



# Special Meeting Agenda City of Sacramento Planning Commission

## COMMISSION MEMBERS:

*Anna Molander  
Michael Mendez, MCP  
Philip Harvey*

*Jameel Pugh  
Joseph Contreras, Vice-Chair  
Michael Notestine  
Rommel Declines*

*Joseph Yee, AIA, Chair  
Panama Bartholomy  
(Vacant)  
(Vacant)*

## CITY STAFF:

*Tom Pace, Principal Planner  
Sabina Gilbert, Senior Deputy City Attorney*

*New City Hall  
915 I Street, 1st Floor – Council Chambers*

*March 24, 2011 – 5:30 P.M.*

*The City Planning Commission was created by the City Council. Its powers and duties include: to develop and maintain the General Plan; to make recommendations to the City Council on amendments to the General Plan and the City's zoning code and on zoning changes; to act upon applications for tentative subdivision maps, special permits and variances; and to make environmental determinations associated with these actions.*

## **NOTICE TO THE PUBLIC**

You are welcomed and encouraged to participate in this meeting. Public comment is taken (3 minutes maximum) on items listed on the agenda when they are called. Public Comment on items not listed on the agenda will be heard at the end of the meeting as noted on the agenda. Comments on controversial items may be limited and large groups are encouraged to select 3-5 speakers to represent the opinion of the group.

**Notice to Lobbyists:** When addressing the Commission you must identify yourself as a lobbyist and announce the client/business/organization you are representing (City Code 2.15.160).

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Meeting facilities are accessible to persons with disabilities. If you require special assistance to participate in the meeting, notify the Office of the City Clerk at (916) 808-7200 at least 48 hours prior to the meeting.



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# SPECIAL AGENDA

**March 24, 2011**

*New City Hall  
915 I Street – 1st Floor, Council Chambers*

All items listed are heard and acted upon by the Planning Commission unless otherwise noted.

## Call to Order – 5:30 p.m.

### Roll Call

### Consent Calendar

All items listed under the Consent Calendar are considered and acted upon by one motion. Anyone may request that an item be removed for separate consideration.

1. **Approval of Minutes for March 10, 2011**

**Location:** Citywide

**Recommendation:** Approve Commission Minutes from March 10, 2011.

**Contact:** Tom Pace, Principal Planner, 916-808-6848

### Director's Report

2. **Director's Report** (Oral)

**Location:** Citywide

**Recommendation: Receive and File-** Status report on pending development applications and appeals; proposed amendments to Zoning Code, design standards, and other development-related regulations; Community Development Department organizational and operational changes, work program, and training program; and similar matters.

**Contact:** Tom Pace, Principal Planner, 916-808-6848

### Public Hearings

Public hearings may be reordered by the Chair at the discretion of the Commission. If you challenge the decision of this Commission you may be limited to raising only those issues that are raised in this hearing or in written correspondence received by the Commission prior to the hearing.

3. **P09-006 Provence** (Noticed 2/28/11) (Continued from 3/10/11)

**Location:** Area southwest of East Commerce Way and Benefit Way, 225-2330-002-0000 through 225-2330-076-0000, 225-2680-002-0000 through 225-2680-083-0000, 225-2690-002-0000 through 225-2690-069-0000, District 1.

**Recommendation: Forward Recommendations of Approval to City Council: Item**

**A:** Mitigated Negative Declaration; **Item B:** Mitigation Monitoring Plan; **Item C:**

Planned Unit Dev-Schematic Pln Amended to re-designate use and density on 13.9

gross acres; **Item D:** Tentative Map from 182 parcels to 28 residential lots, 5 private

street lots, 16 private drive lots and 1 landscape lot; **Item E:** Special Permit to develop

233 condominium units; **Item F:** Special Permit-Major Modification to modify previous approval from 182 townhouse units to 233 condominium units.

**Contact:** David Hung, Associate Planner, 916-808-5530; Lindsey Alagozian, Senior Planner, 916-808-2659

4. **P10-089 Courtyard Condominiums** (Noticed 2/28/11) (Continued from 3/10/11)  
**Location:** Northwest corner of T Street and 24th Street, 010-0036-011-0000, 010-0036-012-0000, District 4.

**Recommendation: Approve - Item A:** Environmental Exemption (Per CEQA 15332); **Item B:** Tentative Map to create one condominium lot in the R-3A zone; **Item C:** Special Permit to construct 6 condominium units in the R-3A zone; **Item D:** Special Permit-Parking Reduction to reduce one required parking space for a multi-family development; **Item E:** Special Permit to reduce the required street side setback from five feet to two feet for an accessory structure; **Item F:** Variance to waive the required trash enclosure for a multi-family development.

**Contact:** David Hung, Associate Planner, 916-808-5530; Sandra Yope, Senior Planner, 916-808-7158

5. **P11-019 19<sup>th</sup>/K Street Parking** (Noticed 3/14/11)  
**Location:** 1831 K Street, 007-0014-008-0000, 007-0081-027-0000, District 3.

**Recommendation: Approve - Item A:** Environmental Exemption (Per CEQA 15301); **Item B:** Variance to allow an offsite parking lot under same ownership as an office building that is located more than 300 feet away.

**Contact:** Evan Compton, Associate Planner, 916-808-5260, Stacia Cosgrove, Senior Planner, 916-808-7110

6. **P11-021 Development Agreement Amendment for Truxel 3 PUD** (Noticed 3/14/11)  
**Location:** 3500 Truxel Road, 225-2110-048-0000, District 1.

