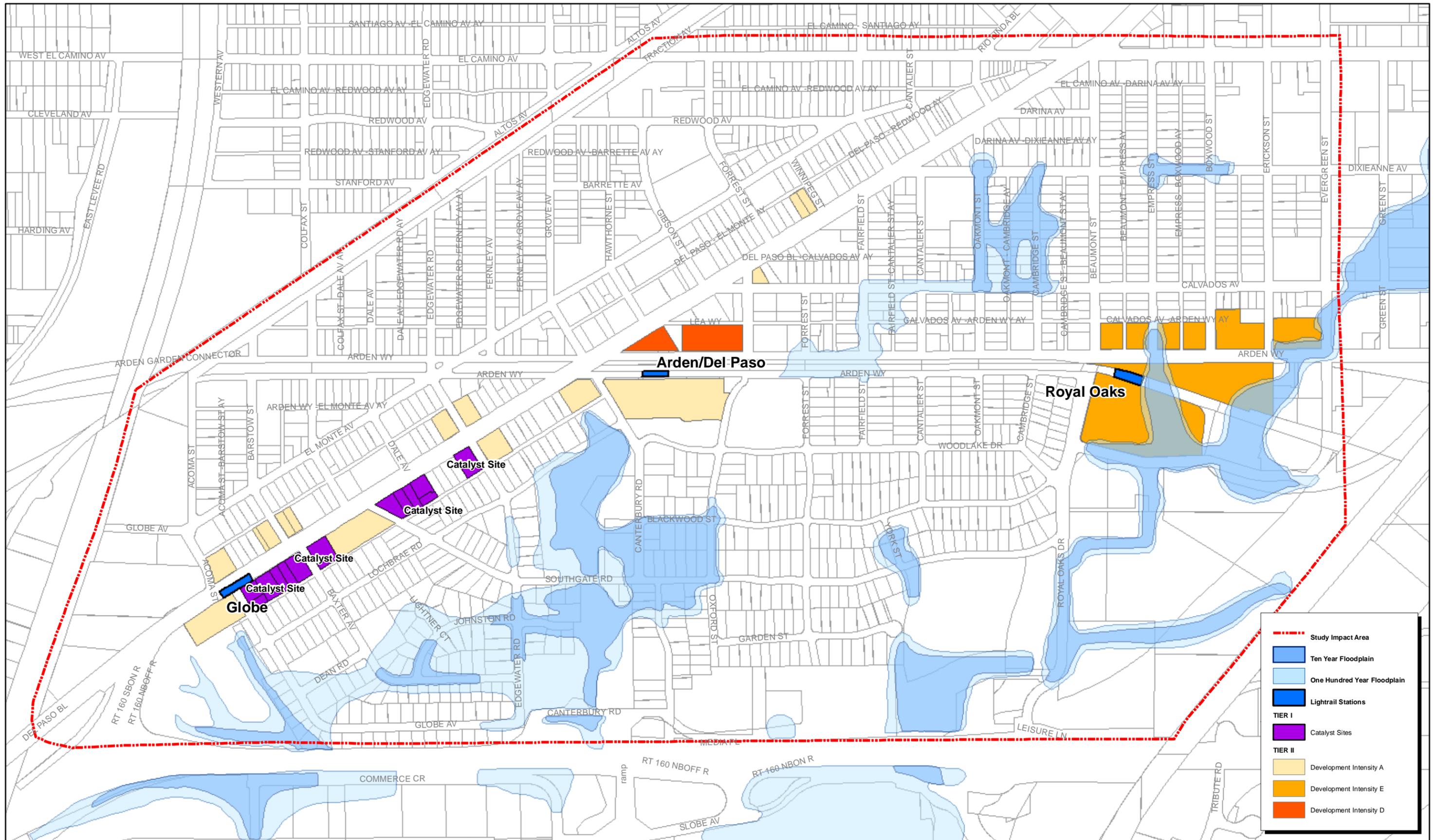
Stormwater Quality

The City of Sacramento adopted the Stormwater Quality Design Manual for the Sacramento and South Placer Regions (May 2007), a joint effort of the communities in the greater Sacramento region. This manual had not yet been adopted at the time of the completion of the original infrastructure study (March 2007). Therefore, a brief description of the water quality requirements for future development is being provided.

The manual provides locally-adapted information for design and selection of three categories of stormwater quality control measures: source control, runoff reduction and treatment control. Per the requirements, multi-family and commercial, projects greater than 1 acre are required to implement permanent post-construction treatment measures.

FUTURE ACTION/RECOMENDATION

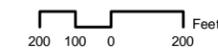
The existing storm drainage detention basins in the Basin 151 area are envisioned with future improvements to implement regional water quality treatment measures. However, until such measures are implemented, multi-family and commercial projects over 1 acre within The Plan area will be required to construct permanent post construction stormwater quality measures.

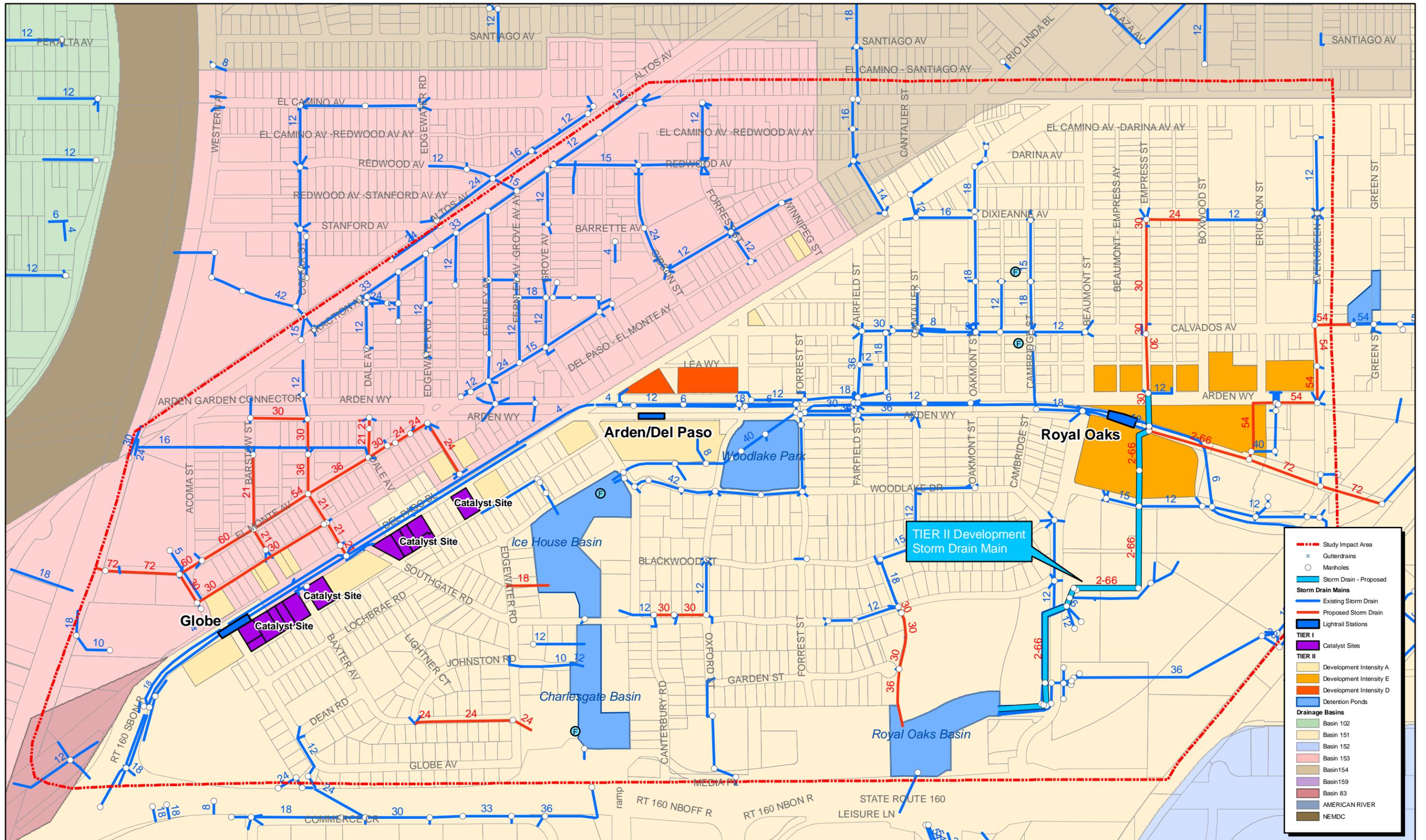


NORTHEAST LINE LIGHT RAIL STATIONS PLAN- 100 YEAR FLOODPLAIN

FIGURE V - 1

January, 2011 - FINAL





NORTHEAST LINE LIGHT RAIL STATIONS PLAN - STORM DRAIN PLAN

FIGURE V-2

January, 2011 - FINAL





WATER SUPPLY

The Northeast Line Light Rail Stations Plan (The Plan) project area is generally served by an extensive system of service mains ranging in size from 4 to 8 inches in diameter. The system in The Plan project area was generally constructed between the 1920s to 1960s.

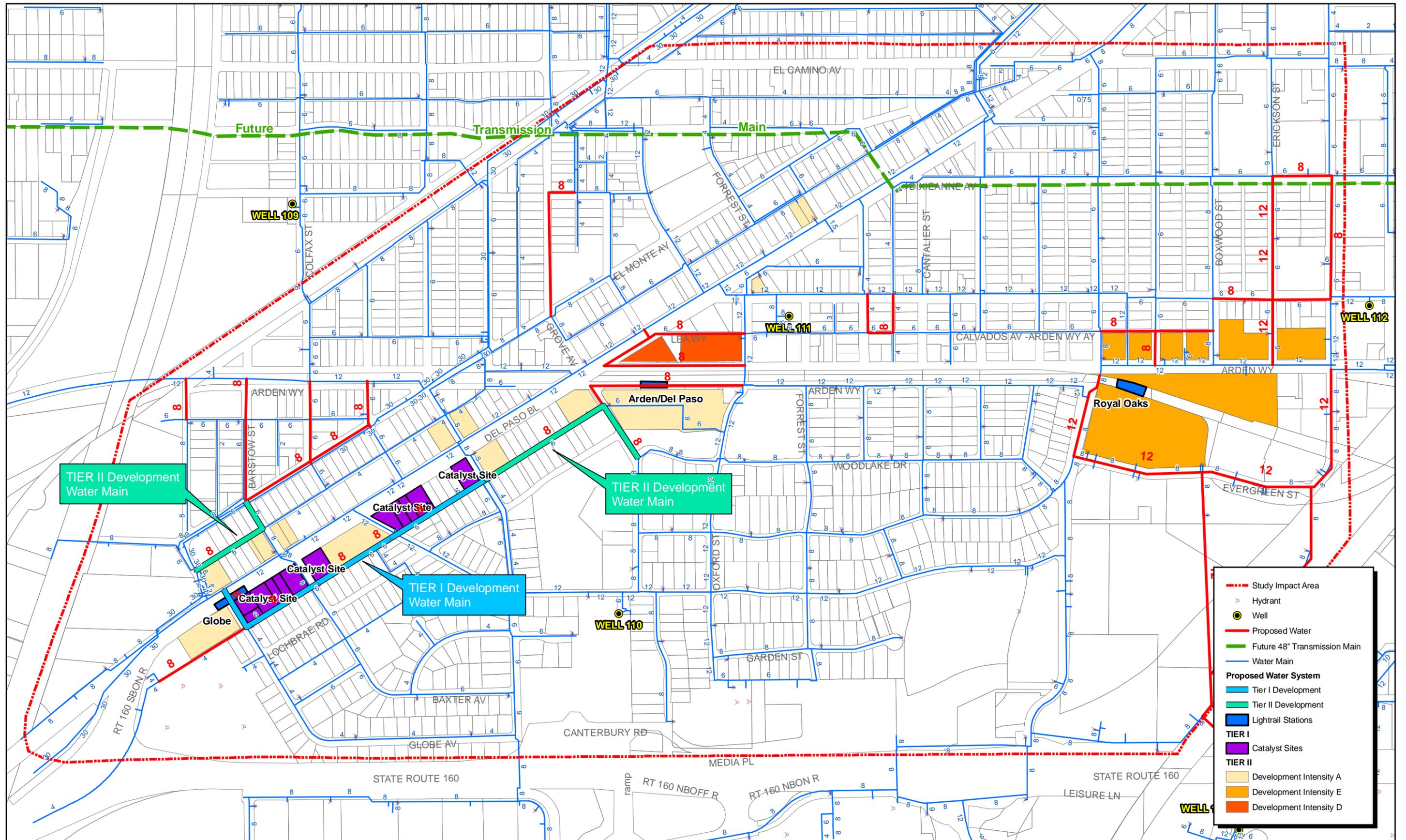
The existing corridors along Del Paso Boulevard and Arden Way are both well served by 12 inch distribution mains. However, the existing mains in the areas adjacent to these two corridors are generally undersized for the expected level of development of this focused study. The following is a description of water improvements for each area.

FUTURE ACTION/RECOMMENDATION

Del Paso Road Corridor: The focused study envisions development to occur within the immediate area adjacent to Del Paso Road. The northerly side of the Corridor is served well by an existing 12 inch watermain located in the street along the northerly frontage. However, the southerly side of the Corridor will need to upsize the existing 6 inch main located in the alley to an 8 inch main to serve the expected development water/fire needs.

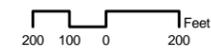
The replacement of this watermain would be a key infrastructure investment to serve the immediate needs of the focused study area. The main replacement could be performed in conjunction with the pavement replacement of the alley on this side of the Corridor.

Arden Way Corridor: The development along the Arden Way Corridor is expected to occur between Royal Oaks Drive and Evergreen Street. This area is well served by an existing 12 inch main located in Arden Way. To the south, the existing 8 inch main located in Royal Oaks Drive and Evergreen Street would serve the needs of the focus study development. However, as recommended in the original infrastructure study, this main should be upsized to a 12 inch main with further development to the south. To the north, the existing 6 inch mains should be replaced with 8 inch mains to serve the water/fire needs of the development.



NORTHEAST LINE LIGHT RAIL STATIONS PLAN - PROPOSED WATER UTILITIES

FIGURE VI - 1
January, 2011 - FINAL





NATURAL GAS

The Pacific Gas & Electric Company (PG&E) supplies natural gas to the Sacramento area. The high pressure gas system in the Northeast Line Light Rail Stations Plan Area, generally is served by a grid system throughout the Plan Area. A 12 inch transmission main is located on the west side of the Plan Area running along the old railroad/Traction Avenue corridor. An 8 inch high pressure main crosses the Plan Area connecting to the 12 inch main at Edgewater Road south to Arden Way where it turns and follows the Arden Way corridor eastward and leaves the project area at the eastern boundary.

As discussed in the original infrastructure study, PG&E has stated the existing gas infrastructure in the Northeast Line Light Rail Stations Plan Area should be adequate to serve the level of development proposed in the majority of the Globe Station and Del Paso – Arden Station areas with relatively minor additions, unless an unusually large gas user locates in the area. In that case, facilities will be upgraded as necessary in order to accommodate the user.

FUTURE ACTION/RECOMMENDATION

With the development of the Royal Oaks Station area it is anticipated that a new transmission main loop will be needed to serve the development south of the Light Rail Tracks where currently only a dead-end 2 inch main exists located in Evergreen Street as well as a 2 inch main located in Royal Oaks Drive. It is anticipated that a 6 inch transmission main will need to be looped from the Arden/Evergreen intersection along Evergreen Street to Royal Oaks and south to the existing 6 inch main at Royal Oaks/Highway 160.

The above system costs are anticipated to be provided by PG&E. As with the original infrastructure study, no costs are anticipated with the development of the core development area.



ELECTRICAL

The Sacramento Municipal Utility District (SMUD) provides electrical service to customers located within the Northeast Line Light Rail Stations Plan (The Plan) area. Power is transmitted to The Plan area by a series of 69 kilovolt (kV) transmission lines that feed overhead/underground 12 kV and 4 kV distribution systems. Within the project area, the 69kV transmission lines are located along the south side of Arden Way, along the west side of Evergreen Street, and along the El Monte-Del Paso Alley.

The Evergreen – Royal Oaks Substation is located south of Arden Way between Evergreen Street and Royal Oaks Drive. This substation is a 69-12kV substation and feeds the majority of the project area via an existing overhead/underground distribution system. The portion of The Plan area north of Arden Way is generally served by a 4kV overhead distribution system.

With the full buildout of the original land use projections for the Northeast Line Light Rail Stations Plan area, SMUD estimated that the additional electrical load from development may be approximately 15 to 23 megawatts at final buildout. With typical system improvements SMUD's distribution system should be able to handle this new load growth.

The Evergreen – Royal Oaks Substation is located on a 0.2 acre parcel just south of the light rail tracks within the middle of proposed development for the area. The development of the area around the substation will need to include proper building setbacks, screening, etc. to the station as well as the transmission lines leading to the station.

FUTURE ACTION/RECOMMENDATION

It is expected that future development in The Plan area will be served from the 12 kV distribution systems. The existing overhead distribution system will remain in order to maintain service to existing customers; however, portions of this system may be placed underground in segments as new buildings or street widening improvements are constructed. For the purposes of this focused study, it is anticipated the existing overhead system will remain in place and no undergrounding of the existing overhead systems will be required.



PROBABLE ESTIMATE OF CONSTRUCTION COSTS

The costs presented here to construct the infrastructure necessary for the Northeast Line Light Rail Stations Plan area are intended for planning level only. They include the general costs for the overall buildout of the anticipated development of The Plan area using today's dollars.

An estimate of the near term "Key Infrastructure" projects has also been prepared. This estimate is intended to provide the costs for the potential project identified as key infrastructure investments to assist development of the core development area.

This estimate is not intended to be utilized for the actual costs for specific projects. The final costs for each specific project will need to be estimated separately and could be considerably different than those shown here due to the uncertainty of the order, timing and scope of the actual development to be constructed. The estimates have been developed solely to give interested parties a magnitude of the scale of the costs of improvements.

The unit costs are based on actual costs of recent development within the Del Paso Boulevard area, planning level costs utilized by various City departments as well as engineering judgment. Final unit costs for each specific project will depend on the actual labor and materials costs for the conditions at the time of construction. These conditions might include the scope of the development and the schedule of the completion of the project.

The estimates are generally separated into the corresponding infrastructure report for the different utilities. For each utility the estimates have been divided either along the major boundaries as for sewer and storm drainage, or by the corresponding Station area. Assumptions and clarifications for the costs are noted at the bottom of the individual sheets.

The unit costs for the storm drainage improvements utilized the 1996 Master Storm Drainage report as a basis and were increased using the ENR cost index from 1996 yearly average (ENR = 5,620) to the July 2010 values (ENR = 8,865).

The Streetwork improvements are based on the conceptual street sections prepared by MIG. The unit cost per foot was developed for each section and multiplied by the length of street within the plan area. Right-of-way acquisition has not been included in the estimates since it is expected that the improvements will be constructed within the existing road right-of-way.



CONSTRUCTION COST ESTIMATE SUMMARY

TIER I - CATALYST SITES

A. STREETWORK		
Streetscape Improvements		\$0
Del Paso Alleys		\$346,300
B. SEWER SYSTEM		
East		\$0
West		\$0
C. DRAINAGE SYSTEM		
Shed 151 East		\$0
D. WATER DISTRIBUTION SYSTEM		
Del Paso Alley		\$477,056
TOTAL TIER I CONSTRUCTION (A-D)		\$823,356

TIER II - DEVELOPMENT SITES

A. STREETWORK		
Streetscape Improvements		\$0
Del Paso Alleys		\$268,088
B. SEWER SYSTEM		
East		\$273,139
West		\$783,641
C. DRAINAGE SYSTEM		
Shed 151 East		\$5,663,908
D. WATER DISTRIBUTION SYSTEM		
Del Paso Alleys		\$347,625
TOTAL TIER II CONSTRUCTION (A-D)		\$7,336,401

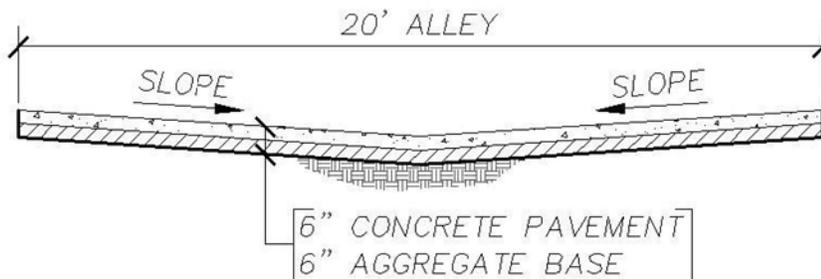


STREETWORK COSTS

DESCRIPTION	QUANTITY	UNIT OF MEASURE	UNIT PRICE	AMOUNT
A. STREETWORK				
1. Del Paso Alleys - Catalyst Sites	1,440	LF	\$142.50	\$205,200
				35% Contingency \$71,800
				Subtotal <u>\$277,000</u>
				15% Engineering \$41,600
				10% Construction Management \$27,700
Total Del Paso Alleys - Catalyst Sites				<u>\$346,300</u>
2. Del Paso Alleys - Tier II Sites	1,115	LF	\$142.50	\$158,888
				35% Contingency \$55,600
				Subtotal <u>\$214,488</u>
				15% Engineering \$32,200
				10% Construction Management \$21,400
Total Del Paso Alleys - Tier II Sites				<u>\$268,088</u>
TOTAL STREETWORK				<u>\$614,388</u>



DEL PASO ALLEY PAVEMENT



Description	Quantity	Unit of Measure	Unit Price	Amount
1. Earthwork	0.75	CY	\$30.00	\$22.50
2. 6" Concrete Pavement	20	SF	\$5.00	\$100.00
3. 6" Aggregate Base	20	SF	\$1.00	\$20.00
Total Street Costs per LF				<u>\$142.50</u>

Assumptions:

1. One foot depth of earthwork over entire cross section.
2. "V" Gutter to be placed on center of alley.



SEWER SYSTEM COSTS

WEST SEWER SYSTEM COSTS

DESCRIPTION	QUANTITY	UNIT OF MEASURE	UNIT PRICE	AMOUNT
B. SEWER SYSTEM				
1. Sewer Manhole	16	EA	\$5,980.00	\$95,680
2. Sewer Pipe, 8"	0	LF	\$80.00	\$0
3. Sewer Pipe, 10"	0	LF	\$90.00	\$0
4. Sewer Pipe, 12"	0	LF	\$105.00	\$0
5. Sewer Pipe, 15"	0	LF	\$120.00	\$0
6. Sewer Pipe, 18"	0	LF	\$130.00	\$0
7. Sewer Pipe, 21"	1,635	LF	\$140.00	\$228,900
8. Sewer Pipe, 24"	420	LF	\$150.00	\$63,000
9. Sewer Pipe, 27"	480	LF	\$160.00	\$76,800
9. Service	0	EA	\$500.00	\$0
		Subtotal		\$464,380
		35% Contingency		\$162,533
		Subtotal		\$626,913
		15% Engineering		\$94,037
		10% Construction Management		\$62,691
SEWER SYSTEM SUBTOTAL				\$783,641



SEWER SYSTEM COSTS

EAST SEWER SYSTEM COSTS

DESCRIPTION	QUANTITY	UNIT OF MEASURE	UNIT PRICE	AMOUNT
B. SEWER SYSTEM				
1. Sewer Manhole	7	EA	\$5,980.00	\$41,860
2. Sewer Pipe, 8"	0	LF	\$80.00	\$0
3. Sewer Pipe, 10"	0	LF	\$90.00	\$0
4. Sewer Pipe, 12"	0	LF	\$105.00	\$0
5. Sewer Pipe, 15"	1,000	LF	\$120.00	\$120,000
6. Sewer Pipe, 18"	0	LF	\$130.00	\$0
7. Sewer Pipe, 21"	0	LF	\$140.00	\$0
8. Sewer Pipe, 24"	0	LF	\$150.00	\$0
9. Service	0	EA	\$500.00	\$0
		Subtotal		\$161,860
		35% Contingency		\$56,651
		Subtotal		<u>\$218,511</u>
		15% Engineering		\$32,777
		10% Construction Management		\$21,851
		SEWER SYSTEM SUBTOTAL		<u>\$273,139</u>



DRAINAGE SYSTEM COSTS - SHED 151 EAST

DESCRIPTION	QUANTITY	UNIT OF MEASURE	UNIT PRICE	AMOUNT
C. DRAINAGE SYSTEM				
1. Storm Drain Pipe, 18"	0	LF	\$96.00	\$0
2. Storm Drain Pipe, 24"	0	LF	\$130.00	\$0
3. Storm Drain Pipe, 30"	129	LF	\$160.00	\$20,640
4. Storm Drain Pipe, 36"	0	LF	\$195.00	\$0
5. Storm Drain Pipe, 48"	0	LF	\$265.00	\$0
6. Storm Drain Pipe, 54"	0	LF	\$310.00	\$0
7. Storm Drain Pipe, 60"	0	LF	\$350.00	\$0
8. Storm Drain Pipe, 66"	3,110	LF	\$395.00	\$1,228,450
9. Storm Drain Pipe, 72"	0	LF	\$435.00	\$0
10. Manhole, 12"-24"	0	EA	\$3,200.00	\$0
11. Manhole, 30"-36"	0	EA	\$3,175.00	\$0
12. Manhole, 42"-48"	1	EA	\$3,800.00	\$3,800
13. Manhole, 54"-60"	0	EA	\$4,150.00	\$0
14. Manhole, 66"-72"	10	EA	\$4,650.00	\$46,500
Detention Basin Improvements				
15. Northern West Basin	0	EA	\$1,415,500.00	\$0
Detention Basin Improvements				
16. Southern West Basin	0	EA	\$1,158,541.88	\$0
Detention Basin Improvements				
17. East Basin	1	EA	\$2,057,000.00	\$2,057,000
18. Flood Proofing (House)	0	EA	\$39,500.00	\$0
19. Flood Proofing (Building)	0	EA	\$78,900.00	\$0
Subtotal				\$3,356,390
35% Contingency				\$1,174,737
Subtotal				<u>\$4,531,127</u>
15% Engineering				\$679,669
10% Construction Management				\$453,113
TOTAL STORM DRAIN SHED 151 EAST				\$5,663,908

Unit prices derived by applying the McGraw-Hill Construction ENR (July, 2010 - 8,865, 1996 - 5620) to the City of Sacramento's 1996 sump 151 Storm Drain Master Plan.



