



# REPORT TO PLANNING COMMISSION City of Sacramento

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915 I Street, Sacramento, CA 95814-2671

PUBLIC HEARING  
January 13, 2011

To: Members of the Planning Commission

**Subject: 8151 Sheldon Commercial Rezone (P10-060)**

A request to amend the General Plan and Zoning Designation from residential to commercial for approximately 3.8 acres for properties located at 8151 Sheldon Road. This request requires:

- A. Environmental Determination:** Mitigated Negative Declaration Addendum
- B. Mitigation Monitoring Program**
- C. General Plan Amendment** to amend the General Plan Land Use Designation for approximately 3.8 acres from Suburban Neighborhood High Density to Suburban Center.
- D. Rezone** of approximately 2.7 acres from Multi-Family Residential (R-2B) to General Commercial Review (C-2-R).

**Location/Council District:**

8151 Sheldon Road  
Assessor's Parcel Number: 117-0220-002, 022, 023, 024  
Council District 8

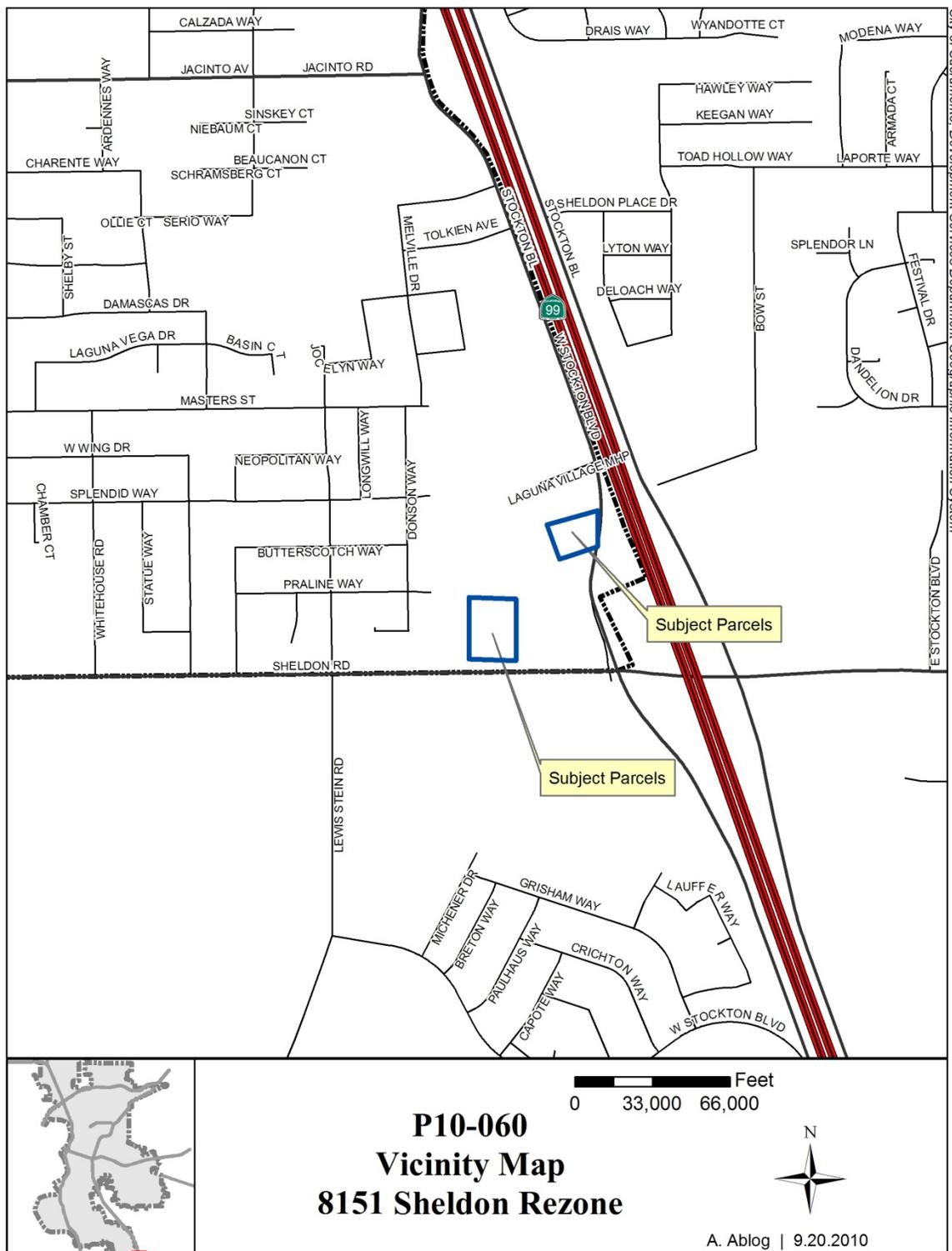
**Recommendation:** Staff recommends the Planning Commission recommend approval and forward the General Plan Amendment and Rezone request to the City Council based on the findings listed in Attachments 2. The City Council has the final approval authority over items B and C. **Staff is not aware of any opposition and the project is non-controversial.**

**Contact:** Antonio Ablog, Associate Planner, 808-7702, Lindsey Alagozian, Senior Planner, 808-2659

**Applicant:** Dr. Jeffrey Moore, Southport, Inc. 31941 Corydon Street, Ste. 1 Lake Elsinore, CA 92530

**Owner:** Joe Rossettie, SLC Sheldon LLC, 12651 High Bluff Drive #250, San Diego, CA 92130

Vicinity Map



**Summary:** The applicant is proposing to amend the General Plan Designation of four parcels from Suburban Neighborhood High Density to Suburban Center, and Rezone three parcels from Multi-Family Residential (R-2B) to General Commercial Review (C-2-R). No development plans are proposed for approval at this time. The “R” review designation is being incorporated into the zoning of the property to ensure that future development will relate to characteristics of the site and surrounding area which will require a Plan Review for any future development.

<b>Table 1: Project Information</b>
<b>General Plan Designation:</b> Suburban Neighborhood High Density.
<b>Existing zoning of site:</b> Multi-Family (R-2B) and General Commercial (C-2-R)
<b>Existing use of site:</b> 2 vacant parcels, 1 residential structure, 1 commercial structure
<b>Property area:</b> Approximately 3.8 acres (after lot line adjustment).

**Background Information:** The applicant proposes to amend the General Plan designation of four parcels and Rezone three of those parcels for future commercial development. Three of the parcels are located on the north side of Sheldon Road (APN: 117-0220- 022, 023, and 024). One parcel is developed with a single-family home, while the others are vacant. These parcels are zoned Multi Family (R-2B) for medium density housing. One of these parcels (APN: 117-002-022) is currently in process for a Lot Line Adjustment to adjust the lot line to coincide with the Rezone and Plan Amendment proposed with this application. This lot line adjustment will be complete prior to any final action on this project. The total area of the Rezone is approximately 2.7 acres.

The fourth parcel is located on the west side of West Stockton Boulevard north of the Sheldon/Highway 99 interchange and is zoned General Commercial Review (C-2-R) (APN: 117-0220-002). This parcel is a part of the aforementioned lot line adjustment and will be reduced in size approximately 1.1 acre. This parcel is already zoned General Commercial (C-2-R), but does require a General Plan Amendment to amend the General Plan Designation from Suburban Neighborhood High Density to Suburban Center.

The four parcels were part of a Tentative Map that was approved in April of 2007. As part of this project, the three subject parcels on the north side of Sheldon Road were Rezoned from Rural Estates (RE-1/4) to Multi-Family Residential (R-2B) (APN: 117-0220- 022, 023, and 024). At that time the applicant did not propose new development on the parcels as he intended to submit for a commercial rezone of the parcels after the approved subdivision began construction. Three parcels under different ownership at the northwest corner of the Sheldon Road and West Stockton Boulevard were an obstacle in pursuing the rezone with the prior application. These parcels, totaling 4.89 acres, were zoned Rural Estates and were key in assembling a commercial site viable for development. The applicant did not see the benefit in rezoning to a commercial designation at the time if the corner piece were to remain residential. In 2009, the General Plan Designation for the corner parcels (apn: 117-0220-019, 020, and 021) adjacent to the subject site was amended from Suburban Neighborhood High Density to Suburban Center and rezoned to General Commercial Review (C-2-R) (see Attachment 1-Land Use Map).

While the corner parcels are still under different ownership, the new owner of the subject parcels wishes to amend the zoning and General Plan Designations of the parcels to create a viable commercial area at the northwest corner of Sheldon Road and West Stockton Boulevard.

**Public/Neighborhood Outreach and Comments:** Early project notifications have been sent to all property owners within five hundred feet of the project site, as well as to the North Laguna Creek Neighborhood Association, and the City of Elk Grove. City staff has not received any comments related to this rezone.

**Environmental Considerations:** The Environmental Services Manager has reviewed the project for compliance with the requirements of the California Environmental Quality Act (CEQA). The project falls within the scope of the Mitigated Negative Declaration for 8151 Sheldon (P05-044) which was approved by the City Council on February 8, 2007. The project changes land use designations for several parcels from residential to commercial uses, but these changes would not result in any additional impacts or mitigation measures not considered in the previously approved Mitigated Negative Declaration.

An Addendum to the previously-adopted Mitigated Negative Declaration has been prepared describing the proposed land use and zoning redesignations and evaluating the potential environmental effects of the proposed project. Based on other development in the project area, it is reasonable to assume that the parcels could support two commercial buildings, one approximately 23,533 square feet in size, and the other approximately 9,583 in size. These estimates have been used to evaluate the potential traffic and other effects that could be generated at the site above and beyond the estimates previously used for the residential land use designations. No new traffic, noise or air quality effects would be anticipated based on the development scenario used for the analysis of impacts.

The Addendum confirms the project description and justification for use of an Addendum pursuant to the California Environmental Quality Act (CEQA) Guidelines (Section 15164).

### **Policy Considerations**

**2030 General Plan:** The subject parcels are designated Suburban Neighborhood High Density on the 2030 General Plan Land Use Map. This designation is reserved for multi-family housing at densities from 15 to 30 dwelling units per net acre. With this project, the applicant proposes to change this designation to Suburban Center to match the changes that were made to the adjacent parcels. The Suburban Center designation is reserved for lower-intensity single-use commercial development or horizontal and vertical Mixed-use development including retail, office, or residential uses. Buildings are encouraged to be located adjacent to the street with heights from 1 to 4 stories. The urban form characteristics envisioned for suburban centers include:

- Commercial development at a Floor Area Ratio (FAR) between 0.25 and 2.00.
- Compact development with buildings sited adjacent to the street
- Building facades and entrances with a high degree of transparency
- Attractive pedestrian streetscapes
- Convenient and attractive pedestrian connections from adjoining neighborhoods.

Staff supports amending the General Plan Designation for these parcels as they are located adjacent to a major highway interchange. Though the General Plan supports higher densities, the subject site would most likely be developed with single-story commercial buildings that would provide space for neighborhood serving commercial uses. The site provides a good location for such commercial development where residential uses could be negatively affected by the noise and traffic from the freeway and from Sheldon Road. The proposed amendments would enlarge the adjacent 4.89 acres that were redesignated in 2009 and make for a larger, more commercially viable site.

**Rezone:** Three of the subject parcels, totaling approximately 2.7 acres, are proposed to be rezoned from Medium Density Residential (R-2B) to General Commercial Review (C-2-R) (APN: 117-0220- 022, 023, and 024). The fourth parcel included with this application is already zoned C-2-R and would not need to be rezoned. The C-2-R zone is reserved for sale of commodities, or performance of services, including repair facilities, offices, small wholesale stores or distributors, and limited processing and packaging and is consistent with the requested Suburban Center General Plan designation.

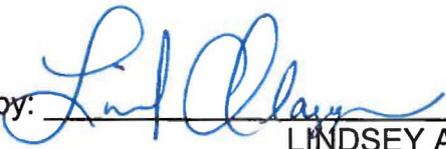
The C-2 zone is typical for small to medium commercial centers and the “R” review designation will require that any future development will require a formal Plan Review. The “R” designation is typical when a site rezoned without an associated development plan.

Similar to the General Plan Amendment, staff supports the Rezone of these parcels as they are located adjacent to a major highway interchange, and they provide a viable location for commercial development where residential uses could be negatively affected by the noise and traffic from the freeway and from Sheldon Road. Further, the “R” review will ensure future Plan Review of any development plans.

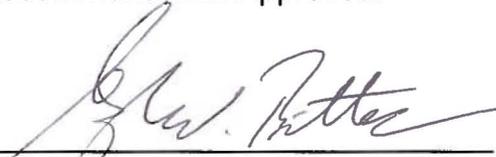
## Summary

Amending the General Plan and rezoning the subject parcels would create a viable commercial district where residential uses would not be desirable. The subject parcels could be developed in conjunction with neighboring commercial sites to form a commercial center that could provide neighborhood serving uses as well as commercial services that take advantage of the site’s proximity to a major freeway interchange. Though no development plans are proposed at this time, the applicant proposes to place an “R” review designation on the site that will require further review of the site once more specific plans are drafted.

Respectfully submitted by:   
ANTONIO ABLOG  
Associate Planner

Approved by:   
LINDSEY ALAGOZIAN  
Senior Planner

Recommendation Approved:

  
GREGORY BITTER, AICP  
Principal Planner

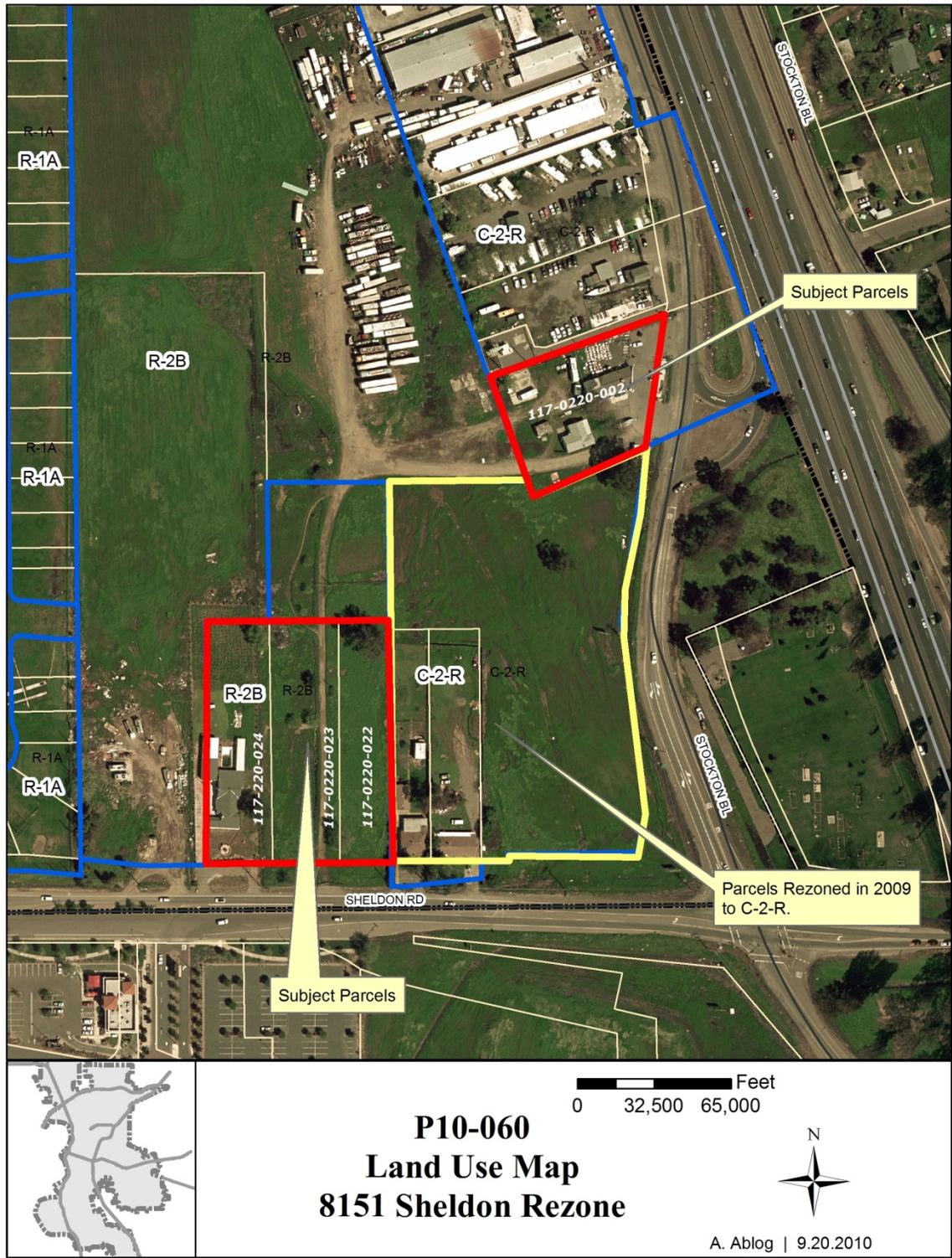
Attachments:

- Attachment 1 Land Use Map
- Attachment 2 Proposed Findings of Fact
- Attachment 3 Resolution - CEQA
- Attachment 4 Resolution – General Plan Amendment
- Attachment 5 Ordinance – Rezone

8151 Sheldon Commercial Rezone (P10-060)

January 13, 2011

Attachment 1 – Land Use Map



City of Sacramento, Development Services Department, Geographic Information System

Attachment 2 – Proposed Findings of Fact

**Findings of Fact**

- A. The Planning Commission has reviewed and considered the information contained in the Mitigated Negative Declaration and Addendum for the Project in making the recommendations set forth below.
  
- B. The Planning Commission recommends approval and forwards to the City Council the **General Plan Amendment** for the Project as set forth in Attachment 3.
  
- C. The Planning Commission recommends approval and forwards to the City Council the **Rezone** for the Project based on the findings as set forth in Attachment 4.

**RESOLUTION NO.**

Adopted by the Sacramento City Council

**ADOPTING THE MITIGATED NEGATIVE DECLARATION ADDENDUM AND THE MITIGATION MONITORING PLAN FOR THE 8151 SHELDON COMMERCIAL REZONE PROJECT (P10-060)**

**BACKGROUND**

A. On January 13, 2011 the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve with conditions the 8151 Sheldon Commercial Rezone Project.

B. On \_\_\_\_\_, 2011 the City Council conducted a public hearing, for which notice was given pursuant Sacramento City Code Section 17.200.010(C), and received and considered evidence concerning the 8151 Sheldon Commercial Rezone.

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:**

Section 1. The City Council finds as follows:

A. On February 7, 2007, pursuant to the California Environmental Quality Act (Public Resources Code §21000 *et seq.* ("CEQA"), the CEQA Guidelines (14 California Code of Regulations §15000 *et seq.*), and the City of Sacramento environmental guidelines, the City Council adopted a mitigated negative declaration (MND) and a mitigation monitoring program and approved 8151 Sheldon Subdivision (P05-044) (Project).

B. The 8151 Sheldon Commercial Rezone Project (P10-060) (Project Modification) proposes to modify the previously approved Project as follows: A request to amend the General Plan Designation from Suburban High Density Residential to Suburban Center and zoning re-designation from RE-1/4 to C-2R for approximately 3.8 acres for properties located at 8151 Sheldon Road. This request requires a General Plan Amendment and a Rezone. There are no building specific proposals at this time.

C. The initial study on the Project Modification determined that the proposed changes to the original Project did not require the preparation of a subsequent environmental impact report or negative declaration. An addendum to the previously adopted MND was then prepared to address the modification to the Project.

Section 2. The City Council has reviewed and considered the information contained in the previously adopted MND for the Project, the addendum, and all oral and

documentary evidence received during the hearing on the Project Modification. The City Council had determined that the previously adopted MND and the addendum constitute an adequate, accurate, objective, and complete review of the proposed Project Modification and finds that no additional environmental review is required based on the reasons set forth below:

A. No substantial changes are proposed by the Project Modification that will require major revisions of the previously adopted MND due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;

B. No substantial changes have occurred with respect to the circumstances under which the Project Modification will be undertaken which will require major revisions to the previously adopted MND due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;

C. No new information of substantial importance has been found that shows any of the following:

1. The Project Modification will have one or more significant effects not discussed in the previously adopted MND;

2. Significant effects previously examined will be substantially more severe than shown in the previously adopted MND;

3. Mitigation measures previously found to be infeasible would in fact be feasible and would substantially reduce one or more significant effects of the Project Modification; or

4. Mitigation measures which are considerably different from those analyzed in the previously adopted MND would substantially reduce one or more significant effects on the environment.

Section 3. Based on its review of the previously adopted MND for the Project, the addendum, and all oral and documentary evidence received during the hearing on the Project Modification, the City Council finds that the MND and addendum reflect the City Council's independent judgment and analysis and adopts the MND and the addendum for the Project Modification and readopts the findings of fact in support of the MND.

Section 4. The mitigation monitoring program for the Project is adopted for the Project Modification, and the mitigation measures shall be implemented and monitored as set forth in the program, based on the following findings of fact:

1. The mitigation monitoring program has been adopted and implemented as part of the Project;

2. The addendum to the MND does not include any new mitigation measures, and has not eliminated or modified any of the mitigation measures included in the mitigation monitoring program;

3. The mitigation monitoring plan meets the requirements of CEQA section 21081.6 and CEQA Guidelines section 15074.

Section 5. Upon approval of the Project, the City's Environmental Planning Services shall file or cause to be filed a Notice of Determination with the Sacramento County Clerk and, if the project requires a discretionary approval from any state agency, with the State Office of Planning and Research, pursuant to section 21152(a) of the Public Resources Code and the State EIR Guidelines adopted pursuant thereto.

Section 6. Pursuant to Guidelines section 15091(e), the documents and other materials that constitute the record of proceedings upon which the City Council has based its decision are located in and may be obtained from, the Office of the City Clerk at 915 I Street, Sacramento, California. The City Clerk is the custodian of records for all matters before the City Council.

Section 7. Exhibit A Mitigation Monitoring Program is a part of this Resolution.

Table of Contents:

Exhibit A: Mitigation Monitoring Program

## Exhibit A – Mitigation Monitoring Plan

**8151 SHELDON SUBDIVISION (P05-044)  
MITIGATION MONITORING PLAN**

This Mitigation Monitoring Plan (MMP) has been required by and prepared for the City of Sacramento Development Services Department, Environmental Planning Services, 2101 Arena Boulevard, Second Floor, Sacramento, CA 95834, pursuant to CEQA Guidelines Section 21081.6.

**SECTION 1: PROJECT IDENTIFICATION**

**Project Name/File Number:** 8151 Sheldon Subdivision (P05-044)

**Owner/Developer:** Robert Nelson  
Nouveau Homes  
6620 Sierra College Boulevard, Suite 200  
Rocklin, CA 95677

**Project Location:**

The proposed project is located north of Sheldon Road, west of State Highway 99, east of Bruceville Road, and north of the City/County line, within the Jacinto Creek Planning Area (APNs #117-0220-002,022,023,024 & 025).

**Project Description:**

The project consists of entitlements to develop a 18.7± acre site in the Rural Estates-Four Residential Units per Acre (RE ¼) zone of the Jacinto Creek Planning Area (JCPA) within the South Sacramento Community Plan area. Specific entitlements include:

**Environmental Determination:** Mitigated Negative Declaration;

**Mitigation Monitoring Plan;**

**General Plan Amendment** to amend the General plan designation from 15.6± acres of low density residential to 14.4± acres of medium density residential, and 1.2± acres of Open Space;

**Community Plan Amendment** to amend the community plan designation of 15.6± acres from Residential 11-21 to 14.1± acres of Residential 11-21 and 1.2± acres of Parks/Open Space;

**Rezone** of 15.6± acres from Rural Estates (RE - 1/4) to 14.1± acres of Multi-Family Residential (R-2B, and 1.2± acres of Agroculture/Open Space (AOS);

**Tentative Map** to subdivide 18.7± acres into 175 lots for residential development;

**Special Permit** to develop 167 single-family homes in the R-2B zone;

**Inclusionary housing plan;**

**Subdivision Modification** to construct a non-standard intersection at Splendid Way and B Street;

**Subdivision Modification** to construct a non-Standard intersection at A Street and Praline Way;

**Subdivision Modification** to construct a 41-foot wide Public Street at A Court;

**Subdivision Modification** to construct a 40-foot outside radius Cul-De-Sac at A Court; and,

**Subdivision Modification** to create lots with less than 20 feet of public street frontage (Lots 53, 78, 79, and 92).

Lots A, B, and C, are properties to be subdivided by the applicant. However, they are proposed to be "remainder" parcels in that they will not be rezoned for development. The Lots are zoned Rural Estates, which allows for only residential development at 4 units per acre, and will remain that zone until rezoned at a later time with a different application.

**SECTION 2: GENERAL INFORMATION**

The Mitigation Monitoring Plan (MMP) includes mitigation for Biological Resources, Noise, and Cultural Resources. The intent of the Plan is to prescribe and enforce a means for properly and successfully implementing the mitigation measures as identified within the Initial Study for this project. Unless otherwise noted, the cost of implementing the mitigation measures as prescribed by this Plan shall be funded by the owner/developer identified above. This MMP is designed to aid the City of Sacramento in its implementation and monitoring of mitigation measures adopted for the proposed project.

The mitigation measures have been taken verbatim from the Initial Study and are assigned the same number they have in the document. The MMP describes the actions that must take place to implement each mitigation measure, the timing of those actions, and the entities responsible for implementing and monitoring the actions. The developer will be responsible for fully understanding and effectively implementing the mitigation measures contained within the MMP. The City of Sacramento will be responsible for ensuring compliance.

**8151 SHELDON SUBDIVISION (P05-044)  
MITIGATION MONITORING PLAN**

Mitigation Measure	Implementing Responsibility	Monitoring Responsibility	Compliance Standards	Timing	Verification of Compliance (Initials and Date)
<p><b>Biological Resources:</b>  <b>Mitigation Measures for nesting birds and foraging habitat:</b>                      BR-1 The applicant/developer/constructor shall submit to the City of Sacramento, Department of Development Services verification from the California Department of Fish and Game (DFG) that the applicant has satisfied DFG requirements for mitigating loss of Swainson's hawk foraging habitat. The project applicant shall purchase compensatory Swainson's hawk foraging habitat at a ratio acceptable to DFG from an approved mitigation bank or develop other arrangements acceptable to the DFG prior to building/grading permits being issued.</p> <p>BR-2 If any construction or grading activity is going to occur during the nesting season of February to August, a qualified biologist shall prepare a pre-construction survey within thirty (30) days prior to any grading or construction activity to determine if any special status species or species of special concern are present (that is, nesting raptor species [including Swainson's hawk] and western burrowing owls).</p> <p>BR-3 If active raptor nests are found on the site, the applicant/developer/constructor shall work with the DFG to determine the</p>	Project Applicant/ Developer	City of Sacramento – Development Services Department, Development Engineering and Finance Division, Environmental Planning Services  Ca. Dept. of Fish and Game (DFG)  US Fish and Wildlife Service (USFWS)  Army Corps of Engineers (ACOE)	Measures shall be included with construction specifications	Construction specifications shall be included on the construction plans prior to the issuance of a grading permit.  Measures shall be implemented prior to and during construction activities.	

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8151 SHELDON SUBDIVISION (P05-044)  
MITIGATION MONITORING PLAN

Mitigation Measure	Implementing Responsibility	Monitoring Responsibility	Compliance Standards	Timing	Verification of Compliance (Initials and Date)
<p>appropriate mitigation actions needed prior to proceeding. A copy of the pre-construction study and DFG concurrence with proposed actions shall be submitted to the Department of Development Services prior to issuance of grading or construction permits.</p> <p>BR-4 If an adults-only active burrowing owl burrow(s) nest is discovered during the pre-construction survey the monitoring biologist shall install a one-way door on the burrow(s) and monitor and inspect per Department of Fish and Game Guidelines. If an active nest with chicks is encountered one-way doors shall not be used unless authorized by DFG in writing. No construction shall occur near the nest until the monitoring biologist has consulted with the DFG on the allowing construction to proceed. The monitoring biologist shall through consultations with DFG determine an appropriate buffer between the nest and any construction activity allowed to proceed on the project site prior to the fledging of the chicks. No construction or grading activities shall begin until the monitoring biologist has submitted a written clearance to the Department of Development Services that the burrowing owl(s) have vacated or been safely relocated by the monitoring biologist.</p>					

8151 SHELDON SUBDIVISION (P05-044)  
MITIGATION MONITORING PLAN

Mitigation Measure	Implementing Responsibility	Monitoring Responsibility	Compliance Standards	Timing	Verification of Compliance (Initials and Date)
<p>BR-5 After active burrows are vacated, the burrow must be destroyed completely by the monitoring biologist prior to grading or construction activity.</p> <p><b>Mitigation Measures for Invertebrates:</b></p> <p>BR-6 Prior to the issuance of a grading permit, the Applicant shall provide to the City of Sacramento Development Services Department evidence of compliance with Federal Endangered Species Act (FESA). The following measures shall be implemented and documented for compliance with FESA:</p> <p>a. The Applicant will provide compensatory mitigation as required by USFWS for VPFS and VPTS.</p> <p>b. Under the consultation process, the Applicant will be required to prepare a mitigation plan for submittal to USFWS. The mitigation plan will include the following components for direct and indirect impacts:</p> <ul style="list-style-type: none"> <li>o Avoidance Component. Demonstrate how the project has been designed to minimize impacts to federal-listed vernal pool crustaceans and their habitat (e.g. ESA, biological monitor, and special-status species training for construction personnel).</li> <li>o Preservation</li> </ul>					

8151 SHELDON SUBDIVISION (P05-044)  
MITIGATION MONITORING PLAN

Mitigation Measure	Implementing Responsibility	Monitoring Responsibility	Compliance Standards	Timing	Verification of Compliance (Initials and Date)
<p>Component. For every acre of habitat directly or indirectly affected, at least two (2) vernal pool credits will be dedicated within a USFWS-approved ecosystem preservation bank (2:1 ratio).</p> <ul style="list-style-type: none"> <li>o Conservation Component. For every acre of habitat directly affected, at least one (1) vernal pool creation credit will be dedicated within a USFWS-approved habitat mitigation bank.</li> <li>o In the event that preservation or conservation credits are not available for purchase at the time of mitigation implementation, the deposit of funds, the amount of money to be deposited determined by USFWS, into a USFWS Species Fund in lieu fee program shall be acceptable to satisfy both the preservation and conservation components of the mitigation plan.</li> <li>c. The USFWS will review the mitigation plan and issue a Biological Opinion. The Biological Opinion will include an incidental take statement and approval of the mitigation plan.</li> <li>d. The Applicant will notify the City of Sacramento Development Services Department that VPFS and VPTS mitigation is complete by submitting a copy of the Biological Opinion and bill of sale for the mitigation credits to the City of Sacramento.</li> </ul>					

8151 SHELDON SUBDIVISION (P05-044)  
MITIGATION MONITORING PLAN

Mitigation Measure	Implementing Responsibility	Monitoring Responsibility	Compliance Standards	Timing	Verification of Compliance (Initials and Date)
<p>BR-7 Prior to the issuance of a grading permit the Owner shall provide to the City of Sacramento Development Services Department evidence of compliance with FESA. The following measures will be implemented to document for the City the Owner's compliance with FESA:</p> <p>BR-8 The Owner will provide compensatory mitigation as required by USFWS for VPFS and VPTS.</p> <p>BR-9 Under the consultation process, the Owner will be required to prepare a mitigation plan for submittal to USFWS. The mitigation plan will include the following components for direct and indirect impacts:</p> <p>a. Avoidance Component. Demonstrate how the project has been designed to minimize impacts to federal-listed vernal pool crustaceans and their habitat (e.g. ESA, biological monitor, and special-status species training for construction personnel).</p> <p>b. Preservation Component. For every acre of habitat directly or indirectly affected, at least two (2) vernal pool credits will be dedicated within a USFWS-approved ecosystem preservation bank (2:1 ratio).</p> <p>c. Conservation Component. For every acre of habitat directly affected, at least one (1) vernal pool creation credit</p>					

8151 SHELDON SUBDIVISION (P05-044)  
MITIGATION MONITORING PLAN

Mitigation Measure	Implementing Responsibility	Monitoring Responsibility	Compliance Standards	Timing	Verification of Compliance (Initials and Date)
<p>will be dedicated within a USFWS-approved habitat mitigation bank.</p> <p>d. In the event that preservation or conservation credits are not available for purchase at the time of mitigation implementation, the deposit of funds, the amount of money to be deposited determined by USFWS, into a USFWS Species Fund in lieu fee program shall be acceptable to satisfy both the preservation and conservation components of the mitigation plan.</p> <p>BR-10 The USFWS will review the mitigation plan and issue a Biological Opinion. The Biological Opinion will include an incidental take statement and approval of the mitigation plan.</p> <p>BR-11 The Owner will notify the City of Sacramento Development Services Department that VPFS and VPTS mitigation is complete by submitting a copy of the Biological Opinion and bill of sale for the mitigation credits to the City of Sacramento.</p> <p><b>Mitigation Measures for Wetlands subject to Section 404 CWA jurisdiction:</b></p> <p>BR-12 Prior to issuance of a grading permit, the Developer shall provide verification of status of the wetlands from the Corps.</p> <p>BR-13 The Developer shall submit a wetland</p>					

**8151 SHELDON SUBDIVISION (P05-044)  
MITIGATION MONITORING PLAN**

Mitigation Measure	Implementing Responsibility	Monitoring Responsibility	Compliance Standards	Timing	Verification of Compliance (Initials and Date)
<p>mitigation and monitoring plan to the City:</p> <p>a. The mitigation plan will be prepared in accordance with the requirements of the Army Corps of Engineer's Regulatory Guidance Letter (RGL 02-02) for compensatory wetland mitigation and the Mitigation and Monitoring Proposal Guidelines (Corps, 30 December 2004).</p> <p>b. The mitigation plan will describe how the jurisdictional wetlands in the grading plan area will be mitigated. Mitigation may include the purchase of wetland mitigation credits at a Corps approved mitigation bank.</p> <p>c. A copy of the bill of sale for the purchase of wetland mitigation credits will be submitted to the City.</p> <p>BR-14 The Grading Permit shall be conditioned to not allow grading within 250 feet of any jurisdictional wetland until the Developer provides the City of Sacramento evidence that the discharge of fill into jurisdictional wetlands is authorized under Section 404 of the Clean Water Act.</p> <p><b>Mitigation Measures for Isolated wetlands subject to Porter-Cologne Act:</b></p> <p>BR-15 Prior to issuance of a grading permit, the Developer shall provide</p>					

8151 SHELDON SUBDIVISION (P05-044)  
MITIGATION MONITORING PLAN

Mitigation Measure	Implementing Responsibility	Monitoring Responsibility	Compliance Standards	Timing	Verification of Compliance (Initials and Date)
<p>verification of status of the wetlands from the Corps.</p> <p>BR-16 Prior to issuance of a grading permit, the Developer will submit a wetland mitigation and monitoring plan to the City:</p> <p>a. The mitigation plan will be prepared in accordance with the requirements of the RWQCB's Water Quality Order No. 2004-0004-DWQ wetland mitigation.</p> <p>b. The mitigation plan will describe how the isolated wetlands will be mitigated. Mitigation may include the purchase of wetland mitigation credits at a mitigation bank.</p> <p>c. A copy of the bill of sale verifying the purchase will be included in the mitigation plan.</p> <p>BR-17 The Grading Permit shall be conditioned to not allow grading within 250-feet of the isolated wetlands until the Developer provides the City of Sacramento evidence that the discharge of fill into the isolated wetlands is authorized under the Porter-Cologne Act.</p> <p>BR-18 The Grading Permit shall be conditioned to require temporary fencing to be installed around the isolated wetlands and the buffer to exclude construction equipment until the Developer provides the City of Sacramento evidence that the discharge of fill into the</p>					

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MITIGATION MONITORING PLAN**

Mitigation Measure	Implementing Responsibility	Monitoring Responsibility	Compliance Standards	Timing	Verification of Compliance (Initials and Date)
isolated wetlands is authorized under the Porter-Cologne Act.					
<p><b>Noise:</b> To reduce exterior sound levels in the backyards of the lots adjacent to Sheldon Road to 60 dB Ldn or less, the normally acceptable level, at a minimum the following mitigation measures shall be incorporated into the project:</p> <p>N-1. For lots adjacent to Sheldon Road (lots 35-36), a sound wall shall be constructed as follows: a. Along the south property line of Lots 35-36. b. On the east property line of Lot 36 (unless a wall is constructed first by future development west of the site). c. To a position even with the back corner of the home on the west property line of Lot 36.</p> <p>N-2. The sound walls must be constructed as follows: a. Continuous along their widths and heights with no gaps, including at the ground. b. A minimum height of seven (7) feet shall be required. c. Suitable materials for the sound wall include earthen berms, masonry block, pre-cast concrete panels, or combinations thereof, provided that it meets all of the other requirements listed above.</p>	Project Applicant/ Developer	City of Sacramento – Development Services Department; Development Engineering Division	Measures shall be included with construction specifications	Construction specifications shall be included on the construction plans prior to the issuance of a grading permit.  Measures shall be implemented during construction activities	

8151 SHELDON SUBDIVISION (P05-044)  
MITIGATION MONITORING PLAN

Mitigation Measure	Implementing Responsibility	Monitoring Responsibility	Compliance Standards	Timing	Verification of Compliance (Initials and Date)
<p>To ensure that interior noise levels do not exceed the threshold of 45 dB L<sub>dn</sub>, the following measures shall be incorporated into the development project.</p> <p>N-3. At a minimum, air conditioning shall be included for all lots where the 45 dB L<sub>dn</sub> interior threshold would potentially be exceeded (lots 35-36) to allow the occupants to close doors and windows as desired to achieve acoustical isolation.</p>					
<p><b>Cultural Resources:</b></p> <ol style="list-style-type: none"> <li>If subsurface archaeological or historical remains are discovered during construction, work in the area shall stop immediately and a qualified archaeologist and a representative of the Native American Heritage Commission shall be consulted to develop, if necessary, further mitigation measures to reduce any archaeological impact to a less than significant level before construction continues.</li> <li>If human burials are encountered, all work in the area shall stop immediately and the Sacramento County Coroner's office shall be notified immediately. If the remains are determined to be Native American in origin, both the Native</li> </ol>	Project Applicant/ Developer	City of Sacramento Development Services Department	<p>Incorporate the protocol and procedures into grading and construction contracts.</p> <p>If any cultural resources are discovered</p> <ol style="list-style-type: none"> <li>Cease all work in the immediate area</li> <li>Retain a qualified archaeologist or cultural resources consultant to perform a site investigation and take appropriate action.</li> </ol>	During grading and construction contract agreements During construction activities	

8151 SHELDON SUBDIVISION (P05-044)  
MITIGATION MONITORING PLAN

Mitigation Measure	Implementing Responsibility	Monitoring Responsibility	Compliance Standards	Timing	Verification of Compliance (Initials and Date)
American Heritage Commission and any identified descendants must be notified and recommendations for treatment solicited (CEQA Section 15064.5); Health and Safety Code Section 7050.5; Public Resources Code Section 5097.94 and 5097.98.					

**RESOLUTION NO.**

Adopted by the **Sacramento** City Council

**AMENDING THE 2030 GENERAL PLAN LAND USE AND URBAN FORM DIAGRAM FOR APPROXIMATELY 3.8 ACRES FROM SUBURBAN NEIGHBORHOOD HIGH DENSITY TO SUBURBAN CENTER (P10-060)(APN: 117-0220-002, 022, 023, 024)**

**BACKGROUND**

- A. On March 3, 2009, Council adopted the 2030 General Plan (Resolution No. 2009-131).
- B. On January 13, 2011 the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve proposed amendment to the 2030 General Plan Land Use and Urban Form Diagram.
- C. On \_\_\_\_\_, the City Council conducted a public hearing, for which notice was given pursuant Sacramento City Code Section 17.200.010(C)(1)(a).

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:**

Section 1. Based on the verbal and documentary evidence received at the hearing, the City Council approves the 2030 General Plan Land Use and Urban Form Diagram Amendment as follows:

- A. Environmental Determination: Addendum to a prior Mitigated Negative Declaration (P05-044)
- B. The 2030 General Plan Land Use and Urban Form Diagram Amendment is approved as set forth in Exhibit A
- C. Exhibit A is part of this Resolution.



8151 Sheldon Commercial Rezone (P10-060)

January 13, 2011

Attachment 5 – Rezone

**ORDINANCE NO.**

Adopted by the Sacramento City Council

**AMENDING TITLE 17 OF THE SACRAMENTO CITY CODE (THE ZONING CODE) BY REZONING CERTAIN REAL PROPERTY FROM MULTI-FAMILY RESIDENTIAL (R-2B) TO GENERAL COMMERCIAL REVIEW (C-2-R).  
(P10-060)(APN: 117-0220-022, 023, 024)**

BE IT ENACTED BY THE COUNCIL OF THE CITY OF SACRAMENTO:

Section 1. Title 17 of the Sacramento City Code (the Zoning Code) is amended by rezoning the property shown in the attached Exhibit A, generally described, known, and referred to as 8151 Sheldon (APN: 117-0220-022, 023, 024) and consisting of approximately 2.8 net acres, from Multi-Family Residential (R-2B) to General Commercial Review (C-2-R).

Section 2. Rezoning of the property shown in the attached Exhibit A, by the adoption of this Ordinance, will be considered to be in compliance with the requirements for the rezoning of property described in the Zoning Code, as amended, as those procedures have been affected by recent court decisions.

Section 3. The City Clerk of the City of Sacramento is directed to amend the official zoning maps, which are part of the Zoning Code, to conform to the provisions of this Ordinance.





# REPORT TO PLANNING COMMISSION City of Sacramento

# 7

915 I Street, Sacramento, CA 95814-2671

PUBLIC HEARING  
January 13, 2011

To: Members of the Planning Commission

**Subject: River District Specific Plan (M09-003)**

**Location/Council District:**

The River District Specific Plan area is bounded by Downtown and the Railyards on the south, the Sacramento River on the west, the American River on the north, and 16th and 18th Streets on the east.

Council District 1 and 3

**Recommendation: Forward Recommendations of Approval to City Council** - 1) a **Resolution** certifying the Environmental Impact Report (EIR) and adopting Findings of Fact, Statements of Overriding Considerations and the Mitigation Monitoring Plan; 2) a **Resolution** amending the General Plan land use diagram to change the land use designation for various parcels in accordance with the River District Specific Plan; 3) a **Resolution** to rescind the Richards Boulevard Area Plan (RBAP) and Facility Element, amend the 2030 General Plan Circulation Element, and adopt the River District Specific Plan and Infrastructure Financing Plan; 4) a **Resolution** to amend the Railyards Specific Plan to change the planned future operation of 5<sup>th</sup> and 7<sup>th</sup> Streets; 5) a **Resolution** to amend the Bikeway Master Plan; 6) a Resolution to adopt the River District Design Review Guidelines; 7) a **Resolution** to approve a Water Supply Assessment Report; 8) a **Resolution** to rescind the Discovery Centre PUD; 9) an **Ordinance** to adopt the River District Special Planning District; and 10) an **Ordinance** rezoning various parcels in the River District.

**Contact:** Evan Compton, Associate Planner, Community Development (916) 808-5260  
Stacia Cosgrove, Senior Planner, Community Development (916) 808-7110  
Greg Taylor, Senior Architect, Community Development (916) 808-5268

**Summary:** City staff is seeking a recommendation from Planning Commission to City Council for approval of: **A)** amendments to the 2030 General Plan land use diagram to change the land use designation for multiple properties in order to provide consistency with the zoning designations (See Attachment 3); **B)** rescinding the Richards Boulevard Area Plan and Facility Element, amending the General Plan Circulation Element, and adopting the River District Specific Plan and Infrastructure Financing Plan to establish



Subject: River District Specific Plan (M09-003)

January 13, 2011

policies to guide the location, intensity, and character of land uses and the necessary infrastructure improvements to support the redevelopment of the River District Area (See Attachment 4); **C**) amendments to the Railyards Specific Plan to change the future operation of 5<sup>th</sup> and 7<sup>th</sup> Streets as two way streets (See Attachment 5); **D**) amendments to the Bikeway Master Plan to incorporate the bikeway network in the Sacramento River District Specific Plan (See Attachment 6); **E**) adopting the River District Design Review District (See Attachment 7) **F**) approving the water supply assessment report which is required by State law to evaluate projected water supplies for the project over a 20 year period (See Attachment 8); **G**) rescinding the Discovery Centre Planned Unit Development Guidelines (See Attachment 9); **H**) amend the Zoning Code 17.20.030, repeal the Richards Boulevard Special Planning District, and adopt the River District Special Planning District (See Attachment 10); and **I**) the rezone of multiple properties to encourage a greater mix of development in the River District (See Attachment 11).

**Background Information:** On December 13, 1994, the City adopted the Richards Boulevard Area Plan (M93-119), commonly referred to as the “RBAP.” The RBAP is a community plan establishing land uses and development standards to guide decisions on development and growth in the River District. On December 11, 2007, the City Council adopted Resolution 2007-915 directing staff to proceed with an update to the RBAP; the River District Specific Plan (M09-003) is that update.

The River District Specific Plan is a plan which addresses zoning, infrastructure, circulation, parks and open spaces, urban design, and the treatment of cultural resources. The Specific Plan also includes an updated financing plan for public infrastructure to set development impact fees, an updated nexus study which examined the costs of public infrastructure and fairly distributed those costs between Downtown, the River District, and the Railyards.

Few warehouse districts have the luxury of two light rail lines (LRT) connecting through their district to aid in the development of a diverse mixed-use area. The examples that exist, such as Portland’s Pearl District which was a former railyard, have capitalized on transit to create a very walkable area, which is one of the goals of the River District Specific Plan. The River District will soon have two LRT stations that will link it to critical hubs in the region including the Sacramento International Airport and the regional Sacramento Intermodal Transit Station.

Previous File Numbers:

Richards Boulevard Area Plan (M93-119)

Facility Element (M93-123)

Richards Boulevard Special Planning District Ordinance (M93-121)

Richards Boulevard SPD Text Amendment (M07-051): Modification for Township 9 to exempt development in a PUD from Design Review. Projects are handled at the Planning Director level consistent with other areas outside of the SPD per Zoning Code Section 17.132 for design review.

Subject: River District Specific Plan (M09-003)

January 13, 2011

**Existing Conditions in the River District:** The River District is home to a large number of government entities. The State of California's Printing Plant, Telecommunications Division, Lottery and California Highway Patrol are located in the District. The County's Comprehensive Alcoholism Treatment Center and Sheriff Department's Work Release Facility as well as the City of Sacramento's Police and Community Development Departments (CDD) and Sacramento City and County Archives are also located in the River District.

The businesses in the River District range from retail to warehousing. Downtown Ford is located North 16<sup>th</sup> Street and is among the larger sales tax revenue generators in the city. General Produce celebrated its 75<sup>th</sup> anniversary and Schetter Electric its 50<sup>th</sup> anniversary. Sacramento Theatrical Lighting is on Richards Boulevard and is celebrating 60 years in business. Development activity is currently focused at 7<sup>th</sup> Street and Richards Boulevard. In August 2008, the Township 9 project was approved for approximately 2,300 housing units, 150,000 square feet of retail and 800,000 square feet of office. The 65 acre project, located on the west side of North 7<sup>th</sup> Street north of Richards Boulevard, was awarded \$17 million in State 1-C funds in 2008. Also approved is the headquarters consolidation of the California Highway Patrol which brought 900 new employees to the District. The CHP Headquarters is located at Continental Plaza, which is on the east side of North 7<sup>th</sup> Street north of Richards Boulevard. Both of these projects will benefit from the Regional Transit Light Rail Station to be located on the northwest corner of Richards Blvd. and North 7<sup>th</sup> Street, adjacent to Township 9. This first segment of the DNA (Downtown/Natomas/Airport) line, called MOS-1, is under construction.

A number of social services providers also have operations in the District. They include Loaves & Fishes, Salvation Army, Volunteers of America, Quinn Cottages, and Union Gospel Mission. Additionally, Sacramento Housing and Redevelopment Agency owns the Two Rivers Housing Project with 218 units of affordable housing.

**River District Update:** The 773-acre River District Area (see boundary map attached) proposes adopting policy documents to support a transit-oriented mixed use urban environment that would include 8,144 dwelling units, 3.956 million square feet of office, 854,000 square feet of retail/wholesale, 1.463 million square feet light industrial, and 3,044 hotel units. The vision for the River District is that of an eclectic mix of uses that will evolve from a primarily light-industrial, low-intensity commercial district, to that of a series of distinctive walkable neighborhoods within a district that is contiguous to the American River and serves as the northern gateway into the Central City. The land is divided into approximately 422 separate parcels held by over 200 property owners. The District is currently home to 386 residential units, approximately 5.07 million square feet of industrial uses, 384,000 square feet of retail/wholesalers and 1.312 million square feet of office.

The overall average density of the project is 14 dwelling units per net acre however, the most intense zoning in the River District allows up to 174 dwelling units per acre. Floor area ratios (FARs) range up to 4.0.

**Land Use:** The following assumptions pertaining to the distribution of land uses and proposed intensities have been made about the future development of the River District Specific Plan. For more information, see Chapter 3 of the River District Specific Plan.

<b>Table 1: River District Specific Plan Land Use Program</b>			
<b>Land Use</b>	<b>Existing (2010)</b>	<b>25 Year Plan Projections</b>	<b>Net Change</b>
Residential Units	386 units	8,144 units	+7,758 units
Civic/Institutional	103,029 sqft	103,029 sqft	No change
Retail/Wholesale	384,000 sqft	854,000 sqft	+470,000 sqft
Office	1,312,000 sqft	3,956,000 sqft	+2,644,000 sqft
Light Industrial	5,070,000 sqft	1,463,000 sqft	-3,607,000 sqft
Hotel (rooms)	1,006 rooms	3,044 rooms	+2,038 rooms
Parks and Open Space (acres)	16 acres	55.5 acres	+39.5 acres

**River District Specific Plan Area Boundary Modifications:** The 773 gross acre River District Specific Plan area (550 net developable acres) was once part of a larger 1,600 acre planning effort in 1994, which established the Richards Boulevard Area and Railyards Specific Plan (also known as the Roma Plan). A separate document known as the Facility Element for the Railyards Specific Plan and Richards Boulevard Area Plan was adopted to provide infrastructure planning policies. With the approval of the Railyards project, the Facility Element was modified however, the Richards Boulevard portion of the document requires updating to be consistent with the new street layouts, Specific Plan boundary, and the relocation of the proposed intermodal facility to the Railyards.

Amendments to the the Richards Boulevard Area Plan and the Railyards/Richards Boulevard Facility Element, and the Richards Boulevard Special Planning District are necessary to incorporate the new Specific Plan. The current boundaries of the River District Specific Plan focuses on the Richards Boulevard Area only but the boundaries differ slightly from the previous Richards Boulevard Specific Plan by deleting many parcels on the east side of 18<sup>th</sup> Street.

The deleted parcels formally under the Richards Boulevard SPD, but outside of the proposed River District SPD, are zoned Industrial (M-2) and are also located in the East Overlay area of the Richards Boulevard SPD. The parcels will be located in the new

River District Design Review District and will be subject to the new River District Design Review Guidelines. No land use or zoning changes are proposed for the areas outside of the current River District Specific Plan Area. The current Richards Boulevard SPD East overlay zone prohibits offices with greater than 25% gross floor area of the building. The overlay zone also requires a different calculation for bicycle parking standards. With the revocation of the Richards Boulevard area, the development standards applied to these parcels will change however, the citywide zoning code will apply and the net effect would be minimal. The zoning code already limits the amount of office in the M-2 zone to a maximum of 10,000 square feet per parcel or up to 25% of the gross floor area per parcel, whichever is greater, without further entitlements. Bicycle parking is required for projects and the citywide standards are very similar to the previous Richards Boulevard SPD East Overlay standards. Furthermore, the parcels will also retain the Parkway Corridor Overlay zone and will be required to meet the standards of this code to ensure development is compatible with the American River by dictating items such as height, setback, building color and materials. Staff believes that removing these parcels from the proposed River District Specific Plan will not have any significant effect on the implementation of the new Specific Plan. The parcels were removed from the new SPD because staff anticipates the current industrial uses such as Blue Diamond would be unlikely to change in the near future and there was no reason to rezone this industrial area.

**Environmental Considerations:** In accordance with CEQA Guidelines Section 15081, the City as Lead Agency, determined that an EIR should be prepared for the proposed project. The EIR analyzed, at a programmatic level, the potential impacts due to the planning, construction, and implementation of the River District Specific Plan. The following were analyzed for potential impacts: air quality, biological resources, cultural resources, hazards and hazardous materials, hydrology and water quality, noise and vibration, parks and open space, public services, public utilities, and transportation and circulation. Land use issues were discussed. The Mitigation Monitoring Plan (MMP) that lists all of the mitigation measures and implementing actions was prepared and is attached (See Exhibit B).

With mitigation, the development and operation of the River District Specific Plan project would result in less-than-significant impacts in all issue areas, with the exception of the following project-level Significant and Unavoidable impacts:

- Change in the significance of a historical resource with the demolition of the State Printing Plant
- Change in the significance of an archaeological resource through potential disturbance of the resource during development of the project
- Exterior and interior noise levels that are above the upper value of the normally acceptable category for various land uses due to increased traffic noise

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- Construction vibration levels at residential and commercial areas that exceed the threshold
- Impacts to intersections and roadway segments within the RDSP area due to increases in traffic in Year 2015
- Impacts to freeway mainline segments, off ramps, and interchanges in Year 2035

The following impacts associated with the cumulative impacts of the River District Specific Plan project were determined to be Significant and Unavoidable:

- Cumulative contribution to regional air quality conditions due to construction-generated NOx and particulate matter
- Cumulative contribution to substantial changes to historic or archeological resources
- Cumulative contribution to impacts to intersections and roadway segments within the RDSP area due to increases in traffic in Year 2035

The City received comments on the Draft EIR. The predominant issues raised by agencies and the public were:

- Impacts to freeway facilities
- Impacts to the site of the State Printing Plant and future use of the site by the State due to proposed road extensions
- Greenhouse gas emissions
- Potential impacts to the levees within the Specific Plan area
- Potential impacts to school facilities
- Adequate provision of bicycle facilities
- Continued viability and existence of industrial uses, specifically Sims Metal, due to adoption of the Specific Plan

The responses to these comments are found in the Final EIR on the City's website at: <http://www.cityofsacramento.org/dsd/planning/environmental-review/eirs/>

A Notice of Completion (NOC) and copies of the Draft EIR were distributed to the Office of Planning and Research on July 27, 2010 (SCH 2009062023). The 45-day public comment period began on July 27, 2010 and ended on September 9, 2010.

A public notice was placed in the Sacramento Bulletin on July 27, 2010 which stated that the Draft EIR was available for public review and comment.

A public notice was posted in the office of the Sacramento County Clerk on July 23, 2010.

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Following closure of the public comment period, all comments received on the Draft EIR, the City's written responses to the significant environmental points raised in those comments, and additional information added by the City were added to the Draft EIR to produce the Final EIR.

The Planning Commission must review and consider the information contained in the EIR in making a recommendation on the Project to the City Council.

**Public/Neighborhood Outreach and Comments:** Public outreach has been ongoing since this project was initiated in December of 2007. Staff regularly met with the River District Stakeholder Group, a working group of River District property owners. In February and March of 2008 staff conducted three community "visioning workshops" to identify issues requiring focused study and to formulate the vision and guiding principles for the future of the district.

In February of 2009, staff conducted targeted "Property Owner Meetings" to introduce the draft land use and circulation elements of the Specific Plan, explain the potential impacts to individual property owners, and to capture their feedback. Those in attendance were largely supportive of the proposed land use and circulation elements.

Public outreach is a very important component of this planning project and every effort is being made to engage with area residents, property owners, public agencies, not-for-profits, and other stakeholders. The following is a compilation of those efforts to date:

Stakeholder Group Meeting	January 17, 2008
Stakeholder Group Meeting	January 29, 2008
Stakeholder Group Meeting	February 12, 2008
Community Visioning Workshop	February 20, 2008
Community Visioning Workshop	February 21, 2008
Dan Burden Workshops	March 4 and 5, 2008
Lower American River Task Force	March 11, 2008
Stakeholder Group Meeting	March 14, 2008
Presentation of Preferred Alternatives	March 19, 2008
American River Parkway Advisory Committee	April 16, 2008
Stakeholder Group Meeting	May 2, 2008
Lower American River Task Force	June 10, 2008
Lunch & Learn (Community Dev. Dept.)	June 18, 2008
Stakeholder Group Meeting	September 30, 2008
Stakeholder Group Meeting	December 17, 2008
Stakeholder Group Meeting	January 23, 2009
Property Owner Workshops	February 11, 2009
Property Owner Workshop	February 12, 2009
Historic Resources Survey Workshop	March 23, 2009
Stakeholder Group Meeting	March 26, 2009
Regional Parks Advisory Group	April 17, 2009
Stakeholder Group Meeting	April 23, 2009
External Stakeholder Workshop	May 28, 2009

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Real Estate Brokers Presentation	June 2, 2009
Stakeholder Group Meeting	June 25, 2009
Stakeholder Group Meeting	September 24, 2009
Historic Survey Workshop	September 24, 2009
Stakeholder Group Meeting	October 22, 2009
SAFCA and ARFCD	October 30, 2009
Stakeholder Group Meeting	November 19, 2009
Stakeholder Group Meeting	December 3, 2009
SAFCA and ARFCD	March 19, 2010
Zoning Changes Public Workshop	April 27, 2010
Zoning Changes Public Workshop	April 29, 2010
Stakeholder Group Meeting	June 23, 2010
River District PBID	August 25, 2010
Twin Rivers Housing	October 12, 2010

Individual meetings with key area stakeholders, including Regional Transit, SMUD, PG&E, SAFCA, ARFCD, SHRA, Sacramento County, SMAQMD, Twin Rivers Unified School District, and the State of California. Furthermore, staff maintains and regularly updates a page on the City's website dedicated to this project.

**Summary of Public Hearings Conducted:** Table 2 below lists the public hearings conducted on the River District Specific Plan project.

<b>Table 2: List of Public Hearings</b>		
<b>Hearing Body</b>	<b>Date</b>	<b>Purpose</b>
City Council	July 15, 2008	Review and Comment
Planning Commission	April 9, 2009	Review and Comment
Preservation Commission	August 4, 2010	Review and Comment
Parks Commission	August 5, 2010	Review and Comment
Planning Commission	August 12, 2010	Review and Comment
Design Commission	August 18, 2010	Review and Comment
Preservation Director	October 13, 2010	Final Recommendation
City Council	October 19, 2010	Review and Comment
Preservation Director	October 20, 2010	Final Recommendation
Preservation Commission	November 3, 2010	Public Hearing Continued
Preservation Commission	December 1, 2010	Final Recommendation
Design Commission	January 12, 2011	Final Recommendation
Planning Commission	January 13, 2011	Final Recommendation
Law and Legislation	January 18, 2011*	Final Recommendation
City Council PFP	January 25, 2011*	Pass for Publication
City Council	February 1, 2011*	Final Adoption

\*Anticipated Hearing Dates Subject to Change

**Historic Resources:** The River District Specific Plan recognizes the economic and cultural value of the historic resources in the area. The plan's goal is to preserve and

incorporate these assets into future developments in a manner that will enhance the urban fabric and neighborhood viability of the River District. The plan proposes to create a historic district for the North 16<sup>th</sup> Street area and also to designate eleven properties for individual landmark status. For more information, see Chapter 4 of the River District Specific Plan.

**Circulation:** The River District Specific Plan seeks to maximize vehicle and pedestrian/bicycle connections within and between the River District and surrounding neighborhoods. The area has historically been disconnected because of the rivers, railroad tracks, and the secondary levee and therefore connectivity is a major component of urban renewal for the area. North 12<sup>th</sup> Street, North 16<sup>th</sup> Street, Interstate 5, and most recently North 7<sup>th</sup> Street have been the major connections for entering and exiting the area. The Specific Plan also has policies for the construction of streets through the secondary levee including North 5<sup>th</sup> Street, North 6<sup>th</sup> Street, Judah Street, North 10<sup>th</sup> Street, and North 14<sup>th</sup> Street. For more information, see Chapter 5 of the River District Specific Plan.

**Parks and Open Space:** The River District Specific Plan seeks to provide a community park of ten acres or larger which is consistent with the City's Parks and Recreation Master Plan 2005-2010. The plan encourages neighborhood parks and open space within one-half mile of all residences and easily accessible by employees working in the district. The goal is to encourage the connection between development and the rivers while at the same time respecting the riparian habitat and critical environmental areas. For more information, see Chapter 6 of the River District Specific Plan.

**Public Services and Community Facilities:** As more intensive uses are developed in the River District, there will be increased demand for expanded school, police, and fire services. Beyond the community facilities that will be developed to meet the specific demands of planned development in the area, the Specific Plan also provides for new facilities which will be of benefit to the larger region. Chapter 7 in the River District Specific Plan describes the community facilities and public services that will be provided in the District as new development occurs.

**Utility Infrastructure:** The redevelopment of the River District and its transformation from predominantly light industrial uses to mixed use development with higher residential densities will require significant improvements to the existing utility systems. These improvements will require a coordinated approach between private and public development to ensure that adequate capacity is provided and to allow for financing of the public infrastructure facilities. The Specific Plan Finance Plan provides costs estimates and identifies funding sources for these public capital improvements. The Specific Plan also addresses key environmental considerations related to water conservation, water quality and energy conservation. For more information, see Chapter 8 in the River District Specific Plan.

### **Implementation:**

The implementation of the Specific Plan has several components which include the Special Planning District, Design Guidelines, Historic District and Individual

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Landmarks, and the Finance Plan. Each component is discussed below. For more information about the implementation of the River District Specific Plan, see Chapter 9 in the River District Specific Plan. (Attachment 4)

**Special Planning District:** The River District area was designated as a Special Planning District (SPD) when Richards Boulevard Area Plan was adopted in 1994. The implementation of the 2010 River District Specific Plan will include enacting the River District Special Planning District, amending the General Plan for specific properties, and completing rezones.

The Sacramento River District Special Planning District, currently Chapter 17.120 of Title 17 of the City Code, will be completely revised to reflect the new Specific Plan zoning designations, development standards, and land uses. The enactment of the proposed River District SPD will ensure the implementation of the goals and policies of the Specific Plan and Design Guidelines.

The Draft Ordinance and Special Planning District is attached as Attachment 11.

### **General Criteria for Special Planning Districts**

*In justifying the use of a SPD, the planning commission and city council need to determine that routinely used zoning and other standard regulatory ordinance provisions, as well as general and community plan policies, should be replaced by or supplemented with specifically tailored provisions intended to positively benefit the district and its immediate surrounding area such as: a) The SPD provisions will offer a greater mix of land uses and/or intensities, thereby increasing the likelihood of attracting new private investment. Staff finds the SPD along with the proposed rezones will encourage the transformation of a primarily industrial area into a transit-supportive mixed use area; b) The SPD provisions will promote retention of unique geographic or historic features consistent with quality land use design practices. Staff finds the SPD along with the creation of the North 16<sup>th</sup> Street Historic District and Individuals Landmarks will retain unique features of the area and allow for an eclectic district; and c) The SPD provisions will promote a significant reversal in a long term trend of area economic stagnation or physical blight. Staff finds the River District has been isolated from the downtown with few street connections which has contributed to economic stagnation and blight. The new circulation network and extension of the grid will connect the area to the Railyards and downtown to encourage new development.*

### **Mandatory Contents for Special Planning Districts**

*A SPD shall be established by ordinance, and shall include provisions that address the following:*

- A. *Reasons for establishing the SPD;*
- B. *Legal description of properties included within the SPD;*

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- C. *A list of general or specific uses permitted in the district;*
- D. *Performance and development standards including, but not limited to setbacks, landscaping, building height, building intensity, security, parking, and pedestrian and auto traffic flow;*
- E. *Design standards including, but not limited to, an overall design theme, façade treatments, lighting, and signing requirements;*
- F. *Project review procedures including, but not limited to, types of projects that require review and levels of review; noticing requirements; and documents required from developers.*

Staff finds that all the mandatory contents for the Special Planning District have been provided in the River District Special Planning District as discussed below.

The River District Special Planning District will implement the River District Specific Plan. The Zoning Ordinance is the planning tool for implementing these objectives through regulations and incentives. In developing the Special Planning District for the River District, two main issues arose: 1) the timing of the zoning changes, and 2) the treatment of nonconforming uses.

- 1) *Timing of Zoning Changes:* Staff considered three options in regards to implementing zoning changes in the River District area:
  - a) Market Driven: No rezones would be proposed. The property owner based on the market would apply for a rezone when the owner wishes to redevelop the property.
  - b) Phased Rezoning: Rezone properties which are consistent with designated land use districts and delay rezones of properties for land use districts in which infrastructure and amenities are needed to support proposed development; and
  - c) Regulatory Approach: Rezone all properties consistent with adopted land use designations.

Given the approvals of Township 9, Continental Plaza, the California Highway Patrol Campus, the current extension of the light rail to Richards Boulevard, and approval of the adjacent Railyards project, staff feels it is an appropriate time to seek option C.

- 2) *Treatment of Nonconforming Uses:* A nonconforming use means any land use which does not conform to the zoning regulations for the area the use is located. As an example, a warehouse may be constructed on an industrial zoned property however, the parcel may be subsequently rezoned to a less intensive zone and the warehouse use would be considered nonconforming. Any requests to expand the use would trigger planning entitlements and if the building becomes vacant for a specified period of time, the nonconforming use would be discontinued and

any further use of the building would have to conform to the requirements of the zone.

The River District SPD treatment of nonconforming uses proposes to be less restrictive than the citywide code so the impact of the implementation of the Specific Plan will not force viable industrial uses out of business. At the same time, it is more restrictive than the existing Richards Boulevard SPD requirements so the desired changes to the district will be more likely to take effect over the life of the plan.

#### *General City Code Requirements for Nonconforming Uses*

The City Code generally allows only 1 year before a vacated nonconforming use is considered discontinued. For nonconforming uses that are destroyed more than 50%, they cannot be rebuilt.

#### *Existing Richards Boulevard Requirements for Nonconforming Uses*

The Richards Boulevard SPD allows restoring nonconforming uses as long as the use has not been discontinued for more than 4 years. The Planning Commission may extend it for 3 years twice, for a total of 10 years. For nonconforming uses that are destroyed by fire, flood, or other calamity, the use may be restored as long as it is commenced within 3 years. The Planning Commission may extend it for 2 years for a total of 5 years.

#### *Proposed River District Requirements for Nonconforming Uses*

The River District SPD would allow operating nonconforming uses to continue. For vacated nonconforming uses, the use would be discontinued after 4 years and the Zoning Administrator may approve a 2 year extension for a total of 6 years. For nonconforming uses that are destroyed by fire, flood, or other calamity, the use may be restored as long as it is commenced within 2 years. The Zoning Administrator may extend it for 2 years for a total of 4 years. After the nonconforming use has been discontinued, any new proposed use would have to conform to the current zoning regulations.

Expansions to existing nonconforming uses would require a Zoning Administrator Special Permit. Changes from one nonconforming use to another nonconforming use may be allowed by right for industrial uses located on Table 1 in the Special Planning District and with a Zoning Administrator Special Permit for land uses on Table 2 in the SPD.

#### ***Summary of Subareas and Rezones within the River District Specific Plan Area:***

The rezone request applies to properties only within the River District Special Planning District. A list of parcel numbers and the current and proposed zoning is outlined in the draft Ordinance as Attachment 11. The following summary explains the vision for each area and reasoning for the proposed rezones.

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Jibboom Street Area- The Jibboom Street area is located west of Interstate-5, between Jibboom Street and the Sacramento River. The area is currently developed with a number of hotels, highway oriented commercial, and the historic PG&E powerplant (unoccupied at this time). Robert T. Matsui park is located at the southern edge of the district. The area has a direct connection to Old Sacramento via an off-street bike trail along the river.

The Specific Plan vision for this area is as a destination for tourists, with a concentration of hotels, restaurants, and entertainment venues. The area is expected to retain its service commercial uses, catering to the traveling public. Buildings along the Sacramento riverfront are expected to take advantage of the view of the river through increased height and convenient riverfront access.

Staff is proposing to eliminate all the Highway Commercial (HC) zoning and replace it with General Commercial (C-2). The HC zone is primarily for uses to serve motorists and provide accommodations. Staff feels that the parcels in the River District that front the Sacramento Riverfront should be zoned with a C-2 zone which is a flexible commercial zone allowing hotels, residential, retail, and office to activate the area.

Sequoia Street Area- The Sequoia transit area is located east of Interstate 5, north of Richards Boulevard to the American River, and east to North 5<sup>th</sup> Street. The area is currently characterized by the development of single-story, small tenant offices. There is a mix of local serving and highway serving commercial uses, including restaurants and a service station.

The Specific Plan vision for the area is one that takes advantage of the District's proximity to the future light rail transit station, to be located on Sequoia Street. The station is expected to be a gateway into the River District and Central City from the Sacramento International Airport. The station area will be the defining feature of the District, including a pedestrian plaza, and surrounded by a transit supportive mix of office and residential uses. The area is also expected to include local and visitor serving retail and commercial uses.

Staff is proposing to eliminate all Heavy Industrial (M-2) zoning and replace it with Residential Mixed Use (RMX), Office (OB), and Limited Commercial (C-1). This area will have a future light rail station and these zones will provide land uses to encourage public transit use.

Bannon Street Area- The Bannon Street Area is generally bounded by Richards Boulevard to the north, Interstate 5 to the west, North 10<sup>th</sup> Street to the east, and the Railyards to the south. The vision includes predominantly office uses fronting Richards Boulevard. The interium Greyhound Terminal is under construction on Richards Boulevard. Moving in a southerly direction, the uses would transition to a lesser intensity with office mixed use and residential mixed uses. Along the southern border of this area is the Railyards development which plans for a primarily residential East End District.