**Recommendation: Forward Recommendations of Approval to City Council: Item A:** Environmental Determination: Previously Adopted Mitigated Negative Declaration; **Item B:** Previously Adopted Mitigation Monitoring Plan; and **Item C:** Amend City Agreement No. 96-051 (the development agreement for the Truxel 3 PUD) to extend the initial term.

**Contact:** Lindsey Alagozian, Senior Planner, 916-808-2659, Greg Bitter, Principal Planner, 916-808-7816

### Staff Reports

Staff reports include oral presentations including those recommending Receive and File.

**None**

### Public Comments- Matters Not on the Agenda

7. **To be announced.**

**Questions, Ideas and Announcements of Commission Members**

8. To be announced.

**Adjournment**



# Minutes City of Sacramento Planning Commission

# 1

## COMMISSION MEMBERS:

*Anna Molander  
Michael Mendez, MCP  
Philip Harvey*

*Jameel Pugh  
Joseph Contreras, Vice-Chair  
Michael Notestine  
Rommel Declines*

*Joseph Yee, AIA, Chair  
Panama Bartholomy  
(Vacant)  
(Vacant)*

## CITY STAFF:

*Greg Bitter, Principal Planner  
Sheryl Patterson, Senior Deputy City Attorney*

*New City Hall  
915 I Street, 1st Floor – Council Chambers*

*March 10, 2011 – 5:30 P.M.*

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# MINUTES

**March 10, 2011**

*New City Hall  
915 I Street – 1st Floor, Council Chambers*

All items listed are heard and acted upon by the Planning Commission unless otherwise noted.

## Call to Order – 5:30 p.m.

**Roll Call – All commissioners present except Pugh. Contreraz absent after 8:10 PM.**

## Consent Calendar

All items listed under the Consent Calendar are considered and acted upon by one motion. Anyone may request that an item be removed for separate consideration.

### 1. **Approval of Minutes for February 24, 2011**

**Location:** Citywide

**Recommendation:** Approve Commission Minutes from February 24, 2011.

**Contact:** Greg Bitter, Principal Planner, 916-808-7816

**Action: Moved, seconded, and carried (Notestine/Bartholomy; 8:0:0) to approve minutes.**

## Director's Report

### 2. **Director's Report (Oral)**

**Location:** Citywide

**Recommendation: Receive and File-** Status report on pending development applications and appeals; proposed amendments to Zoning Code, design standards, and other development-related regulations; Community Development Department organizational and operational changes, work program, and training program; and similar matters.

**Contact:** Greg Bitter, Principal Planner, 916-808-7816

**Action: Received and Filed.**

## Public Hearings

Public hearings may be reordered by the Chair at the discretion of the Commission. If you challenge the decision of this Commission you may be limited to raising only those issues that are raised in this hearing or in written correspondence received by the Commission prior to the hearing.

### 3. **M09-020 Swanston Station Transit Village Specific Plan (Noticed 2/28/11)**

**Location:** The Swanston Station Transit Village Area is generally bounded by El Camino Ave on the north, Arden Way on the south, Capitol City Freeway (Business 80) on the east, and Beaumont and Erickson Streets on the west. Properties fronting on the northern side of El Camino are within the Swanston Station area, Districts 2 & 3.

**Recommendation: Forward Recommendations of Approval to City Council: Item A:** Environmental Determination: Environmental Impact Report (EIR); **Item B:** Mitigation

Monitoring Plan; **Item C:** Swanston Station Transit Village Specific Plan; **Item D:** Rezone various parcels within the Swanston Station Transit Village Specific Plan Area.

**Contact:** Fedolia “Sparky” Harris, Senior Planner, 808-2996; Jim McDonald, Senior Planner, 808-5723

**Action: Moved, seconded, and carried (Harvey/Notestine; 8:0:0) to forward recommendations of approval to City Council.**

4. **P08-100 7114 Indian Lane Subdivision** (Noticed 2/28/11)

**Location:** 7114 Indian Lane, 041-0085-003-0000, District 5

**Recommendation: Approve- Item A:** Environmental Exemption (Per CEQA 15332); **Item B:** Tentative Map to subdivide one vacant 0.97 gross acre lot into 14 petite lots for an alternative-style single-family subdivision in the Residential Mixed-use-Transit Overlay (RMX-TO) zone; **Item C:** Special Permit-Residential to develop alternative-style detached single family homes on petite lots with private streets.

**Contact:** Kimberly Kaufmann-Brisby, Associate Planner, 916-808-5590; Lindsey Alagozian, Senior Planner, 916-808-2659

**Public comment made by Larry Cottle.**

**Substitute Motion: Moved, seconded (Notestine/Contreraz; 5:3:0, No- Harvey, Mendez, Yee) to continue project to March 24, 2011. Motion failed.**

**Action: Moved, seconded, and carried (Mendez/Harvey; 8:0:0) to approve staff recommendation and add a condition of approval regarding a good neighbor policy and direct staff to organize a meeting between the applicant and the neighbor for purposes of addressing access issues and concerns regarding the project.**

5. **P09-006 Provence** (Noticed 2/28/11)

**Location:** Area southwest of East Commerce Way and Benefit Way, 225-2330-002-0000 through 225-2330-076-0000, 225-2680-002-0000 through 225-2680-083-0000, 225-2690-002-0000 through 225-2690-069-0000, District 1

**Recommendation: Forward Recommendations of Approval to City Council: Item A:** A Mitigated Negative Declaration; **Item B:** Mitigation Monitoring Plan; **Item C:** Planned Unit Development Schematic Plan Amendment to re-designate use and density on 13.9 gross acres; **Item D:** Tentative Map to subdivide 182 residential lots and 38 common lots into 28 residential condominium lots, 5 private street lots, 16 private drive lots and one landscape lot; **Item E:** Special Permit to develop 233 condominium units; **Item F:** Special Permit-Major Modification to modify previous approval from 182 townhouse units to 233 condominium units.