WATER DISTRIBUTION SYSTEM COSTS

DESCRIPTION	QUANTITY	UNIT OF MEASURE	UNIT PRICE	AMOUNT
D.1 WATER DISTRIBUTION SYSTEM - CATALYST SITES				
1. Water, 8" (Incl. fittings)	2,427	LF	\$100.00	\$242,700
2. Fire Hydrant	8	EA	\$5,000.00	\$40,000
				Subtotal
				\$282,700
				35% Contingency
				\$98,945
				Subtotal and Contingency
				\$381,645
				15% Engineering
				\$57,247
				10% Construction Management
				\$38,165
				Total Water Distribution System - Catalyst Sites
				<u>\$477,056</u>
D.2 WATER DISTRIBUTION SYSTEM - TIER II				
1. Water, 8" (Incl. fittings)	1,760	LF	\$100.00	\$176,000
2. Fire Hydrant	6	EA	\$5,000.00	\$30,000
				Subtotal
				\$206,000
				35% Contingency
				\$72,100
				Subtotal and Contingency
				\$278,100
				15% Engineering
				\$41,715
				10% Construction Management
				\$27,810
				Total Water Distribution System - Tier II Sites
				<u>\$347,625</u>
				TOTAL WATER DISTRIBUTION SYSTEM
				<u>\$824,681</u>

APPENDIX A
LAND USE CALCULATIONS



APPENDIX A – LAND USE CALCULATIONS

**Table A-1
Proposed Land Use Development Intensity**

	Total Developable Area (Acres)	Residential (Dwelling Units)		Non-Residential (Acres)		Non-Residential (Square Feet)	
		<i>Low</i>	<i>High</i>	<i>Low</i>	<i>High</i>	<i>Low</i>	<i>High</i>
Globe Station							
Development Intensity A	5.55	222	333	1.67	2.22	72,567	96,756
Development Intensity B	6.89	276	413	0.00	0.00	0	0
Development Intensity C	9.66	242	387	0.00	0.00	0	0
Development Intensity D							
Development Intensity E							
Total for Globe Station	22.11	739	1,133	1.67	2.22	72,567	96,756
Del Paso - Arden Station							
Development Intensity A	5.34	214	320	1.60	2.14	69,763	93,017
Development Intensity B							
Development Intensity C	4.06	102	162	0.00	0.00	0	0
Development Intensity D	1.70	25	42	0.76	1.02	33,294	44,392
Development Intensity E							
Total for Del Paso/Arden Station	11.10	341	525	2.37	3.15	103,057	137,409
Royal Oaks Station							
Development Intensity A							
Development Intensity B	27.69	1,107	1,661	0.00	0.00	0	0
Development Intensity C	3.39	85	136	0.00	0.00	0	0
Development Intensity D							
Development Intensity E	13.13	328	525	3.94	5.25	171,579	228,772
Total for Royal Oaks Station	44.21	1,521	2,322	3.94	5.25	171,579	228,772
Total For All Stations	77.41	2,600	3,980	7.97	10.63	347,203	462,937



City of Sacramento

Northeast Line Light Rail Stations Plan - Phased Infrastructure Recommendations

Globe Station

AREA (SQ FT)	AREA (ACS)	APN	LANDUSE_DE	ZONE	Dev_Type	Res_Low	Res_High	NonRes_Low	NonRes_High	NonRes_Low	NonRes_High
3706.26371	0.085	275-0161-008	SMALL RETAIL	C-2-SPD A		3	5	0.026	0.034	1112	1483
11199.10269	0.257	275-0161-007	LIGHT INDUSTRIAL	C-2-SPD A		10	15	0.077	0.103	3360	4480
26674.88007	0.612	275-0260-008	SMALL RETAIL	C-2	A	24	37	0.184	0.245	8002	10670
10754.79895	0.247	275-0165-018		C-2-SPD A		10	15	0.074	0.099	3226	4302
12508.50801	0.287	275-0161-014	HEAVY INDUSTRIAL	C-2-SPD A		11	17	0.086	0.115	3753	5003
14989.85393	0.344	275-0163-006	CEMETARY/MORTUARY	C-2-SPD A		14	21	0.103	0.138	4497	5996
7526.52037	0.173	275-0162-001	VACANT/OFFICE	C-2-SPD A		7	10	0.052	0.069	2258	3011
15093.71871	0.347	275-0162-004	VETERINARIAN	C-2-SPD A		14	21	0.104	0.139	4528	6037
9168.49491	0.210	275-0165-003		C-2-SPD A		8	13	0.063	0.084	2751	3667
5098.83856	0.117	275-0163-002	LOW RISE APARTMENT < 4 STORIES	C-2-SPD A		5	7	0.035	0.047	1530	2040
7304.59349	0.168	275-0164-013	LIGHT INDUSTRIAL	C-2-SPD A		7	10	0.050	0.067	2191	2922
10050.71992	0.231	275-0163-001	VACANT/RETAIL	C-2-SPD A		9	14	0.069	0.092	3015	4020
14794.43068	0.340	275-0163-003	VACANT/OFFICE	C-2-SPD A		14	20	0.102	0.136	4438	5918
7527.97401	0.173	275-0163-005		C-2-SPD A		7	10	0.052	0.069	2258	3011
7533.13738	0.173	275-0165-002		C-2-SPD A		7	10	0.052	0.069	2260	3013
7606.94303	0.175	275-0163-004	VACANT/OFFICE	C-2-SPD A		7	10	0.052	0.070	2282	3043
18531.29888	0.425	275-0165-019	PARKING LOT	C-2-SPD A		17	26	0.128	0.170	5559	7413
6621.93193	0.152	275-0161-013	LIGHT INDUSTRIAL	C-2-SPD A		6	9	0.046	0.061	1987	2649
7256.27135	0.167	275-0164-014	LIGHT INDUSTRIAL	C-2-SPD A		7	10	0.050	0.067	2177	2903
603.60259	0.014	275-0165-017		C-2	A	1	1	0.004	0.006	181	241
7568.00765	0.174	275-0122-008	VACANT/RETAIL	C-2-SPD A		7	10	0.052	0.069	2270	3027
22346.51844	0.513	275-0165-016	SMALL RETAIL	C-2-SPD A		21	31	0.154	0.205	6704	8939
7422.50529	0.170	275-0122-007	VACANT/RETAIL	C-2-SPD A		7	10	0.051	0.068	2227	2969
246916.43919	5.668	275-0111-006		M-1-SPD B		227	340				
8029.47329	0.184	275-0161-016	VACANT/INDUSTRIAL	M-1-SPD B		7	11				
7449.57396	0.171	275-0164-002	LIGHT INDUSTRIAL	C-2-SPD B		7	10				
7617.04243	0.175	275-0161-017	VACANT/INDUSTRIAL	M-1-SPD B		7	10				
7406.16140	0.170	275-0161-004	VACANT/RECREATIONAL	M-1-SPD B		7	10				
7378.88234	0.169	275-0161-006	RESIDENTIAL/SINGFAM/SUBDIV	M-1-SPD B		7	10				
7371.64339	0.169	275-0164-001	LIGHT INDUSTRIAL	C-2-SPD B		7	10				
7852.60653	0.180	275-0122-004	VACANT/OFFICE	C-2-SPD B		7	11				
5379.77286	0.124	275-0121-002	VACANT/RESIDENTIAL	C-2-SPD C		3	5				
3343.54527	0.077	275-0113-010	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD C		2	3				
21707.96949	0.498	275-0114-015	VACANT/RETAIL	C-2-SPD C		12	20				
4435.26015	0.102	275-0113-012	RESIDENTIAL/FOURPLEX	C-2-SPD C		3	4				
7212.03949	0.166	275-0114-006	VACANT/RETAIL	C-2-SPD C		4	7				
7059.86940	0.162	275-0113-004	HEAVY INDUSTRIAL	C-2-SPD C		4	6				
9296.62141	0.213	275-0112-027	LIGHT INDUSTRIAL	M-1-SPD C		5	9				
9494.73286	0.218	275-0114-013	LIGHT INDUSTRIAL	C-2-SPD C		5	9				
6751.09303	0.155	275-0113-015	VACANT/RECREATIONAL	M-1-SPD C		4	6				
7454.36355	0.171	275-0163-007	RESIDENTIAL/SINGFAM/SUBDIV	R-1	C	4	7				
6307.29539	0.145	275-0121-001	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD C		4	6				
42756.04344	0.982	275-0112-001	VACANT/INDUSTRIAL	M-1-SPD C		25	39				
7108.21423	0.163	275-0112-017	LIGHT INDUSTRIAL	M-1-SPD C		4	7				
10168.77658	0.233	275-0113-023	RESIDENTIAL/SINGFAM/NONSUB	C-2-SPD C		6	9				
3510.46015	0.081	275-0113-013	VACANT/RECREATIONAL	M-1-SPD C		2	3				
4298.67769	0.099	275-0113-014	VACANT/RECREATIONAL	M-1-SPD C		2	4				
7486.40286	0.172	275-0113-003	HEAVY INDUSTRIAL	C-2-SPD C		4	7				
7041.35668	0.162	275-0114-005	VACANT/RETAIL	C-2-SPD C		4	6				
7118.10622	0.163	275-0113-005	HEAVY INDUSTRIAL	C-2-SPD C		4	7				
6645.73737	0.153	275-0112-007	LIGHT INDUSTRIAL	M-1-SPD C		4	6				
10242.45544	0.235	275-0113-024	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD C		6	9				
13424.83972	0.308	275-0112-005	VACANT/INDUSTRIAL	M-1-SPD C		8	12				
10597.17432	0.243	275-0112-026	LIGHT INDUSTRIAL	M-1-SPD C		6	10				
6848.42017	0.157	275-0121-003	CITY	C-2-SPD C		4	6				
10037.81656	0.230	275-0114-014	VACANT/INDUSTRIAL	C-2-SPD C		6	9				
7119.03007	0.163	275-0113-011	RESIDENTIAL/DUPLEX	C-2-SPD C		4	7				
8912.89822	0.205	275-0112-011	LIGHT INDUSTRIAL	M-1-SPD C		5	8				
6738.71376	0.155	275-0112-002	LIGHT INDUSTRIAL	M-1-SPD C		4	6				
7604.66902	0.175	275-0163-009	VACANT/RESIDENTIAL	R-1	C	4	7				
6992.89030	0.161	275-0112-015	HEAVY INDUSTRIAL	M-1-SPD C		4	6				
8758.77745	0.201	275-0114-007	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD C		5	8				
6569.20436	0.151	275-0112-004	LIGHT INDUSTRIAL	M-1-SPD C		4	6				
6473.26218	0.149	275-0112-008	RESIDENTIAL/SINGFAM/SUBDIV	M-1-SPD C		4	6				
6694.47535	0.154	275-0112-003	LIGHT INDUSTRIAL	M-1-SPD C		4	6				
6498.42533	0.149	275-0112-006	VACANT/RECREATIONAL	M-1-SPD C		4	6				
6845.83050	0.157	275-0113-016	HEAVY INDUSTRIAL	M-1-SPD C		4	6				
6946.31092	0.159	275-0112-020	HEAVY INDUSTRIAL	M-1-SPD C		4	6				
53371.06382	1.225	275-0113-022	LIGHT INDUSTRIAL	M-1-SPD C		31	49				
4713.01176	0.108	275-0112-009	RESIDENTIAL/SINGFAM/SUBDIV	M-1-SPD C		3	4				
7219.26005	0.166	275-0114-004	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD C		4	7				
4633.30714	0.106	275-0112-010	LIGHT INDUSTRIAL	M-1-SPD C		3	4				
7036.13136	0.162	275-0112-021	HEAVY INDUSTRIAL	M-1-SPD C		4	6				
22133.27586	0.508	275-0112-025	LIGHT INDUSTRIAL	M-1-SPD C		13	20				
22.10510						739	1133	1.7	2.2	72566.7	96755.6



City of Sacramento

Northeast Line Light Rail Stations Plan - Phased Infrastructure Recommendations

Del Paso / Arden Station

AREA	AREA (ACS)	APN	LANDUSE_DE	ZONE	Dev_Type	Res_Low	Res_High	NonRes_Low	NonRes_High	NonRes_Low	NonRes_High
	46241.36821	1.06 275-0134-008	CITY	R-1	A	42	64	0.318	0.425	13872	18497
	7790.11569	0.18 275-0124-009	RESTAURANT	C-2-SPD	A	7	11	0.054	0.072	2337	3116
	614.33386	0.01 275-0134-007	CITY	R-1	A	1	1	0.004	0.006	184	246
	15458.50131	0.35 275-0125-028	VACANT/OFFICE	C-2-SPD	A	14	21	0.106	0.142	4638	6183
	7394.34622	0.17 275-0125-001	VACANT/OFFICE	C-2-SPD	A	7	10	0.051	0.068	2218	2958
	695.35928	0.02 275-0134-006	CITY	R-1	A	1	1	0.005	0.006	209	278
	18577.48239	0.43 275-0134-003	CITY	R-1	A	17	26	0.128	0.171	5573	7431
	24196.07864	0.56 275-0134-010	CITY	R-1	A	22	33	0.167	0.222	7259	9678
	7699.67567	0.18 275-0093-005	SMALL RETAIL	C-2-SPD	A	7	11	0.053	0.071	2310	3080
	7024.02676	0.16 275-0093-004	SMALL RETAIL	C-2-SPD	A	6	10	0.048	0.064	2107	2810
	6059.36712	0.14 275-0095-016	SMALL RETAIL	C-2-SPD	A	6	8	0.042	0.056	1818	2424
	1946.89661	0.04 275-0134-004	CITY	R-1	A	2	3	0.013	0.018	584	779
	49591.96653	1.14 275-0134-012	CITY	R-1	A	46	68	0.342	0.455	14878	19837
	3654.38386	0.08 275-0134-011	CITY	R-1	A	3	5	0.025	0.034	1096	1462
	27839.70489	0.64 275-0125-029	RESTAURANT	C-2-SPD	A	26	38	0.192	0.256	8352	11136
	7759.33631	0.18 275-0124-010	RESTAURANT	C-2-SPD	A	7	11	0.053	0.071	2328	3104
	75384.48272	1.73 275-0085-013	VACANT/RESIDENTIAL	C-2	C	43	69				
	4167.59998	0.10 275-0084-016	VACANT/RETAIL	C-2-SPD	C	2	4				
	6494.04387	0.15 275-0095-007	RESIDENTIAL/SINGFAM/SUBDIV	R-1	C	4	6				
	7685.36187	0.18 275-0125-023	PARKING LOT	R-3	C	4	7				
	1057.25676	0.02 275-0082-001	VACANT/RETAIL	C-2-SPD	C	1	1				
	6502.17503	0.15 275-0145-012	RESIDENTIAL/DUPLEX	R-1	C	4	6				
	4251.65894	0.10 275-0125-024	PARKING LOT	R-3	C	2	4				
	6337.44124	0.15 275-0085-009	VACANT/RESIDENTIAL	R-1	C	4	6				
	7573.76036	0.17 275-0125-022	PARKING LOT	R-3	C	4	7				
	6317.06702	0.15 275-0085-010	VACANT/RESIDENTIAL	R-1	C	4	6				
	6649.49630	0.15 275-0028-004	VACANT/RESIDENTIAL	R-1	C	4	6				
	6519.86828	0.15 275-0145-013	VACANT/RESIDENTIAL	R-1	C	4	6				
	10114.43233	0.23 275-0091-001	VACANT/RETAIL	R-1	C	6	9				
	6459.11745	0.15 275-0085-011	VACANT/RESIDENTIAL	R-1	C	4	6				
	21353.05374	0.49 275-0125-016	LOW RISE APARTMENT < 4 STORIES	R-3	C	12	20				
	6132.34581	0.14 275-0131-014	OFFICE GENERAL	C-2-SPD	D	2	4	0.063	0.084	2760	3679
	9591.12809	0.22 275-0131-020	SERVICE STATION	C-2-SPD	D	3	6	0.099	0.132	4316	5755
	1665.19067	0.04 275-0131-008	NO USE	C-2-SPD	D	1	1	0.017	0.023	749	999
	5720.36923	0.13 275-0131-009	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD	D	2	3	0.059	0.079	2574	3432
	6036.53224	0.14 275-0131-017	LIGHT INDUSTRIAL	C-2-SPD	D	2	3	0.062	0.083	2716	3622
	7659.94704	0.18 275-0131-007	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD	D	3	4	0.079	0.106	3447	4596
	7146.04615	0.16 275-0131-011	RESIDENTIAL CONVERION TO OFFICE	C-2-SPD	D	2	4	0.074	0.098	3216	4288
	7454.07982	0.17 275-0131-010	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD	D	3	4	0.077	0.103	3354	4472
	7499.94778	0.17 275-0131-013	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD	D	3	4	0.077	0.103	3375	4500
	7631.19020	0.18 275-0131-016	SMALL RETAIL	C-2-SPD	D	3	4	0.079	0.105	3434	4579
	7449.56573	0.17 275-0131-012	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD	D	3	4	0.077	0.103	3352	4470
	11.09725					341	525	2.4	3.2	103056.7	137409.0



City of Sacramento

Northeast Line Light Rail Stations Plan - Phased Infrastructure Recommendations

Royal Oaks Station

APN	LANDUSE DESIGNATION	ZONE	Dev_Type	Res_Low	Res_High	NonRes_Low	NonRes_High	NonRes_L	NonRes_High
275-0240-092	OFFICE LARGE SINGLE TENANT	OB-LI	B	125	188				
275-0240-088	STATE	OB-LI	B	20	31				
277-0144-022	STATE	M-1-LI	B	89	133				
275-0240-087	STATE	OB-LI	B	225	337				
277-0134-023	LARGE RETAIL	M-1	B	19	29				
275-0240-074	LARGE RETAIL	OB-LI	B	70	105				
275-0240-094	HEAVY INDUSTRIAL	OB-LI	B	56	83				
275-0240-089	STATE	OB-LI	B	42	63				
275-0240-045	POST OFFICE	OB-LI	B	101	152				
277-0134-024	SPECIAL DISTRICT	M-1	B	22	33				
275-0240-052	POST OFFICE	OB-LI	B	91	137				
275-0240-051	STATE	OB-LI	B	200	299				
275-0240-029	STATE	OB-LI	B	47	71				
277-0132-006	HEAVY INDUSTRIAL	M-1	C	3	4				
277-0131-012	LOW RISE APARTMENT < 4 STORIES	R-1	C	4	6				
277-0133-006	INDUSTRIAL/MULTI-TENANT	M-1	C	5	9				
277-0133-002	RESIDENTIAL/SINGFAM/SUBDIV	M-1	C	3	4				
277-0133-008	INDUSTRIAL/MULTI-TENANT	M-1	C	3	5				
277-0073-009	VACANT/INDUSTRIAL	M-1	C	39	62				
275-0104-023	RESIDENTIAL/SINGFAM/SUBDIV	R-1	C	3	5				
275-0104-024	RESIDENTIAL/SINGFAM/SUBDIV	R-1	C	3	5				
277-0133-003	INDUSTRIAL/MULTI-TENANT	M-1	C	4	7				
277-0132-005	HEAVY INDUSTRIAL	M-1	C	3	4				
277-0072-027	RESIDENTIAL/SINGFAM/SUBDIV	R-1	C	4	6				
277-0071-008	VACANT/RESIDENTIAL	R-1	C	4	6				
277-0131-002	RESIDENTIAL/SINGFAM/SUBDIV	R-1	C	4	6				
277-0072-026	RESIDENTIAL/SINGFAM/SUBDIV	R-1	C	4	6				
277-0134-021	SERVICE STATION	M-1	E	12	20	0.149	0.199	6510	8679
275-0240-071	OFFICE GENERAL	OB-R	E	1	2	0.018	0.023	765	1021
275-0240-090	OFFICE LARGE SINGLE TENANT	OB-LI	E	41	66	0.495	0.661	21582	28776
277-0134-004	LARGE RETAIL	M-1	E	18	28	0.211	0.281	9173	12231
277-0131-007	LIGHT INDUSTRIAL	C-2	E	3	5	0.039	0.052	1711	2281
275-0155-005	VACANT/RESIDENTIAL	R-1	E	4	6	0.045	0.060	1957	2610
277-0131-017	VACANT/RESIDENTIAL	R-1	E	4	6	0.048	0.065	2109	2812
275-0155-013	OFFICE GENERAL	C-2	E	8	12	0.091	0.122	3975	5300
277-0132-011	LIGHT INDUSTRIAL	M-1	E	12	19	0.139	0.185	6047	8063
277-0134-005	LARGE RETAIL	M-1	E	23	36	0.271	0.361	11805	15740
275-0240-072	OFFICE GENERAL	OB-LI	E	76	122	0.913	1.217	39756	53008
275-0155-004	VACANT/RESIDENTIAL	R-1	E	4	6	0.046	0.061	1993	2657
277-0134-003	LARGE RETAIL	M-1	E	30	48	0.364	0.485	15835	21113
277-0133-005		M-1	E	27	44	0.328	0.438	14297	19063
277-0131-016	VACANT/RESIDENTIAL	R-1	E	4	6	0.044	0.058	1899	2532
277-0134-020	VACANT/RETAIL	M-1	E	6	9	0.068	0.090	2952	3936
275-0155-006	USED CAR SALES	R-1	E	3	5	0.040	0.054	1759	2346
275-0155-007	VACANT/RETAIL	C-2	E	3	5	0.041	0.055	1781	2374
277-0132-009	LIGHT INDUSTRIAL	M-1	E	18	29	0.214	0.286	9331	12442
275-0240-070	OFFICE GENERAL	OB-R	E	15	25	0.185	0.246	8043	10723
277-0131-005	RESIDENTIAL/SINGFAM/SUBDIV	R-1	E	4	6	0.047	0.063	2069	2758
277-0131-006	RESIDENTIAL/SINGFAM/SUBDIV	R-1	E	4	6	0.044	0.059	1918	2558
275-0240-076	SPECIAL DISTRICT	OB-LI	E	5	8	0.060	0.080	2608	3477
277-0131-008	RESTAURANT	C-2	E	3	5	0.039	0.052	1704	2272
				1521	2322	3.9	5.3	171579	228772



Assumptions

	FAR		DU/AC	
	Low	High	Low	High
A	0.3	0.4	40	60
B	x	x	40	60
C	x	x	25	40
D	0.45	0.6	15	25
E	0.3	0.4	25	40

Attachment 9

August 26, 2010 Planning Commission Comments and Staff's Responses

Planning Commission Comment	Staff's Response
<ul style="list-style-type: none">▪ Notices should be sent to both property owners and occupants▪ Staff should make an extra effort to get the word out to the community	<ul style="list-style-type: none">▪ Notices have been sent to property owners and occupants of parcels that are to be rezoned or have the General Plan Land Use Designation changed. Properties within 500 feet of these land use changes were noticed as well▪ Thirty three stakeholders were noticed▪ Four hundred and seventy residents in the Dixieanne Neighborhood were noticed▪ Please refer to the list of outreach conducted in Attachment 10
<ul style="list-style-type: none">▪ Staff should develop a process citywide for developing in-lieu fee districts that would allow flexibility in requiring parking for infill developments	<ul style="list-style-type: none">▪ Prior to establishing a in-lieu fee district for the plan area, CDD and DOT management need to agree both on citywide parking strategies and the commitment of staff resources
<ul style="list-style-type: none">▪ Ensure that the land use changes for the Northeast Line are consistent with those for the Swanston Station	<ul style="list-style-type: none">▪ Planning and DOT staff have revised the Swanston Station rezone strategy to be consistent with the zoning surrounding the Globe, Arden/Del Paso and Royal Oaks Stations
<ul style="list-style-type: none">▪ Consider making the notification multi-family developments in the SPD to be similar as that of Planning Commission and provide some assurance that staff level review of these projects will have the same level of independent decision making	<ul style="list-style-type: none">▪ This issue is still under consideration by staff

**Outreach Conducted for the
Northeast Line Implementation Plan**

Del Paso Boulevard Partnership	3/25/10
Regional Transit Staff	4/26/10
Meeting with Property/Business Owners that included:	4/29/10
<ul style="list-style-type: none">• David Plag (PBID)• Rich Meeker (Business Owner)• Deborah Redmond (News & Review)• Rosemary Covington (Regional Transit)• Rob Kerth (North Sacramento Chamber of Commerce)• Shane Curry (Business Owner)• Jerry Greenberg (Business Owner)• Bobby Omery (Business Owner)	
Meeting with Alan Warren (Developer)	5/18/10
Meeting with Bob Slobe (Developer)	5/18/10
Phone Conference with Dan Friedlander (Business Owner, Developer)	5/26/10
Meeting with Russ Wyluda (Developer)	6/11/10
North Sacramento Redevelopment Advisory Committee	7/15/10
Planning Commission Workshop	8/26/10
Woodlake Neighborhood Association	10/6/10
North Sacramento Redevelopment Advisory Committee Members	11/25/10



REPORT TO PLANNING COMMISSION City of Sacramento

7

915 I Street, Sacramento, CA 95814-2671

STAFF REPORT
December 9, 2010

To: Members of the Planning Commission

Subject: 2030 General Plan Annual Report (LR10-007)

Location/Council District: Citywide

Recommendation: Receive and Comment

Contact: Teresa Haenggi, Associate Planner, (916) 808-7554; Tom Pace, Long Range Planning Manager, (916) 808-6848

Description/Analysis

Issue: The 2030 General Plan was adopted by the City Council in March 2009, and set a new direction for the future of Sacramento. Part IV of the General Plan provides an action strategy for implementation of the Plan, how to measure its success, and calls for the development of an annual report on the plan's progress. This year's report focuses on projects that will lay the groundwork for the City's economic recovery, including:

- River District Specific Plan,
- Florin Road Corridor Plan,
- Northeast Line Implementation Plan,
- Innovation & Technology Village, and
- Recent rezones and zoning code amendments.