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In the Bannon Street Area, the Specific Plan envisions a 10 acre park wrapping the northern and eastern edge of the City Water Filtration facility. This open space would provide an excellent connection to the Vista Park planned in the Railyards Development.

Staff is eliminating all of the heavy industrial zoning with the exception of the Water Treatment Plant. Under the previous Richards Boulevard Special Planning District, the M-2 zoning was restricted by placing many industrial uses on the prohibited list. In effect, the industrial zoning only allowed less intense commercial and some heavy commercial uses.

North 7<sup>th</sup> Street Area- The North 7<sup>th</sup> Street area includes the 65-acre Township 9 Planned Unit Development (PUD) project site and the Continental Plaza PUD, located between North 5<sup>th</sup> Street and North 10<sup>th</sup> Street, north of Richards Boulevard and to the American River Parkway. The Township 9 PUD was adopted by City Council on August 28, 2007. It includes an intense mix of 2,350 residential units, 840,000 square feet of office, and 146,000 square feet of retail uses. At the southern end of the Township 9 development along Richards Boulevard is a proposed light rail transit station, currently in the development phase as part of Regional Transit's "MOS-1" project. This is the first phase of the extension of light rail transit from Downtown Sacramento, through Natomas, to the Sacramento International Airport. The Continental Plaza PUD was established in 1996 and is currently entitled for approximately 1.1 million square feet of office uses, including the headquarters of the California Highway Patrol. The California State Lottery also has its headquarters located in this area and is constructing a phased expansion for two office buildings totaling 480,000 square feet, an 8,400 square foot retail component, and a 1,189 space parking garage.

At buildout, the North 7<sup>th</sup> Street area is expected to be employment intensive, with a mix of supportive commercial and high-density residential uses. The Specific Plan supports better connections between the area and the American River Parkway, taking advantage of natural views and recreational opportunities.

Staff is not rezoning any parcels associated with the approved T9 project. However, the portions of the area zoned Heavy Industrial (M-2), would be rezoned to both Office Building (OB) for the CHP Campus and Lottery Campus, and also RMX by the frontage along the American River. The Club Fantasy (adult use) will be rezoned to General Commercial (C-2).

Dos Rios Area- The Dos Rios Area is generally bounded by North 10<sup>th</sup> Street on the west, the American River on the north, and North 12<sup>th</sup> Street on the east. It has an eclectic mix of uses and building types. The area is envisioned to transition from light industrial uses to infill a mix of residential and retail commercial. The area provides excellent opportunities for adaptive reuse and start-up businesses. There are abandoned railroad spurs in the area which could allow the development of a bikeway connection along the rails. The Twin Rivers School District has a facility located in this area off of Richards Boulevard.

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Staff is proposing to rezone Heavy Industrial (M-2) to General Commercial (C-2) and Heavy Commercial (C-4); Residential Mixed Use (RMX) to Multifamily (R-5); Heavy Commercial (C-4) to General Commercial (C-2) and Multifamily (R-3A). The rezones will encourage mixed use development around the future light rail station and rezones industrial land along the American River consistent with the 2030 General Plan policies.

16<sup>th</sup> Street Area- The 16<sup>th</sup> Street Area is generally bounded by North 12<sup>th</sup> Street on the west, the American River on the north, and the Union Pacific Railroad Right-of-way to the east. The 16<sup>th</sup> Street Area is characterized by primarily large warehouse and commercial services uses. The area also includes a mini-storage and Downtown Ford, east of North 16<sup>th</sup> Street near the Highway 160 bridge over the American River. The area is anticipated to be an eclectic area that will retain its light industrial uses for some time, while incorporating an additional mix of residential uses through infill projects and industrial conversions.

The 16<sup>th</sup> Street Area also contains the proposed historic district. It is characterized by many buildings of brick masonry construction. The area is occupied by a mix of businesses and social services. The area is adjacent to Blue Diamond and the Globe Mills project at 12<sup>th</sup> & C Streets. The historic district is expected to retain its light industrial nature.

Staff is also recommending to maintain much of the current C-4 zoning in the 16<sup>th</sup> Street area. The C-4 zoning allows many of the heavy commercial users to remain but will also allow office, retail, and residential uses over time to create an eclectic area.

### ***Parking Requirements***

The Richards Boulevard SPD envisioned the intermodal site at 7<sup>th</sup> Street with limited parking on the surrounding transit-oriented office uses. In the OB and RMX zones, the amount of parking required in the Richards Boulevard SPD provides both minimum and maximums depending on the size of the buildings. As an example, office requires between 1/500 to 1/600. With the approval of the Railyards project, the location of the intermodal station has changed. Furthermore, by restricting the amount of maximum parking allowed for office development in the Richards Boulevard SPD below citywide standards, some property owners have argued that it has limited potential users who request more onsite parking.

In the proposed River District SPD, existing buildings would not trigger additional parking with a change of use and instead the amount of parking existing onsite would satisfy the minimum requirements. If not specifically stated in the River District Special Planning District, the other parking requirements would be the same as the Central City parking requirements in the Zoning Code. For example, in the previously mentioned case of the construction of new office, the minimum parking standards will be 1/400.

### ***Height, Yard, and Stepback Requirements***

With the River District SPD, any deviation from the required height, yard, and stepback standards would be reviewed and approved by the Design or Preservation hearing bodies. The Design or Preservation hearing bodies would evaluate the intent and purpose of the River District Design Guidelines, to ensure that an adequate and appropriate street tree canopy is created and maintained, and to mitigate visual impacts on listed historic resources.

### ***Ground Floor Retail Accommodation Requirements***

Currently the Richards Boulevard SPD requires 25% ground floor retail along Richards Boulevard and North 7<sup>th</sup> Street in the Office Building (OB) zone. Ground floor retail and service uses provide activity for a pedestrian friendly environment. With ground floor retail activity there is less likelihood for dead zones with office building development closed after work hours and on weekends.

With the new River District SPD, staff is proposing ground floor retail requirements in only the most potentially heavy pedestrian traffic areas such as the future transit station in the Sequoia area, Bannon Street between North 5<sup>th</sup> and North 10<sup>th</sup> Streets, and in the North 16<sup>th</sup> Street area. For a detailed map, see the Special Planning District (Attachment 10). To avoid rendering a project infeasible by requiring too much retail in the district, the number of blocks subject to the ground floor retail or service requirement has been limited with the new plan and the Zoning Code only requires the ground floor be constructed to "accommodate" the retail use (storefront windows, entrancing facing the street, etc). The Zoning Code does not require retail uses be located on the ground floor in these areas since the market will determine when this will be economically feasible.

### ***Parkway Corridor Overlay Zone***

With the rezoning of properties along the American River, the Parkway Corridor (PC) Overlay requirements will be removed for those properties within the River District Specific Plan boundary. This does not include the areas to the east of 18<sup>th</sup> Street that are outside of the River District Specific Plan area but within the River District Design Guidelines area. These industrial zoned properties, with their PC overlay, will remain unchanged.

The Parkway Corridor Overlay zone is no longer necessary since staff has drafted a height exhibit and design guidelines that provide appropriate standards for development. Furthermore, the American River Parkway document has already recognized that this stretch of river will be more urban with the ability to see buildings from the river.

**Design Guidelines and Design Review District:** The River District Specific Plan project will establish the River District Design Review District that encompasses the Specific Plan area and will adopt the River District Design Guidelines to address building placement, design, setbacks, heights, massing and overhangs, as well as landscape treatments, streetscapes, lighting, signage and the design of public and civic

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open spaces. Please note that the boundaries of the River District Design Guidelines boundaries do not correspond precisely with the Specific Plan boundaries; rather the Design Guidelines also cover the area east of the Specific Plan area, north of the railroad levee, adjacent to Sutter's Landing Regional Park.

The Design Guidelines for the River District articulate the overall vision for the physical form and character of the public and private improvements within the plan area. The Design Guidelines, which were developed based on guiding principles developed from the property owners and stakeholders with a series of workshops, will ensure a quality of design that is consistent with the River District Specific Plan and the larger Central City area.

Guiding Principles for the Design Guidelines included engaging the rivers, encouraging a walkable district, and providing an opportunity to develop mixed use development.

The Design Commission is scheduled to make a final recommendation to the City Council to establish the River District Design Review District and adopt the River District Design Review Guidelines on January 12, 2011. The outcome of the hearing was not available at the time of writing this report. The Law and Legislation Committee will review the ordinance on January 18, 2011 before the final Council date on February 1, 2011.

**Historic Landmarks and N.16<sup>th</sup> Street Historic District (M10-012):** Two historic resources surveys, one in 1999/2000 and an update in 2009, have been conducted in the River District, and recommendations as to individual historic Landmarks and a Historic District – the North 16<sup>th</sup> Street Historic District – were made through those surveys. Note that the boundaries of the North 16<sup>th</sup> Street Historic District extend beyond the River District Specific Plan area.

On September 24, 2009, staff conducted a workshop to discuss the properties identified in the survey as potentially historic and the proposed nominations of the Historic District and the individual Landmarks in the River District area. The nominations process to list properties in the Sacramento Register is proceeding concurrently with the adoption of the River District Specific plan. That process involved the October 13 and 20, 2010, Preservation Director Hearings to consider the nominations of the properties as Landmarks and the North 16<sup>th</sup> Street Historic District/Contributing Resources. The Preservation Commission Hearing on November 3 and December 1, 2010 considered the nominations and forwarded a recommendation of approval on the nominations to the City Council for adopting the ordinance listing the properties in the Sacramento Register as Landmarks and Historic District/Contributing Resources. The Law and Legislation Committee will review the ordinance on January 18, 2011 before the final Council date on February 1, 2011.

**Infrastructure Financing Plan:** The Finance Plan, which accompanies the Specific Plan, estimates costs and identifies anticipated sources of revenue for the development of the infrastructure and public facilities required for development in the River District Specific Plan area. This includes: the street network; local cost share for the freeway interchange improvements; water, sewer and storm water systems;

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community centers, parks, trails and open spaces; and other public facilities. The Finance Plan will be adopted along with the Specific Plan and will be implemented as development occurs.

The Financing Plan for the public facilities and infrastructure required to implement the River District Specific Plan identifies a total of approximately \$323,160,000 dollars in backbone infrastructure costs for the improvements such as storm drainage, sanitary sewer, water, streets, and parks. The goal of the Financing Plan is to prioritize public infrastructure investment to stimulate further economic investment by implementing the following policies: a) Acquire land to implement construction of priority streets and infrastructure improvements for the Specific Plan circulation network; b) Develop detention basins for storm water quality treatment and detention on a shared cost basis to benefit new development; c) Finance the construction and maintenance of infrastructure through state, federal and local sources to include development impact fees, land-secured infrastructure districts and maintenance assessments or taxes; and d) Incentivize development when appropriate through reduced development impact fees, tax increment financing, reimbursement and credit agreements and other sources.

A draft resolution approving the Financing Plan is attached as Attachment 4.

**Rescinding the Discovery Centre PUD:** The Discovery Centre PUD (P97-037) was adopted by City Council on November 5, 1998 (Resolution 98-544) for a four phase project with 990,000 square feet of office and hotel uses. Currently existing in the PUD is a three story office building with the Community Development Department and Police Department. The Greyhound Bus Terminal is currently under construction. The remainder of the PUD area is developed with surface parking lots.

There were two subsequent amendments to the PUD including P01-059 which approved a variance to allow the height of the light poles to increase from 18 to 25 feet; P01-066 was approved to allow a PUD Guidelines Amendment to exceed the maximum parking ratio of 1:500 to 1:277 for Phase I and II of the PUD Schematic Plan and a PUD Schematic Plan Amendment to lower the intensity of a proposed hotel from 224 to 100 rooms.

Staff is recommending to rescind the Discovery Centre PUD because 1) all the parcels in the PUD are currently owned by the City of Sacramento; 2) the development standards in the new Special Planning District would allow greater heights, no ground floor retail requirement (instead concentrating it around light rail stations where it would be more economically feasible), and rezones the land from Office Building (OB) to a combination of Office Building (OB) and General Commercial (C-2) to provide flexibility for future proposed uses if sold and developed by a nonpublic agency user; and 3) the PUD guidelines allow for large floor plate designs which are discouraged in the River District Specific Plan.

**Bikeway Master Plan Amendment:** The 2010 City/County Bikeway Master Plan was developed to serve the recreational and transportation needs of the public. This document was adopted by the City of Sacramento on April 11, 1995. The current Bikeway Master Plan is based on the 1994 Richards Boulevard Area Plan bicycle

network. Modifications to the bikeway map are necessary to incorporate changes in the street network and circulation for the River District Specific Plan. The main changes the plan include the following modifications: a) to align with the new street network; b) to use railroad right of ways for bike boulevards; c) to connect to the river trails; d) to anticipate new bridge connections; and e) to incorporate new east-west connections on the north side of the railroad bridge to the eastern portion of the district. A draft resolution approving the Bicycle Master Plan Amendment is attached as Attachment 6.

**Water Assessment Report:** According to Senate Bill 610, a water supply assessment is required for proposed residential developments with more than 500 units and office developments of more than 250,000 square feet. In addition, SB 221 requires written verification of sufficient water supply before a project is approved. This assessment and written verification is included in the Draft EIR, which concluded the City of Sacramento has sufficient water allocations available to serve the proposed Project, as well as the projected future growth. A draft resolution approving the Water Supply Assessment is attached as Attachment 8.

**Policy Considerations:** The Specific Plan will contain a comprehensive set of goals and policies to achieve the vision and guiding principles of the Plan. The policies will be consistent with the 2030 General Plan as well as with other guiding policy documents.

### **General Plan Amendments**

There are five specific areas where staff proposes to change the General Plan designation for consistency purposes. See Attachment 3 for additional information.

- 1) The area on the north of Richards Boulevard between North 5<sup>th</sup> and North 7<sup>th</sup> and to the south of Signature Street. The current General Plan designation is Urban Center Low. The new General Plan designation is proposed to be Urban Center High. The current zone is OB-PUD SPD and there is no change to the zoning. However, the PUD allows for heights up to 15 stories which is more consistent with the Urban Center High designation which allows up to 24 stories whereas the Urban Center Low designation generally allows only up to 7 stories.
- 2) The area south of Vine Street between North 10<sup>th</sup> Street and Dos Rios Street and to the north of D Street. The current General Plan is Urban Center Low. The new General Plan designation is proposed to be Employment Center Low Rise. The current zoning is Heavy Industrial (M-2 N SPD) and the proposed zoning is C-4 SPD which are both not consistent with the current designation however, the amendments will provide consistency.
- 3) The area generally to the east of North 12<sup>th</sup> Street, north of B Street, west of 18<sup>th</sup> Street, and south of Sproule Avenue. The current General Plan is Traditional Center and Traditional Medium Density Residential. The new General Plan designation is proposed to be Employment Center Low Rise. The current zoning in the area is Heavy Commercial (C-4) and no rezones

are planned. The current General Plan designation is not consistent with the zoning so the amendments will provide consistency.

- 4) The area east of the Sacramento River, south of the American River, to the west of Bercut, and north of the PG&E building. The current General Plan is Urban Center Low. The new General Plan designation is proposed to be Urban Center High. The current zoning in the area is Highway Commercial (HC-SPD) and the proposed zoning is General Commercial (C-2 SPD). With the proposed heights planned along the Sacramento River, the Urban Center High designation is more appropriate which generally allows up to 24 stories whereas the Urban Center Low generally allows up to 7 stories. The River District Specific Plan and Design Guidelines would generally allow up to 200-300 feet.
- 5) The area to the south of the American River, west of 18<sup>th</sup> Street, east of Louise Street, and north of Sproule Avenue which makes up the remainder of the Traditional Center designation in the River District. The new General Plan designation is proposed to be Urban Center Low to be consistent with the surrounding parcels in the northern part of the River District which abuts the American River. The current zoning is Heavy Commercial (C-4) and the proposed zoning is a mixture of General Commercial (C-2) and Multifamily (R-3A). Due to the proximity of the future light rail station, the Special Planning District allows up to 100 residential units per acre with the option to apply for a Planning Commission Special Permit to exceed this standard. The Traditional Center designation generally allows up to 36 dwelling units per net acre and the proposed amendment to Urban Center Low would allow up to 110 dwelling units per net acre which is more consistent with the vision of the area.

### **2030 General Plan Policies**

***LU 1.1.5 Infill Development.*** *The City shall promote and provide incentives (e.g., focused infill planning, zoning/rezoning, revised regulations, provision of infrastructure) for infill development, redevelopment, mining reuse, and growth in existing urbanized areas to enhance community character, optimize City investments in infrastructure and community facilities, support increased transit use, promote pedestrian- and bicycle-friendly neighborhoods, increase housing diversity, ensure integrity of historic districts, and enhance retail viability.*

Staff finds that the city is promoting infill development by completing General Plan amendments, rezones, revising regulations, and planning needed infrastructure for the growth in an existing, underutilized industrial area. The new plan increases the opportunity for new housing and creates a walkable neighborhood to encourage the use of the planned RT green line. Parking along Richards Blvd and 16<sup>th</sup> Street will increase retail viability and the plan also designates a historic district and individual landmarks to protect cultural resources in the area.

**LU 2.1.2 Protect Established Neighborhoods.** *The City shall preserve, protect, and enhance established neighborhoods by providing sensitive transitions between these neighborhoods and adjoining areas, and requiring new development, both private and public, to respect and respond to those existing physical characteristics, buildings, streetscapes, open spaces, and urban form that contribute to the overall character and livability of the neighborhood.*

Staff finds that the plan will preserve and enhance the existing Dreher-Basler neighborhood by rezoning adjacent areas to a multifamily residential zone and planning parks within walking distance of existing residential uses. The plan also provides a prominent river walk road in the area of the Dos Rios housing to enhance this community as it is redeveloped by SHRA in the future.

**LU 2.2.1 World-Class Rivers.** *The City shall encourage development throughout the city to feature (e.g., access, building orientation, design) the Sacramento and American Rivers and shall develop a world-class system of riverfront parks and open spaces that provide a destination for visitors and respite from the urban setting for residents.*

Staff finds the River District Specific Plan will increase public access to the American and Sacramento rivers and will guide future development in a manner that makes these exceptional resources available for the enjoyment of Sacramento's residents and visitors alike.

**LU 2.3.1 Multi-functional Green Infrastructure.** *The City shall strive to create a comprehensive and integrated system of parks, open space, and urban forests that frames and complements the city's urbanized areas.*

Staff finds the River District Plan provides a vision of specific parcels that could be used as open space in the future which would link the River District open space to Vista Park in the Railyards and Sutter's Landing. The proposed open space parcels also capitalize on existing resources in the River District such as Tiscornia Park and the Two Rivers Trail.

**LU 2.4.1 Unique Sense of Place.** *The City shall promote quality site, architectural and landscape design that incorporates those qualities and characteristics that make Sacramento desirable and memorable including: walkable blocks, distinctive parks and open spaces, tree-lined streets, and varied architectural styles.*

Staff finds the Design Guidelines encourage an eclectic character for the area, highlight the importance of celebrating connections to the river, and promote the adaptive reuse of existing buildings when possible which will foster a unique sense of place for the River District.

**LU 2.6.1 Sustainable Development Patterns.** *The City shall promote compact development patterns, mixed use, and higher-development intensities that use land efficiently; reduce pollution and automobile dependence and the expenditure of energy and other resources; and facilitate walking, bicycling, and transit use.*

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Staff finds the River District Specific Plan encourages compact development patterns by allowing for additional building height, higher density projects, a greater mix of land uses, and infrastructure to support all modes of travel.

***LU 2.7.1 Development Regulations.*** *The City shall promote design excellence by ensuring city development regulations clearly express intended rather than prohibited outcomes and reinforce rather than inhibit quality design.*

Staff finds that the River District Specific Plan and Design Review Guidelines provide a balanced regulatory framework because the documents share a vision for the future possibilities of the district while providing overall guiding principles and the rationale behind the minimum development standards.

***LU 2.7.6 Walkable Blocks.*** *The City shall require new development and redevelopment projects to create walkable, pedestrian-scaled blocks, publicly accessible mid-block and alley pedestrian routes where appropriate, and sidewalks scaled for the anticipated pedestrian use.*

Staff finds that the Design Guidelines provide detailed street sections to ensure circulation that is friendly for pedestrians, bicyclists, and the motoring public. Possible alley locations have also been incorporated into the plan where it was deemed to be the most feasible and desirable.

***LU 2.7.8 Screening of Off-street Parking.*** *The City shall reduce the visual prominence of parking within the public realm by requiring most off-street parking to be located behind or within structures or otherwise fully or partially screened from public view.*

Staff finds the Design Guidelines encourage parking to be screened and the Special Planning District provides parking allowances for changes of use to existing buildings to minimize the amount of new parking developed onsite.

***LU 4.1.4 Alley Access.*** *The City shall encourage the use of well-designed and safe alleys to access individual parcels in neighborhoods in order to reduce the number of curb cuts, driveways, garage doors, and associated pedestrian/automobile conflicts along street frontages.*

Staff finds the Specific Plan and Design Guidelines incorporate alley locations into the circulation network to reduce curb cuts and enhance the pedestrian experience along the street frontages.

***LU 5.5.2 Transit-Oriented Development.*** *The City shall actively support and facilitate mixed-use retail, employment, and residential development around existing and future transit stations.*

Staff finds the plan is transit-oriented because the Special Planning District prohibits self-service Laundromats, hardware stores, and appliance repair stores in the C-1 zone which is directly next to the transit stations. The updated plan encourages retail, office, and residential to provide active, transit supportive uses at light rail stations.

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***LU 7.2.3 Industrial Uses along Rivers.*** *The City shall prohibit new heavy industrial uses along the American River Parkway and prevent incompatible industrial development adjacent to the American and Sacramento Rivers.*

Staff finds that the zoning of the land along the rivers is proposed to change from Highway Commercial (HC), Heavy Commercial (C-4), and Heavy Industrial (M-2) to Residential Mixed Use (RMX) and General Commercial (C-2). This zoning change will encourage existing industrial uses to transition and new development to construct compatible uses.

***LU 7.2.5 Industrial Development Design.*** *The City shall require that new and renovated industrial properties and structures incorporate high-quality design and maintenance including . . . control of on-site lighting, noise, odors, vibrations, toxic materials, truck access, and other factors that may impact adjoining nonindustrial land uses.*

Staff finds that the establishment of the River District Design Review District will allow for the review of all exterior work that requires a building permit and new construction to ensure minimum design standards are enforced.

***LU 9.1.3 Connected Open Space System.*** *The City shall ensure that new development does not create barriers to the connections among the various parts of the city's parks and open space systems.*

Staff finds the Design Guidelines require a public connection from the street to the river for properties that abut the Sacramento or American Rivers. This will ensure that development does not create a barrier for the public to gain access to the rivers.

***HCR 2.1.5 National, California, and Sacramento Registers.*** *The City shall pursue eligibility and listing for qualified resources including historic districts and individual resources under the appropriate register(s).*

Staff finds the River District Update includes the creation of the North 16<sup>th</sup> Street Historic District and the listing of individual landmarks.

***HCR 2.1.13 Adaptive Reuse.*** *The City shall encourage the adaptive reuse of historic resources when the original use of the resource is no longer feasible.*

Staff finds the Special Planning District allows a wide range of uses in the North 16<sup>th</sup> Street Historic District which is zoned Heavy Commercial (C-4). The zoning allows uses including residential, office, retail, and warehouse.

***M 2.1.3 Streetscape Design.*** *The City shall require that pedestrian-oriented streets be designed to provide a pleasant environment for walking including shade trees; plantings; well-designed benches, trash receptacles, news racks, and other furniture; pedestrian-scaled lighting fixtures; wayfinding signage; integrated transit shelters; public art; and other amenities.*

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Staff finds that the Design Guidelines provide specific details on the streetscape design to provide a pedestrian friendly environment and encourage pedestrian activity.

***M 4.1.1 Emergency Access.*** *The City shall develop a roadway system that is redundant (i.e., includes multiple alternative routes) to the extent feasible to ensure mobility in the event of emergencies.*

Staff finds that the proposed circulation network connects the area to downtown and the Railyards by extending the grid. These additional connections provide better access in and out of the district.

***M 4.2.1 Adequate Rights-of-Way.*** *The City shall ensure that all new roadway projects and major reconstruction projects provide appropriate and adequate rights-of-way for all users including bicyclists, pedestrians, transit riders, and motorists except where pedestrians and bicyclists are prohibited by law from using a given facility.*

Staff finds that the right of way needed has been incorporated into the proposed circulation network. As an example, right of way for a future connection of the two light rail lines has been reserved in the Special Planning District along the North side of Richards between North 7<sup>th</sup> and North 16<sup>th</sup> Streets.

***M 5.1.2 Appropriate Bikeway Facilities.*** *The City shall provide bikeway facilities that are appropriate to the street classifications and type, traffic volume, and speed on all right-of-ways.*

Staff finds the Bikeway Master Plan is being updated as part of the River District Specific Plan Effort to provide appropriate bikeway facilities.

***M 6.1.4 Reduction of Parking Areas.*** *The City shall strive to reduce the amount of land devoted to parking through such measures as development of parking structures, the application of shared parking for mixed use developments, and the implementation of Transportation Demand Management plans to reduce parking needs.*

Staff finds the Special Planning District allows no additional parking when a change of use occurs to an existing building. This will help to minimize the amount of new parking to be developed within the district and will encourage alternative modes of travel.

***U 1.1.7 Infrastructure Finance.*** *The City shall develop and implement a financing strategy and assess fees to construct needed water, wastewater, stormwater drainage, and solid waste facilities to maintain established service levels and to mitigate development impacts to these systems (e.g., pay capital costs associated with existing infrastructure that has inadequate capacity to serve new development). The City shall also assist developers in identifying funding mechanisms to cover the cost of providing utility services in infill areas.*

Staff finds the River District Update includes a new Finance Plan to ensure adequate funds are available for infrastructure improvements in the district.

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***U 2.1.5 Comprehensive Water Supply Plans.*** *The City shall prepare, implement, and maintain long-term, comprehensive water supply plans.*

Staff finds a Water Supply Assessment has been completed as a part of this planning effort to evaluate projected water supplies.

***ER 7.1.1 Protect Scenic Views.*** *The City shall seek to protect views from public places to the Sacramento and American rivers and adjacent greenways, landmarks, and urban views of the downtown skyline and the State Capitol along Capitol Mall.*

Staff finds the Design Guidelines provide minimum standards for the distance between towers to protect views to the Sacramento and American Rivers.

***EC 2.1.7 Levee Setbacks for New Development.*** *The City shall prohibit new development within a minimum distance of 50 feet of the landside toe of levees. Development may encroach within the 50-foot area provided that “oversized” levee improvements are made to the standard levee section consistent with local, regional, State, and Federal standards.*

Staff finds that the development standards and policies of the River District Specific Plan, Design Review Guidelines, and Special Planning District do not conflict with this minimum setback requirement.

### **Central City Community Plan Policies**

***CC.LU 1.1 Industrial Areas.*** *The City shall upgrade the industrial-designated areas of the Central City and minimize incompatibilities with adjacent land uses.*

Staff finds the Design Guidelines work with the Specific Plan, Special Planning District, Finance Plan, and Historic District to advance the River District transformation from an existing industrial area into a transit-supportive mixed use urban environment.

***CC.LU 1.6 Office Development.*** *The City shall encourage public and private office development, where compatible with the adjacent land uses and circulation system, in the Central Business District, Southern Pacific Railyards, and Richards Boulevard area.*

Staff finds that parcels along Richards Boulevard and North 7<sup>th</sup> Street are proposed to be rezoned to Office Building (OB) to encourage the development of more office in the district.

***CC.H 1.1 Mixed-Use Buildings.*** *The City shall provide the opportunity for mixture of housing with other uses in the same building or on the same site at selected locations to capitalize on the advantages of close-in living.*

Staff finds the proposed zoning designations and Special Planning District allow and encourage mixed use development.

### **2008-2013 Housing Element:**

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**H-1.2.4** *The City shall actively support and encourage mixed-use retail, employment and residential development around existing and future transit stations, centers and corridors.*

Staff finds the rezones from industrial to residential and commercial in the River District around the transit stations will promote higher density and mixed use development.

#### **Sacramento Riverfront Master Plan:**

The Sacramento Riverfront Master Plan is a study plan, not a regulatory plan that was completed in July 2003. It provides an overall vision for the riverfront and is intended as a blueprint for future actions. Proposed policies include:

- Site housing and other adjacent mixed uses to capture maximum orientation to the river and to the riverfront open space, as well as to parkways and streets.
- Provide continuous, uninterrupted pedestrian and bicycle circulation along the riverfront, connecting to regional networks including the American River Parkway and into Southport.
- Provide new non-vehicular bridge crossings designed with public safety considerations. The proposed pedestrian and bicycle bridge would connect the Jibboom Area of the River District to the proposed marina and state park on the West Sacramento side.
- Provide people-oriented land uses, public space, and amenities that attract people and activity.
- Provide for land uses that are flexible and can respond to market conditions and/or public/private financing opportunities (avoid single-use “dead-zones”).
- Vary development densities, intensities, and mix of uses along the riverfront edge.

Staff finds the River District Specific Plan is consistent with the policies in the Sacramento Riverfront Master Plan.

#### **American River Parkway Plan:**

The *2008 American River Parkway Plan* is the local guiding policy document for activities along this portion of the river. The American River is classified as both a State and Federal Wild and Scenic River, a river classification system that was created in 1968 to preserve certain rivers with outstanding natural, cultural and recreational values in a free-flowing condition for the enjoyment of present and future generations. The section of the American River that borders the River District is classified as a Recreational River within the classification and recognizes its urban edge.

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*10.4.1: Construct the Two Rivers Trail to a Class 1 construction standard bike/pedestrian trail along the left bank (south levee) of the American River from Tiscornia Park to Sutter's Landing Park.*

*10.4.3: Support construction of a trail from Tiscornia Park to West Sacramento including a bike/pedestrian bridge across the Sacramento River.*

*10.4.4: Bike/pedestrian access shall be incorporated into future bridge construction or renovation projects affecting Interstate 5, Highway 160, and Regional Transit's Downtown-Natomas Airport (DNA-RT) line.*

Staff finds the River District Specific Plan project does not conflict with any of the above stated goals.

### **Alkali Flat/Mansion Flats Strategic Neighborhood Action Plan (SNAP):**

This document was accepted by City Council on August 23, 2005. Although this area is outside of the River District Specific Plan Area, staff included the goals of the SNAP into our analysis to ensure consistency with the goals of the River District implementation. Goal 3.3 in the SNAP was to improve parking on 12<sup>th</sup> Street. Other issues included preservation of older buildings and need for more retail in the area. Staff has addressed these issues in the River District area by creating the North 16<sup>th</sup> Street Historic District to promote the retention and rehabilitation of older buildings and incorporating street parking into street sections where feasible to increase the amount of parking for the neighborhood while also increasing the viability of retail in the area. Staff believes these efforts will have an overall positive effect on the Alkali Flat/Mansion Flats area.

### **16<sup>th</sup> Street Design Study:**

This document was completed in 1997 however, the area north of B Street was outside of the study area. The plan included concepts and strategies to enhance the overall image of 16<sup>th</sup> Street between W and B Streets and balance efforts to accommodate future traffic volumes and enhance pedestrian safety and comfort. The report recommended screening of parking, consolidating curb cuts, and infill of canopy trees. Staff has reviewed this document in our preparation of the River District Specific Plan to complement the treatment of 16<sup>th</sup> Street to the north of B Street.

**Central City Parking Master Plan:** The Central City Parking Master Plan was adopted by the City Council on August 2, 2005 (Resolution 2005-587). The River District area is located inside of the Central City and the policies of the Specific Plan and Special Planning District are consistent with the goals and objectives of the Parking Master Plan which includes managing parking supply efficiently and minimizing the negative impacts of parking.

### **Conclusion:**

Staff recommends the Commission forward to City Council a recommendation of approval for the River District Project as shown in Attachment 1.

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Respectfully submitted by:   
EVAN COMPTON  
Associate Planner

Approved by:

  
STACIA COSGROVE  
Senior Planner

Recommendation Approved:

  
GREG BITTER, AICP  
Principal Planner

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Attachments:

Attachment 1	Overview of all entitlements	Page 31
Attachment 2	EIR	Page 33
Exhibit A	Statement of Overriding Considerations	Page 35
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Attachment 3	General Plan Land Use Diagram Amendments	Page 106
Attachment 4	New River District Specific Plan and Finance Plan	Page 114
Attachment 5	Amendment to Operation of 5 <sup>th</sup> and 7 <sup>th</sup> Streets	Page 118
Attachment 6	Bikeway Master Plan Amendments	Page 125
Attachment 7	River District Design Review Guidelines	Page 128
Attachment 8	Water Supply Assessment	Page 131
Attachment 9	Revocation of Discovery Centre PUD	Page 136
Attachment 10	River District Special Planning District	Page 137
Attachment 11	River District Rezones	Page 165

**Proposed Findings of Fact and Conditions of Approval for the River District Specific Plan (M09-003). Generally bounded by Downtown and the Railyards on the south, the Sacramento River on the west, the American River on the north, and 16th and 18th Streets on the east.**

**Findings Of Fact**

**A.&B.** The Planning Commission has reviewed and considered the information in the Environmental Impact Report for the Project in making the recommendations set forth below.

**C.** The Planning Commission recommends approval and forwards to the City Council the General Plan Amendments for multiple properties for the Project as set forth in Attachment 3;

**D.** The Planning Commission recommends approval and forwards to the City Council the revocation of the 1994 Richards Boulevard Area Plan and Facility Element, the General Plan Amendments of the Circulation Element, and Adoption of the River District Specific Plan and Public Financing Facility Element for the Project as set forth in Attachment 4;

**E.** The Planning Commission recommends approval and forwards to the City Council the Amendment to the Railyards Specific Plan for operation of 5<sup>th</sup> and 7<sup>th</sup> Streets for the Project as set forth in Attachment 5;

**F.** The Planning Commission recommends approval and forwards to the City Council the Bikeway Master Plan Amendments to incorporate the River District Specific Plan Bicycle Network for the Project as set forth in Attachment 6;

**G.** The Planning Commission recommends approval and forwards to the City Council the River District Design Review Guidelines for the Project as set forth in Attachment 7;

**H.** The Planning Commission recommends approval and forwards to the City Council the Water Assessment Report for the Project as set forth in Attachment 8;

**I.** The Planning Commission recommends approval and forwards to the City Council the revocation of the Discovery Centre Planned Unit Development Guidelines for the Project as set forth in Attachment 9;

**J.** The Planning Commission recommends approval and forwards to the City Council the adoption of the River District Special Planning District for the Project as set forth in Attachment 10;

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- K.** The Planning Commission recommends approval and forwards to the City Council the rezones for the Project as set forth in Attachment 11.

**[EIR – Certification Findings - City Council Resolution]****RESOLUTION NO. 2011-**

Adopted by the Sacramento City Council

**CERTIFYING THE ENVIRONMENTAL IMPACT REPORT  
AND ADOPTING THE MITIGATION MONITORING PROGRAM FOR THE  
RIVER DISTRICT SPECIFIC PLAN PROJECT (M09-003 and M10-012)**

**BACKGROUND**

- A. On January 13, 2011 the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve with conditions, the River District Specific Plan Project.
- B. On February 1, 2011, the City Council conducted a public hearing , for which notice was given pursuant to Sacramento City Code Section 17.200.010 (C)(2)(a, b, and c)(publication, posting, and mailing (500 feet) and received and considered evidence concerning the River District Specific Plan Project.

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL  
RESOLVES AS FOLLOWS:**

Section 1. The City Council finds that the Environmental Impact Report for River District Specific Plan (herein EIR) which consists of the Draft EIR and the Final EIR (Response to Comments) (collectively the “EIR”) has been completed in accordance with the requirements of the California Environmental Quality Act (CEQA), the State CEQA Guidelines and the Sacramento Local Environmental Procedures.

Section 2. The City Council certifies that the EIR was prepared, published, circulated and reviewed in accordance with the requirements of CEQA, the State CEQA Guidelines and the Sacramento Local Environmental Procedures, and constitutes an adequate, accurate, objective and complete Final Environmental Impact Report in full compliance with the requirements of CEQA, the State CEQA Guidelines and the Sacramento Local Environmental Procedures.

Section 3. The City Council certifies that the EIR has been presented to it, that the Planning Commission has reviewed the EIR and has considered the information contained in the EIR prior to acting on the proposed Project, and that the EIR reflects the Planning Commission’s independent judgment and analysis.

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Section 4. Pursuant to CEQA Guidelines Sections 15091 and 15093, and in support of its approval of the Project, the City Council adopts the attached Findings of Fact and Statement of Overriding Considerations in support of approval of the Project as set forth in the attached Exhibit A of this Resolution.

Section 5. Pursuant to CEQA section 21081.6 and CEQA Guidelines section 15091, and in support of its approval of the Project, the City Council adopts the Mitigation Monitoring Program to require all reasonably feasible mitigation measures be implemented by means of Project conditions, agreements, or other measures, as set forth in the Mitigation Monitoring Program as set forth in Exhibit B of this Resolution.

Section 6. The City Council directs that, upon approval of the Project, the City's Community Development Department shall file a notice of determination with the County Clerk of Sacramento County and, if the Project requires a discretionary approval from any state agency, with the State Office of Planning and Research, pursuant to the provisions of CEQA section 21152.

Section 7. Pursuant to CEQA Guidelines section 15091(e), the documents and other materials that constitute the record of proceedings upon which the City Council has based its decision are located in, and may be obtained from, the Office of the City Clerk at 915 I Street, Sacramento, California. The City Clerk is the custodian of records for all matters before the City Council.

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Exhibit A – CEQA Findings of Fact and Statement of Overriding Considerations

Exhibit B – Mitigation Monitoring Program

**Exhibit A: CEQA Findings of Fact and Statement of Overriding Considerations****Description of the Project**

Currently, the River District area is a mix of underutilized and underdeveloped parcels, large parcels, and parcels with incompatible adjacent land uses, encompassing approximately 748 acres of land. The proposed River District Specific Plan project (RDSP) (Specific Plan) would establish planning and development standards for the redevelopment of the area. The goal of the proposed project is to master plan the district as a transit-oriented, urban neighborhood that supports a mix of uses with parcels ready for development. To meet this goal, the RDSP would lay the policy and implementation framework for the evolution of the Plan area from a primarily light-industrial, low-intensity district, to a cohesive district with a mix of residential, commercial, industrial, public, and open space uses. The Specific Plan would provide the general vision and broad policy concepts to guide development of a new neighborhood.

The RDSP is consistent with the City's 2030 General Plan and provides area-specific development policies that address the unique aspects of the River District. The proposed RDSP is a long range policy and planning document that is intended to guide development in the Specific Plan area over the next 25 years. The Specific Plan would serve to guide future decisions regarding land use, intensity of development, circulation, public spaces, urban design, and the necessary infrastructure improvements to support future development. Finally, the Plan would identify the resources necessary to finance and implement the public improvements and infrastructure needed to support the vision for the new Specific Plan area.

This project would also provide the backbone infrastructure necessary for development of individual parcels in accordance with the Specific Plan. No parcels would be developed as part of this Proposed Project. Instead the individual parcel owners would develop their parcels in accordance with the Specific Plan.

**Findings Required Under CEQA****1. Procedural Findings**

The City Council of the City of Sacramento finds as follows:

The City of Sacramento's Environmental Planning Services determined that the River District Specific Plan Project (hereinafter called "Project") may have a significant effect on the environment and prepared an environmental impact report ("EIR") on the Project, River District Specific Plan EIR (SCH 2009062023). The EIR was prepared, noticed, published, circulated, reviewed, and completed in full compliance with the California Environmental Quality Act (Public Resources Code §21000 *et seq.* ("CEQA"), the CEQA Guidelines (14 California Code of Regulations §15000 *et seq.*), and the City of Sacramento environmental guidelines, as follows:

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a. A Notice of Preparation of the Draft EIR was filed with the Office of Planning and Research and each responsible and trustee agency and was circulated for public comments from June 2, 2009 through July 2, 2009.

b. A Notice of Completion (NOC) and copies of the Draft EIR were distributed to the Office of Planning and Research on July 27, 2010 to those public agencies that have jurisdiction by law with respect to the Project, or which exercise authority over resources that may be affected by the Project, and to other interested parties and agencies as required by law. The comments of such persons and agencies were sought.

c. An official 45-day public comment period for the Draft EIR was established by the Office of Planning and Research. The public comment period began on July 27, 2010 and ended on September 9, 2010.

d. A Notice of Availability (NOA) of the Draft EIR was mailed to all interested groups, organizations, and individuals who had previously requested notice in writing on July 23, 2010. The NOA stated that the City of Sacramento had completed the Draft EIR and that copies were available at the City of Sacramento, 300 Richards Boulevard, Third Floor, Sacramento, CA. The letter also indicated that the official 45-day public review period for the Draft EIR would end on September 9, 2010.

e. A public notice was placed in the Sacramento Bulletin on July 27, 2010 which stated that the Draft EIR was available for public review and comment.

f. A public notice was posted in the office of the Sacramento County Clerk on July 23, 2010.

g. Following closure of the public comment period, all comments received on the Draft EIR during the comment period, the City's written responses to the significant environmental points raised in those comments, and additional information added by the City were added to the Draft EIR to produce the Final EIR.

## **2. Record of Proceedings**

The following information is incorporated by reference and made part of the record supporting these findings:

- a. The Draft and Final EIR and all documents relied upon or incorporated by reference;
- b. The City of Sacramento 2030 General Plan adopted March 3, 2009, and all updates;
- c. The Master Environmental Impact Report for the City of Sacramento 2030 General Plan certified on March 3, 2009, and all updates;

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- d. Findings of Fact and Statement of Overriding Considerations for the Adoption of the Sacramento 2030 General Plan adopted March 3, 2009, and all updates;
- e. Zoning Ordinance of the City of Sacramento;
- f. Blueprint Preferred Scenario for 2050, Sacramento Area Council of Governments, December, 2004;
- g. Richards Boulevard Area Plan;
- h. River District Specific Plan and Design Guidelines;
- i. Application materials, including application information;
- j. The Mitigation Monitoring Program for the Project; and
- k. All records of decision, staff reports, memoranda, maps, exhibits, letters, synopses of meetings, and other documents approved, reviewed, relied upon, or prepared by any City commissions.

### 3. Findings

CEQA requires that the lead agency adopt mitigation measures or alternatives, where feasible, to substantially lessen or avoid significant environment impacts that would otherwise occur. Mitigation measures or alternatives are not required, however, where such changes are infeasible or where the responsibility for the project lies with some other agency. (CEQA Guidelines, § 15091, sub. (a)(b).)

With respect to a project for which significant impacts are not avoided or substantially lessened, a public agency, after adopting proper findings, may nevertheless approve the project if the agency first adopts a statement of overriding considerations setting forth the specific reasons why the agency found that the project's "benefits" rendered "acceptable" its "unavoidable adverse environmental effects." (CEQA Guidelines, §§ 15093, 15043, sub. (b); see also Pub. Resources Code, § 21081, sub. (b))

In seeking to effectuate the substantive policy of CEQA to substantially lessen or avoid significant environmental effects to the extent feasible, an agency, in adopting findings, need not necessarily address the feasibility of *both* mitigation measures and environmentally superior alternatives when contemplating approval of a proposed project with significant impacts. Where a significant impact can be mitigated to an "acceptable" level solely by the adoption of feasible mitigation measures, the agency, in drafting its findings, has no obligation to consider the feasibility of any environmentally superior alternative that could also substantially lessen or avoid that same impact — even if the alternative would render the impact less severe than would the proposed project as mitigated. (*Laurel Hills Homeowners Association v. City Council* (1978) 83 Cal.App.3d 515, 521; see also *Kings County Farm Bureau v. City of Hanford* (1990) 221

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Cal.App.3d 692, 730-731; and *Laurel Heights Improvement Association v. Regents of the University of California* (“*Laurel Heights I*”) (1988) 47 Cal.3d 376, 400-403.)

In these Findings, the City first addresses the extent to which each significant environmental effect can be substantially lessened or avoided through the adoption of feasible mitigation measures. Only after determining that, even with the adoption of all feasible mitigation measures, an effect is significant and unavoidable does the City address the extent to which alternatives described in the EIR are (i) environmentally superior with respect to that effect and (ii) “feasible” within the meaning of CEQA.

In cases in which a project’s significant effects cannot be mitigated or avoided, an agency, after adopting proper findings, may nevertheless approve the project if it first adopts a statement of overriding considerations setting forth the specific reasons why the agency found that the “benefits of the project outweigh the significant effects on the environment.” (Public Resources Code, Section 21081, sub. (b); see *also*, CEQA Guidelines, Sections 15093, 15043, sub.(b).) In the Statement of Overriding Considerations found at the end of these Findings, the City identifies the specific economic, social, and other considerations that, in its judgment, outweigh the significant environmental effects that the Project will cause.

The California Supreme Court has stated that “[t]he wisdom of approving ... any development project, a delicate task which requires a balancing of interests, is necessarily left to the sound discretion of the local officials and their constituents who are responsible for such decisions. The law as we interpret and apply it simply requires that those decisions be informed, and therefore balanced.” (*Goleta II* (1990) 52 Cal.3d 553 at 576.)

In support of its approval of the Project, the Planning Commission makes the following findings for each of the significant environmental effects and alternatives of the Project identified in the EIR pursuant to Section 21080 of CEQA and section 15091 of the CEQA Guidelines:

**A. Significant or Potentially Significant Impacts Mitigated to a Less Than Significant Level.**

The following significant and potentially significant environmental impacts of the Project, including cumulative impacts, are being mitigated to a less than significant level and are set out below. Pursuant to section 21081(a)(1) of CEQA and section 15091(a)(1) of the CEQA Guidelines, as to each such impact, the Planning Commission, based on the evidence in the record before it, finds that changes or alterations incorporated into the Project by means of conditions or otherwise, mitigate, avoid or substantially lessen to a level of insignificance these significant or potentially significant environmental impacts of the Project. The basis for the finding for each identified impact is set forth below.

### Air Quality

**Impact 5.1-1:** Construction activities within the RDSP area could result in NO<sub>x</sub> levels above 85 pounds per day. Without mitigation, this is a *potentially significant* impact.

Many different types of construction equipment would be used in various combinations for the many individual development projects that are expected to occur in the RDSP area. Much of this equipment likely would be diesel-fueled and would emit NO<sub>x</sub> as part of the fuel-combustion process. The amount of NO<sub>x</sub> emitted per day at any individual development project site would depend on the number and type of equipment used; specifically the total daily average construction NO<sub>x</sub> for the entire RDSP area would depend on the number and intensity of concurrent individual development projects. Specific information on the construction schedules and equipment use by every development project that would be built in the RDSP area is currently not available. For this reason the impact is considered ***potentially significant***.

Mitigation Measure (From MMP): The following mitigation measure has been adopted to address this impact:

*MM 5.1-1(a) The following shall be incorporated into all City construction contracts and included on all construction plans.*

- *Water all exposed surfaces two times daily. Exposed surfaces include, but are not limited to soil piles, graded areas, unpaved parking areas, staging areas, and access roads.*
- *Cover or maintain at least two feet of free board space on haul trucks transporting soil, sand, or other loose material on the site. Any haul trucks that would be traveling along freeways or major roadways should be covered.*
- *Use wet power vacuum street sweepers to remove any visible trackout mud or dirt onto adjacent public roads at least once a day. Use of dry power sweeping is prohibited.*
- *Limit vehicle speeds on unpaved roads to 15 miles per hour (mph).*
- *All roadways, driveways, sidewalks, parking lots to be paved should be completed as soon as possible. In addition, building pads should be laid as soon as possible after grading unless seeding or soil binders are used.*
- *Minimize idling time either by shutting equipment off when not in use or reducing the time of idling to 5 minutes (as required by the state airborne toxics control measure [Title 13, Section 2485 of the California Code of Regulations]). Provide clear signage that posts this requirement for workers at the entrances to the site.*

- *Maintain all construction equipment in proper working condition according to manufacturer's specifications. The equipment must be checked by a certified mechanic and determine to be running in proper condition before it is operated.*

*5.1-1(b) The following shall be incorporated into all construction plans for projects that estimated construction related NO<sub>x</sub> emissions exceed 85 lbs/day.*

*Category 1: Reducing NO<sub>x</sub> emissions from off-road diesel powered equipment*

*The project shall provide a plan, for approval by the lead agency and SMAQMD, demonstrating that the heavy-duty (> 50 horsepower) self-propelled off-road vehicles to be used in the construction project, including owned, leased and subcontractor vehicles, will achieve a project wide fleet-average 20 percent NO<sub>x</sub> reduction and 45 percent particulate reduction<sup>1</sup> compared to the most recent CARB fleet average at time of construction.*

*and*

*The project representative shall submit to the lead agency and SMAQMD a comprehensive inventory of all off-road construction equipment, equal to or greater than 50 horsepower, that will be used an aggregate of 40 or more hours during any portion of the construction project. The inventory shall include the horsepower rating, engine production year, and projected hours of use for each piece of equipment. The inventory shall be updated and submitted monthly throughout the duration of the project, except that an inventory shall not be required for any 30-day period in which no construction activity occurs. At least 48 hours prior to the use of subject heavy-duty off-road equipment, the project representative shall provide SMAQMD with the anticipated construction timeline including start date, and name and phone number of the project manager and on-site foreman.*

*5.1-1(c) The following shall be incorporated into all construction plans for projects that estimated construction related NO<sub>x</sub> emissions exceed 85 lbs/day.*

*Category 2: Controlling visible emissions from off-road diesel powered equipment.*

*The project shall ensure that emissions from all off-road diesel powered equipment used on the project site do not exceed 40 percent opacity for more than three minutes in any one hour. Any equipment found to exceed 40 percent opacity (or Ringelmann 2.0) shall be*

*repaired immediately, and the lead agency and SMAQMD shall be notified within 48 hours of identification of non-compliant equipment. A visual survey of all in-operation equipment shall be made at least weekly, and a monthly summary of the visual survey results shall be submitted throughout the duration of the project, except that the monthly summary shall not be required for any 30-day period in which no construction activity occurs. The monthly summary shall include the quantity and type of vehicles surveyed as well as the dates of each survey. The SMAQMD and/or other officials may conduct periodic site inspections to determine compliance. Nothing in this section shall supercede other SMAQMD or state rules or regulations.*

*and/or:*

*If at the time of construction, the SMAQMD has adopted a regulation applicable to construction emissions, compliance with the regulation may completely or partially replace this mitigation. Consultation with SMAQMD prior to construction will be necessary to make this determination.*

*5.1-1(d) The following shall be incorporated into all construction plans for projects that estimated construction related NO<sub>x</sub> emissions exceed 85 lbs/day.*

*If projected construction related emissions for a project are not reduced below the 85 lbs/day by application of MM 5.1-1(b&c), then an off-site construction mitigation fee shall be applied. The construction mitigation fee shall be calculated based upon the SMAQMD's current construction mitigation fee at the time of project specific evaluation. Verification of payment of the mitigation fee shall be provided to the City prior to issuance of any grading permits*

**Finding:** Each project applicant within the RDSP area is required to submit a plan and inventory which demonstrates that the heavy duty off-road vehicles used during construction would achieve project-wide emission reductions, based on the most recent CARB fleet average. In addition, the applicants are required to pay a construction mitigation fee to the SMAQMD sufficient to offset project emissions of NO<sub>x</sub> above 85 pounds per day. A reduction of construction vehicle emissions and payment of mitigation fees would reduce the impact related to a temporary increase in NO<sub>x</sub> emissions to a less than significant level. With implementation of the mitigation measure, this impact is reduced to a *less than significant* level.

**Impact 5.1-2:** Construction within the RDSP could result in PM<sub>10</sub> concentrations that exceed acceptable thresholds. Without mitigation, this is a *potentially significant* impact.

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Most construction sites in the RDSP area would have to be graded and prepared for development. Additionally, many of the areas would require demolition of existing structures. Grading activities involve clearing and leveling the land using heavy equipment such as scrapers, bulldozers, and backhoes. As the ground is disturbed, fugitive dust or PM<sub>10</sub> is generated. The total amount of PM<sub>10</sub> generated is normally determined by the size of the graded area and the length of time of grading activities. The larger the area and the longer the grading operation, the more PM<sub>10</sub> is created. Particulate emissions also occur to a lesser extent during other construction phases. For these reasons, the impact is considered ***potentially significant***.

Mitigation Measure (From MMP): The following mitigation measure has been adopted to address this impact:

*MM 5.1-2(a) Comply with MM 5.1-1(a).*

*MM 5.1-2(b) Grading and ground disturbance activities shall not exceed 15 acres per day for any individual development project.*

Finding: The SMAQMD's Guide to Air Quality Assessment recommends measures to reduce the amount of particulate matter generated during grading. Each project applicant is required to ensure that all off-road diesel powered equipment does not exceed 40 percent opacity for more than three minutes. In addition each applicant shall submit a dust-control plan to the City of Sacramento Community Development Department. Measures within the dust-control plan would reduce fugitive particulate matter emissions to a less than significant level. With implementation of the mitigation measure, this impact is reduced to a *less than significant* level.

### Biological Resources

**Impact 5.2-2:** Implementation of the RDSP could adversely affect special-status birds due to the substantial degradation of the quality of the environment or reduction of the population or habitat below self-sustaining levels due to loss or disturbance of nesting and/or foraging habitat. Without mitigation, this is a *potentially significant* impact.

Implementation of the proposed RDSP would allow for infill development within the project boundary and could result in the demolition of existing structures to redevelop parcels in accordance with the SP. There is a potential for special-status birds (burrowing owl, Swainson's hawk, and purple martins) within the RDSP area that could be adversely impacted by construction within the RDSP area. This is considered a ***potentially significant*** impact.

Mitigation Measure (From MMP): The following mitigation measure has been adopted to address this impact:

*MM 5.2-2(a) Preconstruction surveys for burrowing owls shall be conducted in accordance with the Burrowing Owl Survey Protocol and Mitigation Guidelines (The California Burrowing Owl Consortium 1993), which calls for surveying out to 500 feet from project limits where suitable habitat is present. If owls are identified in the biological study area, mitigation measures will be implemented as outlined in the CDFG's 1995 Staff Report on Burrowing Owl Mitigation (California Department of Fish and Game 1995). These measures will include those listed here.*

*If occupied owl burrows are found within the biological study area, a determination will be made by a qualified biologist in consultation with the CDFG regarding whether work will affect the occupied burrows or disrupt reproductive behavior.*

*If it is determined that construction will affect occupied burrows during August through February, the subject owls will be passively relocated from the occupied burrow(s) using one-way doors. One-way doors will be in place for a minimum of 48 hours before burrows are excavated.*

*If it is determined that construction will physically affect occupied burrows or disrupt reproductive behavior during the nesting season (March through July), avoidance is the only mitigation available. Construction will be delayed within 300 feet of occupied burrows until it is determined that the subject owls are not nesting or until a qualified biologist determines that juvenile owls are self sufficient or are no longer using the natal burrow as their primary source of shelter.*

*MM 5.2-2(b) Construction and demolition activities shall be conducted during the non-nesting season (August 1 through March 19) whenever feasible.*

*If construction or demolition activities occur during the nesting season (between March 20 and July 30), a qualified biologist shall conduct a survey for nesting Swainson's hawk within a 0.5 mile of the demolition/construction activities using the California Department of Fish and Game's (CDFG) Recommended Timing and Methodology for Swainson's hawk Nesting Surveys in California's Central Valley or as required by CDFG.*

*Surveys shall be conducted no less than 14 days and no more than 30 days prior to commencement of construction activities, and shall be conducted in accordance with the California Department of Fish and Game (CDFG) protocol as applicable.*

*If no active Swainson's hawks nests are identified a copy of the preconstruction survey and letter report stating the survey results shall be sent to the City of Sacramento and no further mitigation is required.*

*If active nests are found, measures consistent with the CDFG Staff Report Regarding Mitigation for Impacts to Swainson's hawks in the Central Valley of California shall be implemented. These measures include, but shall not be limited to:*

*No intensive disturbances (such as heavy equipment operation associated with construction, use of cranes, or rock-crushing) or other project-related activities that may cause nest abandonment or forced fledging, can be initiated within 200 yards (buffer zone) of an active nest between March 20 and July 30. The size of the buffer area may be adjusted by a qualified biologist*

*If demolition/construction activities are unavoidable within the buffer zone, the project applicant shall retain a qualified biologist to monitor the nest to determine if abandonment occurs. If the nest is abandoned and the nestlings are still alive, the project applicant shall retain the services of a qualified biologist to reintroduce the nesting(s) (recovery and hacking). Prior to implementation, any hacking plan shall be reviewed and approved by the Environmental Services Division and Wildlife Management Division of the CDFG.*

*Completion of the nesting cycle will be determined by a qualified biologist.*

*MM 5.2-2(c) Prior to any grading, demolition, or construction activities from March 15 to May 15 within 100 feet of the bridges over the American River adjacent to the project site, a preconstruction survey shall be conducted by a qualified biologist within 15 days of the start of project-related activities. If active nests are present, no construction shall be conducted within 100 feet of the edge of purple martin colony (as demarcated by the active nest hole closest to the construction activity) at the beginning of the purple martin breeding season from March 15 to May 15. The buffer areas shall be avoided to prevent disturbance to the nest(s) until it is no longer active. The size of the buffer areas may be adjusted in a qualified biologist and CDFG determine is would not be likely to have adverse effects on the purple martins. No project activity shall commence within the buffer areas until a qualified biologist confirms that the nest(s) is no longer active.*

Finding: Prior to any ground disturbance for the River District Specific Plan project, the applicants shall initiate a burrowing owl consultation with the CDFG. With Implementation of burrowing owl surveys and appropriate mitigation as recommended in consultation with CDFG, the impact to burrowing owls would be less than significant.

Prior to site disturbance, during the Swainson's hawk breeding season, a pre-construction survey shall be conducted within 30 days prior to site

disturbance/construction activities. With implementation of appropriate mitigation as recommend by CDFG, the impact to Swainson's hawk would be less than significant.

Prior to and grading or construction activities during the nesting season, a pre-construction survey would be conducted within 15 days prior to site disturbance/construction activities. With implementation of appropriate mitigation as recommend by CDFG, the impact to purple martins would be less than significant.

With implementation of the mitigation measure, these impacts are reduced to a *less than significant* level.

**Impact 5.2-3:** Implementation of the RDSP could adversely affect special-status mammals due to the substantial degradation of the quality of the environment or reduction of population or habitat below self-sustaining levels. Without mitigation, this is a *potentially significant* impact.

Although no special-status bat species were observed during the biological reconnaissance survey, their potential presence is assumed in this DEIR. There are bridges over the American River adjacent to the RDSP area. Crevices in the bridges could provide marginal roosting habitat for bats. Other structures within the RDSP could also be used by bats as maternity roosts, as evidenced by the findings in the Township 9 project area.

The project does not propose any work on either the bridge structures or within the rights of way for the bridges. However, implementation of the proposed RDSP would involve the removal of existing structures, both for roadway extensions and new roads and to redevelop parcels in accordance with the RDSP vision. For this reason, proposed project would result in ***potentially significant*** impacts to special-status mammals (bats).

Mitigation Measure (From MMP): The following mitigation measures have been adopted to address this impact:

***MM 5.2-3*** *Prior to demolition activities, the project applicant shall retain a qualified biologist to conduct a focused survey for bats and potential roosting sites within the area of disturbance. If no roosting sites or bats are found, a letter report confirming absence shall be sent to the City of Sacramento and no further mitigation is required.*

*If bats are found roosting outside of the nursery season (May 1 through October 1), then they shall be evicted as described under (c) below. If bats are found roosting during the nursery season, then they shall be monitored to determine if the roost site is a maternal roost. This can occur either by visual inspection of the bat pups, if possible, or monitoring the roost for sounds of bat pups after the adults leave for the night. If the*

*roost is determined to not be a maternal roost, then the bats shall be evicted as described under (c). Because bat pups cannot leave the roost until they are mature enough, eviction of a maternal roost cannot occur during the nursery season. A 250-foot (or as determined in consultation with CDFG) buffer zone shall be established around the roosting site within which no construction shall occur.*

*Eviction of bats shall be conducted using bat exclusion techniques, developed by Bat Conservation International (BCI) and in consultation with CDFG, that allow the bats to exist the roosting site but prevent re-entry to the site. This would include, but not be limited to, the installation of one-way exclusion devices. The devices would remain in place for seven days and then the exclusion points and any other potential entrances shall be sealed. This work shall be completed by a BCI-recommended exclusion professional.*

Finding: Implementation of Mitigation Measure 5.2-3 would reduce the potential impacts to a **less-than-significant** level by identifying potential bat roosting sites within the areas of construction disturbance, and either protecting maternal roosts or providing bat exclusion techniques that would allow for the bats to relocate before construction begins. With implementation of the mitigation measure, this impact is reduced to a *less than significant* level.

**Impact 5.2-4:** Implementation of the RDSP could result in the loss of CDFG-defined sensitive natural communities, such as an elderberry savanna, resulting in a substantial adverse effect. Without mitigation, this is a *potentially significant* impact.

The valley elderberry longhorn beetle (VELB) species is almost always found on, or close to, its host plant, the elderberry. Several elderberry shrubs are present within the RDSP study area, in the elderberry savannah in the eastern portion of the plan area, and in scattered disturbed lots and ruderal fields. The VELB is federally listed as threatened; and therefore, the take of the beetle and/or the disturbance of its habitat are prohibited by law. Implementation of the RDSP could result in the loss of habitat for a federally-protected species, the VELB, which is considered a **potentially significant** impact.

Mitigation Measure (From MMP): The following mitigation measure has been adopted to address this impact:

*MM 5.2-4*

*(a) Prior to any ground-disturbing, demolition, or construction activities, the project applicant shall retain a qualified biologist to conduct a survey to identify and document all potential valley elderberry longhorn beetle habitat (VELB). The survey and evaluation methods shall be performed consistent with the US Fish and Wildlife Service's (USFWS) VELB survey methods. The survey shall include a stem count of stems greater than, or*

*equal to, one-inch in diameter and an assessment of historic or current VELB use. If no such habitat is found, mitigation is not necessary.*

*(b) Avoidance*

*(1) The proposed project shall be designed to avoid ground disturbance within 100 feet of the dripline of elderberry shrubs identified in the survey, as noted in (a) above, as having stems greater than or equal to one inch in diameter. The 100-foot buffer could be adjusted in consultation with the USFWS. If avoidance is achieved, a letter report confirming avoidance shall be sent to the City of Sacramento and no further mitigation is required.*

*(2) Before any ground-disturbing activity, a qualified biologist shall flag the elderberry shrubs that will be retained adjacent to the biological study area. Thereafter, the City shall ensure that a minimum 4-foot-tall temporary, plastic mesh-type construction fence (Tensor Polygrid or equivalent) is installed at least 100-feet from the driplines of the flagged elderberry shrubs. This fencing is intended to prevent encroachment by construction vehicles and personnel. The fencing shall be strung tightly on posts set at a maximum interval of 10 feet. The fencing shall be installed in a way that prevents equipment from enlarging the work area beyond the delineated work area. The fencing shall be checked and maintained weekly until all construction is completed. Signs shall be placed at intervals of 50 feet and must be readable at a distance of 20 feet. This buffer zone will be marked by signs stating:*

*“This is habitat of the valley elderberry longhorn beetle, a threatened species, and must not be disturbed. This species is protected by the Endangered Species Act of 1973, as amended. Violators are subject to prosecution, fines, and imprisonment.”*

*(3) No construction activity, including grading, clearing, storage of equipment or machinery shall be allowed until this condition is satisfied. The fencing and a note reflecting this condition will be shown on the construction plans.*

*In addition to (b)(1-3) above, the following shall also be implemented:*

*The City will ensure that dust control measures are implemented for all ground-disturbing activities in the project area. These measures may include application of water to graded and disturbed areas that are unvegetated; however the City or its contractor may use other measures more appropriate for site-specific conditions, as long as dust is minimized to the maximum extent practicable. To avoid attracting Argentine ants, at no time will water be sprayed within the driplines of elderberry shrubs.*

*Pursuant to the USFWS VELB Guidelines, the City will implement the following measures to mitigate for the direct and indirect impacts on VELB before groundbreaking occurs for the proposed project.*

*If disturbance within 100-feet of the dripline, or approved equal by the USFWS, of the elderberry shrub with stems greater than or equal to one-inch in diameter is unavoidable, then the project applicant shall retain the services of a qualified biologist to develop VELB mitigation plan in accordance with the current USFWS mitigation guidelines for unavoidable take of VELB habitat pursuant to either Section 7 or Section 10(a) of the Federal Endangered Species Act. The mitigation plans shall be reviewed and approved by the USFWS prior to any disturbance within the 100-foot dripline.*

*(c) Compensatory Mitigation*

*(1) Transplant Directly Affected Elderberry Shrubs*

*Elderberry shrubs will be transplanted when the plants are dormant, approximately November through the first two weeks in February, after they have lost their leaves. Transplanting during the non-growing season will reduce shock to the plant and increase transplantation success. The project applicant shall follow the specific transplanting guidance provided in the USFWS VELB Guidelines.*

*Shrubs shall be transplanted to the French Camp Conservation Bank, or another UFWS-approved site. Elderberry seedlings and associated native plants will also be established at the site according to the ratios outlined in the Guidelines. See USFWS Biological Opinion, page 6, Table 1 issued on October 8, 2009 for the ratios.*

*(2) Compensate for Direct Impacts on Elderberry Shrubs*

*According to the USFWS VELB Guidelines, adversely affected shrubs that are "transplanted or destroyed" should be mitigated for according to the measures outlined in Table 1 of the USFWS VELB Guidelines. The City will mitigate for impacts on the shrubs by purchasing mitigation credits at a*

*USFWS-approved mitigation bank. A summary of the required mitigation is provided in Table 3.7-2. As shown in the table, the proposed project would require 22 elderberry seedlings and 28 associated native plants (six VELB credits) to be planted at a USFWS-approved mitigation bank. Currently, VELB mitigation credits are available at French Camp Conservation Bank. The shrubs identified for transplantation will be transplanted to this mitigation bank.*

Compensation for Impacts on VELB Habitat					
Location	Stem Diameter Class at Ground Level in Centimeters (inches)	Exit Holes?	Stem Count	Elderberry Seedling Ratio	Associated Native Plant Ratio
Non-riparian	2.5–7.6 (1–3)	No	5	1:1	1:1
		Yes	0	2:1	2:1
Non-riparian	7.6–12.7 (3–5)	No	1	2:1	1:1
		Yes	0	4:1	2:1
Non-riparian	>12.7 (>5)	No	3	3:1	1:1
		Yes	1	6:1	2:1

*If the VELB is delisted by the USFWS prior to the initiation of any ground disturbing, demolition, or construction activities, the project applicant shall comply with any requirements that accompany the VELB delisting notice.*

Finding: Implementation of the mitigation measure would require a site-specific protocol survey be conducted to determine the presence of VELB in any elderberry bushes in the area of disturbance. If habitat is identified, then implementation of the mitigation measure would ensure that the project is designed to avoid disturbance. If disturbance within the buffer is unavoidable, the transplantation and replacement of VELB habitat as specified by the USFWS's VELB mitigation guidelines would ensure that the habitat is protected from loss. For these reasons, potential impacts to the VELB habitat would be less than significant. With implementation of the mitigation measure, this impact is reduced to a *less than significant* level.

**Impact 5.2-5:** Implementation of the RDSP could result in a violation of City Code Section 12.64.040 (related to Heritage trees). Without mitigation, this is a *potentially significant* impact.

Mitigation Measure (From MMP): The following mitigation measures have been adopted to address this impact:

*MM 5.2-5 Prior to the removal of any Heritage tree, the project applicant shall contact the City's Arborist and develop and enact a tree mitigation plan in compliance with the City's requirements.*

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Finding: There could be Heritage trees on parcels that would be developed or redeveloped as part of the RDSP. Implementation of Mitigation Measure 5.2-5 would ensure that development within the RDSP would mitigate for the loss of Heritage trees, as required by the City. For this reason, the impact would be less than significant. With implementation of the mitigation measure, this impact is reduced to a *less than significant* level.

### Hazards and Hazardous Materials

**Impact 5.4-1:** Construction associated with development in accordance with the RDSP could result in the exposure of people to hazards and hazardous materials during construction activities. Without mitigation, this is a *potentially significant* impact.

Construction activities due to development in the RDSP area could expose people to existing contamination. There are areas of known soil and groundwater contamination in the Specific Plan area due to historic uses, both within, and adjacent to, the Project area. In addition, development of some parcels in accordance with the RDSP may result in demolition of existing structures. Due to the age of some existing structures it is likely that asbestos containing materials (ACM) and lead-based paint are present. In addition to demolition, the grading, excavation, and dewatering of parcels for new or re-development within the RDSP area could also expose construction workers and the public to known, or previously unknown, hazards and/or hazardous materials present in the soil or groundwater. This impact is considered *potentially significant*.

Mitigation Measure (From MMP): The following mitigation measures have been adopted to address this impact:

*MM 5.4-1(a) Prior to any ground-disturbing or site construction activities associated with development of a parcel east of 12<sup>th</sup> Street, a determination shall be made by the County's Environmental Management Department (EMD) as to whether the parcel is within 1,000 feet of the following County Assessor's Parcels. If so, the applicant shall contact the County of Sacramento's Local Enforcement Agency, per Title 27, California Code of Regulations, Section 21190. The applicant shall comply with all requirements of the EMD regarding development and use of the parcel and provide written confirmation of such to the City of Sacramento.*

- 003-0032-008
- 003-0032-009
- 001-0160-010
- 001-0160-011
- 003-0032-012
- 003-0041-006
- 001-0170-022
- 003-0410-003

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*5.4-1(b) Prior to demolition or renovation of structures, the project applicant shall provide written documentation to the City that either there is no asbestos-containing materials and/or lead-based paint in the structure or that such materials have been abated and that any remaining hazardous substances and/or waste have been removed in compliance with application State and local laws.*

Finding: Compliance with the federal, State, and local regulatory framework (including General Plan policies) would ensure that workers and the public are protected from hazards and hazardous materials during ground disturbing, demolition and/or construction activities within the RDSP boundary. Mitigation Measure 5.4-1(a)(b) enhances this framework by ensuring that project applicants provide written documentation to the City that development in the RDSP area does not expose people to potential hazards due to asbestos, lead-based paint, and the closed landfill. For these reasons, the potential impacts resulting from construction associated with development in accordance with the RDSP resulting in the exposure of people to hazards and hazardous materials during construction activities are less than significant. With implementation of the mitigation measure, this impact is reduced to a *less than significant* level.