**Contact:** David Hung, Associate Planner, 916-808-5530; Lindsey Alagozian, Senior Planner, 916-808-2659

**Action: Moved, seconded, and carried (Notestine/Harvey; 8:0:0) to continue to March 24, 2011.**

6. **P10-078 New American Poultry Facility** (Noticed 2/28/11)  
**Location:** 8612 Younger Creek Drive, 062-0130-035-0000, District 6  
**Recommendation: Approve - Item A:** Environmental Exemption (Per CEQA 15332); **Item B:** Special Permit to relocate a poultry slaughter processing plant into the Heavy Industrial (M-2S) zone.  
**Contact:** Elise Gumm, Associate Planner, 916-808-1927, Lindsey Alagozian, Senior Planner, 916-808-2659  
**Public comment made by Rick Johnson, Loren Beebe, Chris Tellis, Nadine Nakata, Jennifer Feaning, and Janet Weeks.**  
**Motion: Moved, seconded (Bartholomy/Declines; 5:3:0, No- Contreras, Harvey, Mendez) to approve staff recommendation.**  
**The vote lacked 6 affirmative votes for Planning Commission approval. This item will be reheard by the Planning Commission.**
7. **P10-081 Long Plaza Plan Review** (Noticed 2/28/11)  
**Location:** 6451 Stockton Boulevard, 040-0021-047-0000, District 6  
**Recommendation: Approve - Item A:** Environmental Exemption (Per CEQA 15332); **Item B:** Development Plan Review-New Site Plan for a 24,930 square foot commercial complex comprising retail, medical office, and restaurant uses in the General Commercial Plan Review Broadway-Stockton Special Planning District (C-2-R-SPD) zone.  
**Contact:** Kimberly Kaufmann-Brisby, Associate Planner, 916-808-5590; Lindsey Alagozian, Senior Planner, 916-808-2659  
**Action: Moved, seconded, and carried (Harvey/Contreras; 8:0:0) to approve staff recommendation.**
8. **P10-085 The Broadway Triangle Development** (Noticed 2/28/11)  
**Location:** 3409 Broadway, 3413 Broadway, 3425 Broadway, 3436 2nd Avenue, 3434 2nd Avenue, 3535 3rd Avenue, 3519 3rd Avenue, 3501 3rd Avenue, 2751 35th Street, 2741 35th Street, 2739 35th Street. 010-0375-001-0000, 010-0375-002-0000, 010-0375-003-0000, 010-0375-004-0000, 010-0375-008-0000, 010-0381-012-0000, 010-0381-013-0000, 010-0381-014-0000, 010-0381-015-0000, 010-0381-016-0000, 010-0381-017-0000, 010-0381-018-0000, District 5  
**Recommendation: Approve - Item A:** Environmental Exemption (Per CEQA 15332); **Item B:** Tentative Map to subdivide twelve parcels into twenty-seven parcels including nineteen residential lots, five commercial lots, and three common lots; **Item C:** Subdivision Modification to allow five lots without public street access; **Item D:** Development Plan Review for development of new land locked parcels; **Item E:** Special Permit for alternative single family housing in the General Commercial (C-2-SPD); **Item F:** Special Permit for apartments outside of the Central City; **Item G:** Special Permit to reduce required parking for commercial and restaurant uses; **Item H:** Variance to reduce the maneuvering distance for driveways; **Item I:** Variance to reduce the courtyard requirements and; **Item J:** Variance to increase the height of the live/work units.

**Contact:** Matthew Sites, Associate AIA, LEED AP, Urban Design Staff, 916-808-7646; Lindsey Alagozian, Senior Planner, 916-808-2659.

**Public comment made by Joan Barden, Mark Rentz, Joany Titherington, and Kim Moen.**

**Action: Moved, seconded, and carried (Bartholomy/Notestine; 8:0:0) to approve staff recommendation.**

9. **P10-086 ChaCha's Doggy Daycare** (Noticed 2/28/11)  
**Location:** 910 57<sup>th</sup> Street, 008-0122-003-0000, District 3

**Recommendation: Approve - Item A:** Environmental Exemption (Per CEQA 15301); **Item B:** Special Permit to operate a dog kennel in the Heavy Commercial (C-4) zone; **Item C:** Special Permit to waive required parking for a kennel; **Item D:** Special Permit to allow tandem parking; **Item E:** Variance to waive the masonry wall requirement; **Item F:** Variance to deviate from parking development standards.

**Contact:** Evan Compton, Associate Planner, 916-808-5260, Stacia Cosgrove, Senior Planner, 916-808-7110

**Action: Moved, seconded, and carried (Mendez/Contreras; 8:0:0) to approve staff recommendation.**

10. **P10-088 1600 H Parking Waiver** (Noticed 2/28/11)  
**Location:** 1600 H Street, 006-0063-020-0048, District 3

**Recommendation: Approve - Item A:** Environmental Exemption (Per CEQA 15332); **Item B:** Special Permit-Parking to waive a portion of the required parking for an existing residential mixed use project.

**Contact:** Elise Gumm, Associate Planner, 916-808-1927, Stacia Cosgrove, Senior Planner, 916-808-7710

**Action: Moved, seconded, and carried (Harvey/Notestine; 8:0:0) to approve staff recommendation.**

11. **P10-089 Courtyard Condominiums** (Noticed 2/28/11)  
**Location:** Northwest corner of T Street and 24th Street, 010-0036-011-0000, 010-0036-012-0000, District 4

**Recommendation: Approve - Item A:** Environmental Exemption (Per CEQA 15332); **Item B:** Tentative Map to create one condominium lot; **Item C:** Special Permit to construct 6 condominium units; **Item D:** Special Permit-Parking Reduction to reduce parking for multi-family development; **Item E:** Special Permit to reduce street side setback for an accessory structure; **Item F:** Variance to waive a trash enclosure.