In the coming years, economic and environmental sustainability will be advanced by completing plans for the Swanston Station/Arden Fair/Point West/Cal Expo area; a planned comprehensive update of the zoning code and related development regulations; and the ongoing Green Building Task Force and Climate Action Plan. Meanwhile, in fiscal year 2012, staff will begin scoping for the 5-year update to the general plan, and the update work will continue in FY 2013, with completion in FY 2014.

Policy Considerations: The 2030 General Plan includes an implementation measure to develop an annual report to monitor the implementation of the goals and policies of the General Plan. This report implements that measure.

Environmental Considerations:

California Environmental Quality Act (CEQA): No action is being proposed for this report, and it is not defined as a project under CEQA Section 15061 (b)(3): The activity is covered by the general rule that CEQA applies only to projects which have the potential for causing a significant effect on the environment.

Sustainability Considerations: The 2030 General Plan contains several principles, goals, and policies that address sustainability. The annual report monitors the implementation of the sustainability goals and policies.

Commission/Committee Action: The Planning Commission will review the 2030 General Plan Annual Report on December 9, 2010.

Rationale for Recommendation: The 2030 General Plan was approved by Council on March 3, 2009. The new General Plan calls for an annual report to the Planning Commission and Council on the progress made towards achieving its vision and goals. In assessing this progress, staff and city leaders can highlight accomplishments, identify areas of underperformance, and develop an action plan for the upcoming year. The annual reports will ensure the General Plan continues to be a living document that is responsive to the City's short-term interests and needs while addressing its long term priorities and values.

Financial Considerations: There are no direct financial considerations associated with this report.

Emerging Small Business Development (ESBD): The purchase of goods or services is not associated with this report.

Respectfully submitted by: 
 TERESA HAENGGI
 Associate Planner

Recommendation Approved:


 THOMAS PACE
 Long Range Planning Manager

Attachment: 2030 General Plan Annual Report

1 Executive Summarypg. 4
2. Community Surveypg. 8
3. Livability Index.....pg. 18
 Exhibit A: Livability Index Data Matrixpg. 25
4. Implementation Programpg. 45
 Exhibit A: Implementation Matrix.....pg. 47
5. 2009 Development Activity.....pg. 71
6. General Plan Amendments Analysispg. 79

2030 GENERAL PLAN ANNUAL REPORT (2010) EXECUTIVE SUMMARY

The Guiding Vision of the 2030 General Plan is that...

Sacramento will be the most livable city in America

The 2030 General Plan was adopted by the City Council on March 3, 2009, and set a new direction for the future of Sacramento. The General Plan was shaped by extensive outreach to residents, business, developers, and decision-makers. It was based on the city's Smart Growth Principles and the Council adopted Vision and Guiding Principles for the General Plan. Six themes emerged from this framework:

- Making Great Places
- Growing Smarter
- Maintaining a Vibrant Economy
- Creating a Healthy City
- Living Lightly-Reducing our "Carbon Footprint"
- Developing a Sustainable Future

The City's community and decision makers demonstrated their pride in the city's past and confidence in its future by putting a great deal of effort helping staff develop the General Plan. In order for the General Plan to serve its vision and purpose effectively it must be reviewed, maintained, and implemented in a systematic and consistent manner. To ensure the City is moving forward to achieve the Plan's vision, a report will be provided to the Council on an annual basis. The objectives of the report are to highlight the departments' accomplishments, report on current challenges, identify trends, gauge the public's level of satisfaction and engagement with the City, and measure the success of the General Plan in guiding the city to its vision of being the most livable city in America.

The 2030 General Plan Annual Report is also intended to guide Council on developing policy, establishing priorities, and providing direction to staff. The annual report will be delivered in the fall in order to inform dialogue on the city's budget development which generally begins at this time.

The Annual Report consists of five components that provide input on the City's success in fulfilling the goals and policies of General Plan, and the goals and policies' success in capturing the City's vision:

1. *Community Survey*. On May 14, 2009, Staff presented a draft outline of the annual report to the Planning Commission. The Commission stated the Annual Report needed an additional component that captures the community's perspective on livability. In response, staff developed an informal (non-scientific) web-based survey that was distributed through several contact lists, including to those who participated in General Plan workshops, Neighborhood Services Department's community contacts, and the City's Public Information Officer's outreach list. Full results from the community survey are provided in Section I of the annual report. Highlights of the survey are provided below.
 - 78 % of residents believe Sacramento is a livable city.
 - 84% of residents rate their home or work locations as good, very good or excellent.
 - 38% of residents say their neighborhood is better than it was a few years ago.
 - 47% of residents believe the quality of life in Sacramento will improve over the next few years.
 - 79% of residents sometimes or often get involved in neighborhood activities.
2. *Livability Index (Index)*. The Livability Index was developed to measure the General Plan's success over time in achieving the Plan's vision to become the most livable city in the nation. Livability is largely dependent on the sustainability of the City. The Index consists of fourteen indicators that address the three "E's" of sustainability: environment, economy, and equity. Data points collected for these indicators provide a picture of how "livable" the City is and how the General Plan has contributed to the progress made towards becoming the most livable city in America.

Several years of data are required to accurately track trends. Although attempts were made to collect data for the last five years, the information was not always available. However, there are some notable changes in the Livability Index data that can be informative. More information on the Livability Index, including a matrix of the data collected, is provided in Section II. A few observations on the data are provided below:

- The Downtown and River District redevelopment areas are the only redevelopment areas with increases in total tax revenue.
- Both violent and property crime decreased in 2009.
- Vehicle collisions declined between 2008 and 2009
- Since 2005 there has been a consistent increase in photovoltaic installations. This can be largely attributed to federal and state tax credits for utilizing solar energy, which began in 2007.

- The disposal rate of solid waste has declined both on a per capita and per employee basis.
3. *Implementation Program.* The annual report includes a “progress report” on the General Plan’s Implementation Program. This provides the opportunity to highlight progress made by all the departments and partnering agencies. Also, barriers to progress are identified and addressed. A few highlights are provided below. Progress on the General Plan Implementation Program is provided in Section III.
- The City is completing several planning efforts that will facilitate economic development once the economy improves.
 - Several important milestones for the Climate Action Plan have been met, and staff continues to work with the community on identifying strategies to reduce greenhouse gas emissions.
 - The City/County joint Green Building Task Force launched in spring 2010.
 - Two transportation-related studies have been started, including Ramona Avenue and "Cars on K."
 - Council approved an ordinance that requires recycling and reuse of construction wastes, including recycling materials generated by the demolition and remodeling of buildings.
4. *Growth Activity (Development).* A summary of the types of development that have occurred in the City of Sacramento in the prior year is provided in the annual report, along with an assessment of how well the General Plan has fostered desired development and economic growth. Data provided helps to gauge growth and includes the following: Affordable housing, square feet of commercial, development in Growth Opportunity Areas, residential units, new jobs, and taxes generated.

The downturn in the economy, along with the moratorium on building in Natomas, have had a significant impact on development. Below are a few salient statistics. A more thorough analysis is provided in the Growth Activity section of the annual report (Section IV).

- During 2005-2009, the City met 10% of the General Plan’s housing growth projections.
- From 2004-2008, residential development in the City steadily decreased at an annual average rate of 16%. 2009 saw housing production decline by 63% from the previous year. Chart 3 below illustrates this drastic decline.
- Commercial development, which initially remained strong while the residential sector was declining, began to decline in 2007. In 2007 and 2008 the number of completed commercial building permits declined at a rate of 50%.

5. *General Plan Amendments.* Amendments made in the prior year are assessed as a whole. This cumulative assessment provides an understanding of how a series of amendments made over the previous year can impact the level of success in realizing the goals of the General Plan. This is an important effort, because the impact of individual amendments may not be easily understood at the time they are made.

A majority of the amendments that were made supported the General Plan goals to develop transportation-oriented development, provide a diverse range of housing, and promote economic health. The amendments also addressed the priority implementation measure to bring the zoning into consistency with the General Plan. Therefore, the cumulative effects of the amendments are determined to be consistent and supportive with the General Plan goals and policies. More information is provided in Section V of the Annual Report.

Five-Year General Plan Review and Update. At least once every five years, the City will thoroughly review, revise, and update the General Plan as necessary. This process will encompass the entire General Plan including the goals, policies, and implementation programs. Information from the annual reports will be used to inform the update process.

SECTION I

2030 General Plan Annual Report SUMMARY OF COMMUNITY SURVEY

The Community Development Department conducted the second annual quality of life survey in September as part of the General Plan Annual Report. This is the component of the annual report that asks residents and employees for feedback on the livability of Sacramento. The survey provides information to determine the future effectiveness of the 2030 General Plan.

Summary of Findings

- 78 % of residents believe Sacramento is a livable city.
- The top three reasons people believe Sacramento is a livable city are:
 - Places to eat.
 - Downtown/Midtown
 - Clean neighborhoods
- The top three reasons those responding to the survey believe Sacramento might not be livable are:
 - Lack of public transportation
 - Crime rate is high
 - Lack of job opportunities
- The top five things people would change about the City are:
 - More alternative transportation
 - Reduce the crime rate
 - Promote higher quality education
 - Promote more sustainable development
 - Protect farmland and natural habitat.
- 79% of residents sometimes or often get involved in neighborhood activities.
- The top three reasons residents go to other cities and counties are:
 - Recreation
 - Entertainment
 - Shopping
- 84% of residents rate their home or work locations as good, very good or excellent.
- 38% of residents say their neighborhood is better than it was a few years ago.
- 47% of residents believe the quality of life in Sacramento will improve over the next few years.

- The top 3 postcards images the survey participants said they would send a friend:
 - Capitol (23%)
 - Sacramento River's (14%) (Sacramento River and American River)
 - Sacramento Parks, trees and canopy, and Gardens (12%)

Comparison to 2009 Survey

In general, the survey responses were similar to those of 2009. Only three survey questions resulted in a significantly different response. In 2009, 87% percent of residents who responded to the survey stated they considered Sacramento to be a livable city. This at number decreased to 78% in the 2010 survey. This pessimistic outlook, likely influenced by the downturn in the economy, is reflected in the responses where those who believe the quality of life in Sacramento will improve over the next few years decreased from 51% to 47%.

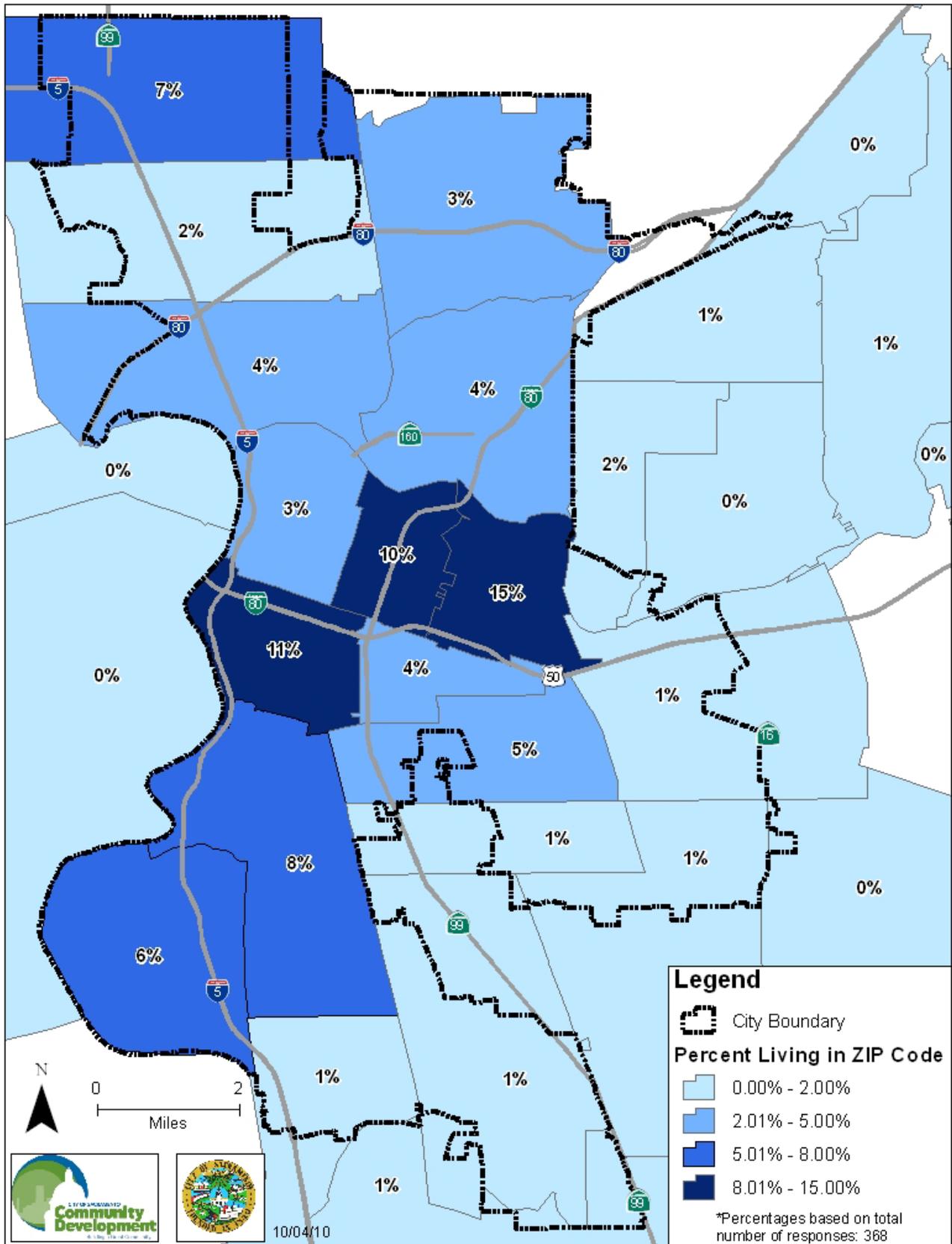
When asked to select the top five things the respondent would change about the city, four out of five responses were the same as last year: 1) provide more alternative transportation choices; 2) reduce crime rate; 3) promote more sustainable development; and 4) promote farmland and natural habitat. Last year's 5th choice, "to reduce traffic congestion", was replaced this year with "promoting higher quality education".

Distribution of Survey

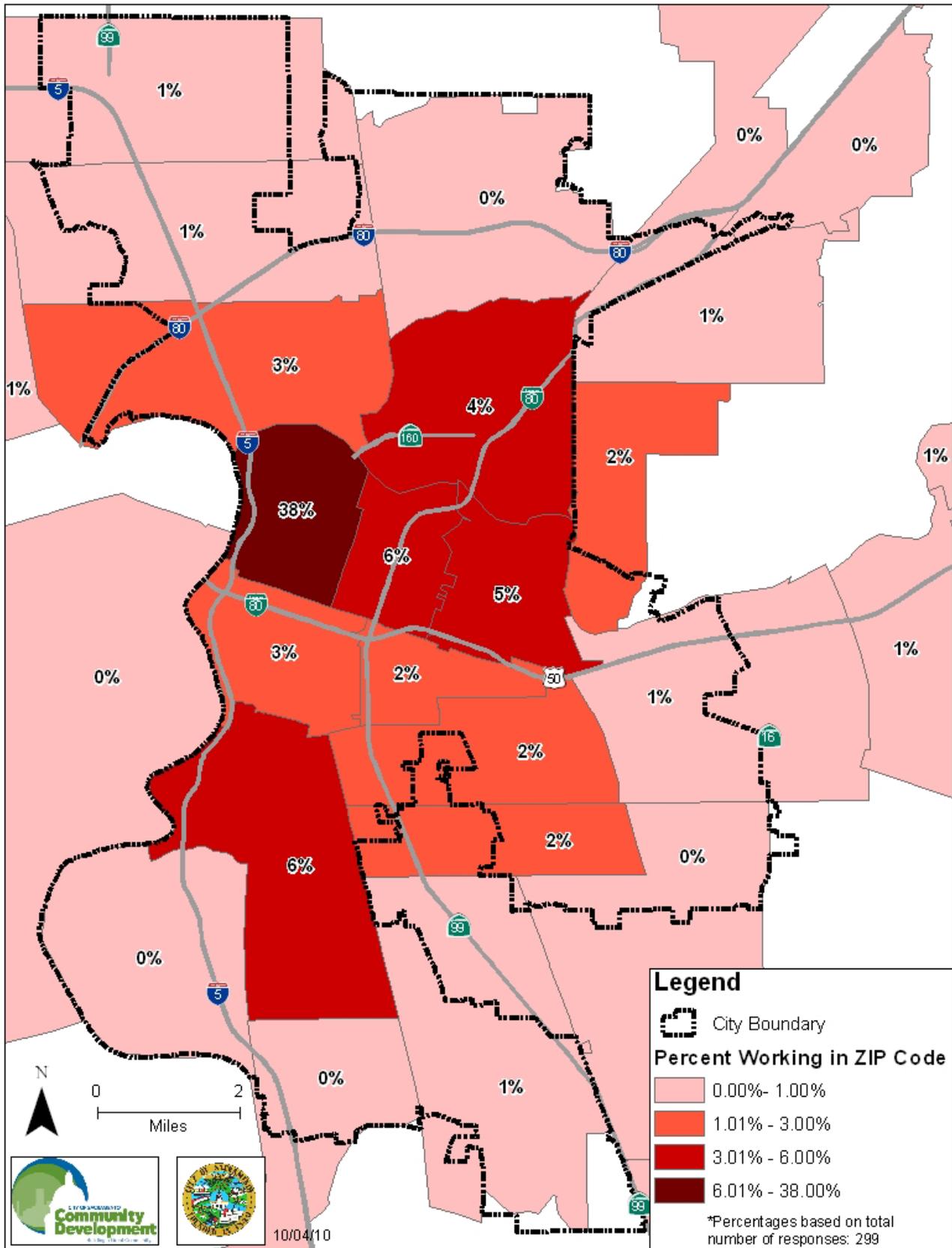
Surveys were distributed electronically to previous general plan workshop participants, key neighborhood services contacts, and neighborhood groups citywide. Additionally, it was posted on the City of Sacramento and Community Development Department websites. As a result, 398 people responded to the survey, 87% of whom live in the City of Sacramento. Although this survey is not considered a scientific survey, it is an important component of the 2030 General Plan because of the participation of the public.

Maps provided on the following pages shows the level of participation by the zip codes provided by those who responded to the survey. The maps are followed by graphs and summaries of the responses to each survey questions.

Percent of Responses from Sacramento Citizens
(By Zip Code)



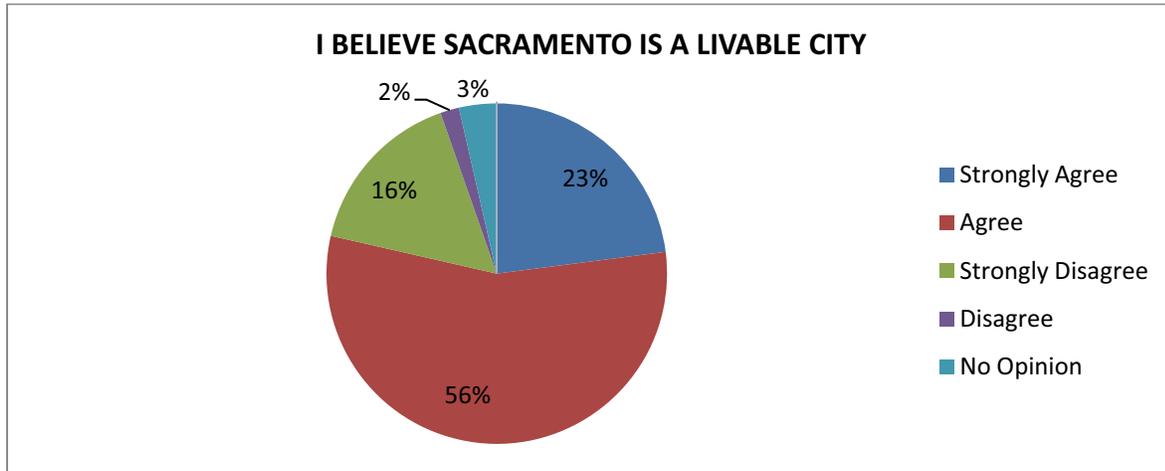
Percent of Responses from Employees who are Not Citizens
(By Zip Code)



SUMMARY OF SURVEY QUESTIONS

Question: I believe Sacramento is a livable city. (Agree/Disagree)

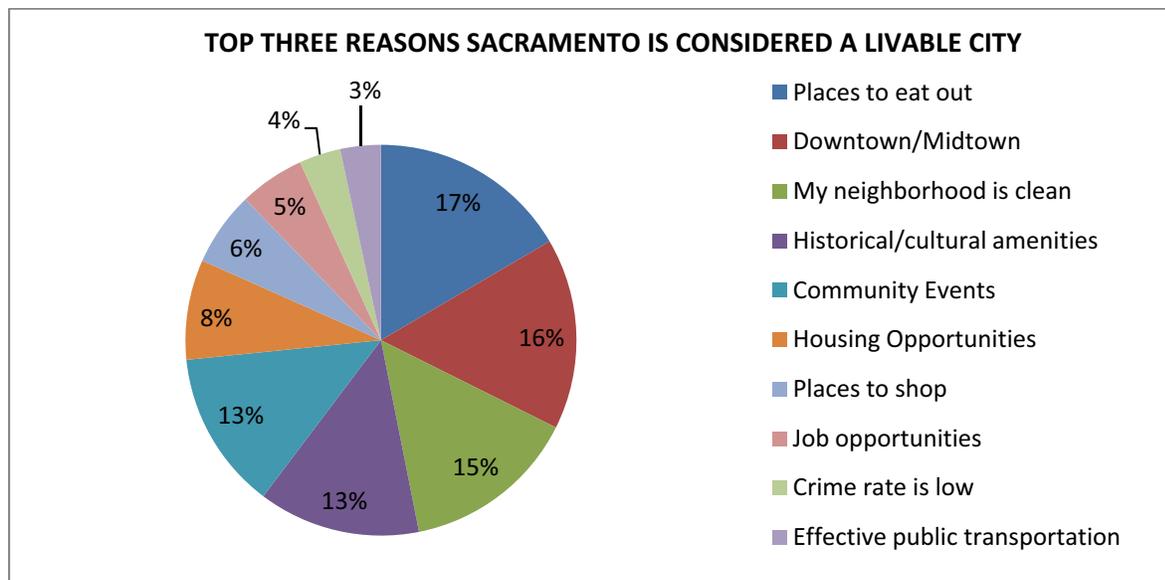
79% of respondents agreed or strongly agreed that Sacramento is a livable city.



Question: If you think Sacramento is a livable city, please check three reasons.

Of those that felt Sacramento is a livable city, the top three reasons considered were:

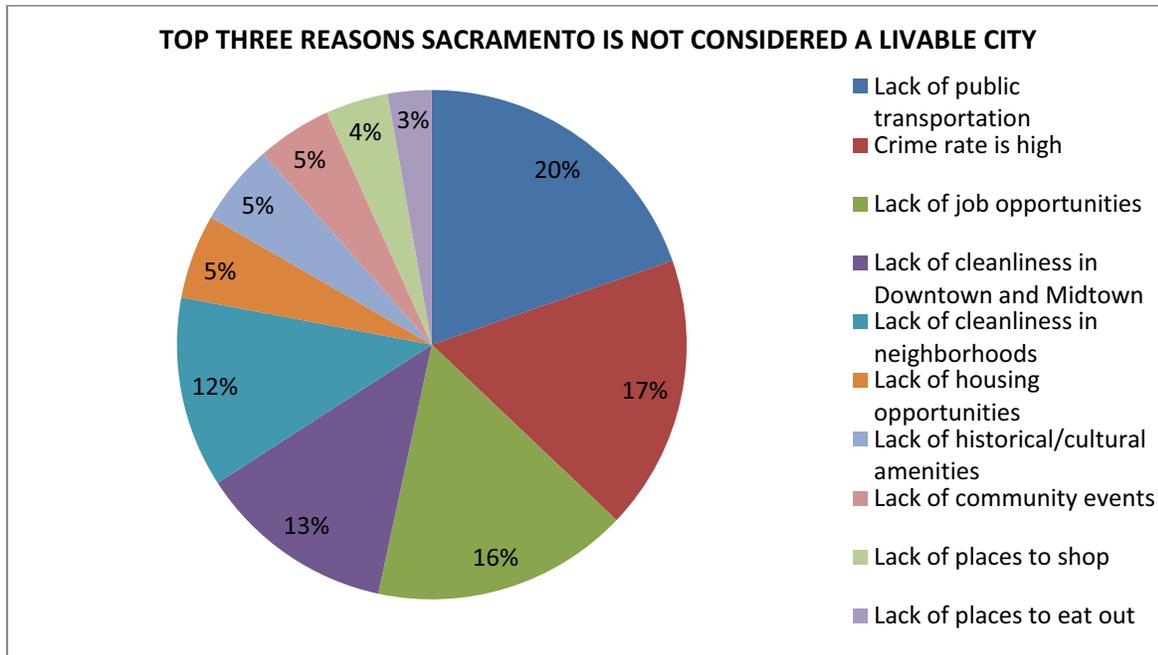
1. Places to eat out (54%)
2. Downtown/Midtown (52%)
3. My neighborhood is clean (48%)



Question: If you don't think Sacramento is a livable city, please check three reasons that apply.