### Noise and Vibration

**Impact 5.6-2:** Implementation of the RDSP could result in residential interior noise levels of L<sub>dn</sub> 45 or greater caused by an increase in noise levels. Without mitigation, this is a *potentially significant* impact.

Proposed residentially zoned areas in the RDSP that are subject to traffic noise and exterior noise sources that exceed the normally acceptable levels, may also result in residential interior noise levels of 45 dBA L<sub>dn</sub> or greater caused by noise level increases due to the project. As a result, areas of the RDSP proposed for residential zoning could result in future uses being subject to interior noise levels that exceed the City's standards.

Mitigation Measure (From MMP): The following mitigation measure has been adopted to address this impact:

*MM 5.6-2 Implement Mitigation Measure 5.6-1*

Finding: Because no development is currently proposed it is not possible provide adequate specific mitigation measures related to the design features of future buildings. In order to achieve the reduction of interior noise levels of future residential uses, future projects involving sensitive receptors that could be exposed to noise levels exceeding the City's noise standards will be required to prepare a project specific acoustical analysis that identifies potential impacts and noise attenuation methods, such as higher sound transmission rated windows, site design, and other mechanisms to reduce interior noise levels resulting in a

less than significant impact. With implementation of the mitigation measure, this impact is reduced to a *less than significant* level.

**Impact 5.6-3:** Construction of the development in accordance with the RDSP could result in construction noise levels that exceed the standards in the City of Sacramento Noise Ordinance. Without mitigation, this is a *potentially significant* impact.

The primary source of temporary or periodic noise within the Plan area would be construction activity. This involves both construction-site activity and the transport of workers and equipment to and from the construction sites. While specific construction activities and schedules are not presently known for the RDSP, future noise from construction activities will occur and will be subject to General Plan Policy EC 3.1.10. This policy requires that development projects subject to discretionary approval assess potential construction noise impacts on nearby sensitive uses and to minimize impacts on these uses to the extent feasible. Since this policy would require mitigation of construction noise from future development, mitigation measures are provided for the Project.

Mitigation Measure (From MMP): The following mitigation measure has been adopted to address this impact:

*MM 5.6-3 The contractor shall ensure that the following measures are implemented during all phases of construction.*

- *Whenever construction occurs adjacent to occupied residences (on or offsite), temporary barriers shall be constructed around the construction sites to shield the ground floor of the noise-sensitive uses. These barriers shall be of ¾-inch Medium Density Overlay (MDO) plywood sheeting, or other material of equivalent utility and appearance, and shall achieve a Sound Transmission Class of STC-30, or greater, based on certified sound transmission loss data taken according to ASTM Test Method E90 or as approved by the City of Sacramento Building Official.*
- *Construction equipment staging areas shall be located as far as feasible from residential areas while still serving the needs of construction contractors.*
- *Quieter “sonic” pile-drivers shall be used, unless engineering studies are submitted to the City that show this is not feasible and cost-effective, based on geotechnical considerations.*

Finding: The mitigation would require construction methods to reduce construction noise from future development. Compliance with the mitigation measure would reduce the severity of construction noise from development in the RDSP area, resulting in a less-than-significant impact. With implementation of the mitigation measure, this impact is reduced to a *less than significant* level.

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**Impact 5.6-5:** Implementation of the RDSP could result in adjacent residential and commercial areas to be exposed to vibration peak particle velocities greater than 0.5 inches per second due to highway traffic and rail operations. Without mitigation, this is a *potentially significant* impact.

Development proposed for sites alongside major heavy and light rail lines or adjacent to major freeways in the RDSP area would have the potential for exposure to vibration peak particle velocities greater than 0.5 inches per second due to highway traffic and rail operations. In general, the potential for vibration-induced structural damage from such sources would be very rare under any circumstances, but vibration-induced disruption could occur if the uses were close enough to rail lines or major freeways, resulting in a *potentially significant* impact.

Mitigation Measure (From MMP): The following mitigation measure was adopted to address this impact:

*MM 5.6-5 Implement Mitigation Measure 5.6-4(b).*

Finding: Compliance with General Plan Policy EC 3.1.6, which requires new residential and commercial projects located adjacent to major freeways, hard rail lines, or light rail lines to follow the FTA screening distance criteria, would limit vibration impacts along with Mitigation Measure 5.6-4(b) and would ensure that vibration guidelines are adhered to. As a result, vibration impacts on residential and commercial areas would be less than significant. With implementation of the mitigation measure, this impact is reduced to a *less than significant* level.

**Impact 5.6-6:** Implementation of the RDSP could result in exposure of historic buildings and archaeological sites to vibration-peak-particle velocities greater than 0.25 inches per second due to project construction, highway traffic, and rail operations. Without mitigation, this is a *potentially significant* impact.

Construction activities or highway traffic in close proximity to historic buildings and archeological sites may cause structural damage under certain circumstances, for example, when blasting, pile driving, heavy earth-moving, etc. take place very close to sensitive buildings or sites. Within the RDSP area there are existing listed historic structures and structures potentially eligible for listing along with a potential historic district and contributing resources. Construction activities could occur adjacent to each of these areas; thereby resulting in a potentially significant impact.

Mitigation Measure (From MMP): The following mitigation measure has been adopted to address this impact:

*MM 5.6-6 Implement Mitigation Measures 5.6-4 and 5.6-5.*

Finding: General Plan Policy EC 3.1.7 would ensure that the City require an assessment of the damage potential of vibration-induced construction activities, highways, and rail lines in close proximity to historic buildings and archeological sites and

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require all feasible mitigation measures be implemented to ensure no damage would occur. In addition to, and compatible with, Policy EC 3.1.7, prior to development activities, project proponents would be required to comply with Mitigation Measures 5.6-4 and 5.6-5. Because historic buildings and archeological sites would be assessed for damage potential prior to construction activities and mitigation implemented to prevent damage, the impact to these resources would be less than significant. With implementation of the mitigation measure, this impact is reduced to a *less than significant* level.

**Impact 5.6-8:** Implementation of the RDSP could result in cumulative construction noise and vibration levels that exceed the standards in the City of Sacramento Noise Ordinance as well as vibration-peak-particle velocities greater than 0.5 inches per second. Without mitigation, this is a ***potentially significant*** impact.

For a cumulative impact due to vibration to occur, project-related construction would have to occur within 50 feet of a receptor simultaneously with construction of some other development in the area. It is not anticipated that this would occur in residential areas where many sensitive receptors are located. Construction at distances greater than 50 feet from a receptor would not have the capacity to add to any cumulative vibration effect. However, numerous pieces of equipment operating within 50 feet of a receptor would have a combined effect that could result in substantial VdB levels resulting in a significant cumulative impact due to vibration levels.

Mitigation Measure (From MMP): The following mitigation measure has been adopted to address this impact:

*MM 5.6-8 Implement Mitigation Measures 5.6-3 and 5.6-4.*

Finding: Because City policy would require mitigation of construction noise and vibration from individual future development projects and because construction noise and vibration from each project would be restricted in intensity and hours of occurrence by the City Code, construction noise and vibration from each project would be mitigated and the project's contribution would not be considerable. With implementation of the mitigation measure, this cumulative impact is reduced to a *less than significant* level.

**Impact 5.6-9:** Implementation of the RDSP could result in cumulative impacts on adjacent residential and commercial areas exposed to vibration peak particle velocities greater than 0.5 inches per second due to highway traffic and rail operations. Without mitigation, this is a ***potentially significant*** impact.

Development proposed for sites alongside major heavy and light rail lines or adjacent to major freeways in the RDSP area would have the potential for exposure to vibration peak particle velocities greater than 0.5 inches per second due to highway traffic and rail operations. In general, the potential for vibration-induced structural damage from such sources would be very rare under any circumstances, but vibration impacts could occur if the uses were close enough to rail lines or major freeways. Since it is anticipated that traffic volumes would increase along the I-5 Freeway and that in the

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future is it anticipated that more freight trains may access the city along with an increase in light rail trains resulting in exposing more sensitive areas to vibration-borne effects. Compliance with General Plan policies would limit vibration impacts. Implementation of these policies along with the Mitigation Measure 5.6-4(b) would ensure that vibration guidelines are adhered to. With implementation of the mitigation measure, this cumulative impact is reduced to a *less than significant* level.

**B. Significant or Potentially Significant Impacts for which Mitigation Measures Found To Be Infeasible.**

Mitigation measures to mitigate, avoid, or substantially lessen the following significant and potentially significant environmental impacts of the Project have been identified. However, pursuant to section 21081(a)(3) of the Public Resources Code and section 15091(a)(3) of the CEQA Guidelines, as to each such impact and mitigation measure, the Planning Commission, based on the evidence in the record before it, specifically finds that the mitigation measures are infeasible. The impact and mitigation measures and the facts supporting the finding of infeasibility of each mitigation measure are set forth below. Notwithstanding the disclosure of these impacts and the finding of infeasibility, the Planning Commission elects to approve the Project due to the overriding considerations set forth below in Section (G), the statement of overriding considerations.

Noise and Vibration

**Impact 5.6-1:** Implementation of the RDSP could result in exterior noise levels that are above the upper value of the normally acceptable category for various land uses due to an increase in noise levels. Without mitigation this is a *potentially significant* impact.

Residential development in the RDSP area could experience traffic related exterior noise greater than the “Normally Acceptable” levels. The installation of sound walls could reduce the exterior noise levels to levels below the normally acceptable level; however, this is not considered a feasible mitigation measure because this would require new access points so that continuous soundwalls could be constructed along the street frontages. In addition the installation of sound walls would also be in conflict with the City’s General Plan Policy EC 3.1-11, which encourages the use of design strategies and other methods along transportation corridors to attenuate noise in lieu of sound walls. As a result, sensitive receptors to noise could be subject to exterior noise levels above the upper value of the normally acceptable level category for the residential land use. This would be a ***significant impact***.

Mitigation Measure (From MMP): The following mitigation measure has been identified to reduce this impact to a less than significant level. However, for the reasons set forth below, the mitigation measure is rejected as infeasible:

*MM 5.6-1 Future development projects in the RDSP area consisting of noise sensitive receptors shall have an acoustical analysis prepared to measure any potential project noise impacts and identify specific noise attenuation features to*

*reduce impacts associated with exterior noise, to the extent feasible, to a less than significant level consistent with the policies of the General Plan.*

Finding: While mitigation measures could be implemented to reduce exterior noise impacts there are no feasible mitigation measures to reduce noise generated by traffic adjacent to several residentially-zoned areas below the upper value of the normally acceptable noise category. The installation of sound walls could reduce the noise levels to acceptable levels; however, this is not considered feasible mitigation because this would require new access points so that continuous sound walls could be constructed along the street frontages. In addition, the installation of sound walls would be in conflict the City's General Plan policy encouraging the use of design strategies along transportation corridors to attenuate noise in lieu of sound walls. For these reasons, the impact remains *significant and unavoidable*.

**Impact 5.6-4:** Implementation of the RDSP could result in existing and/or planned residential and commercial areas to be exposed to vibration-peak-particle velocities greater than 0.5 inches per second due to project construction. Without mitigation, this is a *potentially significant* impact.

Existing and proposed residential and commercial uses could be exposed to vibration-peak-particle velocities greater than 0.5 inches per second due to construction activities within the RDSP. Future construction activities that could occur under the River District Specific Plan could have the potential to generate ground-borne vibration. Construction activities would occur at discrete locations throughout the RDSP area and vibration from such activities may impact existing buildings (i.e., through structural damage) and their occupants (i.e., through activity disruption, annoyance, etc.) if they are located close enough to the construction sites. In general, vibration-induced structural damage could only occur when certain types of construction activity (e.g., blasting, pile driving, heavy earth-moving) take place very close to existing structures, while vibration-induced disruption/annoyance could occur during more common types of construction activity (e.g., truck movements) at greater distance from the activity area.

Impacts related to construction vibration are event- and location-specific; these impacts would not occur at great distances. However, when construction vibration occurs at sensitive land uses close to construction sites, the impacts would be considered ***significant***.

Mitigation Measure (From MMP): The following mitigation measure has been identified to reduce this impact to a less than significant level. However, for the reasons set forth below, the mitigation measure is rejected as infeasible:

MM 5.6-4: *Implement Mitigation Measure 5.6-3 and;*

- a)** *During construction, should damage occur despite the above mitigation measures, construction operations shall be halted and the problem activity shall be identified. A qualified engineer shall establish vibration limits based on soil conditions and the*

*types of buildings in the immediate area. The contractor shall monitor the buildings throughout the remaining construction period and follow all recommendations of the qualified engineer to repair any damage that has occurred to the pre-existing state, and to avoid further structural damage.*

- b)** *Prior to individual development projects, the applicant shall have a certified vibration consultant prepare a site-specific vibration analysis for residential uses and historic structures that are within the screening distance (shown in Table 5.6-7) for freight and passenger trains or light rail trains. The analysis shall detail how the vibration levels at these receptors would meet the applicable vibration standards to avoid potential structural damage and annoyance. The results of the analysis shall be incorporated into project design.*

Vibration-induced structural damage could be avoided in all cases by prohibiting any construction projects that have any potential for causing structural damage to nearby structures. Since it is not feasible to prohibit all construction close to existing structures (i.e., within 150 feet), the residual potential for vibration impacts at certain receptors could be *significant and unavoidable*.

**Impact 5.6-7:** Implementation of the RDSP along with other development in the region could result in an increase in interior and exterior noise levels in the Policy Areas that are above acceptable levels. Without mitigation, this is a *potentially significant* impact

Due to anticipated increases in traffic on most local roadways due to increases in development within and outside of the Project area, noise levels in excess of City standards attributed to growth per the General Plan and the Project would represent a considerable contribution. This is considered a *significant cumulative impact*.

Mitigation Measure (From MMP): The following mitigation measure has been identified to reduce this impact to a less than significant level; however, for the reasons set forth below, the mitigation is rejected as infeasible:

*MM 5.6-1 Future development projects in the RDSP area consisting of noise sensitive receptors shall have an acoustical analysis prepared to measure any potential project noise impacts and identify specific noise attenuation features to reduce impacts associated with exterior noise to a less than significant level, to the extent feasible, consistent with the policies of the General Plan, to the extent feasible.*

Finding: While mitigation measures could be implemented to reduce exterior noise impacts there are no feasible mitigation measures to reduce noise generated by cumulative traffic conditions adjacent to several residentially-zoned areas below the upper value of the normally acceptable noise category. The installation of sound walls could reduce the noise levels to acceptable levels; however, this is not considered feasible mitigation because this would require new access points so that continuous sound walls could be constructed along the street frontages. In addition, the installation of sound walls would be in conflict the City's General

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Plan policy encouraging the use of design strategies along transportation corridors to attenuate noise in lieu of sound walls. For this reason, the impact remains *significant and unavoidable*.

### **Traffic and Circulation**

**Impact 5.10-2:** Implementation of the RDSP could result in potentially significant impact on study roadway segments in 2015. Without mitigation this is **a potentially significant** impact.

The traffic generated by development within the RDSP area in year 2015 would result in significant traffic impacts for the following roadway segments:

- Richards Boulevard just east of Bercut Drive
- 16<sup>th</sup> Street south of Richards Boulevard

Finding: No feasible mitigation measure was found to lessen the impact to a less than significant level. Mitigation would require widening of Richards Boulevard wider than planned in the RDSP to add vehicle lanes for additional vehicle capacity. This is inconsistent with the City's goals to create pedestrian-friendly streets and the City's Smart Growth policies. For this reason, the impact remains *significant and unavoidable*.

**Impact 5.10-3:** Implementation of the RDSP could result in potentially significant impacts on study freeway mainline segments in 2015.

The traffic generated by RDSP would result in significant traffic impacts in 2015 for one freeway mainline segment in the study area:

- State Route 160 northbound at the American River bridge during P.M. peak hour

Finding: No feasible mitigation measure was found to lessen the impact on SR 160 northbound at the bridge. To fully mitigate this impact, it would be necessary to reduce the RDSP traffic such that no additional traffic is added to the freeway segment, or to improve the operation of the freeway segment from LOS F to LOS E. Widening the freeway would reduce the impact, but is not considered feasible because of the numerous transportation structures that would need to be modified or replaced. For this reason, the impact remains *significant and unavoidable*.

**Impact 5.10-11:** Implementation of the RDSP could result in potentially significant impacts on study roadway segments in 2035. Without mitigation, this is a *potentially significant* impact.

The traffic generated by RDSP would result in significant traffic impact under cumulative conditions for the following roadway segments in the study area:

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- Richards Boulevard east of Bercut Drive
- Richards Boulevard east of Dos Rios Street
- 16<sup>th</sup> Street south of Richards Boulevard
- 12<sup>th</sup> Street north of Richards Boulevard
- 16<sup>th</sup> Street north of Richards Boulevard
- North 4th Street north of Richards Boulevard
- North 4th Street south of Richards Boulevard
- North 4th Street south of Bannon Street
- 10<sup>th</sup> Street south of Railyards Boulevard
- 12<sup>th</sup> Street south of North B Street

Finding: No feasible mitigation measures were identified that would reduce the impacts on the roadway segments. Mitigation would require additional widening of the roadways within the RDSP area, to add more vehicle lanes to increase vehicle capacity, which is inconsistent with City goals to create pedestrian-friendly streets and the City's Smart Growth policies. For these reasons, the impact remains *significant and unavoidable*.

**Impact 5.10-12:** Implementation of the RDSP could result in potentially significant impacts on study freeway mainline segments in 2035. Without mitigation, this is a *potentially significant* impact.

The traffic generated by RDSP would result in significant traffic impact in 2035 for the following freeway mainline segments in the study area:

- Northbound I-5 south of I Street on-ramp – AM and PM peak hours
- Northbound I-5 south of Richards Boulevard off-ramp – PM peak hour
- Northbound I-5 north of Richards Boulevard off-ramp – PM peak hour
- Northbound I-5 north of Richards Boulevard on-ramp – PM peak hour
- Southbound I-5 north of Richards Boulevard off-ramp – AM and PM peak hours
- Southbound I-5 north of Richards Boulevard on-ramp – AM and PM peak hours
- Southbound I-5 north of J Street off-ramp – AM and PM peak hours
- Southbound I-5 north of I Street on-ramp – PM peak hour
- Northbound SR 160 at the American River – PM peak hour

Finding: No feasible mitigation measures were found to lessen the impact on these freeway segments. It would be necessary to reduce the traffic generated in the RDSP area such that no additional traffic were added to the freeway segment or to improve the operations of the freeway segments from Level of Service F to Level of Service E. Widening the freeway would reduce the impact, but was not considered feasible because of the numerous transportation structures that would need to be modified/replaced.

For these reasons, the impact remains *significant and unavoidable*.

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**Impact 5.10-13:** Implementation of the RDSP could result in potentially significant impacts on study freeway interchanges in 2035. Without mitigation, this is a *potentially significant impact*.

The traffic generated by RDSP would result in significant traffic impact the following freeway interchange locations within the study area:

- Northbound I-5 off-ramp to Richards Boulevard – PM peak hour
- Northbound I-5 off-ramp to Garden Highway – PM peak hour
- Southbound I-5 off-ramp to Richards Boulevard – AM peak hour
- Southbound I-5 on-ramp from Richards Boulevard – PM peak hour
- Southbound I-5 off-ramp to J Street – PM peak hour

Finding: No feasible mitigation measures were identified that would reduce the impact of the project on I-5 off-ramps. It would be necessary to reduce the traffic generated in the RDSP area such that no additional traffic were added to the freeway ramps or to improve the operations of the freeway ramps. Widening the ramps would reduce the impact, but was not considered feasible because of the numerous transportation structures that would need to be modified/ replaced.

Therefore the impact of the project remains **significant and unavoidable**.

**Impact 5.10-14:** Implementation of the RDSP could result in potentially significant impacts on study freeway off-ramp queues in 2035. Without mitigation, this is a *potentially significant impact*.

The traffic generated by RDSP would result in significant traffic impact for one freeway off-ramp queue in the study area:

- I-5 northbound off-ramp to J Street – AM peak hour.

Finding: With implementation of MM 5.10-10(gg), freeway off-ramp queues at the I-5 northbound off-ramp at J Street would be 1,028 feet in the A.M. peak hour, and would exceed the available storage. No feasible mitigation measures were identified at this location. It would be necessary to reduce the traffic generated in the RDSP area such that no additional traffic were added to the freeway ramp or to improve the operations of the freeway ramp. Widening the ramp would reduce the impact, but was not considered feasible because of the numerous transportation structures that would need to be modified/ replaced.

Therefore the impact of the project remains **significant and unavoidable**.

### C. Significant and Unavoidable Impacts.

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The following significant and potentially significant environmental impacts of the Project, including cumulative impacts, are unavoidable and cannot be mitigated in a manner that would substantially lessen the significant impact.

Notwithstanding disclosure of these impacts, the Planning Commission elects to approve the Project due to overriding considerations as set forth below in Section G, the statement of overriding considerations.

### Air Quality

**Impact 5.1-6:** Implementation of the RDSP, in conjunction with other construction activities in the SVAB, would increase cumulative construction-generated NO<sub>x</sub> levels above 85 pounds per day. Without mitigation this is a *potentially significant* impact.

Construction activities for other projects outside of the RDSP Area that occur simultaneously with project construction within the RDSP Area would contribute emissions of NO<sub>x</sub>. While those emissions would be temporary, combined they could exceed the SMAQMD thresholds. However, the SMAQMD oversees a large area outside of the RDSP Area boundaries that would require projects comply with SMAQMD mitigation requirements. It is anticipated that individual projects within the RDSP Area would comply with policies requiring implementation of feasible mitigation. Nonetheless, concurrent projects both within the RDSP Area as well as within the SVAB would likely exceed the SMAQMD significance threshold, resulting in a ***significant cumulative*** impact.

Mitigation Measure (From MMP): The following mitigation measure has been identified to reduce this impact to a less than significant level. However, for the reasons set forth below, the mitigation measure is rejected as infeasible:

*MM 5.1-6 Comply with MM 5.1-1 (a - d)*

Finding: Compliance with General Plan policies requiring implementation of SMAQMD standard mitigation measures (MM 5.1-1(a – d)) would result in reductions in construction emissions from individual projects in the RDSP Area including compliance with SMAQMD standard construction measures; payment into SMAQMD's construction mitigation fund would reduce off-site sources to ensure that construction emissions would not result in substantial increases in ozone precursors in the air basin. However, there are no other feasible mitigation measures to ensure that construction emissions for multiple concurrent projects, including projects outside of the Policy Area, can be reduced below the 85 pounds per day threshold.

Therefore, the project's contribution to this impact would remain considerable and the impact would be *significant and unavoidable*.

**Impact 5.1-8:** Implementation of the RDSP, in conjunction with other development in the SVAB, would emit particulate pollutants associated with construction activities at a

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cumulative level equal to, or greater than, five percent of the CAAQS (50 micrograms/cubic meter for 24 hours). Without mitigation, this is a *potentially significant* impact.

Significant levels of particulate matter could be generated during project grading and other construction activities taking place within the RDSP Area. Those impacts could be reduced below the significance threshold for individual projects through the implementation of the identified mitigation measures. However, PM<sub>10</sub> emissions from construction projects that occur simultaneously in the vicinity of one another and within the RDSP Area combined with development in the larger SVAB could have significant cumulative effects. Because the particulate matter emissions due to implementation of the RDSP and other development in the region could exceed established thresholds, its contribution would be considerable resulting in a ***significant cumulative impact***.

*MM 5.1-8 Comply with MM 5.1-2(a & b)*

Finding: Compliance with General Plan policies, which requires implementation of feasible mitigation measures, including MM 5.1-2(a & b) to reduce PM<sub>10</sub> emissions, would result in reductions in construction PM<sub>10</sub> emissions from individual projects within the RDSP Area. However, there are no other feasible mitigation measures to ensure that construction emissions for multiple concurrent projects, including those outside of the RDSP Area boundaries, can be reduced to ensure that PM<sub>10</sub> emissions would not exceed thresholds.

Therefore, emissions of PM<sub>10</sub> in the Policy Area would remain cumulatively considerable and the impact would be *significant and unavoidable*.

### **Cultural and Historic Resources**

**Impact 5.3-1:** Implementation of the RDSP could cause a substantial change in the significance of historical resources (State Printing Plant) as defined in the CEQA Guidelines Section 15064.5. Without mitigation, this is a *potentially significant* impact.

*Mitigation Measure 5.3-1: None available (for State Printing Plant only).*

Finding: Full implementation of the RDSP requires the construction of several streets within the Specific Plan area. North 6<sup>th</sup> Street would be extended from North B Street to Richards Boulevard, in order to extend the Central City street grid pattern. Bannon Street would be extended eastward to 7<sup>th</sup> Street. Portions of the extended North 6<sup>th</sup> Street and Bannon Streets would traverse the site of the State of California Printing Plant. This facility is eligible as a historic resource in the Sacramento Register. However, the extension of the street grid to the RDSP area is one of the primary objectives of the project. The traffic circulation within the District is dependent upon traffic accessing the area from the south, to include North 6<sup>th</sup> Street. For this reason, the impact is *significant and unavoidable*.

**Impact 5.3-2:** Implementation of the RDSP could cause a substantial change in the significance of an archaeological resource as defined in CEQA Guidelines Section 15064.5. Without mitigation, this is a *potentially significant* impact.

Mitigation Measure 5.3-2 (From MMP): The following mitigation measures have been adopted to address this impact:

*MM 5.3-2 The following shall apply to any ground disturbing activities associated with development in accordance with the RDSP.*

*a. Prior to any excavation, grading or other construction on the project site, and in consultation with Native American Tribes and the City's Preservation Director: a qualified archaeologist will prepare a testing plan for testing areas proposed for excavation or any other ground-disturbing activities as part of future projects, which plan shall be approved by the City's Preservation Director. Testing in accordance with that plan will then ensue by the qualified archaeologist, who will prepare a report on findings, and an evaluation of those findings, from those tests and present that report to the City's Preservation Director. Should any findings be considered as potentially significant, further archaeological investigations shall ensue, by the qualified archaeologist, and the archaeologist shall prepare reports on those investigations and evaluations relative to eligibility of the findings to the Sacramento, California or National Registers of Historic & Cultural Resources/ Places and submit that report to the City's Preservation Director and SHPO with recommendations for treatment, disposition, or reburials of significant findings, as appropriate. Also, at the conclusion of the pre-construction testing, evaluation and reports and recommendations, a decision will be made by the City's Preservation Director as to whether on-site monitoring during any project-related excavation or ground-disturbing activities by a qualified archaeologist will be required.*

*b. Discoveries during construction: For those projects where no on-site archaeological monitoring was required, in the event that any prehistoric subsurface archeological features or deposits, including locally darkened soil ("midden"), that could conceal cultural deposits, animal bone, obsidian and/or mortars are discovered during construction-related earth-moving activities, all work within 50 meters of the resources shall be halted, and a qualified archeologist will be consulted to assess the significance of the find. Archeological test excavations shall be conducted by a qualified archeologist to aid in determining the nature and integrity of the find. If the find is determined to be significant by the qualified archeologist, representatives of the City and the qualified archeologist shall coordinate to determine the appropriate course of action. All significant cultural materials recovered shall be subject to scientific*

*analysis and professional museum curation. In, a report shall be prepared by the qualified archeologist according to current professional standards.*

*c. If a Native American site is discovered, the evaluation process shall include consultation with the appropriate Native American representatives.*

*d. If Native American archeological, ethnographic, or spiritual resources are involved, all identification and treatment shall be conducted by qualified archeologists, who are certified by the Society of Professional Archeologists (SOPA) and/or meet the federal standards as stated in the Code of Federal Regulations (36 CFR 61), and Native American representatives, who are approved by the local Native American community as scholars of the cultural traditions.*

*e. In the event that no such Native American is available, persons who represent tribal governments and/or organizations in the locale in which resources could be affected shall be consulted. If historic archeological sites are involved, all identified treatment is to be carried out by qualified historical archeologists, who shall meet either Register of Professional Archeologists (RPA), or 36 CFR 61 requirements.*

*f. If a human bone or bone of unknown origin is found during construction, all work shall stop in the vicinity of the find, and the County Coroner, and City's Preservation Director, shall be contacted immediately. If the remains are determined to be Native American, the Coroner shall notify the Native American Heritage Commission, who shall notify the person most likely believed to be a descendant. The most likely descendant shall work with the contractor to develop a program for re-interment of the human remains and any associated artifacts. No additional work is to take place within the immediate vicinity of the find until the identified appropriate actions have taken place. Work can continue on other parts of the project site while the unique archeological resource mitigation takes place.*

Finding: Mitigation 5.3-2 outlines a plan to test sites in the RDSP area where projects will involve excavation or other ground-disturbing activities, and to specifies the proper handling of any archeological resources uncovered during ground-disturbing construction anticipated by the RDSP. While unforeseen archeological resources may still be found during any ground disturbing activities, following the guidelines in Mitigation 5.3-2 will significantly reduce potential impacts to archeological resources in the RDSP area; however, because the potential impacts to significant archeological resources may still occur during ground disturbing activity there is the potential that implementation of the RDSP may cause a significant environmental impact as defined by CEQA Guidelines Section 15064.5. For these reasons, the impact remains *significant and unavoidable*.

**D. Findings Related to the Relationship Between Local Short-term Uses of the Environment and Maintenance and Enhancement of Long-term Productivity.**

Based on the EIR and the entire record before the City Council, the City Council makes the following findings with respect to the project's balancing of local short term uses of the environment and the maintenance of long term productivity:

- As the project is implemented, certain impacts would occur on a short-term level. Such short-term impacts are discussed above. Where feasible, measures have been incorporated in the project to mitigate these potential impacts.
- The project would result in the long-term commitment of resources to develop and operate the project including water, natural gas, fossil fuels, and electricity. The long-term implementation of the project would provide economic benefits to the City. The project would be developed within an existing urban area and not contribute to urban sprawl. Notwithstanding the foregoing, some long-term impacts would result.

Although there are short-term and long-term adverse impacts from the project, the short-term and long-term benefits of the project justify implementation.

**E. Project's Contribution of Greenhouse Gas Emissions**

The City of Sacramento has adopted a proactive and comprehensive approach to climate change issues, including adoption of the 2030 General Plan to encourage a pattern of urban development that avoids dispersed residential and employment centers that by their design encourage motor vehicle trips, one of the largest contributors to greenhouse gas emissions. Likewise, the 2030 General Plan calls for strengthening the City's efforts to promote building standards to reduce the carbon footprint of buildings, another of the major contributors. The River District Specific Plan project is consistent with this approach and implements the City's plan to reduce greenhouse gas emissions.

**The 2030 General Plan and the Master Environmental Impact Report**

The City Council approved the 2030 General Plan on March 3, 2009. As part of its action, the City Council certified the Master Environmental Impact Report (Master EIR) that evaluated the environmental effects of development that is reasonably anticipated under the 2030 General Plan. The Master EIR includes extensive discussion of the potential effects of greenhouse gas emissions. The Master EIR discussions regarding climate change are incorporated here by reference. See, for example:

Draft EIR: 6.1 Air Quality (Page 6.1-1)

Final EIR: City Climate Change master Response (Page 4-1)

Errata No. 2: Climate Change (Page 12)

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The impact of greenhouse gas emissions from human activities, specifically with regard to global climate change, has been acknowledged by the City of Sacramento and others as an inherently cumulative effect. Global climate change occurs, by definition, on a global basis. Greenhouse gases remain in the atmosphere for extended periods, and combine with GHG emissions from other areas of the globe, thus creating an inherently cumulative impact.

The 2030 General Plan and Master EIR recognized these unique aspects of the problem. The Master EIR acknowledges that the greenhouse gas emissions resulting from development that would be consistent with the 2030 General Plan would be cumulatively considerable, and significant and unavoidable. See Errata 2, February 23, 2009.

In addition, at City Council direction staff reviewed the various policies and implementation programs in the 2030 General Plan that could mitigate greenhouse gas emissions, and determined that a number of these policies could be revised. A list of such policies, and the changes that were made to respond to the continuing discussion of climate change, were included as part of the Mitigation Monitoring Plan that implemented mitigation identified in the Master EIR.

The effects of the 2030 General Plan promote denser urban development within the current City territorial limits to accommodate population growth, which will reduce growth pressures and sprawl in outlying areas. While total greenhouse gas emissions within the General Plan policy area may increase over time due to growth in population in the region, this increase is less than what would have occurred if the 2030 General Plan were not adopted and development of more land in outlying areas had been permitted under the 1988 General Plan. Adoption of the 2030 General Plan put these key strategies in place immediately and has begun to shape development as well as the activities of day-to-day living and move the City and the region toward a more sustainable future.

Because the actual effectiveness of all the feasible policies and programs included in the 2030 General Plan that avoid, minimize, or reduce greenhouse gas could not be quantified, the impact was identified in the Master EIR as a significant and unavoidable cumulative impact.

#### General Plan Consistency of the River District Specific Plan Project

The 2030 General Plan identifies a mix of Traditional Neighborhood Low Density (TNLD), Traditional Neighborhood Medium Density (TNMD) and Traditional Center (TC) on the River District Specific Plan site. These designations include detached and attached single-family homes, multifamily dwellings, commercial or mixed use development and compatible public and quasi-public uses. The Land Use and Urban Form Diagram in the 2030 General Plan designates TNLD for the northern portion of the site, TNMD for the central portion and TC in the southern portion. Each of the three designations permit residential and commercial development. The development program analyzed in the Master EIR for the River District Specific Plan site included a

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mix of 549 attached and detached dwelling units and 200,000 square feet of commercial development.

The proposed River District Specific Plan project development program and mix of uses is generally consistent with the development program anticipated by the 2030 General Plan and the Master EIR. The River District Specific Plan project proposes a mix of TNLD, TNMD, Traditional Neighborhood High Density, and TC development. The proposal locates lower density single family homes to the north, higher density attached homes and apartments in the central area and commercial uses to the south. The proposed 527 dwelling units fall within the range anticipated by the General Plan (549). The 259,000 square feet of commercial space appears to be about 30% greater than was studied in the Master EIR. However, the commercial floor area ratio (FAR) of 0.37 is well within the range of 0.3-2.0 FAR permitted in TC. As a result, the land uses and their associated density and intensity are consistent with the 2030 General Plan.

In addition to determining consistency with the Land Use and Urban Form Diagram, goals and policies of the General Plan's ten elements are relevant.

*Land Use and Urban Design Element:*

LU 5 Traditional Center Urban Form Guidelines (2030 General Plan, Page 2-68)

While the guidelines are not goals or policies, and are not mandatory or binding on the applicant, they do express the City's desired urban form vision. For Traditional Centers, the guidelines call for:

1. small, rectangular blocks;
2. small, narrow lots providing a fine-grained development pattern;
3. building heights ranging from one to four stories;
4. lot coverage not exceeding 80 percent;
5. buildings sited at or near the sidewalk and typically abutting one another with limited side yard setbacks;
6. building entrances set at the sidewalk;
7. rear alleys and secondary streets providing service access to reduce the need for driveways and curb cuts on the primary street;
8. parking provided on-street as well as in...lots at the side or rear of structures;
9. transparent building frontages with pedestrian-scaled articulation and detailing;
10. moderately wide side sidewalks;
11. public streetscapes serving as the center's primary open space, complemented by outdoor seating, plazas, courtyards, and sidewalk dining areas.

These guidelines provide the staff and applicant with guidance regarding project design, and support the City's identified goal of encouraging development by providing specific and enforceable standards for development.

LU 5 Traditional Centers Goals and Policies

Policy LU 5.3.1 Development Standards. The City shall continue to support development and operation of centers in traditional neighborhoods by providing flexibility in development standards, consistent with public health and safety, in response to constraints inherent in retrofitting older structures and in creating infill development in established neighborhoods.

*Mobility Element:*

The following goals and policies are relevant to the design of the River District Specific Plan project. They primarily relate to the design of public and private streets and the desired relationships among buildings, streets and parking facilities.

Policy M 1.3.1 Grid Network. The City shall require all new residential, commercial, or mixed-use development that proposes or is required to construct or extend streets to develop a transportation network that provides for a well-connected, walkable community, preferably as a grid or modified grid.

Policy M 1.3.2 Private Complete Streets. The City shall require large private developments (e.g., office parks, apartment complexes, retail centers) to provide internal complete streets that connect to the existing roadway system.

Policy M 2.1.3 Streetscape Design. The City shall require that pedestrian-oriented streets be designed to provide a pleasant environment for walking including shade trees; plantings; well-designed benches, trash receptacles, news racks, and other furniture; pedestrian-scaled lighting fixtures; wayfinding signage; integrated transit shelters; public art; and other amenities.

Policy M 2.1.4 Cohesive Network. The City shall develop a cohesive pedestrian network of public sidewalks and street crossings that makes walking a convenient and safe way to travel.

Policy M 2.1.5 Continuous Network. The City shall provide a continuous pedestrian network in existing and new neighborhoods that facilitates convenient pedestrian travel free of major impediments and obstacles.

Policy M 2.1.6 Building Design. The City shall ensure that new buildings are designed to engage the street and encourage walking through design features such as placing the building with entrances facing the street and providing connections to sidewalks.

Policy M 2.1.7 Parking Facility Design. The City shall ensure that new automobile parking facilities are designed to facilitate safe and convenient pedestrian access, including clearly defined corridors and walkways connecting parking areas with buildings.

Policy M 2.1.8 Housing and Destination Connections. The City shall require new subdivisions and large-scale developments to include safe pedestrian walkways

that provide direct links between streets and major destinations such as transit stops and stations, schools, parks, and shopping centers.

**Policy M 3.1.12 Direct Access to Stations.** The City shall ensure that projects located in the Central City and within ½ mile walking distance of existing and planned light rail stations provide direct pedestrian and bicycle access to the station area, to the extent feasible.

**Goal M 4.3 Neighborhood Traffic.** Enhance the quality of life within existing neighborhoods through the use of neighborhood traffic management techniques, while recognizing the City's desire to provide a grid system that creates a high level of connectivity.

**Policy M 4.3.1 Neighborhood Traffic Management.** The City shall continue wherever possible to design streets and approve development applications in such a manner as to reduce high traffic flows and parking problems within residential neighborhoods.

**M 5.1.8 Connections between New Development and Bikeways.** The City shall ensure that new commercial and residential development projects provide frequent and direct connections to the nearest bikeways.

Buildings constructed as part of the project would be required to comply with current California building codes that enforce energy efficiency.

The City of Sacramento has adopted an approach that seeks to implement community development principles that encourage pedestrian-friendly, multi-use development that reduces vehicle miles travelled. The various goals and policies applicable to the project through the 2030 General Plan provides just such a framework, and are effective tools to mitigate climate change through reduction of greenhouse gas emissions. These goals and policies have accurately been described in the Master EIR as mitigation for such effects.

The City has acknowledged that the sum of greenhouse gas emissions that could be generated by development under the 2030 General Plan would be cumulatively considerable, and has identified the goals and policies under the 2030 General Plan as the primary vehicle to mitigating such impacts. This programmatic approach achieves reductions in the two main emitting categories: motor vehicle emissions and energy used in buildings. By adopting measures that are applicable community-wide, the City has implemented a reduction strategy that is fair and can be implemented with confidence that emission reductions will actually occur.

The City has identified greenhouse gas reductions goals as stated in AB 32 and other State guidance as relevant to the impact analysis. This is consistent with guidance provided by the Sacramento Metropolitan Air Quality Management District (SMAQMD). In its CEQA Guide, December 2009, the District suggests that local agencies properly consider adopting a threshold that considers whether an individual project's GHG

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emissions would substantially hinder the State's ability to attain the goals identified in AB 32. (CEQA Guide, page 6-11)

The Master EIR concluded that greenhouse gas emissions that could be emitted by development that is consistent with the 2030 General Plan would be cumulatively considerable and unavoidable (Errata No. 2, Page 12). The Master EIR includes a full analysis of greenhouse gas emissions and climate change, and adequately addresses these issues.

The project is consistent with the City's goals and policies as set forth in the 2030 General Plan and Master EIR relating to reduction of greenhouse gas emissions. The project would not impede the City's efforts to comply with AB32 requirements. The project would not have any significant additional environmental effects relating to greenhouse gas emissions or climate change.

#### **F. Project Alternatives.**

The City Council has considered the Project alternatives presented and analyzed in the final EIR and presented during the comment period and public hearing process. Some of these alternatives have the potential to avoid or reduce certain significant or potentially significant environmental impacts, as set forth below. The City Council finds, based on specific economic, legal, social, technological, or other considerations, that these alternatives are infeasible. Each alternative and the facts supporting the finding of infeasibility of each alternative are set forth below.

#### **Alternatives Considered and Dismissed from Further Consideration**

##### **Alternative Site**

Section 15126.6(f)(2)(B) of the CEQA Guidelines states, "If the lead agency concludes that no feasible alternative locations exist, it must disclose the reasons for this conclusion, and should include the reason in the EIR." A feasible alternative location for the proposed project that would result in substantially reduced impacts does not exist.

The CEQA Guidelines (Section 15126.6[b]) requires that only locations that would avoid or substantially lessen any of the significant effects of the project need be considered for inclusion in the EIR. The Off-Site Alternative would involve the construction of the proposed project on an alternative location. The Off-Site Alternative could have the same type and intensity of uses as the proposed project. Although other vacant properties are located in the City of Sacramento, infill parcels of substantial size like the project site are limited. It should also be noted that, by definition, CEQA states that an alternative should avoid or substantially lessen one or more of the environmental effects of the project. Alternative locations within the City would generally contain similar characteristics as the project site, and the development of greenfield sites located outside the City would likely result in greater impacts than the proposed project. Therefore, development of the project on an alternative location would be expected to result in at least the same level of impacts as the proposed project. As a result, an

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environmentally feasible off-site location that would meet the requirements of CEQA, as well as meet the basic objectives of the proposed project, does not exist.

### **No Project/No Development Alternative**

This alternative assumes that the Project would not be implemented and that there would not be any new development within the RDSP area. The project area is composed of approximately 400 parcels, under the ownership of approximately 200 entities. It is not feasible to consider an alternative that assumes no owners would want to develop their properties.

### **Summary of Alternatives Considered**

#### **No Project/Existing Zoning Alternative**

Section 15126.6 (e)(1) of the State CEQA Guidelines requires that a “no project alternative” be evaluated in comparison to the proposed project.

The No Project/No Build Alternative is defined in this section as the continuation of the existing condition of the project site. Development would be consistent with the currently allowed land uses, zoning, and development alternative. The No Project/No Development Alternative would allow the project site to continue in the existing state. Currently the RDSP area is a mix of underutilized and underdeveloped parcels and parcels with incompatible adjacent land uses.

#### *Facts in Support of Finding of Infeasibility*

This alternative would result in a continuation of the current mix of underutilized and underdeveloped parcels and parcels with incompatible adjacent uses and would not meet any of the project objectives to redevelop and revitalize the area.

#### **Existing Street Pattern/Historic Preservation Alternative**

This alternative assumes that there would be a River District Specific Plan to guide the development and redevelopment of the area and that no new streets would be developed. As with the Project, this alternative assumes that the density of development allowed within the Specific Plan area would be less than allowed by the Zoning Code, due to the proposed Specific Plan and the Design Guidelines. Parcel sizes would remain the same as the current configuration, which is large in some areas than would occur with the Project's street grid. This could result in different types of development than envisioned by the Project and could result in less residential development. It is assumed that the amount of office and commercial development would remain the same as the Project.

This alternative would develop the same footprint as the Project; and therefore, the impacts related to the location of development, such as potential loss of biological and

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archeological resources, exposure to hazards and hazardous materials, and changes to local hydrology would be the same.

Assuming less residential development, this alternative could result in less impacts to public services. However, the need for expanded or new facilities would result from development of either the Project or this alternative.

The impacts to residents on Bannon Street due to increased noise from traffic could be less under this alternative because the street grid would not be extended. Traffic on Bannon Steet would not be anticipated to increase enough to result in significantly increased noise for the residents. However, without the gridded street pattern, it is anticipated that more cars would travel on Richards Boulevard than with the Project, thereby resulting in greater traffic noise to the existing residential development on Dos Rios Street.

The impacts to public utilities would be slightly less because less residential development is assumed with this alternative.

It is anticipated that operational air impacts would be greater because there would not be the gridded street pattern to expand the circulation system and provide drivers with more choices.

This alternative would not require the demolition of the State Printing Plant, which is eligible for listing as a historic resource. The demolition of this building is considered a Significant and Unavoidable impact of the Project. This alternative would not result in this impact and would not result in significant impacts to historic resources.

#### *Facts in Support of Finding of Infeasibility*

This alternative would meet some of the objectives established for the Project; however, the objectives of making the River District area an integral part of the circulation system with the areas to the east and south would not be met.

#### **G. Statement of Overriding Considerations:**

Pursuant to Guidelines section 15092, the City Council finds that in approving the Project it has eliminated or substantially lessened all significant and potentially significant effects of the Project on the environment where feasible, as shown in Sections 5.1 through 5.10. The City Council further finds that it has balanced the economic, legal, social, technological, and other benefits of the Project against the remaining unavoidable environmental risks in determining whether to approve the Project and has determined that those benefits outweigh the unavoidable environmental risks and that those risks are acceptable. The City Council makes this statement of overriding considerations in accordance with section 15093 of the Guidelines in support of approval of the Project.

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The project would establish the planning and development standards for redevelopment of an underutilized area. The goal of the Project is to master plan the district as a transit-oriented, urban neighborhood that supports a mix of uses with parcels ready for development. The Project would provide the policy and implementation framework for the evolution of the Project area from a primarily light-industrial, low intensity district to a cohesive district with a mix of residential, commercial, industrial, public and open space uses.

The City Council adopts the mitigation measures in the final Mitigation and Monitoring Program, incorporated by reference into these Findings (see Exhibit B), and finds that any residual or remaining effects on the environment resulting from the project, identified as significant and unavoidable in the Findings of Fact, are acceptable due to the benefits set forth in this Statement of Overriding Considerations. The City Council makes this Statement in accordance with section 10593 of the CEQA Guidelines in supporting approval of the project.

Exhibit B: Mitigation Monitoring Plan

Impact	Mitigation Measure	Implementing Party	Timing	Verification of Compliance
<p>5.1-1: Construction activities within the RDSP area could result in NO<sub>x</sub> levels above 85 pounds per day.</p>	<p style="text-align: center;"><b>5.1 Air Quality</b></p> <p><b>MM 5.1-1(a)</b> The following shall be incorporated into all City construction contracts and included on all construction plans</p> <ul style="list-style-type: none"> <li>• Water all exposed surfaces two times daily. Exposed surfaces include, but are not limited to soil piles, graded areas, unpaved parking areas, staging areas, and access roads.</li> <li>• Cover or maintain at least two feet of free board space on haul trucks transporting soil, sand, or other loose material on the site. Any haul trucks that would be traveling along freeways or major roadways should be covered.</li> <li>• Use wet power vacuum street sweepers to remove any visible trackout mud or dirt onto adjacent public roads at least once a day. Use of dry power sweeping is prohibited.</li> <li>• Limit vehicle speeds on unpaved roads to 15 miles per hour (mph).</li> <li>• All roadways, driveways, sidewalks, parking lots to be paved should be completed as soon as possible. In addition, building pads should be laid as soon as possible after grading unless seeding or soil binders are used.</li> <li>• Minimize idling time either by shutting equipment off when not in use or reducing the time of idling to 5 minutes (as required by the state airborne toxics control measure [Title 13, Section 2485 of the California Code of Regulations]). Provide clear signage that posts this requirement for workers at the entrances to the site.</li> </ul> <p>Maintain all construction equipment in proper working condition according to manufacturer's specifications. The equipment must be checked by a certified mechanic and determine to be running in proper condition before it is operated.</p> <p><b>MM 5.1-1(b)</b> The following shall be incorporated into all construction plans for projects that estimated construction related NO<sub>x</sub> emissions exceed 85 lbs/day.</p> <p>Category 1: Reducing NO<sub>x</sub> emissions from off-road diesel powered equipment</p> <p>The project shall provide a plan, for approval by the lead agency and SMLAQMD, demonstrating that the heavy-duty (&gt; 50 horsepower) self-propelled off-road vehicles to be used in the construction project, including owned, leased and subcontractor vehicles, will achieve a project wide fleet-average 20 percent NO<sub>x</sub> reduction and 45 percent particulate reduction compared to the most recent CARB fleet average at time of construction; and</p> <p>The project representative shall submit to the lead agency and SMLAQMD a comprehensive inventory of all off-road construction equipment, equal to or greater than 50 horsepower, that will be used an aggregate of 40 or more hours during any portion of the construction project. The inventory shall include</p>	<p>Contractor</p>	<p>Prior to Approval of Construction Plans</p>	

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	<p><i>the horsepower rating, engine production year, and projected hours of use for each piece of equipment. The inventory shall be updated and submitted monthly throughout the duration of the project, except that an inventory shall not be required for any 30-day period in which no construction activity occurs. At least 48 hours prior to the use of subject heavy-duty off-road equipment, the project representative shall provide SMAQMD with the anticipated construction timeline including start date, and name and phone number of the project manager and on-site foreman.</i></p> <p><b>MM 5.1-1(c)</b> <i>The following shall be incorporated into all construction plans for projects that estimated construction related NO<sub>x</sub> emissions exceed 85 lbs/day.</i></p> <p><b>Category 2: Controlling visible emissions from off-road diesel powered equipment</b></p> <p><i>The project shall ensure that emissions from all off-road diesel powered equipment used on the project site do not exceed 40 percent opacity for more than three minutes in any one hour. Any equipment found to exceed 40 percent opacity (or Ringelmann 2.0) shall be repaired immediately, and the lead agency and SMAQMD shall be notified within 48 hours of identification of non-compliant equipment. A visual survey of all in-operation equipment shall be made at least weekly, and a monthly summary of the visual survey results shall be submitted throughout the duration of the project, except that the monthly summary shall not be required for any 30-day period in which no construction activity occurs. The monthly summary shall include the quantity and type of vehicles surveyed as well as the dates of each survey. The SMAQMD and/or other officials may conduct periodic site inspections to determine compliance. Nothing in this section shall supersede other SMAQMD or state rules or regulations.</i></p> <p><b>and/or:</b></p> <p><i>If at the time of construction, the SMAQMD has adopted a regulation applicable to construction emissions, compliance with the regulation may completely or partially replace this mitigation. Consultation with SMAQMD prior to construction will be necessary to make this determination.</i></p> <p><b>MM 5.1-1(d)</b> <i>The following shall be incorporated into all construction plans for projects that estimated construction related NO<sub>x</sub> emissions exceed 85 lbs/day.</i></p> <p><i>If projected construction related emissions for a project are not reduced below the 85 lbs/day by application of MM 5.1-1 (b-c), then an off-site construction mitigation fee shall be applied. The construction mitigation fee shall be calculated based upon the SMAQMD's current construction mitigation fee at the time of project specific evaluation. Verification of payment of the mitigation fee shall be provided to the City prior to issuance of any grading permits.</i></p>		
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<p>5.1-2: Construction within the RDSP could result in PM<sub>10</sub> concentrations that exceed acceptable thresholds.</p>	<p>MM 5.1-2(a) <i>Comply with MM 5.1-1(a)</i> MM 5.1-2(b) <i>Grading and ground disturbance activities shall not exceed 15 acres per day for any individual development project.</i></p>	<p>Contractor Applicant</p>	<p>Prior to Approval of Construction Plans During Construction</p>
<p>5.1-6: Implementation of the RDSP, in conjunction with other construction activities in the SVAB, would increase cumulative construction-generated NO<sub>x</sub> levels above 85 pounds per day.</p>	<p>MM 5.1-6 <i>Comply with MM 5.1-1 (a - d)</i></p>	<p>Contractor</p>	<p>Prior to Approval of Construction Plans</p>
<p>5.1-8: Implementation of the RDSP, in conjunction with other development in the SVAB, would emit particulate pollutants associated with construction activities at a cumulative level equal to, or greater than, five percent of the CAAQS (50 micrograms/cubic meter for 24 hours).</p>	<p>MM 5.1-8 <i>Comply with MM 5.1-2(a &amp; b)</i></p>	<p>Contractor Applicant</p>	<p>Prior to Approval of Construction Plans During Construction</p>
<p><b>5.2 Biological Resources</b></p>			
<p>5.2-2: Implementation of the RDSP could adversely affect special-status birds due to the substantial degradation of the</p>	<p>MM 5.2-2(a) <i>Preconstruction surveys for burrowing owls shall be conducted in accordance with the Burrowing Owl Survey Protocol and Mitigation Guidelines (The California Burrowing Owl Consortium 1993), which calls for surveying out to 500 feet from project limits where suitable habitat is present. If owls are identified in the biological study area, mitigation measures will be implemented as outlined in the CDFG's 1995 Staff Report on Burrowing Owl Mitigation (California Department of Fish and Game 1995). These measures will include those listed here.</i></p>	<p>Applicant</p>	<p>Prior to Ground Disturbance, Demolition,</p>

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<p>quality of the environment or reduction of the population or habitat below self-sustaining levels due to loss or disturbance of nesting and/or foraging habitat.</p>	<p><i>If occupied owl burrows are found within the biological study area, a determination will be made by a qualified biologist in consultation with the CDFG regarding whether work will affect the occupied burrows or disrupt reproductive behavior.</i></p> <p><i>If it is determined that construction will affect occupied burrows during August through February, the subject owls will be passively relocated from the occupied burrow(s) using one-way doors. One-way doors will be in place for a minimum of 48 hours before burrows are excavated.</i></p> <p><i>If it is determined that construction will physically affect occupied burrows or disrupt reproductive behavior during the nesting season (March through July), avoidance is the only mitigation available. Construction will be delayed within 300 feet of occupied burrows until it is determined that the subject owls are not nesting or until a qualified biologist determines that juvenile owls are self-sufficient or are no longer using the natal burrow as their primary source of shelter.</i></p> <p><b>MM 5.2-2(b)</b> <i>Construction and demolition activities shall be conducted during the non-nesting season (August 1 through March 19) whenever feasible.</i></p> <p><i>If construction or demolition activities occur during the nesting season (between March 20 and July 30), a qualified biologist shall conduct a survey for nesting Swainson's hawk within a 0.5 mile of the demolition/ construction activities using the California Department of Fish and Game's (CDFG) Recommended Timing and Methodology for Swainson's Hawk Nesting Surveys in California's Central Valley or as required by CDFG.</i></p> <p><i>Surveys shall be conducted no less than 14 days and no more than 30 days prior to commencement of construction activities, and shall be conducted in accordance with the California Department of Fish and Game (CDFG) protocol as applicable.</i></p> <p><i>If no active Swainson's hawk nests are identified a copy of the preconstruction survey and letter report stating the survey results shall be sent to the City of Sacramento and no further mitigation is required.</i></p> <p><i>If active nests are found, measures consistent with the CDFG Staff Report Regarding Mitigation for Impacts to Swainson's Hawks in the Central Valley of California shall be implemented. These measures include, but shall not be limited to:</i></p> <p><i>No intensive disturbances (such as heavy equipment operation associated with construction, use of cranes, or rock-crushing) or other project-related activities that may cause nest abandonment or forced fledging, can be initiated within 200 yards (buffer zone) of an active nest between March 20 and July 30. The size of the buffer area may be adjusted by a qualified biologist.</i></p> <p><i>If demolition/ construction activities are unavoidable within the buffer zone, the project applicant shall retain a qualified biologist to monitor the nest to determine if abandonment occurs. If the nest is abandoned and the nestlings are still alive, the project applicant shall retain the services of a qualified biologist to reintroduce the nestlings (recovery and backing). Prior to implementation, any backing plan shall be reviewed and approved by the Environmental Services Division and Wildlife Management Division of the CDFG.</i></p>		<p>or Construction</p>
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<p>5.2-3: Implementation of the RDSP could adversely affect special-status mammals due to the substantial degradation of the quality of the environment or reduction of population or habitat below self-sustaining levels.</p>	<p>Completion of the nesting cycle will be determined by a qualified biologist.</p> <p><b>MM 5.2-2(c)</b> Prior to any grading, demolition, or construction activities from March 15 to May 15 within 100 feet of the bridges over the American River adjacent to the project site, a preconstruction survey shall be conducted by a qualified biologist within 15 days of the start of project-related activities. If active nests are present, no construction shall be conducted within 100 feet of the edge of purple martin colony (as demarcated by the active nest hole closest to the construction activity) at the beginning of the purple martin breeding season from March 15 to May 15. The buffer areas shall be avoided to prevent disturbance to the nest(s) until it is no longer active. The size of the buffer areas may be adjusted in a qualified biologist and CDFG determine it would not be likely to have adverse effects on the purple martins. No project activity shall commence within the buffer areas until a qualified biologist confirms that the nest(s) is no longer active.</p> <p><b>MM 5.2-3</b> Prior to demolition activities, the project applicant shall retain a qualified biologist to conduct a focused survey for bats and potential roosting sites within the area of disturbances. If no roosting sites or bats are found, a letter report confirming absence shall be sent to the City of Sacramento and no further mitigation is required.</p> <p>If bats are found roosting outside of the nursery season (May 1 through October 1), then they shall be evicted as described under (c) below. If bats are found roosting during the nursery season, then they shall be monitored to determine if the roost site is a maternal roost. This can occur either by visual inspection of the bat pups, if possible, or monitoring the roost for sounds of bat pups after the adults leave for the night. If the roost is determined to not be a maternal roost, then the bats shall be evicted as described under (c). Because bat pups cannot leave the roost until they are mature enough, eviction of a maternal roost cannot occur during the nursery season. A 250-foot (or as determined in consultation with CDFG) buffer zone shall be established around the roosting site within which no construction shall occur.</p> <p>Eviction of bats shall be conducted using bat exclusion techniques, developed by Bat Conservation International (BCI) and in consultation with CDFG, that allow the bats to exit the roosting site but prevent re-entry to the site. This would include, but not be limited to, the installation of one-way exclusion devices. The devices would remain in place for seven days and then the exclusion points and any other potential entrances shall be sealed. This work shall be completed by a BCI-recommended exclusion professional.</p>	<p>Applicant</p>	<p>Prior to Demolition</p>	
<p>5.2-4: Implementation of the RDSP could result in the loss of CDFG-defined sensitive natural communities, such as an elderberry savanna, resulting in</p>	<p><b>MM 5.2-4</b> Prior to any ground-disturbing, demolition, or construction activities, the project applicant shall retain a qualified biologist to conduct a survey to identify and document all potential valley elderberry longhorn beetle habitat (VELB). The survey and evaluation methods shall be performed consistent with the US Fish and Wildlife Service's (USFWS) VELB survey methods. The survey shall include a stem count of stems greater than, or equal to, one-inch in diameter and an assessment of historic or current VELB use. If no such habitat is found, mitigation is not necessary.</p> <p><i>Avoidance</i></p>	<p>Applicant</p>	<p>Prior to Ground Disturbance, Demolition, or Construction</p>	

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<p><b>a substantial adverse effect.</b></p>	<p><i>The proposed project shall be designed to avoid ground disturbance within 100 feet of the dripline of elderberry shrubs identified in the survey, as noted in (a) above, as having stems greater than or equal to one inch in diameter. The 100-foot buffer could be adjusted in consultation with the USFWS. If avoidance is achieved, a letter report confirming avoidance shall be sent to the City of Sacramento and no further mitigation is required.</i></p> <p><i>Before any ground-disturbing activity, a qualified biologist shall flag the elderberry shrubs that will be retained adjacent to the biological study area. Thereafter, the City shall ensure that a minimum 4-foot-tall temporary, plastic mesh-type construction fence (Tensor Polygrid or equivalent) is installed at least 100-feet from the dripline of the flagged elderberry shrubs. This fencing is intended to prevent encroachment by construction vehicles and personnel. The fencing shall be strung tightly on posts set at a maximum interval of 10 feet. The fencing shall be installed in a way that prevents equipment from enlarging the work area beyond the delineated work area. The fencing shall be checked and maintained weekly until all construction is completed. Signs shall be placed at intervals of 50 feet and must be readable at a distance of 20 feet. This buffer zone will be marked by signs stating:</i></p> <p><i>"This is habitat of the valley elderberry loughorn beetle, a threatened species, and must not be disturbed. This species is protected by the Endangered Species Act of 1973, as amended. Violators are subject to prosecution, fines, and imprisonment."</i></p> <p><i>No construction activity, including grading, clearing, storage of equipment or machinery shall be allowed until this condition is satisfied. The fencing and a note reflecting this condition will be shown on the construction plans.</i></p> <p><i>In addition to (b)(1-3) above, the following shall also be implemented:</i></p> <p><i>The City will ensure that dust control measures are implemented for all ground-disturbing activities in the project area. These measures may include application of water to graded and disturbed areas that are unvegetated; however the City or its contractor may use other measures more appropriate for site-specific conditions, as long as dust is minimized to the maximum extent practicable. To avoid attracting Argentine ants, at no time will water be sprayed within the driplines of elderberry shrubs.</i></p> <p><i>Pursuant to the USFWS VLELB Guidelines, the City will implement the following measures to mitigate for the direct and indirect impacts on VLELB before groundbreaking occurs for the proposed project.</i></p> <p><i>If disturbance within 100-feet of the dripline, or approved equal by the USFWS, of the elderberry shrub with stems greater than or equal to one-inch in diameter is unavoidable, then the project applicant shall retain the services of a qualified biologist to develop VLELB mitigation plan in accordance with the current USFWS mitigation guidelines for unavoidable take of VLELB habitat pursuant to either Section 7 or Section 10(a) of the Federal Endangered Species Act. The mitigation plans shall be reviewed and approved by the USFWS prior to any disturbance within the 100-foot dripline.</i></p>		
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	<p><i>Compensatory Mitigation</i></p> <p><i>Transplant Directly Affected Elderberry Shrubs</i></p> <p><i>Elderberry shrubs will be transplanted when the plants are dormant, approximately November through the first two weeks in February, after they have lost their leaves. Transplanting during the non-growing season will reduce shock to the plant and increase transplantation success. The project applicant shall follow the specific transplanting guidance provided in the USFWS VTELB Guidelines.</i></p> <p><i>Shrubs shall be transplanted to the French Camp Conservation Bank, or another USFWS-approved site. Elderberry seedlings and associated native plants will also be established at the site according to the ratios outlined in the Guidelines. See USFWS Biological Opinion, page 6, Table 1 issued on October 8, 2009 for the ratios.</i></p> <p><i>Compensate for Direct Impacts on Elderberry Shrubs</i></p> <p><i>According to the USFWS VTELB Guidelines, adversely affected shrubs that are "transplanted or destroyed" should be mitigated for according to the measures outlined in Table 1 of the USFWS VTELB Guidelines. The City will mitigate for impacts on the shrubs by purchasing mitigation credits at a USFWS-approved mitigation bank. A summary of the required mitigation is provided in Table 3.7-2. As shown in the table, the proposed project would require 22 elderberry seedlings and 28 associated native plants (six VTELB credits) to be planted at a USFWS-approved mitigation bank. Currently, VTELB mitigation credits are available at French Camp Conservation Bank. The shrubs identified for transplantation will be transplanted to this mitigation bank.</i></p> <p><i>If the VTELB is deleted by the USFWS prior to the initiation of any ground disturbing, demolition, or construction activities, the project applicant shall comply with any requirements that accompany the VTELB delisting notice.</i></p>		
<p><b>5.2-5:</b> Implementation of the RDSP could result in a violation of City Code Section 12.64.040 (related to Heritage trees)</p>	<p><b>MM 5.2-5</b> Prior to the removal of any Heritage tree, the project applicant shall contact the City's Arborist and develop and enact a tree mitigation plan in compliance with the City's requirements.</p>	<p>Applicant</p>	<p>Prior to Removal of Heritage Tree</p>
<p><b>5.3-2:</b> Implementation of the RDSP could cause a substantial change in the significance of an archaeological resource as defined</p>	<p><b>5.3 Cultural Resources</b></p> <p><b>MM 5.3-2 a.</b> Prior to any excavation, grading or other construction on the project site, and in consultation with Native American Tribes and the City's Preservation Director, a qualified archaeologist will prepare a testing plan for testing areas proposed for excavation or any other ground-disturbing activities as part of future projects, which plan shall be approved by the City's Preservation Director. Testing in accordance with that plan will then ensue by the qualified archaeologist, who will prepare a report on findings, and an evaluation of those findings, from those tests and present that report to the City's Preservation Director. Should any findings be considered as potentially significant, further archaeological investigations shall ensue, by the qualified archaeologist, and the archaeologist</p>	<p>Applicant</p>	<p>Prior to Ground Disturbance, Demolition, or Construction</p>

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<p><b>in CEQA Guidelines Section 15064.5.</b></p>	<p>shall prepare reports on those investigations and evaluations relative to eligibility of the findings to the Sacramento, California or National Registers of Historic &amp; Cultural Resources. Plans and submit that report to the City's Preservation Director and SHPO with recommendations for treatment, disposition, or reburial of significant findings, as appropriate. Also, at the conclusion of the pre-construction testing, evaluation and reports and recommendations, a decision will be made by the City's Preservation Director as to whether on-site monitoring during any project-related excavation or ground-disturbing activities by a qualified archaeologist will be required.</p> <p>b. Discoveries during construction: For those projects where no on-site archaeological monitoring was required, in the event that any prehistoric subsurface archaeological features or deposits, including locally darkened soil ("midden"), that could conceal cultural deposits, animal bone, obsidian and/or mortars are discovered during construction-related earth-moving activities, all work within 50 meters of the resources shall be halted, and a qualified archaeologist will be consulted to assess the significance of the find. Archeological test excavations shall be conducted by a qualified archaeologist to aid in determining the nature and integrity of the find. If the find is determined to be significant by the qualified archaeologist, representatives of the City and the qualified archaeologist shall coordinate to determine the appropriate course of action. All significant cultural materials recovered shall be subject to scientific analysis and professional museum curation. In a report shall be prepared by the qualified archaeologist according to current professional standards.</p> <p>c. If a Native American site is discovered, the evaluation process shall include consultation with the appropriate Native American representatives.</p> <p>d. If Native American archaeological, ethnographic, or spiritual resources are involved, all identification and treatment shall be conducted by qualified archaeologists, who are certified by the Society of Professional Archaeologists (SOPA) and/or meet the federal standards as stated in the Code of Federal Regulations (36 CFR 61), and Native American representatives, who are approved by the local Native American community as scholars of the cultural traditions.</p> <p>e. In the event that no such Native American is available, persons who represent tribal governments and/or organizations in the locale in which resources could be affected shall be consulted. If historic archaeological sites are involved, all identified treatment is to be carried out by qualified historical archaeologists, who shall meet either Register of Professional Archaeologists (RPA), or 36 CFR 61 requirements.</p> <p>f. If a human bone or bone of unknown origin is found during construction, all work shall stop in the vicinity of the find, and the County Coroner, and City's Preservation Director, shall be contacted immediately. If the remains are determined to be Native American, the Coroner shall notify the Native American Heritage Commission, who shall notify the person most likely believed to be a descendant. The most likely descendant shall work with the contractor to develop a program for re-interment of the human remains and any associated artifacts. No additional work is to take place within the immediate vicinity of the find until the identified appropriate actions have taken place. Work can continue on other parts of the project site while the unique archaeological resource mitigation takes place.</p>	<p>Applicant</p>	<p>Prior to Ground</p>
<p><b>Cumulative 5.3-3:</b></p>	<p>Implement Mitigation Measure 5.3-2</p>		

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<p>Implementation of the RDSP, in conjunction with other development within the Central Valley, could cause a substantial change in the significance of a historic or archaeological resource as defined in CEQA Guidelines Section 15064.5.</p>			<p>Disturbance, Demolition, or Construction</p>	
<p>5.4-1: Construction associated with development in accordance with the RDSP could result in the exposure of people to hazards and hazardous materials during construction activities.</p>	<p><b>5.4 Hazards and Hazardous Materials</b></p> <p><b>MM 5.4-1(a)</b> Prior to any ground-disturbing or site construction activities associated with redevelopment of a parcel east of 12<sup>th</sup> Street, a determination shall be made by the County's Environmental Management Department (EMD) as to whether the parcel is within 1,000 feet of the following County Assessor's Parcels. In so, the applicant shall contact the County of Sacramento's Local Enforcement Agency, per Title 27, California Code of Regulations, Section 21190. The applicant shall comply with all requirements of the EMD regarding development and use of the parcel.</p> <ul style="list-style-type: none"> <li>• 003-0032-008</li> <li>• 003-0032-009</li> <li>• 001-0160-010</li> <li>• 001-0160-011</li> <li>• 003-0032-012</li> <li>• 003-0041-006</li> <li>• 001-0170-022</li> <li>• 003-00410-003</li> </ul>	<p>Applicant</p>	<p>Prior to Ground Disturbance or Construction</p>	
<p>5.6-1: Implementation of the RDSP could result in exterior noise levels that are above the upper value of the normally acceptable category for various land uses due to an increase in</p>	<p><b>5.6 Noise and Vibration</b></p> <p><b>MM 5.6-1</b> Future development projects in the RDSP Area consisting of noise sensitive receptors shall have an acoustical analysis prepared to measure any potential project specific noise impacts and identify specific noise attenuation features to reduce impacts associated with exterior noise to a less than significant level, to the extent feasible, consistent with the Policies of the General Plan.</p>	<p>Applicant</p>	<p>Prior to Approval of Building Plans</p>	