**Contact:** David Hung, Associate Planner, 916-808-5530; Sandra Yope, Senior Planner, 916-808-7158

**Public comments made by William Burg, Steven Brigham, Heather Scott, Bill Robertson, Heather Scott, Bridget Whitted, Al Moncada, Joseph Hurley, Teri Duarte, Rick Bettis, Jeffery Rosenhall, Jon Ellison, Paul Menard, Doug Morrow, Owen Howlett, Merle Serlin, Lorena Beightler, Pamela Wade, Kay Knepperath,**

Alex Zabelin, Michael McKeever, Christina Jewett, Alan Lofaso, Alex Kelter, Carrie Camarena, Pat Mulby, Earl Withycombe, Linda McNamara, Olga Mandrusson, Michael Trostel, John Hagar, and Chas Steffan.

**Motion: Moved, seconded, (Notestine/Bartholomy) to approve staff recommendation.**

**Substitute motion: Moved, seconded, and carried (Molander/Mendez; 7:0:1) to continue to March 24, 2011.**

12. **P10-092 El Dorado Savings Bank Signage** (Noticed 2/28/11)  
**Location:** 5500 Folsom Boulevard, 008-0444-001-0000, District 3

**Recommendation: Approve - Item A:** Environmental Exemption (Per CEQA 15311); **Item B:** Variance to allow a detached monument sign to exceed the maximum size allowed in the Office (OB) zone; **Item C:** Variance to allow two attached signs to exceed the maximum size allowed in the OB zone.

**Contact:** Evan Compton, Associate Planner, 916-808-5260, Stacia Cosgrove, Senior Planner, 916-808-7110

**Public comment made by Julie Dixon.**

**Action: Moved, seconded, and carried (Harvey/Molander; 7:1:0, No- Notestine) to approve staff recommendation with the condition to use a non- internally illuminated sign on the west side of the building and external sign lighting turned off at midnight.**

### Staff Reports

Staff reports include oral presentations including those recommending Receive and File.

13. **P11-009 800 Block** (Noticed 2/18/11)  
**Location:** 800 K Street, 802 K Street, 806 K Street, 812 K Street, 1115 8th Street, 809 L Street, 815 L Street, 006-0098-003-0000, 006-0098-004-0000, 006-0098-006-0000, 006-0098-007-0000, 006-0098-008-0000, 006-0098-014-0000, 006-0098-021-0000, 006-0098-022-0000, 006-0098-024-0000, District 1

**Recommendation: Review and Comment –**A request to develop a 190,000 square foot mixed use development at the southeast corner of 8th and K Streets, consisting of 134 residential units and ground floor retail, and to develop a 98,000 square foot mixed use development at the northeast corner of 8th and L Streets, consisting of 66 residential unit, rehabilitation of the existing landmark Bel Vue building, and ground floor retail.

**Contact:** Matthew Sites, Associate AIA, LEED AP, Urban Design Staff, 916-808-7646; Stacia Cosgrove, Senior Planner, 916-808-7110

**Action: Project reviewed and comments provided.**

### Public Comments- Matters Not on the Agenda

14. **None.**

## Questions, Ideas and Announcements of Commission Members

15. Chair Yee noted the next Planning Commission meeting will be March 24, 2011. Greg Bitter mentioned the projects that will be heard at that meeting.
16. Chair Yee also reminded commissioners that their form 700, Conflict of Interest Statement is due to the City Clerk by April 1<sup>st</sup>.

Adjournment – 11:50 PM

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# Oral Report

For

## City of Sacramento

Planning Commission

### Agenda Packet

**For the Special Meeting of: March 24, 2011**

**Title:** Director's Report - **Receive and File-** Status report on pending development applications and appeals; proposed amendments to Zoning Code, design standards, and other development-related regulations; Community Development Department organizational and operational changes, work program, and training program; and similar matters.

Contact Information: Tom Pace, Principal Planner, 916-808-6848

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# REPORT TO PLANNING COMMISSION City of Sacramento

# 3

915 I Street, Sacramento, CA 95814-2671

**PUBLIC HEARING**  
**March 24, 2011**

To: Members of the Planning Commission

**Subject: Provence (P09-006)**

A request to develop 233 condominium units on approximately 13.9 gross acres (10.8 net acres) in the Employment Center 50 Planned Unit Development (EC-50-PUD) zone in the Natomas Crossing PUD.

- A. Environmental Determination: Mitigated Negative Declaration;
- B. Mitigation Monitoring Plan;
- C. PUD Schematic Plan Amendment to re-designate the use and density on 10.8 net acres in the Natomas Crossing PUD;
- D. Tentative Map to subdivide 182 residential lots and 38 common lots into 28 residential condominium lots, 5 private street lots, 16 private drive lots and one landscape lot on 10.8 net acres;
- E. Special Permit to develop 233 condominium units in the Employment Center 50 Planned Unit Development (EC-50-PUD) zone; and
- F. Special Permit Major Modification to amend previous approval for 187 townhouse units per P06-194.