Of those that did not feel that Sacramento is livable, the top three reasons cited were:

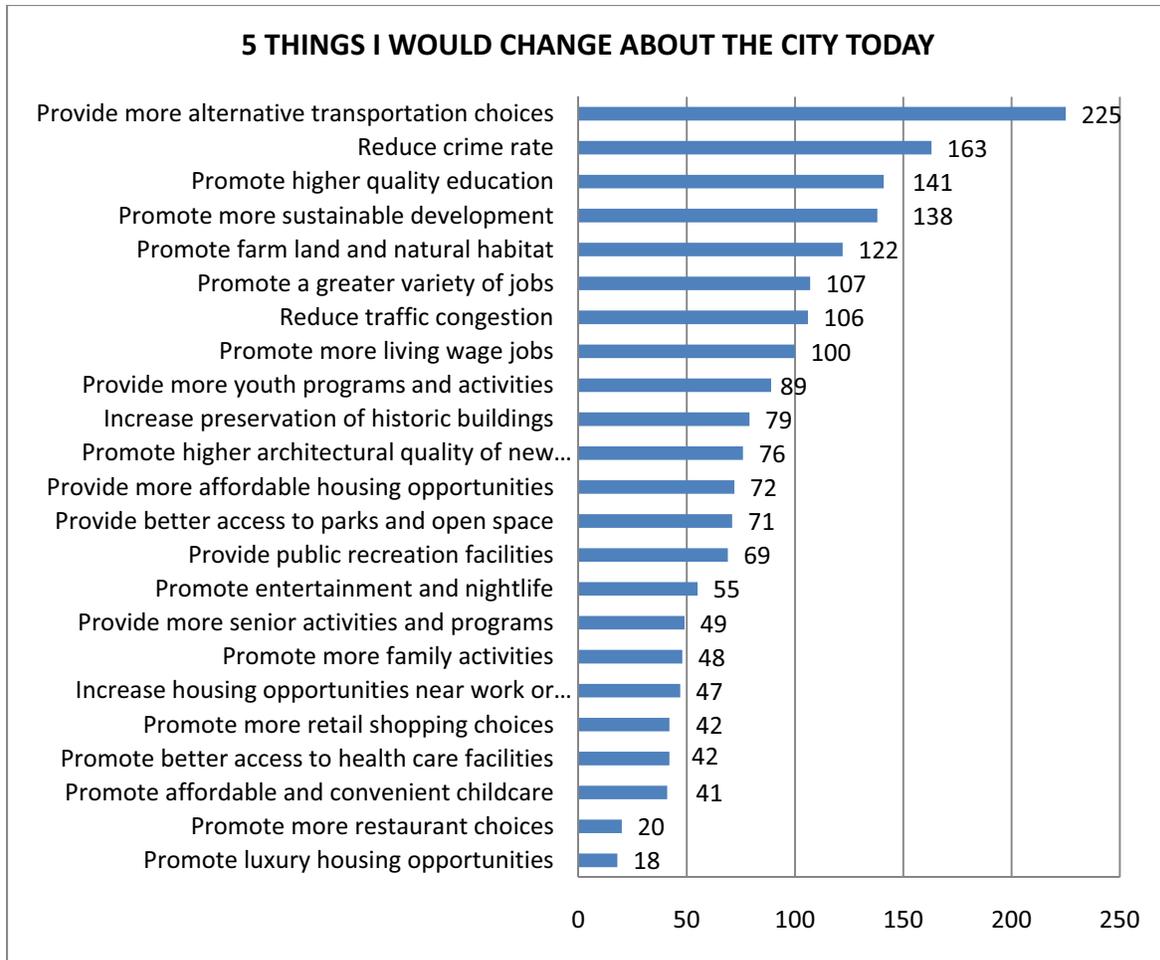
1. Lack of public transportation (53%)
2. Crime rate is high (47%)
3. Lack of job opportunities (44%)



Question: If you could change 5 things about the City of Sacramento today, what would you change?

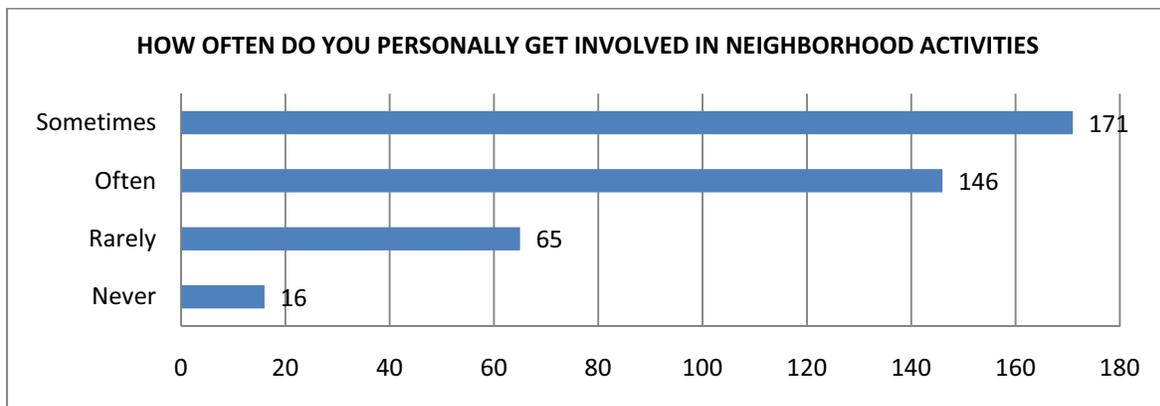
Of those that would change 5 things about the City of Sacramento, the top 5 considered were:

1. Provide more alternative transportation choices (57%)
2. Reduce crime rate (42%)
3. Promote higher quality education (36%)
4. Promote more sustainable development (35%)
5. Promote farm land and natural habitat (31%)



Question: How often do you personally get involved in neighborhood activities?

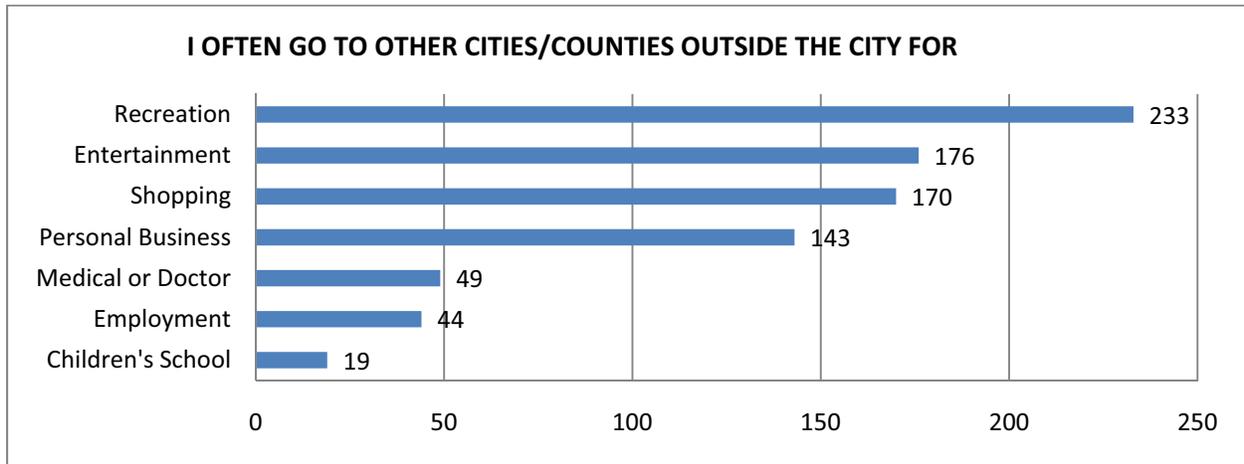
79% of the respondents stated that they sometimes or often get personally involved with neighborhood activities.



Question: I often go to other cities/counties outside the City of Sacramento for (check those that apply):

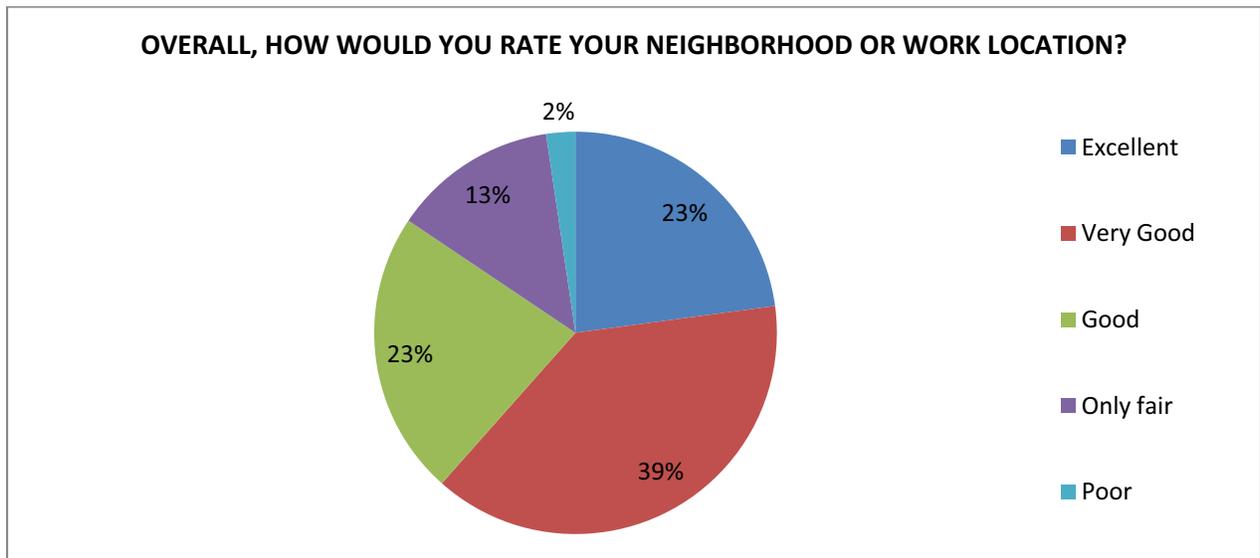
The top three popular reasons to make trips outside the City of Sacramento were:

1. Recreation (69%)
2. Entertainment (52%)
3. Shopping (51%)



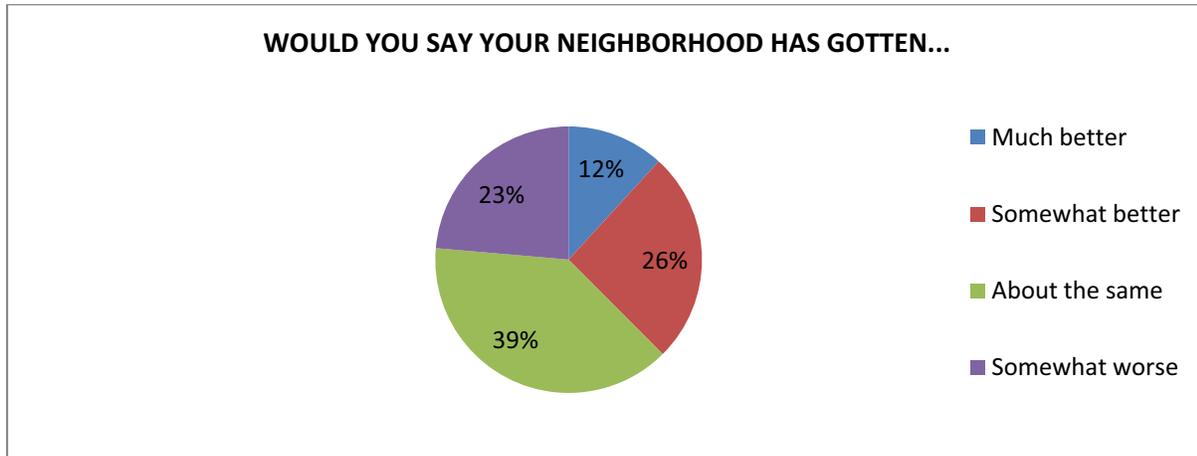
Question: Overall, how would you rate your neighborhood or work location?

62% thought their neighborhood or work location rated excellent or very good.



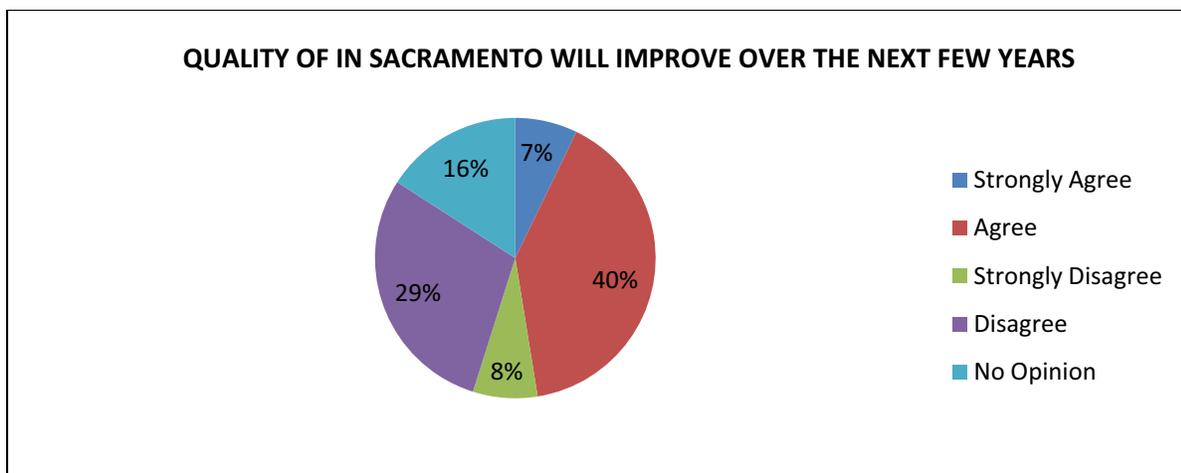
Question: Looking back a few years, would you say your neighborhood has gotten... (much better – somewhat worse).

Approximately 38% of respondents felt their neighborhood has gotten much better or somewhat better in recent years.



Question: The quality of life in Sacramento will improve over the next few years. (Agree – disagree)

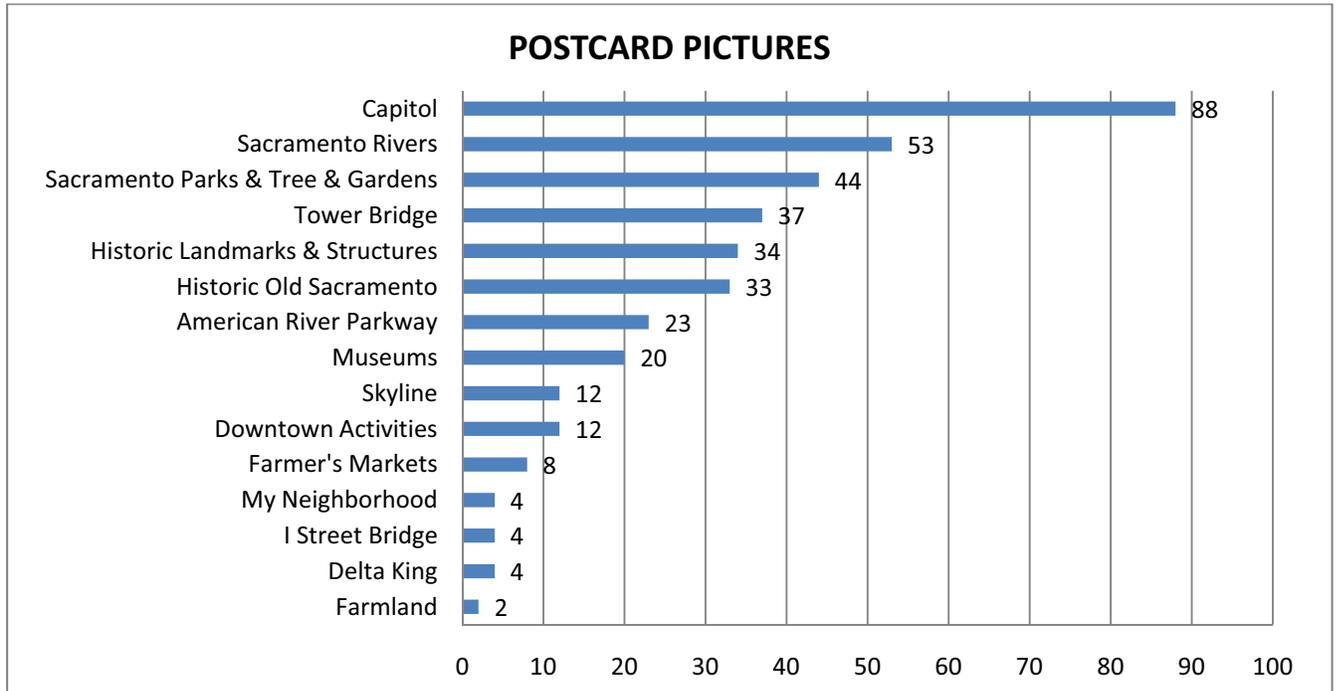
47% of respondents agreed or strongly agreed that the quality of life will improve over the next few years.



Question: If you were to send a postcard to a friend with snapshots of the City of Sacramento, what would you send a snapshot of? (Open-ended question)

The top 3 Postcards include:

1. Capitol (23%)
2. Sacramento River's (14%) (Sacramento River and American River)
3. Sacramento Parks, trees and canopy, and Gardens (12%)



SECTION II**2030 General Plan Annual Report
SUMMARY OF LIVABILITY INDEX**

The Livability Index was developed concurrently with the 2030 General Plan's goals, policies, and implementation program. Input from the City Council and the General Plan Advisory Committee contributed to this effort. It includes the following fourteen indicators that together reflect the various dimensions of "livability" as framed by the General Plan's Vision and Guiding Principles, themes, goals, policies, and programs:

- | | |
|-----------------------------------------------|---------------------------------------------|
| 1. City Profile | 8. Housing Affordability and Diversity |
| 2. Urban Forest | 9. Mobility and Transportation Choices |
| 3. Economic Diversity and Business Innovation | 10. Public Safety |
| 4. Vibrant Central City | 11. Energy, Water and Waste |
| 5. Infill Development and Redevelopment | 12. Carbon Footprint and Air Quality |
| 6. Historic Preservation | 13. Equity |
| 7. Complete Neighborhoods | 14. Civic Engagement and Government Service |

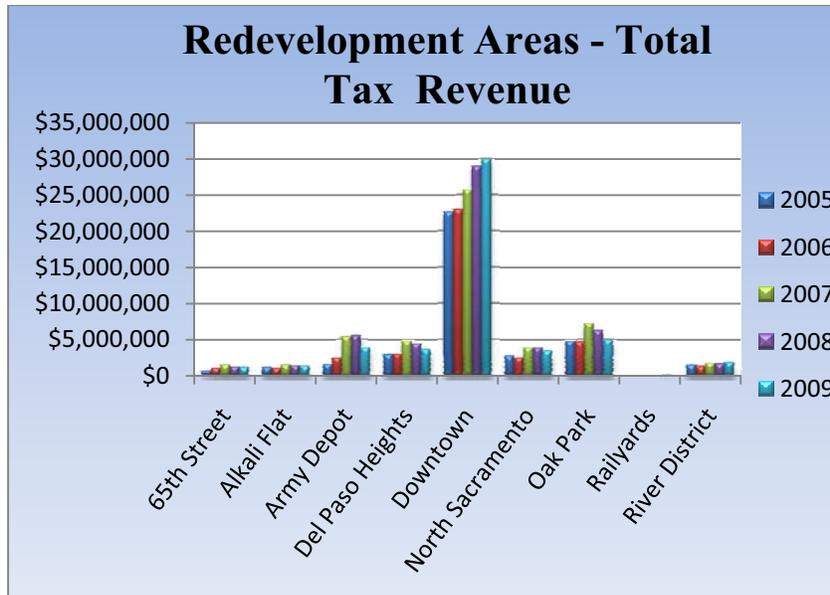
Each indicator is comprised of a series of sub-indicators to more fully encompass the complexity of each indicator.

The first indicator establishes basic statistics (e.g., population, diversity, median income, cost of living, unemployment rate). This profile information is used to set the context for current conditions.

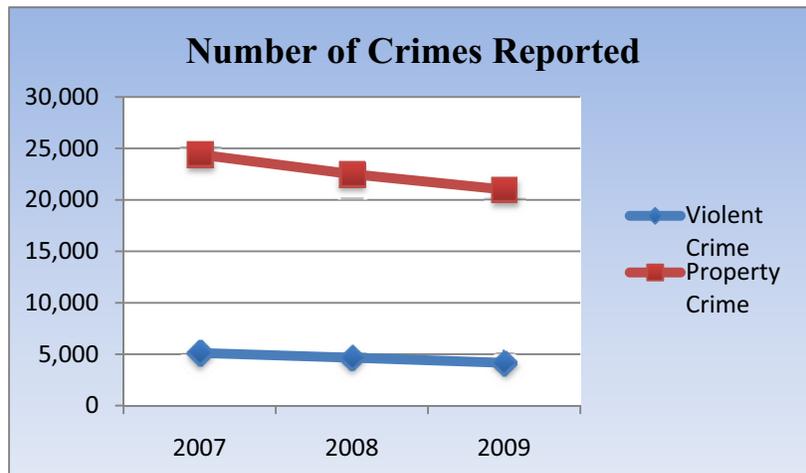
The remaining indicators measure Sacramento's overall progress in 13 key areas, reflecting the major themes in the 2030 General Plan.

Indicator Highlights

With each annual report that is completed, the City will increasingly be able to evaluate its progress in becoming the most livable city in America. Many of the data points tracked for the indicators saw little or no change over the past two years. This is in part due to a slow economy and a relatively short time period. There were, however, a few data points that made notable changes. These are identified below.

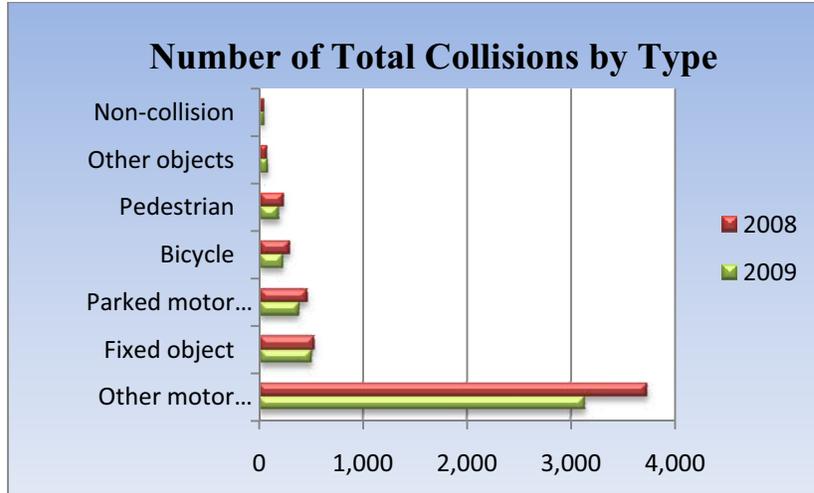


As indicated in the chart above, the Downtown and River District redevelopment areas are the only redevelopment areas with increases in total tax revenue. The remainder of the redevelopment areas have seen declining revenues, largely due to the slowing economy.

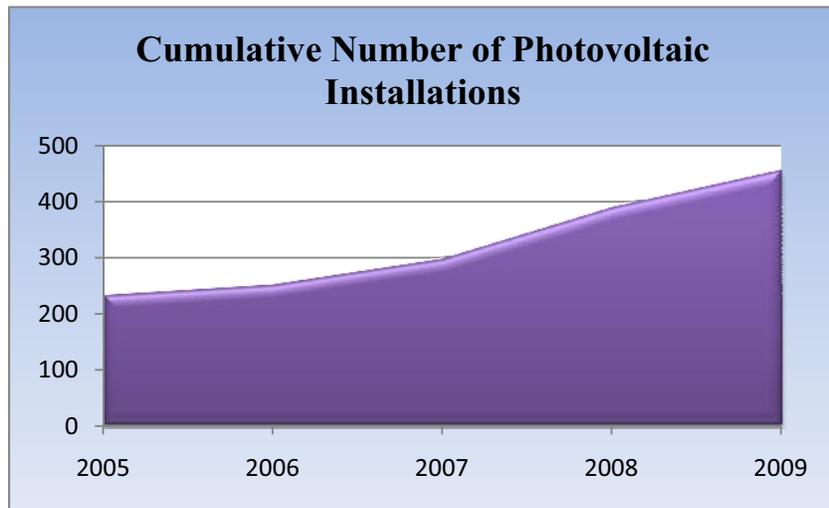


While it is too early to determine the impact of the 2030 General Plan on crime, there is a downward trend in both violent and property crime indicated in graph above. The following general plan implementation programs will help to continue this downward trend in crime:

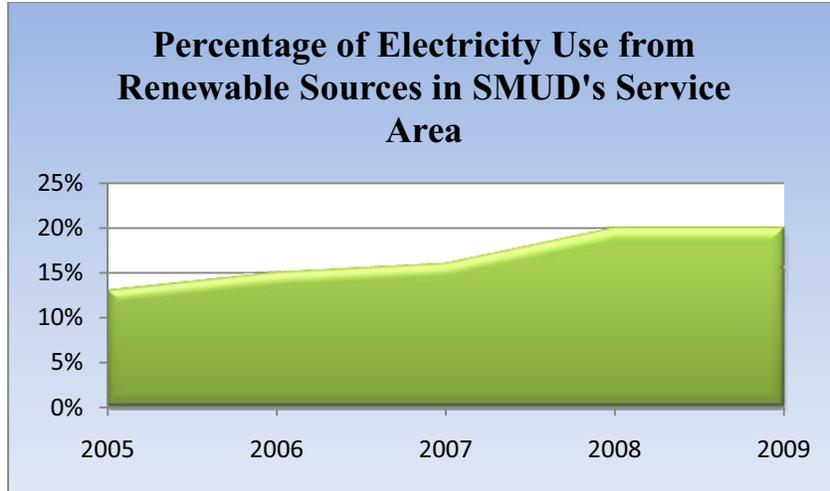
- Ongoing public education program regarding crime prevention measures, and support community programs, activities, and strategies aimed at preventing crime including youth gang activities, and
- Incorporating principles of Crime Prevention through Environmental Design (CPTED) in new development.