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<p>noise levels.</p> <p>5.6-2: Implementation of the RDSP could result in residential interior noise levels of Ldn 45 or greater caused by an increase in noise levels.</p>	<p>MM 5.6-2 Implement Mitigation Measure 5.6-1</p>	<p>Applicant</p>	<p>Prior to Approval of Building Plans</p>
<p>5.6-3: Construction of the development in accordance with the RDSP could result in construction noise levels that exceed the standards in the City of Sacramento Noise Ordinance.</p>	<p>MM 5.6-3 The contractor shall ensure that the following measures are implemented during all phases of construction.</p> <ul style="list-style-type: none"> <li>Whenever construction occurs near residential or other noise-sensitive uses (on or offsite), temporary barriers shall be constructed around the construction site to shield the ground floor and lower stories of the noise-sensitive uses. The barriers shall be of 3/4-inch Medium Density Overlay (MDO) plywood sheeting, or other material of equivalent utility and appearance, and shall achieve a Sound Transmission Class of STC-30, or greater, based on certified sound transmission loss data taken according to ASTM Test Method E90, or as approved by the City of Sacramento Building Official. The barrier shall not contain any gaps at its base or face, except for site access and surveying openings. The barrier height shall be designed to break the line of sight and provide at least a 5 dBA insertion loss between the noise producing equipment and the upper-most</li> <li>Construction equipment staging areas shall be located as far as feasible from residential areas while still serving the needs of construction contractors.</li> <li>Quieter "sonic" pile-drivers shall be used unless engineering studies are submitted to the City that show this is not feasible and cost-effective, based on geotechnical considerations.</li> </ul>	<p>Contractor</p>	<p>During All Phases of Construction</p>
<p>5.6-4: Implementation of the RDSP could result in existing and/or planned residential and commercial areas to be exposed to vibration-peak-particle velocities greater than 0.5 inches per second due to project construction.</p>	<p>MM 5.6-4 Implement Mitigation Measure 5.6-3 and</p> <ol style="list-style-type: none"> <li>During construction, should damage occur despite the above mitigation measures, construction operations shall be halted and the problem activity shall be identified. A qualified engineer shall establish vibration limits based on soil conditions and the type of buildings in the immediate area. The contractor shall monitor the buildings throughout the remaining construction period and follow all recommendations of the qualified engineer to repair any damage that has occurred to the pre-existing state, and to avoid further structural damage.</li> <li>Prior to individual development projects, the applicant shall have a certified vibration consultant prepare a site-specific vibration analysis for residential uses and historic structures that are within the screening distance (shown in Figure 5.6-3) for freight and passenger trains or light rail trains. The analysis shall detail how the vibration levels at these receptors would meet the applicable vibration standards to avoid potential structural damage and annoyance. The results of the analysis shall be incorporated into project design.</li> </ol>	<p>Contractor</p>	<p>During All Phases of Construction</p>
		<p>Applicant</p>	<p>Prior to Approval of Building Plans</p>

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<p>5.6-5: Implementation of the RDSP could result in adjacent residential and commercial areas to be exposed to vibration peak particle velocities greater than 0.5 inches per second due to highway traffic and rail operations.</p>	<p>MM 5.6-5 <i>Implement Mitigation Measure 5.6-4b.</i></p>	<p>Applicant</p>	<p>Prior to Approval of Building Plans</p>	
<p>5.6-6: Implementation of the RDSP could result in exposure of historic buildings and archaeological sites to vibration-peak-particle velocities greater than 0.25 inches per second due to project construction, highway traffic, and rail operations.</p>	<p>MM 5.6-6 <i>Implement Mitigation Measure 5.6-4 and 5.6-5</i></p>	<p>Contractor</p>	<p>During All Phases of Construction</p>	
<p>Cumulative 5.6-7: Implementation of the RDSP along with other development in the region could result in an increase in interior and exterior noise levels in the Policy Area that are above acceptable levels.</p>	<p>MM 5.6-7 <i>Implement Mitigation Measure 5.6-1</i></p>	<p>Applicant</p>	<p>Prior to Approval of Building Plans</p>	
<p>5.6-8: Implementation of the RDSP could result in cumulative construction noise and vibration levels</p>	<p>MM 5.6-8 <i>Implement Mitigation Measures 5.6-3 and 5.6-4</i></p>	<p>Contractor</p>	<p>During All Phases of Construction</p>	

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<p>that exceed the standards in the City of Sacramento Noise Ordinance as well as vibration-peak-particle velocities greater than 0.5 inches per second.</p>		Applicant	Prior to Approval of Building Plans	
<p>5.6-9: Implementation of the RDSP could result in cumulative impacts on adjacent residential and commercial areas exposed to vibration peak particle velocities greater than 0.5 inches per second due to highway traffic and rail operations.</p>	<p>MM 5.6-9 Implementation of Mitigation Measure 5.6-4(f)</p>	Applicant	Prior to Approval of Building Plans	
<p>5.10-1: Implementation of the RDSP could result in potentially significant impact at study intersections in 2015.</p>	<p><b>5.10 Transportation and Circulation</b>  <b>MM 5.10-1 Error!</b> Reference source not found. (a) At the I-5 southbound ramps / Richards Boulevard intersection, the RDSP Finance Plan shall pay City's Traffic Operations Center to monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.                  (b) At the I-5 northbound ramps / Richards Boulevard intersection, modify/ restripe the eastbound approach to provide two left-turn lanes and two through lanes and adjust the signal timing. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.                  (c) At the 3rd Street / Richards Boulevard intersection, the RDSP Finance Plan shall pay City's Traffic Operations Center to monitor and adjust the signal timing when needed.                  (e) At the 12th Street / 16th Street / Richards Boulevard intersection, the RDSP Finance Plan shall pay City's Traffic Operations Center to monitor and adjust the signal timing when needed.                  (f) At the 7th Street / North B Street intersection, add one eastbound left-turn lane to provide one left-turn lane and one through-right turn lane; modify the westbound approach lanes to provide one left-turn lane and one through-right turn lane; add one northbound right-turn lane to provide one left-through lane and one right-turn lane; provide protected left-turning movements for the eastbound and</p>	Applicant	Prior to Issuance of Building Permits	

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<p>5.10-4: Implementation of the RDSP could result in potentially significant impact on study freeway interchanges in 2015.</p> <p>5.10-5: Implementation of the RDSP could result in potentially significant impact on study freeway off-ramp queues in 2015.</p> <p>5.10-10: Implementation of the RDSP could result in potentially significant impact at study intersections in 2035.</p>	<p><i>westbound left-turn lanes and provide split phasing for the northbound and southbound movements; and optimize signal timing. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.</i></p> <p><i>(b) At the 16th Street / North B Street intersection, the RDSP Finance Plan shall pay City's Traffic Operations Center to monitor and adjust the signal timing when needed.</i></p> <p><i>(f) At the 14th Street / F Street intersection, the RDSP Finance Plan shall pay City's Traffic Operations Center to monitor and adjust the signal timing when needed.</i></p> <p><i>(g) At the 16th Street / H Street intersection, the RDSP Finance Plan shall pay City's Traffic Operations Center to monitor and adjust the signal timing when needed.</i></p> <p><i>(k) At the 5th Street / I Street intersection, the RDSP Finance Plan shall pay City's Traffic Operations Center to monitor and adjust the signal timing when needed.</i></p> <p><i>(l) At the 6th Street / I Street intersection, prohibit parking during the p.m. peak hour for 100 feet along the right side of westbound I Street to provide one combination through-left lane, two through lanes, and one-combination through-right turn lane; modify the northbound lanes to provide one left-turn lane and two through lanes, and optimize signal timing.</i></p>	<p>-Applicant</p>	<p>Prior to Issuance of Building Permits</p>
<p>5.10-4: Implementation of the RDSP could result in potentially significant impact on study freeway interchanges in 2015.</p>	<p><b>MM 5.10-4</b> Prior to building permit, each developer shall pay the I-5 impact fee that is in effect at the time of the issuance of building permit.</p>	<p>-Applicant</p>	<p>Prior to Issuance of Building Permits</p>
<p>5.10-5: Implementation of the RDSP could result in potentially significant impact on study freeway off-ramp queues in 2015.</p>	<p><b>MM 5.10-5</b> Implement MM 5.10-1(a)</p>	<p>-Applicant</p>	<p>Prior to Issuance of Building Permits</p>
<p>5.10-10: Implementation of the RDSP could result in potentially significant impact at study intersections in 2035.</p>	<p><b>MM 5.10-10</b> (a) At the I-5 southbound ramps / Richards Boulevard intersection, add a third westbound left-turn lane approximately 100 feet in length; modify the eastbound approach lanes to provide one through lane, one through-right turn lane, and one right-turn lane; and optimize signal timing. To accommodate these modifications without widening proposed roadway, modifications at the adjacent I-5 northbound ramps are required. At the I-5 northbound ramps / Richards Boulevard intersection, the City shall reduce the length of the eastbound left-turn lane to approximately 100 feet; convert one eastbound through lane to a second left-turn lane; and optimize signal timing. The City, in coordination with Caltrans, is in the process of preparing a Project Study Report for this interchange and the final lane configurations will be an element of that study.</p>	<p>-Applicant</p>	<p>Prior to Issuance of Building Permits</p>

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<p><i>The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair-share contribution shall be collected by the City prior to the issuance of building permits.</i></p>	<p><i>(b) At the Berce Drive / Richards Boulevard intersection, provide two left-turn lanes and a left-through-right turn lane; modify the southbound lanes to provide a right-turn lane and a combination left-through-right turn lane; and optimize signal timing. The City, in coordination with Caltrans, is in the process of preparing a Project Study Report for this interchange and the final lane configurations will be an element of that study.</i></p>	<p><i>The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair-share contribution shall be collected by the City prior to the issuance of building permits.</i></p>	<p><i>(c) At the North 4th Street / Richards Boulevard intersection, provide two northbound left-turn lanes, and one through-right turn lane; add one westbound right-turn lane with overlap signal phasing, to provide one left-turn, two through lanes, and one right-turn lane; monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair-share contribution shall be collected by the City prior to the issuance of building permits.</i></p>	<p><i>(e) At the 7th Street / Richards Boulevard intersection, modify the eastbound approach to provide two left-turn lanes; one through lane, and one through-right turn lane; add lanes to the northbound approach to provide two left-turn lanes, two through lanes, and one right-turn lane with overlap signal phasing; increase the traffic signal cycle length from 100 to 150 seconds during both the a.m. and p.m. peak hours; monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair-share contribution shall be collected by the City prior to the issuance of building permits.</i></p>	<p><i>(f) At the Street W / Richards Boulevard intersection, the RDSP Finance Plan shall include the cost to modify the eastbound approach to add one northbound right-turn lane to provide one left-turn lane, one through lane, and one right-turn lane; monitor and adjust the signal timing when needed.</i></p>	<p><i>(g) At the 12th Street / Richards Boulevard intersection, the RDSP Finance Plan shall include the cost to remove one westbound through lane and add one eastbound through lane, this could be accomplished without widening the street; monitor and adjust the signal timing when needed.</i></p>	<p><i>(h) At the 16th Street / Richards Boulevard intersection, the RDSP Finance Plan shall include the cost to remove one westbound through lane west of the intersection to add one eastbound left-turn lane, this could be accomplished without widening the street; monitor and adjust the signal timing when needed.</i></p>	<p><i>(i) At the Vine Street / Street W intersection, add one northbound right-turn lane to provide one left-through-right turn lane, and one right-turn lane; add one southbound left-turn lane to provide one left-turn lane, one left-through-right turn lane; add one eastbound through lane to provide one left-turn</i></p>																

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lane, one through lane, one through-right turn lane; provide a fully actuated traffic signal; monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.

(j) At the Vine Street / 12th Street intersection, add two eastbound through lanes to provide three through lanes, one through-right turn lane, convert Vine Street to one-way eastbound between 12th Street and 16th Street, there would be no road widening in this section; monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.

(k) At the 16th Street / Vine Street intersection, convert Vine Street to one-way eastbound between 12th Street and 16th Street and add one eastbound left-turn lane, this could be accomplished without widening the street. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.

(m) At the 10th Street / North B Street intersection, add one eastbound through lane to provide one left-turn lane, one through lane, and one through-right turn lane, this can be accomplished without widening the existing street; monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP.

(o) At the 14th Street / North B Street intersection, convert the westbound left-through lane to a left-turn only lane and provide protected left-turn signal phasing; monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP.

(p) At the Auburn Street / North B Street intersection, convert eastbound left-through lane to a left-turn only lane to provide one left-turn lane and one through-right turn lane; convert the westbound left-through lane to a left-turn only lane to provide one left-turn lane and one through-right turn lane; monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP.

(r) At the 10th Street / C Street intersection, add one left-turn lane to provide one left-turn lane and one through-right turn lane to southbound, eastbound and westbound approaches; provide leading protected left-turn phase for southbound approach; monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.

(s) At the 14th Street / C Street intersection, install a new traffic signal at the time when one or more warrants are satisfied; provide one northbound right-turn lane by prohibiting on-street parking for 150 feet during the p.m. peak hour. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by

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*the City prior to the issuance of building permits.*

*(p)At the 16th Street / C Street intersection, convert the eastbound through lane to a left-through lane to provide one left-turn lane and one through-right turn lane; provide split signal phasing for eastbound and westbound traffic movements; monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.*

*(q)At the 7th Street / F Street intersection, modify the northbound and southbound approaches to provide one left-turn lane and one through-right turn lane; modify the westbound lanes on F Street to provide one left-through lane and one right-turn lane; provide permitted left-turn signal phasing for the east and westbound movements; provide overlap signal phasing for the westbound right turn movement; monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.*

*(r)At the 10th Street / F Street intersection, install a traffic signal at the time when one or more warrants are satisfied. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.*

*(t)At the 14th Street / F Street intersection, add one southbound left-turn to provide one left-turn lane and one through-right turn lane, this would require converting the angle parking to parallel parking on the east side of 14<sup>th</sup> Street north of F Street; provide leading, protected-permitted signal phasing for the southbound left turn movement; monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.*

*(c)At the 7th Street / G Street intersection, modify westbound lanes to provide one left-turn lane, one through lane and one right-turn lane; provide permitted phasing for the northbound left turn movement; monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.*

*(aa)At the 5th Street / H Street intersection, add one northbound right-turn lane to provide one left-turn lane, one through lane and one right-turn lane; monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.*

*(bb)At the 6th Street / H Street intersection, provide protected signal phasing for the southbound left turn movement.*

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<p>5.10-13: Implementation of the RDSP could result in potentially significant impact on study freeway interchanges in 2035.</p>	<p>(cc) At the 16th Street / H Street intersection, the RDSP Finance Plan shall pay City's Traffic Operations Center to increase the signal cycle length to 100 seconds and re-optimizing signal splits during the p.m. peak hour.</p> <p>(ee) At the 5th Street / I Street intersection, the RDSP Finance Plan shall pay City's Traffic Operations Center to monitor and adjust the signal timing when needed.</p> <p>(ff) At the 6th Street / I Street intersection, prohibit parking during the p.m. peak hour for 100 feet along the right side of westbound I Street to provide one through-left lane, two through lanes, and one through-right turn lane; modify the northbound approach to provide one left-turn lane and two through lanes; monitor and adjust the signal timing when needed.</p> <p>(gg) At the 3rd Street / J Street intersection, modify the southbound I-5 off-ramp approach to the intersection to provide one left-through lane, two through lanes, and one right-turn lane. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.</p>		
<p>5.10-14: Implementation of the RDSP could result in potentially significant impact on study freeway off-ramp queues in 2035.</p>	<p>MM 5.10-13 Prior to building permit, each developer shall pay the I-5 impact fee that is in effect at the time of the issuance of building permit.</p>	<p>Applicant</p>	<p>Prior to Issuance of Building Permits</p>
	<p>MM 5.10-14 Implement MM 5.10-10(gg)</p>	<p>Applicant</p>	<p>Prior to Issuance of Building Permits</p>

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<p>5.1-2: Construction within the RDSP could result in PM<sub>10</sub> concentrations that exceed acceptable thresholds.</p>	<p><b>MM 5.1-1(g)</b> The following shall be incorporated into all construction plans for projects that estimated construction related NO<sub>x</sub> emissions exceed 85 lbs./day.</p> <p><i>Category 2: Controlling visible emissions from off-road diesel powered equipment.</i></p> <p>The project shall ensure that emissions from all off-road diesel powered equipment used on the project site do not exceed 40 percent opacity for more than three minutes in any one hour. Any equipment found to exceed 40 percent opacity (or Paragraph 2.0) shall be repaired immediately, and the lead agency and CM-ACQMD shall be notified within 48 hours of identification of non-compliant equipment. A visual survey of all in-operation equipment shall be made at least weekly, and a monthly summary shall not be required throughout the duration of the project, except that the monthly summary shall not be required for any 30-day period in which no construction activity occurs. The monthly summary shall include the quantity and type of vehicle surveyed as well as the date of each survey. The CM-ACQMD and/or other officials may conduct periodic site inspections to determine compliance. Nothing in this section shall interfere with other CM-ACQMD or state rules or regulations.</p> <p><b>and/or:</b></p> <p>If at the time of construction, the CM-ACQMD has adopted a regulation applicable to construction emissions, compliance with the regulation may completely or partially replace this mitigation. Consultation with CM-ACQMD prior to construction will be necessary to make this determination.</p> <p><b>MM 5.1-1(g)</b> The following shall be incorporated into all construction plans for projects that estimated construction related NO<sub>x</sub> emissions exceed 85 lbs./day.</p> <p>If projected construction related emissions for a project are not reduced below the 85 lbs./day by application of MM 5.1-1 (f-cv), then an off-site construction mitigation fee shall be applied. The construction mitigation fee shall be calculated based upon the CM-ACQMD's current construction mitigation fee at the time of project specific evaluation. Verification of payment of the mitigation fee shall be provided to the City prior to issuance of any grading permits.</p> <p><b>MM 5.1-2(a)</b> Comply with MM 5.1-1 (g)</p> <p><b>MM 5.1-2(b)</b> Grading and ground disturbance activities shall not exceed 15 acres per day for any individual development project.</p>	<p>Contractor</p> <p>Applicant</p>	<p>Prior to Approval of Construction Plans</p> <p>During Construction</p>
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<p>5.1-6: Implementation of the RDSP, in conjunction with other construction activities in the SVAB, would increase cumulative generated NO<sub>x</sub> levels above 85 pounds per day.</p>	<p>MM 5.1-6 Comply with MM 5.1.4 (a - d)</p>	<p>Contractor</p>	<p>Prior to Approval of Construction Plans</p>
<p>5.1-8: Implementation of the RDSP, in conjunction with other development in the SVAB, would emit particulate pollutants associated with construction activities at a cumulative level equal to, or greater than, five percent of the CAAQs (50 micrograms/cubic meter for 24 hours).</p>	<p>MM 5.1-8 Comply with MM 5.1.2(a &amp; b)</p>	<p>Contractor Applicant</p>	<p>Prior to Approval of Construction Plans During Construction</p>
<p><b>5.2 Biological Resources</b></p>			
<p>5.2-2: Implementation of the RDSP could adversely affect special-status birds due to the substantial degradation of the quality of the environment or reduction of the population or habitat below self-sustaining levels due to loss or disturbance of nesting and/or foraging habitat.</p>	<p>MM 5.2-2(a) Preservation survey for borrowings shall be retained in accordance with the Borrowing Cost Survey Protocol and Mitigation Guidelines (The California Borrowing Cost Convention 1993) which call for a buffer of 200 feet from project limits where suitable habitat is present. If not, an identified in the biological study area, mitigation measures will be implemented as outlined in the CDFG's 1995 Staff Paper on Borrowing Cost Mitigation (California Department of Fish and Game 1995). These measures will include those listed here.</p> <p>If occupied owl burrows are found within the biological study area, a determination will be made by a qualified biologist in consultation with the CDFG regarding whether work will affect the occupied burrows or disrupt reproductive behavior.</p> <p>If it is determined that construction will affect occupied burrows during August through February, the subject owl will be passively relocated from the occupied burrow(s) using one-way doors. One-way doors will be in place for a minimum of 48 hours before burrows are excavated.</p> <p>If it is determined that construction will ultimately affect occupied burrows or disrupt reproductive behavior during the nesting season (March through July), avoidance is the only mitigation available. Construction will be delayed until it is determined that the subject</p>	<p>Applicant</p>	<p>Prior to Ground Disturbance, Demolition, or Construction</p>

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	<p>out) are not nesting or until a qualified biologist determines that juvenile oads are self-sufficient or are no longer using the natal burrow as their primary source of shelter.</p> <p><b>MM 5.2-2(b)</b> Construction and demolition activities shall be conducted during the non-nesting season (August 1 through March 15) whenever feasible.</p> <p>If construction or demolition activities occur during the nesting season (between March 30 and July 30), a qualified biologist shall conduct a survey for nesting Swainson's hawk within a 0.5 mile of the demolition/construction activities using the California Department of Fish and Game's (CDFG) Recommended Trapping and Methodology for Swainson's Hawk Nesting Surveys in California's Central Valley or as required by CDFG.</p> <p>Surveys shall be conducted no less than 14 days and no more than 30 days prior to commencement of construction activities, and shall be conducted in accordance with the California Department of Fish and Game (CDFG) protocol as applicable.</p> <p>If an active Swainson's hawk nest is identified a copy of the preconstruction survey and later report during the survey results shall be sent to the City of Sacramento and no further mitigation is required.</p> <p>If active nests are found, measures consistent with the CDFG Staff Report Egregious Mitigation for Impact to Swainson's Hawks in the Central Valley of California shall be implemented. These measures include, but shall not be limited to:</p> <p>No intensive disturbance (such as heavy equipment operation associated with construction, use of chainsaws, or work activities) or other project-related activities that may cause nest abandonment or food flushing can be initiated within 200 yards (buffer zone) of an active nest between March 30 and July 30. The size of the buffer area may be adjusted by a qualified biologist.</p> <p>If demolition/construction activities are unavoidable within the buffer zone, the project applicant shall retain a qualified biologist to monitor the nest to determine if abandonment occurs. If the nest is abandoned and the nestlings are still alive, the project applicant shall retain the services of a qualified biologist to reintroduce the nestling(s) (rearing and feeding). Prior to implementation, any rearing plan shall be reviewed and approved by the Environmental Services Division and Wildlife Management Division of the CDFG.</p> <p>Completion of the nesting cycle will be determined by a qualified biologist.</p> <p><b>MM 5.2-2(c)</b> Prior to any grading, demolition, or construction activities from March 15 to May 15 within 100 feet of the buffer over the American River adjacent to the project site, a preconstruction survey shall be conducted by a qualified biologist within 15 days of the start of proposed activities. If active nests are present, no construction shall be conducted within 100 feet of the edge of purple marsh (as demarcated by the active nest site) until the construction activity at the beginning of the purple marsh breeding season from March 15 to May 15. The buffer areas shall be created to prevent disturbance to the nest(s) until it is no longer active. The size of the buffer areas may be adjusted by a qualified biologist and CDFG. Determination is required to be made to have adverse effect on the purple marsh. No project activity shall commence within the buffer areas until a qualified biologist confirms that the nest(s) is no longer active.</p>	

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<p>5.2-3: Implementation of the RDSP could adversely affect special-status mammals due to the substantial degradation of the quality of the environment or reduction of population or habitat below self-sustaining levels.</p>	<p><b>MM 5.2-3</b> Prior to demolition activities, the project applicant shall retain a qualified biologist to conduct a focused survey for bat and potential roosting sites within the area of disturbance. If no roosting sites or bats are found, a letter report confirming absence shall be sent to the City of Sacramento and no further mitigation is required.</p> <p>If bats are found roosting outside of the survey season (May 1 through October 1), then they shall be excluded as described under (c) below. If bats are found roosting during the survey season, then they shall be monitored to determine if the roost site is a maternal roost. If roosts occur either by visual inspection of the bat signs, if possible, or monitoring the roost for sounds of bat pups after the adults leave for the night. If the roost is determined to not be a maternal roost, then the bats shall be entered as a maternal roost cannot occur during the survey season. A 250-foot (or as determined in consultation with CDFG) buffer zone shall be established around the roosting site within which no construction shall occur.</p> <p>Excavation of bats shall be conducted using bat exclusion techniques, developed by Bat Conservation International (BCI) and in consultation with CDFG, that allow the bats to exit the roosting site but prevent re-entry to the site. This would include, but not be limited to, the installation of one-way exclusion devices. The devices would remain in place for seven days and then the exclusion points and any other potential entrances shall be sealed. This work shall be completed by a BCI recommended exclusion professional.</p>	<p>Applicant</p>	<p>Prior to Demolition</p>	
<p>5.2-4: Implementation of the RDSP could result in the loss of CDFG-defined sensitive natural communities, such as an elderberry savanna, resulting in a substantial adverse effect.</p>	<p><b>MM 5.2-4</b> Prior to any ground disturbing, demolition, or construction activities, the project applicant shall retain a qualified biologist to conduct a survey to identify and document all potential valley elderberry longhorn beetle habitat (VELB). The survey and evaluation methods shall be performed consistent with the US Fish and Wildlife Service's (USFWS) VELB survey methods. The survey shall include a stem count of stems greater than or equal to one inch in diameter and an assessment of flowers or cones. VELB are. If no such habitat is found, mitigation is not necessary.</p> <p>-Avoidance</p> <p>The proposed project shall be designed to avoid ground disturbance within 100 feet of the drip-line of elderberry shrubs identified in the survey, as noted in (d) above, as long as stems greater than or equal to one inch in diameter. The 100-foot buffer would be adjusted in consultation with the USFWS. If avoidance is achieved, a letter report confirming avoidance shall be sent to the City of Sacramento and no further mitigation is required.</p> <p>Before any ground disturbing activity, a qualified biologist shall flag the elderberry shrubs that will be retained adjacent to the biological study area. Thereafter, the City shall ensure that a minimum 4-foot-tall temporary plastic mesh-type construction fence (Twice Polygrid or equivalent) is installed at least 100 feet from the drip-line of the flagged elderberry shrubs. This fencing is intended to prevent encroachment by construction vehicles and personnel. The fencing shall be strong enough to prevent maximum inward of 10 feet. The fencing shall be installed in a way that prevents equipment from enlarging the work area beyond the delineated work area. The fencing shall be checked and maintained weekly until all construction is completed. Signs shall be placed at intervals of 30 feet and must be visible at a distance of 20 feet. This buffer zone will be marked by signs stating:</p>	<p>Applicant</p>	<p>Prior to Ground Disturbance, Demolition, or Construction</p>	

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This is habitat of the valley elderberry longhorn beetle, a threatened species, and must not be disturbed. This species is protected by the Endangered Species Act of 1973, as amended. Violations are subject to prosecution, fines, and imprisonment.

No construction activity including grading, clearing, storage of equipment or machinery shall be allowed until this condition is satisfied. The timing and a note reflecting this condition will be shown on the construction plans.

In addition to (b)(1-3) above, the following shall also be implemented:

The City will ensure that dust control measures are implemented for all ground disturbing activities in the project area. These measures may include application of water to graded and disturbed areas that are susceptible, however the City or its contractor may use other means more appropriate for site-specific conditions, as long as that is recognized in the maximum extent practicable. To avoid attracting, abatement, and, at no time will water be sprayed within the dripline of elderberry shrubs.

Pursuant to the USFWS VELS Guidelines, the City will implement the following measures to mitigate for the direct and indirect impacts on VELS before ground disturbing occurs for the proposed project.

If disturbance within 100 feet of the dripline or stream equal by the USFWS of the elderberry shrubs with stems greater than or equal to one inch in diameter is unavoidable, then the project applicant shall retain the services of a qualified biologist to develop VELS mitigation plan in accordance with the current USFWS mitigation guidelines for unavoidable take of VELS bird and pursuant to either Section 7 or Section 10(d) of the Federal Endangered Species Act. The mitigation plans shall be reviewed and approved by the USFWS prior to any disturbance within the 100-foot dripline.

**Compensatory Mitigation**

**Transplant Directly-Affected Elderberry Shrubs**

Elderberry shrubs will be transplanted when the plants are dormant, approximately November through the first two weeks in February after they have lost their leaves. Transplanting during the non-growing season will reduce shock to the plants and increase transplantation success. The project applicant shall follow the specific transplanting guidelines provided in the USFWS VELS Guidelines.

Shrubs shall be transplanted to the French Camp Conservation Bank, or another USFWS approved site. Elderberry seedlings and associated native plants will also be established at the site according to the ratios outlined in the Guidelines. See USFWS Biological Opinion, page 6, Table 1 issued on October 3, 2009 for the ratios.

**Compensate for Direct Impact on Elderberry Shrubs**

According to the USFWS VELS Guidelines, adversely affected shrubs that are "transplanted or salvaged" should be mitigated for according to the measures outlined in Table 1 of the USFWS

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<p>5.2-5: Implementation of the RDSP could result in a violation of City Code Section 12.64.040 (related to Heritage trees)</p>	<p><b>VELB Guidelines:</b> The City will mitigate for impacts on the Velb by increasing mitigation credits at a USFWS-approved mitigation bank. A summary of the required mitigation is provided in Table 3.7.2. As shown in the table, the proposed project would require 22 identity seedlings and 28 associated native plants (six VELB seeds) to be planted at a USFWS-approved mitigation bank. Currently, VELB mitigation credits are available at French Camp Conservation Bank. The credits identified for transplantation will be transplanted to this mitigation bank.</p> <p>If the VELB is deleted by the USFWS prior to the initiation of any ground disturbing, development, or construction activities, the project applicant shall comply with any requirements that accompany the VELB delisting notice.</p>	<p>Applicant</p>	<p>Poor to Removal of Heritage Tree</p>	
<p>5.3-2: Implementation of the RDSP could cause a substantial change in the archaeological significance of an resource as defined in CEQA Guidelines Section 15064.5.</p>	<p><b>MM 5.3-2.5</b> Prior to the removal of any Heritage tree, the project applicant shall submit the City's Approval and develop and start a tree mitigation plan in compliance with the City's requirements.</p> <p><b>5.3 Cultural Resources</b></p> <p><b>MM 5.3-2 a</b> Prior to any excavation, grading or other construction on the project site, and in consultation with Native American Tribes and the City's Preservation Director, a qualified archaeologist will prepare a site plan for testing areas proposed for excavation or any other ground-disturbing activities as part of future projects, which plan shall be approved by the City's Preservation Director. Testing in accordance with that plan will then occur by the qualified archaeologist, who will prepare a report on findings, and an evaluation of those findings, from those tests and present that report to the City's Preservation Director. Should any findings be considered as potentially significant, further archaeological investigations shall ensue, by the qualified archaeologist, and the archaeologist shall prepare reports on those investigations and evaluations relative to eligibility of the findings to the Sacramento, California or National Register of Historic or Cultural Resources. Plans and submit that report to the City's Preservation Director and SHPO with recommendations for treatment, disposition, or removal of significant findings, as appropriate. Also, at the conclusion of the pre-construction testing, evaluation and reports and recommendations, a decision will be made by the City's Preservation Director as to whether on-site monitoring during any project-related excavation or ground-disturbing activities by a qualified archaeologist will be required.</p> <p><b>b</b> Discoveries during construction. For this project, where no on-site archaeological monitoring was required in the event that any potential cultural archaeological features or deposits, including locally distinct soil ("mudflats"), that could contain cultural deposits, animal bones, aboriginal and/or other artifacts are discovered during construction-related earth-moving activities, all work within 50 meters of the feature shall be halted, and a qualified archaeologist will be contacted to assess the significance of the find. Archaeological test excavations shall be conducted by a qualified archaeologist to aid in determining the nature and integrity of the find. If the find is determined to be significant by the qualified archaeologist, representatives of the City and the qualified archaeologist shall coordinate to determine the appropriate course of action. All significant cultural materials recovered shall be subject to scientific analysis and professional museum curation. In a report shall be prepared by the qualified archaeologist according to current professional standards.</p>	<p>Applicant</p>	<p>Poor to Ground Disturbance, Demolition, or Construction</p>	

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<p><b>Cumulative 5.3-3:</b> Implementation of the RDSP, in conjunction with other development within the Central Valley, could cause a substantial change in the significance of a historic or archeological resource as defined in CEQA Guidelines Section 15064.5.</p>	<p>c. If a Native American site is discovered, the evaluation process shall include consultation with the appropriate Native American representative.</p> <p>d. If Native American archeological, ethnographic, or spiritual resources are involved, all identification and treatment shall be conducted by qualified archeologists, who are certified by the Society of Professional Archeologists (SOPA) and/or meet the federal standards as stated in the Code of Federal Regulations (36 CFR, 61) and Native American representatives, who are approved by the local Native American community or scholars of the cultural traditions.</p> <p>e. In the event that no such Native American is available, persons who represent tribal governments and/or organizations in the locale in which resources could be affected shall be consulted. If historic archeological sites are involved, all identified treatment is to be carried out by qualified historical archeologists, who shall meet either Registrar of Professional Archeologists (RPA), or 36 CFR, 61 requirements.</p> <p>f. If a human bone or bone of unknown origin is found during construction, all work shall stop in the vicinity of the find, and the County Coroner and City's Prevention Director shall be contacted immediately. If the remains are determined to be Native American, the Coroner shall notify the Native American Heritage Commission, who shall notify the person most likely believed to be a descendant. The most likely descendant shall work with the contractor to develop a program for reburial of the human remains and any associated artifacts. No additional work is to take place until the immediate vicinity of the find until the identified appropriate actions have taken place. If one can continue on other parts of the project site while the unique archeological resource mitigation takes place.</p>	<p>Prior to Ground Disturbance, Demolition, or Construction</p>	<p>Applicant</p>
<p><b>5.4-1:</b> Construction associated with development in accordance with the RDSP could result in the exposure of people to hazards</p>	<p><b>5.4 Hazards and Hazardous Materials</b></p> <p><b>MM 5.4-1(c)</b> Prior to any ground disturbing or site construction activities associated with development of a parcel east of 1<sup>st</sup> Street, a determination shall be made by the County's Environmental Management Department (EMD) as to whether the parcel is within 1,000 feet of the following County-Accessor's Parcel. In so, the applicant shall contact the County of Sacramento's Local Enforcement Agency per Title 27, California Code of Regulations, Section 27190. The applicant shall comply with all requirements of the EMD regarding development and use of the parcel.</p> <ul style="list-style-type: none"> <li>• 003-0032-068</li> <li>• 003-0032-069</li> </ul>	<p>Prior to Ground Disturbance or Construction</p>	<p>Applicant</p>

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<p>and hazardous materials during construction activities.</p> <ul style="list-style-type: none"> <li>• 001-0160-010</li> <li>• 001-0160-011</li> <li>• 003-0032-012</li> <li>• 003-0041-006</li> <li>• 001-0170-022</li> <li>• 003-0041-003</li> </ul> <p><b>MM 5.4-1(b)</b> Prior to demolition or renovation of structures, the project applicant shall provide written documentation to the City that site-to-site containing material, soil or lead-based paint have been abated and that any remaining hazardous substance and/or waste have been removed in compliance with applicable state and local laws.</p>			
<p><b>5.6 Noise and Vibration</b></p>			
<p><b>5.6-1:</b> Implementation of the RDSP could result in exterior noise levels that are above the upper value of the normally acceptable category for various land uses due to an increase in noise levels.</p>	<p><b>MM 5.6-1</b> Future development projects in the EDCP Area consisting of noise sensitive receptors shall have an acoustic and vibration analysis prepared to measure any potential project specific noise and/or vibration impacts and identify specific noise attenuation features to reduce impacts associated with exterior noise to a level that is consistent with the Policies of the General Plan.</p>	<p>Applicant</p>	<p>Prior to Approval of Building Plans</p>
<p><b>5.6-2:</b> Implementation of the RDSP could result in residential interior noise levels of Ldn 45 or greater caused by an increase in noise levels.</p>	<p><b>MM 5.6-2</b> Implement Mitigation Measure 5.6-1</p>	<p>Applicant</p>	<p>Prior to Approval of Building Plans</p>
<p><b>5.6-3:</b> Construction of the development in accordance with the RDSP could result in noise levels that exceed the standards in the City of Sacramento Noise Ordinance.</p>	<p><b>MM 5.6-3</b> The contractor shall ensure that the following measures are implemented during all phases of construction.</p> <ul style="list-style-type: none"> <li>• Whenever construction occurs near residential or other noise-sensitive uses (on or off-site), temporary barriers shall be constructed around the construction site to shield the ground floor and lower stories of the noise-sensitive uses. The barriers shall be of 4-inch Medium Density Overlay (MDO) plywood sheathing or other material of equivalent rigidity and approximate, and shall achieve a Sound Transmission Class (STC) of 40 or greater, based on certified sound transmission loss data taken covering 90-1100 Hz. The barriers shall not contain any gaps at the base or joints, except for site access and servicing openings. The barrier height shall be designed to break the line of sight and provide at least a 5-dB(A) insertion loss between the noise-producing equipment and the apartment.</li> <li>• Construction equipment staging areas shall be located as far as feasible from residential</li> </ul>	<p>Contractor</p>	<p>During All Phases of Construction</p>

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<p>5.6-4: Implementation of the RDSP could result in existing and/or planned residential and commercial areas to be exposed to vibration-peak particle velocities greater than 0.5 inches per second due to project construction.</p>	<p>areas shall still using the needs of construction contractor.</p> <ul style="list-style-type: none"> <li>• <i>Owner "mask" title drivers shall be used unless engineering studies are submitted to the City that show this is not feasible and not-effective, based on geotechnical considerations.</i></li> </ul> <p>MM 5.6-4 <i>Implement Mitigation Measure 5.6-3 and</i></p> <p>a) <i>During construction, should damage occur despite the above mitigation measures, construction operations shall be halted and the problem strategy shall be identified. A qualified engineer shall establish vibration limits based on soil vibrations, and the types of buildings in the immediate area. The contractor shall monitor the building throughout the ongoing construction period and follow all recommendations of the qualified engineer to repair any damage that has occurred to the pre-existing site and to avoid further structural damage.</i></p> <p>b) <i>Prior to individual development projects, the applicant shall have a verified vibration consultant prepare a site-specific vibration analysis for residential use and business structures that are within the serving distance (shown in Figure 5.6-3) for freight and passenger trains or light rail trains. The analysis shall detail how the vibration level at these receptors would meet the applicable vibration standards to avoid potential structural damage and annoyance. The results of the analysis shall be incorporated into project design.</i></p>	<p>Contractor</p>	<p>During All Phases of Construction</p>
<p>5.6-5: Implementation of the RDSP could result in adjacent residential and commercial areas to be exposed to vibration peak particle velocities greater than 0.5 inches per second due to highway traffic and rail operations.</p>	<p>MM 5.6-5 <i>Implement Mitigation Measure 5.6-4b</i></p>	<p>Applicant</p>	<p>Prior to Approval of Building Plans</p>
<p>5.6-6: Implementation of the RDSP could result in exposure of historic buildings and archeological sites to vibration-peak particle velocities greater than 0.25 inches per second due to project construction.</p>	<p>MM 5.6-6 <i>Implement Mitigation Measure 5.6-4 and 5.6-5</i></p>	<p>Contractor</p>	<p>During All Phases of Construction</p>
<p>5.6-6: Implementation of the RDSP could result in exposure of historic buildings and archeological sites to vibration-peak particle velocities greater than 0.25 inches per second due to project construction.</p>	<p>MM 5.6-6 <i>Implement Mitigation Measure 5.6-4 and 5.6-5</i></p>	<p>Applicant</p>	<p>Prior to Approval of Building Plans</p>

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Highway traffic, and rail operations. Cumulative 5.6-7: Implementation of the RDSP along with other development in the region could result in an increase in interior and exterior noise levels in the Policy Area that are above acceptable levels. 5.6-8: Implementation of the RDSP could result in cumulative construction noise and vibration levels that exceed the City standards in the City of Sacramento. Noise Ordinance as well as vibration-particle velocities greater than 0.5 inches per second. 5.6-9: Implementation of the RDSP could result in cumulative impacts on adjacent residential and commercial areas exposed to vibration peak particle velocities greater than 0.5 inches per second due to highway traffic and rail operations.	Implement Mitigation Measure 5.6-1	Applicant	Prior to Approval of Building Plans
MM 5.6-8 Implement Mitigation Measures 5.6-3 and 5.6-4		Contractor	During All Phases of Construction
MM 5.6-9 Implementation of Mitigation Measure 5.6-4(b)		Applicant	Prior to Approval of Building Plans
5.10-1: Implementation of the RDSP could result in potentially significant impact at	5.10 Transportation and Circulation MM 5.10-1 Error Reference source not found. (a) At the I-5 interchange ramp / Exit/Entrance intersection, the RDSP Finance Plan shall pay City's Traffic Operations Center to monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be submitted by the City prior to the issuance of building permits.	Applicant	Prior to Issuance of Building Permits

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<p>study intersections in 2015.</p>	<p>(b) At the I-5 northbound ramps / Eschborn Boulevard intersection, modify / re-strip the eastbound approach to provide two left-turn lanes and two through lanes and adjust the signal timing. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be calculated by the City prior to the issuance of building permits.</p> <p>(c) At the 3rd Street / Eschborn Boulevard intersection, the RDSP Finance Plan shall pay City's Traffic Operations; Center to monitor and adjust the signal timing when needed.</p> <p>(d) At the 7th Street / North B Street intersection, modify the eastbound approach lanes to provide one left-turn lane and one through-right-turn lane, and one northbound right-turn lane to provide one left-through lane and one right-turn lane; provide protected left-turn movement; for the eastbound and westbound left-turn lanes and provide split phasing for the northbound and eastbound movements; and optimize signal timing. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be calculated by the City prior to the issuance of building permits.</p> <p>(e) At the 14th Street / North B Street intersection, the RDSP Finance Plan shall pay City's Traffic Operations; Center to monitor and adjust the signal timing when needed.</p> <p>(f) At the 14th Street / F Street intersection, the RDSP Finance Plan shall pay City's Traffic Operations; Center to monitor and adjust the signal timing when needed.</p> <p>(g) At the 16th Street / H Street intersection, the RDSP Finance Plan shall pay City's Traffic Operations; Center to monitor and adjust the signal timing when needed.</p> <p>(h) At the 5th Street / I Street intersection, the RDSP Finance Plan shall pay City's Traffic Operations; Center to monitor and adjust the signal timing when needed.</p> <p>(i) At the 6th Street / I Street intersection, prohibit parking along the 1/2 mile block area for 100 feet along the right side of westbound I Street to provide one combination through-left lane, two through lanes, and one combination through-right turn lane; modify the eastbound lanes to provide one left-turn lane and two through lanes, and optimize signal timing.</p>			
<p>5.10-4: Implementation of the RDSP could result in potentially significant impact on study freeway interchanges in 2015.</p>	<p>MM 5.10-4 Prior to building permit, each developer shall pay the I-5 impact fee that is in effect at the time of the issuance of building permit.</p>	<p>Applicant</p>	<p>Prior to Issuance of Building Permits</p>	
<p>5.10-5: Implementation of</p>	<p>MM 5.10-5 Implement MM 5.10-1(a)</p>	<p>Applicant</p>	<p>Prior to Issuance of Building Permits</p>	

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<p>the RDSP could result in potentially significant impact on study freeway off-ramp queues in 2035.</p> <p>5.10-10: Implementation of the RDSP could result in potentially significant impact at study intersections in 2035.</p>	<p><b>MM 5.10-10</b> (a)-(4) the 1.5 northbound ramps / Eckhardt. Easement intervention, add a third westbound left-turn lane approximately 100 feet in length, modify the eastbound approach lanes to provide one through lane, one through-right turn lane, and one right-turn lane, and adjust signal timing. To accommodate these modifications without widening proposed roadway modifications at the adjacent 1.5 northbound ramps are required. At the 1.5 northbound ramps / Eckhardt, Easement intervention, the City shall reduce the length of the eastbound left-turn lane to approximately 100 feet, convert one eastbound through lane to a second left-turn lane, and adjust signal timing. The City, in coordination with Caltrans, is in the process of preparing a Project Study Report for this intersection and the final lane configurations will be an element of that study.</p> <p>The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The four share contribution shall be reflected by the City prior to the issuance of building permits.</p> <p>(b)-(4) the Forest Drive / Eckhardt. Easement intervention, provide two left-turn lanes and a left-through-right turn lane, modify the southbound lanes to provide a right-turn lane and a combination left-through-right turn lane, and adjust signal timing. The City, in coordination with Caltrans, is in the process of preparing a Project Study Report for this interchange and the final lane configurations will be an element of that study.</p> <p>The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The four share contribution shall be reflected by the City prior to the issuance of building permits.</p>	<p>Applicant</p>	<p>Prior to Issuance of Building Permits</p>
<p>(c) At the North 4th Street / Eckhardt. Easement intervention, provide two northbound left-turn lanes, and one through-right turn lane, add one westbound right-turn lane with overlap signal, leaving to provide one left-turn, two through lanes, and one right-turn lane, monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The four share contribution shall be reflected by the City prior to the issuance of building permits.</p> <p>(d) At the 7th Street / Eckhardt. Easement intervention, modify the eastbound approach to provide two left-turn lanes, one through lane and one through-right turn lane, add lanes to the northbound approach to provide two left-turn lanes, two through lanes, and one right-turn lane with overlap signal. Increase the traffic signal cycle length from 100 to 130 seconds during both the a.m. and p.m. peak hours, monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The four share contribution shall be reflected by the City prior to the issuance of building permits.</p> <p>(e) At the Street W / Eckhardt. Easement intervention, the RDSP Finance Plan shall include the cost to modify the eastbound approach to add one northbound right-turn lane to provide one left-turn lane, one through lane, and one right-turn lane, monitor and adjust the signal timing when needed.</p>	<p>Applicant</p>	<p>Prior to Issuance of Building Permits</p>	

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		<p>(14) At the 10th Street / C Street intersection, add one left-turn lane to provide one left-turn lane and one through-right turn lane to southbound, eastbound and westbound approaches; provide leading protected left-turn phase for southbound approach; monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.</p> <p>(15) At the 14th Street / C Street intersection, install a new traffic signal at the time when one or more warrants are satisfied; provide one northbound right-turn lane by prohibiting one-third parking for 150 feet during the p.m. peak hour. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.</p> <p>(16) At the 16th Street / C Street intersection, convert the eastbound through lane to a left through lane to provide one left-turn lane and one through-right turn lane; provide split signal phasing for eastbound and westbound through movements; monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.</p> <p>(17) At the 7th Street / F Street intersection, modify the northbound and eastbound approaches to provide one left-turn lane and one through-right turn lane; modify the westbound lanes on F Street to provide one left through lane and one right-turn lane; provide protected left-turn signal phasing for the east and westbound movements; provide overlap signal phasing for the westbound right turn movement; monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.</p> <p>(18) At the 10th Street / F Street intersection, install a traffic signal at the time when one or more warrants are satisfied. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.</p> <p>(19) At the 14th Street / F Street intersection, add one northbound left-turn to provide one left-turn lane and one through-right turn lane; this would require converting the angle parking to parallel parking on the east side of 14th Street north of F Street; provide leading protected-permitted signal phasing for the northbound left turn movement; monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.</p> <p>(20) At the 7th Street / G Street intersection, modify westbound lanes to provide one left-turn lane, one through lane and one right-turn lane; provide protected phasing for the eastbound left turn movement; monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDSP Financing Plan which will be approved for the RDSP. The fair share contribution shall be collected by the City prior to the issuance of building permits.</p>
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	<p>(6d) At the 5th Street / H Street intersection, add one northbound right-turn lane to provide one left-turn lane, one through lane and one right-turn lane, monitor and adjust the signal timing when needed. The City has included the cost of this improvement in the RDCSP Financing Plan which will be approved for the RDCSP. The fair share contribution shall be calculated by the City prior to the issuance of building permits.</p> <p>(6e) At the 6th Street / H Street intersection, provide protected signal phasing for the southbound left-turn movement.</p> <p>(6f) At the 16th Street / H Street intersection, the RDCSP Finance Plan shall pay City's Traffic Operations Center to increase the signal cycle length to 100 seconds and re-timing signal splits during the p.m. peak hour.</p> <p>(6g) At the 5th Street / J Street intersection, the RDCSP Finance Plan shall pay City's Traffic Operations Center to monitor and adjust the signal timing when needed.</p> <p>(7) At the 6th Street / I Street intersection, prohibit parking along the p.m. peak hour for 100 feet along the right side of westbound I Street to provide one through-left lane, two through lanes, and one through-right turn lane, modify the northbound approach to provide one left-turn lane and two through lanes, monitor and adjust the signal timing when needed.</p> <p>(8) At the 3rd Street / J Street intersection, modify the southbound I-5 off-ramp approach to the intersection to provide one left-through lane, two through lanes, and one right-turn lane. The City has included the cost of this improvement in the RDCSP Financing Plan which will be approved for the RDCSP. The fair share contribution shall be calculated by the City prior to the issuance of building permits.</p>	
<p>5.10-13: Implementation of the RDCSP could result in potentially significant impact on study freeway interchanges in 2035.</p>	<p>MM 5.10-13 Prior to building permits, each developer shall pay the I-5 impact fee that is in effect at the time of the issuance of building permits.</p>	<p>Prior to Issuance of Building Permits</p>
<p>5.10-14: Implementation of the RDCSP could result in potentially significant impact on study freeway off-ramp queues in 2035.</p>	<p>MM 5.10-14 Implementation MM 5.10-10(g)</p>	<p>Prior to Issuance of Building Permits</p>

Subject: River District Specific Plan (M09-003)

January 13, 2011

Attachment 3

**[2030 General Plan Amendments - City Council Resolution]****RESOLUTION NO. 2011-**

Adopted by the Sacramento City Council

**AMENDING THE SACRAMENTO 2030 GENERAL PLAN LAND USE  
AND URBAN FORM DIAGRAM (M09-003)****BACKGROUND**

A. On December 13, 1994, the City Council adopted the Railyards Specific Plan, Richards Boulevard Area Plan, and the Facility Element for the Railyards and Richards Boulevard Planning Areas. (Resolution 94-736 and Resolution 96-645)

B. On December 11, 2007 the City Council directed staff to proceed in updating the Facility Element and the Richards Boulevard Area Plan. (Resolution 2007-915)

C. On March 3, 2009, City Council adopted the 2030 General Plan (Resolution No. 2009-131). A priority implementation measure in the General Plan is to achieve zoning and land use consistency. This requires making modifications to the Land Use and Urban Form Diagram, and staff has brought forward amendments to achieve this consistency.

E. On January 13, 2011, the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve the River District Specific Plan implementation actions (M09-003).

F. On February 1, 2011, the City Council conducted a public hearing, for which notice was given pursuant Sacramento City Code Section 17.200.010 (C) (1) (a) and (c) (publication and mail (500 feet)), and received and considered evidence concerning the River District Specific Plan implementation actions (M09-003).

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL  
RESOLVES AS FOLLOWS:**

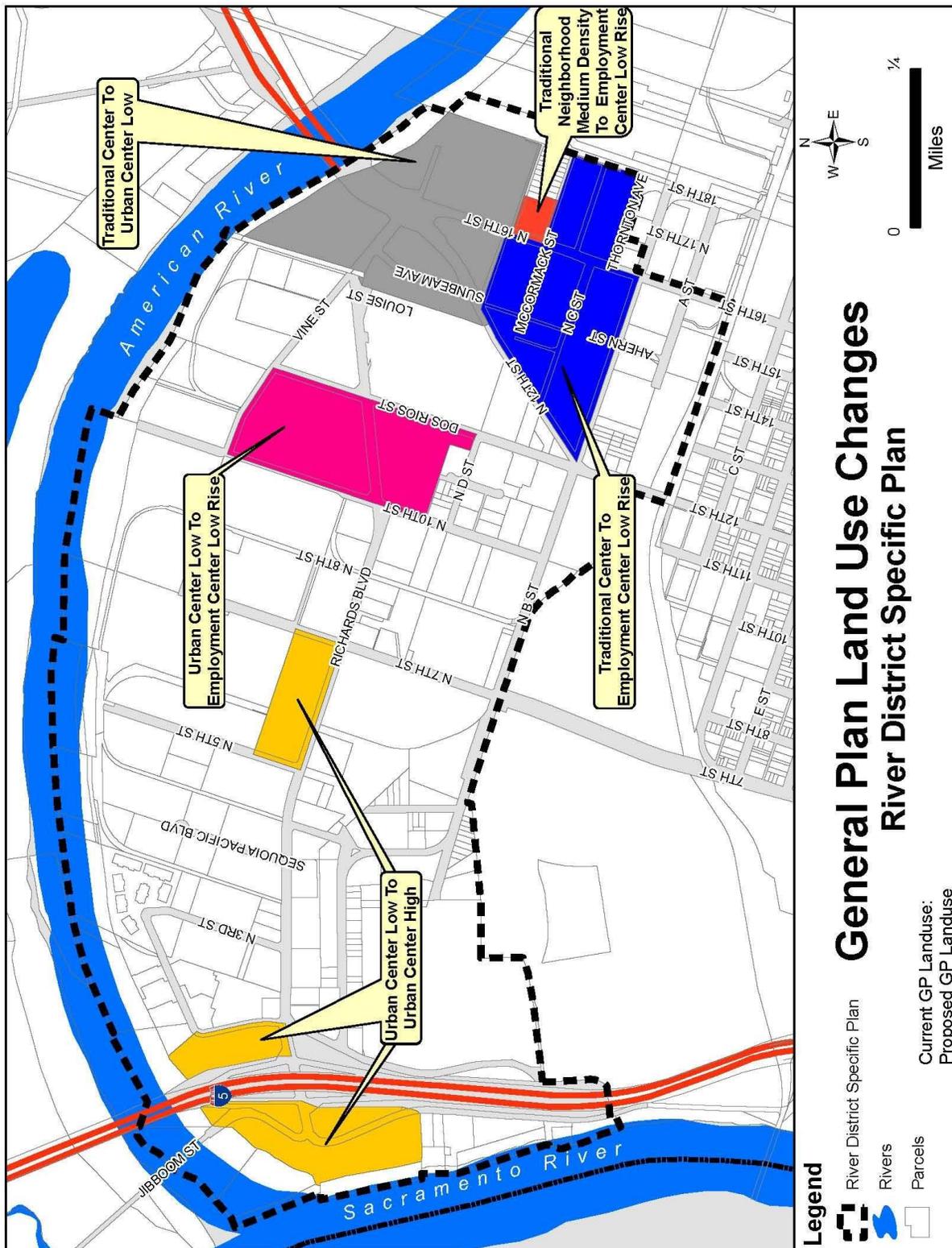
Section 1. Based on verbal and documentary evidence received at the hearing, the City Council approves the 2030 General Plan Land Use and Urban Form Diagram Amendment as set forth in Exhibits A and B.

Section 2. Exhibit A and B are a part of this Resolution.

Table of Contents:

Exhibit A –Land Use Diagram Changes Map  
Exhibit B – Land Use Changes Property List

Exhibit A – Land Use Diagram Changes Map



Subject: River District Specific Plan (M09-003)

January 13, 2011

## Exhibit B – Land Use Changes Property List

APN	Situs Address	Situs ZIP	Existing General Plan	Proposed General Plan
00100110010000	221 JIBBOOM ST	95811	Urban Center Low	Urban Center High
00100110030000	225 JIBBOOM ST	95811	Urban Center Low	Urban Center High
00100110040000	227 JIBBOOM ST	95811	Urban Center Low	Urban Center High
00100110050000	231 JIBBOOM ST	95811	Urban Center Low	Urban Center High
00100120020000	0 STATE HWY	95811	Urban Center Low	Urban Center High
00100120150000	226 JIBBOOM ST	95811	Urban Center Low	Urban Center High
00100120160000	222 JIBBOOM ST	95811	Urban Center Low	Urban Center High
00100120180000	228 JIBBOOM ST	95811	Urban Center Low	Urban Center High
00100120200000	232 JIBBOOM ST	95811	Urban Center Low	Urban Center High
00100120210000	236 JIBBOOM ST	95811	Urban Center Low	Urban Center High
00100120220000	0 JIBBOOM ST	95811	Urban Center Low	Urban Center High
00100120250000	200 JIBBOOM ST	95811	Urban Center Low	Urban Center High
00100200450000	430 N 7TH ST	95811	Urban Center Low	Urban Center Low, Urban Center High
00100700070000	1401 RICHARDS BL	95811	Traditional Center	Employment Center Low Rise
00100700280000	1421 RICHARDS BL	95811	Traditional Center	Employment Center Low Rise
00100700290000	1441 RICHARDS BL	95811	Traditional Center	Employment Center Low Rise
00100810040000	1001 RICHARDS BL	95811	Urban Center Low	Employment Center Low Rise
00100810050000	515 N 10TH ST	95811	Urban Center Low	Employment Center Low Rise
00100810060000	521 N 10TH ST	95811	Urban Center Low	Employment Center Low Rise
00100810070000	601 N 10TH ST	95811	Urban Center Low	Employment Center Low Rise
00100810090000	1000 VINE ST	95811	Urban Center Low	Employment Center Low Rise
00100810100000	0 VINE ST	95811	Urban Center Low	Employment Center Low Rise
00100810110000	609 N 10TH ST	95811	Urban Center Low	Employment Center Low Rise
00100810120000	0 N 10TH ST	95811	Urban Center Low	Employment Center Low Rise
00100810130000	701 DOS RIOS ST	95811	Urban Center Low	Employment Center Low Rise
00100810140000	601 DOS RIOS ST	95811	Urban Center Low	Employment Center Low Rise
00100810150000	0 DOS RIOS ST	95811	Urban Center Low	Employment Center Low Rise
00100810160000	1101 RICHARDS BL	95811	Urban Center Low	Employment Center Low Rise
00100900040000	1050 RICHARDS BL	95811	Urban Center Low	Employment Center Low Rise
00100900050000	1100 RICHARDS BL	95811	Urban Center Low	Employment Center Low Rise
00101010010000	0 RICHARDS BL	95811	Traditional Center	Employment Center Low Rise
00101010020000	0 RICHARDS BL	95811	Traditional Center	Employment Center Low Rise
00101010040000	510 N 12TH ST	95811	Traditional Center	Employment Center Low Rise

Subject: River District Specific Plan (M09-003)

January 13, 2011

00101010050000	1400 RICHARDS BL	95811	Traditional Center	Employment Center Low Rise
00101010060000	620 SUNBEAM AV	95811	Traditional Center	Employment Center Low Rise
00101010070000	430 N 12TH ST	95811	Traditional Center	Employment Center Low Rise
00101020070000	520 N 12TH ST	95811	Traditional Center	Employment Center Low Rise
00101020080000	1450 RICHARDS BL	95811	Traditional Center	Employment Center Low Rise
00101020090000	625 SUNBEAM AV	95811	Traditional Center	Employment Center Low Rise
00101020100000	605 SUNBEAM AV	95811	Traditional Center	Employment Center Low Rise
00101020110000	522 N 12TH ST	95811	Traditional Center	Employment Center Low Rise
00101030010000	550 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101030020000	540 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101030030000	520 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101030060000	515 N 12TH ST	95811	Traditional Center	Employment Center Low Rise
00101030080000	0 SPROULE AV	95811	Traditional Center	Employment Center Low Rise
00101030090000	500 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101030100000	1451 SPROULE AV	95811	Traditional Center	Employment Center Low Rise
00101040140000	625 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101040160000	775 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101040170000	769 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101040180000	525 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101040190000	0 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101040200000	0 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101140030000	1151 N D ST	95811	Urban Center Low	Employment Center Low Rise
00101230030000	201 N 12TH ST	95811	Traditional Center	Employment Center Low Rise
00101230040000	211 N 12TH ST	95811	Traditional Center	Employment Center Low Rise
00101300060000	1351 N C ST	95811	Traditional Center	Employment Center Low Rise
00101300070000	1341 N C ST	95811	Traditional Center	Employment Center Low Rise
00101300220000	311 N 12TH ST	95811	Traditional Center	Employment Center Low Rise

Subject: River District Specific Plan (M09-003)

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00101300240000	306 AHERN ST	95811	Traditional Center	Employment Center Low Rise
00101300250000	1321 N C ST	95811	Traditional Center	Employment Center Low Rise
00101300260000	321 N 12TH ST	95811	Traditional Center	Employment Center Low Rise
00101410010000	0 MCCORMACK AV	95811	Traditional Center	Employment Center Low Rise
00101410020000	0 SPROULE AV	95811	Traditional Center	Employment Center Low Rise
00101410030000	1400 SPROULE AV	95811	Traditional Center	Employment Center Low Rise
00101410070000	1506 SPROULE AV	95811	Traditional Center	Employment Center Low Rise
00101410130000	440 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101410140000	430 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101410150000	0 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101410160000	410 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101410170000	400 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101410210000	1501 MCCORMACK ST	95811	Traditional Center	Employment Center Low Rise
00101410220000	1448 MCCORMACK ST	95811	Traditional Center	Employment Center Low Rise
00101410230000	1450 SPROULE AV	95811	Traditional Center	Employment Center Low Rise
00101410240000	470 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101410250000	1517 MCCORMACK ST	95811	Traditional Center	Employment Center Low Rise
00101420010000	0 MCCORMACK AV	95811	Traditional Center	Employment Center Low Rise
00101420020000	0 MCCORMACK ST	95811	Traditional Center	Employment Center Low Rise
00101420030000	0 MCCORMACK ST	95811	Traditional Center	Employment Center Low Rise
00101420040000	305 AHERN ST	95811	Traditional Center	Employment Center Low Rise
00101420050000	1500 MCCORMACK ST	95811	Traditional Center	Employment Center Low Rise
00101420080000	0 MCCORMACK ST	95811	Traditional Center	Employment Center Low Rise
00101420090000	0 MCCORMACK ST	95811	Traditional Center	Employment Center Low Rise
00101420100000	324 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101420110000	324 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101420120000	324 N 16TH ST	95811	Traditional Center	Employment Center Low Rise

Subject: River District Specific Plan (M09-003)

January 13, 2011

00101420130000	318 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101420140000	1527 N C ST	95811	Traditional Center	Employment Center Low Rise
00101420180000	1401 N C ST	95811	Traditional Center	Employment Center Low Rise
00101420190000	1501 N C ST	95811	Traditional Center	Employment Center Low Rise
00101420200000	1515 N C ST	95811	Traditional Center	Employment Center Low Rise
00101420210000	1510 MCCORMACK ST	95811	Traditional Center	Employment Center Low Rise
00101420220000	1516 MCCORMACK ST	95811	Traditional Center	Employment Center Low Rise
00101430040000	1603 BASLER ST	95811	Traditional Center	Employment Center Low Rise
00101430050000	1605 BASLER ST	95811	Traditional Center	Employment Center Low Rise
00101430060000	1607 BASLER ST	95811	Traditional Center	Employment Center Low Rise
00101430070000	1611 BASLER ST	95811	Traditional Center	Employment Center Low Rise
00101430130000	1625 BASLER ST	95811	Traditional Center	Employment Center Low Rise
00101430140000	1627 BASLER ST	95811	Traditional Center	Employment Center Low Rise
00101430150000	1629 BASLER ST	95811	Traditional Center	Employment Center Low Rise
00101440030000	1604 BASLER ST	95811	Traditional Neighborhood Medium Density	Employment Center Low Rise
00101440040000	1604 BASLER ST	95811	Traditional Neighborhood Medium Density	Employment Center Low Rise
00101440050000	1608 BASLER ST	95811	Traditional Neighborhood Medium Density	Employment Center Low Rise
00101440320000	411 N 16TH ST	95811	Traditional Neighborhood Medium Density	Employment Center Low Rise
00101440330000	1607 DREHER ST	95811	Traditional Neighborhood Medium Density	Employment Center Low Rise
00101450120000	1632 DREHER ST	95811	Traditional Center	Employment Center Low Rise
00101450130000	1632 DREHER ST	95811	Traditional Center	Employment Center Low Rise
00101450220000	325 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101450230000	311 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101450240000	311 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101450260000	311 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101510010000	0 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101510020000	0 N C ST	95811	Traditional Center	Employment Center Low Rise

Subject: River District Specific Plan (M09-003)

January 13, 2011

00101510050000	200 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101520010000	1610 N C ST	95811	Traditional Center	Employment Center Low Rise
00101520020000	0 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101520030000	0 N C ST	95811	Traditional Center	Employment Center Low Rise
00101520040000	1610 N C ST	95811	Traditional Center	Employment Center Low Rise
00101520050000	1610 N C ST	95811	Traditional Center	Employment Center Low Rise
00101520060000	1616 N C ST	95811	Traditional Center	Employment Center Low Rise
00101520070000	1616 N C ST	95811	Traditional Center	Employment Center Low Rise
00101520110000	1626 N C ST	95811	Traditional Center	Employment Center Low Rise
00101520120000	1626 N C ST	95811	Traditional Center	Employment Center Low Rise
00101520130000	0 N C ST	95811	Traditional Center	Employment Center Low Rise
00101520140000	1630 N C ST	95811	Traditional Center	Employment Center Low Rise
00101520150000	1701 THORNTON AV	95811	Traditional Center	Employment Center Low Rise
00101520170000	1615 THORNTON AV	95811	Traditional Center	Employment Center Low Rise
00101520180000	221 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101520190000	235 N 16TH ST	95811	Traditional Center	Employment Center Low Rise
00101520210000	1624 N C ST	95811	Traditional Center	Employment Center Low Rise
00101520220000	1625 THORNTON AV	95811	Traditional Center	Employment Center Low Rise
00101810170000	450 BERCUT DR	95811	Urban Center Low	Urban Center High
00101810190000	300 BERCUT DR	95811	Urban Center Low	Urban Center High
00101810240000	400 BERCUT DR	95811	Urban Center Low	Urban Center High
00101810250000	350 BERCUT DR	95811	Urban Center Low	Urban Center High
00101810260000	300 BERCUT DR	95811	Urban Center Low	Urban Center High
00102000120000	424 N 5TH ST	95811	Urban Center Low	Urban Center Low, Urban Center High
00102000130000	0 RICHARDS BL	95811	Urban Center Low	Urban Center Low, Urban Center High
00102000340000	0 RICHARDS BL	95811	Urban Center Low	Urban Center Low, Urban Center High
00200200090000	0 N B ST	95811	Traditional Center	Employment Center Low Rise
00200200100000	1405 N B ST	95811	Traditional Center	Employment Center Low Rise
00200200120000	0 N B ST	95811	Traditional Center	Employment Center Low Rise
00200200150000	1317 N B ST	95811	Traditional Center	Employment Center Low Rise

Subject: River District Specific Plan (M09-003)

January 13, 2011

00200200160000	1400 N C ST	95811	Traditional Center	Employment Center Low Rise
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Subject: River District Specific Plan (M09-003)

January 13, 2011

Attachment 4

**[Rescind RBAP, Amend 2030 General Plan Circulation Element, and Adopt the River District Specific Plan and Infrastructure Financing Plan - City Council Resolution]**

**RESOLUTION NO. 2011-**

Adopted by the Sacramento City Council

**RESOLUTION RESCINDING THE RICHARDS BOULEVARD AREA PLAN (RBAP) AND 1994 FACILITY ELEMENT, AMENDING THE GENERAL PLAN CIRCULATION ELEMENT, AND ADOPTING THE RIVER DISTRICT SPECIFIC PLAN AND THE INFRASTRUCTURE FINANCING PLAN WHICH ESTABLISH POLICIES TO GUIDE THE LOCATION, INTENSITY, AND CHARACTER OF LAND USES; CIRCULATION PATTERN AND NECESSARY INFRASTRUCTURE IMPROVEMENTS TO SUPPORT DEVELOPMENT FOR THE REDEVELOPMENT OF THE RIVER DISTRICT AREA. (M09-003)**

**BACKGROUND**

- A. On December 13, 1994, the City Council adopted the Railyards Specific Plan, Richards Boulevard Area Plan, and the Facility Element for the Railyards and Richards Boulevard Planning Areas. (Resolution 94-736 and Resolution 96-645)
- B. On December 11, 2007 the City Council directed staff to proceed in updating the Facility Element and the Richards Boulevard Area Plan. (Resolution 2007-915)
- C. On April 9, 2009 and August 12, 2010, the City Planning Commission participated in public hearings on the River District Specific Plan.
- D. On January 13, 2011, the City Planning Commission held a noticed public hearing on the River District Specific Plan in accordance with Government Code Section 65353 and 65453, received and considered evidence, and forwarded to the City Council a recommendation to approve rescinding the RBAP, amending the Circulation Element of the General Plan, and adopt the River District Specific Plan and Public Financing Facility Element.
- E. On February 1, 2011, the City Council conducted a noticed public hearing in accordance with Government Code Sections 65355 and 65453, and received and considered evidence concerning the Sacramento River District Specific Plan and Public Financing Facility Element.

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:**

Subject: River District Specific Plan (M09-003)

January 13, 2011

Section 1. Based on the verbal and documentary evidence received at the hearing held on February 1, 2011, the City Council hereby rescinds the Richards Boulevard Area Plan, amends the General Plan Circulation Element, and adopts the River District Specific Plan and Facility Element attached as Exhibit A.

Section 2. Exhibit A and Exhibit B are part of this Resolution.

Table of Contents:

Exhibit A – River District Specific Plan

Exhibit B – River District Infrastructure Financing Plan

Subject: River District Specific Plan (M09-003)

January 13, 2011

Exhibit A – River District Specific Plan

Please find the document at the following link:

<http://www.cityofsacramento.org/dsd/projects/riverdistrict.cfm>

Subject: River District Specific Plan (M09-003)

January 13, 2011

Exhibit B – River District Public Financing Facility Element

Please find the document at the following link:

<http://www.cityofsacramento.org/dsd/projects/riverdistrict.cfm>

**[Amendment to the Railyards Plan for Operation of 5<sup>th</sup> and 7<sup>th</sup> Streets]****RESOLUTION NO. 2011-**

Adopted by the Sacramento City Council

**AMENDING THE RAILYARDS SPECIFIC PLAN TO CHANGE THE PLANNED  
FUTURE OPERATION OF 5<sup>TH</sup> AND 7<sup>TH</sup> STREETS****BACKGROUND**

A. On December 11, 2007, the City Council adopted the Railyards Specific Plan, which provided for the conversion of 5<sup>th</sup> Street and 7<sup>th</sup> Street from two way operation to one way operation after completion of the initial phase of development based on the traffic study that was contained in the Environmental Impact Report for the Railyards Specific Plan (Resolution No. 2007-903).