**Location/Council District:**

Area southwest of East Commerce Way and Benefit Way, Sacramento, CA

Assessor's Parcel Number 225-2330-002-0000 through 225-2330-076-0000, 225-2680-002-0000 through 225-2680-083-0000, and 225-2690-002-0000 through 225-2690-069-0000

Council District 1

**Recommendation:** Staff recommends the Commission approve the request based on the findings and subject to the conditions listed in Attachment 1. **At the writing of this report, the project is non-controversial; this project was continued from the March 10<sup>th</sup> Planning Commission meeting.** Staff recommends the Commission forward to City Council a recommendation of approval for items A to F, and to approve a Private Recreational Facilities Agreement.

**Contact:** David Hung, Associate Planner, (916) 808-5530; Lindsey Alagozian, Senior Planner, (916) 808-2659

**Applicant:** RC Natomas, LLC c/o Kevin Smith, (916) 838-6651, 2225 Third Street, Suite 113, Sacramento, CA 95616

**Owner:** RC Natomas, LLC. List of owners as follows:

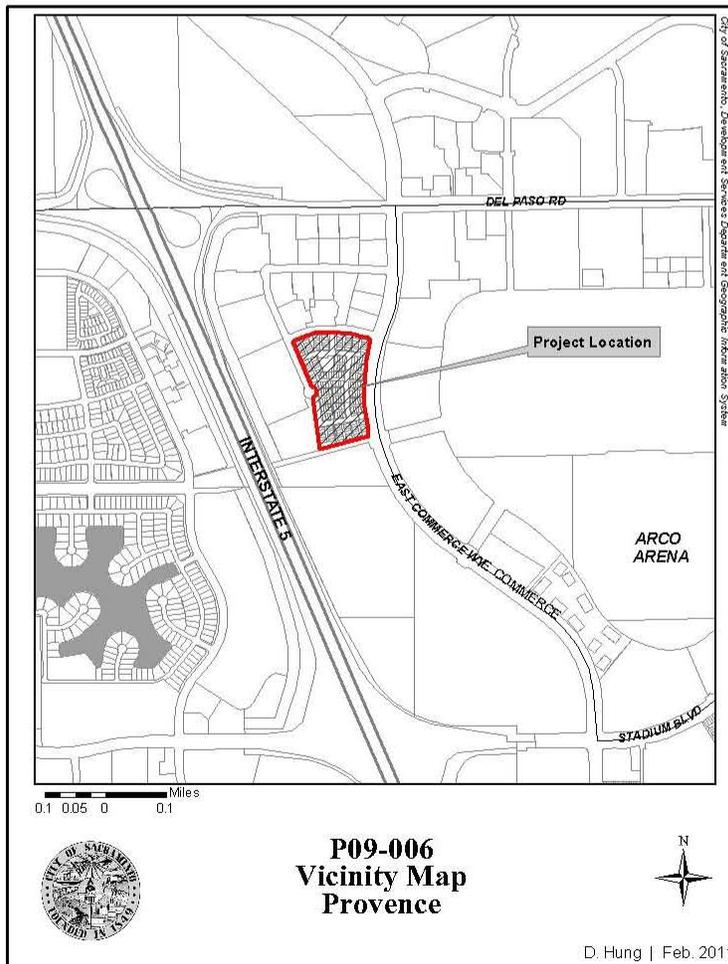
Aries Capital Partners c/o Richard Durham, Jason Reading and Andrew Dent, Salt Lake City, UT

Ranch Capital c/o Lawrence S. Hershfield, Randall Jenson and Dustin Gillman, (858) 523-1799, San Diego, CA

RRDC c/o E. James Murar, Rob Murar and Patrick Brown, (949) 533-0627, 4060 Campus Drive, Suite 100, Newport Beach, CA 92660

Paragon c/o Kevin Smith, (916) 838-6651, 2225 Third Street, Suite 113, Sacramento, CA 95616

Vicinity Map



**Summary:** Entitlements were previously approved to develop a 187 unit townhouse complex at the subject site (P06-194) within the Natomas Crossing – Area #3 Planned Unit Development (PUD). The site is currently developed with five of the 187 units, along with interior streets and the pool and pool house. The current application request is for the necessary entitlements to develop 233 condominium units within the undeveloped areas of the site. The project requires a PUD Schematic Plan Amendment, a Tentative Map, a Special Permit for condominiums and a Special Permit Major Modification to amend the previous approval. Staff notified all property owners within 500 feet of the site for this public hearing and received no opposition at the writing of this report.

<b>Table 1: Project Information</b>
<b>General Plan designation:</b> Employment Center Mid Rise (18 to 60 units per acre)
<b>PUD designation:</b> Townhouse
<b>Existing zoning of site:</b> EC-50-PUD
<b>Existing use of site:</b> Residential (partially developed)
<b>Property area:</b> 13.9 gross acres (10.8 net acres)

**Background Information:** On June 24, 1997, the City Council approved a Development Agreement and Rezone (P96-084) to designate this area as the Natomas Crossing - Alleghany #3 Planned Unit Development (PUD). Also approved were development guidelines and a schematic plan for the PUD. On May 8, 1997, the Planning Commission approved a Tentative Master Parcel Map for the site (P96-084). The current project site is a small portion of the Natomas Crossing – Alleghany Area #3 Planned Unit Development area.

On June 25, 2002, the City Council approved a General Plan Amendment, Community Plan Amendment, and a Rezone (P01-028) to re-configure the land use designations for the overall Natomas Crossing – Alleghany Area #3 PUD area. Also approved were PUD development guidelines and schematic plan amendments. On June 6, 2002, the Planning Commission approved a Tentative Map, Subdivision Modifications, and a Special Permit for an office building (for a parcel south of this location).