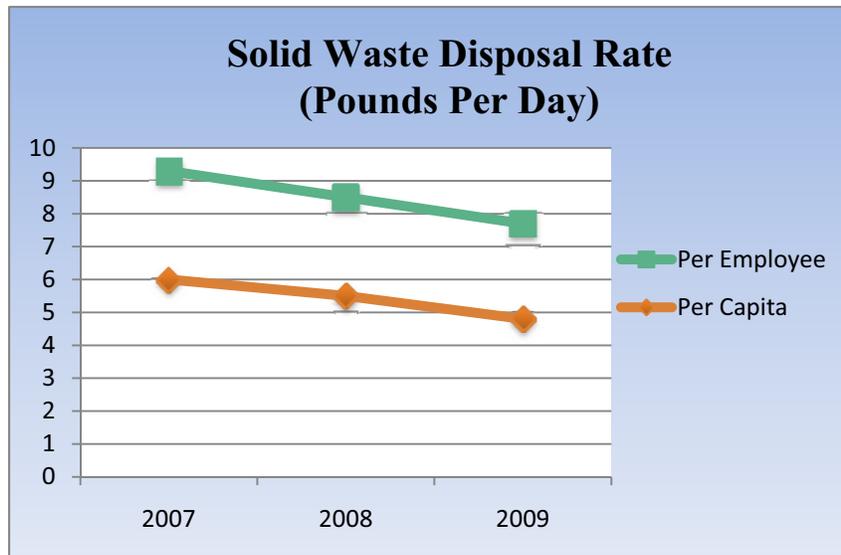
Between 2008 and 2009 collisions of all types declined. The complete streets policies of the General Plan promote all modes of transportation to make streets safer for residents using a variety transportation options.



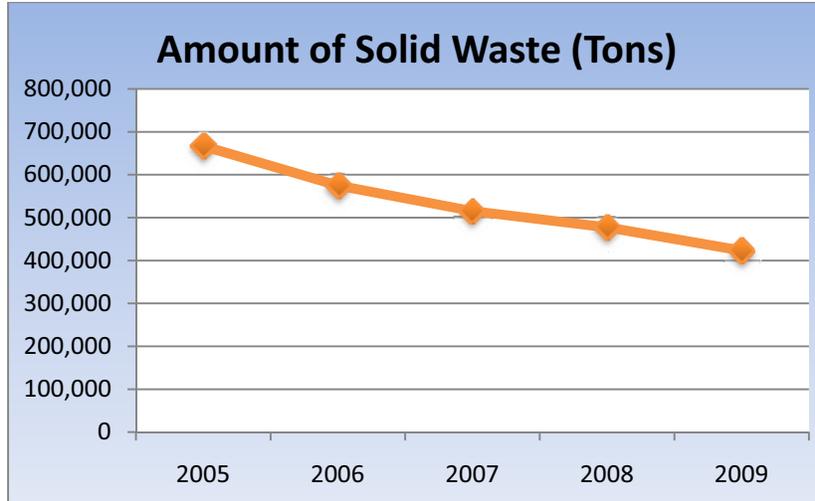
Since 2005 there has been a consistent increase in photovoltaic installations. This can be largely attributed to federal and state tax credits for utilizing solar energy, which began in 2007. In recent roundtable discussions hosted by Green Capital Alliance it was recommended that Sacramento can speed the adoption of solar PV and solar hot water systems by continuing to maintain a standardized solar permit application, creating a predictable review and inspection process, and changing to a permit flat fee. These strategies will reduce the cost of installation, and consequently, the cost that is passed onto the home or business owner.



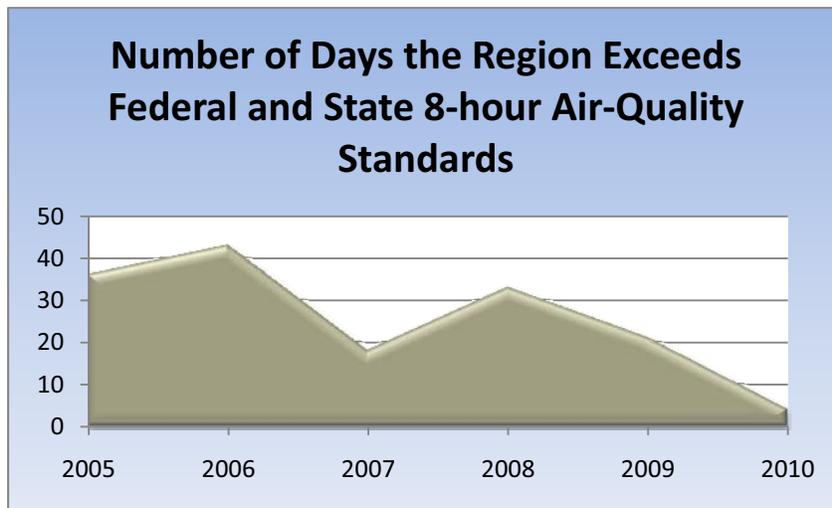
The percentage of electricity use from renewable sources in SMUD's service area almost doubled in less than 5 years. It is anticipated that 22% of the electricity used will be from renewable sources by 2010. The city's effort to streamline the permitting of photovoltaics has made, and will continue to make, an impact on this upward trend.



In 2007 the State Department of Resources, Recycling and Recovery (CalRecycle) for daily pounds of solid waste disposed of is 6.9 pounds per capita and 10.8 pounds per employee. Even in 2007 residents and employees in the City were falling well within the target set by the State. This downward trend indicates that the City is on track to meet the General Plan's target of achieving "zero waste to landfills by 2040 through reusing, reducing, and recycling solid waste; and using conversion technology if appropriate" (Policy U 5.1.1).



The overall amount of solid waste disposed of has also declined. In March of 2009 an ordinance was adopted that required the recycling and reuse of construction waste. This ordinance was the result of a General Plan implementation program which will help to continue the trend of reduced solid waste.



In 2010, the Sacramento Valley had the best air quality in years. While this can be largely attributed to near absence of 100 degree days during the previous summer, the policies, programs and land use patterns of the 2030 General Plan are projected to reduce vehicle miles travelled per capita by 13.2%, and reduce vehicle trips per capita by 11.1%¹.

2030 General Plan Master Environmental Impact Report

Criteria for Indicators

A long list of potential indicators was assessed for inclusion in the Livability Index. Consistent with other successful indicator programs, the following criteria were used to screen the potential indicators and select the proposed set of indicators:

- Aligns with the General Plan Vision and Guiding Principles;
- Can be easily understood and communicated;
- Is available from a reliable source of objective information;
- Is available for different geographic areas (e.g., community plan area, city, and/or region);
- Is collected annually or at other regular intervals;
- Is relatively inexpensive to collect and report;
- Indicates an output rather than an input; and
- Allows for comparison to other jurisdictions, or is a commonly reported measure.

Relationship between the Livability Index and the 2030 General Plan

The indicators in the Livability Index are aligned with the General Plan's Vision and Guiding Principles, and the type of data needed to track the indicator trends is based on the Plan's goals and policies.

The trends identified in the Livability Index are largely determined by the success of the Plan's Implementation Program. If trends indicate the city is not heading in the desired direction, or is not getting to this destination in an acceptable timeframe, staff will revisit the Implementation Program to determine if it should be modified to achieve greater success in realizing the General Plan's vision. The Index will be also be used in the 5-year update of the General Plan.

Community Involvement

One of the more important features of the Livability Index is its usefulness in keeping the public engaged in the implementation of the 2030 General Plan. Staff intends to post the results on the website, offer hardcopy summaries, and provide public outreach on the annual report. The Index will be used as a tool to educate the community on issues affecting their neighborhood, and assist in providing information to the public on how to 1) participate in addressing these issues; 2) inform staff about those issues that are most important to them; and 3) provide suggestions on how to resolve problems and celebrate successes.

Data Collection

The Livability Index is annually populated with data points. This requires a significant amount of staff time and coordination but with the benefit of the City being able to evaluate its progress in meeting its goals is well worth the staff time. With each new

year the City will have a better perspective on how well it is growing and redeveloping. The collection of data will continually be improved upon. Near-term tasks include the following:

- editing data points based on feedback from data subject experts;
- identifying desired trends and goals;
- confirming data sources, availability of data;
- developing a manual with detailed recording of data to ensure consistent data collection throughout the years;
- identifying resource/data gaps; and
- developing a format for communicating information.

Future Data Collection

Once resources are available, staff will develop a more comprehensive data base to capture more data points on specific uses and track trends at a smaller geographic level, e.g. Community Plan Areas and neighborhoods.

City Profile

	2005		2006		2007		2008		2009		2010
1. Total Population											
U.S. Census - American Community Survey	445,287		438,246		451,404		457,849		466,685		
2. Physical Area of City (sq. miles)	98.7		98.7		98.7		99.68		99.68		
3. Diversity: Percent population* by ethnicity/race											
	Total	%									
a. Hispanic or Latino	111,559	25.05%	104,641	23.88%	110,669	24.52%	109,787	23.98%	117,035	25.08%	
b. White	160,599	36.07%	172,653	39.40%	169,053	37.45%	174,667	38.15%	176,586	37.84%	
c. Black or African American	71,452	16.05%	60,002	13.69%	61,141	13.54%	63,068	13.77%	61,696	13.22%	
d. American Indian/Alaska Native	3,871	0.87%	4,015	0.92%	2,795	0.62%	2,964	0.65%	2,521	0.54%	
e. Asian	80,307	18.03%	74,301	16.95%	83,300	18.45%	84,304	18.41%	84,092	18.02%	
f. Hawaiian/Pacific Islander	3,867	0.87%	6,083	1.39%	5,841	1.29%	4,873	1.06%	5,695	1.22%	
g. Some Other Race Alone	1,236	0.28%	2,179	0.50%	2,082	0.46%	1,988	0.43%	2,514	0.54%	
h. Two or more races/Other	12,396	2.78%	14,372	3.28%	16,523	3.66%	15,742	3.44%	16,546	3.55%	
4. Density: Number of people per square mile	4511.5		4440.2		4573.5		4593.2		4681.8		
5. City of Sacramento Unemployment Rate	5.9%		5.6%		6.4%		8.4%		13.3%		15% (Est. Aug, 2010)
6. Median Household Income	2005		2006		2007		2008		2009		
Actual	\$44,867		\$46,055		\$49,849		\$50,958		\$47,107		
Per Capita	\$22,841		\$23,886		\$25,536		\$25,313		\$24,471		
7. Cost of Living (U.S. Avg. 100)							98.3		93.0		

Urban Forest

	2005	2006	2007	2008	2009	2010
1. Percentage cover tree canopy		20.79				
2. Number of trees in Sacramento	FY 05/06	FY 06/07	FY 07/08	FY 08/09	FY 09/10	FY 10/11
# of city-owned trees					100,118	100,118
# of trees planted by city per year	2,947	659	184	446	693	693
3. Tree health (percentage of)						
Good					50.37	
Fair					34.33	
Poor					5.08	

Economic Diversity

	2005		2006		2007		2008		2009		2010	
1. City Bond Rating												
<i>Standard & Poor's</i>												
Rating as issuer											A+	
<i>Moody's</i>												
Rating as issuer											Aa2	
2. Jobs/Housing Balance												
Total Employment												
Housing Units			185,843		188,897		192,371		195,763			
Jobs/Housing Ratio												
3. Educational Attainment												
<i>Population over 25 yrs old</i>	283,176		284,104		292,350		298,448		302,663			
No HS Degree	57,171	20.2%	55,226	19.4%	56,396	19.3%	54,301	18.2%	57,845	19.1%		
HS Degree	54,703	19.3%	64,228	22.6%	69,378	23.7%	63,169	21.2%	58,318	19.3%		
Some College	69,068	24.4%	60,464	21.3%	61,909	21.2%	70,847	23.7%	70,760	23.4%		
Associates Degree	22,639	8.0%	20,094	7.1%	20,108	6.9%	23,655	7.9%	25,239	8.3%		
Bachelors Degree	52,842	18.7%	57,889	20.4%	54,972	18.8%	58,083	19.5%	62,087	20.5%		
Graduate/Professional Degree	26,753	9.4%	26,203	9.2%	29,587	10.1%	28,393	9.5%	28,414	9.4%		
Percent of HS graduates or higher						80.7%		81.8%	80.9%			
Percent Bachelor's degree or higher						28.9%		29.0%	29.9%			
4. Growth in selected industry sectors (County)												
Health Care & Social Assistance	53,700		56,400		58,800		60,200		44,333			
Manufacturing	30,900		25,000		24,000		22,700		20,600			
Financial services	45,300		45,900		42,800		39,900		37,000			
Information technology	15,500		15,300		15,500		15,000		14,100			
Construction	45,700		43,700		41,000		34,400		26,600			
Government (Federal, State and Local)	158,400		163,100		169,100		171,700		172,000			

Economic Diversity

	2005	2006	2007	2008	2009	2010
5. Growth in selected industry sectors (City)						
Educational Services, health care, and social assistance	35,954	36,196	42,636	46,627	44,333	
Manufacturing	10,545	11,715	9,219	13,416	9,828	
Financial, insurance, real estate, rental, leasing	15,301	15,426	16,007	14,361	14,317	
Information technology	5,300	6,405	6,605	6,335	6,182	
Construction	15,915	15,938	14,729	14,827	10,548	
Public Administration	24,493	22,971	23,009	24,485	27,302	
6. Number of startups/new businesses by type				TBD	TBD	
7. Number of minority- and female-owned businesses				TBD	TBD	
Female						
Hispanic or Latino						
Black or African American						
American Indian & Alaskan Native						
Asian						

Vibrant Central City

	2005	2006	2007	2008	2009	2010
1. Number of New Housing Units						
New Housing	17	315	58	461	33	
2. Affordability of rental housing for average worker (e.g. income needed by occupation)						
3. Total Annual Revenue	TBD	TBD	TBD	TBD	TBD	TBD
Property tax						
Hotel tax (TOT)						
Eating and drinking establishments tax						
sales tax						
other revenue tax						
4. Total annual attendance at major arts and cultural venues						
Crocker Art Museum (Fiscal Year)				103,383	132,271	92,442
Sacramento Theater Company (Season: October through May)				32,848	31,517	TBD
Sacramento Ballet (Collected FY Oct - May)				47,723	42,638	TBD
Convention Center Complex = Memorial Auditorium, Convention				863,759	863,770	779,388
California Music Theatre - Music Circus	143,196	132,586	136,936	137,515	107,973	TBD
Railroad Museum (Jan-Dec)			301,212	297,488	323,301	TBD
Excursion Train Ridership (Jan-Dec)			69,849	70,508	72,075	
Farmers Market at Cesar Chavez Park				4,000	3,000 -3,500 week	TBD
Friday Night Concerts in the park				67,003	68,948	60,148

Infill and Redevelopment

	2005	2006	2007	2008	2009	2010
1. Number of residential units and sq.ft. of other buildings in designated infill areas						
Single family	354	319	284	144	110	
Multi-family	523	424	332	435	57	
Non-residential	411,409	815,513	346,507	1,007,399	74,500	
2. Redevelopment Areas total dollars of investment*						
<i>City Only RDA's</i>						
65th Street	\$100,639	\$145,706	\$485,226	\$1,028,768	\$3,067,573	
Alkali Flat	\$1,159,664	\$3,350,182	\$2,573,554	\$1,315,082	\$869,367	
Army Depot	\$633,412	\$1,131,057	\$1,904,608	\$2,655,641	\$2,802,014	
Del Paso Heights	\$3,833,605	\$11,247,915	\$4,049,615	\$3,207,725	\$3,394,776	
Downtown	\$23,524,453	\$36,414,246	\$32,181,578	\$31,865,516	\$47,108,691	
North Sacramento	\$1,704,449	\$7,670,811	\$3,839,887	\$2,532,161	\$5,063,205	
Oak Park	\$3,702,235	\$4,893,009	\$18,757,976	\$5,300,757	\$4,890,050	
Railyards	\$0	\$0	\$0	\$52,304	\$620,333	
River District	\$1,855,358	\$6,672,953	\$3,284,441	\$1,207,054	\$1,496,161	
<i>Joint City/County RDA's</i>						
Franklin Boulevard	\$1,000,019	\$1,075,537	\$897,241	\$2,707,728	\$1,509,787	
Stockton Boulevard	\$609,628	\$1,036,095	\$1,584,993	\$3,969,625	\$10,080,785	
3. Redevelopment Areas total tax revenue						
<i>City Only RDA's</i>						
65th Street	\$553,475	\$961,184	\$1,435,030	\$1,143,613	\$1,021,868	
Alkali Flat	\$1,025,166	\$947,195	\$1,368,976	\$1,325,069	\$1,222,239	
Army Depot	\$1,465,679	\$2,358,803	\$5,285,360	\$5,435,029	\$3,813,639	
Del Paso Heights	\$2,844,030	\$2,945,797	\$4,655,503	\$4,297,346	\$3,493,720	
Downtown	\$22,530,255	\$22,922,409	\$25,604,059	\$28,898,246	\$29,881,806	
North Sacramento	\$2,610,428	\$2,400,571	\$3,741,888	\$3,779,778	\$3,386,185	
Oak Park	\$4,679,090	\$4,604,425	\$7,091,199	\$6,139,920	\$4,836,339	
Railyards	\$0	\$0	\$0	\$0	\$81,291	
River District	\$1,419,968	\$1,193,792	\$1,703,622	\$1,615,034	\$1,771,736	
<i>Joint City/County RDA's</i>						
Franklin Boulevard	\$2,303,531	\$2,329,335	\$3,591,020	\$1,995,331	\$2,865,438	
Stockton Boulevard	\$2,152,082	\$1,981,321	\$3,196,370	\$3,021,284	\$2,298,685	
4. Number of development projects in a Tier One site				TBD	TBD	

Historic Preservation

	2005	2006	2007	2008	2009	2010
1. Number of building permits in historic districts by type	89	68	79	76	51	59 (up to October)
2. Number of Housing Cases	Not available. Staff is in the process of developing reports to obtain data.					
Within Historic Districts						
Landmark Properties						

Complete Neighborhoods

	2005	2006	2007	2008	2009	2010
					FY 2009/2010	
1. Percentage owner-occupied single family units					55.51%	
					FY 2009/2010	
2. Overall residential density within 1/2 mile of LRT stations					7.58 residents/acre	
3. Number of acres of parkland per 1,000 population						
Total acres				2,261.3	2,266.8	
Total population				457,849	466,685	
Acres per 1,000 population				4.94	4.86	
4. Total acres of parkland added					5.5 acres	
					FY 2009/2010	
5. Number of neighborhoods with 6 or more amenities					51.64%	
6. Child care: number of licensed child care facilities in a CP per 1,000 residents					TBD	
Family Child Care				967		
Per 1,000 population				2.11		
Child Care Center				154		
Per 1,000 population				0.34		
7. Number of certified farmers markets				9	9	

Housing Affordability and Diversity

	2005	2006	2007	2008	2009	2010
1. Percentage of homes that are affordable to median income families						
Median Houshold Income		\$46,055	\$49,849	\$50,958	\$47,107	
Median Price of Home		\$339,500	\$300,000	\$291,400	\$250,300	
<i>Price/Income Ratio</i>		7.37	6.02	5.72	5.31	
2. Number of households that spend more than 30 percent of income on rent						
	35,162	32,082	38,499	40,888	50,846	
3. Production of affordable housing units by type						
Extremely Low Income (ELI)	0	0	53	1	0	
Very Low Income (VLI)	578	410	125	106	24	
Low Income (LI)	431	345	346	707	100	
Moderate Income (MOD)	473	684	920	900	42	
<i>Total</i>	1,482	1,439	1,444	1,714	166	
4. Number of foreclosures						
Notice of Default (NOD)				8,212	8,221	
Real Estate Owned (REO)				5,742	4,670	

Mobility and Transportation Choices

	2005	2006	2007	2008	2009	2010
1. Mode split for trips	TBD					
Auto	87.00%			TBD		
Transit	6.10%			TBD		
Bike/Walk	7.00%			TBD		
2. Vehicle Miles Traveled						
Household-generated weekday VMT	6,197,787			TBD		
Weekday VMT per capita	14.54			TBD		
3. Regional Transit			Pre 2008 Avg. (baseline)	1/1/08-9/5/09 (7% Service Cut)	9/6/09-6/19/10 (4% Service Cut)	
Bus Revenue Hours			1,362	1,290	1,209	
Bus Revenue Miles			14,103	13,610	12,943	
<i>Daily Boardings</i>						
Bus			30,220	34,698	32,185	
Light Rail			40,148	44,045	40,762	
4. Miles of bike lanes						
Class I				75.8	76.7	
Class II				214.0	235.5	
Class III or II/III combo				26.0	29.0	

Public Safety

	2007	2008	2009	2010
1. Number of crimes reported under FBI Uniform Crime Report (UCR) by type				
Violent Crime	5,128	4,660	4,165	
Property Crime	24,399	22,499	21,001	
2. Number of total collisions by type				
Other motor vehicle		3,741	3,138	
Fixed object		517	495	
Parked motor vehicles		456	376	
Bicycle		291	226	
Pedestrian		230	182	
Other objects		71	77	
Non-collision		41	40	
3. Average response times for police				
Priority 1		4:07	3:17	
Priority 2		6:50	7:15	
Priority 3		8:51	8:50	
4. Average response times for fire			5:08	
5. Total number of citizens who have participated in city sponsored disaster preparedness education				
	3948	4644	4403	
6. With at least 500-year protection				
		18.5%	18.3%	

Energy, Water, and Waste

	2005	2006	2007	2008	2009	2010
1. Residential electricity and natural gas consumption						
<i>Electricity (kWh)</i>						
Citywide	1,307,297,646	1,356,424,402	1,330,221,618	1,349,787,559	1,362,496,399	TBD
Per Capita	2,748	2,961	2,851	2,845	2,832	TBD
Per Household	7,745	8,016	7,808	7,731	7,689	TBD
<i>Natural Gas (Therms)</i>						
Citywide	65,698,581	68,259,079	67,943,491	67,186,096	69,892,661	TBD
Per Capita	138	149	146	142	145	TBD
Per Household	389	403	399	385	394	TBD
2. Non-residential electricity and natural gas consumption						
<i>Electricity (kwh)</i>						
Citywide	2,319,281,156	2,471,710,797	2,478,886,777	2,480,335,739	2,411,768,200	TBD
<i>Natural Gas (Therms)</i>						
Citywide	61,998,270	66,372,188	66,976,990	68,521,938	65,559,751	TBD
3. PV installations and installed capacity (rooftop and ground mounted)						
Cumulative Number of Photovoltaic Installations	231	250	296	388	455	493 (as of 8/3/10)
New PV installations		19	46	92	67	
Cumulative Installed capacity (kW)	2,999.30	3,183.40	3,335.80	3,568.70	3,943.90	4192.3 (as of 8/3/10)
4. Percentage of electricity use from renewable sources in SMUD's service area	13%	15%	16%	20%	20%	22% (projected)

Energy, Water, and Waste

5. Water usage						
Per capita		TBD	TBD	TBD	TBD	TBD
		FY 05/06	FY 06/07	FY 07/08	FY 08/09	FY 09/10
Number of meters installed		638	2,001	2,221	1,982	20,800
% completed to date						29%
6. Tons of solid waste						
Reporting-Year Disposal Amount (tons)	666,799	574,711	515,079	477,253	423,325	TBD
Calculated Disposal Rate (CDR) lbs/capita/day (Note: Target is 6.9)	unknown	unknown	6	5.5	4.8	TBD
Calculated Disposal Rate (CDR) lbs/employee/day (Note: Target is 10.8)	unknown	unknown	9.3	8.5	7.7	TBD
7. Recycling Volume (tons diverted)						
Type						
Residential Curbside Green Greenwaste	86,448.75	80,258.55	71,199.06	78,265.23	73,751.06	TBD
Hazardous	unknown	unknown	381.16	370.58	384.27	TBD
Electronic	unknown	unknown	unknown	128.42	135.91	TBD
User						
Residential Recycling	36,978.27	37,265.19	40,665.13	39,661.94	37,502.27	TBD
Commercial Recycling	2,826.79	2,273.45	2,564.24	2,590.52	2,375.05	TBD
Residential & Commercial Recycling	39,805.06	39,538.64	43,229.37	42,252.46	39,877.32	
8. Number of "Green" Buildings						
LEED Certified (silver, gold, or platinum)						Cumulative: 27 LEED Certified as of 7/30/10
LEED for Homes (new only)						Not yet tracked
Green Point Rated Homes (new & existing)						None in city boundary; 2 in County