B. Also on December 11, 2007, the City Council adopted a resolution directing staff to proceed in updating the Facility Element and the Richards Boulevard Area Plan, so that the future operation of 5<sup>th</sup> Street and 7<sup>th</sup> Street could be further studied (Resolution No. 2007-915).

C. The River District Specific Plan and Environmental Impact Report, which is to be adopted concurrently with this resolution, is a comprehensive update of the Richards Boulevard Area Plan and Facility Element. The traffic study that is contained in the Environmental Impact Report for the River District Specific Plan analyzed the change in the street system which connects the Railyards and the River District specific plan areas, including the continued operation of 5<sup>th</sup> Street and 7<sup>th</sup> Street as two way streets in the future.

D. On April 9, 2009 and August 12, 2010, the City Planning Commission held public hearings on the River District Specific Plan.

E. On January 13, 2011, the City Planning Commission held a noticed public hearing on the River District Specific Plan in accordance with Government Code Section 65353 and 65453, received and considered evidence, and forwarded to the City Council a recommendation to approve adoption of the River District Specific Plan and amending the Railyards Specific Plan to change the future operation of 5<sup>th</sup> and 7<sup>th</sup> Streets so that they remain as two way streets within the Railyards plan area to provide a better circulation system to serve both plan areas.

F. On February 1, 2011, the City Council conducted a noticed public hearing in accordance with Government Code Sections 65355 and 65453, and received and

Subject: River District Specific Plan (M09-003)

January 13, 2011

considered evidence concerning the River District Specific Plan and the proposed amendment to the Railyards Specific Plan.

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:**

Section 1. The Railyards Specific Plan is hereby amended so that the operation of 5<sup>th</sup> Street and 7<sup>th</sup> Street shall remain as two way (two lane) streets after the initial phase of development and shall not be converted into one way (three lane) operations. The roadway right of way widths as set forth in the Railyards Specific Plan and tentative map shall remain unchanged to accommodate medians and turn lanes along each street and the light rail tracks along 7<sup>th</sup> Street as shown in the street sections in Exhibit A.

Section 2. Exhibit A is part of this resolution.

Exhibit A: Updated Street Sections for 5<sup>th</sup> and 7<sup>th</sup> Streets

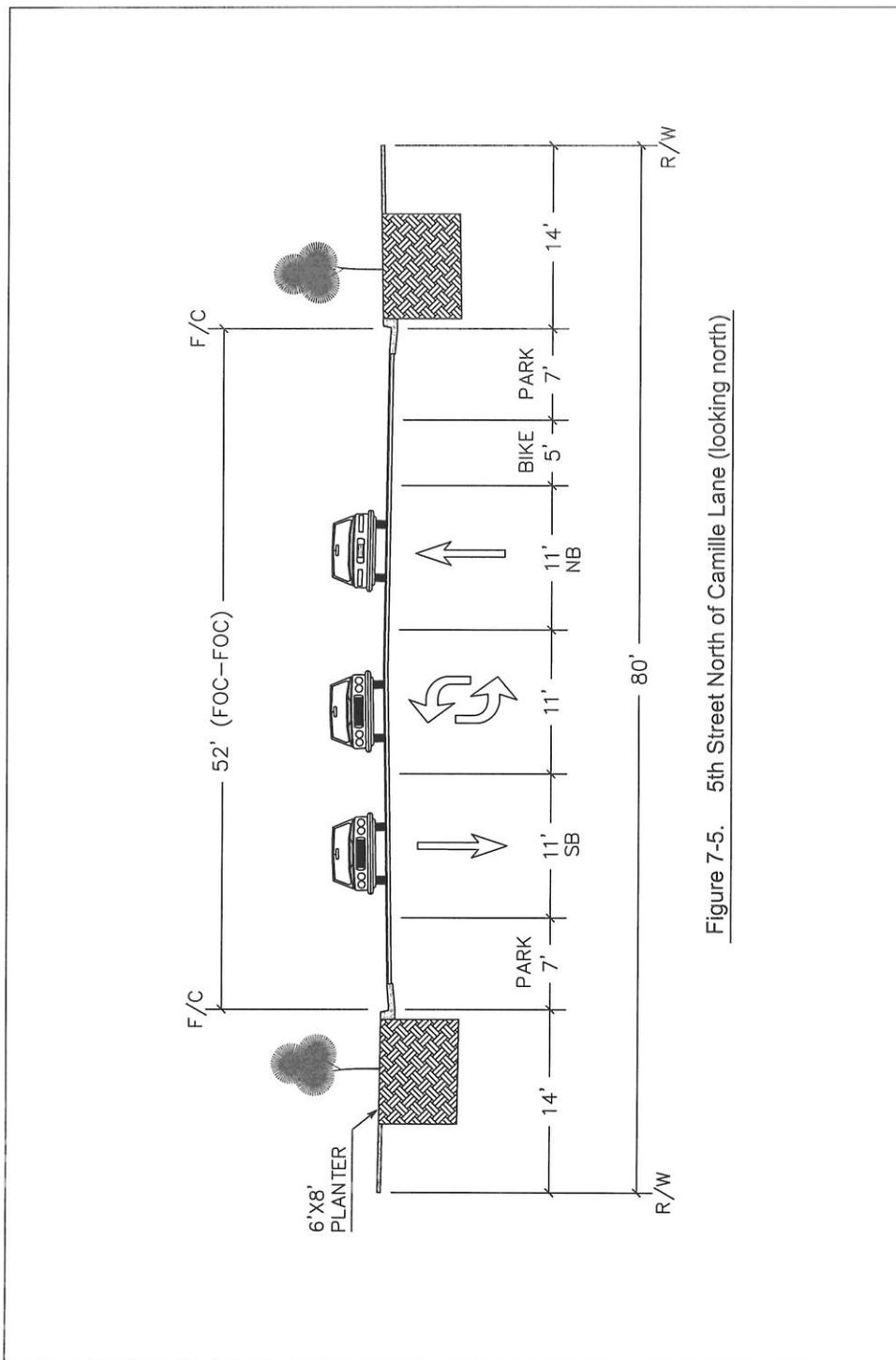


Figure 7-5. 5th Street North of Camille Lane (looking north)

THE RAILYARDS  
TYPICAL STREET CROSS-SECTIONS  
5TH STREET (NORTH OF CAMILLE LANE)  
Figure 7-5



Date: Dec. 28, 2010 2:50 pm  
By: P. Sanchez

Exhibit B: Updated Street Sections for 5<sup>th</sup> and 7<sup>th</sup> Streets

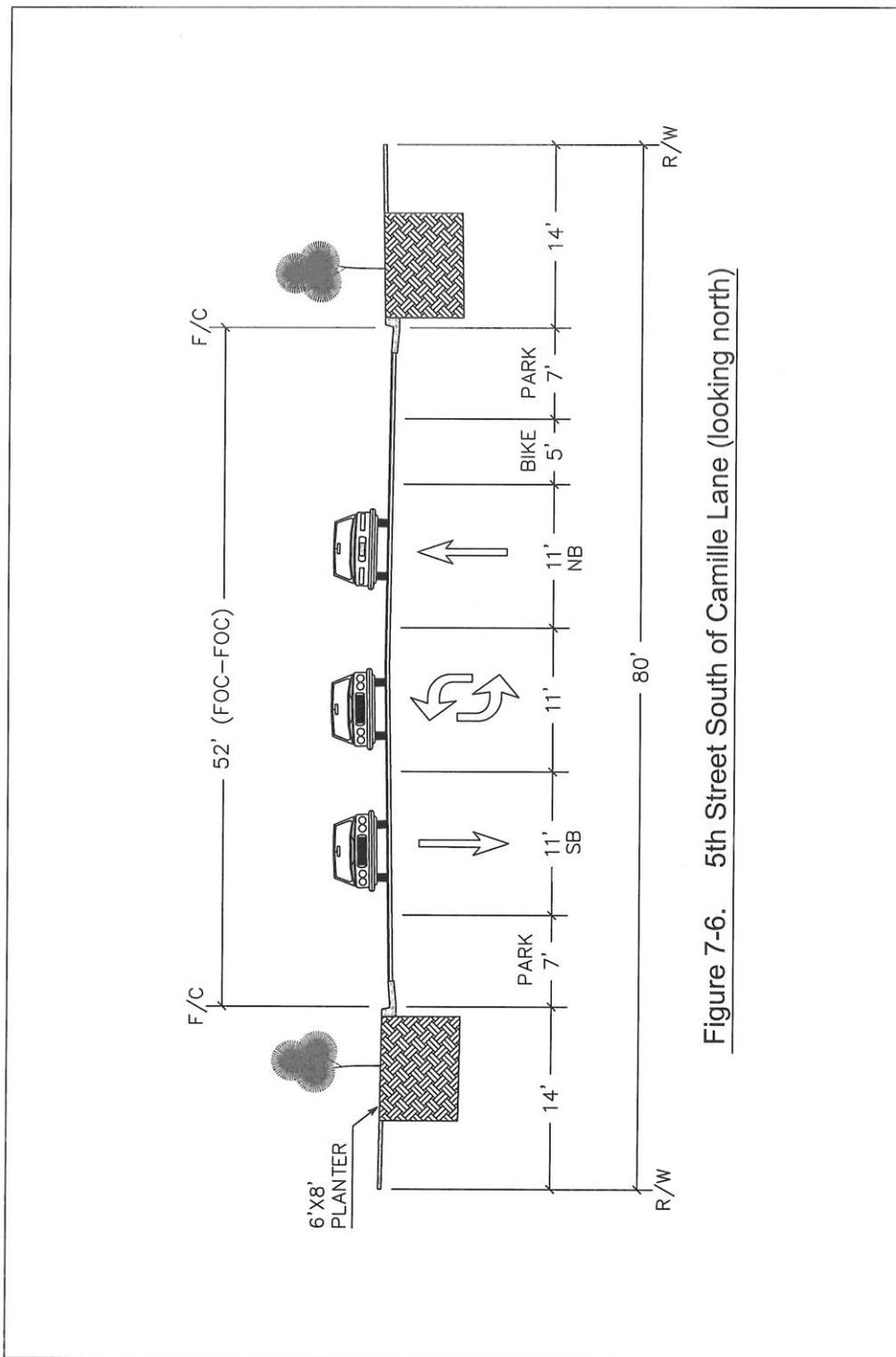


Figure 7-6. 5th Street South of Camille Lane (looking north)

**THE RAILYARDS**  
 TYPICAL STREET CROSS-SECTIONS  
 5TH STREET (SOUTH OF CAMILLE LANE)

Figure 7-6

Department of **TRANSPORTATION**  
 City of Sacramento  
 Date: Dec. 28, 2010 2:52 pm By: P. Sanchez

Exhibit C: Updated Street Sections for 5<sup>th</sup> and 7<sup>th</sup> Streets

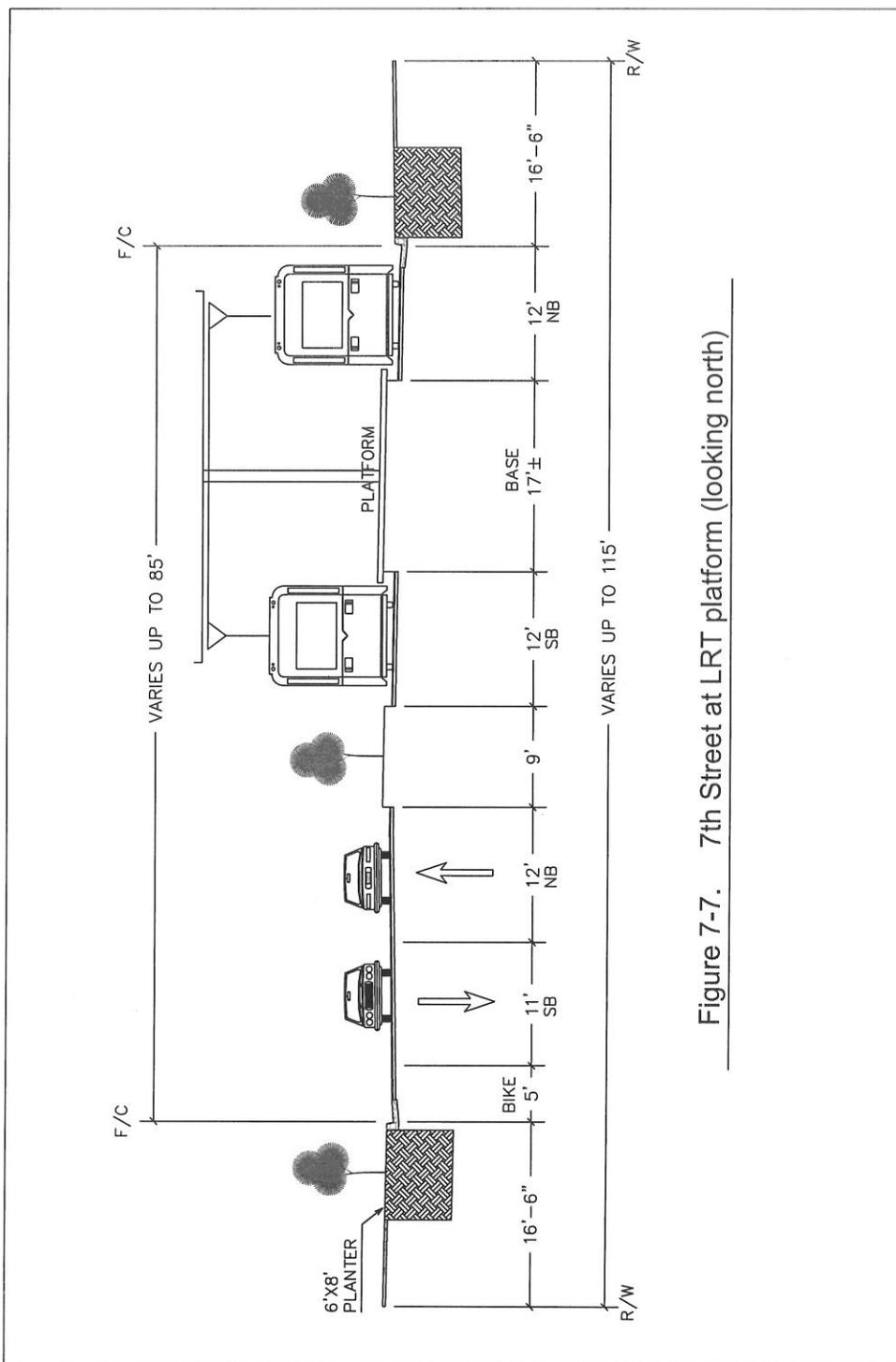


Figure 7-7. 7th Street at LRT platform (looking north)

**THE RAILYARDS**  
 TYPICAL STREET CROSS-SECTIONS  
 7TH STREET (AT LRT PLATFORM)

Figure 7-7

Date: Dec. 28, 2010 2:52 pm  
 By: P. Sanchez



Exhibit D: Updated Street Sections for 5<sup>th</sup> and 7<sup>th</sup> Streets

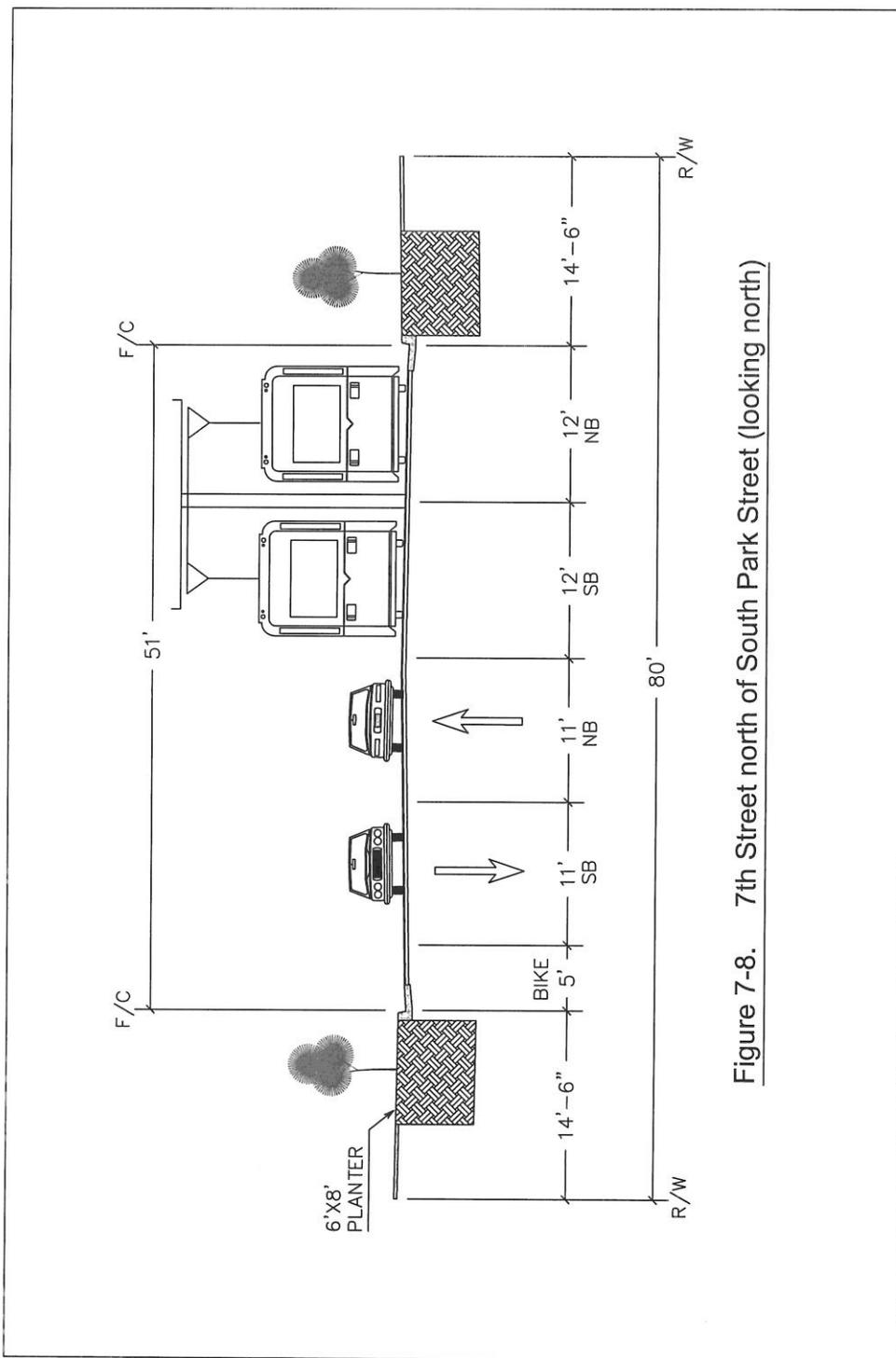


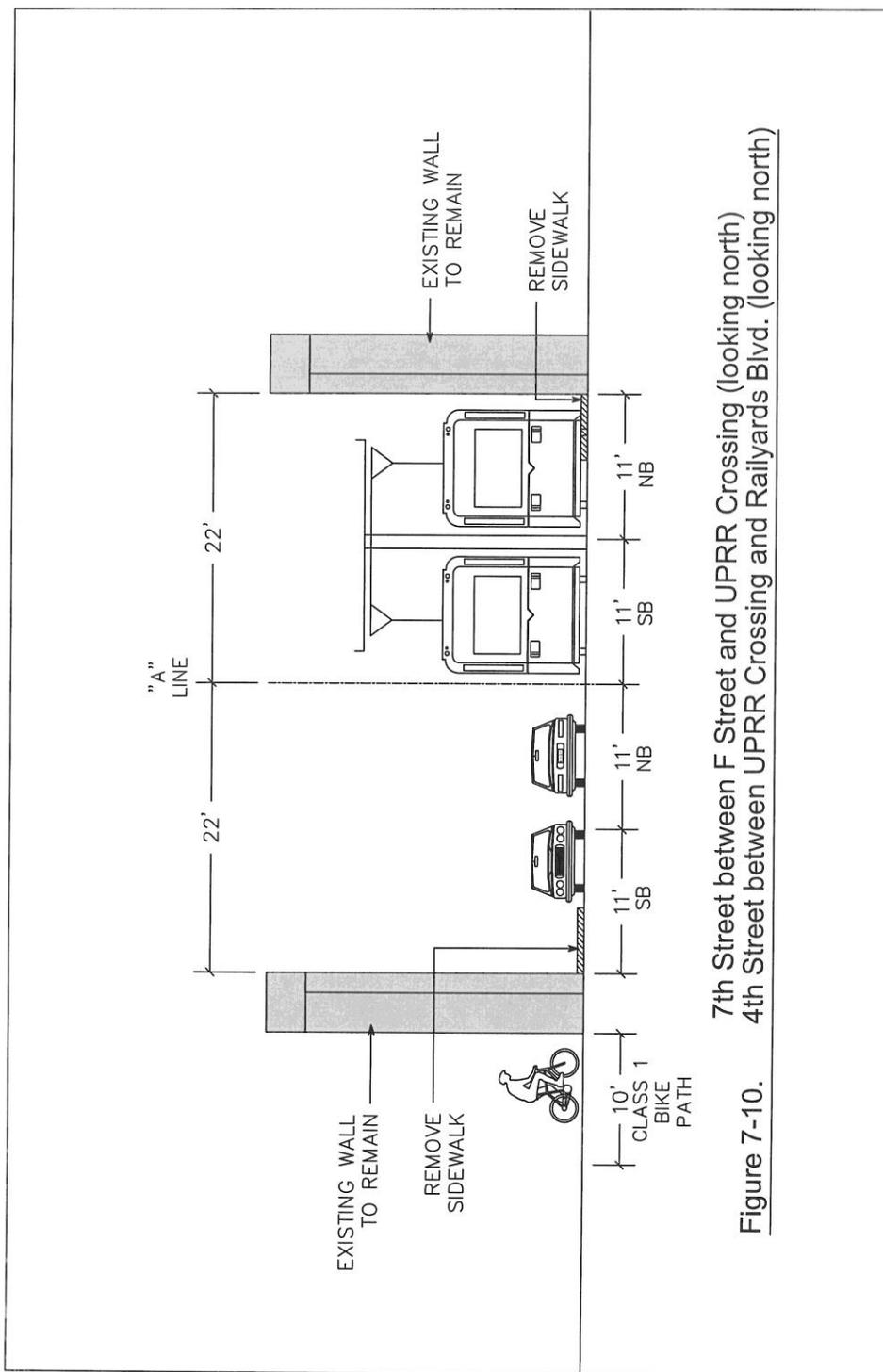
Figure 7-8. 7th Street north of South Park Street (looking north)

THE RAILYARDS  
TYPICAL STREET CROSS-SECTIONS  
7TH STREET (NORTH OF SOUTH PARK STREET)  
Figure 7-8



Date: Dec. 28, 2010 2:53 pm  
By: P. Sanchez

Exhibit E: Updated Street Sections for 5<sup>th</sup> and 7<sup>th</sup> Streets



7th Street between F Street and UPRR Crossing (looking north)  
4th Street between UPRR Crossing and Railyards Blvd. (looking north)

THE RAILYARDS  
TYPICAL STREET CROSS-SECTIONS  
7TH STREET BETWEEN F ST AND UPRR CROSSING  
4TH STREET BETWEEN UPRR CROSSING AND RAILYARDS BL.

Figure 7-10



Date: Dec. 28, 2010 4:13 pm By: P. Sanchez

Subject: River District Specific Plan (M09-003)

January 13, 2011

Attachment 6

**[Amend the Bikeway Master Plan - City Council Resolution]**

**RESOLUTION NO. 2011-**

Adopted by the Sacramento City Council

**AMENDING THE CITY'S BIKEWAY MASTER PLAN TO INCORPORATE THE BIKEWAY NETWORK IN THE SACRAMENTO RIVER DISTRICT SPECIFIC PLAN. (M09-003)**

**BACKGROUND**

A. On April 9, 2009 and August 12, 2010, the City Planning Commission participated in public hearings on the River District Specific Plan.

B. On January 13, 2011, the City Planning Commission held a public hearing on the River District Specific Plan, for which notice was given pursuant to Sacramento City Code Section 17.200.010 (C) (2) (a and c) (publication and mail 500 feet), and received and considered evidence, and forwarded to the City Council a recommendation to approve the River District Specific Plan Effort.

E. On February 1, 2011, the City Council conducted a noticed public hearing in accordance with Government Code Sections 65355 and 65453, and received and considered evidence regarding the adoption of the Sacramento River District Specific Plan, which includes changes to the City's Bikeway Master Plan.

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:**

Section 1. Based on the verbal and documentary evidence received at the hearings on the River District Specific Plan, the City Council finds that amending the City Bikeway Master Plan to modify the bikeway network in the River District is consistent with the City's General Plan goals to:

1. Create and maintain a safe, comprehensive, and integrated bicycle system and support facilities throughout the city that encourages bicycling that is accessible to all.
2. Promote bicycling as a feasible transportation alternative which conserves energy, improves air quality, reduces traffic congestion, and improves public health.

Section 2. The Environmental Impact Report and Mitigation Monitoring Program for the River District Specific Plan, which included the proposed changes to the City's

Subject: River District Specific Plan (M09-003)

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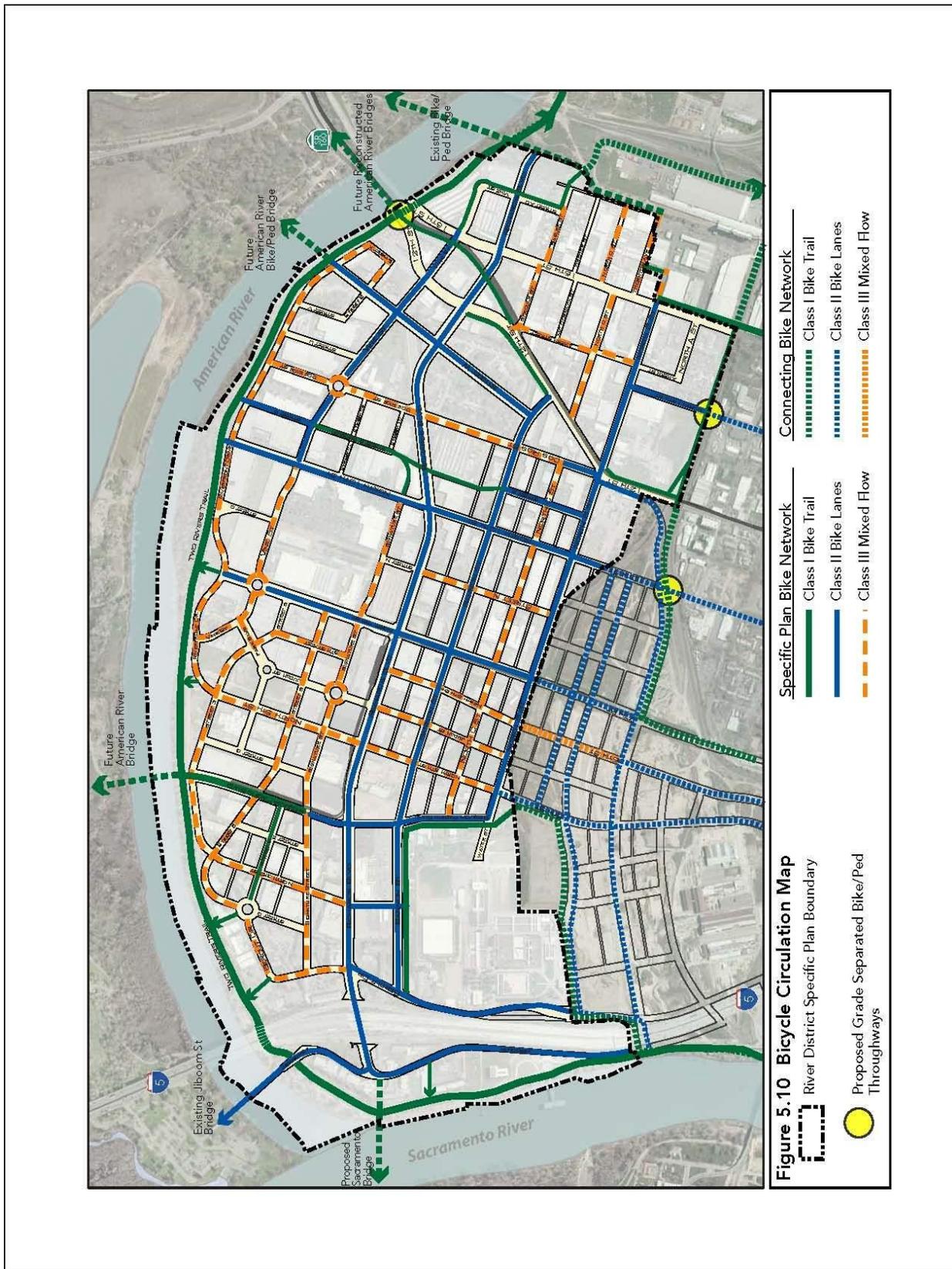
Bikeway Master Plan, have been adopted by resolution as of the same date set out above.

Section 3. City Council hereby amends the City's Bikeway Master Plan to modify the River District bikeway network as shown in Exhibit A.

Table of Contents:

Exhibit A – Sacramento River District Bikeway Plan

Exhibit A – Sacramento River District Bikeway Plan



Subject: River District Specific Plan (M09-003)

January 13, 2011

Attachment 7

**[Adopt Design Guidelines - City Council Resolution]****RESOLUTION NO. 2011- \_\_\_\_**

Adopted by the Sacramento City Council

**ADOPTING THE RIVER DISTRICT DESIGN GUIDELINES  
FOR THE RIVER DISTRICT DESIGN REVIEW DISTRICT****BACKGROUND**

- A. The River District Design Guidelines were prepared in conjunction with the River District Specific Plan. A noticed public hearing was held to review the River District Design Guidelines and establishment of the River District Design Review District to accept public comments and to recommend approval of the new design review district and adoption of the design guidelines by the Design Commission.
- B. The River District Design Guidelines contain architectural and streetscape design standards to be applied to projects located within the River District Design Review District and Specific Plan boundaries.
- C. The River District Design Guidelines provide design guidance for private and public projects within the River District Design Review District in a manner that will allow for transit-oriented and mixed use development while preserving and enhancing the qualities that would contribute to a vibrant, economically robust and pedestrian- and transit- friendly urban area.
- D. The River District Design Guidelines include both design principles and guidelines that distinguish between mandatory and advisory provisions that will be used by city staff and the Design Commission, Preservation Commission and Planning Commission in determining the appropriateness of any proposed building or structure, or the alteration of an existing building or structure located within the River District Design Review District and the North 16<sup>th</sup> Street Historic District.
- E. The River District Design Guidelines are consistent with the River District Specific Plan, the Central City Community Plan and the 2030 General Plan.
- F. On January 12, 2011, the Design Commission conducted a public hearing for which notice was given pursuant Sacramento City Code Section 17.132.60 and forwarded to the City Council a recommendation to approve the River District Design Guidelines for application within the River District Design Review District.

Subject: River District Specific Plan (M09-003)

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- G. On February 1, 2011, the City Council conducted a public hearing, for which notice was given pursuant Sacramento City Code Section 17.132.60, and received and considered evidence concerning adoption of the River District Design Guidelines.

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:**

Section 1. Based on the verbal and documentary evidence received at the hearing held on February 1, 2011, the City Council hereby adopts the River District Design Guidelines attached as Exhibit A for application within the River District Design Review District.

Table of Contents:

Exhibit A - River District Design Guidelines

Subject: River District Specific Plan (M09-003)

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**Exhibit A: River District Design Review Guidelines**

Please find the document at the following link:

<http://www.cityofsacramento.org/dsd/projects/riverdistrict.cfm>

Subject: River District Specific Plan (M09-003)

January 13, 2011

Attachment 8

**[Approve Water Supply Assessment - City Council Resolution]****RESOLUTION NO. 2011-**

Adopted by the Sacramento City Council

**APPROVING THE WATER SUPPLY ASSESSMENT REPORT FOR THE  
SACRAMENTO RIVER DISTRICT PROJECT (M09-003)****BACKGROUND**

A. State law requires a water supply and demand analysis (Water Supply Assessment) for development projects of a certain size or type, which would include the Sacramento River District Plan Effort, based on the City's Urban Water Management Plan.

B. The Water Supply Assessment evaluates projected water supplies, determined to be available by the City for the project during normal, single dry and multiple dry years over a 20 year period. The City prepared the Water Supply Assessment for the River District Plan in July of 2010, which was set out as Appendix F of the River District Specific Plan Draft Environmental Impact Report dated July of 2010.

C. On January 13, 2011, the City Planning Commission held a noticed public hearing on the River District Specific Plan in accordance with Government Code Section 65353 and 65453, received and considered evidence, and forwarded to the City Council a recommendation to adopt the River District Specific Plan and Public Financing Facility Element.

E. On February 1, 2011, the City Council conducted a noticed public hearing in accordance with Government Code Sections 65355 and 65453, and received and considered evidence concerning the Sacramento River District Specific Plan and Public Financing Facility Element.

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL  
RESOLVES AS FOLLOWS:**

Section 1. The Environmental Impact Report and Mitigation Monitoring Program for the Sacramento River District, which included all of the impacts associated with the adoption and implementation of the proposed Specific Plan and approval of the Sacramento River District Plan Effort, have been adopted by resolution as of the same date set out above.

Subject: River District Specific Plan (M09-003)

January 13, 2011

Section 2. Based on the verbal and documentary evidence received at the hearings on the Sacramento River District Plan, Environmental Impact Report, and the Sacramento River District Effort, the City Council approves the Water Supply Assessment Report for the Sacramento River District Project and approves the SB 210/SB 221 Water Supply Assessment and Certification Form attached as Exhibit A.

Table of Contents:

Exhibit A – Sacramento River District Project Water Supply Assessment and Certification Form – 3 pages

## Exhibit A: Water Supply Assessment and Certification Form

**City of Sacramento  
SB 610/SB 221 Water Supply Assessment and Certification Form**

This form may be used to complete water supply assessments for projects located in an area covered by the City's most recent Urban Water Management Plan.

Note: Please do not use this form if the projected water demand for your project area was not included in the City's latest Urban Water Management Plan. To review the City's Urban Water Management Plan, please visit:  
<http://www.cityofsacramento.org/utilities/urbanwater/index.html>

**Project:** River District

**Date:** 6/11/2010

**Project Applicant (Name of Company):** City of Sacramento

**Applicant Contact (Name of Individual):** Greg Bitter

**Phone Number:** (916) 808-7816

**E-mail:** [gbitter@cityofsacramento.org](mailto:gbitter@cityofsacramento.org)

**Address:** 300 Richards Blvd, Sacramento, CA 95811

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**Project Applicant to fill in the following:**

1. Does the project include:

Type of Development	Yes	No
A proposed residential development of 500 or more dwelling units	X	
A shopping Center employing more than 1,000 persons or having more than 500,000 square feet?		X
A Commercial Office building employing more than 1,000 persons or having more than 250,000 square feet?	X	
A proposed hotel or motel, or both, having more that 500 rooms	X	
A proposed industrial, manufacturing, or processing plant or industrial park planned to house more than 1,000 persons, occupying more than 40 acres of land, or having more than 650,000 square feet of floor area		X
A mixed use project that includes one or more of the projects specified above		X
A project that would demand an amount of water equivalent to, or greater than, the water required by a 500 dwelling unit project	X	

If the answer is no to all of the above, a water supply assessment is not required for the project.

2. Is the projected water demand for the project location included in the City's 2005 Urban Water Management Plan, adopted November 14, 2006?

Yes:  X

No:

If the answer is no, you cannot use this form. Please refer to the requirements of SB 610 for preparing a water supply assessment.

3. Please fill in the project demands below:

Type of Development	Demand Factor (acre feet per acre)	Proposed Development		Current Zoning	
		Acres	Total Demand	Acres	Total Demand
Residential - Low and Medium Density	3.60	5.00	18	4.50	16.2
Residential - High Density	4.00	139.00	556	2.34	9.36
Commercial/Retail	3.00	10.33	30.99	5.74	17.22
Office	3.00	16.58	49.74	19.51	58.53
Warehouse/Industrial	4.00	0	0	114.78	459.12
Hotels	4.00	22.86	91.44	12.08	48.32
Parks and Recreation	4.20	31.0	130.2	16	67.2
<b>Subtotal</b>			876.37		675.95
Losses - 7.5% of subtotal			65.73		50.70
<b>Total Demand</b>			942.10		726.65

4. Required Elements of Water Supply Assessment (Government Code § 10910)

A. Water supply entitlements, water rights or water service contracts (Gov't Code § 10910(d)):

The City's water supply entitlements, water rights and water service contract are identified and discussed in the Urban Water Management Plan, Chapters 4, 5 and 6.

All infrastructure necessary to deliver a water supply to the project is in place, excepting any distribution facilities required to be constructed and financed by the project applicant: Yes:   No:  X

B. Identification of other sources of water supply if no water has been received under City's existing entitlements, water rights or water service contracts (Gov't Code § 10910(e)):

Not applicable.

C. Information and analysis pertaining to groundwater supply (Gov't Code § 10910(f)):

Addressed by Urban Water Management Plan, Chapters 4, 5 and 6.

**Verification of Water Supply**  
(for residential development of more than 500 dwelling units)

Based on the City's most recent Urban Water Management Plan, are there sufficient water supplies for the project during normal, single dry and multiple dry years over a 20 year period?

Yes:  No:

By: Jim Peifer

Title: Senior Engineer

Date: 7/1/2010

**This box to be filled in by the City**

Distribution:

Applicant  
 Development Services Department (Org: 4913) – Assigned Planner: \_\_\_\_\_  
 Utilities Department (Org: 3334) - Development Review (Robert Thuang)  
 Utilities Department (Org: 3332) - Capital Improvements (Jim Peifer)

Subject: River District Specific Plan (M09-003)

January 13, 2011

**[Rescind Discovery Center PUD Guidelines - City Council Resolution]**

**RESOLUTION NO. 2011-**

Adopted by the Sacramento City Council

**RESOLUTION RESCINDING THE DISCOVERY CENTRE PLANNED UNIT DEVELOPMENT GUIDELINES. (M09-003)**

**BACKGROUND**

- A. On November 5, 1998, the City Council adopted the Discovery Center PUD Guidelines. (Resolution 98-544)
- B. On November 18, 2010, the City Planning Commission held a noticed public hearing on the River District Specific Plan in accordance with Government Code Section 65353 and 65453, received and considered evidence, and forwarded to the City Council a recommendation to approve rescinding the Discovery Centre PUD Guidelines and Schematic Plan.
- C. On January 11, 2011, the City Council conducted a noticed public hearing in accordance with Government Code Sections 65355 and 65453, and received and considered evidence for rescinding the Discovery Centre PUD Guidelines.

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:**

Section 1. Based on the verbal and documentary evidence received at the hearings on the River District Specific Plan, the City Council finds that rescinding the Discovery Centre PUD Guidelines and Schematic Plan in the River District is consistent with the City's General Plan goals to:

1. Strive to ensure that the City-owned buildings, sites, and infrastructure are designed to be compatible in scale, mass, character, and architecture with the district or neighborhood in which they are located. (LU 8.1.6)
2. Encourage public/private partnerships when developing surplus City properties to enhance the surrounding community and provide a source of revenue to fund improvements to city service or facilities. (LU 8.1.11)

Section 2. The Environmental Impact Report and Mitigation Monitoring Program for the River District Specific Plan, which included the rescinding of the Discovery Centre PUD Guidelines and Schematic Plan, have been adopted by resolution as of the same date set out above.

Section 3. City Council hereby rescinds the Discovery Centre PUD Guidelines and Schematic Plan.

Subject: River District Specific Plan (M09-003)

January 13, 2011

**New Special Planning District – DRAFT City Council**

**ORDINANCE NO.**

Adopted by the Sacramento City Council

Date Adopted

**AN ORDINANCE REPEALING AND ADDING CHAPTER 17.120 TO, AND AMENDING SECTIONS 17.20.030, 17.24.050, AND 17.134.430 OF, TITLE 17 OF THE SACRAMENTO CITY CODE (THE ZONING CODE) RELATING TO THE RIVER DISTRICT SPECIAL PLANNING DISTRICT (M09-003)**

**BE IT ENACTED BY THE COUNCIL OF THE CITY OF SACRAMENTO:**

**SECTION 1.** Chapter 17.120 of Title 17 of the Sacramento City Code (the Zoning Code) is repealed.

**SECTION 2.** Chapter 17.120 is added to Title 17 of the Sacramento City Code (the Zoning Code) to read as follows:

**Chapter 17.120 River District Special Planning District**

**17.120.010 Purpose and intent.**

A. The River District Special Planning District (SPD) establishes procedures to implement the policies and development standards of the River District Specific Plan. The River District Specific Plan designates the land uses within the boundaries of the River District Specific Plan area and is the primary policy and regulatory document used to guide development of the properties within the River District Specific Plan area.

B. The goals of the River District SPD are as follows:

1. Establish a greater mix of land uses and intensities to attract private investment;

2. Provide the opportunity for reuse and rehabilitation of heavy commercial and industrial uses to take advantage of the light rail facilities in the area and to reduce the number of obsolete and underutilized buildings and sites;

3. Allow for the retention and continued operation of industrial and service oriented uses;
4. Provide for improved circulation, infrastructure, and community facilities that will serve existing and future needs within the area;
5. Provide for the future creation of a significant residential population within the River District area, as industrial uses relocate or are replaced, to achieve the housing objectives of the General Plan and Central City Community Plan and provide a jobs/housing balance for future office growth;
6. Provide for the intensification of commercial and office uses within close proximity to the planned and existing light rail stations and Interstate 5;
7. Discourage uses that contribute to visual or economic blight;
8. Encourage the preservation of historic structures; and
9. Promote aesthetic improvements to the area by implementing development standards and design guidelines.

#### **17.120.020 River District SPD boundaries.**

River District SPD consists of approximately 773 acres of land within the River District Specific Plan area and is generally bounded by the Sacramento River on the west, the American River on the north, the Sacramento Railyards on the south, and 18th Street on the east. The map in Exhibit A at the end of this chapter shows the boundaries of the River District SPD.

#### **17.120.030 River District special regulations.**

Development in the River District SPD shall be subject to the regulations and development standards set forth in this chapter in addition to the regulations of this title and code. If a conflict between the provisions of this chapter and other provisions of this title and code occurs, the provisions of this chapter shall prevail.

#### **17.120.040 Uses and development standards—General.**

- A. Allowed Uses and Development Standards.

The allowed uses and specific development standards for each land use zone in the River District SPD are set forth in this chapter.

- B. Notice of Industrial Uses.

To avoid conflicts and incompatibility between existing industrial uses and new development in the River District SPD, the City, as a condition of approval of any application for new development, may require the owners and developers of the new development to provide written notice of the presence of existing industrial uses, and potential impacts associated with the continued use and operation of such industrial uses, to tenants and occupants of the new development.

C. Design Review and Preservation Review.

The River District SPD is located within the River District Design Review District and includes the North 16<sup>th</sup> Street Historic District. All development in the River District SPD, including without limitation all uses allowed by right as well as the repair and reconstruction of nonconforming buildings and structures under Section 17.120.170, is subject to design review under Chapter 17.132 or preservation review under Chapter 17.134.

**17.120.050 Single- and two-family R-1B zone.**

A. Allowed Uses.

Uses permitted in the R-1B zone under this title outside of the River District SPD shall be allowed in the R-1B zone within the River District SPD. If this title requires the approval of a special permit or other discretionary entitlement, or imposes other restrictions or requirements on the establishment of a particular use in the R-1B zone outside of the River District SPD, approval of the same discretionary entitlement and compliance with the same restrictions or requirements shall be required to establish the use in the R-1B zone within the River District SPD.

B. Development Standards.

Except as provided below, development in the R-1B zone in the River District SPD shall be subject to the same development standards that govern development in the R-1B zone outside of the River District SPD.

1. Height Standards.

The height standards for the R-1B zone in the River District SPD are set out in Exhibit B at the end of this chapter.

**17.120.060 Multi-family R-3A zone.**

A. Allowed Uses.

Uses permitted in the R-3A zone under this title outside of the River District SPD shall be allowed in the R-3A zone within the River District SPD. If this title requires the

approval of a special permit or other discretionary entitlement, or imposes other restrictions or requirements on the establishment of a particular use in the R-3A zone outside of the River District SPD, approval of the same discretionary entitlement and compliance with the same restrictions or requirements shall be required to establish the use in the R-3A zone within the River District SPD.

B. Development Standards.

Except as provided below, development in the R-3A zone in the River District SPD shall be subject to the same development standards that govern development in the R-3A zone outside of the River District SPD.

1. Residential Density.

The permitted density in the R-3A zone within the River District SPD shall be the same as the permitted density in the R-3A zone outside of the River District, except that a higher density may be approved upon the issuance of a planning commission special permit pursuant to and subject to the findings required by Chapter 17.212; provided, that the higher density is consistent with the applicable density range established by the city's General Plan.

2. Height Standards.

The height standards for the R-3A zone in the River District SPD are set out in Exhibit B at the end of this chapter. Requests to vary a height standard shall be heard and decided under Section 17.120.130 and shall not be subject to footnote (8) of Section 17.60.030.

3. Open Space Requirements.

a. Open space areas specifically designed for recreation or passive enjoyment of the outdoors are required for new residential development.

b. A minimum of 80 square feet of usable common open space per residential unit is required. Usable common open space may include courtyard, garden, recreational, or similar common areas.

c. A minimum of 50 square feet of usable private open space per residential unit is required. Usable private open space shall be designed for the exclusive use of the associated unit and may include decks, balconies, and patios. Private usable open space shall be directly accessible from the associated unit.

d. Open space requirements shall be satisfied onsite; provided that the planning commission may approve a special permit pursuant to and subject to the findings required by Chapter 17.212 to allow not more than 20% of the total required open space offsite. Required offsite open space shall be located in the River District SPD. In approving the special permit, the planning commission shall specify how the

remaining open space to be provided on site shall be allocated between usable common open space and usable private open space.

#### **17.120.070 Multi-family R-5 zone.**

##### **A. Allowed Uses.**

1. Uses permitted in the R-5 zone under this title outside of the River District SPD shall be allowed in the R-5 zone within the River District SPD.

2. If this title requires the approval of a special permit or other discretionary entitlement, or imposes other restrictions or requirements on the establishment of a particular use in the R-5 zone outside of the River District SPD, approval of the same discretionary entitlement and compliance with the same restrictions or requirements shall be required to establish the use in the R-5 zone within the River District SPD, except the following uses are permitted in the R-5 zone within the River District SPD and shall not be subject to footnote (7) of Section 17.24.050:

- a. Offices;
- b. Medical clinic or office;
- c. Retail, Pedestrian Oriented, and Personal Service Uses. All of the uses listed in Table 1 of Section 17.96.070 shall be permitted uses, except that bars shall be subject to footnote (40) of Section 17.24.050.

##### **B. Development Standards.**

Except as provided below, development in the R-5 zone in the River District SPD shall be subject to the same development standards that govern development in the R-5 zone outside of the River District SPD.

##### **1. Height Standards.**

The height standards for the R-5 zone in the River District SPD are set out in Exhibit B at the end of this chapter. Requests to vary a height standard shall be heard and decided under Section 17.120.130 and shall not be subject to footnote (8) of Section 17.60.030.

##### **2. Open Space Requirements.**

- a. Office.

Subject: River District Specific Plan (M09-003)

January 13, 2011

i. Open space shall be provided for new office development at a ratio of one square foot of open space for every 15 square feet of the total square footage of the development.

ii. Open space shall be in the form of courtyards or public plazas.

iii. Open space requirements shall be satisfied onsite; provided, that the planning commission may approve a special permit pursuant to and subject to the findings required by Chapter 17.212 to allow not more than 20% of the required open space offsite. Required off-site open space shall be located within the River District SPD.

b. Residential.

i. Open space areas specifically designed for recreation or passive enjoyment of the outdoors are required for new residential development.

ii. A minimum of 80 square feet of usable common open space per residential unit is required. Usable common open space may include courtyard, garden, recreational, or similar common areas.

iii. A minimum of 50 square feet of usable private open space per residential unit is required. Usable private open space shall be designed for the exclusive use of the associated unit and may include decks, balconies, and patios. Usable private open space shall be directly accessible from the associated unit.

iv. Open space requirements shall be satisfied onsite; provided, that the planning commission may approve a special permit pursuant to and subject to the findings required by Chapter 17.212 to allow not more than 20% of the total required open space offsite. Required offsite open space shall be located in the River District SPD. In approving the special permit, the planning commission shall specify how the remaining open space to be provided on site shall be allocated between usable common open space and usable private open space.

3. Parking Requirements.

a. No off-street parking shall be required for retail, commercial service (including banks and beauty salons) athletic club/fitness center, or restaurant uses, if the use is a component of a residential project and does not exceed 20% of the total building square footage for the project or 9,600 square feet, whichever is less.

b. No off-street parking shall be required for restaurant outdoor seating located on private property.

#### **17.120.080 Residential mixed use RMX zone.**

A. Allowed Uses.

Uses permitted in the RMX zone under this title outside of the River District SPD shall be allowed in the RMX zone within the River District SPD. If this title requires the approval of a special permit or other discretionary entitlement, or imposes other restrictions or requirements on the establishment of a particular use in the RMX zone outside of the River District SPD, approval of the same discretionary entitlement and compliance with the same restrictions or requirements shall be required to establish the use in the RMX zone within the River District SPD.

B. Development Standards.

Except as provided below, development in the RMX zone in the River District SPD shall be subject to the same development standards that govern development in the RMX zone outside of the River District SPD.

1. Building Size and Lot Coverage.

Development in the RMX zone in the River District SPD shall not be subject to footnote (9) of Section 17.60.030. A planning commission special permit shall be required for any building to be constructed or expanded to exceed 40,000 square feet of gross floor area. A zoning administrator's special permit shall be required for nonresidential buildings to be constructed or expanded to exceed 10,000 square feet up to and including 40,000 square feet of gross floor area. Maximum lot coverage shall be 70%.

2. Height Standards.

The height standards for the RMX zone in the River District SPD are set out in Exhibit B at the end of this chapter. Requests to vary a height standard shall be heard and decided under Section 17.120.130 and shall not be subject to footnote (8) of Section 17.60.030.

3. Residential Density.

The maximum residential density shall be 100 dwelling units per net acre. A higher density may be approved upon the issuance of a planning commission special permit pursuant to and subject to the findings required by Chapter 17.212 and consistent with the applicable density range established by the city's General Plan.

4. Open Space Requirements.

a. Office.

i. Open space shall be provided for new office development at a ratio of one square foot of open space for every 15 square feet of the total square footage of the development.

- ii. Open space shall be in the form of courtyards or public plazas.
  - iii. Open space requirements shall be satisfied onsite; provided that the planning commission may approve a special permit pursuant to and subject to the findings required by Chapter 17.212 to allow not more than 20% of the required open space off-site. Required off-site open space shall be located within the River District SPD.
- b. Residential.
    - i. Open space areas specifically designed for recreation or passive enjoyment of the outdoors are required for new residential development.
    - ii. A minimum of 80 square feet of usable common open space per residential unit is required. Usable common open space may include courtyard, garden, recreational, or similar areas.
    - iii. A minimum of 50 square feet of usable private open space per residential unit is required. Usable private open space shall be designed for the exclusive use of the associated unit and may include decks, balconies, and patios. Private usable open space shall be directly accessible from the associated unit.
    - iv. Open space requirements shall be satisfied onsite; provided that the planning commission may approve a special permit pursuant to and subject to the findings required by Chapter 17.212 to allow not more than 20% of the total required open space offsite. Required offsite open space shall be located in the River District SPD. In approving the special permit, the planning commission shall specify how the remaining open space to be provided on site shall be allocated between usable common open space and usable private open space.

#### 5. Parking Requirements.

- a. No off-street parking shall be required for retail, commercial service (including banks and beauty salons), athletic club/fitness center, or restaurant uses if the use is a component of a residential project and does not exceed 20% of the total building square footage for the project or 9,600 square feet, whichever is less.
- b. No off street parking shall be required for restaurant outdoor seating located on private property.

#### **17.120.090 Office building OB zone.**

- A. Allowed Uses.

Subject: River District Specific Plan (M09-003)

January 13, 2011

1. Uses permitted in the OB zone under this title outside of the River District SPD shall be allowed in the OB zone within the River District SPD, except the following additional uses are allowed, subject to the restrictions and requirements stated for each use:

a. Vocational schools and dance/music/art/martial art schools, subject to the approval of a Planning Director Plan Review pursuant to and subject to the findings required by Chapter 17.220;

b. Apartments, subject to footnote (75) of Section 17.24.050;

c. Alternative ownership housing, subject to footnote (8) of Section 17.24.050.

2. If this title requires the approval of a special permit or other discretionary entitlement, or imposes other restrictions or requirements on the establishment of a particular use in the OB zone outside of the River District SPD, approval of the same discretionary entitlement and compliance with the same restrictions or requirements shall be required to establish the use in the OB zone within the River District SPD, except the following uses are permitted in the OB zone within the River District SPD and shall not be subject to footnote (18) and/or footnote (64) of Section 17.24.050 but shall be subject to the restrictions and requirements as stated for each use:

i. Offices;

ii. Medical clinic or office;

iii. Athletic club/fitness centers, subject to the approval of a Planning Director Plan Review pursuant to and subject to the findings required by Chapter 17.220;

iv. Retail stores exceeding 20% of the total square footage of the building, subject to a zoning administrator special permit pursuant to and subject to the findings required by Chapter 17.212.

B. Development Standards.

Except as provided below, development in the OB zone in the River District SPD shall be subject to the same development standards that govern development in the OB zone outside of the River District SPD.

1. Residential Density.

The maximum residential density shall be 100 dwelling units per net acre. A higher density may be approved upon the issuance of a planning commission special permit pursuant to and subject to the findings required by Chapter 17.212 and consistent with the applicable density range established by the city's General Plan.

## 2. Building Size and Lot Coverage.

Development in the OB zone in the River District SPD shall not be subject to footnote (9) of Section 17.60.030. A planning commission special permit shall be required for any building to be constructed or expanded to exceed 40,000 square feet of gross floor area. A zoning administrator's special permit shall be required for nonresidential buildings to be constructed or expanded to exceed 10,000 square feet up to and including 40,000 square feet of gross floor area. There is no maximum lot coverage standard.

## 3. Height and Setback Standards.

a. Front Setback. No minimum setback shall be required in the River District SPD area along Richards Boulevard except as required through the design review or preservation review under Chapters 17.132 and 17.134.

b. The height standards for the OB zone in the River District SPD are set out in Exhibit B at the end of this chapter. Requests to vary a height standard shall be heard and decided under Section 17.120.130 and shall not be subject to footnote (8) of Section 17.60.030.

## 4. Open Space Requirements.

### a. Office.

i. Open space shall be provided for new office development at a ratio of one square foot of open space for every 15 square feet of the total square footage of the development.

ii. Open space shall be in the form of courtyards or public plazas.

iii. Open space requirements shall be satisfied onsite; provided, that the planning commission may approve a special permit pursuant to and subject to the findings required by Chapter 17.212 to allow not more than 20% of the required open space off-site. Required off-site open space shall be located within the River District SPD.

### b. Residential.

i. Open space areas specifically designed for recreation or passive enjoyment of the outdoors are required for new residential development.

ii. A minimum of 80 square feet of usable common open space per residential unit is required. Usable common open space may include courtyard, garden, recreational, or similar common areas.

iii. A minimum of 50 square feet of usable private open space per residential unit is required. Usable private open space shall be designed for the exclusive use of the associated unit and may include decks, balconies, and patios. Usable private open space shall be directly accessible from the associated unit.

iv. Open space requirements shall be satisfied onsite; provided, that the planning commission may approve a special permit pursuant to and subject to the findings required by Chapter 17.212 to allow not more than 20% of the total required open space offsite. Required offsite open space shall be located in the River District SPD. In approving the special permit, the planning commission shall specify how the remaining open space to be provided on site shall be allocated between usable common open space and usable private open space.

#### 5. Parking Requirements.

a. No off-street vehicle parking shall be required for retail, commercial service (including banks and beauty salons), athletic club/fitness center, or restaurant uses if the use is a component of an office or residential project and does not exceed 20% of the total building square footage for the project or 9,600 square feet, whichever is less.

b. No off-street vehicle parking shall be required for restaurant outdoor seating located on private property.

#### 6. Entrances.

Development with frontage along Richards Boulevard shall provide an entrance facing the public street.

### **17.120.100 Limited commercial C-1 zone.**

#### A. Allowed Uses.

1. Uses permitted in the C-1 zone under this title outside of the River District SPD shall be allowed in the C-1 zone within the River District SPD, except the following uses are prohibited in the C-1 zone within the River District SPD:

- a. Appliance repair shop;
- b. Unattended uses, such as self-serve laundromats;
- c. Hardware store.

2. If this title requires the approval of a special permit or other discretionary entitlement, or imposes other restrictions or requirements on the establishment of a particular use in the C-1 zone outside of the River District SPD, approval of the same

discretionary entitlement and compliance with the same restrictions or requirements shall be required to establish the use in the C-1 zone within the River District SPD.

B. Development Standards.

Except as provided below, development in the C-1 zone in the River District SPD shall be subject to the same development standards that govern development in the C-1 zone outside of the River District SPD.

1. Residential Density.

The maximum residential density shall be 29 dwelling units per net acre. A higher density may be approved upon the issuance of a planning commission special permit pursuant to and subject to the findings required by Chapter 17.212 and consistent with the applicable density range established by the city's General Plan.

2. Building Size.

Development in the C-1 zone in the River District SPD shall not be subject to footnote (9) of Section 17.60.030. A planning commission special permit shall be required for any building to be constructed or expanded to exceed 40,000 square feet of gross floor area. A zoning administrator's special permit shall be required for nonresidential buildings to be constructed or expanded to exceed 10,000 square feet up to and including 40,000 square feet of gross floor area. There is no maximum lot coverage standard.

3. Height Standards.

The height standards for the C-1 zone in the River District SPD are set out in Exhibit B at the end of this chapter. Requests to vary a height standard shall be heard and decided under Section 17.120.130 and shall not be subject to footnote (8) of Section 17.60.030.

4. Open Space Requirements.

a. Office.

i. Open space shall be provided for new office development at a ratio of one square foot of open space for every 15 square feet of the total square footage of the development.

ii. Open space shall be in the form of courtyards or public plazas.

iii. Open space requirements shall be satisfied onsite; provided, that the planning commission may approve a special permit pursuant to and subject to the findings required by Chapter 17.212 to allow not more than 20% of the required open

space off-site. Required off-site open space shall be located within the River District SPD.

b. Residential.

i. Open space areas specifically designed for recreation or passive enjoyment of the outdoors are required for new residential development.

ii. A minimum of 80 square feet of usable common open space per residential unit is required. Usable common open space may include courtyard, garden, recreational, or similar common areas.

iii. A minimum of 50 square feet of usable private open space per residential unit is required. Usable private open space shall be designed for the exclusive use of the associated unit and may include decks, balconies, and patios. Usable private open space shall be directly accessible from the associated unit.

iv. Open space requirements shall be satisfied onsite; provided, that the planning commission may approve a special permit pursuant to and subject to the findings required by Chapter 17.212 to allow not more than 20% of the total required open space offsite. Required offsite open space shall be located in the River District SPD. In approving the special permit, the planning commission shall specify how the remaining open space to be provided on site shall be allocated between usable common open space and usable private open space.

5. Parking Requirements.

a. No off-street vehicle parking shall be required for retail, commercial service (including banks and beauty salons), athletic club/fitness center, or restaurant uses if the use does not exceed 9,600 square feet.

b. No off-street vehicle parking shall be required for restaurant outdoor seating located on private property.

**17.120.110 General commercial C-2 zone.**

A. Allowed Uses.

Uses permitted in the C-2 zone under this title outside of the River District SPD shall be allowed in the C-2 zone within the River District SPD. If this title requires the approval of a special permit or other discretionary entitlement, or imposes other restrictions or requirements on the establishment of a particular use in the C-2 zone outside of the River District SPD, approval of the same discretionary entitlement and compliance with the same restrictions or requirements shall be required to establish the use in the C-2 zone within the River District SPD.

## B. Development Standards.

Except as provided below, development in the C-2 zone in the River District SDP shall be subject to the same development standards that govern development in the C-2 zone outside of the River District SPD.

### 1. Residential Density.

The maximum residential density shall be 100 dwelling units per net acre. A higher density may be approved upon the issuance of a planning commission special permit pursuant to and subject to the findings required by Chapter 17.212 and consistent with the applicable density range established by the city's General Plan.

### 2. Building Size.

Development in the C-2 zone in the River District SPD shall not be subject to footnote (9) of Section 17.60.030. A planning commission special permit shall be required for any building to be constructed or expanded to exceed 40,000 square feet of gross floor area. A zoning administrator's special permit shall be required for nonresidential buildings to be constructed or expanded to exceed 10,000 square feet up to and including 40,000 square feet of gross floor area. There is no maximum lot coverage standard.

### 3. Height Standards.

The height standards for the C-2 zone in the River District SPD are set out in Exhibit B at the end of this chapter. Requests to vary a height standard shall be heard and decided under Section 17.120.130 and shall not be subject to footnote (8) of Section 17.60.030.

### 4. Open Space Requirements.

#### a. Office.

i. Open space shall be provided for new office development at a ratio of one square foot of open space for every 15 square feet of the total square footage of the development.

ii. Open space shall be in the form of courtyards or public plazas.

iii. Open space requirements shall be satisfied onsite; provided, the planning commission may approve a special permit pursuant to and subject to the findings required by Chapter 17.212 to allow not more than 20% of the required open space off-site. Required off-site open space shall be located within the River District SPD.

b. Residential.

i. Open space areas specifically designed for recreation or passive enjoyment of the outdoors are required for new residential development.

ii. A minimum of 80 square feet of usable common open space per residential unit is required. Usable common open space may include courtyard, garden, recreational, or similar common areas.

iii. A minimum of 50 square feet of usable private open space per residential unit is required. Usable private open space shall be designed for the exclusive use of the associated unit and may include decks, balconies, and patios. Usable private open space shall be directly accessible from the associated unit.

iv. Open space requirements shall be satisfied onsite; provided, that the planning commission may approve a special permit pursuant to and subject to the findings required by Chapter 17.212 to allow not more than 20% of the total required open space offsite. Required offsite open space shall be located in the River District SPD. In approving the special permit, the planning commission shall specify how the remaining open space to be provided on site shall be allocated between usable common open space and usable private open space.

5. Parking

No off-street vehicle parking shall be required for restaurant outdoor seating located on private property.

**17.120.120 Heavy Commercial C-4 zone.**

A. Allowed Uses.

1. Uses permitted in the C-4 zone under this title outside of the River District SPD shall be allowed in the C-4 zone within the River District SPD.

2. If this title requires the approval of a special permit or other discretionary entitlement, or imposes other restrictions or requirements on the establishment of a particular use in the C-4 zone outside of the River District SPD, approval of the same discretionary entitlement and compliance with the same restrictions or requirements shall be required to establish the use in the C-4 zone within the River District SPD, except the following uses are permitted in the C-4 zone within the River District SPD subject to the restrictions and requirements as stated for each use:

a. Apartments, subject to footnote (75), but not to footnote (13), of Section 17.24.050.

b. Medical marijuana dispensaries, subject to footnote 85 except that a planning commission special permit shall be required.

B. Development Standards.

Except as provided below, development in the C-4 zone in the River District SPD shall be subject to the same development standards that govern development in the C-4 zone outside of the River District SPD.

1. Height Standards.

The height standards for the C-4 zone in the River District SPD are set out in Exhibit B at the end of this chapter and shall not be subject to footnote (18) of Section 17.60.030.

2. Building Size.

Except as provided below, development in the C-4 zone in the River District SPD shall not be subject to footnote (18) of Section 17.60.030. A planning commission special permit shall be required for any building to be constructed or expanded to exceed 40,000 square feet of gross floor area. A zoning administrator's special permit shall be required for nonresidential buildings to be constructed or expanded to exceed 10,000 square feet up to and including 40,000 square feet of gross floor area. For nonresidential development, there is no maximum lot coverage standard. For residential and mixed residential and nonresidential development, the lot coverage and density standards in subsection (b)(ii) of footnote (18) of Section 17.60.030 shall apply.

3. Open Space Requirements.

a. Office.

i. Open space shall be provided for new office development at a ratio of one square foot of open space for every 15 square feet of the total square footage of the development.

ii. Open space shall be in the form of courtyards or public plazas.

iii. Open space requirements shall be satisfied onsite; provided, that the planning commission may approve a special permit pursuant to and subject to the findings required by Chapter 17.212 to allow not more than twenty 20% of the required open space off-site. Required off-site open space shall be located within the River District SPD.

b. Residential.

i. Open space areas specifically designed for recreation or passive enjoyment of the outdoors are required for new residential development.

ii. A minimum of 80 square feet of usable common open space per residential unit is required. Usable common open space may include courtyard, garden, recreational, or similar common areas.

iii. A minimum 50 square feet of usable private open space per residential unit is required. Usable private open space shall be designed for the exclusive use of the associated unit and may include decks, balconies, and patios. Usable private open space shall be directly accessible from the associated unit.

iv. Open space requirements shall be satisfied onsite; provided, that the planning commission may approve a special permit pursuant to and subject to the findings required by Chapter 17.212 to allow not more than 20% of the total required open space offsite. Required offsite open space shall be located in the River District SPD. In approving the special permit, the planning commission shall specify how the remaining open space to be provided on site shall be allocated between usable common open space and usable private open space.

#### 4. Parking Requirements.

a. No off-street vehicle parking shall be required for retail, commercial service (including banks and beauty salons), athletic club/fitness center, or restaurant uses if the use is a component of a residential project and does not exceed 20% of the total building square footage for the project or 9,600 square feet, whichever is less.

b. No off-street vehicle parking shall be required for restaurant outdoor seating located on private property.

#### 5. Outdoor Storage.

All storage for appliance repair shops, cabinet shops, contractor's storage yards, building/landscape contractor shops, equipment rental and sales yards, furniture refinishing, lumber yards-retail, truck and tractor sales, service, and repair, and warehouse and distribution centers shall be inside an enclosed building or, if located outdoors, shall be completely screened from street views with landscaping and/or solid fencing.

### **17.120.130 Modification of height, yard, and stepback standards.**

Design review or preservation review conducted at the director or commission level under Chapters 17.132 or 17.134 may address and modify the required height, yard, and stepback standards to achieve the intent and purposes of the River District Urban Design Guidelines, to ensure adequate light and air and compatibility with surrounding land uses, to ensure that an adequate and appropriate street tree canopy is created and maintained, and to mitigate visual impacts on listed historic resources. The

director or commission may approve up to an additional 50 feet of height for development on the west side of Interstate 5 if a public observation deck is incorporated into the building consistent with the River District Urban Design Guidelines. Where the design director or design commission has authority to modify the required height, yard, and setback standards under this section, neither the zoning administrator nor the planning commission shall have authority to consider or grant special permits, variances, plan reviews, modifications of these entitlements, or any other entitlement to modify the height, yard, or setback standards for a development.

#### **17.120.140 Required setback on Richards Boulevard for light rail transit.**

Notwithstanding the provisions of this chapter to the contrary, the minimum setback on the north side of Richards Boulevard from North 7<sup>th</sup> Street to North 16<sup>th</sup> Street shall be 35 feet; provided, that upon establishment of a 30 foot wide right-of-way at this location for light rail transit purposes, the minimum setback shall be five feet.

#### **17.120.150 Parking.**

If the use of an existing building is changed to another use that is consistent with this chapter, the following parking requirements shall apply:

A. If the change of use is not accompanied by a building expansion or reconstruction, then the change of use shall not require any additional parking.

B. If the change in use is accompanied by a building expansion, the new use shall be required to meet the parking requirements only as applied to the additional square footage added by the expansion.

C. If the change in use is accompanied by the building being demolished and rebuilt, in whole or in part, the new use shall conform to all applicable parking requirements.

#### **17.120.160 Building design to accommodate ground floor retail.**

New buildings shall be designed to accommodate future ground floor retail uses consistent with Exhibit C at the end of this chapter and the River District Urban Design Guidelines. The design review or preservation review conducted under Chapters 17.132 or 17.134 may address and modify or waive the ground floor retail accommodation requirement provided that the design or preservation review is performed at the director or commission level.

#### **17.120.170 Nonconforming use regulations.**

A. General.

Except as provided below, the nonconforming use regulations set forth in Chapter 17.88 of this title shall apply to nonconforming uses and to the use of nonconforming buildings, structures, and lots within the River District SPD.

B. Discontinuance of Nonconforming Uses.

Notwithstanding the provisions of Section 17.88.030(10)):

1. A nonconforming use of a lot, building or structure that ceases operation, voluntarily or involuntarily, for a continuous period of four years or more shall not resume operation unless the use of the lot, building, or structure conforms to the use regulations of the zone in which it is located.

2. The zoning administrator may approve a single extension of time of not more than two years to resume the operation of a nonconforming use upon a showing of good cause and a determination that the applicant has made reasonable and diligent efforts to resume the nonconforming use. The application for an extension of time to resume the operation of a nonconforming use shall be filed not less than 30 days prior to the expiration of the four year period within which the nonconforming use may be resumed by right. The application for extension of time shall be noticed and heard, and shall be subject to appeal, in the same manner as an application for a zoning administrator special permit.

C. Repair and Reconstruction of Buildings Damaged or Destroyed by Disaster.

1. Subject to the restrictions set forth in this subsection C, and notwithstanding the provisions of Section 17.88.030(3), a nonconforming building or structure, or a building or structure lawfully used for a nonconforming use, that is damaged or destroyed by disaster, in whole or in part, may be repaired or reconstructed, and any occupation or use of the building or structure that lawfully existed at the time of damage or destruction may be resumed.

2. The repair or reconstruction work shall commence within two years following the date of damage or destruction and shall be diligently prosecuted to completion. Commencement shall be deemed to occur when a building permit is obtained and construction physically commenced. All repair or reconstruction work shall be in accordance with the regulations of the building code existing at the time the building permit application for the work is filed.