On January 17, 2006, the City Council approved a PUD Schematic Plan Amendment (P05-079) to re-designate this site for multi-family development in the Natomas Crossing – Alleghany Area #3 Planned Unit Development (PUD). On October 13, 2006, the Planning Commission approved a Tentative Subdivision Map to create one 10.9± net acre condominium parcel and a PUD Special Permit to develop a 187 unit condominium complex in the Natomas Crossing – Area #3 PUD.

On February 22, 2007, the Planning Commission approved the entitlement to revoke the Special Permit to develop the 187 unit condominium complex and approved a Tentative Map, Subdivision Modification and Special Permit to develop a 187 unit townhouse complex (P06-194). The site is currently developed with five of the 187 units, along with interior streets and the pool and pool house.

**Public/Neighborhood Outreach and Comments:** The project was routed to the following neighborhood advisory groups: Heritage Park HOA; Natomas Community Association (NCA); North Natomas Alliance (NNA); North Natomas Community Association (NNCA); Natomas Park Master Association (NPMA); Regency Park Neighborhood Association (RPNA); SABA; Terrace Park Neighborhood Association; WALKSacramento; and Witter Ranch Neighborhood Association. No comments were received. WALKSacramento has provided comments for the project and is attached to the staff report; in response to the comments, staff is conditioning that the final landscape and circulation plans be submitted to Planning Division for review and comment prior to submittal for building permits.

**Environmental Considerations:** The City of Sacramento prepared a Mitigated Negative Declaration (MND) for the Provence project. In accordance with the California Environmental Quality Act (CEQA), the MND was submitted to a 20-day public review period from May 8, 2009 through May 28, 2009. The Notice of Availability was advertised in a newspaper of general circulation, posted with the Sacramento County Clerk and sent to stakeholders in the project area. No comments were received during circulation for public comment.

The following sections were identified to have potentially significant impacts: Hydrology and Noise. Mitigation measures were incorporated into the project description to reduce these impacts to a less-than-significant level, consistent with CEQA Guidelines Section 15070.

Following preparation of the MND and circulation for public comment, the project was revised to reduce the dwelling unit count from 237 to 233, along with some minor alterations in layout. The reduction in dwelling unit count would have no effects that were not identified and evaluated in the MND, and no changes to the environmental document are required. Recirculation of the MND is not required pursuant to CEQA Guidelines Section 15073.5.

The MND is currently posted on the Community Development Department's web site at: <http://www.cityofsacramento.org/dsd/planning/environmental-review/eirs/>

## **Policy Considerations:**

### ***General Plan***

The 2030 General Plan Update was adopted by City Council on March 3, 2009. The 2030 General Plan's goals, policies, and implementation programs define a roadmap to achieving Sacramento's vision to be the most livable city in America. The 2030 General Plan Update designation of the subject site is Employment Center Mid Rise which provides for large mixed-use office/employment centers that includes residential uses as a supportive mixed use or adjacent to large employment center; this designation allows a minimum density of 18 units per net acre to a maximum of 60 units

per net acre. The 2030 General Plan has identified goals and policies under the Land Use and Urban Design Element as well as the Housing Element. Some of the goals and policies supported by this project are:

- *Citywide Land Use and Urban Design. (Policy LU 7.1.2) Housing in Employment Centers.* The City shall require compatible integration of housing in existing and proposed employment centers to help meet housing needs and reduce vehicle trips and commute times, where such development will not compromise the City's ability to attract and maintain employment-generating uses.
- *Housing Element. Housing Diversity (Goal H-1.2)* Provide a variety of quality housing types to encourage neighborhood stability.
- *Housing Element. Balanced Communities (Goal H-1.3)* Promote racial, economic, and demographic integration in new and existing neighborhoods

The project, with a density of 21 units per net acre, is within the range of 18 to 60 units per net acre of the General Plan land use designation. The proposed project meets the 2030 General Plan goals and policies related to Citywide Land Use and Urban Design and the development of Employment Center Mid Rise.

### ***North Natomas Community Plan***

The policies contained in the North Natomas Community Plan, found within Part 3 of the 2030 General Plan, are organized to mirror the structure of the citywide General Plan elements and are intended to supplement, but not repeat, citywide policies. Staff believes that the proposed project to develop 233 condominium units does not contradict nor interfere with the identified policies in the community plan.

### ***Employment Center Zoning***

The development of a multi-family residential use in the employment center zone is considered a non-primary use ancillary to the primary use intended for the greater area of the employment center zoned parcels within a Planned Unit Development. A maximum of twenty-five percent (25%) of the PUD net acreage may be designated for and devoted to residential uses. The proposed project will not exceed the 25% residential threshold in the Natomas Crossing PUD. The Zoning Code allows an entitlement process to exceed the maximum allowable area of 25% residential use within a PUD if it exists within the specific area delineated as being bounded by the East Drain, I-5, Del Paso Road and Arena Boulevard. This geographic area contains approximately 340 net acres. The Zoning Code allows 25% of the defined area to be residential which equates to approximately 85 net acres residential use. The proposed project in combination with existing multi-family residential development will total approximately 72.28 net acres of residential development leaving a remainder of approximately 12.72 acres available for residential use, provided the appropriate findings can be made.

Within the defined geographic area which allows up to 25% residential use, the other approved or constructed multi-family complexes include the Bella Rose Condominiums, the Ashton Parc Apartments, the Tuscaro Apartments, Arena Seniors, and the Fairfield Apartments. With the previously approved project on the subject site, the total amount of acreage devoted to residential uses within the defined geographic area is 72.28 acres and thus will not exceed the maximum 25%, or 85 acres, as shown on the following table.