Carbon Footprint Air Quality

	2005	2006	2007	2008	2009	2010
1. Carbon dioxide emissions (in metric tons CO2e)						
2. City operations (MT CO2e)	78,584 (GHG Inventory)	Unknown. Funding is not available for continued tracking.				
3. Community wide (MT CO2e)	4,553,051	Unknown. Funding is not available for continued tracking.				
4. Community wide per capita (MT CO2e)	9.9	Unknown. Funding is not available for continued tracking.				
5. Peaked Number of days the Region exceeds Federal and State 8-hour air-quality standards	36	43	18	33	21	4

Equity

	2006	2007	2008	2009	2010
1. Number of residents below poverty level		14.30%			
2. High school graduation rates					
	FY 05/06	FY 06/07	FY 07/08	FY 08/09	FY 09/10
3. Percentage of local high school graduates who completed all courses required for admission to UC and CSU systems	51.90%	40.70%	42.20%	Data not available until November, 2010	TBD
4. Number of homeless persons (Countywide)		2,452	2,678	2,800	TBD
5. Number of subsidized school lunches : 2007 - Grant Joint Union became Twin Rivers Unified	53,889	49,262	62,018	64,500	TBD

Civic Engagement and Government Services

	2006	2007	2008	2009	2010
1. City service dollar spent per resident (expenditure per capita) using FY Approved Budget	FY 05/06	FY 06/07	FY 07/08	FY 08/09	FY 09/10
Fire	\$173	\$205	\$204	\$214	\$214
Neighborhood Services	\$4	\$4	\$3	\$3	\$2
Parks & Recreation	\$101	\$114	\$120	\$98	\$83
Police	\$260	\$279	\$289	\$289	\$279
2. Voter turnout (Local Election)					
Total registered city voters	193,770		216,220		
Total ballots cast	110,535		169,488		
Turnout percentage	57.04%		78.39%		
3. Public library cardholders as percentage of total population					
Total cardholders			206,734	245,307	
Percentage of total population			45.80%	53.58%	
			FY08/09	FY09/10	
4. Number of volunteer hours per year			322,281	390,437	

DATA SOURCE LISTING

City Profile	
1. Total Population	U.S. Census Bureau, American Community Survey
2. Physical Area of City (sq. miles)	City GIS
3. Diversity: Percent population* by ethnicity/race	U.S. Census Bureau, American Community Survey
4. Density: Number of people per square mile	City GIS
5. City of Sacramento Unemployment Rate	Bureau of Labor Statistics
6. Median Household Income	U.S. Census Bureau, American Community Survey
7. Cost of Living (U.S. Avg. 100)	city-data.com
Urban Forest	
1. Percentage cover tree canopy	Sacramento Tree Foundation
2. Number of trees in Sacramento	City Department of Transportation, Tree Services
3. Tree health (percentage of)	City Department of Transportation, Tree Services
Economic Diversity	
1. City Bond Rating	City Treasurer's Office
2. Jobs/Housing Balance	
Total Employment	City Accela Database - Development Activity for office, retail, industrial
Housing Units	U.S. Census Bureau, American Community Survey
3. Educational Attainment	U.S. Census Bureau, American Community Survey
4. Growth in selected industry sectors (County)	U.S. EDD
5. Growth in selected industry sectors (City)	U.S. Census Bureau, American Community Survey
6. Number of startups/new businesses by type	TBD
7. Number of minority- and female-owned businesses	TBD
Vibrant Central City	
1. Number of New Housing Units	TBD
2. Affordability of rental housing for average worker (e.g. income needed by occupation)	TBD
3. Total Annual Revenue	TBD
4. Total annual attendance at major arts and cultural venues	
Crocker Art Museum (Fiscal Year)	City General Services
Sacramento Theater Company (Season: October through May)	Sacramento Theater Company
Sacramento Ballet (Collected FY Oct - May)	The Sacramento Ballet
Convention Center Complex = Memorial Auditorium, Convention Center, Community Center Theater (FY)	City General Services

DATA SOURCE LISTING

California Music Theatre - Music Circus	California Music Theater
Railroad Museum (Jan-Dec) Excursion Train Ridership (Jan-Dec)	Capital District State Museums & Historic Parks
Farmers Market at Cesar Chavez Park	Downtown Sacramento Partnership
Friday Night Concerts in the park	Downtown Sacramento Partnership
Infill and Redevelopment	
1. Number of residential units and sq.ft. of other buildings in designated infill areas	City Community Development Department, Infill Coordinator
2. Redevelopment Areas total dollars of investment*	Sacramento Housing and Redevelopment Agency
3. Redevelopment Areas total tax revenue	Sacramento Housing and Redevelopment Agency
4. Number of development projects in a Tier One site	TBD
Historic Preservation	
1. Number of building permits in historic districts by type	City Accela database
2. Number of Housing Cases	City Accela database
Complete Neighborhoods	
1. Percentage owner-occupied single family units	City Accela Database/GIS
2. Overall residential density within 1/2 mile of LRT stations	City Accela Database/GIS
3. Number of acres of parkland per 1,000 population	City Parks and Recreation
4. Total acres of parkland added	City Parks and Recreation
5. Number of neighborhoods with 6 or more amenities	City Accela Database/GIS
6. Child care: number of licensed child care facilities in a CP per 1,000 residents	Child Action
7. Number of certified farmers markets	www.MarketLocations.com
Housing Affordability and Diversity	
1. Percentage of homes that are affordable to median income families	U.S. Census Bureau, American Community Survey
2. Number of households that spend more than 30 percent of income on rent	U.S. Census Bureau, American Community Survey
3. Production of affordable housing units by type	Sacramento Housing and Redevelopment
4. Number of foreclosures	Sacramento Housing and Redevelopment
Mobility and Transportation Choices	

DATA SOURCE LISTING

1. Mode split for trips	Sacramento Area Council of Governments
2. Vehicle Miles Traveled	Sacramento Area Council of Governments
3. Regional Transit	Sacramento Regional Transit
4. Miles of bike lanes	City Department of Transportation
Public Safety	
1. Number of crimes reported under FBI Uniform Crime Report (UCB) by type	U.S. Department of Justice
2. Number of total collisions by type	City Department of Transportation
3. Average response times for police	City Police Department
4. Average response times for fire	City Fire Department
5. Total number of citizens who have participated in city sponsored disaster preparedness education	City Office of Emergency Services
6. With at least 500-year protection	City Department of Utilities
Energy, Water, and Waste	
1. Residential electricity and natural gas consumption	SMUD, PG&E
2. Non-residential electricity and natural gas consumption	SMUD
3. PV installations and installed capacity (rooftop and ground mounted)	SMUD
4. Percentage of electricity use from renewable sources in SMUD's service area	SMUD
5. Water usage	City Department of Utilities
6. Tons of solid waste	City Department of Utilities
7. Recycling Volume (tons diverted)	City Department of Utilities
8. Number of "Green" Buildings	U.S. Green Building Council, Build it Green

DATA SOURCE LISTING

Carbon Footprint & Air Quality	
1. Carbon dioxide emissions (in metric tons CO2e)	2005 Greenhouse Gas Emissions Inventory
2. City operations (MT CO2e)	2005 Greenhouse Gas Emissions Inventory
3. Community wide (MT CO2e)	2005 Greenhouse Gas Emissions Inventory
4. Community wide per capita (MT CO2e)	2005 Greenhouse Gas Emissions Inventory
5. Peaked Number of days the Region exceeds Federal and State 8-hour air-quality standards	Sacramento Metropolitan Air Quality Management District
6. Peaked Number of Days Sacramento County (including City) exceeds Federal and State 8-hour air-quality standards	Sacramento Metropolitan Air Quality Management District
Equity	
1. Number of residents below poverty level	U.S. Census Bureau, American Community Survey
2. High school graduation rates	California Department of Education
3. Percentage of local high school graduates who completed all courses required for admission to UC and CSU systems	California Department of Education
4. Number of homeless persons (Countywide)	County Department of Human Services
5. Number of subsidized school lunches : 2007 - <i>Grant Joint Union became Twin Rivers Unified</i>	California Department of Education
Civic Engagement and Government Services	
1. City service dollar spent per resident (expenditure per capita) using FY Approved Budget	City Budget Document, FY 2009/2010
2. Voter turnout (Local Election)	Sacramento Voter Registration and Elections
3. Public library cardholders as percentage of total population	Sacramento Library
4. Number of volunteer hours per year	City Volunteer Coordination

SECTION III

2030 General Plan Annual Report IMPLEMENTATION PROGRAM

The 2030 General Plan was adopted by Council on March 3, 2009, and went into effect on April 2, 2009. If the City's General Plan is to be effective, it must be reviewed, maintained, and implemented in a systematic and consistent manner. Part IV of the 2030 General Plan is an implementation program that identifies phasing of implementation, as well as a summary of the most important programs for implementing the 2030 General Plan, particularly those that need to be undertaken in the first three years (2009–2011) after adoption. The General Plan implementation program lists specific implementation measures for each element, the community plans, and the administration of the Plan.

General Plan Implementation - 2010

Although there has been a lot of activity in implementing the General Plan, many departments reported that the economic downturn has limited their resources, and as a result, some of the implementation measures will not be completed as soon as initially was hoped, or even be completed by the 2011 goal. Impacts from the current economy include:

- Recent staff reductions
- Decreased revenue for the city's enterprise funds
- Slowed development
- More restrictive use of funds, such as Measure A funds
- Reduction of redevelopment funding for matching
- Lack of general fund revenue
- Slower sale of State bonds that has slowed or halted some projects.

Nevertheless, progress towards implementing many of the measures in the General Plan has been made. Highlights of recent accomplishments follow:

- In collaboration with SHRA, the City has been working to encourage infill development in several shovel-ready areas including Florin Road, the River District, Railyards, Curtis Park Village, Northeast Line stations, and the Technology Village Area (65th Street).
- Phase 1 of the Climate Action Plan was presented to City Council in February 2010. This phase identifies strategies that reduce greenhouse gas emissions from the City's internal operations 22% below 2005 levels by 2020.
- Phase 2 of the Climate Action Plan is currently underway and will identify strategies to reduce greenhouse gas emissions from all communitywide

emission sources. Phase 2 is expected to be substantially completed by July 2011.

- The joint City/County Green Building Task Force launched in spring 2010, and recommendations are expected to be delivered by December 2010 and will potentially include strategies to improve energy efficiency in existing buildings, including rental housing.
- Sacramento Historic Register Update is now available on the City's website.
- Preservation Commission's Historic Survey Committee has commenced scoping the task of surveying and creating an inventory of the entire city.
- Two transportation-related studies have been started, including Ramona Avenue and "Cars on K."
- Council approved an ordinance to require recycling and reuse of construction wastes, including recycling materials generated by the demolition and remodeling of buildings.
- Community outreach continues to be an important component of the Police Department's services. In the summer of 2010, the department conducted Fitness Boot Camp, a six week program staffed largely by volunteers that focuses on developing "pillars of character," including physical fitness, self esteem, honesty, integrity, fairness, and justice.

The following pages provide a matrix of the General Plan implementation measures that were projected to be completed by 2011 or are considered an annual or on-going task. The numbering of the implementation measures corresponds to how they were numbered in the General Plan. The numbering in this report is off-sequence because the implementation measures scheduled to be completed in 2015 and 2030 were omitted from the matrix, but will be included in next year's annual report.

IMPLEMENTATION AREA: ADMINISTRATION

#	Implementation	Completed	In Progress On Schedule	In Progress Not on Schedule	Delayed/Deferred	2010 Update
1	The City shall prepare an annual report to the City Council on the status of the General Plan and progress in its implementation, including the status of priority implementation programs and the progress in meeting the city's fair share of regional housing needs, and submit it to the California Office of Planning and Research.		x			Staff took annual report to Council in November, 2009, and plans to return in December 2010. The report will be submitted to the California Office of Planning and Research as is required per state code
3	The City shall work with Sacramento Local Agency Formation Commission (LAFCo) to update and adopt a Sphere of Influence consistent with the growth planned for in the General Plan.				x	Budgetary constraints do not favor a comprehensive update of the SOI. Rather, staff is working on 3 major geographic areas for possible SOI amendments. The General Plan contemplates development of the Camino Norte site (400 acres east of El Centro in North Natomas); a Sphere of Influence Amendment was initiated by the City in December 2006, but progress has lagged due to lack of landowner funding for environmental review. The General Plan also provides for study area status for the Natomas Joint Vision Area; City and County Planning staffs are working with the landowners to determine whether to initiate a request for SOI amendment. The General Plan also provides for study area status for the East Policy Area. Most recently, the County staff has indicated that they will not support a SOI amendment for this area; the County intends to develop this area as an unincorporated growth area. City staff is working cooperatively with the County and landowners to master plan the area.
4	The City shall work with Sacramento Local Agency Formation Commission (LAFCo) to prepare and adopt a Municipal Service Review (MSR) to identify the existing capacity and means of financing the probable demand for services based on the build out of the General Plan.				x	City staff has prepared MSR's for Aspen 1 (brought into the City's SOI in April 2009) and Camino Norte (SOI amendment in progress). Staff has approached the landowners of the Natomas Joint Vision, but has not yet obtained landowner funding for a MSR. A comprehensive MSR (covering all potential growth areas) is unfunded. An MSR does not typically identify detailed means of financing public infrastructure. This level of detail for financing public infrastructure would be addressed through a finance plan and Plan For Services that are accomplished during the annexation phase.
5	The City shall improve upon and expand the city's MATRIX program to include outside agencies and neighborhood and business groups.		x			Development projects are routed to neighborhood groups, outside agencies, and business groups.
6	The City shall develop a Livability Index program (i.e., Indicators Program) that monitors the success of the City in achieving the goals of the General Plan.	x	x			Staff has developed a Livability Index consisting of 14 indicators and several data points for each indicator.
7	The City shall conduct an annual review of the Livability Index and report the findings to City Council.		x			Data and analysis of the Livability Index is included in the annual report. The 2010 Annual Report will be presented to the Planning Commission and City Council in December 2010.

IMPLEMENTATION AREA: ADMINISTRATION

#	Implementation					2010 Update
		Completed	In Progress On Schedule	In Progress Not on Schedule	Delayed/Deferred	
8	The City shall work with department managers and directors in evaluating the effectiveness of the General Plan in its role in guiding city operations, development review, master planning, and budgeting. A summary of this discussion and evaluation shall be reported to the City Council on an annual basis.		x			Staff works with the other departments to identify the status of the implementation measures identified in the Part IV of the 2030 General Plan. This status will be included in the annual report.
9	The City shall prepare and implement a public outreach program to educate business and community groups on the General Plan and its role in guiding city operations, development review, master planning, and budgeting.		x			Staff has provided presentations to the state, the Urban Design Alliance, and other groups and agencies. Outreach on the General Plan is also provided as it relates to specific implementation project such as the Northeast line and the rezones for General Plan consistency.
10	The City shall create a priority list for how sections of the Sacramento Code and applicable guidelines will be updated consistent with the General Plan in areas including, but not limited to, the following:			x		A "bundle" of zoning code amendments were approved by the Council in the fall of 2010. Staff will continue to bring forward amendmens to the zoning code as priorities are identified. Staff applied for state and federal grants to update the zoning code. If awarded, prioritization will be part of the update process.
	Streamlining infill development					
	Building heights					
	Densities/intensities					
	Transit-oriented development regulations					
	Mixed-use development					
	Urban design and transitions between uses					
	Green/sustainable development practices					
	Transportation/streetscape standards					
	Parking standards					
	Neighborhood/building preservation					
	Protection of Historic Resources					
	Parks and open space standards					
	Site Planning					
11	The City shall identify and prioritize key areas (e.g., transit centers, mixed-use corridors), development sites, opportunity areas, and infill areas for rezoning to promote infill development and ensure consistency with the General Plan.	x				The City Concl confirmed the location of Tier One and Tier Two Shovel Ready sites in October 2009.
12	The City shall review master plans and service level goals and standards to ensure they achieve the highest level of services the City can reasonably afford, using the tools and methods described in the Citywide Financing Strategy.		x			Staff continues to participate in the review of the TPG, and is currently working with Utilities' on their master plans and prioritization of CIPs.

IMPLEMENTATION AREA: ADMINISTRATION

#	Implementation	Completed	In Progress On Schedule	In Progress Not on Schedule	Delayed/Deferred	2010 Update
13	The City shall review and update the Sacramento Code, Zoning Ordinance, and applicable guidelines, consistent with the policies and diagrams of the General Plan.	x				Staff brought a second round of zoning code modifications in the Fall of 2010.
14	As part of its annual review and adoption of the Budget and Capital Improvement Program, the City shall review the policies and implementation programs of the General Plan to ensure consistency in the Budget and Capital Improvement Program.		x			Long Range Planning Staff provided a General Plan consistency review of the FY 10/11 CIP.
15	The City shall prepare and implement internal guidelines for preparation and adoption of geographic and operational master plans and strategies that includes the types of master plans and strategies that will be prepared, updated, and adopted by city departments in the future.		x			The assignment is in progress and will be completed by 2011.

IMPLEMENTATION AREA: LAND USE

#	Implementation					2010 Update
		Completed	In Progress On Schedule	In Progress Not on Schedule	Deferred	
1	The City shall develop and maintain a citywide database of vacant and underutilized sites to monitor the city's growth and change. The City shall prepare an annual report on the number of vacant sites and underutilized sites that were developed during the previous year.				x	Due to lack of resources, this effort has been indefinitely delayed.
2	The City shall prepare a comprehensive update of the Infill Strategy, including integration of the city's Commercial Corridor Revitalization Strategy into the Infill Strategy, to address obstacles to development in target infill areas, commercial corridors, and key opportunity sites.			x		Progressing slowly due to lack of resources.
3	The City shall submit an annual report to the City Council that evaluates implementation of the Infill Strategy.		x			City is in the process of preparing an annual report. Will be presented to Council by end of 2010.
5	In conjunction with the Shovel Ready Program, the City shall work with the Sacramento Housing and Redevelopment Agency to identify key infill sites in opportunity areas and established infill areas and ensure that major entitlements and incentives (e.g., rezone, CIP investment, environmental review, and economic development assistance) are in place to facilitate development.		x			In collaboration with SHRA, the City has been working to encourage infill development in several shovel-ready areas including Florin Road, the River District, Railyards, Curtis Park Village, Northeast Line stations, and the Technology Village Area (65th Street).
6	The City shall work with Sacramento County to develop a Master Property Tax sharing agreement for annexation areas.	x				City staff researched the previous master property tax sharing agreement as well as more recent individual project annexation agreements. Rather than developing a new master agreement, staff has completed (in 2010) an identification of considerations (e.g., existing tax base and municipal service loads) for future tax sharing agreements.
7	The City shall update the zoning code to identify a process to allow a development project to exceed maximum allowed FAR, based on the project's "significant benefits" to the public. "Significant benefits" shall also be defined through the zoning code update.		x			To be completed by 2011.
8	The City shall prepare guidelines that describe the City's process for qualifying for CEQA streamlining for residential mixed-use projects and "Transportation Priority Projects" as provided under State law.	x				On schedule, pending data/research from SACOG and completion of MTP update.

IMPLEMENTATION AREA: LAND USE

#	Implementation	Completed	In Progress On Schedule	In Progress Not on Schedule	Deferred	2010 Update
9	The City shall develop and implement a green neighborhood design checklist to evaluate large-scale residential development in new growth areas. The checklist will incorporate principles that support healthy sustainable neighborhoods (e.g., Healthy Development Checklist and green rating programs such as LEED for Neighborhood Development). Compliance with the checklist would qualify the development for incentives such as reduced fees, expedited entitlement and permit processing, and density bonuses for new construction.			x		Staff applied for the state's Sustainable Communities Planning grant. Pending grant award, this program will be completed by 2014.
10	The City shall develop and implement a program to promote sustainable development through regular regional coordination meetings that include local government representatives and other applicable agencies.	x				Community Development and General Services staff meet regularly with the Sacramento Area Green Partnership, the Build It Green Public Agency Council, and other alliances and partnerships with the County of Sacramento, SMUD, and other jurisdictions in the region. Additionally, the Green Building Task Force is a joint City/County program that will be convened in January 2010 to develop recommendations for moving forward with a comprehensive set of green building design guidelines & standards.
12	The City shall work with the Sacramento Housing and Redevelopment Agency to prepare midterm implementation plans status reports every 2.5 years, in accordance with state Redevelopment Law.		x			In progress and on schedule.
13	The City shall work with the Sacramento Housing and Redevelopment Agency to review and update the Consolidated Plan every 5 years.		x			A status of the Consolidated Plan is provided in the annual Housing Element Annual Report.
14	The City shall work with the Sacramento Housing and Redevelopment Agency to prepare and submit an annual report to the City Council that evaluates implementation of the Consolidated Plan.		x			In progress and on schedule.
15	The City shall provide ongoing training to staff and their respective commissions to implement the General Plan.		x			Staff continues to provide General Plan training on a regular basis.
16	The City shall amend the Sacramento Code to establish additional standards, including cool roofing, green roofs, light colored pavement, and other measures, to minimize the heat island effect. Such standards shall be incorporated into the City's Green Building Program and Climate Action Plan, as appropriate.			x		Staff applied for the state's Sustainable Communities Planning grant. Pending grant award, this program will be completed by 2014.

IMPLEMENTATION AREA: LAND USE

#	Implementation					2010 Update
		Completed	In Progress On Schedule	In Progress Not on Schedule	Deferred	
18	The City shall develop and implement an educational program to train City staff and inform the development community and other community groups about the new land use and urban form guidelines and standards and policies of the General Plan.		x			Staff has provided presentations to the state, the Urban Design Alliance, and other groups and agencies. Outreach on the General Plan is also provided as it relates to specific project such as the implementation of the Northeast line and the rezones for General Plan consistency.
19	The City shall prepare and adopt Central City Urban Design Guidelines and Plan.	x	x			The Central City Urban Design Guidelines were approved by Council in May 2009. Staff is currently working on an infrastructure study for the Central City Urban Plan.
21	The City shall work with the City of West Sacramento to update and adopt the 2003 Sacramento Riverfront Master Plan.				x	This should be considered with first 5-year update of the General Plan.
23	The City shall prepare and adopt guidelines for the conversion of nonresidential uses to residential uses.				x	Resources not available
24	The City shall work with Sacramento State University to establish a committee that includes city staff, University staff, and community members to plan development around the Sacramento State University campus.			x		The Technology Village Specific Plan is in progress. Estimated time of completion is 2012.
25	The City shall work cooperatively with Cal Expo to plan future development on the State Fair grounds, including consideration of the proposed development of an 18-20,000 seat indoor multi-purpose arena.			x		In progress. Working with Cal Expo to explore options for improvements to Cal Expo.
26	The City shall work with local hospitals to conduct a study to identify possible locations for a new hospital north of the American River.	x				A hospital site is tentatively sited in North Natomas as part of a private development application.

IMPLEMENTATION AREA: HISTORIC AND CULTURAL RESOURCES

#	Implementation	Completed	In Progress On Schedule	In Progress Not on Schedule	Delayed/Deferred	2010 Update
1	The City shall report annually to the City Council on the status of the Preservation Office, Commission, and programs.			x		Due to lack of resources, an annual report to Council is not scheduled at this time.
2	The City shall review the established criteria and standards for research, survey, assessment, inventory, designation, alterations, additions, rehabilitation, reconstruction, restoration, and preservation of historic and cultural resources to ensure that they remain consistent with federal and state Standards and criteria.			x		Staff continues to work with Commission and outside parties to finalize as resources allow.
3	The City shall expand and update the existing context statement for Sacramento as necessary to include more recent history of the Sacramento area in order to maintain a basis for evaluating the significance of a resource.				x	Not complete, and not anticipated to be undertaken this or next fiscal year due to budget limits.
4	The City shall maintain provisions in the Sacramento Code for a preservation program consistent with the federal and state Certified Local Government requirements.		x			This program is maintained on an ongoing basis. Staff, program, and commission comply with CLG requirements.
5	The City shall ensure that public and private projects and activities involving historic and cultural resources are consistent with the General Plan and Preservation Ordinance and shall periodically review and update appropriate procedures, including sanctions for non-compliance.		x			A major part of Preservation staff's workload involves these items.
6	The City shall review the established minimum maintenance program and standards for historic properties and identify other options and programs to provide for maintenance and upkeep of historic properties and resources.			x		The Preservation Commission has an ad hoc Minimum Maintenance Committee looking at incentives and regulatory options to ensure compliance.
8	The City shall designate additional Landmarks and Historic Districts based on the findings of survey efforts.		x			River District nominations proceeding contemporaneously with River District Plan adoption, expected to go to Council by the end of 2010.
9	The City shall conduct a study to assess the need for Historic Preservation Overlay Zones and other mechanisms to increase protections for historic resources, and review and revise, as needed, the Zoning Ordinance.				x	The implementation of this program is dependent on resources becoming available.

IMPLEMENTATION AREA: HISTORIC AND CULTURAL RESOURCES

#	Implementation	Completed	In Progress On Schedule	In Progress Not on Schedule	Delayed/Deferred	2010 Update
10	The City shall review its Listed Structure Plan for larger commercial structures and other resources not covered in the Preservation Development Standards for Residential and Neighborhood Commercial Properties.				x	Preservation Development Standards currently underway, but have been limited to residential properties only. Neighborhood and larger commercial structure standards will be pursued dependent on availability of resources. Likely to not have funding until or after 2012.
11	<p>The City shall continue to incorporate information on historic resources into its Geographic Information System (GIS), Automated Permit System (APS), web site, and video production. The City shall make copies (e.g., brochures and booklets) of the above information available to interested parties, as follows:</p> <ul style="list-style-type: none"> How to Nominate a Property to the Sacramento Register City's application review process California Historical Building Code Loan or grant programs, if any Secretary of the Interior's Standards for the Treatment of Historic Properties, and other applicable Secretary of the Interior standards Sacramento Register Preservation Development Standards Displays of historic/archaeological artifacts. California Environmental Quality Act (CEQA) relative to historic and cultural 	x				Sacramento Register has been posted on the website.
12	The City shall develop procedures that integrate preservation (e.g., historic resource surveys) in other land use planning efforts such as Community Plans, Redevelopment Area Plans, and Specific Plans.		x			The River District Specific Plan is on-going and incorporates an update to the Richards Boulevard Historic Survey and will include nominations of recommended historic properties.