3. Any nonconforming occupation or use of the building or structure that lawfully existed at the time of damage or destruction may be resumed no later than six months following issuance of a certificate of occupancy or approval of final inspection of the repair or reconstruction work. If the nonconforming use is not resumed within six months following issuance of a certificate of occupancy or approval of final inspection, any future use of the building or structure shall conform to the use regulations of the zone in which it is located.

4. The repaired or reconstructed building or structure shall not exceed the square footage of the original building or structure, but may differ in height, lot coverage, design or other features if it complies with the development standards for new development in the River District SPD.

5. The zoning administrator may approve a single extension of time of not more than two years to commence the repair or reconstruction of a damaged or destroyed building or structure under this section upon a showing of good cause and a determination that the applicant has made reasonable and diligent efforts to commence the repair or reconstruction of the building or structure. The application for extension of time to commence the repair or reconstruction of the building or structure shall be filed not less than 30 days prior to expiration of the two year period for commencement of work under this subsection C. The application for extension of time shall be noticed and heard, and shall be subject to appeal, in the same manner as an application for a zoning administrator special permit.

D. Allowed Expansion of Nonconforming Uses.

Notwithstanding the provisions of Section 17.88.030(2)(b), a nonconforming use may be enlarged within the building it occupies, enlarged or increased to occupy a greater area of land than that occupied by the use at the time the use became nonconforming, or moved in whole or in part to any other portion of the lot or parcel of land occupied by the nonconforming use upon the approval of a zoning administrator special permit pursuant to and subject to the findings required by Chapter 17.212.

E. Change from a Nonconforming Use to Another Nonconforming Use.

Notwithstanding the provisions of Section 17.88.030(9), an existing nonconforming use is permitted by right to change to another nonconforming use if the new nonconforming use is listed in Table 1, below. The zoning administrator may approve a special permit pursuant to and subject to the findings required by Chapter 17.212 to allow a nonconforming use to be changed to another nonconforming use listed in Table 2 below.

<b>Table 1: Nonconforming Use Change Permitted by Right</b>
Appliance Repair Shop
Assembly of electrical &/or electronic equipment
Assembly of plastic &/or rubber items
Beverage Bottling Plant
Billboard manufacture
Building/Landscape contractor shop
Cabinet shop
Cement or clay products manufacturing
Cleaning plant, commercial
Contractor's storage yard
Equipment rental & sales yard
Furniture refinishing
Garment shop
Janitorial service company
Laboratory
Laundry, commercial plant
Lumber yard—Retail
Machine shop
Manufacturing, assembly, and treatment of merchandise
Monument works, stone
Nursery for plants and flowers
Printing and blueprinting
RV Storage (Commercial)
Warehouse and distribution center
Wholesale stores and distributors

<b>Table 2: Nonconforming Use Change With Zoning Administrator Special Permit</b>
Auto dismantler
Concrete batch plant
Food processing plant
Fuel Storage Yard
Junk Yard
Planing mill
Recycling facilities (minor, major, greenwaste)
Terminal yard, trucking
Towing service & vehicle storage yard
Truck and tractor sales, service, and repair

Subject: River District Specific Plan (M09-003)

January 13, 2011

Exhibit A: River District Specific Plan Boundary



Exhibit B: Maximum Allowed Height (Measured from Existing Grade)

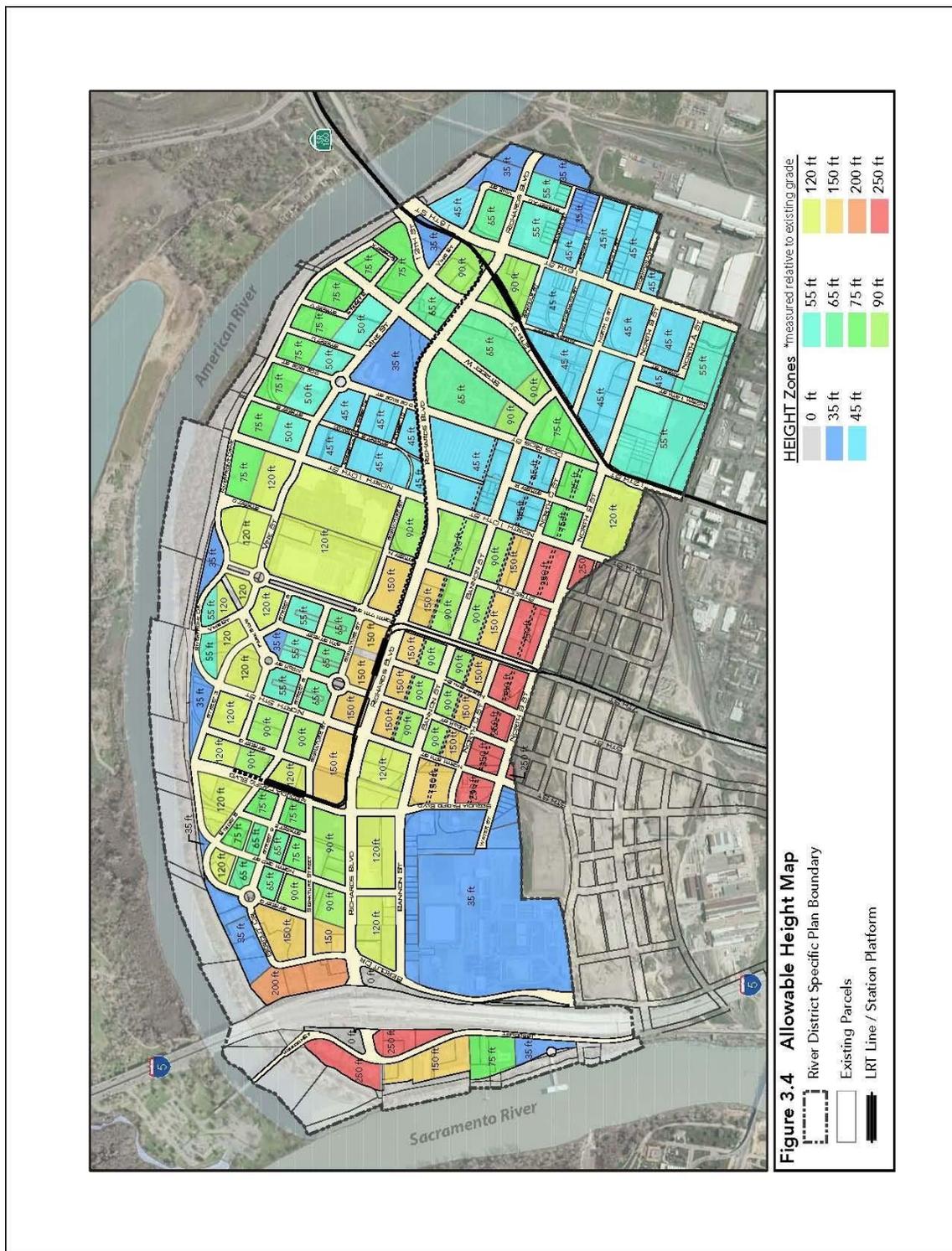
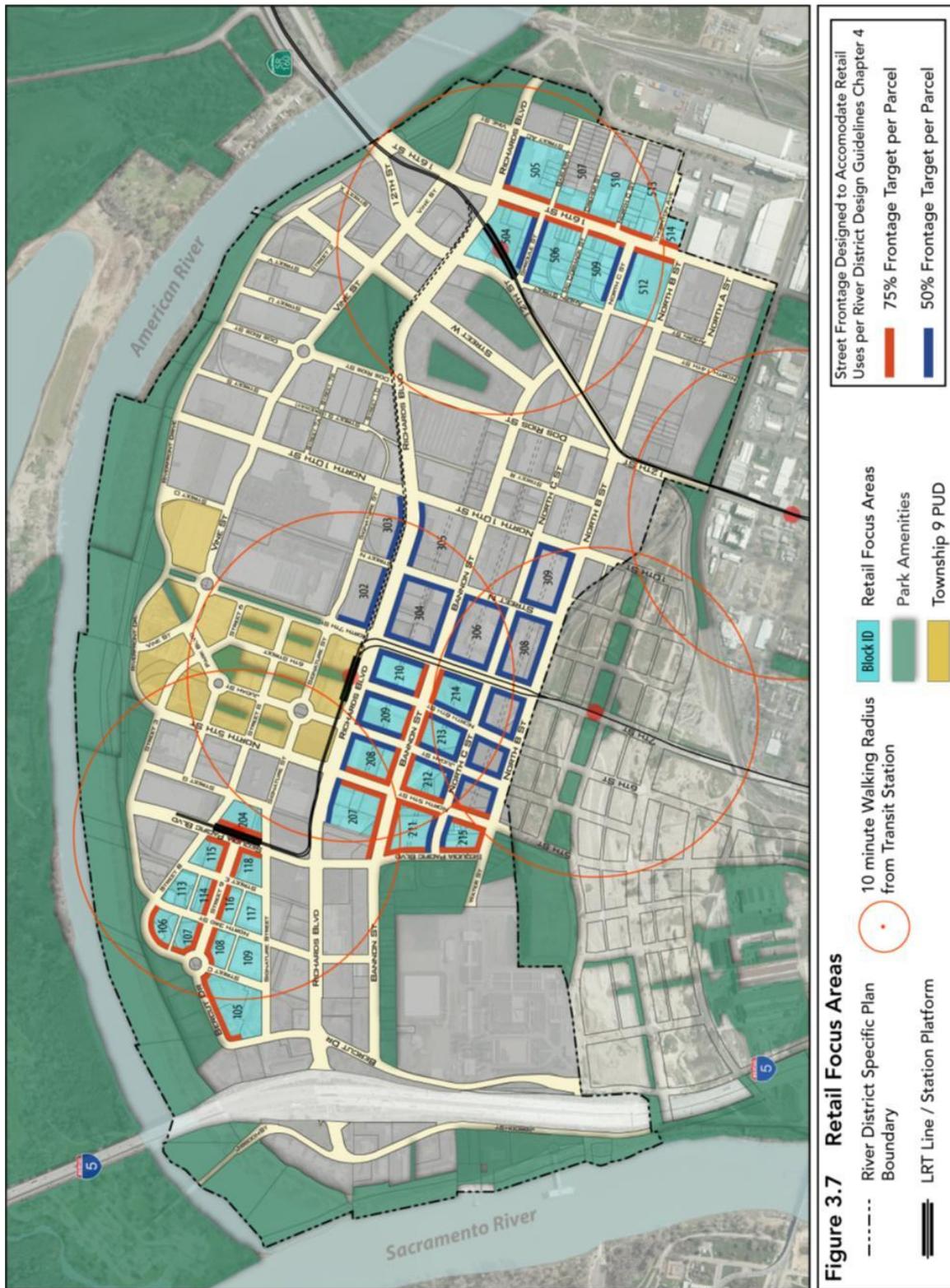


Exhibit C: Ground Floor Retail Accommodation



Subject: River District Specific Plan (M09-003)

January 13, 2011

**SECTION 3.** Section 17.20.030 of Title 17 of the Sacramento City Code (the Zoning Code) is amended to read as follows:

**17.20.030 Special planning districts.**

The following special planning districts (SPDs) are discussed in more detail in Chapters 17.92 through 17.130 of this title and are listed here for convenience only:

Broadway-Stockton SPD	Ch. 17.94
Central business district SPD	Ch. 17.96
McClellan Heights/Parker Homes SPD	Ch. 17.98
Northgate Boulevard SPD	Ch. 17.100
Alhambra Corridor SPD	Ch. 17.104
Del Paso Boulevard SPD	Ch. 17.108
Del Paso Nuevo SPD	Ch. 17.112
Sacramento Army Depot SPD	Ch. 17.116
<del>Richards Boulevard</del> <u>River District</u> SPD	Ch. 17.120
Sacramento Railyards SPD	Ch. 17.124
R Street Corridor SPD	Ch. 17.128
Freeport SPD	Ch. 17.130

**SECTION 4.** Section 17.24.050 of Title 17 of the Sacramento City Code (the Zoning Code) is amended as follows:

A. Footnote 78 of Section 17.24.050 is amended to read as follows:

78. a. Small Temporary Residential Shelter (24 or Fewer Beds) in the C-4, M-1, M-1(S), M-2, M-2(S) Zones.

A small temporary residential shelter consisting of not more than twenty-four (24) beds, is allowed in the C-4, M-1, M-1(S), M-2, and M-2(S) zones, provided that all of the location requirements and development standards set forth below are satisfied. A planning commission special permit shall be required to establish a small temporary residential shelter that does not meet all of the following location requirements and development standards. Notwithstanding the foregoing, a planning commission special permit shall be required to establish a small temporary residential shelter in the ~~Richards Boulevard~~River District special planning district.

i. Location Requirements. Small temporary residential shelters shall meet the following location requirements:

(A) Small temporary residential shelters serving single adults only shall be situated more than one thousand (1,000) feet from any other temporary residential shelter, measured from property line to property line, and more than five hundred (500) feet from a public park, a public or private K-12 school, churches, or single-family

residential zones, measured from property line to property line. Programs may have multiple buildings on a single parcel.

(B) All other small temporary residential shelters shall be situated more than one thousand (1,000) feet from any other temporary residential shelter, measured from property line to property line. Programs may have multiple buildings on a single parcel.

(C) Small temporary residential shelters shall either be located within one thousand (1,000) feet of a designated transit corridor or bus route, or shall provide transportation between the facility and transit lines and/or services.

ii. Development Standards. Small temporary residential shelters shall meet the following development standards:

(A) Maximum Number of Beds. No more than twenty-four (24) beds shall be provided in any single small temporary residential shelter.

(B) Parking. Off-street parking shall be provided in the ratio of one space for every four adult beds, plus an additional space designated exclusively for the manager. All parking is required to be off-street and on-site.

(C) Hours of Operation. Facilities shall establish and maintain set hours for client intake/discharge. These hours shall be posted.

(D) On-site Personnel. On-site personnel shall be provided during hours of operation when clients are present. The manager's area shall be located near the entry to the facility.

(E) Lighting. Facilities shall provide exterior lighting on pedestrian pathways and parking lot areas on the property. Lighting shall reflect away from residential areas and public streets.

(F) Telephones. Facilities shall provide telephone(s) for use by clients.

(G) Personal Property. Facilities shall provide secure areas for personal property.

(H) Waiting Area. If intake of clients is to occur onsite, enclosed or screened waiting area must be provided on the property to prevent queuing in the public right-of-way. For purposes of this condition, small emergency shelters shall have waiting area consisting of not less than one hundred (100) square feet in the same location.

(I) Common Space. Interior and/or exterior common or recreational space for residents to congregate shall be provided on the property at a ratio of not less than fifteen (15) square feet per occupant and a minimum overall area of one hundred (100) square feet. Common space must be counted separately from the waiting area.

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b. Large Temporary Residential Shelters (More Than 24 Beds) in the C-4, M-1, M-1(S), M-2, M-2(S) Zones.

A large temporary residential shelter consisting of more than twenty-four (24) beds is allowed with a planning director's special permit in the C-4, M-1, M-1(S), M-2, and M-2(S) zones, provided that all of the location requirements and development standards set forth below are satisfied. A planning commission special permit shall be required to establish a large temporary residential shelter that does not meet all of the following location requirement and development standards. Notwithstanding the foregoing, a planning commission special permit shall be required to establish a large temporary residential shelter in the ~~Richards-Boulevard~~River District special planning district.

i. Location Requirements. Large temporary residential shelters shall meet the following location requirements:

(A) Large temporary residential shelters serving single adults only shall be situated more than one thousand (1,000) feet from any other temporary residential shelter, measured from property line to property line, and no closer than five hundred (500) feet from a public park, a public or private K-12 school, churches, or single-family residential zones, measured from property line to property line. Programs may have multiple buildings on the same parcel.

(B) All other large temporary residential shelters must be situated more than one thousand (1,000) feet from any other temporary residential shelter, measured from property line to property line. Programs may have multiple buildings on a single parcel.

(C) Temporary residential shelters must either be located within one thousand (1,000) feet of a designated transit corridor or bus route, or shall provide transportation between the facility and transit lines to the satisfaction of the planning director.

ii. Development Standards. Large temporary residential shelters shall meet the following development standards:

(A) Parking. Off-street parking shall be provided in the ratio of one space for every five adult beds, plus an additional space designated exclusively for the manager. All parking is required to be off-street and on-site.

(B) Hours of Operation. Facilities shall establish and maintain set hours for client intake/discharge. These hours shall be posted.

(C) On-site Personnel. On-site personnel shall be provided during hours of operation when clients are present. The manager's area shall be located near the entry to the facility.

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(D) Lighting. Facilities shall provide exterior lighting on pedestrian pathways and parking lot areas on the property. Lighting shall reflect away from residential areas and public streets.

(E) Telephones. Facilities shall provide telephone(s) for use by clients.

(F) Personal Property. Facilities shall provide secure areas for personal property.

(G) Waiting Area. If intake of clients is to occur onsite, enclosed or screened waiting area must be provided on the property to prevent queuing in the public right-of-way. For purposes of this condition, two hundred (200) square feet shall be deemed to constitute adequate waiting space unless the director determines that additional waiting space is required to meet the needs of the anticipated client load, in which case the higher figure shall apply.

(H) Common Space. Interior and/or exterior common or recreational space for residents to congregate shall be provided on the property at a ratio of not less than fifteen (15) square feet per occupant. Common space must be counted separately from the waiting area.

B. Except as specifically amended by the amendments to Footnote 78, Section 17.24.050 remains unchanged and in full force and effect.

**SECTION 5.** Section 17.134.430 of Title 17 of the Sacramento City Code (the Zoning Code) is amended as follows:

A. Subsection A.1.a. of Section 17.134.430 is amended to read as follows:

a. Buildings and Structures within the ~~Richards Boulevard~~River District Special Planning District. In the ~~Richards Boulevard~~River District special planning district (formerly the Richards Boulevard special planning district), the requirements of this section shall apply only to applications to demolish or relocate buildings or structures that are identified in the Richards Boulevard area architectural and historical property survey (hereinafter "survey"), as either potential essential structures, priority structures or contributing structures within the potential North 16th Street preservation area. Applications to demolish or relocate buildings or structures within ~~Richards Boulevard~~the River District special planning district that are not so identified in the survey shall not be subject to the requirements of this section.

B. Except as specifically amended by the amendments to subsection A.1.a., Section 17.134.430 remains unchanged and in full force and effect.

Subject: River District Specific Plan (M09-003)

January 13, 2011

**Rezoning – DRAFT City Council**

**ORDINANCE NO.**

Adopted by the Sacramento City Council

**AMENDING TITLE 17 OF THE SACRAMENTO CITY CODE (THE ZONING CODE) BY REZONING VARIOUS PARCELS OF REAL PROPERTY WITHIN THE SACRAMENTO RIVER DISTRICT SPECIFIC PLAN AREA FOR CONSISTENCY WITH THE 2030 GENERAL PLAN**

BE IT ENACTED BY THE COUNCIL OF THE CITY OF SACRAMENTO THAT:

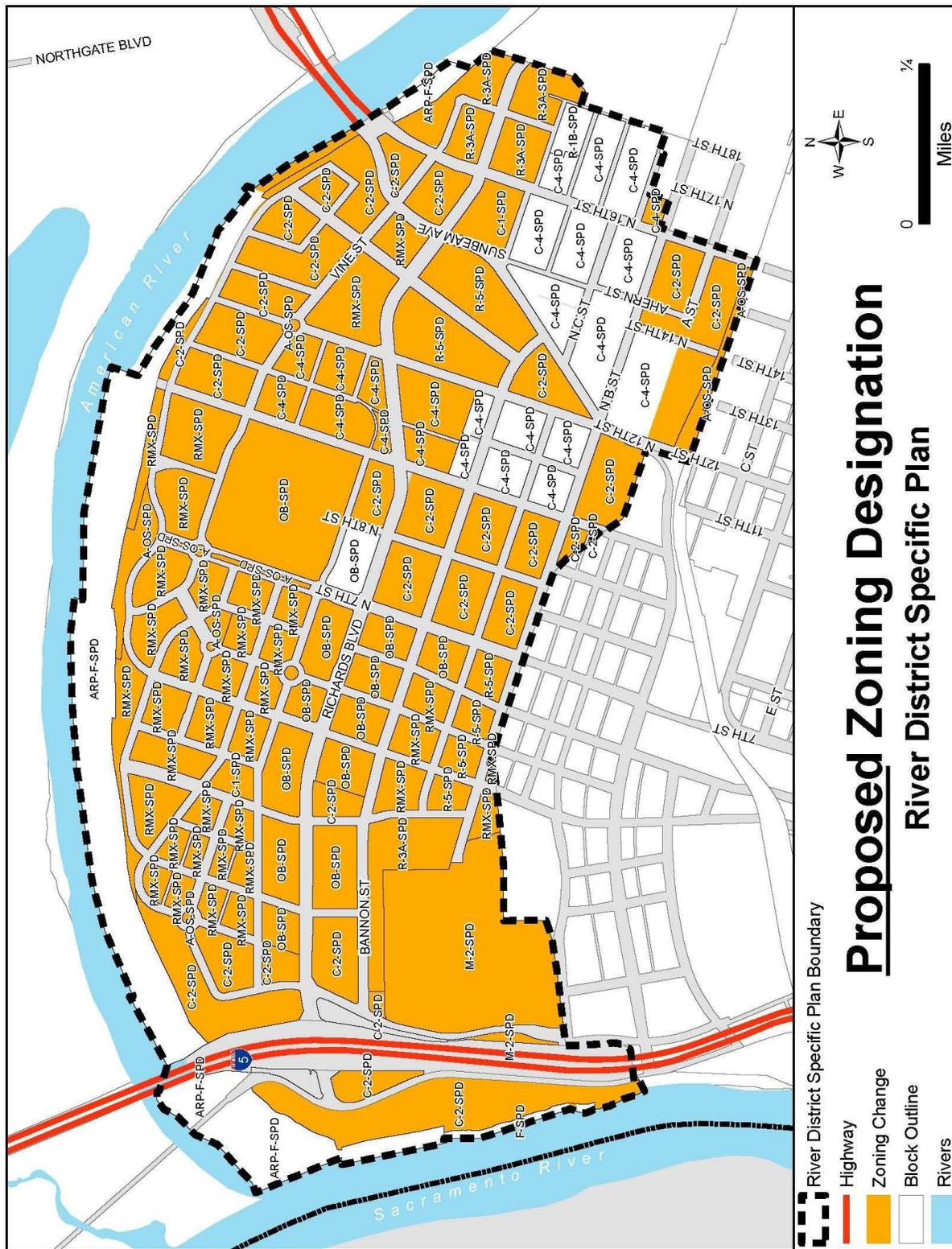
- Section 1. Title 17 of the Sacramento City Code (the Zoning Code) is amended by rezoning the properties depicted in the attached Exhibit A and identified by APN and address in the attached Exhibit B, from the existing zone to the proposed zone as set forth in Exhibit B. The attached Exhibits A and B are incorporated herein by reference.
  
- Section 2. Rezoning of the property shown in the attached Exhibit A, by the adoption of this Ordinance, will be considered to be in compliance with the requirements for the rezoning of property described in the Zoning Code, as amended, as those procedures have been affected by recent court decisions.
  
- Section 3. The City Clerk of the City of Sacramento is hereby directed to amend the official zoning maps, which are a part of the Zoning Code, to conform to the provisions of this Ordinance.

Table of Contents:

Exhibit A – River District Rezone Map – 1 page

Exhibit B – List of Rezone Properties

Exhibit A – River District Rezone Map



**River District Specific Plan Boundary**

# Proposed Zoning Designation

**River District Specific Plan**

- River District Specific Plan Boundary
- Highway
- Zoning Change
- Block Outline
- Rivers

Subject: River District Specific Plan (M09-003)

January 13, 2011

## Exhibit B – List of Rezone Properties

APN	Situs Address	Situs ZIP	Existing Zone	Proposed Zone
00100110010000	221 JIBBOOM ST	95811	HC-PC-SPD	C-2-SPD
00100110030000	225 JIBBOOM ST	95811	HC-PC-SPD	C-2-SPD
00100110040000	227 JIBBOOM ST	95811	HC-PC-SPD	C-2-SPD
00100110050000	231 JIBBOOM ST	95811	HC-PC-SPD	C-2-SPD
00100120150000	226 JIBBOOM ST	95811	HC-PC-SPD	C-2-SPD
00100120160000	222 JIBBOOM ST	95811	HC-PC-SPD	C-2-SPD
00100120180000	228 JIBBOOM ST	95811	HC-PC-SPD	C-2-SPD
00100120200000	232 JIBBOOM ST	95811	HC-PC-SPD	C-2-SPD
00100120210000	236 JIBBOOM ST	95811	HC-PC-SPD	C-2-SPD
00100120220000	0 JIBBOOM ST	95811	HC-PC-SPD	C-2-SPD
00100120250000	200 JIBBOOM ST	95811	HC-PC-SPD	C-2-SPD
00100200030000	0 N 7TH ST	95811	M-2-PC-SPD(N)	A-OS-SPD
00100200080000	800 N 10TH ST	95811	M-2-PC-SPD(N)	RMX-SPD
00100200090000	600 N 10TH ST	95811	M-2-SPD(N)	OB-SPD
00100200100000	0 N 10TH ST	95811	M-2-SPD(C)	C-2-SPD
00100200110000	851 N 10TH ST	95811	M-2-SPD(C)	C-2-SPD
00100200120000	951 RICHARDS BL	95811	M-2-SPD(C)	C-2-SPD
00100200130000	851 RICHARDS BL	95811	M-2-SPD(C)	C-2-SPD
00100200190000	0 N 7TH ST	95811	RMX-PUD-SPD(N)	RMX-SPD
00100200450000	430 N 7TH ST	95811	(A-OS,OB,RMX)- PUD/SPD(C/N)	(A-OS, OB, RMX)-PUD/SPD
00100200460000	819 N 7TH ST	95811	(A-OS, RMX)- PUD/SPD(N)	(A-OS, RMX)-PUD/SPD
00100200490000	601 N 7TH ST	95811	M-2-PUD/SPD(N)	OB-PUD/SPD
00100200500000	611 N 7TH ST	95811	M-2-PUD/SPD(N)	OB-PUD/SPD
00100200510000	845 RICHARDS BL	95811	(M-2, OB)-PUD/SPD(N)	OB-SPD
00100200520000	0 RICHARDS BL	95811	A-OS-PUD/SPD, (M-2,M- 2-PC)-SPD	(A-OS, RMX)-SPD
00100310020000	0 N B ST	95811	M-2-SPD(C)	ROW
00100310030000	0 N B ST	95811	M-2-SPD(C)	ROW
00100310040000	915 N B ST	95811	M-2-SPD(C)	C-2-SPD
00100310050000	320 N 10TH ST	95811	M-2-SPD(C)	C-2-SPD
00100310060000	360 N 10TH ST	95811	M-2-SPD(C)	C-2-SPD
00100310070000	410 N 10TH ST	95811	M-2-SPD(C)	C-2-SPD
00100310080000	950 RICHARDS BL	95811	M-2-SPD(C)	C-2-SPD
00100310090000	900 RICHARDS BL	95811	M-2-SPD(C)	C-2-SPD
00100310120000	800 RICHARDS BL	95811	M-2-SPD(C)	C-2-SPD
00100310130000	0 RICHARDS BL	95811	M-2-SPD(C)	C-2-SPD
00100310140000	0 N 7TH ST	95811	M-2-SPD(C)	C-2-SPD
00100310150000	325 N 7TH ST	95811	M-2-SPD(C)	C-2-SPD
00100310160000	333 N 7TH ST	95811	M-2-SPD(C)	C-2-SPD
00100310170000	325 N 7TH ST	95811	M-2-SPD(C)	C-2-SPD
00100310210000	0 N 7TH ST	95811	M-2-SPD(C)	C-2-SPD
00100310220000	840 RICHARDS BL	95811	M-2-SPD(C)	C-2-SPD
00100310230000	721 N B ST	95811	M-2-SPD(C)	C-2-SPD
00100310240000	0 N B ST	95811	M-2-SPD(C)	C-2-SPD
00100310250000	750 RICHARDS BL	95811	M-2-SPD(C)	C-2-SPD
00100310260000	0 RICHARDS BL	95811	M-2-SPD(C)	C-2-SPD
00100400120000	316 BANNON ST	95811	M-2-SPD(W)	R-3A-SPD
00100400130000	310 BANNON ST	95811	M-2-SPD(W)	R-3A-SPD
00100400140000	260 BANNON ST	95811	M-2-SPD(W)	R-3A-SPD

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00100400150000	258 BANNON ST	95811	M-2-SPD(W)	R-3A-SPD
00100400180000	238 BANNON ST	95811	M-2-SPD(W)	R-3A-SPD
00100400190000	230 BANNON ST	95811	M-2-SPD(W)	R-3A-SPD
00100400200000	222 BANNON ST	95811	M-2-SPD(W)	R-3A-SPD
00100400310000	200 RICHARDS BL	95811	HC-SPD	C-2-SPD
00100400340000	210 RICHARDS BL	95811	HC-SPD	C-2-SPD
00100400350000	210 RICHARDS BL	95811	HC-SPD	C-2-SPD
00100400360000	0 BANNON ST	95811	OB-PUD/SPD	C-2-SPD
00100400370000	216 BANNON ST	95811	HC-SPD	C-2-SPD
00100400380000	0 BERGUT DR	95811	HC-SPD	C-2-SPD
00100400390000	246 BANNON ST	95811	M-2-SPD(W)	R-3A-SPD
00100520010000	320 BANNON ST	95811	M-2-SPD(W)	R-3A-SPD
00100520020000	324 BANNON ST	95811	M-2-SPD(W)	R-3A-SPD
00100520030000	330 BANNON ST	95811	M-2-SPD(W)	R-3A-SPD
00100520040000	334 BANNON ST	95811	M-2-SPD(W)	R-3A-SPD
00100520070000	354 BANNON ST	95811	M-2-SPD(W)	R-3A-SPD
00100520220000	350 BANNON ST	95811	M-2-SPD(W)	R-3A-SPD
00100520240000	400 BANNON ST	95811	M-2-SPD(W)	R-3A-SPD
00100520250000	358 BANNON ST	95811	M-2-SPD	R-3A-SPD
00100530240000	455 BANNON ST	95811	M-2-SPD(C)	(R-5, RMX)-SPD
00100550020000	245 N 5TH ST	95811	M-2-SPD(C)	RMX-SPD
00100550030000	0 N 5TH ST	95811	M-2-SPD(C)	RMX-SPD
00100550040000	251 N 5TH ST	95811	M-2-SPD(C)	RMX-SPD
00100610250000	0 N B ST	95811	M-2-SPD(W)	R-3A-SPD
00100610260000	470 N B ST	95811	M-2-SPD(W)	R-3A-SPD
00100620020000	0 N 5TH ST	95811	M-2-SPD(C)	R-5-SPD
00100620030000	224 N 5TH ST	95811	M-2-SPD(C)	R-5-SPD
00100620040000	0 N 5TH ST	95811	M-2-SPD(C)	R-5-SPD
00100620050000	0 N 5TH ST	95811	M-2-SPD(C)	R-5-SPD
00100620060000	224 N 5TH ST	95811	M-2-SPD(C)	R-5-SPD
00100620070000	479 BANNON ST	95811	M-2-SPD(C)	R-5-SPD
00100620080000	471 BANNON ST	95811	M-2-SPD(C)	R-5-SPD
00100630010000	0 N 5TH ST	95811	M-2-SPD(C)	RMX-SPD
00100630020000	0 N 5TH ST	95811	M-2-SPD(C)	RMX-SPD
00100630030000	501 N 5TH ST	95811	M-2-SPD(C)	(R-5, RMX)-SPD
00100640040000	0 N B ST	95811	M-2	RMX-SPD
00100640050000	422 N B ST	95811	M-2	RMX-SPD
00100640060000	428 N B ST	95811	M-2	RMX-SPD
00100640070000	434 N B ST	95811	M-2	RMX-SPD
00100640080000	440 N B ST	95811	M-2	RMX-SPD
00100640090000	446 N B ST	95811	M-2	RMX-SPD
00100640100000	452 N B ST	95811	M-2	RMX-SPD
00100640110000	458 N B ST	95811	M-2	RMX-SPD
00100640120000	464 N B ST	95811	M-2	ROW
00100640130000	468 N B ST	95811	M-2	RMX-SPD
00100640140000	522 N B ST	95811	M-2	RMX-SPD
00100640150000	400 N B ST	95811	M-2	M-2-SPD
00100700050000	0 N B ST	95811	F	ARP-F-SPD
00100700070000	1401 RICHARDS BL	95811	C-4-PC-SPD	C-2-SPD
00100700090000	0 VINE ST	95811	M-2-PC-SPD(N)	C-2-SPD
00100700220000	0 RICHARDS BL	95811	C-4-PC-SPD	C-2-SPD
00100700230000	1351 VINE ST	95811	M-2-PC-SPD(N)	C-2-SPD

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00100700240000	0 VINE ST	95811	M-2-PC-SPD(N)	C-2-SPD
00100700280000	1421 RICHARDS BL	95811	C-4-PC-SPD	C-2-SPD
00100700290000	1441 RICHARDS BL	95811	C-4-PC-SPD	C-2-SPD
00100700360000	1199 VINE ST	95811	M-2-PC-SPD(N)	(A-OS, C-2)-SPD
00100700370000	1275 VINE ST	95811	M-2-PC-SPD(N)	C-2-SPD
00100700450000	0 VINE ST	95811	M-2-PC-SPD(N)	ARP-F-SPD
00100700460000	0 VINE ST	95811	M-2-PC-SPD(N)	C-2-SPD
00100700480000	0 VINE ST	95811	M-2-PC-SPD(N)	C-2-SPD
00100700490000	1059 VINE ST	95811	M-2-PC-SPD(N)	C-2-SPD
00100700500000	0 VINE ST	95811	M-2-PC-SPD(N)	C-2-SPD
00100700510000	819 N 10TH ST	95811	M-2-PC-SPD(N)	C-2-SPD
00100810040000	1001 RICHARDS BL	95811	M-2-SPD(N)	C-4-SPD
00100810050000	515 N 10TH ST	95811	M-2-SPD(N)	C-4-SPD
00100810060000	521 N 10TH ST	95811	M-2-SPD(N)	C-4-SPD
00100810070000	601 N 10TH ST	95811	M-2-SPD(N)	C-4-SPD
00100810090000	1000 VINE ST	95811	M-2-SPD(N)	C-4-SPD
00100810100000	0 VINE ST	95811	M-2-SPD(N)	ROW
00100810110000	609 N 10TH ST	95811	M-2-SPD(N)	C-4-SPD
00100810120000	0 N 10TH ST	95811	M-2-SPD(N)	C-4-SPD
00100810130000	701 DOS RIOS ST	95811	M-2-SPD(N)	C-4-SPD
00100810140000	601 DOS RIOS ST	95811	M-2-SPD(N)	C-4-SPD
00100810150000	0 DOS RIOS ST	95811	M-2-SPD(N)	C-4-SPD
00100810160000	1101 RICHARDS BL	95811	M-2-SPD(N)	C-4-SPD
00100900030000	1209 SITKA ST	95811	RMX-SPD	(R-5, RMX)-SPD
00100900040000	1050 RICHARDS BL	95811	M-2-SPD(N)	C-4-SPD
00100900050000	1100 RICHARDS BL	95811	M-2-SPD(N)	C-4-SPD
00101010010000	0 RICHARDS BL	95811	C-4-SPD	RMX-SPD
00101010020000	0 RICHARDS BL	95811	C-4-SPD	RMX-SPD
00101010040000	510 N 12TH ST	95811	C-4-SPD	(R-5, RMX)-SPD
00101010050000	1400 RICHARDS BL	95811	C-4-SPD	RMX-SPD
00101010060000	620 SUNBEAM AV	95811	C-4-SPD	RMX-SPD
00101010070000	430 N 12TH ST	95811	C-4-SPD	R-5-SPD
00101020070000	520 N 12TH ST	95811	C-4-SPD	C-1-SPD
00101020080000	1450 RICHARDS BL	95811	C-4-SPD	C-2-SPD
00101020090000	625 SUNBEAM AV	95811	C-4-SPD	C-2-SPD
00101020100000	605 SUNBEAM AV	95811	C-4-SPD	C-2-SPD
00101020110000	522 N 12TH ST	95811	C-4-SPD	C-1-SPD
00101030010000	550 N 16TH ST	95811	C-4-SPD	(C-1, C-2)-SPD
00101030020000	540 N 16TH ST	95811	C-4-SPD	C-1-SPD
00101030030000	520 N 16TH ST	95811	C-4-SPD	C-1-SPD
00101030060000	515 N 12TH ST	95811	C-4-SPD	C-1-SPD
00101030080000	0 SPROULE AV	95811	C-4-SPD	C-1-SPD
00101030090000	500 N 16TH ST	95811	C-4-SPD	C-1-SPD
00101030100000	1451 SPROULE AV	95811	C-4-SPD	C-1-SPD
00101040140000	625 N 16TH ST	95811	C-4-PC-SPD	R-3A-SPD
00101040160000	775 N 16TH ST	95811	C-4-PC-SPD	R-3A-SPD
00101040170000	769 N 16TH ST	95811	C-4-PC-SPD	R-3A-SPD
00101040180000	525 N 16TH ST	95811	(C-4, C-4-PC, R-1B)- SPD	R-3A-SPD
00101040190000	0 N 16TH ST	95811	C-4-PC-SPD	R-3A-SPD
00101040200000	0 N 16TH ST	95811	C-4-PC-SPD	R-3A-SPD
00101220090000	255 DOS RIOS ST	95811	C-4-SPD	C-2-SPD

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0010122010000	210 N 12TH ST	95811	C-4-SPD	C-2-SPD
0010122012000	300 N 12TH ST	95811	C-4-SPD	C-2-SPD
0010122013000	304 N 12TH ST	95811	C-4-SPD	(C-2, R-5)-SPD
0010122014000	200 N 12TH ST	95811	C-4-SPD	C-2-SPD
0010143004000	1603 BASLER ST	95811	R-1B-SPD	R-3A-SPD
0010143005000	1605 BASLER ST	95811	R-1B-SPD	R-3A-SPD
0010143006000	1607 BASLER ST	95811	R-1B-SPD	R-3A-SPD
0010143007000	1611 BASLER ST	95811	R-1B-SPD	R-3A-SPD
0010143013000	1625 BASLER ST	95811	R-1B-SPD	R-3A-SPD
0010143014000	1627 BASLER ST	95811	R-1B-SPD	R-3A-SPD
0010143015000	1629 BASLER ST	95811	R-1B-SPD	R-3A-SPD
0010153001000	211 N 16TH ST	95811	M-2-SPD(E)	C-4-SPD
0010153002000	1610 THORNTON AV	95811	M-2-SPD(E)	C-4-SPD
0010160050000	0 N B ST	95811	M-2-PC-SPD(E)	R-3A-SPD
0010181007000	345 RICHARDS BL	95811	M-2-SPD(N)	OB-SPD
0010181009000	295 N 3RD ST	95811	M-2-SPD(N)	(OB, RMX)-SPD
0010181010000	401 N 3RD ST	95811	M-2-SPD(N)	RMX-SPD
0010181014000	301 RICHARDS BL	95811	M-2-SPD(N)	OB-SPD
0010181015000	0 RICHARDS BL	95811	M-2-SPD(N)	OB-SPD
0010181016000	500 BERCUT DR	95811	HC-PC-SPD	C-2-SPD
0010181017000	450 BERCUT DR	95811	HC-PC-SPD	C-2-SPD
0010181019000	300 BERCUT DR	95811	HC-SPD	C-2-SPD
0010181020000	510 BERCUT DR	95811	HC-PC-SPD	C-2-SPD
0010181021000	530 BERCUT DR	95811	HC-PC-SPD	C-2-SPD
0010181022000	550 BERCUT DR	95811	HC-PC-SPD	C-2-SPD
0010181024000	400 BERCUT DR	95811	HC-SPD	C-2-SPD
0010181025000	350 BERCUT DR	95811	HC-SPD	C-2-SPD
0010181026000	300 BERCUT DR	95811	HC-SPD	C-2-SPD
0010181027000	0 BERCUT DR	95811	HC-PC-SPD	C-2-SPD
0010181028000	570 BERCUT DR	95811	HC-PC-SPD	(A-OS, C-2)-SPD
0010181029000	600 BERCUT DR	95811	HC-PC-SPD	RMX-SPD
0010181030000	610 BERCUT DR	95811	HC-PC-SPD	RMX-SPD
0010181031000	620 BERCUT DR	95811	HC-PC-SPD	RMX-SPD
0010181032000	630 BERCUT DR	95811	HC-PC-SPD	RMX-SPD
0010181033000	640 BERCUT DR	95811	HC-PC-SPD	RMX-SPD
0010181034000	660 BERCUT DR	95811	HC-PC-SPD	RMX-SPD
0010181035000	650 BERCUT DR	95811	HC-PC-SPD	RMX-SPD
0010181036000	0 BERCUT DR	95811	HC-PC-SPD	(C-2, RMX)-SPD
0010182001000	201 RICHARDS BL	95811	HC-SPD	C-2-SPD
0010182004000	221 RICHARDS BL	95811	M-2-SPD(N)	OB-SPD
0010182005000	251 RICHARDS BL	95811	M-2-SPD(N)	OB-SPD
0010182010000	601 BERCUT DR	95811	HC-SPD	(A-OS, RMX)-SPD
0010182015000	444 N 3RD ST	95811	HC-SPD	(A-OS, OB, RMX)-SPD
0010182019000	321 BERCUT DR	95811	HC-SPD	C-2-SPD
0010182024000	321 BERCUT DR	95811	HC-SPD	C-2-SPD
0010182025000	455 BERCUT DR	95811	HC-SPD	C-2-SPD
0010190004000	400 JIBBOOM ST	95811	HC-SPD	C-2-SPD
0010190005000	0 JIBBOOM ST	95811	HC-SPD	C-2-SPD
0010190006000	450 JIBBOOM ST	95811	HC-SPD	C-2-SPD
0010190007000	0 JIBBOOM ST	95811	HC-SPD	C-2-SPD
0010190009000	0 JIBBOOM ST	95811	HC-SPD	C-2-SPD
0010190011000	0 JIBBOOM ST	95811	HC-SPD	C-2-SPD

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00101900150000	240 JIBBOOM ST	95811	HC-SPD	C-2-SPD
00101900160000	0 JIBBOOM ST	95811	HC-SPD	C-2-SPD
00101900170000	0 JIBBOOM ST	95811	(F, HC)-SPD	(C-2, F)-SPD
00102000100000	700 N 5TH ST	95811	M-2-PC-SPD(N)	RMX-SPD
00102000110000	431 RICHARDS BL	95811	M-2-SPD(C), M-2-SPD(N)	(OB, RMX)-SPD
00102000120000	424 N 5TH ST	95811	(A-OS,OB,RMX)-PUD/SPD(C/N)	(A-OS, OB, RMX)-PUD/SPD
00102000130000	0 RICHARDS BL	95811	(OB, RMX)-PUD/SPD	ROW
00102000180000	500 SEQUOIA PACIFIC	95811	M-2-SPD(C), M-2-SPD(N)	(OB, RMX)-SPD
00102000190000	421 RICHARDS BL	95811	M-2-SPD(C), M-2-SPD(N)	(C-1, OB)-SPD
00102000230000	500 SEQUOIA PACIFIC	95811	M-2-SPD(N)	RMX-SPD
00102000250000	601 SEQUOIA PACIFIC	95811	M-2-SPD(N), M-2-PC-SPD(N)	RMX-SPD
00102000290000	650 N 5TH ST	95811	M-2-SPD(N)	RMX-SPD
00102000300000	600 N 5TH ST	95811	M-2-SPD(N)	RMX-SPD
00102000310000	630 SEQUOIA PACIFIC	95811	M-2-PC-SPD(N)	RMX-SPD
00102000320000	600 SEQUOIA PACIFIC	95811	M-2-SPD(N), M-2-PC-SPD(N)	RMX-SPD
00102000340000	0 RICHARDS BL	95811	(A-OS, RMX)-PUD/SPD(N)	(A-OS, OB, RMX)-PUD/SPD
00102000350000	551 SEQUOIA PACIFIC	95811	M-2-SPD(N)	(C-1, RMX)-SPD
00102000360000	0 SEQUOIA PACIFIC	95811	M-2-PC-SPD(N)	RMX-SPD
00102000370000	0 SEQUOIA PACIFIC	95811	M-2-SPD(C), M-2-SPD(N), M-2-PC-SPD(N)	(OB, RMX)-SPD
00102100040000	450 RICHARDS BL	95811	M-2-SPD(C)	(C-2, OB)-SPD
00102100050000	444 RICHARDS BL	95811	M-2-SPD(C)	(C-2, OB)-SPD
00102100060000	0 N 5TH ST	95811	M-2-SPD(C)	(C-2, OB)-SPD
00102100070000	500 RICHARDS BL	95811	M-2-SPD(C)	OB-SPD
00102100100000	300 N 7TH ST	95811	M-2-SPD(C)	(OB, R-5, RMX)-SPD
00102100120000	100 RICHARDS BL	95811	M-2-SPD(W)	M-2-SPD
00102100180000	0 STATE HW	95811	M-2-SPD	C-2-SPD
00102100240000	0 JIBBOOM ST	95811	M-2-SPD(W)	R-3A-SPD
00102100350000	0 N B ST	95811	M-2-SPD(C)	(R-5, RMX)-SPD
00102100370000	111 BERCUT DR	95811	M-2-SPD(W)	M-2-SPD
00102100380000	101 BERCUT DR	95811	M-2-SPD(W)	M-2-SPD
00102100410000	0 BANNON ST	95811	OB-PUD/SPD	RMX-SPD
00102100450000	300 RICHARDS BL	95811	OB-PUD/SPD	OB-SPD
00102100460000	0 RICHARDS BL	95811	OB-PUD/SPD	OB-SPD
00102100470000	420 RICHARDS BL	95811	OB-PUD/SPD	C-2-SPD
00102100480000	0 BANNON ST	95811	OB-PUD/SPD	C-2-SPD
00102100490000	0 RICHARDS BL	95811	OB-PUD/SPD	(C-2, OB)-SPD
00102100500000	325 N 5TH ST	95811	M-2-SPD(C)	(OB, RMX)-SPD
00102100510000	300 RICHARDS BL	95811	OB-PUD/SPD	C-2-SPD
00102100520000	0 BANNON ST	95811	OB-PUD/SPD	C-2-SPD
00102100530000	0 BANNON ST	95811	OB-PUD/SPD	(C-2, OB)-SPD
00200100060000	821 N B ST	95811	M-2-SPD(C)	C-2-SPD
00200200120000	0 N B ST	95811	C-4-SPD	ROW
00200310010000	916 N B ST	95811	M-2-SPD(C)	C-2-SPD

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00200310020000	130 N 12TH ST	95811	C-4-SPD	C-2-SPD
00200410730000	1400 A ST	95811	C-4-SPD	(C-2, C-4)-SPD
00200410740000	1500 A ST	95811	C-4-SPD	C-2-SPD
00200410750000	0 B ST	95811	C-4-SPD	A-OS-SPD
00200410770000	1400 N B ST	95811	C-4-SPD	C-2-SPD
00200410780000	111 N 16TH ST	95811	C-4-SPD	C-2-SPD
00200410790000	0 N B ST	95811	C-4-SPD	C-2-SPD
00200410800000	0 N B ST	95811	C-4-SPD	C-2-SPD
00200410820000	0 N B ST	95811	C-4-SPD	C-2-SPD
00200510020000	100 N 16TH ST	95811	C-4-SPD	C-2-SPD
00200550140000	0 B ST	95811	C-4-SPD	A-OS-SPD
00200550160000	1590 A ST	95811	C-4-SPD	C-2-SPD
00200550170000	0 A ST	95811	C-4-SPD	C-2-SPD
00200550180000	0 A ST	95811	C-4-SPD	C-2-SPD



# REPORT TO PLANNING COMMISSION City of Sacramento

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915 I Street, Sacramento, CA 95814-2671

**STAFF REPORT**  
**January 13, 2011**

Honorable Members of the Planning Commission:

**Subject:** Northeast Line Implementation Plan (LR09-021)

**Council District:** 2

**Recommendation:** Review and comment.

**Contact:** Greg Sandlund, Associate Planner, (916) 808-8931; Jim McDonald AICP, Senior Planner, (916) 808-5723.

**Presenters:** Greg Sandlund, Associate Planner, (916) 808-8931

**Department:** Community Development

**Division:** Planning

**Organization Number:** 22001111

## **Description/ Analysis**

**Issue:** The Northeast Line Implementation Plan is a planning effort to promote reinvestment, redevelopment, and revitalization along the light rail corridor that includes the Globe, Arden/Del Paso and Royal Oaks Stations. The Plan includes specific strategies to address housing, economic development, the strategic financing of infrastructure, public safety, and design needs along the light rail corridor.

The land use changes proposed are intended to better streamline uses that support an active and safe commercial corridor such as mixed use and mixed density housing as well as office and general commercial uses. Staff is also recommending that future infrastructure improvements be focused in key areas along the light rail corridor to encourage catalyst and near term development in the area.

This is a public workshop to solicit public and commission comments on the draft documents. Staff will return to the Planning Commission on February 11<sup>th</sup> for final action.

**Policy Considerations:** The Northeast Line Implementation Plan is implementing the 2030 General Plan, which amended land use designations in key opportunity areas, including light rail station areas and commercial corridors, to facilitate the revitalization of corridors and centers.

**Environmental Considerations:** At the time action is requested, staff will provide the appropriate discussion and findings to comply with the California Environmental Quality Act (CEQA).

**Rationale for Recommendation:** Staff would like any comments from the Commission prior the Commission taking formal action on the project on January 13<sup>th</sup> 2011.

**Financial Considerations:** None

Respectfully submitted by:   
Greg Sandlund  
Associate Planner

Recommendation Approved:

  
Jim McDonald AICP  
Senior Planner

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## Background

### Project Background

The Globe, Arden/Del Paso, and Royal Oaks Stations were built as part of the light rail starter line in 1987. Much of the land used for the starter line was existing right of way from freight rail lines. Therefore, most of the surrounding land uses were industrial or heavy commercial and not supportive of transit.

In 2002, Regional Transit and the City of Sacramento collaborated to identify land use and policy changes for areas within a 1/4 mile of transit stations to support transit. This planning effort was called Transit for Livable Communities (TLC).

As a follow up to the TLC planning effort, the Northeast Line Light Rail Stations Plan was approved by the City Council in 2007. This plan was predominately an urban design document that recommended, among other things: streetscape improvements, revisions to the North Sacramento Design Guidelines, rezones and urban design schemes for the Globe, Arden/Del Paso and Royal Oaks Station. The plan also analyzed the necessary infrastructure improvements to support 30 years of growth in project area.

The 2030 General Plan, adopted in 2009, amended land use designations in key opportunity areas, including light rail station areas and commercial corridors, to facilitate the revitalization of corridors and centers. The TLC and Northeast Line Light Rail Stations Plan informed the identification of the 2030 General Plan land use designations for this area.

### Project Description

The Northeast Line Implementation Plan is an effort to implement the previous planning efforts mentioned above and includes the following actions:

- ◆ Rezone specified sites;
- ◆ Amend general plan land use designations;
- ◆ Expand the boundaries of the Del Paso Boulevard Special Planning District;
- ◆ Amend the Del Paso Boulevard Special Planning District;
- ◆ Amend the North Sacramento Design Guidelines;
- ◆ Amend the North Sacramento Community Plan to establish a transit village plan;
- ◆ Amend the RMX Zone;
- ◆ Establish phased infrastructure finance recommendations.

### Rezones and General Plan Amendments:

The project includes rezoning sixteen parcels along Del Paso Boulevard to add the Transit Overlay Zone. This overlay zone will allow greater heights and densities than

the base General Commercial (C-2) Zone as well as allow for expedited application review for transit friendly development. These zoning designations are consistent with the 2030 General Plan which was adopted on March 3, 2009.

A single site would be rezoned from the Standard Single Family (R-1) Zone to the General Commercial (C-2) Zone. Until recently, this site was used a firehouse. The C-2 designation would be consistent with adjacent and nearby parcels along Del Paso Boulevard.

Twenty six parcels, located between Del Paso Boulevard and the Royal Oaks Station, are proposed to be rezoned from the Standard Single Family (R-1) Zone to the Residential Mixed Use (RMX) Zone. The RMX zone would allow for neighborhood and transit friendly commercial uses along Arden Way. It would also allow for future multi-family housing to be located nearby the Del Paso/Arden and Royal Oaks stations. Rezoning these parcels will require an amendment to the general plan land use designations, from Traditional Low Density Residential to Urban Corridor Low.

Approximately 110 parcels located northwest of Del Paso Boulevard are proposed to have amended general plan designations. Ten of the 110 parcels would have land use designations changed from Urban Corridor Low to Employment Center Low Rise. The rest of the 110 parcels would have land use designations changed from Urban Low Density Residential to Employment Center Low Rise. The purpose of these land use amendments is to continue to allow viable industrial uses to operate and allow for a more gradual transition of the area from a predominantly an industrial area to one of a more commercial/residential nature.

#### **Amend and Expand the Del Paso Boulevard Special Planning District:**

The project includes an expansion of the Del Paso Boulevard Special Planning District to include parcels, one block deep, located along the north side of Arden Way as well as the parcels immediately south of the Royal Oaks Station. These parcels are proposed to be included in the Special Planning District (SPD) because of their location along a busy corridor and their close proximity to light rail stations. The expansion of the SPD into Arden Way will change to name of the SPD to the Del Paso/Arden Special Planning District.

Additionally, one parcel on the southwest edge of the SPD and twelve parcels north of Del Paso Boulevard, fronting El Monte Avenue, would be included in the SPD. These parcels are proposed to be included in the district because of their current non-residential uses and their close proximity to the commercial corridor.

The amendments to the Special Planning District will help to facilitate a more flexible and expedited planning application process for uses that support the commercial corridor. Additionally, residential mixed use developments would be allowed with a plan review, as opposed to a special permit. The specific changes to the SPD are listed in Attachment 4.

**Design Review Guidelines Amendments:** The project includes amendments to the North Sacramento Design Review Guidelines that incorporate design guidelines from

the Northeast Light Rail Stations Plan. These new design guidelines would enhance the existing residential and commercial guidelines and also give specific guidance on transit friendly housing such as live-work lofts, town houses/row houses, and residential mixed use developments.

**North Sacramento Community Plan Amendments:** The project includes amending the North Sacramento Community Plan to include new policies resulting from the Northeast Line Implementation Plan effort as well as policies from the Northeast Light Rail Stations Plan. These policies are consistent with the existing 2030 General Plan policies. Policy additions include: those that designate the Northeast Line section of the North Sacramento Community Plan as a transit village plan; and the addition of a new map showing the Northeast Line station area.

The new section in the North Sacramento Community Plan would include the designation of the Globe, Arden/Del Paso, and Royal Oaks stations a transit village districts per the California Transit Village Development Planning Act of 1994 (Section 65460 *et al* of the State of California Government Code). Under State law, a transit village plan shall include land within  $\frac{1}{4}$  mile from the station; should encourage development in close proximity to the transit station; should offer intermodal service; should include a mix of uses and housing types; and provide a number of benefits such as increased infill, greater transit ridership and live-travel opportunities. A transit village plan shall be prepared, adopted, and amended in the same manner as a general plan. The City's General Plan was adopted by City Council resolution and this transit village plan will be adopted through a resolution.

**Amendments to the RMX Zone:** Staff is recommending that parcels zoned RMX along Arden Way be able to have up to 100% commercial uses with a zoning administrator's special permit. After initially considering this provision to be applied only in the special planning district, staff reasoned that such a provision should be applied citywide. The amendments to the RMX zone will allow for greater flexibility in permitting neighborhood supporting commercial uses while still emphasizing residential mixed use.

**Phased Infrastructure Finance Recommendations:** The infrastructure finance strategy will include specific recommendations for the public/private financing of prioritized infrastructure improvements in the study area. The recommendations will be for near term improvements that will help facilitate catalyst development in the area.

**DRAFT ORDINANCE NO.**

Adopted by the Sacramento City Council

**AMENDING TITLE 17 OF THE SACRAMENTO CITY CODE  
(THE ZONING CODE) BY REZONING VARIOUS PARCELS  
OF REAL PROPERTY AS PART OF THE NORTHEAST LINE  
IMPLEMENTATION PLAN (LR09-021)**

BE IT ENACTED BY THE COUNCIL OF THE CITY OF SACRAMENTO:

**Section 1.** Title 17 of the Sacramento City Code (the Zoning Code) is amended by rezoning the properties depicted in the attached Exhibit A and identified by APN and address in the attached Exhibit B, from the existing zone to the proposed zone as set forth in Exhibit B.

**Section 2.** Rezoning of the property shown in the attached Exhibit A, by the adoption of this Ordinance, will be considered to be in compliance with the requirements for the rezoning of property described in the Zoning Code, as amended, as those procedures have been affected by recent court decisions.

**Section 3.** The City Clerk of the City of Sacramento is directed to amend the official zoning maps, which are part of the Zoning Code, to conform to the provisions of this Ordinance.

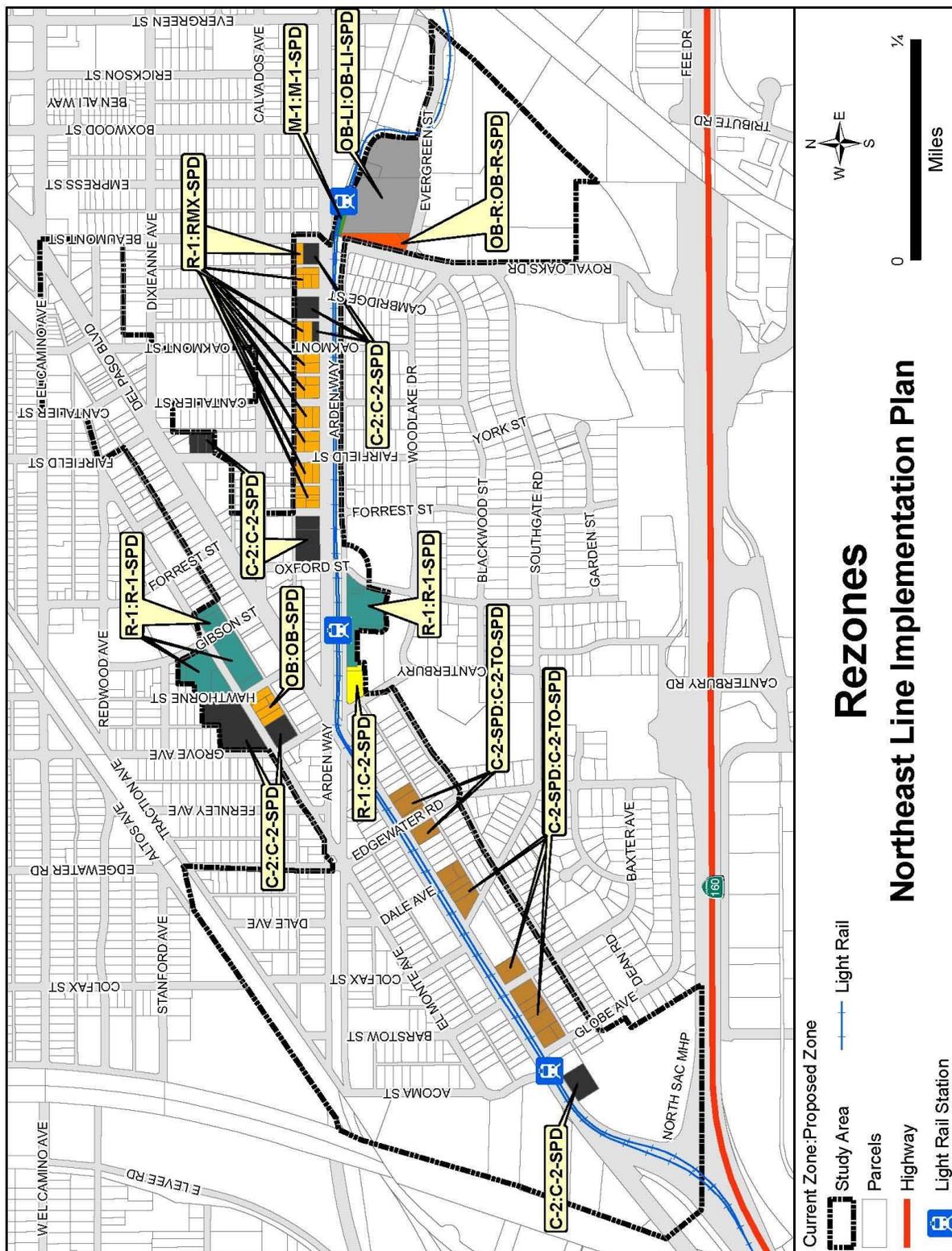
**Section 4.** Exhibits A and B are a part of this Ordinance.

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Exhibit A – Rezone Maps

Exhibit B – List of Rezone Properties

Exhibit A



**Exhibit B**

**[Property List to be Generated Prior to the Hearing]**

**DRAFT RESOLUTION NO.**

Adopted by the Sacramento City Council

**AMENDING THE 2030 GENERAL PLAN LAND USE AND URBAN FORM DIAGRAM RELATING TO THE NORTHEAST LINE IMPLEMENTATION PLAN (LR09-21)**

**BACKGROUND**

- A. On October 15, 2002, the City Council accepted the Transit for Livable Communities (TLC) recommendations, which provided recommendations and strategies for transit-supportive development proximate to existing and future light rail stations.
- B. On July 24, 2007, the City Council accepted the Northeast Line Light Rail Stations Plan as the guiding vision for development within the quarter mile radius around the Globe, Arden/Del Paso, and Royal Oaks light rail stations. This plan consisted of design guidelines, recommended land use changes and an infrastructure assessment.
- C. On March 3, 2009, the City Council adopted the 2030 General Plan, which includes land use and policy direction to promote infill development in key opportunity areas, including commercial corridors and areas served by transit, such as the Northeast Line Light Rail Corridor.
- D. The 2030 General Plan Urban Corridor Low and Urban Neighborhood Low land use designation for the area known as the El Monte Triangle have been re-evaluated and found to not acknowledge the many viable industrial uses in the area. The Employment Center Low Rise general plan land use designation is consistent with the current heavy commercial uses as well as future urban uses, including office, retail, and housing.
- E. On January 13, 2011, the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve proposed amendments to the 2030 General Plan Land Use and Urban Form Diagram consistent with the Northeast Line Implementation Plan.
- F. On \_\_\_\_\_, the City Council conducted a public hearing, for which notice was given pursuant Sacramento City Code Section 17.200.010(C)(1)(a) (publication).

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:**

**Section 1.** Environmental Determination: The City Council has approved the environmental review of the Project as being within the scope of the 2030 General Plan Master EIR by Resolution No. \_\_\_\_.

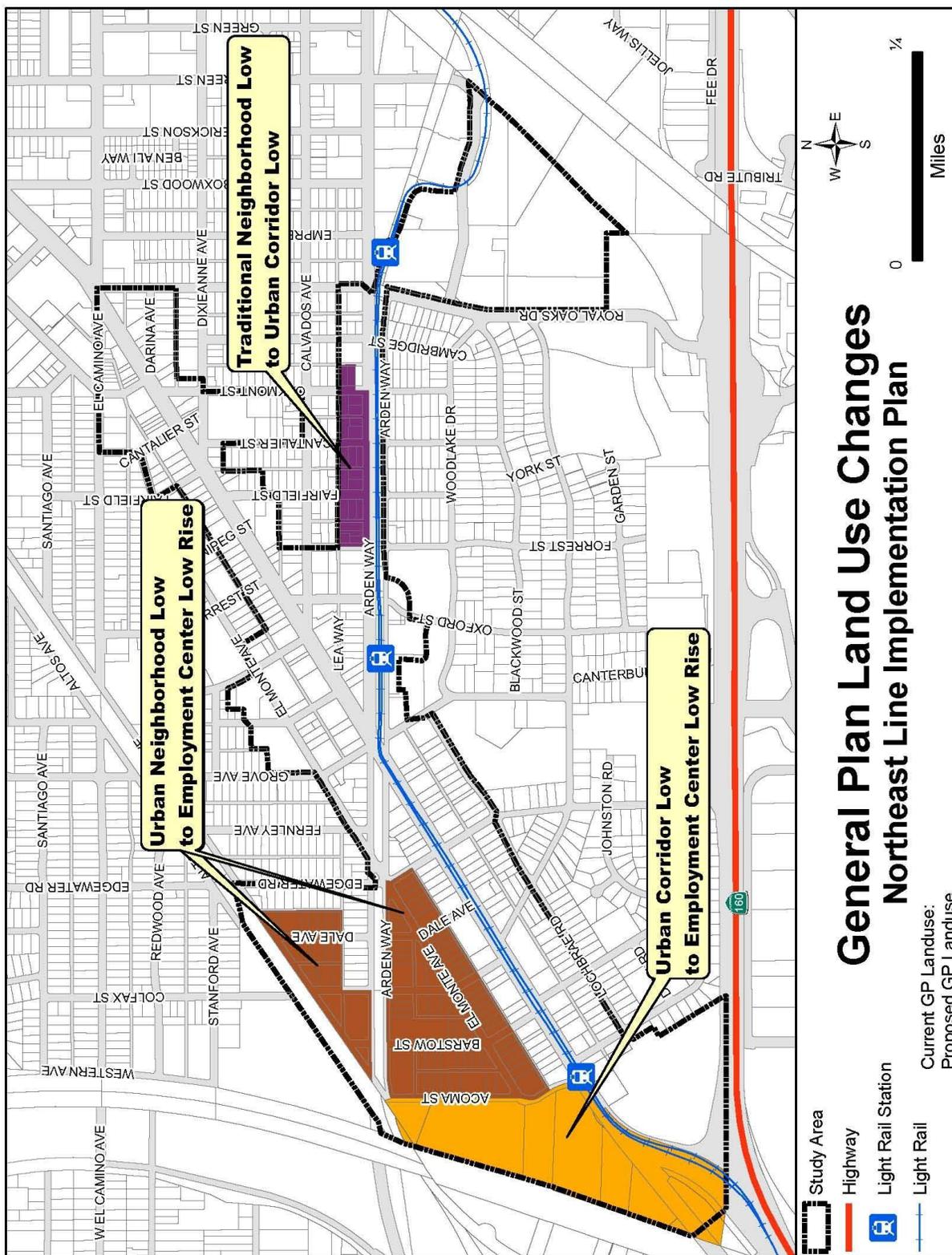
**Section 2.** Based on the verbal and documentary evidence received at the hearing, the City Council approves the 2030 General Plan Land Use and Urban Form Diagram Amendment as set forth in Exhibits A and B.

**Section 3.** Exhibits A and B are a part of this Resolution.

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EXHIBIT A: Land Use Diagram Changes Maps

EXHIBIT B: Land Use Changes Property List



**Exhibit B**

**[Property List will be Generated Prior to Hearing]**

**Attachment 4****Highlighted Draft Amendments to the  
Del Paso Blvd Special Planning District**

- Change the name to Del Paso Boulevard/Arden Way Special Planning District in section 17.20.030 and throughout 17.108
- Allow apartments in the General Commercial (C-2) Zone with a planning directors plan review (instead of a zoning administrators special permit)
- Set the maximum allowable density for residential uses in the General Commercial (C-2) Zone to be 60 dwelling units per net acre
- Require that new residential development of 12 dwelling units per net acre include the following open space standards:
  - A minimum of fifty (50) square feet of usable common open space per unit is required. This open space area may include courtyards, gardens, recreation areas, and similar areas.
  - A minimum of fifty (50) square feet of usable private open space per unit is required. This area is for the exclusive use of the unit and may include decks, balconies and patios. Private useable open space shall be directly accessible from the unit.
  - For each square foot of usable private open space over fifty (50) square feet that is provided, the required fifty (50) square feet of usable common open space may be reduced by one square foot.
- Require that manufacturing uses fronting Del Paso Boulevard in the General Commercial (C-2) Zone have an office or other active commercial use facing the street
- Allow height, yard, and stepback standards to be modified through the design review process at the director or commission level
- Allow up to 50% residential uses in the Office (OB) Zone with a zoning administrators plan review

**DRAFT RESOLUTION NO. 2010- \_\_\_\_**

Adopted by the Sacramento City Council

**AMENDING THE NORTH SACRAMENTO COMMUNITY PLAN CHAPTER OF THE 2030 GENERAL PLAN TO ADD POLICIES FOR THE NORTHEAST LINE TRANSIT VILLAGES AND TO ESTABLISH THE NORTHEAST LINE TRANSIT VILLAGE DEVELOPMENT DISTRICTS FOR THE GLOBE, ARDEN/DEL PASO, AND ROYAL OAKS LIGHT RAIL STATIONS (LR09-021)****BACKGROUND**

- A. On October 15, 2002, the City Council accepted the Transit for Livable Communities (TLC) recommendations, which provided recommendations and strategies for transit-supportive development proximate to existing and future light rail stations.
- B. On July 24, 2007, the City Council accepted the Northeast Line Light Rail Stations Plan as the guiding vision for development within the quarter mile radius around the Globe, Arden/Del Paso, and Royal Oaks light rail stations. This plan consisted of design guidelines, recommended land use changes and an infrastructure assessment.
- C. On March 3, 2009, the City Council adopted the 2030 General Plan, which includes land use and policy direction to promote infill development in key opportunity areas, including commercial corridors and areas served by transit, such as the Northeast Line Light Rail Corridor.
- D. On October 6, 2009, the City Council designated the Northeast Line Light Rail Corridor as a Tier 2, shovel-ready area in order to promote reinvestment efforts in the area and to prepare the area for new development that would fulfill the vision of the 2030 General Plan and other past planning efforts.
- E. The policies in Exhibit A, comprising the Northeast Line Transit Village Plan for the Globe, Arden/Del Paso, and Royal Oaks stations, are consistent with the goals and policies of the North Sacramento Community Plan and the 2030 General Plan.
- F. The policies included in Exhibit A of this resolution support the City's vision for the Northeast Line Light Rail Corridor and were drafted in accordance with the provisions of the State Transit Village Development Act (Government Code section 65460 et seq.), which encourages mixed-use development at higher

densities around transit stations.