Item	Area/Project		Net Acres	Description
1	Natomas Crossing PUD	(EC-50, R-2B)-PUD	<16.9 na>	Fairfield Apartments (P01-014)
2	Natomas Crossing PUD	EC-50-PUD	<10.9 na>	Provence (P06-194)
3	Arena Corporate Center PUD	EC-40-PUD	<10.85 na>	Bella Rose Condominiums (P03-162)
4	Arena Corporate Center PUD	EC-40-PUD, AOS	<16.9 na>	Tuscaro Apartments (P98-042)
5	Arena Corporate Center PUD	EC-40-PUD	<8.23 na>	Ashton Parc Apartments (P04-240)
6	Arena Corporate Center PUD	EC-40-PUD, EC-80-PUD	<8.5 na>	Arena Seniors (P08-013)
	<b>Total Residential (in defined geographic area)</b>		<b>72.28 na</b>	
	<b>Total Net Acreage in defined area</b>		<b>340 net acres</b>	<b>Allowable acres for residential if criteria met = 85 na</b>
	<b>% Residential in Defined Area</b>		<b>21.3%</b>	

The following table shows a list of multi-family housing projects in the North Natomas Community Plan area, including apartments and condominiums:

Location	Project Name	Type	Units	
<b>West of Interstate 5</b>	Lofts (P02-084)	Apartment	188 (app'd)	
	Atrium Court (P02-035)	Apartment	224 (app'd)	
	Irongate (P98-071)	Apartment	280 (app'd)	
	Terracina Meadows (P01-050)	Apartment	148 (app'd)	
	Hurley Creek (P06-007)	Apartment	208 (app'd)	
	Valencia Point (P05-212)	Apartment	168 (app'd)	
	Cambay West (P03-047)	Apartment	216 (app'd)	
	Duckhorn Village (P06-201)	Apartment	75 (app'd)	
	Vista del Lago (P06-093)	Condominium	219 (app'd)	
	Westlake Villas (P01-053)	Condominium	285 (app'd)	
	Brias del Lago (P06-139)	Townhouses	126 (app'd)	
		Subtotal:		2137

<b>East of Interstate 5</b>	Homecoming (P01-115)	Apartment	450 (app'd)
	Bella Rose (P03-162)	Condominium	201 (app'd)
	Ashton Parc (P04-240)	Apartment	168 (app'd)
	Villagio (P99-059)	Apartment	272 (app'd)
	Creekside Crossing (P05-177)	Condominium/ Townhouses	434 (app'd)
	Tuscaro (P98-042)	Apartment	296 (app'd)
	Granite Pointe (P01-014)	Apartment	384 (app'd)
	Terracina Gold (P99-142)	Apartment	280 (app'd)
	Natomas Field (P04-236)	Townhouses	211 (app'd)
	JMA/Laing Condos (P05-164)	Condominium	92 (app'd)
	JMA/St. Anton (P05-136)	Apartment	108 (app'd)
	McKenzie (P01-016)	Apartment	152 (app'd)
	Miramonte/Trovass (P99-082)	Apartment	440 (app'd)
	Carriage Lane I (P03-085)	Condominium	156 (app'd)
	Carriage Lane II (P04-167)	Condominium	39 (app'd)
	Provence (P06-194)	Townhouses	187 (app'd)
	Syrah (P02-132)	Condominium	245 (app'd)
	Amara (P04-087)	Condominium	200 (app'd)
	Natomas Park (P01-100)	Apartment	212 (app'd)
	Broadstone (P04-096)	Condominium	142 (app'd)
	Regency Park (P04-065)	Condominium	135 (app'd)
	Carefree (P00-005)	Apartment	500 (app'd)
	Northpointe (P03-046)	Apartment	180 (app'd)
	Terraces (P04-196)	Condominium	321 (app'd)
	Natomas Place (P06-124)	Apartment	135 (app'd)
	Natomas Market Rate (P08-047)	Condominium	120 (app'd)
	Vintage at Natomas Field (P05-116)	Apartment	200 (app'd)
	Hampton Village (P04-058)	Condominium	264 (app'd)
	Heritage Point (P07-035)	Condominium	229 (app'd)
	Arena Seniors (P08-013)	Apartment	240 (app'd)
		Subtotal:	6,993
<b>Total Multi-Family Units in North Natomas:</b>			<b>9,130</b>

There are a total of 9,130 approved multi-family dwelling units in the North Natomas Community Plan area as of February 2011. The project, if approved, will add an additional 51 units of multi-family housing.

The proposed condominium use is compatible with the adjacent/proposed future uses within the PUD, and the site can be adequately served by public facilities, transit and open space. This project is a high density development and has 22 dwelling units per net acre; however, circulation is eased by the fact that the development is bordered by three streets and allows easy access to the surrounding area. In conclusion, staff supports the project and finds the proposed project is in compliance with the goals and policies of the North Natomas Community Plan.

***Natomas Basin Habitat Conservation Plan (NBHCP):***

The 1994 North Natomas Community Plan required the development and implementation of a Habitat Conservation Plan as mitigation for development in North Natomas. In 1997, the NBHCP was approved by the City of Sacramento, USFWS, and CDFG.

The NBHCP is a conservation plan supporting application for incidental take permits (ITP's) under Section 10(a)(1)(B) of the Endangered Species Act and under Section 2081 of the California Fish and Game Code. The purpose of the NBHCP is to promote biological conservation while allowing urban development and continuation of agriculture within the Natomas Basin. The NBHCP establishes a multi-species conservation program to mitigate the expected loss of habitat values and incidental take of protected species that would result from urban development, operation of irrigation and drainage systems, and rice farming. The goal of the NBHCP is to preserve, restore, and enhance habitat values found in the Natomas Basin.