IMPLEMENTATION AREA: HISTORIC AND CULTURAL RESOURCES

#	Implementation	Completed	In Progress On Schedule	In Progress Not on Schedule	Delayed/Deferred	2010 Update
14	The City shall amend the Sacramento Code relative to archaeological resources to require pre-construction field surveys, research and testing procedures for those areas proposed for grading, excavation or construction in high-sensitivity areas, and to require discovery procedures for archaeological resources found during grading, excavation, or construction, whether or not the project site is located in a high-sensitivity area. These procedures shall include protocols and criteria for qualifications of personnel, and for survey, research, testing, training, monitoring, cessation and resumption of construction, identification, evaluation, and reporting, as well as compliance with recommendations to address any significant adverse effects where determined by the City to be feasible.					TBD
15	The City shall amend the Sacramento Code to require discovery procedures for paleontological resources found during grading, excavation, or construction. These procedures shall include protocols and criteria for qualifications of personnel, and for survey, research, testing, training, monitoring, cessation and resumption of construction, identification, evaluation, and reporting, as well as compliance with recommendations to address any significant adverse effects where determined by the City to be feasible.			x		Staff plans to initiate an evaluation of the work that needs to be done, pending staff and resources availability.
16	The City shall work with its partner public and private organizations to develop and implement a heritage tourism plan through its Preserve America Community designation.		x			Staff has submitted a grant application for the Preserve Americas Cities grant. Awards will announced in November.

IMPLEMENTATION AREA: ECONOMIC DEVELOPMENT

#	Implementation	Completed	In Progress On Schedule	Not In Progress on Schedule	Delayed/Deferred	2010 Update
1	The City shall develop an aggressive marketing campaign that promotes the city's assets and strengths to prospective businesses and employees looking to move to the region.		x			The Economic Development Department, in cooperation w/ Sacramento County, City of Rancho Cordova and the Sacramento Employment Training Agency (SETA) submitted an application for an expanded Enterprise Zone to the State of CA Housing & Community Development Dept. In August 2009, the partnership was awarded conditional designation and is working towards final approval. The team is developing an aggressive marketing strategy, including a website, www.sacez.org, which will be live in October 2010.
2	The City shall review and update the Economic Development Strategy every 5 years.		x			An update to the economic development strategy will be initiated as part of the City's economic recovery efforts.
3	The City shall submit an annual report to the City Council that evaluates implementation of the Economic Development Strategy.		x			An annual report for 2009-2010 fiscal year will be initiated.
4	The City shall work with government, businesses, and higher education industries to establish targeted marketing programs (e.g., "Campus Sacramento Initiative") to assimilate current students and new college graduates into the city's and region's workforce.		x			The City continues to support the quarterly LEED Convergence events and is also supporting LEED's career education fair in the Fall 2010 at Luther Burbank High School in South Sacramento. The purpose of the fair is to educate high school students about careers and post-secondary training, provide students with a better understanding of the necessary skills, training requirements, and educational pathways for their field of interest, and prepare students to make an informed decision about their future.
5	The City shall survey existing businesses and targeted growth industries to identify workforce skill needs and use this information to recruit technical institutions with specialized training programs that focus on these needs.		x			The City's Economic Development Department is supporting the efforts of SETA and the Los Rios Community College District to train displaced construction workers in the growing green construction trades. The City is also supporting Sacramento City College's Career Technical Education Programs and the development of new classroom facilities on campus to accommodate the growth of these programs.
7	In conjunction with the preparation of opportunity area urban form concepts, the City shall develop specific economic development strategies for targeted opportunity areas.		x			As part of the department's marketing effort and attraction efforts, informational fact sheets highlighting key opportunity areas throughout the city have been made and distributed.
9	The City shall work with SACTO and other local economic development organizations to develop an "economic intelligence" database of market and industry knowledge about companies the City desires to recruit and locate within the city limits and the region.		x			The City continues to work with all its appropriate economic development partners to establish this "economic intelligence" database.
11	The City shall conduct a study to identify and establish one of the city's opportunity areas as a center for cutting-edge technology businesses and develop a marketing program to attract firms to that area.	x				A study to assess this opportunity area was conducted and completed during the fourth quarter of the 2010 calendar year.

IMPLEMENTATION AREA: ECONOMIC DEVELOPMENT

#	Implementation	Completed	In Progress	On Schedule	Not In Progress on Schedule	Delayed/Deferred	2010 Update
12	The City shall develop a "business leads action process" (i.e., materials and processes that respond to business leads) that assures quick and complete responsiveness to business opportunities.	x					The Economic Development Department has developed materials and processes for the action process. Additionally, staff has individual geographical and industry-specific knowledge to complete the process.
13	The City shall conduct customer service rating surveys to elicit feedback from businesses in the city on city services and adjust its customer service policies and procedures to provide the best possible service.	x	x				Economic Development has conducted 5 business walks within calendar year 2010, resulting in contacts with over 625 businesses.

IMPLEMENTATION AREA: MOBILITY

#	Implementation	Completed	In Progress On Schedule	In Progress Not on Schedule	Delayed/Deferred	2010 Update
2	The City shall update its Traffic Impact Analysis guidelines to reflect the Level of Service (LOS) policies standards in the General Plan.		x			Staff is working on the updated the guidelines for staff evaluation.
3	The City shall prepare and adopt a methodology to measure neighborhood level of service.		x			Staff will coordinate to address this need over the 2011 calendar year.
6	The City shall expand the city's existing wayfinding programs to other areas of the city and improve wayfinding signage		x			DOT acts as support for this effort to Economic Development.
9	The City shall conduct a study to identify major barriers to connectivity and appropriate means and locations for overcoming those barriers, including potential river crossings.		x			The Planning and Policy Unit will complete a study to determine the appropriate location(s) and modal configuration for additional crossings of the Sacramento River by 2011. Funding is being sought for a similar study for the American River.
10	The City shall develop and adopt multi-modal circulation plans for all light rail and bus transfer station areas within the city.		x			Plans have been completed for Florin and Meadowview. 65th Street Plan should be approved by Council by 2011. Swanston Station Transit Village Plan should approved in the Winter of 2011.
11	The City shall update and enhance its Transportation System Management program consistent with the policies of the General Plan.		x			Planning and Policy staff have drafted the TSM ordinance amendments for Council consideration. Adoption expected by 2011.
12	The City shall conduct a study to identify economic incentives for private transportation partners seeking to enhance mobility in the Central City, centers, corridors, employment centers, and other high-intensity districts in the city.		x			Two studies have been started, including Ramona Avenue and "Cars on K." Once additional resources have been identified to conduct other studies and when staff availability warrants, additional progress will be made.
13	The City shall conduct a study to identify appropriate routes and roadway markings/signage for Neighborhood Electric Vehicles (NEVs) within the Central City and between urban centers and corridors.		x			State Highway Code already allows NEV's on streets signed for 35 mph or below and has standards for signage. Planning and Policy staff will identify potential gaps that could hamper a coordinated NEV network.
15	The City shall submit a bi-annual report to the City Council that evaluates implementation of the Pedestrian Master Plan.		x			Staff expects to bring a a report before Council in 2011.
16	The City shall work with walking advocates, such as WalkSacramento, to develop a comprehensive educational and promotional package for pedestrians.		x			DOT's Alternate Mode Coordinator will work with Traffic Engineering to develop educational and promotional benefits of walking akin to the Captain Jerry Program.

IMPLEMENTATION AREA: MOBILITY

#	Implementation					2010 Update
		Completed	In Progress On Schedule	In Progress Not on Schedule	Delayed/Deferred	
17	The City shall submit periodic reports to the City Council that summarizes traffic collision data at the top collision locations for automobiles, bicycles, and pedestrians, and recommend countermeasures where needed.		x			Traffic Engineering is considering best practices for collecting and reporting collision data. Periodic reports to the City Council could be part of the ultimate methodology chosen.
18	The City shall review and update its Bikeway Master Plan every 3 years.		x			Staff intends to begin the next BMP update for the City of Sacramento following the County's adoption of their update.
19	The City shall submit a bi-annual report to the City Council that evaluates implementation of the Bikeway Master Plan.		x			An update on the implementation of the BMP will be conducted along with the process of updating the BMP.
20	The City shall conduct a study to identify gaps in transit service provided within the city and strategies to fill them.		x			Planning and Policy Division is beginning a Citywide streetcar study to begin planning for future service.
24	City shall conduct a study of the existing street network to identify streets that can be more complete based upon adopted design standards and the policies in the General Plan.		x			Planning and Policy will investigate methods of evaluating the existing street network using information from the Bicycle Master Plan and the Pedestrian Master Plan to develop a comprehensive map of future needs.
25	The City shall continue its efforts to manage neighborhood traffic by incorporating traffic control measures in new residential neighborhoods and through the Neighborhood Traffic Management Program (NTMP) in existing residential neighborhoods.		x			This is an on-going function of the DOT.
27	The City shall develop and adopt an ordinance for bicycle support facilities (e.g., bicycle racks, personal lockers, showers, and other bicycle support facilities).		x			The TSM ordinance that has been drafted will address bicycle support facilities. DOT continues to install bike racks by owner request for the time being.
28	The City shall conduct a study of current parking requirements in the Central City and urban centers (i.e., Urban Center Low and Urban Center High) to evaluate options for dedicated parking spaces for car-sharing and incentives (e.g., receive credit for meeting the "parking minimum" zoning requirements).		x			DOT has selected Zip Car for the initial roll out of carsharing in Sacramento.
31	The City shall review and update its Helicopter Ordinance as needed to provide for business and emergency needs.		x			TBD
32	The City shall prepare and adopt a citywide transportation development impact fee program to support the development of all travel modes needed for new development.		x			A draft nexux study has been prepared and is under review.

IMPLEMENTATION AREA: UTILITIES

#	Implementation	Completed	In Progress	On Schedule	In Progress	Not on Schedule	Delayed/Deferred	2010 Update
1	The City shall prepare and adopt level of service (LOS) standards for each city-provided utility (i.e., water, wastewater, stormwater drainage, and solid waste).		x					Due to reduction in staff, some level of services have been amended. For example, Solid Waste has started a 4-10 schedule for residential schedule. Neighborhood Cleanup is still suspended. Illegal dumping is not part of DOU due to 218 issues. Code Enforcement is taking over in January. In Drainage, the response time during rain events will be affected. Wasterwater has reduced certain preventive maintenance of lower priority items and suspended renewal of service connections by the city. In Water, full service taps for the customers have also been suspended.
2	The City shall develop an asset management program for the maintenance of infrastructure facilities.		x					DOU is continuing to develop an asset management program through the use of condition assessment and its computerized maintenance management system (CMMS) to improve maintenance efficiencies as well as prioritizing its rehabilitation/replacement needs.
4	The City shall update existing and develop new fee programs to ensure adequate funding is available to provide infrastructure improvements for new development.		x					An Administrative Draft of the upcoming Nexus Study was submitted July 16, 2010. This is being reviewed and commented upon by City staff.
5	The City shall prepare and adopt standards for the development of joint use facilities (e.g., schools/parks/drainage detention).	x						The agreement has been incorporated in the Design and Procedures Manual (Appendix to Chapter 11) and a link has been placed on the DOU Intranet website.
6	The City shall review and update its Water Distribution System Master Plan every 5 years.		x					Development of Water Master Plan is in progress. Expected completion in 2011.
7	The City shall review and update its Urban Water Management Plan every 5 years.		x					UWMP work is not expected to begin until 2011 per SBx7 7. Some work on the Water Master Plan needs to be completed prior to the UWMP.
8	The City shall continue to produce and distribute an annual Operational Statistics Report which includes sections on water distribution, wastewater collection, storm drainage collection, and solid waste collection.		x					The DOU will continue to prepare and distribute Annual Operational Statistics Report.
10	The City shall continue to produce and distribute an annual Water Quality Consumer Confidence Report to verify that water quality standards are being met.		x					The DOU will continue to produce and distribute the report annually.
11	In coordination with the Regional Water Authority (RWA), the City shall develop and implement a voluntary landscaping water efficiency certification training program for irrigation designers, installers, and property managers.							TBD

IMPLEMENTATION AREA: UTILITIES

#	Implementation	Completed	In Progress On Schedule	In Progress Not on Schedule	Delayed/Deferred	2010 Update
12	The City shall update its Emergency Operations Center procedures to include procedures for providing potable water supplies during emergencies.		x			This is a continuing effort that is reflected in existing procedure, and will be incorporated into revised standard operation procedures that will be completed by end of 2012. Standard operating procedures that are more specific to the EOC environment.
13	As part of the Urban Water Management Plan update in 2010, required by the Urban Water Management Planning Act, the City shall update the response plan that outlines an approach on how to assist citizens in reducing water use during periods of water shortages and emergencies.		x			UWMP work is not expected to begin until 2011 per SBx7 7. Some work on the Water Master Plan needs to be completed prior to the UWMP.
16	The City shall review and update its Wastewater Master Planning Program every 5 years.	x				This task has been completed
17	The City shall prepare and adopt design standards that reduce infiltration into new city-maintained sewer pipes.	x				Task completed. Our current construction standards include connections that are substantially water tight, thus greatly reducing the inflow of extraneous groundwater infiltration.
18	The City shall review and update its Stormwater Drainage Master Planning Program every 5 years.		x			The DOU last updated the program in 2007. The next update will be in 2012.
19	The City shall prepare and adopt an ordinance to require recycling and reuse of construction wastes, including recycling materials generated by the demolition and remodeling of buildings.	x	x			Completed and now being implemented. Update also is in progress to match new California Building Standards Commission requirements.
20	The City shall review and update its Solid Waste Master Plan every 5 years.		x			Still seeking green waste transfer site north of American River to save cost of hauling south
21	The City shall prepare a plan to achieve energy efficiency targets.		x			The Climate Action Plan will address this. Cross-reference needed to Climate Action Plan implementation measures.
22	The City shall work with neighborhood associations, local electric and gas utilities, and other interested groups to develop programs to encourage conservation and increase energy efficiency (e.g., the SMUD / Sierra Curtis Neighborhood Association's "Curtis Park Energy Stars" program, etc.).		x			The City and SMUD will be launching a Neighborhood Performance Program in 2010 to target education and outreach on SMUD incentive programs for whole-house energy efficiency audits and retrofits, and a similar program will be launched to target small commercial building owners and tenants. The Mayor's Greenwise Sacramento Initiative is also currently underway, and is anticipated to include raising the community's "Green IQ" as a major outcome in terms of expanded policies, programs and funding to educate and engage the community in green choices to improve efficiency and save money.

IMPLEMENTATION AREA: UTILITIES

#	Implementation	Completed	In Progress On Schedule	In Progress Not on Schedule	Delayed/Deferred	2010 Update
24	The City shall conduct a study to explore the economic feasibility of using methane to generate electricity.		x			DOU Solid waste is looking at this again due to changes in the operation by Blue Diamond and they are not taking any of the methane gas.
26	The City shall prepare, adopt, and implement energy efficiency and water conservation standards for residential rental properties.		x			The City/County joint Green Building Task Force launched in Spring 2010, and recommendations are expected to be delivered by December 2010 and will potentially include strategies to improve energy efficiency in existing buildings, including rental housing. The Climate Action Plan, also currently in progress, will also include appropriate strategies to address existing housing. And finally, the State of California adopted a mandatory green building code, known as CalGreen, that includes water and waste provisions that will go into effect on January 1, 2011. Community Development staff will be working to identify strategies for implementation of CalGreen enforcement.
27	The City shall develop a public information program to promote sustainable development and resource conservation such as responsible purchasing and recycling.		x			Public information program development in progress.
28	The City shall annually review and modify building codes and development standards to facilitate the inclusion of evolving state-of-the-art telecommunication technologies and facilities.		x			Staff reviews telecommunication technologies as they are proposed by applicants.

IMPLEMENTATION AREA: EDUCATION, RECREATION & CULTURE

#	Implementation	Completed	In Progress On Schedule	In Progress Not on Schedule	Delayed/Deferred	2010 Update
2	The City shall review Chapter 15.132 Building Permits for Dwelling Units in Impacted School Areas of the Sacramento City Code to determine if changes are required.					TBD
3	The City shall review and update its Parks and Recreation Master Plan every 5 years to coincide with updates of the General Plan.			x		A technical update of the Parks and Recreation Master Plan 2005-2010 was adopted on April 21, 2009. A complete update of the Master Plan was originally scheduled to begin in 2010 but was postponed until 2014 to coincide with the City's General Plan update. It will be started in 2013.
5	The City shall establish standards for cultural heritage gardens and teen centers in the Parks and Recreation Master Plan.			x		Standards for cultural heritage gardens and teen centers will be established in conjunction with the Master Plan Update, scheduled for 2014.
6	The City shall study and incorporate into the city's Quimby Ordinance mechanisms to provide flexibility in meeting parkland requirements.		x			The Technical Update of the Parks and Recreation Master Plan 2005-2010 included new policies and guidelines for small public places (neighborhood parks that are smaller than 5 acres in size.) An evaluation of the city's Quimby Ordinance is underway, but the Ordinance evaluation does not preclude the City from implementing the policies and guidelines for small public places.
7	The City shall review and amend, as needed, the city's Quimby Ordinance and Park Development Impact Fee Ordinances to ensure that fee schedules keep pace with actual costs to acquire and develop parks/open space citywide.		x			The Park Development Impact Fee required no annual adjustment for the 2010-11 fiscal year. Preparation of a Park Development Impact Fee Nexus Study to evaluate fees in relation to actual costs is underway.
8	The City shall complete a study to identify potential new funding mechanisms to supplement existing ones for (1) acquisition and development of regional parks, parkways, recreation trails, and open space; (2) provision of equal Service Levels to underserved areas; (3) 100% ongoing maintenance; (4) recreation and community facilities; and (5) capital infrastructure repair/replacement.		x			(1) A new Nexus Study is underway and is scheduled for completion by the end of the 2010 calendar year. It includes provisions for the acquisition and development of regional parks, parkways, recreational trails and open space. (2) Priority Project of the DPR in FY 10-11; (3) 100% of ongoing maintenance is now the departmental standard for larger residential subdivisions. Tentative maps are conditioned to form or annex into a neighborhood park maintenance district providing full cost recovery; (4) Priority Project of the DPR in FY 10-11; (5) Priority Project of the DPR in FY 10-11.
9	The City shall work with the Sacramento Library Authority to review and update the Library Master Plan every 5 years.	x	x			The Sacramento Public Library Authority's Facility Master Plan was adopted in March 2007. An update on the Plan is provided to the Authority Board each November. A biennial review of social trends as they pertain to the Library is provided to the Authority Board every other April.
10	The City shall work with the Sacramento Library Authority to prepare an annual report on the Library Master Plan.		x			The Sacramento Library Authority partners with local jurisdictions on planning for library services.

IMPLEMENTATION AREA: EDUCATION, RECREATION & CULTURE

#	Implementation	Completed	In Progress On Schedule	In Progress Not on Schedule	Delayed/Deferred	2010 Update
12	The City shall conduct a study to identify alternative locations for the Sacramento Zoo to facilitate its expansion.		x			Feasibility study on relocation of zoo has been completed and will be presented to Council. More analysis may be required upon Council review.
13	The City shall review and update its Historic Cemetery Master Plan every 5 years.	x				Adopted by Council in 2009. The review and update will take place in five years.

IMPLEMENTATION AREA: PUBLIC HEALTH AND SAFETY

#	Implementation	Completed	In Progress On Schedule	In Progress Not on Schedule	Delayed/Deferred	2010 Update
1	The City shall review and update its Police Master Plan every 5 years.		x			The Master Plan is still in progress, but due to budgetary constraints, has not been finalized yet
2	The City shall submit an annual report to the City Council that evaluates implementation of its Police Master Plan.		x			Once we have the document finalized, the Chief will present it to the council for the first time.
4	The City shall develop and implement a public education program regarding crime prevention measures and support community programs, activities, and strategies aimed at preventing crime including youth gangs.		x			In 2010, the Sacramento Police Department and the Sacramento Police Activities League developed a summer Fitness Boot Camp. The Sacramento Police Activities League is a community-based program providing healthy activities for kids, building self-esteem and promoting teamwork with police officers. The 2010 Fitness Boot Camp was a six-week long program in which SAC PAL kids attended two days per week, three hours a day. The program focused on physical fitness as well as developing pillars of character which included self esteem, honesty, integrity, fairness and justice. The kids were put through drill and ceremony practice and a variety of sports and games. The camp instructors consisted of Sacramento police officers and physical fitness trainers who volunteered their time to this program.
5	The City shall review and update its Fire Department Master Plan every 5 years.			x		The Fire Department is in the process of developing a strategic plan. Target completion is 2012.
6	The City shall submit an annual report to the City Council that evaluates implementation of its Fire Department Master Plan.		x			This process will be conducted annually once the plan has been approved and adopted.
8	The City shall adopt official fire service level standards to provide adequate fire protection and emergency medical services.			x	x	The Fire Department is in the process of determining standards of cover. Target completion is 2012.
9	The City shall review the call volumes of fire companies annually to evaluate the need for additional response units.			x	x	The Fire Department is in the process of conducting a risk assessment and determining standards of cover. Target completion is 2012.
10	The City shall identify and designate transportation corridors for preemptive traffic signals.		x			Traffic Engineering staff is on a continual quest to enhance the operation of the existing circulation system including the installation of preemptive traffic signals as warranted.
11	The City shall prepare and adopt a fire protection development impact fee program to support the fire protection services needed for new development.				x	Resources are unavailable at this time.
12	The City shall develop and implement a community program for educating city residents in fire prevention and emergency preparedness.				x	The Fire Department community program to educate the public was eliminated as of July 2010 due to limited resources. The Department is researching alternatives.
14	The City shall develop and maintain a list of older buildings that may be considered fire hazards, and which were constructed prior to requirements for fire-resistant construction materials, internal sprinklers, and other fire safety systems.		x			See Implementation #16 (Public Health and Safety).

IMPLEMENTATION AREA: PUBLIC HEALTH AND SAFETY

#	Implementation						2010 Update
		Completed	In Progress On Schedule	In Progress Not on Schedule	Delayed/Deferred		
16	The City shall maintain and update a list of hazardous sites, buildings, and uses in the city.		x				The County has the authority and provides updates quarterly of the hazardous sites and materials. The uses are being maintained by the Fire Prevention Division
17	The City shall review and update its Multi Hazard Emergency Plan (Emergency Operations Plan) every 5 years.				x		Funding not secured. Project will be delayed to undetermined date.
18	The City shall conduct annual emergency response training for city staff.		x				Training is provided to Emergency Service and citywide staff on a continuous basis to ensure emergency response preparedness. Staff reductions have increased training demands to address shifting staffing and organizational change.
19	The City shall develop and implement a program for training city residents in emergency response and disaster preparedness.		x				Public education is provided on an ongoing basis. Approximately 4,000 residents have been trained so far in 2010.
20	The City shall work with Sacramento County to develop and maintain a database of health and human service facilities within the city and Sacramento County.	x					Staff uses data tracked by Community Services Planning Council
21	The City shall review and update its Code Enforcement Master Plan every 5 years.				x		Effective FY 10, Code merged with the Community Development Department. Staff will prepare a combined master plan.
22	The City shall submit an annual report to the City Council that evaluates implementation of its Code Enforcement Master Plan.				x		Effective FY 10, Code merged with the Community Development Department. Staff will prepare a combined master plan.
23	The City shall adopt code enforcement service level standards to provide adequate code compliance services to city residents.				x		Effective FY 10, Code merged with the Community Development Department. Staff will prepare a combined master plan.
24	The City shall work with stakeholders and property owners to develop programs to identify focus areas throughout the city that need a comprehensive approach to code compliance.				x		Due to limited resources, this item is pending.
25	The City shall develop and periodically update a comprehensive survey of older buildings and places of public assembly within the city, and recommend realistic measures to rehabilitate or remove those structures determined to be structurally unsafe.				x		Due to limited resources, this item is pending.
26	The City shall work with stakeholders and property owners to develop and adopt a Proactive Code Enforcement ordinance that requires systematic inspection of all residential rental properties in the city.	x					Completed

IMPLEMENTATION AREA: ENVIRONMENTAL RESOURCES

#	Implementation	Completed	In Progress On Schedule	In Progress Not on Schedule	Delayed/Deferred	2010 Update
2	The City shall review and amend, as necessary, the subdivision, grading, and other ordinances of the Sacramento City Code to ensure that biological resources are protected consistent with General Plan policies					TBD
5	The City shall work with the Sacramento Tree Foundation to prepare and continually update an inventory of trees within the city.	x				Inventory was completed February of 2010 and bridged to our work management system in April. Data for individual trees is refreshed as they are maintained. Staff is developing an inventory maintenance program for the general tree population by Community Planning Area with implementation scheduled for spring or summer of 2011.
6	The City shall prepare and adopt an ordinance to require tree replacements or suitable mitigation for loss of heritage trees.	x				Staff is reviewing the City's tree-related ordinances which will include mitigation for loss of trees. The study and recommended code changes should be completed by the end of 2011.
7	The City shall amend the Parks and Recreation Master Plan to promote community gardens in both new growth and infill development areas.	x				The Technical Update of the Parks and Recreation Master Plan 2005-2010 included new policies and guidelines to encourage community gardens. The City has added two new community gardens this year (at Zapata Park and Sparrow Park) and now operates eight community gardens. There are plans to add community gardens at Bill Bean Jr. Park and in the Oak Park community within the upcoming year.
8	The City shall establish land use restrictions such as agricultural conservation easements to protect the land for agricultural use in perpetuity.					TBD
9	The City shall establish a plan and process to improve monitoring and enforcement of all CEQA mitigation measures, including air quality measures.	x				Community Development staff initiated tracking of mitigation measures through its Accela permitting system. In addition, the City now requires all mitigation measures and conditions of approval to be placed on a separate sheet for all plans submitted for review prior to issuance of building permits.
10	The City shall establish a process to require the completion of an Air Quality Management Plan for proposed development projects that would result in substantial air quality impacts exceeding the Sacramento Metropolitan Air Quality Management District, ROG, and Nitrous Oxide operational threshold. The Plan shall outline how a project will incorporate design or operational features that reduce emissions by 15 percent from the level that would be produced by an unmitigated project.	x				The City requires operational air quality mitigation plans through the development review process as part of CEQA review and mitigation for projects that exceed operational thresholds established by SMAQMD.