- G. On January 13, 2011 the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve the components of the Northeast Line Implementation Plan, including the amendments to the North Sacramento Community Plan chapter of the 2030 General Plan as set forth in Exhibit A (LR09-021).
- H. On \_\_\_\_\_, the City Council conducted a public hearing, for which notice was given pursuant Sacramento City Code Sections 17.200.010(C)(1) (a) and (c) (publication and mail (500 feet)), and received and considered evidence concerning the Northeast Line Implementation Plan, including the amendments to the North Sacramento Community Plan chapter of the 2030 General Plan as set forth in Exhibit A (LR09-021).

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:**

**Section 1.** Environmental Determination: The City Council has approved the environmental review of the Project as being within the scope of the 2030 General Plan Master EIR by Resolution No. \_\_\_\_.

**Section 1.** The North Sacramento Community Plan chapter of the 2030 General Plan is hereby amended to add the language and policies related to urban development in the Northeast Line Corridor as identified in Exhibit A.

**Section 2.** All that land within the North Sacramento Community Plan that is not more than a quarter mile from the Globe light rail station is hereby designated the Globe Transit Village Development District pursuant to the Transit Village Development Planning Act of 1994 (Government Code section 65460 et seq.). The Northeast Line Transit Village Plan set forth in Exhibit A, supporting policies have been prepared and are adopted as the transit village plan for the district.

**Section 3.** All that land within the North Sacramento Community Plan that is not more than a quarter mile from the Arden/Del Paso light rail station is hereby designated the Arden/Del Paso Transit Village Development District pursuant to the Transit Village Development Planning Act of 1994 (Government Code section 65460 et seq.). The Northeast Line Transit Village Plan set forth in Exhibit A, supporting policies have been prepared and are adopted as the transit village plan for the district.

**Section 4.** All that land within the North Sacramento Community Plan that is not more than a quarter mile from the Royal Oaks light rail station is hereby designated the Royal Oaks Transit Village Development District pursuant to the Transit Village Development Planning Act of 1994 (Government Code section 65460 et seq.). The Northeast Line Transit Village Plan set forth in Exhibit A, supporting policies have been prepared and are adopted as the transit village plan for the district.

**Section 5.** Exhibit A is a part of this Resolution.

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Exhibit A – North Sacramento Community Plan Amendment Language and Figures

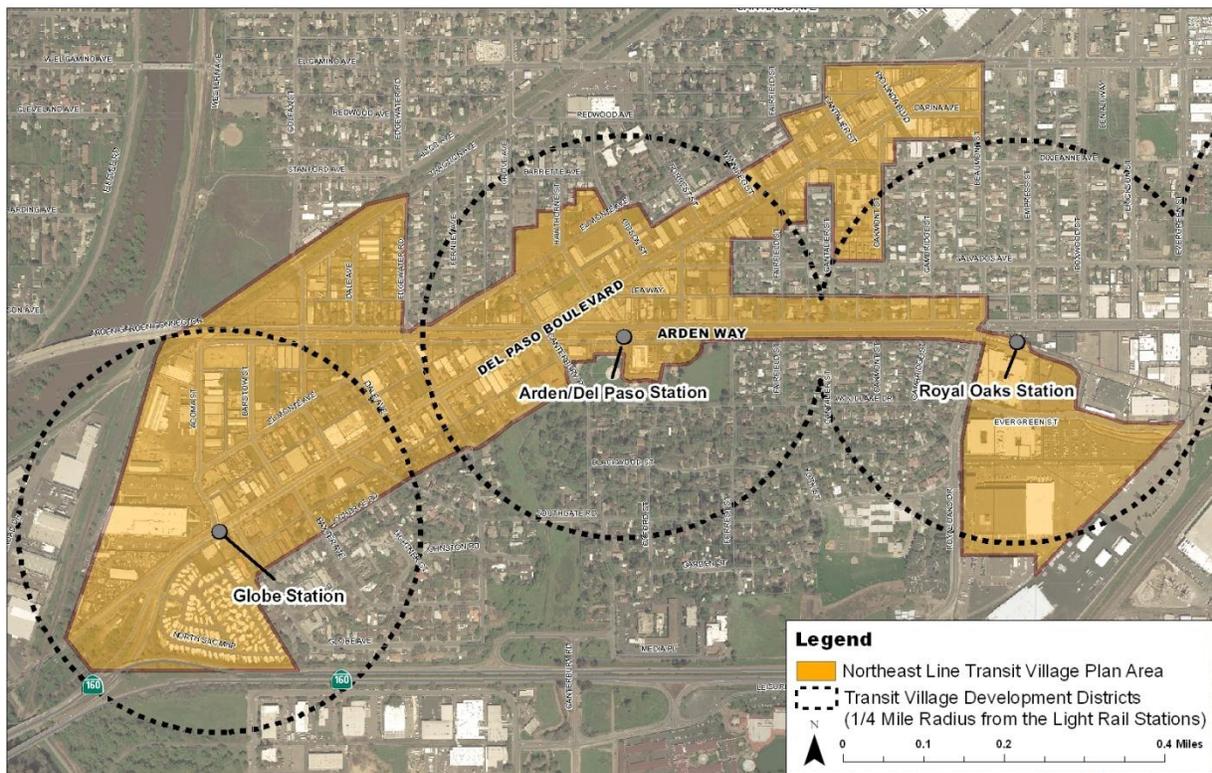
## Exhibit A

## North Sacramento Community Plan Amendment Language and Figures

[To be inserted after the infrastructure challenges discussion on page 3-NS-17 of the North Sacramento Community Plan chapter of the City's 2030 General Plan.]

### Policies for the Northeast Line Transit Village Plan

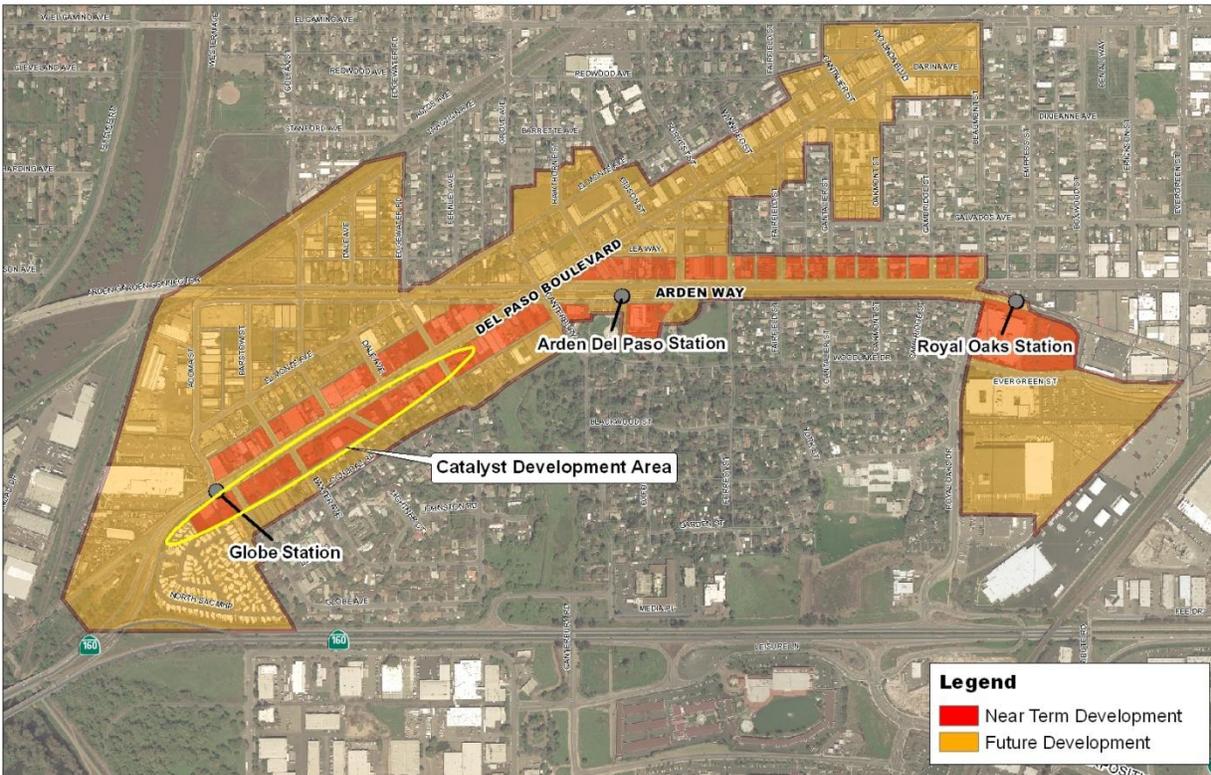
In order to promote reinvestment and the long-term success of the Northeast Line Light Rail Corridor, the City prepared the Northeast Line Implementation Plan (2011), a planning effort to promote new housing, economic development, the strategic financing of infrastructure, public safety, and design needs along the light rail corridor that includes the Globe, Arden/Del Paso, and Royal Oaks stations. The Plan is based on previous planning efforts, including the Northeast Line Light Rail Stations Plan (2007) and Transit for Livable Communities (2002).



**Figure NS-NELTV 1: Northeast Line Transit Village Plan Area** (Pursuant to the Transit Village Development Act of 1994 [Government Code section 65460 et seq.]

The Northeast Line Transit Village shown in Figure NS-NELTV 1 above has three transit village development districts, which are encompassed by land within a 1/4 mile radius of the Globe, Arden/Del Paso and Royal Oaks Stations. These three separate transit village development districts are each subject to polices of the overall Transit Village Plan Area where the transit village development districts overlap

the Northeast Line Transit Village Plan area. The Northeast Line Transit Village Plan as well as the Globe, Arden/Del Paso, and Royal Oaks Transit Village Development Districts have been adopted pursuant to State law and embody both the State and City's vision of intensified development near transit and mixed-use activity centers, which in turn will lead to increased walking and reduced automobile use.



**Figure NS-NELTV-2 – Policy Area for the Northeast Line Transit Village**

The policies included in this section will help to shape a transit village that efficiently utilizes the land around each light rail station and provides a mix of uses that benefit the surrounding community. The areas that will accommodate catalyst development and near term development are shown in Figure NS-NELTV-2, above. Specific infrastructure improvements to facilitate development in these areas have been identified in the 2011 report entitled “Northeast Line Light Rail Stations Plan Phased Infrastructure Recommendations.” Parking facilities shall be developed when on street parking is required to promote economic development.

**NS.NELTV 1.1 Active Ground Level Uses.** The City shall require larger residential mixed use projects along Del Paso Boulevard to have active ground level uses built up to the right of way in order to provide strong street definition and an active edge along the sidewalk. (RDR)

**NS.NELTV 1.2 Prioritized Infrastructure Improvements.** The City shall prioritize infrastructure improvements to support the catalyst development indicated in Figure NS-NELTV-2, above. (SO)

- NS.NELTV 1.3**      **Street Walls.** The City shall ensure that each block along Del Paso Boulevard has a predominant street wall. The street wall shall have a consistent height, be composed of contiguous buildings, and have upper stories stepped back when necessary. (RDR)
- NS.NELTV 1.4**      **Sensitivity to Adjacent Neighborhood Scale.** The City shall ensure that development along Del Paso Boulevard and Arden Way is sensitive to adjacent neighborhood scale and provide a height and mass transition to the medium to higher density development at the corridor. (RDR)
- NS.NELTV 1.5**      **Existing Industrial and Service Oriented Uses.** The City shall allow for the retention and continued operation of existing light industrial and service oriented uses, while providing for a comfortable coexistence with future new residential and commercial development. (RDR)
- NS.NELTV 1.6**      **Ground Floor Visibility.** The City shall require windows to be provided on the street level of new buildings in the Northeast Line Transit Village as a visual link between business and pedestrians. Ground-floor commercial facades facing streets, sidewalks, pedestrian routes and public plazas shall have non-reflective, transparent windows. (RDR)
- NS.NELTV 1.7**      **Parking.** The City shall support reduced parking ratios for transit oriented residential or commercial development in the transit village area while promoting the efficient design and use of parking, including curbside parking, shared parking, and the use of parking structures for higher density development and park-and-ride areas. (RDR)
- NS.NELTV 1.8**      **Temporary Parking Facilities along Del Paso Boulevard.** The City shall work with the Sacramento Housing and Redevelopment Agency to provide temporary parking facilities along Del Paso Boulevard when necessary. (IGC)

**DRAFT ORDINANCE NO.**

Adopted by the Sacramento Council

**AMENDING SECTION 17.28.030 OF TITLE 17  
OF THE SACRAMENTO CITY CODE (THE ZONING  
CODE) RELATING TO THE RESIDENTIAL MIXED USE  
ZONE (LR09-021)**

**BE IT ENACTED BY THE COUNCIL OF THE CITY OF SACRAMENTO:**

**Section 1.** Section 17.28.030 of Title 17 of the Sacramento City Code (the Zoning Code) is amended as follows:

A. Subsection A of Section 17.28.030 is amended to read as follows:

A. Nonresidential Development Limitations.

1. For new development in the RMX zone, commercial and office uses are limited to the ground floor only and may occupy up to a maximum of fifty (50) percent of the building square footage; provided, that

a. On lots that are less than or equal to three acres in size, the percentage of commercial or office use may be increased up to 100% of the building square footage, subject to approval of a zoning administrator's special permit;

b. On lots that are greater than 3 acres in size, the percentage of commercial or office use may be increased up to 100% of the building square footage, subject to approval of a planning commission special permit.

2. The design of the proposed commercial or office development shall conform to the commercial corridor design principles adopted under Section 17.132.180 as they may be amended from time to time. The commercial corridor design principles shall be applied in addition to the design guidelines applicable under Chapter 17.132, Design Review, if any. In the event of a conflict, the design guidelines applicable under Chapter 17.132 shall take precedence over the commercial corridor design principles.

3. An architecturally or historically significant structure of any size may be converted entirely to commercial or office uses, subject to approval of a zoning administrator's special permit, in order to ensure preservation and maintenance of the structure. The intent of this provision is to make structural repair and restoration economically viable, and ensure the community's continued benefit from the preservation of the significant structure.,

4. The percentage of nonresidential use may be increased up to one hundred (100) percent of the building square footage if the building is occupied by a community or neighborhood-based nonprofit organization, subject to approval of a zoning administrator's special permit.

B. Except as specifically amended by the amendments to subsection A, Section 17.28.030 remains unchanged and in full force and effect.

**DRAFT RESOLUTION NO.**

Adopted by the Sacramento City Council

**AMENDING THE NORTH SACRAMENTO DESIGN GUIDELINES AS  
PART OF THE NORTHEAST LINE IMPLEMENTATION PLAN (LR09-21)**

**BACKGROUND**

- A. On October 15, 2002, the City Council accepted the Transit for Livable Communities (TLC) recommendations, which provided recommendations and strategies for transit-supportive development proximate to existing and future light rail stations.
- B. On July 24, 2007, the City Council accepted the Northeast Line Light Rail Stations Plan as the guiding vision for development within the quarter mile radius around the Globe, Arden/Del Paso, and Royal Oaks light rail stations. This plan consisted of design guidelines, recommended land use changes and an infrastructure assessment.
- C. On March 3, 2009, the City Council adopted the 2030 General Plan, which includes land use and policy direction to promote infill development in key opportunity areas, including commercial corridors and areas served by transit, such as the Northeast Line Light Rail Corridor.
- D. Design guidelines from the Northeast Line Light Rail Stations Plan will augment the North Sacramento Design Guidelines and give specific design direction for housing types that will occupy the urban corridor.
- E. On January 12, 2011 the City Design Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve the proposed amendments to the North Sacramento Design Guidelines, for which notice was given pursuant to Sacramento City Code Section 17.200.010(C)(2)(a) (publication).
- F. On \_\_\_\_\_, the City Council conducted a public hearing, for which notice was given pursuant to Sacramento City Code Section 17.200.010(C)(2)(a) (publication).

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL  
RESOLVES AS FOLLOWS:**

**Section 1.** Environmental Determination: The City Council has approved the environmental review of the Project as being within the scope of the 2030 General Plan Master EIR by Resolution No. \_\_\_\_.

**Section 2.** Based on the verbal and documentary evidence received at the hearing, the City Council approves the amendments to the North Sacramento Design Guidelines as set forth in Exhibit A.

**Section 3.** Exhibit A is a part of this Resolution.

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EXHIBIT A: Amended North Sacramento Design Guidelines

Exhibit A

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## Multi-family Residential

### 27 Interior Common Spaces

#### Design Principle

Multi-family structures should provide interior common spaces that are easily accessible to residents. Individual units adjacent to common spaces should have facades with entry features and windows that open onto common spaces, where possible.

#### Rationale

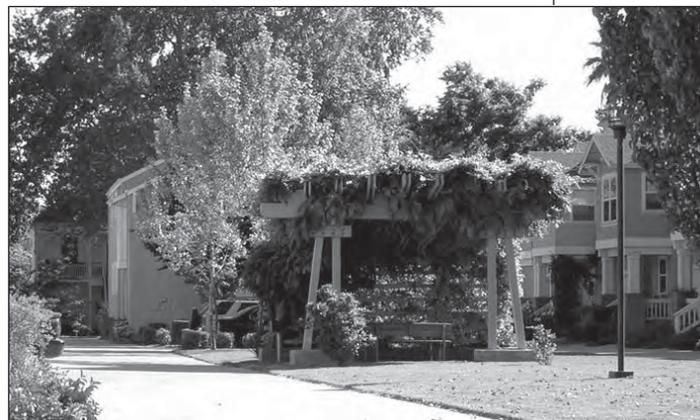
Interior common spaces should foster a sense of community by designing buildings that allow residents to see and access common spaces. Common spaces should offer amenities that invite use, such as seating, shade, and tot lots.

#### Design Guidelines

- 27-1 Ground floor units should have doorways that open onto interior common spaces.
- 27-2 All units that overlook interior common spaces should have windows that allow residents to easily see these areas.
- 27-3 Common amenities, such as tot lots, seating areas, and swimming pools, should be provided that cater to all age ranges, from small children to the elderly, as appropriate.
- 27-4 Common facilities such as recreation rooms, and laundry and mail areas should be located adjacent to common open space to increase activity in these areas.
- 27-5 Common open space should be designed as a visible, accessible transition between the street and individual units.
- 27-6 Outside storage facilities for (bicycles, bbq's, ect.) are strongly encouraged to minimize clutter on balconies.



*Interior common spaces can offer seating and areas for informal activities.*



*This multi-family complex has an inviting interior common space with picnic area.*

## Town House and Row House

Town houses and Row houses are defined as multi-story single-family residential units and are currently the most market-friendly building prototype. Row houses generally front public streets, while town houses are often located along internal pedestrian pathways and mews.

Development can also be designed to have more of a multi-family character. Depending on the intended character of the development, staff and the applicant can refer either to the single family section of these guidelines or the multi-family section for further design guidance.



*Row houses that face the street create an attractive environment.*

## Town House and Row House

### SITE DESIGN

This section addresses the location of row houses and town house on their lots, its overall layout relative to the site, its orientation toward the street and adjacent buildings, and the location of parking and utilities. Good site design of row house and town house structures, should:

- complement the scale, massing and setbacks of existing detached homes on the block;
- structures located in or near a commercial corridor may have smaller setbacks similar to the guidelines for new commercial buildings;
- provide an entry facing the street to create a welcoming appearance and to give homes “curb appeal”;
- guest parking areas, utilities, and service facilities should be located toward the interior of the site;
- common spaces should be toward the interior of the site.

## Town House and Row House

### 39 Relationship to the Street

#### Design Principle

Development should present a facade that encourages interaction with the street by including entry features, windows, and landscaping along the street side of the building.

#### Rationale

Development adjacent to a public street should encourage residents to actively engage with that street through a variety of design elements. In addition to improving the visual quality of the streetscape, design elements should allow residents to see and be seen from the street, enhancing neighborhood interaction, improving safety and providing "eyes on the street."

#### Design Guidelines

- 39-1 Maximize the number of units and building entries fronting the street to allow maximum "eyes on the street".
- 39-2 Configure residential developments so that the majority of the units minimize exposure to the south-west and west sun while still allowing plenty of light and ventilation from at least two sides in each unit.
- 39-3 Provide parking in the rear of the lots accessed by existing alleys and new minimum 20 feet wide driveways.
- 39-4 Ensure adequate (5-20 ft) setbacks for each unit to allow for open spaces for gardening, barbecuing, etc.
- 39-5 Where possible, provide variation in front facade depth to enrich the pedestrian experience.
- 39-6 Stepback upper floors to create opportunities for balconies.



*Maximize the number of units and building entries fronting the street to allow maximum "eyes on the street".*

## Town House and Row House



*Design front setbacks to allow maximum opportunities for interaction between residents and neighbors.*



*This development has setbacks similar to those of surrounding single-family homes.*



*This development has smaller setbacks that are similar to those of adjacent commercial buildings.*

### 40 Setbacks

#### Design Principle

Setbacks of structures should reflect the appropriate commercial or residential context.

#### Rationale

When development is placed on busy commercial streets, smaller setbacks that locate the building closer to the street are preferred. Development constructed near single-family residential neighborhoods should reflect the larger setbacks typically found in those areas.

#### Design Principles

- 40-1 Development should be designed with varied setbacks that contribute to an interesting streetscape and avoid a monotonous streetwall. Continuous lines of buildings with the same setback should be avoided.
- 40-2 Individual buildings can also be designed with an articulated front, with porches closer to the street.
- 40-3 In residential neighborhoods, row house and town house should adopt the predominant setback, but should also vary the building facade to relieve the appearance of mass.
- 40-4 In residential neighborhoods, design front setbacks to allow maximum opportunities for interaction between residents and neighbors.
- 40-5 In commercial areas, setbacks that locate buildings close to the street are preferred.

## Town House and Row House

### 41 Scale and Mass

#### Design Principle

Development should be compatible with the scale and mass of existing structures in the vicinity.

#### Rationale

Development should use design and construction methods that minimize the appearance of mass with multiple rooflines, articulated facades, and architectural detailing that break up the facade.

#### Design Guidelines

- 41-1 Development that is constructed as infill near an existing single-family residential neighborhood should provide a streetside facade that is complementary to these single-family homes in style and massing.
- 41-2 Encourage two- to four-story buildings.
- 41-3 Setback upper floors to create opportunities for balconies.
- 41-4 Multi-story structures should be articulated to break up the facade and minimize massing.
- 41-5 Two-story structures should have multiple rooflines with corresponding gables that are consistent in style and materials with the overall structure.
- 41-6 Architectural detailing, such as dormer and other types of decorative windows, complementary trim, porch details, decorative shutters, color and wainscoting, should vary from unit to unit to reduce the appearance of bulk and mass by providing visual interest.



*This three-story development sets the third floor back and has a facade that is complementary to nearby single-family homes.*

## Town House and Row House

### 42 Circulation

#### Design Principle

A network of public streets, internal streets, driveways, and paseos should be used throughout the development to enhance circulation within the site and connectivity to the adjacent neighborhood.

#### Rationale

Good site design of streets, driveways, and paseos enhances the interaction between pedestrians and motorists. A hierarchy of circulation options will promote safety and add to the character of the development.

#### Design Guidelines

- 42-1 A network of public streets, internal streets, driveways, paseos etc. is encouraged, when feasible.
- 42-2 Driveways should be designed to be accessible and safe for both pedestrians and motorists.
- 42-3 Internal paths such as paseos should be designed to improve pedestrian circulation and connections throughout the site.
- 42-4 Pedestrian connections to adjacent existing or future retail developments is encouraged.

## Town House and Row House

### 43 Interior Common Spaces

#### Design Principle

Development should provide interior common spaces that are easily accessible. Individual units adjacent to common spaces should have facades with entry features and windows that open onto those common spaces.

#### Rationale

Interior common spaces should ideally foster a sense of community. This can be facilitated by building facades that allow residents to see and easily use common spaces. Common spaces should offer amenities that invite use, such as seating, shade, and tot lots.

#### Design Guidelines

- 43-1 Units should have doorways that open onto interior common spaces.
- 43-2 All units that overlook interior common spaces should have windows that allow residents to easily see these areas.
- 43-3 Common amenities, such as tot lots, seating areas, and swimming pools, should be provided that cater to all age ranges, from small children to the elderly, as appropriate.
- 43-4 Common open space should be designed as a visible, accessible transition between the street and individual units.
- 43-5 Outside storage facilities for (bicycles, bbq's, ect.) are strongly encouraged to minimize clutter on balconies.



*Development with doors and windows that face out on the common open space area.*



*This development has a common area with amenities such as play equipment.*

## Town House and Row House



The garages are located at the rear of this row house development.



Access to these garages is at the rear of each unit.

### 44 Garages

#### Design Principle

Row house garages should be located in the rear of the unit and accessed by an internal street or alley. Town house garages should be located at the front of the unit.

#### Rationale

To minimize the visual prominence of garages row house and town house garages should be designed to blend into the structure.

#### Design Guidelines

- 44-1 Row house developments should use tuck-under or below grade garages.
- 44-2 Town house developments are encouraged to use two car tandem garages rather than traditional two car garages to reduce the visual impact of large garage doors, when feasible.
- 44-3 Garage doors should have small opaque or transparent windows, to allow light into the garage and to reduce the visual prominence of the door.

## Town House and Row House

### 45 Guest Parking

#### Design Principle

Guest parking should be located on internal streets throughout the site. Parking lots that face the street or are on the side of row house and town house should be minimized.

#### Rationale

Development should encourage residents to have an active relationship with the street(s) adjacent to the development. To this end, guest parking should be located in the interior of the development so as not to interfere with access to the street or interior common spaces.

#### Design Guidelines

- 45-1 Parking lots shall conform to City Municipal Code Section 17.64.030, "development standards for parking facilities," which specifies stall size and design.
- 45-2 Smaller, scattered lots will provide better access to residents and be less visually obtrusive than a single large lot.
- 45-3 Parking areas should be screened from adjacent structures with landscaping strips. However, screening should not exceed 4 feet in height, and should be permeable so that areas can be viewed by passing pedestrians and vehicles.
- 45-4 Underground parking in private or shared garages accessible from the street is acceptable if it does not interfere with pedestrian access to the street.
- 45-5 Provide parking in the rear of lots accessed by side streets or alleyways.

## Lofts and Live Work Units

Lofts and live-work units allow for flexible spaces that can be used for both residential and non-residential purposes. This building prototype is well suited for the largely industrial sections of North Sacramento as the transit stations area transition into non-industrial mixed use residential neighborhoods. Industrial character and design refers to a style that evokes back to the reuse of structures. Although new construction does not necessarily have to follow an industrial character or design.

For further design guidance please refer to the multi-family section of these guidelines.



*Live-work lofts.*

## Lofts and Live Work Units

### 46 Orientation and Layout

#### Design Principle

Lofts and live work units should be oriented towards public streets to increase pedestrian interaction and facilitate activity between residential and non-residential building uses.

#### Rationale

Proper building orientation can promote pedestrian friendly design and energy efficiency.

#### Design Guidelines

- 46-1 Orient the flexible space component of the unit towards the public realm of streets and pedestrian pathways to optimize business visibility.
- 46-2 Facades with large amounts of glazing should be oriented towards the north to minimize glare and reduce heat gain.



*Live work units flex space oriented towards public realm.*

## Lofts and Live Work Units

### 47 Massing & Setbacks

#### Design Principle

Maintain an industrial nature of the building while signaling the human, residential elements of the use. Building massing and setbacks should occur at a human scale and promote connectivity to streets, and complements the best examples of surrounding massing and setbacks..

#### Rationale

Massing and setbacks will transition smoothly from predominate uses that surround the property.

#### Design Guidelines

- 47-1 Encourage floor-to-floor heights of fifteen feet.
- 47-2 Allow five to fifteen foot wide front setbacks to provide privacy and to accommodate architectural elements such as colonnades and awnings.
- 47-3 Encourage the street facing facades to be vertical with little or no setbacks.



Loft and live work structure with industrial character and appropriate massing and setbacks which actively engage the street.

## Lofts and Live Work Units



Live-work lofts articulated with large windows and awnings.

### 48 Building Articulation

#### Design Principle

The facades of structures should be visually interesting and while may emphasize an industrial character, the project should complement adjacent structures.

#### Rationale

The unique nature of industrial buildings should be promoted with interesting esthetic treatments.

#### Design Guidelines

- 48-1 Design the front façade of live work units to reflect the simple and functional, yet edgy, character of industrial buildings.
- 48-2 Front facades can be articulated with big double height windows, awnings, saw tooth roofs, etc.
- 48-3 Allow upper story balconies to protrude four to six feet from the building edge.

## Lofts and Live Work Units

### 49 Private Realm

#### Design Principle

The “private realm” refers to the buildings and land that are on privately-owned lots and parcels. The private realm should consist of private and semi-private transitional spaces between the public realm and buildings, that serve to enhance the vitality of the community.

#### Rationale

The design of the private realm will have a significant impact on the quality of the public realm, as private buildings provide the edges to streets and open spaces. These guidelines serve to guide those aspects of the private realm that have a direct affect on the surrounding public context.

#### Design Guidelines

- 49-1 Accommodate elements in the front setbacks, that provide flexibility to be used as residential oriented porches or business entry alcoves, whichever best suits the use of the live-work unit.
- 49-2 Allow awnings and signage to extend into front setbacks.
- 49-3 Consider the use of elevated front porches that evoke an appearance of industrial loading docks.
- 49-4 Outside storage facilities for (bicycles, bbq's, ect.) are strongly encouraged to minimize clutter on balconies.



*Lofts with elevated front porches.*

## Commercial

### 50 Building Orientation, Setbacks, and Build-to Lines

#### Design Principle

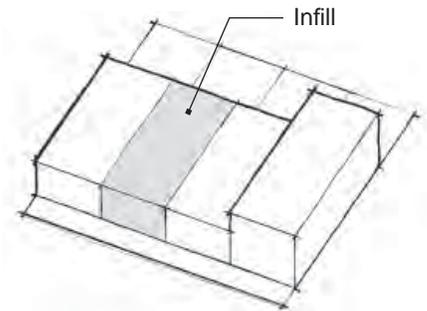
Buildings should be constructed to the front of the property line behind the sidewalk, with allowable variation in the setback to provide for café seating, plazas, and other additions to the public realm.

#### Rationale

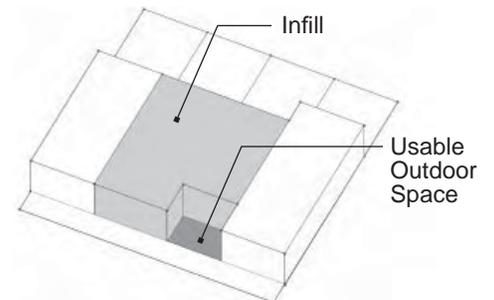
Commercial buildings in urban areas have typically been built to the front of the property line behind the sidewalk, creating a line of buildings with a consistent “streetwall” that supports a strong relationship between the building, the sidewalk, and the street. This streetwall should be reinforced by new construction and additions. The streetwall may be varied to create usable public spaces such as outdoor café dining and small plazas with seating.

#### Design Guidelines

- 50-1 Buildings should be constructed to the front of the property line and from side property line to side property line.
- 50-2 Facades that front onto a public street should be built parallel or nearly parallel to the public right-of-way.
- 50-3 A portion of the front setback may be increased by as much as 15 feet, if that setback is used as public space, such as outdoor restaurant seating or a courtyard with public access. A minimum of 60% of the front facade should be constructed up to the front setback.
- 50-4 Buildings at corners may be set back to create corner entries or “chamfered” entries in order to actively address both streets with pedestrian friendly entries.
- 50-5 New buildings should provide an appropriate setback to allow rear- and side-yard facing windows on existing buildings to have access to light, air, and usable space between buildings.



*New construction and additions should be built to the back of the sidewalk or at the front of the property line.*



*New construction and additions may increase a portion of the front setback if designed as usable outdoor space.*



*Many buildings on Del Paso Boulevard are built to the property line.*

## Commercial

50-6 The ground floor of buildings within or near transit-oriented development areas should be oriented toward the street, adjacent plazas, or parks.

50-7 Orient buildings such that the primary active building facades and key pedestrian entries of the buildings face the street.

50-9 Encourage maximum building edges and open spaces, such as front yards and outdoor restaurant seating, to front on to sidewalks to encourage pedestrian activity.

50-10 Orient new buildings to minimize solar heat gain.

50-11 Individual residential units should have access to sun and air on at least two sides to encourage adequate light and ventilation.

50-12 Incorporate pedestrian friendly elements including balconies and front porches within front setbacks.

## Commercial

### 51 Parking

#### Design Principle

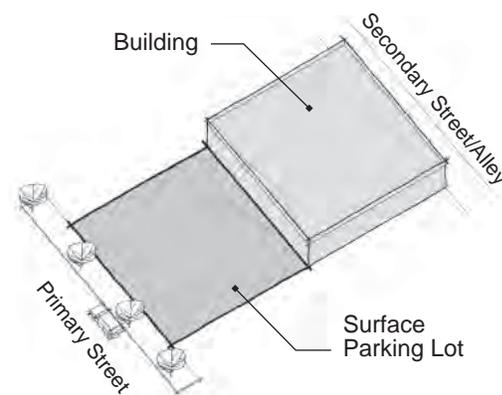
Parking areas should provide vehicular access without compromising pedestrian accessibility and the character of the public realm on primary commercial streets. Parking lots should be placed at the rear of the building, when feasible, to not obstruct views of the building's front facade from the street.

#### Rationale

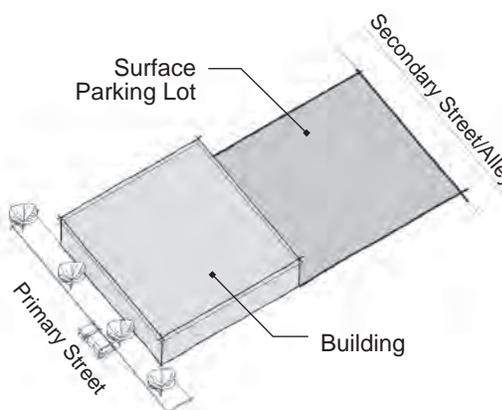
Adequate and accessible parking areas are important to the viability of commercial districts. However, large surface parking lots fronting the street can create the appearance of a vacant and uninviting area that detracts from the visual continuity of the commercial streetwall and impedes and discourages pedestrian traffic. Smaller parking lots located at the rear or sides of commercial buildings are a recommended alternative.

#### Design Guidelines

- 51-1 Parking lots should be located behind the commercial frontage on Del Paso Boulevard, which is the major pedestrian street in North Sacramento. Where parking at the rear of the building is not possible, it may be located in an interior side lot. Parking at the front of the building or corner lots is highly discouraged.
- 51-2 Large surface parking lots should be avoided in favor of several smaller parking lots.
- 51-3 A portion of a project's parking requirements may be satisfied by on-street parking, as permitted by the City.
- 51-4 Driveways into parking lots should be located on side streets, where feasible. Access to parking on major pedestrian streets should be minimized.
- 51-5 Parking lots should include signage and well-designed locations for ingress and egress that reduce conflicts with pedestrian movement.
- 51-6 Access to commercial buildings from rear or side parking lots or alleys should be well maintained and kept clear of obstructions.
- 51-7 Parking lots, driveways, and walkways should be connected with those of neighboring sites to consolidate traffic and minimize conflicts with pedestrian and automobile circulation.
- 51-8 Shared parking for such uses as retail, office, entertainment and housing is strongly encouraged, especially near the transit centers.



*Avoid placing parking in the front of the building.*



*Parking should be unobtrusive to encourage an active street life and a comfortable pedestrian environment. Parking should be placed behind, under, or on the side of buildings.*

## Commercial



*The facade of this parking structure has been designed to complement the adjoining commercial building.*

51-9 Provide convenient on-street motorcycle parking to encourage motorcycle and scooter use. Parking bays should be striped perpendicular to the sidewalk in the on-street vehicular parking zone.

51-10 Easily visible and accessible bicycle parking should be provided near Del Paso Boulevard, El Camino Avenue, and Arden Way.

### Parking Structure Design Guidelines

51-11 Parking structures are encouraged, where financially feasible, particularly near transit centers. Surface parking should be avoided in close proximity to transit centers.

51-12 Parking structures that are located on primary commercial streets should be designed with retail, office, or other uses at the street level to avoid monotonous blank walls.

51-13 Parking structures should be designed with architectural features that complement existing commercial, office, and mixed use buildings in the vicinity.

51-14 Parking structures should be designed to incorporate passive safety design features to create a secure facility. The use of glass for pedestrian stairways and adequate interior lighting are encouraged.

51-15 Automobile entry and exit ramps should be located mid-block or toward service areas rather than facing primary pedestrian streets.

51-16 Pedestrian entry and exit features should be clearly marked and open onto primary pedestrian streets and routes.

## Commercial

### ARCHITECTURAL ELEMENTS

Architectural design guidelines address the exterior of buildings and their relationship to the surrounding built context. It is paramount to ensure that the design of the building complements the community setting and character and contributes to the public realm. Architectural design should promote commercial buildings that are:

- visually welcoming from the primary pedestrian street;
- similar in mass and scale to other commercial buildings in the area; and
- constructed of high-quality materials that will contribute to the longevity of the building.

Respect the past Art Moderne and Streamline Moderne architectural style along Del Paso Boulevard by not replicating or imitating the architecture, but continuing its essence, which was inspired by technology and the emerging love affair America had with machines. Simple and functional architecture that highlights the juxtaposition of strong architectural elements, such as contrasting strong horizontal and vertical lines with curving forms and complimenting subdued earthy base building colors with bright and dark colored trims.

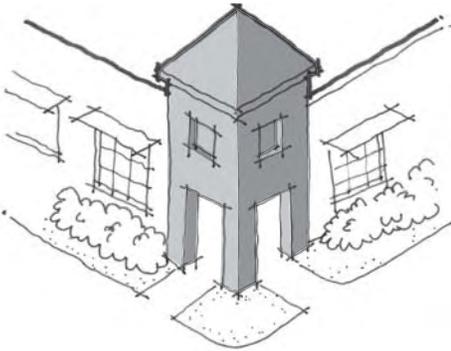


*High quality materials and creative design on the Plaza del Paso building*



*This retail store references traditional local architectural elements with its small round windows and entry feature, while the building's signage and sculptures display cutting-edge architectural design.*

## Commercial



*Building entries at corners should address both sides.*

### 52 Building Height, Massing, and Scale

#### Design Principle

The size and scale of commercial buildings should be compatible with existing development in commercial districts.

#### Rationale

To ensure compatibility with existing development, new development should appear similar in massing and scale, and the heights of new buildings should generally fall within the height range of existing buildings on the block. Corner sites offer a special opportunity for providing additional building height and can serve as anchor sites for a block.

#### Design Guidelines

- 52-1 New, higher buildings can reinforce the established building heights along a block by stepping back upper floors that are above the average building height along the street.
- 52-2 A building that is larger than the average of buildings on the same block should break up the mass of the structure with articulation of the structure into smaller components and the creation of multiple surfaces.

## Commercial

- 52-3 Appropriately scaled doors, windows, awnings, and detailing can reduce the appearance of mass.
- 52-4 Buildings on corner lots provide an opportunity for structures that exceed the average height on the block and can serve as anchor points.
- 52-5 Building heights should not block important view corridors in the neighborhood.
- 52-6 The floor-to-floor height used in older, established buildings should be maintained in new construction.
- 52-7 Encourage larger scale buildings along major arterial roads like Del Paso Boulevard and Arden Way to transition to lower scale buildings along local streets such as Canterbury Road and Boxwood.
- 52-8 Respect the adjoining residential developments with the massing and scale of new developments.

### Sustainability Guidelines

- 52-9 Massing design should provide opportunities for daylighting and solar panels. Glazing should be located predominantly on the north and south sides of the structure, with glazing on the west side of the structure minimized unless the west side is the street side.



*New construction and additions that deviate from the typical proportions of height, width, and depth may appear out of scale with existing buildings.*



*New construction and additions should respect the typical proportions of height, width, and depth.*

## Commercial

### 53 Building Facades

#### Design Principle

Building facades should be designed to create visually interesting buildings that offer variety along the commercial street.

#### Rationale

Building facades provide the interface between the built environment and the public realm. Historically, commercial districts have consisted of buildings that are one or two stories in height and cover entire lots. This pattern creates a regular rhythm of building mass and streetwalls. A streetwall of varied building facades is visually appealing and enhances the pedestrian environment. Blank walls at the ground floor level are unattractive and uninviting and should be avoided. Instead, elements should be used to create visual interest, including windows, doors, awnings and canopies, trellises, detailed parapets, or arcades.

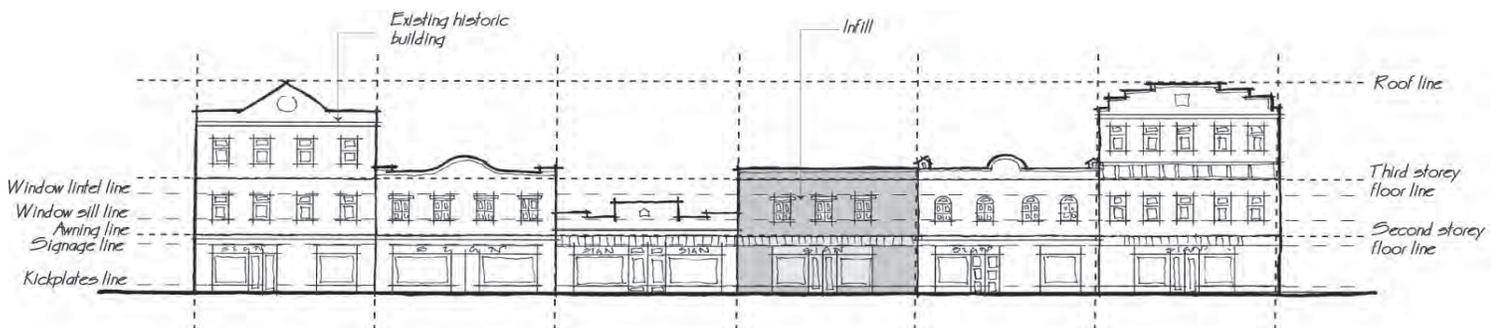
In recent decades, new buildings have increased in size and scale, creating greater challenges to creating human-scaled commercial environments. Therefore, appropriate architectural elements, such as window openings, commercial displays, frequent building entries, ornamentation, awnings and canopies, contribute to a pleasant urban streetscape.

#### Design Guidelines

- 53-1 Doors, windows, floor heights, cornice lines, signage, and awnings should be appropriately scaled to reduce the mass of buildings as they are experienced at the street level.
- 53-2 The primary facade of a building must face a public street and include an entry that is accessible from that street.
- 53-3 The main entrance of a building without street edge facades should open directly onto a publicly accessible walkway. This walkway should connect directly to an adjacent street sidewalk.



*Avoid expansive blank walls along streets.*



*New construction, additions, and alterations should draw from existing architectural features.*

## Commercial

- 53-4 Building facades facing streets should be lined with windows, entries, and openings that provide indoor and outdoor views to the public rights-of-way and sidewalks. Continuous blank wall surfaces are not allowed.
- 53-5 Architectural features, such as display windows, pilasters, lattices, and alcoves for product display, can provide visual relief on buildings that cannot achieve continuous openings along the street and sidewalk.
- 53-6 Facades can also be articulated with insets, partial setbacks, and small pedestrian plazas, (see Section 39, "Building Orientation").
- 53-7 Solid roll-down security grates should not be used on the exterior of the building; however, they may be placed on the interior of storefront glazing or entry doors.
- 53-8 Highly reflective or dark tinted glass should be avoided.
- 53-9 Street facades of commercial buildings in areas of predominantly older buildings must have a ground floor base of a durable material, such as stone, tile, or certain types of finished concrete, where feasible.



*Renovated corner entry on Del Paso Boulevard*



*This commercial structure is a contemporary interpretation of traditional design.*

## Commercial

53-10 Building facades should be designed to create a recognizable “base” and “top.” Building bases and tops can be created with variations in:

building wall thickness;

- use of special materials;
- changes in colors and materials on window trim;
- cornice treatments;
- roof overhangs with brackets; and
- use of ornamental building lines.

53-11 Utilize building elements such as cornices, lintels, sills, balconies, awnings, porches, stoops, etc to enhance building facades.

53-12 Incorporate vertical and horizontal architectural elements to mitigate long unbroken building facades.

53-13 When windows face southwest and west, frame windows with protruding vertical and horizontal shading elements such as lintels, sills, etc to provide required protection from glare and heat load.

53-14 Interpret key signature elements of the Art/ Streamline moderne style in modern 21st Century building context, to create extremely pedestrian friendly and visually interesting building facades, by grouping windows to create strong horizontal lines, using doors made of large plate glass, and incorporating materials in innovative ways.

53-15 Reduce the mass of some of the long and larger commercial buildings with architectural design including vertical elements and minor setbacks.

53-16 If possible, provide opportunities for seating and gathering within the building façade, minor building setback and sidewalks adjacent to the building.



*New construction and additions are encouraged to use horizontal elements to create a “top” and “base” that give definition to the building and break down its elements to a more human scale.*

## Commercial

### 56 Entry Features

#### Design Principle

Entry features of commercial buildings should be clearly visible to pedestrians, with a defined relationship to the street and sidewalk.

#### Rationale

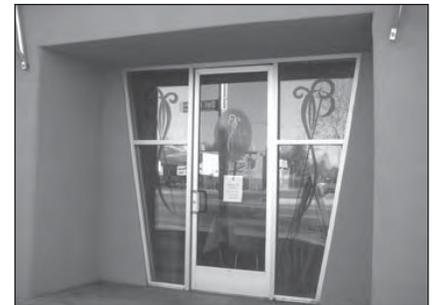
A recessed entry helps to break up the massing of a building and makes the threshold immediately apparent to pedestrians. Decorative features, such as awnings, canopies, lighting, and signage, can also be used to clearly define and articulate an entryway.

#### Design Guidelines

- 56-1 Primary entries should be located on major sidewalks to provide clearly visible pedestrian access.
- 56-2 The size of the entry should be proportional to the building.
- 56-3 Secondary entries may be located at the side or rear of the building to provide access from parking areas.
- 56-4 Entries should be clearly defined with signage and architectural details.
- 56-5 In mixed-use buildings, the entrance to residential uses on the second story should be clearly defined and easily accessible.
- 56-6 Buildings near transit centers should provide clear pedestrian access and entry features oriented toward the transit center.
- 56-7 Maximize the building entries along the primary street façade. Emphasize the primary entry of buildings.



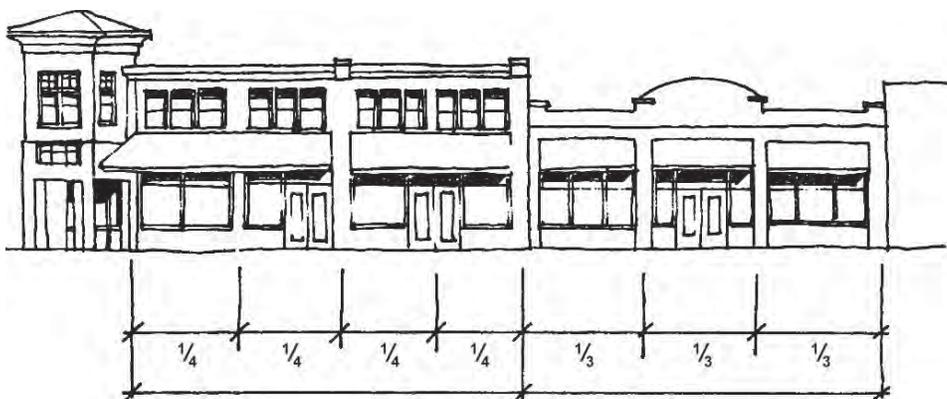
*This recessed entry on the public library is typical of many older buildings on Del Paso Boulevard.*



*The Supper Club has a more contemporary recessed entry and door.*



*New Faze on Del Paso Boulevard has a dramatic corner feature with a street level entry opening onto the pedestrian way.*



*Building openings should maintain the proportions and spacing of other openings on the block.*

## Commercial



*Landscaped areas add to the beauty of commercial districts.*

### 66 Landscape Elements

#### Design Principle

Landscape elements should be used to foster an attractive and comfortable commercial environment.

#### Rationale

Parks, plazas, and town squares should be developed as the focus of commercial areas, with commercial development opening directly onto these spaces. Parks, plazas and town squares should include landscape elements, such as ornamental plants and water features, to create visual interest and an attractive, appealing environment.

#### Design Guidelines

- 66-1 Landscaping shall conform to all relevant City of Sacramento regulations and guidelines, including the City of Sacramento Municipal Code, "Landscaping and Paving Regulations," Chapter 124.625.
- 66-2 Plant species should be suitable for the Sacramento climate. Low-water landscaping materials are encouraged.
- 66-3 High-maintenance annuals and perennials should be used only as smaller landscape elements.
- 66-4 Anticipate the full growth of landscaping materials so that trees and shrubs do not conflict with lighting and roofs.
- 66-5 Landscaped areas are preferred over impermeable paved surfaces.
- 66-6 An automatic irrigation system must be installed to provide consistent coverage of all landscaped areas. Automatic controllers with rain shut-off valves will allow for greater water conservation. Irrigation controls should be screened from view by landscaping or other attractive site materials.
- 66-7 Turf and groundcover are more effectively irrigated with a conventional spray system. Head-to-head spray coverage is recommended. Avoid overspray onto adjacent areas.
- 66-8 A drip irrigation system is recommended for shrubs and trees to provide deeper, more even watering. Drip irrigation permits greater water conservation than a conventional spray system.
- 66-9 Bare soil should be planted or mulched to minimize run-off.
- 66-10 Include tree planting along the alley to screen and soften the impact of new development to create a more pedestrian-friendly environment along alleyways.

## Mixed-Use Development

Mixed-use development combines commercial with other uses, such as office and residential. When mixed-use development is vertical in form, the commercial and office professional uses should be on the first story, with residential above. The first story should be designed with a large percentage of windows, doors, and other transparent surfaces. Upper stories should have a larger percentage of opaque surface, which can be articulated with windows, balconies, and patios.

Additional design guidelines from the multifamily and commercial chapters should be referenced as well.



*Mixed-use building with ground floor retail and residential above, Orenco Station, Oregon*



*This mixed use building has a strong corner treatment, a clearly defined base, and an articulated facade.*

## Mixed-Use Development

### 68 Orientation & Layout

#### Design Principle

Mixed-Use buildings should be constructed to the property line behind the sidewalk, with allowable variation in the setback to provide public amenities.

#### Rationale

Mixed-Use buildings in urban areas have typically been built to the front of the property line behind the sidewalk, creating a line of buildings with a consistent "streetwall" that supports a strong relationship between the building, and the public realm. This streetwall should be reinforced by new construction and additions. The streetwall may be varied to create usable public spaces such as outdoor café dining and small plazas with seating.

#### Design Guidelines

- 68-1 Create a strong building edge along the street to maximize visibility of the commercial uses, which in turn provides eyes on the street.
- 68-2 Provide parking in the rear of the lot, preferably accessed by side roads, and existing alleys and new minimum 20 feet wide driveways.
- 68-3 Articulate driveways and parking lots with special paving and trees.



Mixed-use building built to the street edge with ground floor retail and residential above.

## Mixed-Use Development

### 69 Massing & Setbacks

#### Design Principle

The size and scale of mixed-use buildings should be complement existing development in commercial districts.

#### Rationale

New mixed-use development should respect the scale and massing of existing surrounding development. Corner sites offer a special opportunity for providing additional building height and mass can serve as an anchor for the block.

#### Design Guidelines

- 69-1 Locate the majority of the building façade and commercial building uses along the edge of sidewalk.
- 69-2 Step back the massing of the building development such that it is at its highest intensity along major streets, and at its lowest when adjacent to existing smaller scale residential development.



*Mixed-use building with varied setbacks and massing .*

## Mixed-Use Development



Ground floor commercial uses should have larger windows to engage the public realm and differentiate from the residential above.

### 70 Building Articulation

#### Design Principle

Buildings should include ground floor transparency, design details and features that provide a significant contribution to the streetwall and overall pedestrian experience.

#### Rationale

Public access and greater visibility will promote successful development.

#### Design Guidelines

- 70-1 Maximize the number of building entries, especially of office and retail businesses, along the façade fronting the major street. Emphasize primary entry of buildings (e.g. entrance lobby) with vertical elements.
- 70-2 Where possible, locate pedestrian-oriented entries of the upper floor residential units along the street facing façade.
- 70-3 Articulate the front facades with rhythm of windows, both along the ground floor and upper residential floors.
- 70-4 Ensure that ground floor is as transparent as possible to connect the pedestrians and the building users.



This mixed-use building has a clearly defined base, and a well articulated facade.

## Mixed-Use Development

### 71 Private Realm

#### Design Principle

The “private realm” refers to the buildings and land that are on privately-owned lots and parcels. The private realm should consist of private and semi-private transitional spaces between the public realm and buildings, that serve to enhance the vitality of the community.

#### Rationale

The design of the private realm will have a significant impact on the quality of the public realm, as private buildings provide the edges to streets and open spaces. These guidelines serve to guide those aspects of the private realm that have a direct affect on the surrounding public context.

#### Design Guidelines

- 59-1 The use of residential balconies and commercial awnings which extend into the public realm is encouraged.
- 59-2 Landscape front setbacks of the street facing ground floor residential component of the mixed-use buildings.
- 59-3 Provide privacy for first floor office and residential units by allowing them to be three feet above the sidewalk level.

**DRAFT RESOLUTION NO.**

Adopted by the Sacramento City Council

**RESOLUTION APPROVING INFRASTRUCTURE  
RECOMMENDATIONS CONTAINED IN THE REPORT ENTITLED “THE  
NORTHEAST LINE LIGHT RAIL STATIONS PLAN PHASED  
INFRASTRUCTURE RECOMMENDATIONS” AS PART OF THE  
NORTHEAST LINE IMPLEMENTATION PLAN (LR09-21)**

**BACKGROUND**

- A. On October 15, 2002, the City Council accepted the Transit for Livable Communities (TLC) recommendations, which provided recommendations and strategies for transit-supportive development proximate to existing and future light rail stations.
- B. On July 24, 2007, the City Council accepted the Northeast Line Light Rail Stations Plan as the guiding vision for development within the quarter mile radius around the Globe, Arden/Del Paso, and Royal Oaks light rail stations. This plan consisted of design guidelines, recommended land use changes and an infrastructure assessment.
- C. On March 3, 2009, the City Council adopted the 2030 General Plan, which includes land use and policy direction to promote infill development in key opportunity areas, including commercial corridors and areas served by transit, such as the Northeast Line Light Rail Corridor.
- D. The infrastructure assessment from the Northeast Line Light Rail Stations plan has been revised to identify affordable, phased, and prioritized infrastructure improvements that will facilitate initial catalyst development and near term growth consistent with the 2030 General Plan’s growth projections for the plan area.
- E. On December 9, 2010, the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve the recommendations for future infrastructure improvements along the Northeast Line Light Rail Corridor.

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL  
RESOLVES AS FOLLOWS:**

**Section 1.** Environmental Determination: The City Council has approved the environmental review of the Project as being within the scope of the 2030 General Plan Master EIR by Resolution No. \_\_\_\_.

**Section 2.** Based on the verbal and documentary evidence received at the hearing, the City Council approves infrastructure recommendations contained in the report entitled “The Northeast Line Light Rail Stations Plan Phased Infrastructure Recommendations” which is attached as Exhibit A of this Resolution.

**Section 3.** Exhibit A is a part of this Resolution.

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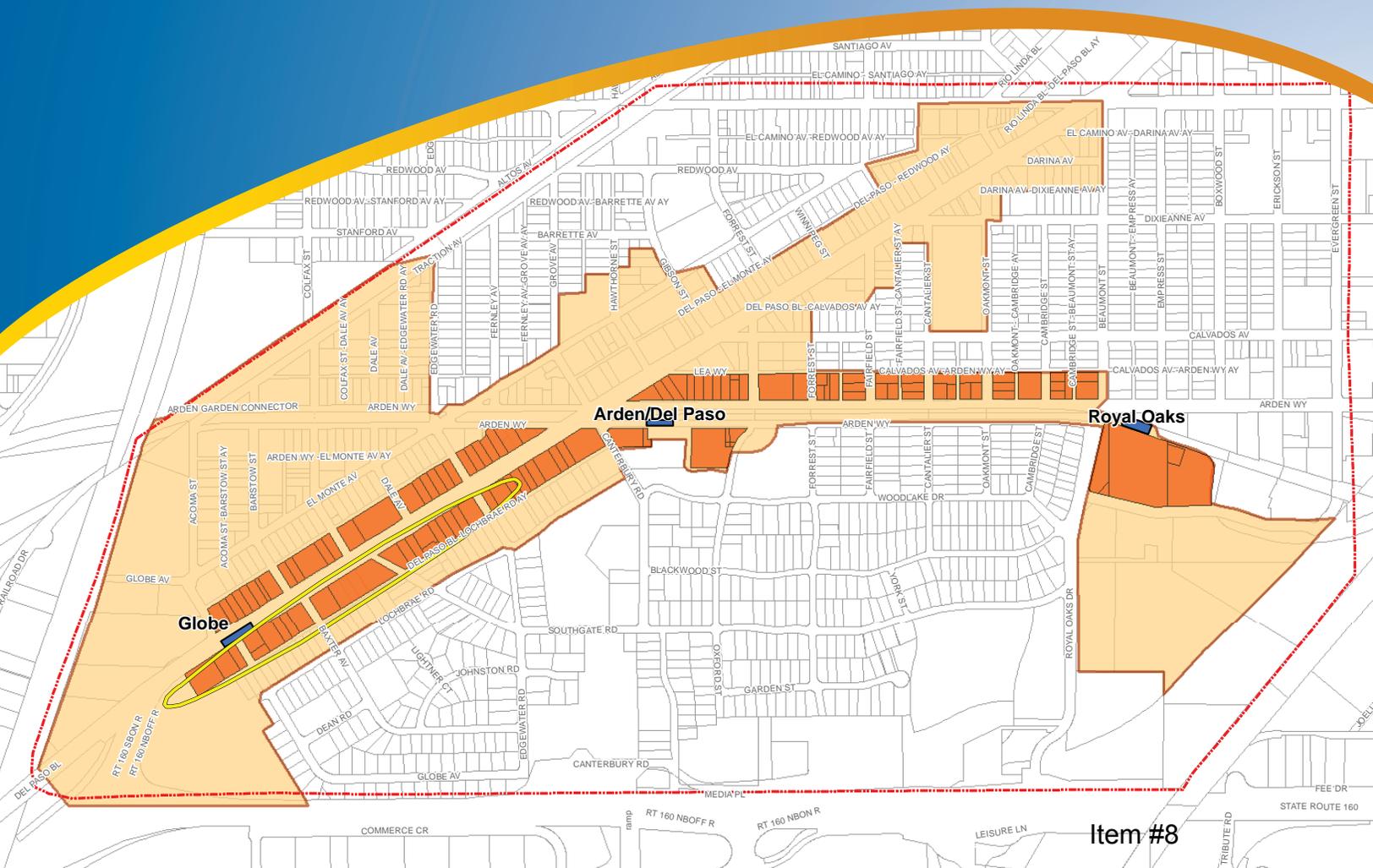
EXHIBIT A: Northeast Line Light Rail Stations Plan Phased Infrastructure Recommendations

# Northeast Line Light Rail Stations Plan Exhibit A Phased Infrastructure Recommendations

DRAFT

SUBMITTED TO THE  
City of Sacramento

January 2011





**FINAL REPORT FOR THE  
NORTHEAST LINE IMPLEMENTATION PLAN  
PHASED INFRASTRUCTURE RECOMMENDATIONS**

**City Agreement #2010-0434**

**lead agency:**

City of Sacramento  
Community Development Department  
Long Range Planning  
300 Richards Boulevard, 3rd Floor  
Sacramento, California 95811

**lead agency contact:**

Mr. Greg Sandlund  
916.808.8931

**project consultant:**

Nolte Associates, Inc.  
2495 Natomas Park Drive, Fourth Floor  
Sacramento, CA 95833-2935

**project consultant contact:**

Jay F. Radke  
Project Manager  
916.641.9139

**JANUARY 2011 - FINAL**



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## INTRODUCTION

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### Project Description

The Northeast Line Light Rail Stations Plan (The Plan) was adopted by the City Council in December 2007. The Plan set forth the vision of an active, thriving transit-oriented residential and commercial neighborhood to maximize the advantages of the proximity to the existing three Light Rail Stations – Globe, Del Paso/Arden, and Royal Oaks. The Plan established proposed mixed land uses, goals, and policies that will guide future development.

The Plan study area encompassed a study impact area of roughly 570 acres, with a development focus within a quarter mile radius surrounding each of the existing three light rail stations. Newly envisioned land uses for these areas will present added infrastructure demands. Existing sanitary sewer, storm drainage, water, electrical power, telecommunications, natural gas and street improvement infrastructure capacity was analyzed and modifications proposed to adequately serve these new demands.

This report is being prepared with the goal to revisit the previously prepared infrastructure study for The Plan (dated March 2007) prepared by Nolte Associates, Inc. as a member of the Moore Iacofano & Goltman (MIG) Team. The report performs an analysis of the basic infrastructure needs and associated costs to support a realistic projection of growth by 2030 consisting of approximately 1,384 dwelling units and 112,950 square feet of commercial development. This reduced growth is located in a narrower Core Development Area focused on the Del Paso Boulevard Corridor and the Arden Way Corridor. This analysis relies on the previous infrastructure study with a focus on just the essential improvements necessary for the proposed development in the near term. The focus of the report is to identify key infrastructure investments that can be made at minimal cost to maximize development in the near term.

If the recommended infrastructure improvements specified in this report cannot be made in a timely manner, this report can serve as a guide for developers to determine which sites have the least infrastructure constraints. For such sites, there is a greater chance that infrastructure improvements can be realistically made on a project by project basis.



## EXECUTIVE SUMMARY

### Tier I - Catalyst Sites

There are a total of 13 parcels grouped together in four areas consisting of a total of 3.15 acres that are considered the catalyst sites for the near term development. The Sacramento Housing and Redevelopment Agency owns 8 of the parcels, Sacramento Regional Transit District owns 1, and the remaining 4 are privately owned. The anticipated development of the combined catalyst sites is a total of 189 residential dwelling units together with a total of 54,960 square feet of non-residential (ground floor commercial) development.

For the development of these catalyst sites, it is recommended to upgrade the existing water main in the Del Paso/Lochbrae Alley and reconstruct the pavement of the alley with concrete pavement. The following is a summary of the estimated cost of construction for the Tier I infrastructure improvements.

<b>TIER I - CATALYST SITES</b>	
<b>A. STREETWORK</b>	
Streetscape Improvements	\$0
Del Paso Alleys	\$346,300
<b>B. SEWER SYSTEM</b>	
East	\$0
West	\$0
<b>C. DRAINAGE SYSTEM</b>	
Shed 151 East	\$0
<b>D. WATER DISTRIBUTION SYSTEM</b>	
Del Paso Alley	\$477,056
<b>TOTAL TIER I CONSTRUCTION (A-D)</b>	<b>\$823,356</b>



## City of Sacramento

### Northeast Line Light Rail Stations Plan – Phased Infrastructure Recommendations

## Tier II – Near Term Development

The remainder of the Del Paso/Arden Way Corridor area is anticipated to have potential development in the near term to selected opportunity sites along the Del Paso and Arden Way Corridors. The anticipated development of all of the Tier II areas totaling 16.10 acres is 834 residential dwelling units together with a total of 285,601 square feet of non-residential development. Significant improvements are needed for the existing drainage system to allow development near the Royal Oaks Station. Upsizing of the existing sanitary sewer system on Edgewater Road is required for the added development along Del Paso Boulevard. The following is a summary of the estimated cost of construction for the Tier II infrastructure improvements.

#### TIER II - DEVELOPMENT SITES

<b>A. STREETWORK</b>	
Streetscape Improvements	\$0
Del Paso Alleys	\$268,088
<b>B. SEWER SYSTEM</b>	
East	\$273,139
West	\$783,641
<b>C. DRAINAGE SYSTEM</b>	
Shed 151 East*	\$5,663,908
<b>D. WATER DISTRIBUTION SYSTEM</b>	
Del Paso Alleys	\$347,625
<b>TOTAL TIER II CONSTRUCTION (A-D)</b>	<b>\$7,336,401</b>

\*The drainage system improvement necessary for the Tier II development in the vicinity of the Royal Oaks Station area assumes full construction of the piping and detention system downstream of Arden Way. Alternative mitigations and/or offsite improvement strategies (that achieve City performance requirements) of this system may be allowed on a case by case basis with approval of the City's Department of Utilities.



City of Sacramento

Northeast Line Light Rail Stations Plan – Phased Infrastructure Recommendations

## Tier III – Full Buildout

Tier III is considered the full buildout of the Northeast Line Light Rail Stations Plan area. The original infrastructure study prepared in March 2007 details the anticipated growth projection and associated infrastructure costs for the full buildout of the Plan area. The following is the cost estimate summary table from the original infrastructure study. The costs estimates are inclusive of the Tier I and Tier II estimates above. The costs provides for major street beautification on Del Paso and Arden Way and major drainage improvements as well as the improvements necessary for the additional growth capacity. For brevity, the full detail of these estimates is not included with this focused study.

A. STREETWORK	\$19,569,360
B. SEWER SYSTEM	
East	\$273,139
West	\$1,234,617
C. DRAINAGE SYSTEM	
Shed 151 East	\$7,559,047
Shed 151 West	\$4,301,480
Shed 153	\$2,337,660
D. WATER DISTRIBUTION SYSTEM	
Globe Station Area	\$1,507,359
Arden - Del Paso Station Area	\$1,466,859
Royal Oaks Station Area	\$2,715,188
<b>TOTAL CONSTRUCTION (A-D)</b>	<b>\$40,964,708</b>



## LAND USE

A proposed development intensity land use analysis was prepared for the original Plan Area by the project planners Moore, Iacofano & Goltsman, Inc. (MIG). The land use analysis proposed higher intensity land uses for selected parcels surrounding the general area of each of the three existing light rail stations - Globe, Del Paso/Arden, and Royal Oaks.

It is envisioned that the sites will develop as either multi-family residential or mixed use multi-family residential/non-residential (commercial). The land use analysis proposed five different levels of development intensities (A-E) for the selected parcels. Each of the five development intensities were given a “Low” and “High” range for expected density of multi-family residential dwelling units per acre (DU/AC) and commercial floor area ratio (FAR). The following summarizes the assumptions used in the original Northeast Line Light Rail Plan analysis:

Development Intensity A:	Residential - Low = 40 DU/AC, High = 60 DU/AC Non-Residential - Low = 0.3 FAR, High = 0.4 FAR
Development Intensity B:	Residential - Low = 40 DU/AC, High = 60 DU/AC Non-Residential – None Proposed
Development Intensity C:	Residential - Low = 25 DU/AC, High = 40 DU/AC Non-Residential – None Proposed
Development Intensity D:	Residential - Low = 15 DU/AC, High = 25 DU/AC Non-Residential - Low = 0.45 FAR, High = 0.6 FAR
Development Intensity E:	Residential - Low = 25 DU/AC, High = 40 DU/AC Non-Residential - Low = 0.3 FAR, High = 0.4 FAR

Projections of the number of multi-family residential units and the gross square feet of non-residential by land use were developed. Table A-1 in Appendix A presents the results of the original land use development intensity analysis. For the purposes of the original infrastructure analysis, the Technical Advisory Committee asked that only the “High” range be analyzed.

### TIER I - CATALYST SITES

For the purposes of this report, the core development area has been narrowed to encompass approximately 24.1 acres immediately adjacent to the main roadway corridors of Del Paso Boulevard and Arden Way. Within this core development area, there are a total of 13 parcels grouped together in four areas consisting of a total of 3.15 acres that are considered the catalyst sites for the near term development. The Sacramento Housing and Redevelopment Agency owns 8 of the parcels, Sacramento Regional Transit District owns 1, and the remaining 4 are privately owned.



The four groups of lots are 0.35, 0.43, 1.00, and 1.38 in size located on the southerly side of Del Paso Boulevard between Globe Avenue and Edgewater Road. Using the assumed High level of development intensity “A” from the original study (High : Residential = 60 DU/acre & Non-Residential = FAR 0.4), this would yield a total of 189 residential dwelling units together with a total of 54,960 square feet of non-residential (ground floor commercial) development over the 3.15 acres of the catalyst sites.

## **TIER II – NEAR TERM DEVELOPMENT SITES**

The remainder of the Del Paso Boulevard Corridor area is anticipated to have a potential of development in the near term to selected opportunity sites. The original Land Use Plan prepared by MIG identified opportunity sites along the Corridor. In addition to the sites identified above in the Tier I – Catalyst Sites, there is an additional 4.84 acres of development anticipated in these opportunity sites. Using the assumed High level of development intensity “A” from the original study (High : Residential = 60 DU/acre & Non-Residential = FAR 0.4), this would yield a total of 299 residential dwelling units together with a total of 84,410 square feet of non-residential development.

At the intersection of Del Paso and Arden Way there are three sites with a total area of 3.93 acres identified as opportunity sites. The two sites on the north side of Arden Way were assumed with a High level of development intensity “D” (High : Residential = 25 DU/acre & Non-Residential = FAR 0.6). The one larger site on the south side of Arden Way was assumed with a High level of development intensity “A” noted above. Using these densities would yield a total of 242 residential dwelling units together with a total of 73,685 square feet of non-residential development.

Near the Globe Station area on Arden Way there are three sites with a total of 7.32 acres identified as opportunity sites. The two sites on the north side of Arden Way were assumed with a High level of development intensity “E” (High : Residential = 40 DU/acre & Non-Residential = FAR 0.4). Using these densities would yield a total of 293 residential dwelling units together with a total of 127,506 square feet of non-residential development.