IMPLEMENTATION AREA: ENVIRONMENTAL RESOURCES

#	Implementation	Completed	In Progress On Schedule	In Progress Not on Schedule	Delayed/Deferred	2010 Update
11	<p>The City shall work with the Sacramento County, SACOG, Sacramento Metropolitan Air Quality Management District, and the California Air Resources Board to develop and annually update the city's Greenhouse Gas Emissions Inventory.</p>				x	<p>The City, in coordination with the County of Sacramento and other incorporated jurisdictions in Sacramento County, completed a comprehensive 2005 baseline inventory for each jurisdiction's internal and communitywide greenhouse gas emissions. Additionally, the City continues to monitor and inventory its own greenhouse gas emissions for internal operations and has registered & certified them with the CA Climate Action registry through the year 2007. Ongoing efforts to conduct an annual update to the comprehensive 2005 baseline inventory for both internal and communitywide will be determined through the Climate Action Plan (see Environmental Resources implementation program 4-10-12 below).</p>
12	<p>The City shall develop and adopt a Climate Action Plan that: monitors climate change impacts; outlines a strategy for reducing greenhouse gas emissions and adapting to climate change; and describes how to improve air quality. The climate action plan will include the following:</p> <ul style="list-style-type: none"> ▪ A clear timeline for completion (2009–2011) ▪ An inventory of emissions ▪ Reduction targets consistent with the requirements set forth in AB 32 and with the direction set forth in the City's Sustainability Master Plan ▪ Specific reduction strategies that will help to achieve reduction targets ▪ Monitoring and reporting requirements to ensure that reduction targets are updated over time ▪ Adaptive management strategies that respond to the changing environment associated with climate change <p>The City shall work with the Sacramento Metropolitan Air Quality Management District (SMAQMD), the California Air Resources Board (CARB), and other appropriate agencies to ensure that the City's Climate Action Plan is regionally consistent and to pursue state funding to implement the plan. The Climate Action Plan, once adopted, shall be incorporated in the General Plan.</p>		x			<p>Phase 1 of the Climate Action Plan was presented to City Council in February 2010. The Phase 1 CAP identified strategies to reduce GHG emissions from the City's internal operations 22% below 2005 levels by 2020. The Phase 2 CAP is currently underway and will identify strategies to reduce GHG from all communitywide emission sources. The Phase 2 CAP is expected to be substantially completed by July 2011.</p>
13	<p>The City shall submit an annual report to the City Council on implementation of the Climate Action Plan. The report shall be made available to the public and responsible city officials.</p>				x	<p>As indicated above, the Climate Action Plan will not be completed until July 2011. Annual reporting on plan implementation will begin the year following CAP adoption (i.e. 2012).</p>

IMPLEMENTATION AREA: ENVIRONMENTAL RESOURCES

#	Implementation	Completed	In Progress On Schedule	In Progress Not on Schedule	Delayed/Deferred	2010 Update
14	The City shall continue to enforce its existing ordinance that limits idling of diesel vehicles used in construction projects.	x	x			Staff meets with the Sacramento Metropolitan Air Quality District on a quarterly and as-needed basis to discuss current projects, including mitigation requirements.
15	The City shall develop a public information program to encourage employees to use alternative modes of transportation (e.g., public transit, carpool, walking, biking) to get to work.				x	The TSM ordinance amendments will encourage the inclusion of measure intended to reduce employees dependence on single occupant driving. This combined with the educational program being developed for pedestrian safety and convenience as well as the Bicycle Master Plan and the Pedestrian Master Plan will function well to encourage increased use of alternative modes. Any further efforts should be lead by the Dept of General Services while DOT provides support.
16	The City shall develop a plan that identifies scenic resources and views to be protected, and establish standards for development location and design to protect these resources.		x			Design guidelines for the River District and the Central Core Urban Design Guidelines will include viewshed protection components along rivers.
18	The City shall develop and adopt an ordinance to require light for development to be directed downward to minimize spill-over onto adjacent properties and reduce vertical glare.		x			Included as part of the rezone bundle that was approved by Council on October 12, 2010.

IMPLEMENTATION AREA: ENVIRONMENTAL CONSTRAINTS

#	Implementation					2010 Update	
		Completed	In Progress	On Schedule	In Progress Not on Schedule		Delayed/Deferred
1	The City shall review and update its seismic and geologic safety standards when there are updates to the Uniform Building Code and California Building Code to ensure consistency with these codes and best management practices.					x	Resources not available.
2	The City shall conduct a study to identify and prepare a database of all existing city-owned critical facilities (e.g. police stations and fire stations) that do not meet current building code standards and are within areas susceptible to seismic or geologic hazards. The City shall identify a strategy to upgrade, retrofit, and/or relocate those city facilities.						TBD
4	The City shall update the General Plan for consistency with AB 162 (chaptered December 2007) (Government Code 65302) related to flooding.						TBD
7	The City shall annually review and update, as necessary, the local Comprehensive Flood Management Plan.				x		The CFMP should go to Council in the Nov-Dec 2010 time frame. Once updated CFMP has been approved, staff will provide annual review.
8	The City shall update the Comprehensive Flood Management Plan to include current evacuation plans for a Folsom, Nimbus, or Oroville dam failure.				x		Evacuation plan for dam failures have not been developed. The goal is to have them developed by mid 2011.

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SECTION IV
2030 General Plan Annual Report
DEVELOPMENT ACTIVITY

The development analysis of the General Plan Annual Report is intended to provide an overview of all new development in the City. The analysis will show whether the City is adequately accommodating growth and how the City may encourage more infill development. Additionally, an analysis of the current economic climate is given to provide a perspective on our progress in meeting development projections and goals. Future annual reports will indicate whether or not desired development patterns are taking place.

General Plan Growth Projections

The 2030 General Plan projected that 100,000 housing units and 140,000 jobs would be needed to accommodate new city residents by 2030. These numbers were obtained from the Sacramento Area Council of Government (SACOG) and are consistent with the projections SACOG’s Regional Blueprint.

Progress Towards Meeting our Growth Projections

Housing

Table 1 indicates housing growth citywide from 2005-2009. During that five-year period the City met 10% of the General Plan’s housing growth projections. Currently the City is on track to meet our projected housing needs. However, a weakened housing market and building restrictions in North Natomas have slowed the City’s progress in the short term.

Table 1

Housing Units Developed from 2005-2009		
	Single Family	Multi-Family
Citywide Housing Development	5685	4348
Total Housing Units Produced	10,090	

Employment

Growth in the retail, office, and industrial sectors from 2005-2009 is indicated below in Table 2. In that five-year period the City added an estimated 25,209 jobs or 18% of the 140,000 jobs needed by 2030. More than half of the estimated employment created was office related work.

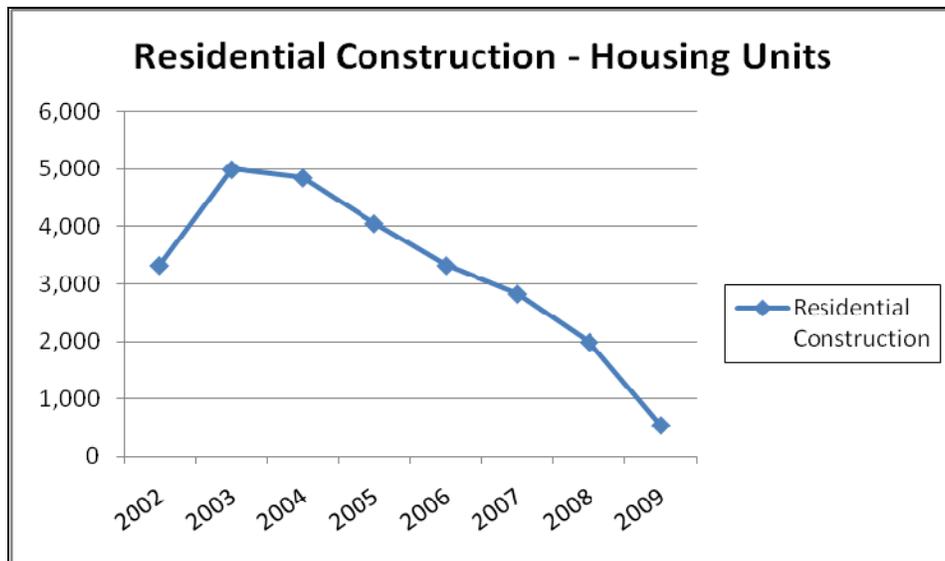
Table 2

Employment Generated from 2005-2009		
	Total Sq. Ft. of New Development	Estimated Jobs Created*
Industrial	1,442,775	1,443
Office	3,461,989	13,848
Retail/Commercial	4,463,439	9,919
Total Jobs Created	25,209	
*Estimations of jobs created by sector is based on factor of square feet per employee. These factors were used in the 2030 General Plan Infrastructure Financing Strategy.		

Current Economic Climate

Local development in 2009 continued to reflect the nationwide economic recession with even lower amounts of development than previous years. From 2004-2008, residential development in the City steadily decreased at an annual rate of 16%. 2009 saw housing production decline by 63% from the previous year. Chart 3 below illustrates this drastic decline.

Chart 3

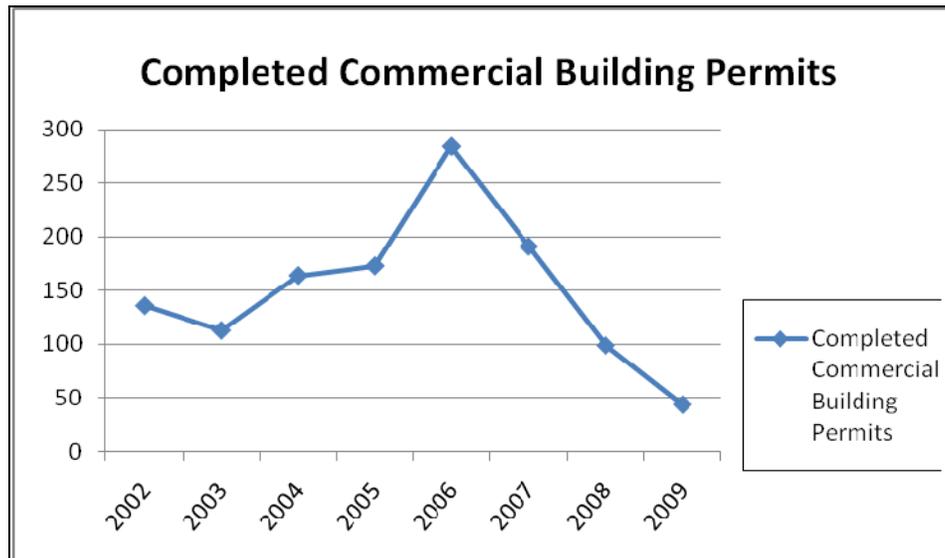


For the last three years, the decline in housing production has been primarily due to:

- A weak housing market;
- High foreclosure rates;
- Lack of financing; and
- Building restrictions in Natomas due to inadequate flood protection.

Commercial development initially remained strong while the residential sector was declining. However, this trend ended in 2006. In 2007 and 2008, completed commercial building permits declined at a rate of 50%. Chart 4 shows this increase in commercial development and then the sharp decline.

Chart 4



The decline in commercial development in Sacramento is largely due to:

- A retail sector impacted by job losses and a decline in consumer confidence;
- An office market that is oversaturated while companies are downsizing; and
- A industrial sector facing reduced demand from the contracting construction and other commercial sectors

Meeting our Infill Development Goals

2009 Building Activity in Infill Areas

Building permits issued for housing in infill areas dropped dramatically from the high-water mark of 2006. In 2009, only 77 building permits were issued for single family units and 46 building permits were issued for multiple family infill units

Similarly, non-residential building activity in infill areas was minimal in 2009. New commercial activity was predominantly small retail and service stations. However the Sutter medical complex continued to take shape adjacent to the Sutter General Hospital which is located east of downtown.

2009 Planning, Demolition & Infrastructure Activity

While cranes were disappearing from the skyline, site preparation and planning workshops were in full swing. Following are some highlights from these activities:

River District

- River District Specific Plan: The 2009 planning efforts came to fruition as the draft Specific Plan and EIR were released in August 2010. The Specific Plan envisions a cohesive district with a mix of residential, commercial, industrial, public, and open space uses. Elements of the Specific Plan include future street layout, bicycle and pedestrian circulation, opportunities for parks and open space, as well as proposed land uses and zoning. Several construction projects are underway or will begin shortly in the River District.
- The new California Lottery headquarters building is under construction. This 155,000 square foot building, located on north 10th Street, will replace the Lottery's current building.
- Regional Transit's light rail line to the Township 9 project. This is the first stop on the new Green Line which will connect to the Amtrak station and eventually reach the Sacramento International Airport.
- The Greyhound Terminal (10,000 square foot facility) will be located at 420 Richards Boulevard, broke ground on August 18, 2010.
- The Interim Improvement project for the Interstate 5 interchange at Richards Boulevard will start construction in the fall of 2010. This project will improve the on- and off-ramps at this freeway exit and connect Bercut Street to the Railyards. The project is funded by approximately \$2 million of local funds and \$9 million of federal funds.
- Township 9 (Demolition): Township 9 development commenced with the early stages of demolition of former cannery buildings. In total, demolition began on 1.4 million square feet of warehouse facilities.

Downtown Railyards

- Infrastructure: The EIR and entitlements were approved for the 240-acre site in December 2007. Infrastructure funding commitments have been secured from the following sources: Federal ARRA Stimulus Grants, HCD 1C Bond Funding under TOD and Infill programs, Measure A sales tax, SHRA, Caltrans, California Transportation Commission, Regional Transit District, Federal Transit Authority, Federal Railroad Administration, and Federal Highways Administration.



Curtis Park Village

- Planning & Brownfield Remediation: Curtis Park Village is a 72-acre mixed-use infill development on a brownfield site formerly used as a Railyard. The proposal calls for 129 single-family homes, 45 brownstone residences, 248 multifamily homes and 259,000 square feet of commercial, retail and office space. The property was purchased from the Union Pacific Railroad in 2003 and since then millions of dollars have been spent cleaning up the site. In Spring 2010, the City Council held hearings on the project and certified the EIR; project entitlements were approved on September 28, 2010. Soil remediation is currently underway.



Florin Road

- Auto Dealer Redevelopment Efforts: In conjunction with Economic Development staff, CDD staff worked with the former auto dealers to prepare the 43-acre area for redevelopment. City staff conducted a number of critical studies and actions necessary to spur the redevelopment of the area including: water and sewer infrastructure studies; a circulation, access, and traffic analysis; a Phase 1 environmental site assessment; a market study and strategy report; rezones to address zoning inconsistencies; and identified economic incentives for the area. In addition, the City secured over \$100,000 in an EPA Brownfield grant to conduct a phase 2 environmental analysis of the site to determine which areas were clean and ready for development. The Phase 2 work is expected



to be completed in mid-2011. The focus will then shift to infrastructure financing and marketing of the area.

Northeast Line Implementation Plan

- **Redevelopment of the Corridor:** The Northeast Line Implementation Plan is a planning effort to promote reinvestment, redevelopment, and revitalization along the light rail corridor that includes the Globe, Arden/Del Paso and Royal Oaks Stations. The 2030 General Plan, adopted in 2009, amended land use designations in key opportunity areas, including light rail station areas and commercial corridors, to facilitate the revitalization of corridors and centers. Previous planning efforts, including the Northeast Line Light Rail Stations Plan (2007) and Transit for Livable Communities (2002), informed the identification of the 2030 General Plan land use designations for this area. The Northeast Line Implementation Plan is an effort to implement these previous planning efforts. The Plan includes specific strategies to address housing, economic development, the strategic financing of infrastructure, public safety, and design needs along the light rail corridor.

Downtown

- **Infrastructure Planning:** An infrastructure study was commissioned to identify existing resources and deficiencies to inform future development and eliminate surprises in terms of infrastructure, historical resources and toxic contamination. This planning effort will identify priority infrastructure required to catalyze early phases of development, and will also serve to guide future infrastructure financing strategies.

Infill Development Tools

Infill House Plans:

The City of Sacramento Infill House Plan Program was developed to streamline the process for development of high quality single family homes in the City's older neighborhoods and redevelopment areas. The homes were designed to suit the character and style of the City's older neighborhoods. Under this program, users can purchase plans that are pre-approved through the City's design review and building plan check review process, saving time and money.

In 2010, the City obtained funding from SMUD and CDBG funding from SHRA and retained the original architects to update the infill house plans to comply with new building code requirements. In addition, the City also launched new energy



efficient pre-approved home models that comply with SMUD's Home of the Future Program and Solar Smart Program standards. These energy efficient homes are up to 80% more efficient than the standard single-family home. The City also initiated new accessible pre-approved models that comply with the City's newly adopted Universal Design Ordinance. These homes are designed to address these needs of persons with disabilities. Pre-approved plans will be available for purchase from the architects in late 2010.

Economic Development Treatment Capacity Bank

The Sewer Credit program supports the development of small businesses, infill/transit oriented development, and low income housing projects by reducing applicants' sewer connection fees by 67-87%. An important role of the Sewer Credit Program is the promotion of economic development through the creation of new jobs, infill development and affordable housing projects. Currently, the Economic Development Treatment Capacity Bank has a total of 4,071 credits remaining. In 2009, the City approved approximately 193 credits in the Infill/TOD category for six infill projects.

Proposition 1C:

After helping secure over \$80 million in Proposition 1C grants in 2008 for Railyards, Township 9, Broadway Lofts, La Valentina, and Curtis Park Village, staff working with the City Manager's Office and developers in 2009, staff helped secure an additional \$55 million from the State's Proposition 1C grant programs for infrastructure and site clean up for several infill projects including the Railyards, Township 9, Curtis Park Village and Capitol Lofts.



California Sustainable Strategies Pilot Program:

Working with the City Manager's Office and developers, the City received a designation of California Catalyst Project for Township 9 under the State's new pilot program. This infill project was one of only 13 projects statewide to receive this designation and one of only five to receive the gold level ranking – the highest ranking available. The designation provides Township 9 with \$1.35 million from HCD's Innovative Homeownership Program and makes them eligible for other future State and federal assistance.

Strategy and Expectations for Infill Development 2010

While overall development activity continues to be experiencing recession, the City continues to position infill sites as "shovel ready":

- The infill house plans are expected to be approved;
- The Township Nine project will receive \$1,350,000 under the State Department and Community Development's Innovative Homeownership Program;
- The Florin Road Corridor Plan will be adopted;
- The Northeast Line Implementation Plan will be adopted in 2011; and

- The Innovation & Technology Village Specific Plan (a plan to establish a business park for research and innovation south of Sacramento State University) is underway.

Conclusion

While development is at its lowest point in recent memory, the ongoing implementation of the 2030 General Plan is providing a tremendous amount of capacity for responding to the next upturn in the economy. The planning efforts taking place now will help Sacramento not only to take on more growth, but have a growth pattern that will be more compact, include the “infill” and reuse of underutilized properties, intensify development near transit and mixed-use activity centers, and locate jobs closer to housing. This new way of growing will help Sacramento become the most livable city in America.

SECTION V
2030 General Plan Annual Report
CUMULATIVE AMENDMENTS

As conditions and needs change, the City will need to consider proposed amendments to the General Plan. Some of these will be policy changes, while many will likely be changes to the Land Use and Urban Form Diagram. Each of the changes will need to be carefully evaluated not only for merit and potential impact, but also for consistency with the rest of the General Plan.

The purpose of this section of the Annual Report is to review the amendments of the prior year and to analyze their cumulative effects to ensure the integrity and intent of the General Plan is preserved.

Five projects in 2010 resulted in amendments to the 2030 General Plan:

- 1) The 65th Street Transit Village
- 2) The Curtis Park Project
- 3) Florin Road Corridor Plan
- 4) General Plan Consistency Project – Amendments to the Land Used and Urban Form Diagram
- 5) Technical changes to General Plan Text

I. Curtis Park

Minor changes were made to the General Plan's Land Use and Urban Form Diagram as part of the 73-acre Curtis Park infill project. The land use designation of approximately 8-acres was changed from Traditional Center to Traditional Neighborhood Medium.

II. 65th Street Transit Village

The approval of the 65th Street Transit Village, a transit oriented development planning document, modified the circulation plan for the 65th Street Station Area in the General Plan's Mobility Element and East Sacramento and Fruitridge/Broadway Community Plan.

III. Florin Road Corridor Plan

The Florin Road Corridor Plan encompasses 43-acre that includes several former auto dealerships. The adoption of the Florin Road Corridor Plan was accompanied by amendments to the South Area Community Plan to add policies and figures that

address issues and opportunities identified in the Florin Road Corridor Plan. Additionally, the land use designation for one parcel (approximately 5.6 acres) was changed from Suburban Low Density Residential to Traditional Neighborhood Low to allow for a transitional buffer from commercial to residential uses.

IV. Land Use Designation Changes for General Plan Consistency

Pursuant to the 2030 General Plan adopted by City Council in March 2009, staff immediately initiated a project to bring zoning into consistency with the 2030 General Plan's Land Use and Urban Form Diagram. This project entailed a combination of zoning and changes in land use designations.

Council approved changes to the land use designation changes of 552 parcels in October 2009, and 553 parcels in November 2010.

V. Technical Changes to the text of the 2030 General Plan

In November, the Council approved text amendment in the 2030 General Plan that addressed the following: 1) the addition of a policy to address minimum floor-area-ratio; 2) the addition of a policy inadvertently omitted from the North Natomas Community Plan relating to the Financing Plan; 3) deletion of a duplicate policy in the North Natomas Community Plan relating to financing of drainage facilities; 4) modification of an existing policy for non-conforming density in the Traditional Neighborhood designation; and 5) the addition of a footnote to a table to clarify noise standards for balconies or patios.

Analysis of Cumulative Amendments

The above listed amendments to the 2030 General Plan – both to the land use diagram and text – are consistent with the Plan's goals to encourage transit-oriented development, revitalize commercial corridors, promote economic diversity, and provide a wide range of housing products. The following policies are supported by the amendments:

LU 6.1.2 Transformed Corridors. The City shall facilitate the transformation of major thoroughfares dominated by auto-oriented strip commercial uses to include a broader mix of uses, both horizontal and vertical, that provides opportunities for medium- and higher-density housing, while also addressing local and citywide demand for retail and services.

LU 6.1.3 Redeveloping Automobile-Oriented Corridors. The City shall promote redevelopment of existing automobile-oriented corridors and the upgrading of existing

commercial development to create vibrant, mixed-use boulevards that balance efficient movement of motor vehicles with the creation of attractive pedestrian-friendly districts that serve the adjoining neighborhoods as well as passing motorists.

M 1.2.1 Multimodal Choices. The City shall promote development of an integrated, multi-modal transportation system that offers attractive choices among modes including pedestrianways, public transportation, roadways, bikeways, rail, waterways, and aviation and reduces air pollution and greenhouse gas emissions.

M 1.3.5 Connections to Transit Stations. The City shall provide connections to transit stations by identifying roadway, bikeway, and pedestrianway improvements to be constructed within ½ mile of major transit stations. Transportation improvements in the vicinity of major transit stations shall emphasize the development of complete streets.

M 3.1.3 Variety of Transit Types. The City shall consider a variety of transit types including high speed rail, inter-city rail, regional rail, light rail transit, bus rapid transit, trolleys (streetcars), enhanced buses, express buses, local buses, neighborhood shuttles, pedicabs, and jitneys to meet the needs of residents, workers, and visitors.

H 1.2.2 The City shall encourage a greater variety of housing types and sizes to diversify, yet maintain compatibility with, single family neighborhoods.

ED 3.1.7 Infrastructure and Public Facilities. The City shall continue to identify, construct, and maintain infrastructure systems and facilities required to promote and sustain a positive economic climate.

ED 3.1.8 Infrastructure Investments. The City shall anticipate needs and coordinate City infrastructure investments with economic development opportunities.

ED 4.1.3 Public/Private Partnerships. The City shall support and encourage public/private partnerships and other efforts to implement the key development projects that meet the City's revitalization and redevelopment goals.

LU 1.1.5 Infill Development. The City shall promote and provide incentives (e.g., focused infill planning, zoning/rezoning, revised regulations, provision of infrastructure) for infill development, redevelopment, mining reuse, and growth in existing urbanized areas to enhance community character, optimize City investments in infrastructure and community facilities, support increased transit use, promote pedestrian- and bicycle-friendly neighborhoods, increase housing diversity, ensure integrity of historic districts, and enhance retail viability.

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Election of the Chair and Vice-Chair of the Planning Commission for 2011

For City of Sacramento Planning Commission Agenda Packet

For the Meeting of: December 9, 2010

Title: Election of the Chair and Vice-Chair of the Planning
Commission for 2011

Location: Citywide

Recommendation: Nomination and selection of Chair and Vice-Chair
for 2011

Contact Information: Tom Pace, Long Range Planning Manager,
916-808- 6848

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