The total anticipated development of all of these three Tier II areas totaling 16.10 acres is 834 residential dwelling units together with a total of 285,601 square feet of non-residential development.

The Community Development Department (CDD) has estimated the total anticipated realistic growth projection of development within the year 2030 in the Plan area is approximately 1,384 residential dwelling units and 112,950 square feet of commercial development. This is somewhat less than the combined Tier I and Tier II projections of 1023 (= 299 + 834) for residential dwelling units.



## City of Sacramento

### Northeast Line Light Rail Stations Plan – Focus Study

The combined projection for non-residential of 340,561 (= 84,410 + 285,601) square feet is considerably more than the CDD's 2030 growth projection for the area. However, the non-residential uses do not have as significant of an impact on the utility system as the residential uses. The difference between the two estimates in non-residential development is roughly equivalent to only 60 multi-family residential units.





# NORTHEAST LINE LIGHT RAIL STATIONS PLAN - FOCUSED LAND USE PLAN

FIGURE II - 1

January, 2011 - FINAL





## STREETSCAPE

The Circulation and Pedestrian Access portion of the Northeast Line Light Rail Stations Plan (The Plan) was prepared by Moore Iacofano & Goltsman, Inc. (MIG). Working directly with the City of Sacramento Planning and Transportation staff as well as the Northeast Line Light Rail Stations Technical Steering Committee, MIG developed a streetscape master plan for the Plan area together with a set of illustrative typical plan and sections for each of the proposed modifications to the existing streets. For the original infrastructure study, the typical street sections developed by MIG were used to develop conceptual cost estimates for The Plan.

For the purposes of this focused study, the street modifications are limited to the Del Paso Boulevard and the Arden Way modifications. The following is a discussion of the proposed improvements for each of these two Corridors.

**Del Paso Boulevard:** The City of Sacramento Transportation Department is currently under contract with a consultant for the design of improvements to Del Paso Boulevard within the Plan area from Highway 160 to Arden Way. The design of the improvements is being funded through a mixture of funding sources including City of Sacramento, Sacramento Area Council of Governments (SACOG), and Sacramento Housing and Redevelopment Agency (SHRA). The construction of these improvements will be funded through a mixture of sources including SACOG and Federal Grants.

The project is designed to improve the aesthetic and travel experience along Del Paso Boulevard. The improvements will largely follow the design principles set forth in the original Northeast Line Light Rail Stations Plan streetscape guidelines with a focus on the bulbout, on-street parking, tree well modifications, high visibility crosswalks, and sidewalk areas. A new traffic signal is planned at the Colfax/Southgate intersection. Underground utility work is limited to storm drainage modifications necessary to support the bulbout design. The plans do not include the Globe Light Rail Station decorative streetscape plan originally envisioned in The Plan. The total project cost is estimated at \$3.3 million with construction of the project scheduled for 2011. This project will greatly enhance the development potential of the Del Paso Boulevard Corridor portion of the Study Area by providing frontage improvements for the parcels facing the street.

**Arden Way:** The City's 2008 Transportation Programming Guide (TPG) has identified three projects along Arden Way within The Plan area. The following is a brief description of each project:

**Arden Way - Del Paso Boulevard to Royal Oaks Drive:** This is a streetscape project designed to improve both the aesthetics and travel experience along Arden Way. The project is listed as 15<sup>th</sup> on the Streetscape Enhancements (Other Corridors) list contained in the TPG.



Arden Way - Royal Oaks Drive to Evergreen Street: This is a streetscape project designed to improve both the aesthetics and travel experience along Arden Way. The project is listed as 17<sup>th</sup> on the Streetscape Enhancements (Other Corridors) list contained in the TPG.

Arden Way - Beaumont Street to Evergreen Street: This is a project to install curb, gutter, and sidewalk improvements. The project is listed as 9<sup>th</sup> on the Pedestrian Improvements list contained in the TPG.

While all of the above three projects are contained in the TPG, none of these projects are currently funded. Conceptual cost estimates for these three projects are not available. As funding is made available, the projects will be implemented based upon their TPG rankings. Due to the significant costs of these projects, this focused study does not recommend improvements to Arden Way be included as a key infrastructure investment for the immediate needs of the Focus Study Area.

**Del Paso Boulevard Alleys:** While not a focus of the original infrastructure study improvements, the existing Alleys parallel to Del Paso Boulevard (El Monte/Del Paso Alley on the north and the Del Paso/Lochbrae Alley on the south) have been identified by this focused study as a potential catalyst to development along the corridor. The majority of the existing alleys are a mixture of gravel and/or deteriorated asphalt paving, with limited areas of recently paved asphalt, and a small section of concrete paving. Two sections of the existing alleys have asphalt pavement in good condition, the Del Paso/El Monte Alley between Colfax Street and Dale Avenue, and the Del Paso/Lochbrae Alley between Edgewater Road and Canterbury Road.

With development along the Corridor, access to the developing parcels will primarily be provided at the rear of the frontage lots by utilizing the existing alleys. The alley must be fully improved if it is used as the main vehicular access to a project. The development of a single parcel in the middle of a block would trigger the need to improve the pavement of the full length of the alley access to the main connecting side street. These alley improvements can be cost prohibitive to a single developing parcel in the middle of a block that would need improvements to the entire alley length out to the main street.

The City's standard for alley improvements is 6-inch concrete paving (per Design and Procedures Manual, Section 15, Plate 15-14). The concrete paving is a requirement because the typical standard 20 foot alley does not meet the minimum requirements for street width for Federal roadway maintenance funds. The concrete paving provides a longer lasting surface; however, the initial construction costs are considerably more expensive.

However, the City has allowed the use of asphalt pavement on alleys in selected areas within the City. The use of asphalt paving in the Study area may be allowed for a project on a case by case basis with approval from the City's Department of Transportation. For the purposes of this study, concrete paving has been used to provide a conservative estimate for the cost of alley pavement reconstruction.



### **FUTURE ACTION/RECOMMENDATION**

Improvement of the alley pavement (possibly in conjunction with watermain upsizing improvements) would be a significant benefit to individual parcel development along the Del Paso Boulevard Corridor. Therefore, this study recommends reconstruction and concrete pavement of the alleys as a key infrastructure investment to serve the immediate needs of the core development area.



- - - Study Impact Area
- Light Rail Station
- ★ Add Art Feature on Arden Way
- Full Signal
- Mid-street pedestrian crossing
- Pedestrian Signal
- TIER I**
- Catalyst Sites
- TIER II**
- Development Intensity A
- Development Intensity E
- Development Intensity D
- Improvements**
- Tier I - Reconstruct Alley
- Tier II - Reconstruct Alley

# NORTHEAST LINE LIGHT RAIL STATIONS PLAN - STREETScape PLAN

FIGURE III-1

January, 2011 - FINAL

200 100 0 200 Feet

N  
W E  
S

**NOLTE**  
BEYOND ENGINEERING

Item #8



## SANITARY SEWER

The Northeast Line Light Rail Stations Plan (The Plan) project area is primarily served by two separate Sewerage Collection Basins, Basins G304 & G305. The Basins are generally divided through the project area following Canterbury Road, Woodlake Drive, Cambridge Street, Beaumont Street and El Camino Avenue/Darina Avenue Alley.

For this focused study, the two main development areas along the Del Paso Boulevard and Arden Way Corridors were examined. The following is a description of the sewer improvements for each area.

**Del Paso Boulevard Corridor:** This area is served by the G304 collection system with the existing 10 inch main line located in Edgewater Road, the Del Paso Road/Lochbrae Alley, and the El Monte/Del Paso Alley. As identified in the original infrastructure study, the full development of this area will require significant sewer improvements to the downstream collection system. However, this included the impacts from the full development of the El Monte Triangle area.

The original study also noted that a portion of the Globe Station/Del Paso Station areas could be developed by utilizing the existing excess capacity of the existing collection system. An estimate of the existing flow rates in the system was made at the junction of the collection system pipelines at the intersection of Edgewater and Del Paso/Lochbrae. It was found that the main collection pipeline had an excess capacity at this point of approximately 207 ESDs (Equivalent Single Family Dwelling Units with an average flow rate of 400 gallons per day per unit). Using a multi-family rate of 0.75 ESDs per unit, this would potentially allow up to 276 multi-family units to be constructed before this pipeline would need to be upsized.

The total of the Tier I catalyst sites in this focus study area along the Del Paso Corridor are estimated to have 189 multi-family residential units and 54,960 square feet of non residential development. Using the above sewer generation rates, this would be a total of 153 ESDs ( $= 0.75 \times 189 + 0.2/1000 \times 54,960$ ). This is well within the additional estimated capacity of the existing sewer system of 207 ESDs as noted above.

Based on the opportunity sites and associated land use densities presented in the Land Use Plan from the original Northwest Light Rail Stations Plan by MIG, a total of 408 multi-family residential units and 91,598 square feet of commercial development are anticipated for the Del Paso Boulevard Corridor. Note the boundary of these development estimates are limited to the area southwest of Canterbury Lane and do not include the development along Arden Way immediately east of the Del Paso/Arden intersection. Using a factor of 0.75 ESDs per multi-family unit and 0.2 ESDs per 100 square feet of commercial, this equates to a total of 324 ESDs. This means that approximately 64% ( $=207/324$ ) of this focused study area of the Del Paso Corridor can be developed before the upgrades to the downstream system are necessary.



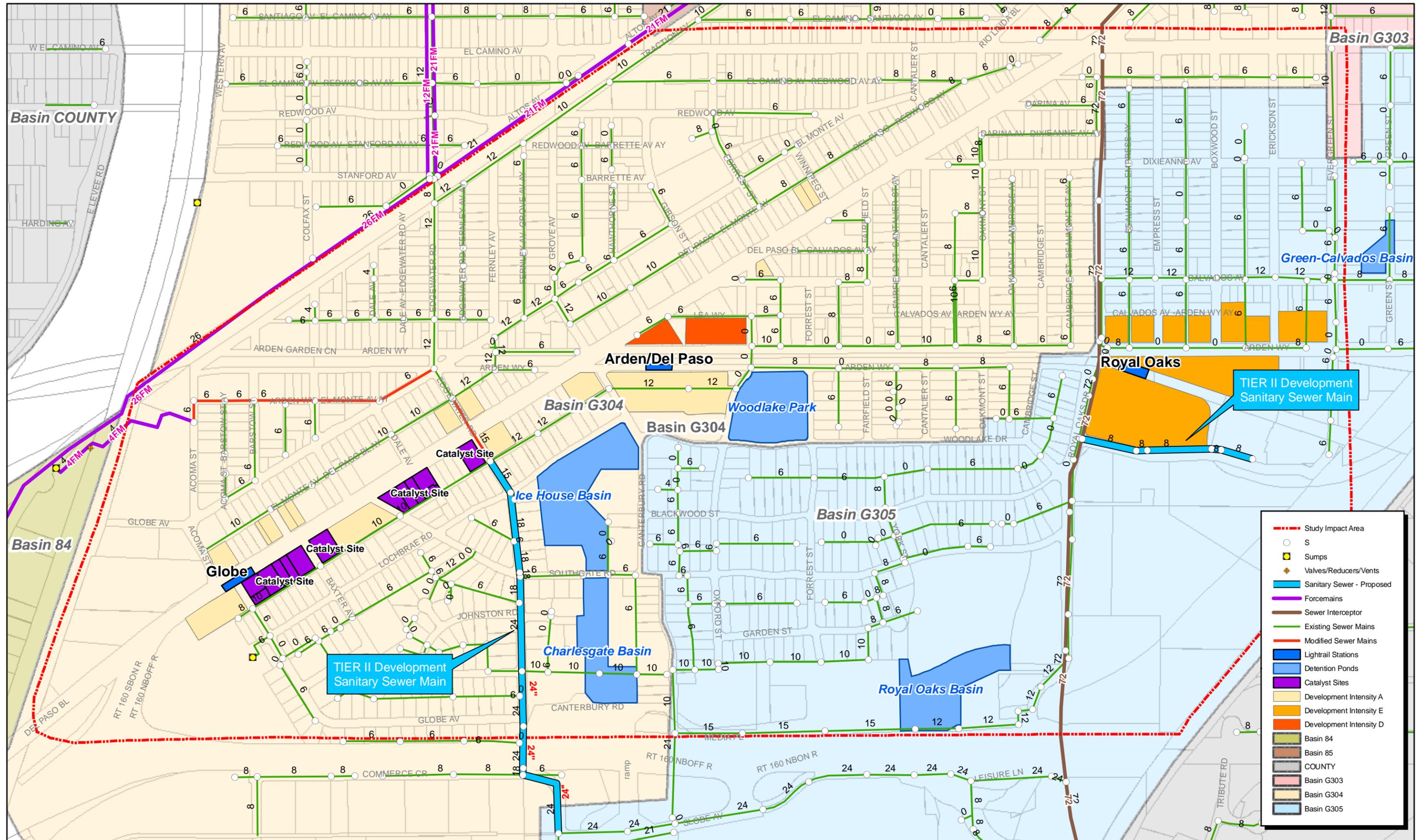
## FUTURE ACTION/RECOMMENDATION

Upgrades to the downstream system are anticipated to be necessary with approximately 64% of the anticipated development along the focused study area of the Del Paso Corridor. Impact fees should be collected from both the Tier I and Tier II development to pay a fair share of the future system upgrades.

**Arden Way Corridor:** This area is served by the G305 collection system. As noted in the original infrastructure study, the main 12 inch collection pipeline located in Royal Oaks Drive does not have sufficient capacity for the increased flows from the proposed development around the Royal Oaks Station. Rather than upsize the entire length of the main pipeline from the Royal Oaks Drive / Evergreen Street intersection all the way to where it leaves The Plan area at Canterbury Road at Highway 160, it was recommended to create a new direct connection to the 72 inch interceptor at the Royal Oaks Drive / Evergreen Street intersection. The existing 12 inch pipeline north of the intersection and the proposed 15 inch pipeline in Evergreen Street would both be connected directly to the 72 inch interceptor at this point. This will eliminate the need to upsize a considerable length of pipeline. It will also reduce the flows into the downstream system thus allowing the G304 system modifications as noted in the original infrastructure study.

## FUTURE ACTION/RECOMMENDATION

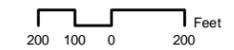
The direct connection of the existing system and the construction of the new 15 inch pipeline in Evergreen Street would be a key infrastructure investment to serve the needs of this focused study area.



# NORTHEAST LINE LIGHT RAIL STATIONS PLAN- SEWER PLAN

## FIGURE IV - 1

January, 2011 - FINAL





## STORM DRAINAGE

In general, the majority of the Northeast Line Light Rail Stations Plan (The Plan) area drainage system is more than 40 years old. There have been numerous reported instances of street flooding within The Plan project area. Modeling studies indicate that there will likely be localized structure flooding during the projected 100-year storm event.

The Plan project area is located primarily within two separate Drainage Basin Areas, Basins 151 and 153. These two Basins are generally divided along the Del Paso Boulevard corridor. The following is a description of the drainage improvements for each area.

**Del Paso Boulevard Corridor:** The Del Paso Boulevard Corridor generally drains northwesterly into the Basin 153 system to Sump 153 located near the western end of Stanford Avenue which pumps into the Natomas East Main Drainage Canal. Minor improvements to the collection system inlets are proposed with the Del Paso Boulevard Streetscape Project (Highway 160 to Arden Way).

The system improvements envisioned in the original infrastructure study were to upsize the collection system. The study utilized the Hydrology Standards contained in the Sacramento City/County Drainage Manual (December 1996) for this analysis. The peak 10-year storm flow rates were determined utilizing the 10-Year Peak Flow rates from the Sacramento Method Rainfall Zone 2 (Figure 2-14), an assumed imperviousness of 80%, and the basin sub-shed areas. Proposed pipe sizes were determined using Manning's Equation and a minimum flow rate of two feet per second in the pipe. A detailed topographic survey of the Plan Area was considered beyond the scope of the work, and therefore the pipe sizes will need to be verified when more accurate information is available during the detail design of the system.

The proposed development of this focused study is limited to the parcels immediately adjacent to the Del Paso Boulevard Corridor. The majority of these parcels are highly impervious with either existing structures or paving. Therefore the drainage characteristics are not expected to change significantly.

The 100-year flooding is limited in this Corridor to a few parcels at the northeasterly end near the Canterbury/Lochbrae intersection. It is expected that development of parcels in this area will require floodproofing of the proposed structures.

**Arden Way Corridor:** The Arden Way Corridor generally drains southerly into the Basin 151 system to Sump 151 located east of Lathrop Way which drains into the American River. The original infrastructure study divided the Basin 151 improvements into two basic areas, West and East. The majority of the improvements identified in the original infrastructure study for the Basin 151 East area affect the anticipated development of this focused study for the area surrounding the Royal Oaks Station. This area has significant drainage capacity and floodplain issues. Upsizing of the existing main drainage pipeline system will be very expensive. In



addition, upstream pipeline and detention improvements within the Swanston Station area are also necessary.

### **FUTURE ACTION/RECOMENDATION**

For this focused study, we have included an estimate of the costs for the main drainage pipeline system improvements for the Basin 151 East shed from Arden Way south to the detention basin. These improvements are considered necessary for unrestricted development of this area.

Funding for these drainage improvements has not been identified at this time. The City does not currently have funds available for drainage system Capital Improvement Projects (CIPs), but is hoping to implement a City wide drainage fee to fund projects in the future.

Development in the Royal Oaks Station area may be able to provide alternative solutions to mitigate the drainage impacts. Through a more detailed hydraulic study of the system and the project impacts (considered beyond the scope of this focused study), it may be possible to provide on-site/off-site storage, piping improvements, or combination of the two that can effectively mitigate the project impacts at a reduced cost. These improvements would be reviewed and approved by the Department of Utilities on a case by case basis.

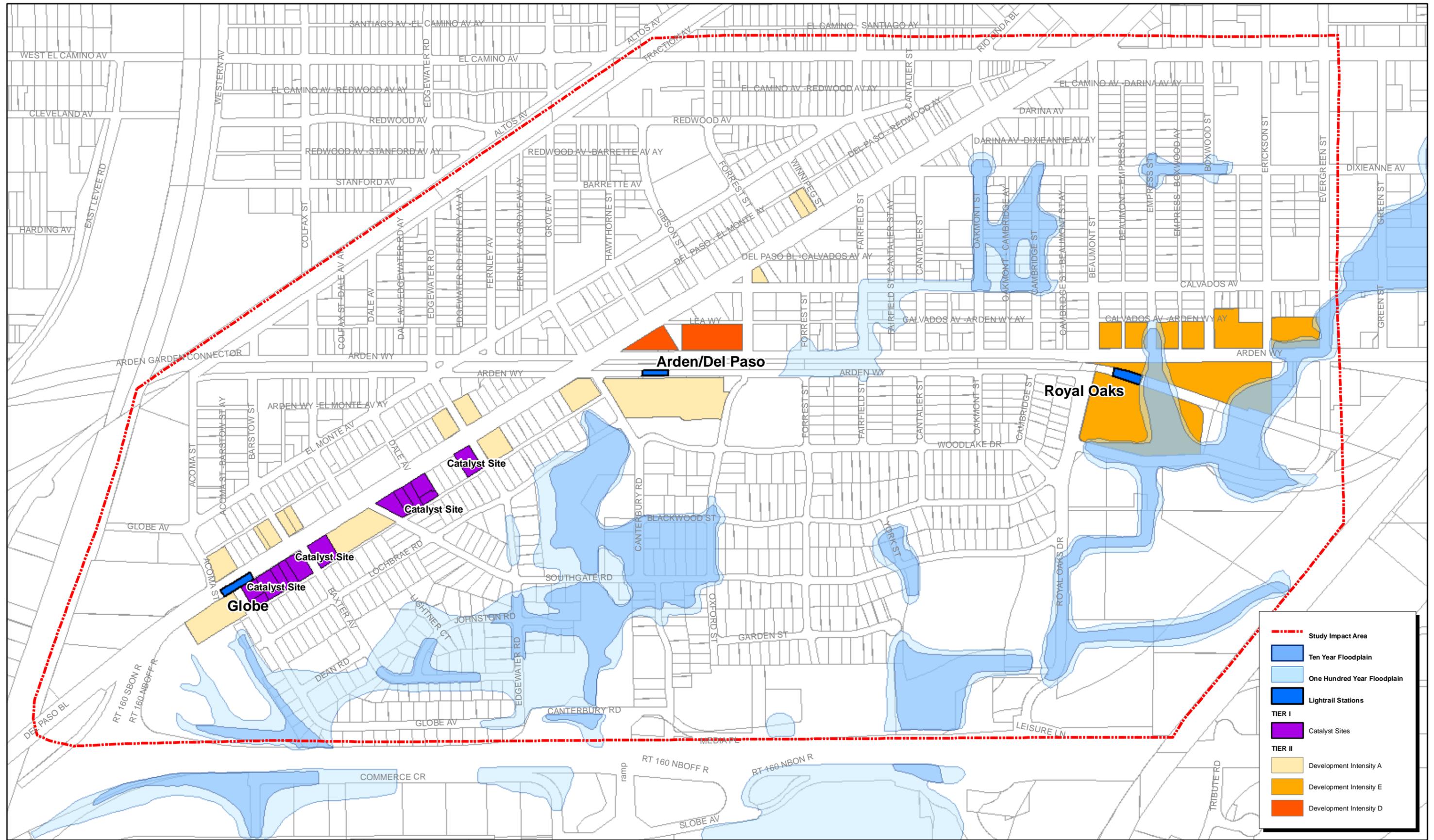
## **Stormwater Quality**

The City of Sacramento adopted the Stormwater Quality Design Manual for the Sacramento and South Placer Regions (May 2007), a joint effort of the communities in the greater Sacramento region. This manual had not yet been adopted at the time of the completion of the original infrastructure study (March 2007). Therefore, a brief description of the water quality requirements for future development is being provided.

The manual provides locally-adapted information for design and selection of three categories of stormwater quality control measures: source control, runoff reduction and treatment control. Per the requirements, multi-family and commercial, projects greater than 1 acre are required to implement permanent post-construction treatment measures.

### **FUTURE ACTION/RECOMENDATION**

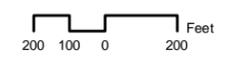
The existing storm drainage detention basins in the Basin 151 area are envisioned with future improvements to implement regional water quality treatment measures. However, until such measures are implemented, multi-family and commercial projects over 1 acre within The Plan area will be required to construct permanent post construction stormwater quality measures.



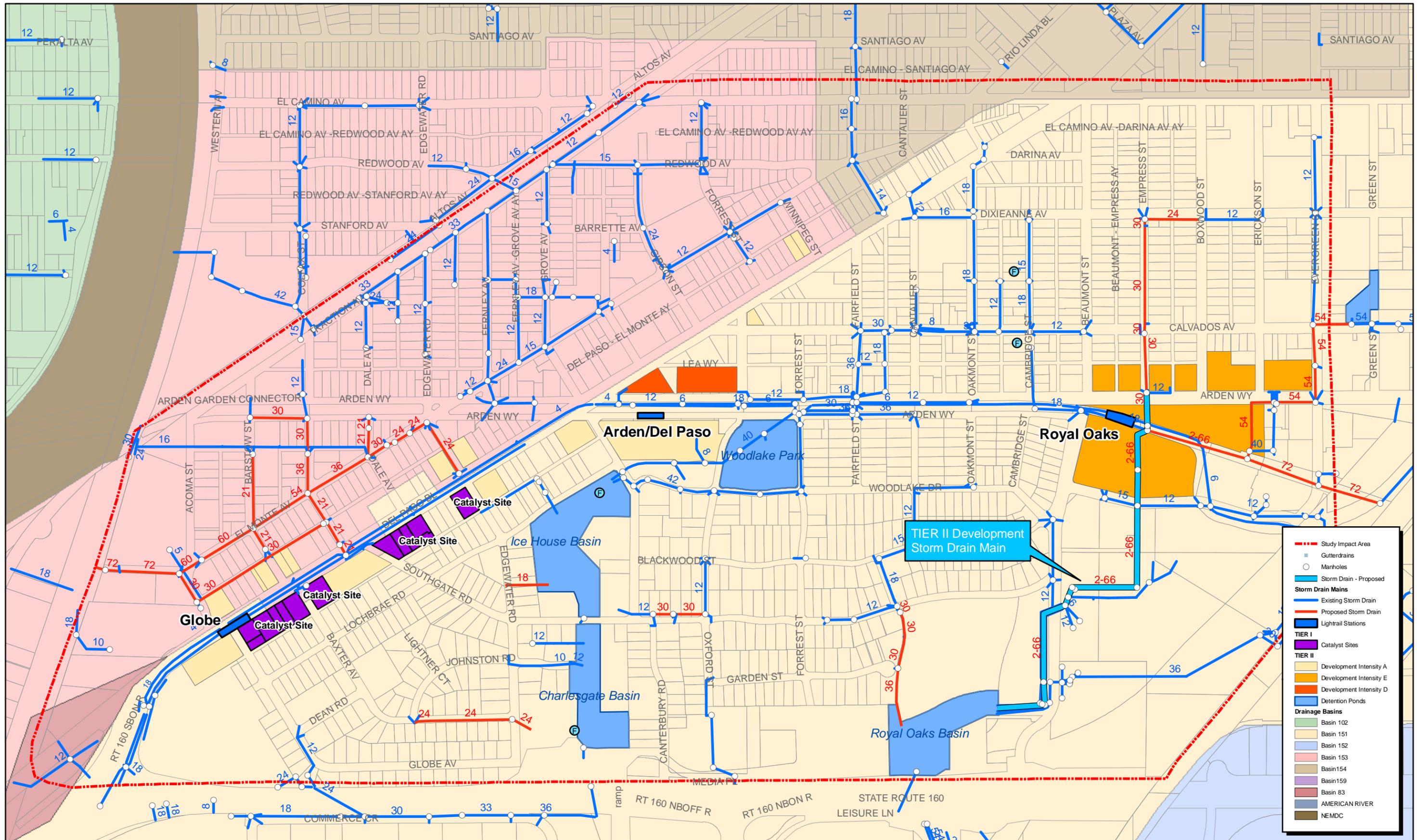
# NORTHEAST LINE LIGHT RAIL STATIONS PLAN- 100 YEAR FLOODPLAIN

FIGURE V - 1

January, 2011 - FINAL



Item #8



# NORTHEAST LINE LIGHT RAIL STATIONS PLAN - STORM DRAIN PLAN

FIGURE V-2

January, 2011 - FINAL

200 100 0 200 Feet

**NOLTE**  
BEYOND ENGINEERING

Item #8



## WATER SUPPLY

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The Northeast Line Light Rail Stations Plan (The Plan) project area is generally served by an extensive system of service mains ranging in size from 4 to 8 inches in diameter. The system in The Plan project area was generally constructed between the 1920s to 1960s.

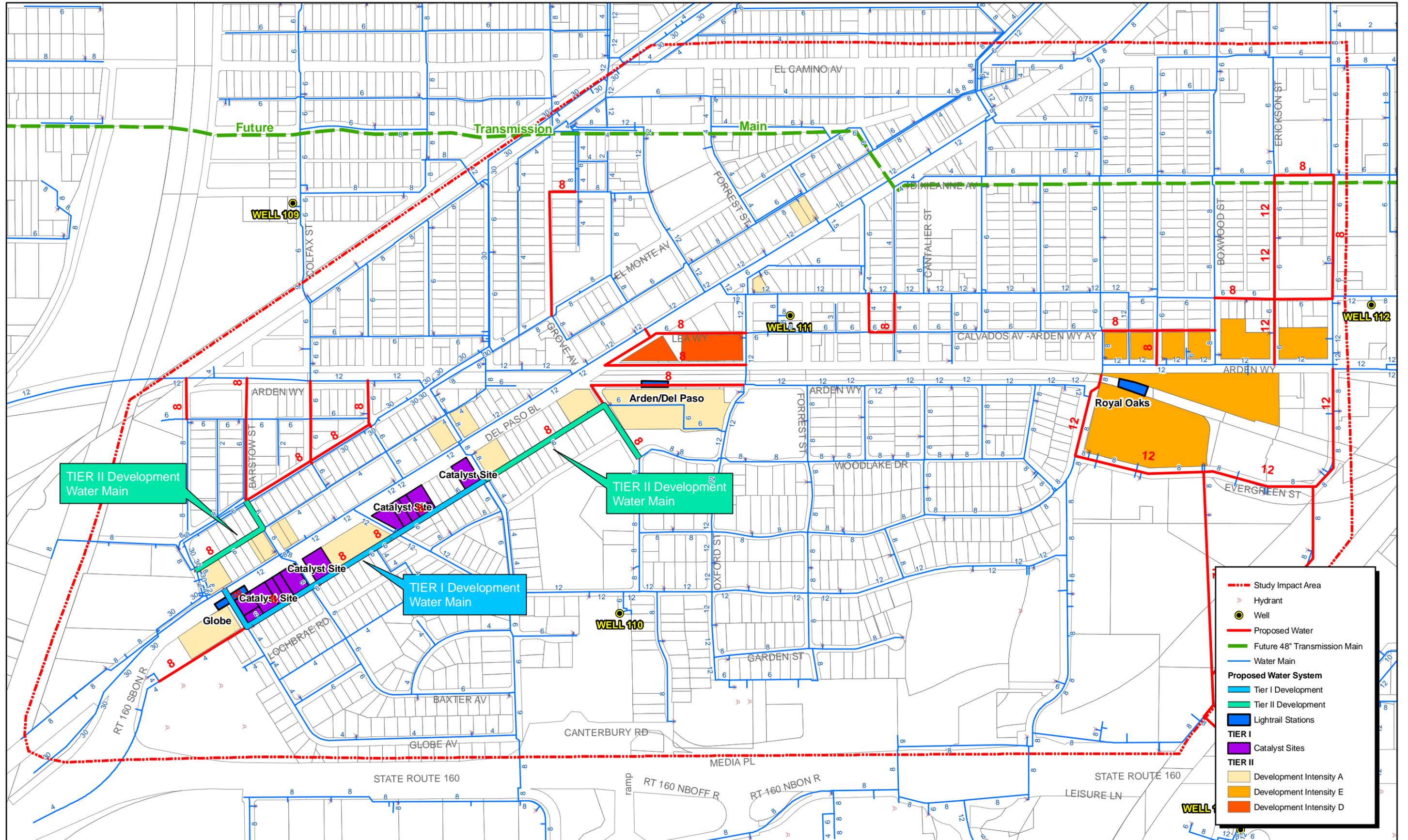
The existing corridors along Del Paso Boulevard and Arden Way are both well served by 12 inch distribution mains. However, the existing mains in the areas adjacent to these two corridors are generally undersized for the expected level of development of this focused study. The following is a description of water improvements for each area.

### FUTURE ACTION/RECOMMENDATION

**Del Paso Road Corridor:** The focused study envisions development to occur within the immediate area adjacent to Del Paso Road. The northerly side of the Corridor is served well by an existing 12 inch watermain located in the street along the northerly frontage. However, the southerly side of the Corridor will need to upsize the existing 6 inch main located in the alley to an 8 inch main to serve the expected development water/fire needs.

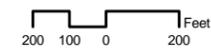
The replacement of this watermain would be a key infrastructure investment to serve the immediate needs of the focused study area. The main replacement could be performed in conjunction with the pavement replacement of the alley on this side of the Corridor.

**Arden Way Corridor:** The development along the Arden Way Corridor is expected to occur between Royal Oaks Drive and Evergreen Street. This area is well served by an existing 12 inch main located in Arden Way. To the south, the existing 8 inch main located in Royal Oaks Drive and Evergreen Street would serve the needs of the focus study development. However, as recommended in the original infrastructure study, this main should be upsized to a 12 inch main with further development to the south. To the north, the existing 6 inch mains should be replaced with 8 inch mains to serve the water/fire needs of the development.



# NORTHEAST LINE LIGHT RAIL STATIONS PLAN - PROPOSED WATER UTILITIES

FIGURE VI - 1  
January, 2011 - FINAL





## NATURAL GAS

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The Pacific Gas & Electric Company (PG&E) supplies natural gas to the Sacramento area. The high pressure gas system in the Northeast Line Light Rail Stations Plan Area, generally is served by a grid system throughout the Plan Area. A 12 inch transmission main is located on the west side of the Plan Area running along the old railroad/Traction Avenue corridor. An 8 inch high pressure main crosses the Plan Area connecting to the 12 inch main at Edgewater Road south to Arden Way where it turns and follows the Arden Way corridor eastward and leaves the project area at the eastern boundary.

As discussed in the original infrastructure study, PG&E has stated the existing gas infrastructure in the Northeast Line Light Rail Stations Plan Area should be adequate to serve the level of development proposed in the majority of the Globe Station and Del Paso – Arden Station areas with relatively minor additions, unless an unusually large gas user locates in the area. In that case, facilities will be upgraded as necessary in order to accommodate the user.

### FUTURE ACTION/RECOMMENDATION

With the development of the Royal Oaks Station area it is anticipated that a new transmission main loop will be needed to serve the development south of the Light Rail Tracks where currently only a dead-end 2 inch main exists located in Evergreen Street as well as a 2 inch main located in Royal Oaks Drive. It is anticipated that a 6 inch transmission main will need to be looped from the Arden/Evergreen intersection along Evergreen Street to Royal Oaks and south to the existing 6 inch main at Royal Oaks/Highway 160.

The above system costs are anticipated to be provided by PG&E. As with the original infrastructure study, no costs are anticipated with the development of the core development area.



## ELECTRICAL

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The Sacramento Municipal Utility District (SMUD) provides electrical service to customers located within the Northeast Line Light Rail Stations Plan (The Plan) area. Power is transmitted to The Plan area by a series of 69 kilovolt (kV) transmission lines that feed overhead/underground 12 kV and 4 kV distribution systems. Within the project area, the 69kV transmission lines are located along the south side of Arden Way, along the west side of Evergreen Street, and along the El Monte-Del Paso Alley.

The Evergreen – Royal Oaks Substation is located south of Arden Way between Evergreen Street and Royal Oaks Drive. This substation is a 69-12kV substation and feeds the majority of the project area via an existing overhead/underground distribution system. The portion of The Plan area north of Arden Way is generally served by a 4kV overhead distribution system.

With the full buildout of the original land use projections for the Northeast Line Light Rail Stations Plan area, SMUD estimated that the additional electrical load from development may be approximately 15 to 23 megawatts at final buildout. With typical system improvements SMUD's distribution system should be able to handle this new load growth.

The Evergreen – Royal Oaks Substation is located on a 0.2 acre parcel just south of the light rail tracks within the middle of proposed development for the area. The development of the area around the substation will need to include proper building setbacks, screening, etc. to the station as well as the transmission lines leading to the station.

### FUTURE ACTION/RECOMMENDATION

It is expected that future development in The Plan area will be served from the 12 kV distribution systems. The existing overhead distribution system will remain in order to maintain service to existing customers; however, portions of this system may be placed underground in segments as new buildings or street widening improvements are constructed. For the purposes of this focused study, it is anticipated the existing overhead system will remain in place and no undergrounding of the existing overhead systems will be required.



## PROBABLE ESTIMATE OF CONSTRUCTION COSTS

The costs presented here to construct the infrastructure necessary for the Northeast Line Light Rail Stations Plan area are intended for planning level only. They include the general costs for the overall buildout of the anticipated development of The Plan area using today's dollars.

An estimate of the near term "Key Infrastructure" projects has also been prepared. This estimate is intended to provide the costs for the potential project identified as key infrastructure investments to assist development of the core development area.

This estimate is not intended to be utilized for the actual costs for specific projects. The final costs for each specific project will need to be estimated separately and could be considerably different than those shown here due to the uncertainty of the order, timing and scope of the actual development to be constructed. The estimates have been developed solely to give interested parties a magnitude of the scale of the costs of improvements.

The unit costs are based on actual costs of recent development within the Del Paso Boulevard area, planning level costs utilized by various City departments as well as engineering judgment. Final unit costs for each specific project will depend on the actual labor and materials costs for the conditions at the time of construction. These conditions might include the scope of the development and the schedule of the completion of the project.

The estimates are generally separated into the corresponding infrastructure report for the different utilities. For each utility the estimates have been divided either along the major boundaries as for sewer and storm drainage, or by the corresponding Station area. Assumptions and clarifications for the costs are noted at the bottom of the individual sheets.

The unit costs for the storm drainage improvements utilized the 1996 Master Storm Drainage report as a basis and were increased using the ENR cost index from 1996 yearly average (ENR = 5,620) to the July 2010 values (ENR = 8,865).

The Streetwork improvements are based on the conceptual street sections prepared by MIG. The unit cost per foot was developed for each section and multiplied by the length of street within the plan area. Right-of-way acquisition has not been included in the estimates since it is expected that the improvements will be constructed within the existing road right-of-way.



## CONSTRUCTION COST ESTIMATE SUMMARY

### TIER I - CATALYST SITES

<b>A. STREETWORK</b>		
Streetscape Improvements		\$0
Del Paso Alleys		\$346,300
<b>B. SEWER SYSTEM</b>		
East		\$0
West		\$0
<b>C. DRAINAGE SYSTEM</b>		
Shed 151 East		\$0
<b>D. WATER DISTRIBUTION SYSTEM</b>		
Del Paso Alley		\$477,056
<b>TOTAL TIER I CONSTRUCTION (A-D)</b>		<b>\$823,356</b>

### TIER II - DEVELOPMENT SITES

<b>A. STREETWORK</b>		
Streetscape Improvements		\$0
Del Paso Alleys		\$268,088
<b>B. SEWER SYSTEM</b>		
East		\$273,139
West		\$783,641
<b>C. DRAINAGE SYSTEM</b>		
Shed 151 East		\$5,663,908
<b>D. WATER DISTRIBUTION SYSTEM</b>		
Del Paso Alleys		\$347,625
<b>TOTAL TIER II CONSTRUCTION (A-D)</b>		<b>\$7,336,401</b>

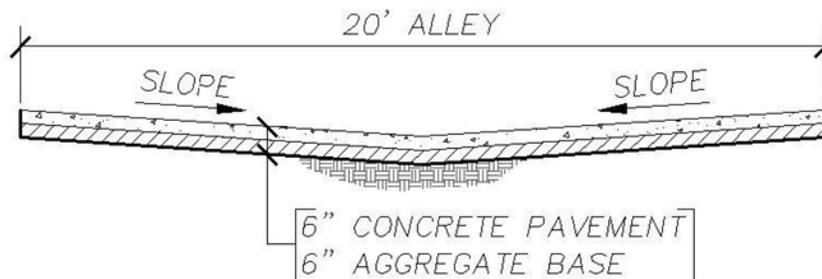


## STREETWORK COSTS

DESCRIPTION	QUANTITY	UNIT OF MEASURE	UNIT PRICE	AMOUNT
<b>A. STREETWORK</b>				
1. Del Paso Alleys - Catalyst Sites	1,440	LF	\$142.50	\$205,200
				35% Contingency \$71,800
				Subtotal <u>\$277,000</u>
				15% Engineering \$41,600
				10% Construction Management \$27,700
Total Del Paso Alleys - Catalyst Sites				<b><u>\$346,300</u></b>
2. Del Paso Alleys - Tier II Sites	1,115	LF	\$142.50	\$158,888
				35% Contingency \$55,600
				Subtotal <u>\$214,488</u>
				15% Engineering \$32,200
				10% Construction Management \$21,400
Total Del Paso Alleys - Tier II Sites				<b><u>\$268,088</u></b>
<b>TOTAL STREETWORK</b>				<b><u>\$614,388</u></b>



## DEL PASO ALLEY PAVEMENT



Description	Quantity	Unit of Measure	Unit Price	Amount
1. Earthwork	0.75	CY	\$30.00	\$22.50
2. 6" Concrete Pavement	20	SF	\$5.00	\$100.00
3. 6" Aggregate Base	20	SF	\$1.00	\$20.00
<b>Total Street Costs per LF</b>				<b><u>\$142.50</u></b>

### Assumptions:

1. One foot depth of earthwork over entire cross section.
2. "V" Gutter to be placed on center of alley.



## SEWER SYSTEM COSTS

### WEST SEWER SYSTEM COSTS

DESCRIPTION	QUANTITY	UNIT OF MEASURE	UNIT PRICE	AMOUNT
<b>B. SEWER SYSTEM</b>				
1. Sewer Manhole	16	EA	\$5,980.00	\$95,680
2. Sewer Pipe, 8"	0	LF	\$80.00	\$0
3. Sewer Pipe, 10"	0	LF	\$90.00	\$0
4. Sewer Pipe, 12"	0	LF	\$105.00	\$0
5. Sewer Pipe, 15"	0	LF	\$120.00	\$0
6. Sewer Pipe, 18"	0	LF	\$130.00	\$0
7. Sewer Pipe, 21"	1,635	LF	\$140.00	\$228,900
8. Sewer Pipe, 24"	420	LF	\$150.00	\$63,000
9. Sewer Pipe, 27"	480	LF	\$160.00	\$76,800
9. Service	0	EA	\$500.00	\$0
		<b>Subtotal</b>		<b>\$464,380</b>
		35% Contingency		\$162,533
		Subtotal		<b>\$626,913</b>
		15% Engineering		\$94,037
		10% Construction Management		\$62,691
		<b>SEWER SYSTEM SUBTOTAL</b>		<b>\$783,641</b>



## SEWER SYSTEM COSTS

### EAST SEWER SYSTEM COSTS

DESCRIPTION	QUANTITY	UNIT OF MEASURE	UNIT PRICE	AMOUNT
<b>B. SEWER SYSTEM</b>				
1. Sewer Manhole	7	EA	\$5,980.00	\$41,860
2. Sewer Pipe, 8"	0	LF	\$80.00	\$0
3. Sewer Pipe, 10"	0	LF	\$90.00	\$0
4. Sewer Pipe, 12"	0	LF	\$105.00	\$0
5. Sewer Pipe, 15"	1,000	LF	\$120.00	\$120,000
6. Sewer Pipe, 18"	0	LF	\$130.00	\$0
7. Sewer Pipe, 21"	0	LF	\$140.00	\$0
8. Sewer Pipe, 24"	0	LF	\$150.00	\$0
9. Service	0	EA	\$500.00	\$0
		<b>Subtotal</b>		<b>\$161,860</b>
		35% Contingency		\$56,651
		Subtotal		<u>\$218,511</u>
		15% Engineering		\$32,777
		10% Construction Management		\$21,851
		<b>SEWER SYSTEM SUBTOTAL</b>		<b><u>\$273,139</u></b>



## DRAINAGE SYSTEM COSTS - SHED 151 EAST

DESCRIPTION	QUANTITY	UNIT OF MEASURE	UNIT PRICE	AMOUNT
<b>C. DRAINAGE SYSTEM</b>				
1. Storm Drain Pipe, 18"	0	LF	\$96.00	\$0
2. Storm Drain Pipe, 24"	0	LF	\$130.00	\$0
3. Storm Drain Pipe, 30"	129	LF	\$160.00	\$20,640
4. Storm Drain Pipe, 36"	0	LF	\$195.00	\$0
5. Storm Drain Pipe, 48"	0	LF	\$265.00	\$0
6. Storm Drain Pipe, 54"	0	LF	\$310.00	\$0
7. Storm Drain Pipe, 60"	0	LF	\$350.00	\$0
8. Storm Drain Pipe, 66"	3,110	LF	\$395.00	\$1,228,450
9. Storm Drain Pipe, 72"	0	LF	\$435.00	\$0
10. Manhole, 12"-24"	0	EA	\$3,200.00	\$0
11. Manhole, 30"-36"	0	EA	\$3,175.00	\$0
12. Manhole, 42"-48"	1	EA	\$3,800.00	\$3,800
13. Manhole, 54"-60"	0	EA	\$4,150.00	\$0
14. Manhole, 66"-72"	10	EA	\$4,650.00	\$46,500
15. Northern West Basin Detention Basin Improvements	0	EA	\$1,415,500.00	\$0
16. Southern West Basin Detention Basin Improvements	0	EA	\$1,158,541.88	\$0
17. East Basin	1	EA	\$2,057,000.00	\$2,057,000
18. Flood Proofing (House)	0	EA	\$39,500.00	\$0
19. Flood Proofing (Building)	0	EA	\$78,900.00	\$0
		<b>Subtotal</b>		<b>\$3,356,390</b>
		35% Contingency		\$1,174,737
		Subtotal		<u>\$4,531,127</u>
		15% Engineering		\$679,669
		10% Construction Management		\$453,113
<b>TOTAL STORM DRAIN SHED 151 EAST</b>				<b>\$5,663,908</b>

Unit prices derived by applying the McGraw-Hill Construction ENR (July, 2010 - 8,865, 1996 - 5620) to the City of Sacramento's 1996 sump 151 Storm Drain Master Plan.



## WATER DISTRIBUTION SYSTEM COSTS

DESCRIPTION	QUANTITY	UNIT OF MEASURE	UNIT PRICE	AMOUNT
<b>D.1 WATER DISTRIBUTION SYSTEM - CATALYST SITES</b>				
1. Water, 8" (Incl. fittings)	2,427	LF	\$100.00	\$242,700
2. Fire Hydrant	8	EA	\$5,000.00	\$40,000
				<b>Subtotal</b>
				\$282,700
				35% Contingency
				\$98,945
				<b>Subtotal and Contingency</b>
				<b>\$381,645</b>
				15% Engineering
				\$57,247
				10% Construction Management
				\$38,165
				<b>Total Water Distribution System - Catalyst Sites</b>
				<b><u>\$477,056</u></b>
<b>D.2 WATER DISTRIBUTION SYSTEM - TIER II</b>				
1. Water, 8" (Incl. fittings)	1,760	LF	\$100.00	\$176,000
2. Fire Hydrant	6	EA	\$5,000.00	\$30,000
				<b>Subtotal</b>
				\$206,000
				35% Contingency
				\$72,100
				<b>Subtotal and Contingency</b>
				<b>\$278,100</b>
				15% Engineering
				\$41,715
				10% Construction Management
				\$27,810
				<b>Total Water Distribution System - Tier II Sites</b>
				<b><u>\$347,625</u></b>
				<b>TOTAL WATER DISTRIBUTION SYSTEM</b>
				<b><u>\$824,681</u></b>

**APPENDIX A**  
**LAND USE CALCULATIONS**



## APPENDIX A – LAND USE CALCULATIONS

**Table A-1  
Proposed Land Use Development Intensity**

	Total Developable Area (Acres)	Residential (Dwelling Units)		Non-Residential (Acres)		Non-Residential (Square Feet)	
		<i>Low</i>	<i>High</i>	<i>Low</i>	<i>High</i>	<i>Low</i>	<i>High</i>
<b>Globe Station</b>							
Development Intensity A	5.55	222	333	1.67	2.22	72,567	96,756
Development Intensity B	6.89	276	413	0.00	0.00	0	0
Development Intensity C	9.66	242	387	0.00	0.00	0	0
Development Intensity D							
Development Intensity E							
<b>Total for Globe Station</b>	22.11	739	<b>1,133</b>	1.67	2.22	72,567	<b>96,756</b>
<b>Del Paso - Arden Station</b>							
Development Intensity A	5.34	214	320	1.60	2.14	69,763	93,017
Development Intensity B							
Development Intensity C	4.06	102	162	0.00	0.00	0	0
Development Intensity D	1.70	25	42	0.76	1.02	33,294	44,392
Development Intensity E							
<b>Total for Del Paso/Arden Station</b>	11.10	341	<b>525</b>	2.37	3.15	103,057	<b>137,409</b>
<b>Royal Oaks Station</b>							
Development Intensity A							
Development Intensity B	27.69	1,107	1,661	0.00	0.00	0	0
Development Intensity C	3.39	85	136	0.00	0.00	0	0
Development Intensity D							
Development Intensity E	13.13	328	525	3.94	5.25	171,579	228,772
<b>Total for Royal Oaks Station</b>	44.21	1,521	<b>2,322</b>	3.94	5.25	171,579	<b>228,772</b>
<b>Total For All Stations</b>	77.41	2,600	<b>3,980</b>	7.97	10.63	347,203	<b>462,937</b>



City of Sacramento

Northeast Line Light Rail Stations Plan - Phased Infrastructure Recommendations

Globe Station

AREA (SQ FT)	AREA (ACS)	APN	LANDUSE_DE	ZONE	Dev_Type	Res_Low	Res_High	NonRes_Low	NonRes_High	NonRes_Low	NonRes_High
3706.26371	0.085	275-0161-008	SMALL RETAIL	C-2-SPD A		3	5	0.026	0.034	1112	1483
11199.10269	0.257	275-0161-007	LIGHT INDUSTRIAL	C-2-SPD A		10	15	0.077	0.103	3360	4480
26674.88007	0.612	275-0260-008	SMALL RETAIL	C-2	A	24	37	0.184	0.245	8002	10670
10754.79895	0.247	275-0165-018		C-2-SPD A		10	15	0.074	0.099	3226	4302
12508.50801	0.287	275-0161-014	HEAVY INDUSTRIAL	C-2-SPD A		11	17	0.086	0.115	3753	5003
14989.85393	0.344	275-0163-006	CEMETARY/MORTUARY	C-2-SPD A		14	21	0.103	0.138	4497	5996
7526.52037	0.173	275-0162-001	VACANT/OFFICE	C-2-SPD A		7	10	0.052	0.069	2258	3011
15093.71871	0.347	275-0162-004	VETERINARIAN	C-2-SPD A		14	21	0.104	0.139	4528	6037
9168.49491	0.210	275-0165-003		C-2-SPD A		8	13	0.063	0.084	2751	3667
5098.83856	0.117	275-0163-002	LOW RISE APARTMENT < 4 STORIES	C-2-SPD A		5	7	0.035	0.047	1530	2040
7304.59349	0.168	275-0164-013	LIGHT INDUSTRIAL	C-2-SPD A		7	10	0.050	0.067	2191	2922
10050.71992	0.231	275-0163-001	VACANT/RETAIL	C-2-SPD A		9	14	0.069	0.092	3015	4020
14794.43068	0.340	275-0163-003	VACANT/OFFICE	C-2-SPD A		14	20	0.102	0.136	4438	5918
7527.97401	0.173	275-0163-005		C-2-SPD A		7	10	0.052	0.069	2258	3011
7533.13738	0.173	275-0165-002		C-2-SPD A		7	10	0.052	0.069	2260	3013
7606.94303	0.175	275-0163-004	VACANT/OFFICE	C-2-SPD A		7	10	0.052	0.070	2282	3043
18531.29888	0.425	275-0165-019	PARKING LOT	C-2-SPD A		17	26	0.128	0.170	5559	7413
6621.93193	0.152	275-0161-013	LIGHT INDUSTRIAL	C-2-SPD A		6	9	0.046	0.061	1987	2649
7256.27135	0.167	275-0164-014	LIGHT INDUSTRIAL	C-2-SPD A		7	10	0.050	0.067	2177	2903
603.60259	0.014	275-0165-017		C-2	A	1	1	0.004	0.006	181	241
7568.00765	0.174	275-0122-008	VACANT/RETAIL	C-2-SPD A		7	10	0.052	0.069	2270	3027
22346.51844	0.513	275-0165-016	SMALL RETAIL	C-2-SPD A		21	31	0.154	0.205	6704	8939
7422.50529	0.170	275-0122-007	VACANT/RETAIL	C-2-SPD A		7	10	0.051	0.068	2227	2969
246916.43919	5.668	275-0111-006		M-1-SPD B		227	340				
8029.47329	0.184	275-0161-016	VACANT/INDUSTRIAL	M-1-SPD B		7	11				
7449.57396	0.171	275-0164-002	LIGHT INDUSTRIAL	C-2-SPD B		7	10				
7617.04243	0.175	275-0161-017	VACANT/INDUSTRIAL	M-1-SPD B		7	10				
7406.16140	0.170	275-0161-004	VACANT/RECREATIONAL	M-1-SPD B		7	10				
7378.88234	0.169	275-0161-006	RESIDENTIAL/SINGFAM/SUBDIV	M-1-SPD B		7	10				
7371.64339	0.169	275-0164-001	LIGHT INDUSTRIAL	C-2-SPD B		7	10				
7852.60653	0.180	275-0122-004	VACANT/OFFICE	C-2-SPD B		7	11				
5379.77286	0.124	275-0121-002	VACANT/RESIDENTIAL	C-2-SPD C		3	5				
3343.54527	0.077	275-0113-010	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD C		2	3				
21707.96949	0.498	275-0114-015	VACANT/RETAIL	C-2-SPD C		12	20				
4435.26015	0.102	275-0113-012	RESIDENTIAL/FOURPLEX	C-2-SPD C		3	4				
7212.03949	0.166	275-0114-006	VACANT/RETAIL	C-2-SPD C		4	7				
7059.86940	0.162	275-0113-004	HEAVY INDUSTRIAL	C-2-SPD C		4	6				
9296.62141	0.213	275-0112-027	LIGHT INDUSTRIAL	M-1-SPD C		5	9				
9494.73286	0.218	275-0114-013	LIGHT INDUSTRIAL	C-2-SPD C		5	9				
6751.09303	0.155	275-0113-015	VACANT/RECREATIONAL	M-1-SPD C		4	6				
7454.36355	0.171	275-0163-007	RESIDENTIAL/SINGFAM/SUBDIV	R-1	C	4	7				
6307.29539	0.145	275-0121-001	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD C		4	6				
42756.04344	0.982	275-0112-001	VACANT/INDUSTRIAL	M-1-SPD C		25	39				
7108.21423	0.163	275-0112-017	LIGHT INDUSTRIAL	M-1-SPD C		4	7				
10168.77658	0.233	275-0113-023	RESIDENTIAL/SINGFAM/NONSUB	C-2-SPD C		6	9				
3510.46015	0.081	275-0113-013	VACANT/RECREATIONAL	M-1-SPD C		2	3				
4298.67769	0.099	275-0113-014	VACANT/RECREATIONAL	M-1-SPD C		2	4				
7486.40286	0.172	275-0113-003	HEAVY INDUSTRIAL	C-2-SPD C		4	7				
7041.35668	0.162	275-0114-005	VACANT/RETAIL	C-2-SPD C		4	6				
7118.10622	0.163	275-0113-005	HEAVY INDUSTRIAL	C-2-SPD C		4	7				
6645.73737	0.153	275-0112-007	LIGHT INDUSTRIAL	M-1-SPD C		4	6				
10242.45544	0.235	275-0113-024	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD C		6	9				
13424.83972	0.308	275-0112-005	VACANT/INDUSTRIAL	M-1-SPD C		8	12				
10597.17432	0.243	275-0112-026	LIGHT INDUSTRIAL	M-1-SPD C		6	10				
6848.42017	0.157	275-0121-003	CITY	C-2-SPD C		4	6				
10037.81656	0.230	275-0114-014	VACANT/INDUSTRIAL	C-2-SPD C		6	9				
7119.03007	0.163	275-0113-011	RESIDENTIAL/DUPLEX	C-2-SPD C		4	7				
8912.89822	0.205	275-0112-011	LIGHT INDUSTRIAL	M-1-SPD C		5	8				
6738.71376	0.155	275-0112-002	LIGHT INDUSTRIAL	M-1-SPD C		4	6				
7604.66902	0.175	275-0163-009	VACANT/RESIDENTIAL	R-1	C	4	7				
6992.89030	0.161	275-0112-015	HEAVY INDUSTRIAL	M-1-SPD C		4	6				
8758.77745	0.201	275-0114-007	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD C		5	8				
6569.20436	0.151	275-0112-004	LIGHT INDUSTRIAL	M-1-SPD C		4	6				
6473.26218	0.149	275-0112-008	RESIDENTIAL/SINGFAM/SUBDIV	M-1-SPD C		4	6				
6694.47535	0.154	275-0112-003	LIGHT INDUSTRIAL	M-1-SPD C		4	6				
6498.42533	0.149	275-0112-006	VACANT/RECREATIONAL	M-1-SPD C		4	6				
6845.83050	0.157	275-0113-016	HEAVY INDUSTRIAL	M-1-SPD C		4	6				
6946.31092	0.159	275-0112-020	HEAVY INDUSTRIAL	M-1-SPD C		4	6				
53371.06382	1.225	275-0113-022	LIGHT INDUSTRIAL	M-1-SPD C		31	49				
4713.01176	0.108	275-0112-009	RESIDENTIAL/SINGFAM/SUBDIV	M-1-SPD C		3	4				
7219.26005	0.166	275-0114-004	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD C		4	7				
4633.30714	0.106	275-0112-010	LIGHT INDUSTRIAL	M-1-SPD C		3	4				
7036.13136	0.162	275-0112-021	HEAVY INDUSTRIAL	M-1-SPD C		4	6				
22133.27586	0.508	275-0112-025	LIGHT INDUSTRIAL	M-1-SPD C		13	20				
22.10510						739	1133	1.7	2.2	72566.7	96755.6



City of Sacramento

Northeast Line Light Rail Stations Plan - Phased Infrastructure Recommendations

Del Paso / Arden Station

AREA	AREA (ACS)	APN	LANDUSE_DE	ZONE	Dev_Type	Res_Low	Res_High	NonRes_Low	NonRes_High	NonRes_Low	NonRes_High
46241.36821		1.06 275-0134-008	CITY	R-1	A	42	64	0.318	0.425	13872	18497
7790.11569		0.18 275-0124-009	RESTAURANT	C-2-SPD	A	7	11	0.054	0.072	2337	3116
614.33386		0.01 275-0134-007	CITY	R-1	A	1	1	0.004	0.006	184	246
15458.50131		0.35 275-0125-028	VACANT/OFFICE	C-2-SPD	A	14	21	0.106	0.142	4638	6183
7394.34622		0.17 275-0125-001	VACANT/OFFICE	C-2-SPD	A	7	10	0.051	0.068	2218	2958
695.35928		0.02 275-0134-006	CITY	R-1	A	1	1	0.005	0.006	209	278
18577.48239		0.43 275-0134-003	CITY	R-1	A	17	26	0.128	0.171	5573	7431
24196.07864		0.56 275-0134-010	CITY	R-1	A	22	33	0.167	0.222	7259	9678
7699.67567		0.18 275-0093-005	SMALL RETAIL	C-2-SPD	A	7	11	0.053	0.071	2310	3080
7024.02676		0.16 275-0093-004	SMALL RETAIL	C-2-SPD	A	6	10	0.048	0.064	2107	2810
6059.36712		0.14 275-0095-016	SMALL RETAIL	C-2-SPD	A	6	8	0.042	0.056	1818	2424
1946.89661		0.04 275-0134-004	CITY	R-1	A	2	3	0.013	0.018	584	779
49591.96653		1.14 275-0134-012	CITY	R-1	A	46	68	0.342	0.455	14878	19837
3654.38386		0.08 275-0134-011	CITY	R-1	A	3	5	0.025	0.034	1096	1462
27839.70489		0.64 275-0125-029	RESTAURANT	C-2-SPD	A	26	38	0.192	0.256	8352	11136
7759.33631		0.18 275-0124-010	RESTAURANT	C-2-SPD	A	7	11	0.053	0.071	2328	3104
75384.48272		1.73 275-0085-013	VACANT/RESIDENTIAL	C-2	C	43	69				
4167.59998		0.10 275-0084-016	VACANT/RETAIL	C-2-SPD	C	2	4				
6494.04387		0.15 275-0095-007	RESIDENTIAL/SINGFAM/SUBDIV	R-1	C	4	6				
7685.36187		0.18 275-0125-023	PARKING LOT	R-3	C	4	7				
1057.25676		0.02 275-0082-001	VACANT/RETAIL	C-2-SPD	C	1	1				
6502.17503		0.15 275-0145-012	RESIDENTIAL/DUPLEX	R-1	C	4	6				
4251.65894		0.10 275-0125-024	PARKING LOT	R-3	C	2	4				
6337.44124		0.15 275-0085-009	VACANT/RESIDENTIAL	R-1	C	4	6				
7573.76036		0.17 275-0125-022	PARKING LOT	R-3	C	4	7				
6317.06702		0.15 275-0085-010	VACANT/RESIDENTIAL	R-1	C	4	6				
6649.49630		0.15 275-0028-004	VACANT/RESIDENTIAL	R-1	C	4	6				
6519.86828		0.15 275-0145-013	VACANT/RESIDENTIAL	R-1	C	4	6				
10114.43233		0.23 275-0091-001	VACANT/RETAIL	R-1	C	6	9				
6459.11745		0.15 275-0085-011	VACANT/RESIDENTIAL	R-1	C	4	6				
21353.05374		0.49 275-0125-016	LOW RISE APARTMENT < 4 STORIES	R-3	C	12	20				
6132.34581		0.14 275-0131-014	OFFICE GENERAL	C-2-SPD	D	2	4	0.063	0.084	2760	3679
9591.12809		0.22 275-0131-020	SERVICE STATION	C-2-SPD	D	3	6	0.099	0.132	4316	5755
1665.19067		0.04 275-0131-008	NO USE	C-2-SPD	D	1	1	0.017	0.023	749	999
5720.36923		0.13 275-0131-009	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD	D	2	3	0.059	0.079	2574	3432
6036.53224		0.14 275-0131-017	LIGHT INDUSTRIAL	C-2-SPD	D	2	3	0.062	0.083	2716	3622
7659.94704		0.18 275-0131-007	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD	D	3	4	0.079	0.106	3447	4596
7146.04615		0.16 275-0131-011	RESIDENTIAL CONVERION TO OFFICE	C-2-SPD	D	2	4	0.074	0.098	3216	4288
7454.07982		0.17 275-0131-010	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD	D	3	4	0.077	0.103	3354	4472
7499.94778		0.17 275-0131-013	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD	D	3	4	0.077	0.103	3375	4500
7631.19020		0.18 275-0131-016	SMALL RETAIL	C-2-SPD	D	3	4	0.079	0.105	3434	4579
7449.56573		0.17 275-0131-012	RESIDENTIAL/SINGFAM/SUBDIV	C-2-SPD	D	3	4	0.077	0.103	3352	4470
	11.09725					341	525	2.4	3.2	103056.7	137409.0



## Royal Oaks Station

APN	LANDUSE DESIGNATION	ZONE	Dev_Type	Res_Low	Res_High	NonRes_Low	NonRes_High	NonRes_L	NonRes_High
275-0240-092	OFFICE LARGE SINGLE TENANT	OB-LI	B	125	188				
275-0240-088	STATE	OB-LI	B	20	31				
277-0144-022	STATE	M-1-LI	B	89	133				
275-0240-087	STATE	OB-LI	B	225	337				
277-0134-023	LARGE RETAIL	M-1	B	19	29				
275-0240-074	LARGE RETAIL	OB-LI	B	70	105				
275-0240-094	HEAVY INDUSTRIAL	OB-LI	B	56	83				
275-0240-089	STATE	OB-LI	B	42	63				
275-0240-045	POST OFFICE	OB-LI	B	101	152				
277-0134-024	SPECIAL DISTRICT	M-1	B	22	33				
275-0240-052	POST OFFICE	OB-LI	B	91	137				
275-0240-051	STATE	OB-LI	B	200	299				
275-0240-029	STATE	OB-LI	B	47	71				
277-0132-006	HEAVY INDUSTRIAL	M-1	C	3	4				
277-0131-012	LOW RISE APARTMENT < 4 STORIES	R-1	C	4	6				
277-0133-006	INDUSTRIAL/MULTI-TENANT	M-1	C	5	9				
277-0133-002	RESIDENTIAL/SINGFAM/SUBDIV	M-1	C	3	4				
277-0133-008	INDUSTRIAL/MULTI-TENANT	M-1	C	3	5				
277-0073-009	VACANT/INDUSTRIAL	M-1	C	39	62				
275-0104-023	RESIDENTIAL/SINGFAM/SUBDIV	R-1	C	3	5				
275-0104-024	RESIDENTIAL/SINGFAM/SUBDIV	R-1	C	3	5				
277-0133-003	INDUSTRIAL/MULTI-TENANT	M-1	C	4	7				
277-0132-005	HEAVY INDUSTRIAL	M-1	C	3	4				
277-0072-027	RESIDENTIAL/SINGFAM/SUBDIV	R-1	C	4	6				
277-0071-008	VACANT/RESIDENTIAL	R-1	C	4	6				
277-0131-002	RESIDENTIAL/SINGFAM/SUBDIV	R-1	C	4	6				
277-0072-026	RESIDENTIAL/SINGFAM/SUBDIV	R-1	C	4	6				
277-0134-021	SERVICE STATION	M-1	E	12	20	0.149	0.199	6510	8679
275-0240-071	OFFICE GENERAL	OB-R	E	1	2	0.018	0.023	765	1021
275-0240-090	OFFICE LARGE SINGLE TENANT	OB-LI	E	41	66	0.495	0.661	21582	28776
277-0134-004	LARGE RETAIL	M-1	E	18	28	0.211	0.281	9173	12231
277-0131-007	LIGHT INDUSTRIAL	C-2	E	3	5	0.039	0.052	1711	2281
275-0155-005	VACANT/RESIDENTIAL	R-1	E	4	6	0.045	0.060	1957	2610
277-0131-017	VACANT/RESIDENTIAL	R-1	E	4	6	0.048	0.065	2109	2812
275-0155-013	OFFICE GENERAL	C-2	E	8	12	0.091	0.122	3975	5300
277-0132-011	LIGHT INDUSTRIAL	M-1	E	12	19	0.139	0.185	6047	8063
277-0134-005	LARGE RETAIL	M-1	E	23	36	0.271	0.361	11805	15740
275-0240-072	OFFICE GENERAL	OB-LI	E	76	122	0.913	1.217	39756	53008
275-0155-004	VACANT/RESIDENTIAL	R-1	E	4	6	0.046	0.061	1993	2657
277-0134-003	LARGE RETAIL	M-1	E	30	48	0.364	0.485	15835	21113
277-0133-005		M-1	E	27	44	0.328	0.438	14297	19063
277-0131-016	VACANT/RESIDENTIAL	R-1	E	4	6	0.044	0.058	1899	2532
277-0134-020	VACANT/RETAIL	M-1	E	6	9	0.068	0.090	2952	3936
275-0155-006	USED CAR SALES	R-1	E	3	5	0.040	0.054	1759	2346
275-0155-007	VACANT/RETAIL	C-2	E	3	5	0.041	0.055	1781	2374
277-0132-009	LIGHT INDUSTRIAL	M-1	E	18	29	0.214	0.286	9331	12442
275-0240-070	OFFICE GENERAL	OB-R	E	15	25	0.185	0.246	8043	10723
277-0131-005	RESIDENTIAL/SINGFAM/SUBDIV	R-1	E	4	6	0.047	0.063	2069	2758
277-0131-006	RESIDENTIAL/SINGFAM/SUBDIV	R-1	E	4	6	0.044	0.059	1918	2558
275-0240-076	SPECIAL DISTRICT	OB-LI	E	5	8	0.060	0.080	2608	3477
277-0131-008	RESTAURANT	C-2	E	3	5	0.039	0.052	1704	2272
				1521	2322	3.9	5.3	171579	228772



# Assumptions

	FAR		DU/AC	
	Low	High	Low	High
A	0.3	0.4	40	60
B	x	x	40	60
C	x	x	25	40
D	0.45	0.6	15	25
E	0.3	0.4	25	40

**Attachment 9**

**August 26, 2010 Planning Commission Comments and Staff's Responses**

<b>Planning Commission Comment</b>	<b>Staff's Response</b>
<ul style="list-style-type: none"> <li>▪ Notices should be sent to both property owners and occupants</li> <li>▪ Staff should make an extra effort to get the word out to the community</li> </ul>	<ul style="list-style-type: none"> <li>▪ Notices have been sent to property owners and occupants of parcels that are to be rezoned or have the General Plan Land Use Designation changed. Properties within 500 feet of these land use changes were noticed as well</li> <li>▪ Thirty three stakeholders were noticed</li> <li>▪ Three hundred and eighty one residents in the Dixieanne Neighborhood were noticed</li> <li>▪ Please refer to the list of outreach conducted in Attachment 10</li> </ul>
<ul style="list-style-type: none"> <li>▪ Staff should develop a process citywide for developing in-lieu fee districts that would allow flexibility in requiring parking for infill developments</li> </ul>	<ul style="list-style-type: none"> <li>▪ Prior to establishing a in-lieu fee district for the plan area, CDD and DOT management need to agree both on citywide parking strategies and the commitment of staff resources</li> </ul>
<ul style="list-style-type: none"> <li>▪ Ensure that the land use changes for the Northeast Line are consistent with those for the Swanston Station</li> </ul>	<ul style="list-style-type: none"> <li>▪ Planning and DOT staff have revised the Swanston Station rezone strategy to be consistent with the zoning surrounding the Globe, Arden/Del Paso and Royal Oaks Stations</li> </ul>
<ul style="list-style-type: none"> <li>▪ Consider making the notification multi-family developments in the SPD to be similar as that of Planning Commission and provide some assurance that staff level review of these projects will have the same level of independent decision making</li> </ul>	<ul style="list-style-type: none"> <li>▪ This issue is still under consideration by staff</li> </ul>

### Outreach Conducted for the Northeast Line Implementation Plan

Del Paso Boulevard Partnership	3/25/10
Regional Transit Staff	4/26/10
Meeting with Property/Business Owners that included: <ul style="list-style-type: none"><li>• David Plag (PBID)</li><li>• Rich Meeker (Business Owner)</li><li>• Deborah Redmond (News &amp; Review)</li><li>• Rosemary Covington (Regional Transit)</li><li>• Rob Kerth (North Sacramento Chamber of Commerce)</li><li>• Shane Curry (Business Owner)</li><li>• Jerry Greenberg (Business Owner)</li><li>• Bobby Omery (Business Owner)</li></ul>	4/29/10
Meeting with Alan Warren (Developer)	5/18/10
Meeting with Bob Slobe (Developer)	5/18/10
Phone Conference with Dan Friedlander (Business Owner, Developer)	5/26/10
Meeting with Russ Wyluda (Developer)	6/11/10
North Sacramento Redevelopment Advisory Committee	7/15/10
Planning Commission Workshop	8/26/10
Woodlake Neighborhood Association	10/6/10
North Sacramento Redevelopment Advisory Committee Members	11/25/10