To support the issuance of an ITP, an Environmental Assessment was prepared by the USFWS for the National Environmental Policy Act requirement and a Negative Declaration was prepared by the City of Sacramento for the California Environmental Quality Act (CEQA) requirement. The USFWS and CDFG issued ITP's to the City of Sacramento. The NBHCP and ITP were subsequently challenged, and on August 15, 2000, the United States District Court, Eastern District, ruled that the ITP was invalid and an EIS was required for the project. Based on this ruling, the City of Sacramento and Sutter County jointly prepared a revised NBHCP and an Environmental Impact Report/Environmental Impact Statement (EIR/EIS) for use by the USFWS and CDFG. The USFWS is the lead federal agency for the preparation of the EIS and the City of Sacramento and Sutter County are co-lead agencies for the preparation of the EIR. The Sacramento City Council adopted the revised NBHCP and EIR/EIS on May 13, 2003. On June 27, 2003 the USFWS issued a new Incidental Take Permit for the NBHCP for development within the Natomas Basin. This project is subject to the requirements of the revised HCP/ITP. HCP fees have been paid and the site has been graded, thereby complying with the requirements of the HCP/ITP.

### ***Smart Growth Planning Principles:***

“Smart Growth” is a term coined by the United States Environmental Protection Agency (USEPA) as an umbrella term for the many initiatives intended to address some of the negative consequences of urban sprawl. Smart Growth generally occurs when development patterns are sustainable and balanced in terms of economic objective, social goals, and use of environmental/natural resources. The following Smart Growth principles apply to the proposed project:

- Higher-density, cluster development.
- Multi-modal transportation and land use patterns that support walking, cycling and public transit.
- Streets designed to accommodate a variety of activities.
- Planned and coordinated projects between jurisdictions and stakeholders.

The proposed project has been designed to incorporate many of the Smart Growth Principles listed above.

***Mixed Income Inclusionary Housing Ordinance:***

The project is allowed in the EC zone due to amendments to the Community Plan (Resolution 2002-047) and the EC Zone (Ordinance 2002-001) adopted in 2002. These amendments allow for the residential component of EC development within the geographic area bounded by the East Drain, Interstate 5, Del Paso Road and Arena Boulevard to exceed 25% within the individual PUD with the approval of a special permit; the subject site is within the above referenced geographic area.

Per section 17.190.070(E) of the Zoning Ordinance, any residential project in the North Natomas community plan area which is the subject of a development agreement executed on or before June 20, 2000 shall be exempted from the inclusionary housing component, unless subsequent to June 20, 2000 the residential project requires the approval of one or more legislative entitlements or amendments to legislative entitlements which are major rather than minor, in which case the residential project shall not be exempt from inclusion of the inclusionary housing component. Per section 17.190.020 of the Zoning Ordinance, "minor legislative entitlements" means legislative entitlements or amendments to legislative entitlements which satisfy one or more of the following:

1. Entitlements that do not result in any of the following criteria as defined by the North Natomas community plan target average densities: a net loss of residential acreage; a net loss of acreage of land designated for high density residential (HDR) or medium density residential (MDR) development, unless the HDR total residential units replace the loss of MDR residential units; or a net loss of total residential units;
2. Entitlements that are the result of, and required by, amendments to public facilities or roadways designated in the North Natomas community plan; provided further that the entitlements are limited to addressing the amendments required by the city or other public agency; or
3. Entitlements that are limited to amendments to a previously approved PUD schematic plan, tentative map, or PUD development guidelines, provided that the amendments do not result in a loss of more than five (5) percent between the density of the proposed project and the density of the previously approved project.

The project site was under a Development Agreement approved prior to June 20, 2000. Staff has found that the 2002 amendments to the Community Plan and the EC Zone as well as the required PUD Schematic Plan Amendment for the project fall within the definition of a minor legislative entitlement per the criteria discussed above; the project does not result in a net loss of residential acreage and the entitlements are limited to amendments to a previously approved PUD schematic plan, Plan Review and Special Permits. As a result, the project is exempt from the Mixed Income Housing Ordinance under section 17.190.170.

***Multi-Family Residential Design Guidelines:***

The Multi-Family Residential Design Guidelines were approved by the City Council on August 5, 2000 (Resolution CC2000-487). This document articulates design principles for multi-family residences to assist the Planning Commission, City Council, City staff and project planners and designers by identifying the City's design criteria for multi-family development. The intent is to achieve well-designed projects to enhance the community's overall value and appearance. The project is generally consistent with the Multi-Family Residential Design Guidelines as identified in the building design section of this staff report.

### **Project Design:**

The following discusses project in relation to the PUD Schematic Plan, the Tentative Map, the Special Permit for condominiums and the Special Permit Major Modification to amend the previous approval.

### **Land Use**

#### ***PUD Schematic Plan Amendment***

The applicant is proposing a PUD Schematic Plan Amendment to designate a 233-unit condominium complex on 10.8 net acres within the Natomas Crossing PUD. Applicants wishing to obtain entitlements in order to proceed to the construction phase of development are required to submit a Schematic Plan along with other drawings delineating the anticipated developments proposed in the near future for a site. The existing Schematic Plan depicts the site with 187 townhouse units. The proposed Schematic Plan Amendment delineates the use and density of the project under consideration. Since the PUD Schematic Plan Amendment reflects an increase in density of more than 10% of the existing condition, the project requires approval of the City Council.

<b>Project Designation</b>	<b>Proposed Land Use Designation</b>	<b>Gross Acres</b>	<b>Net Acres</b>	<b>Units</b>	<b>Buildings</b>	<b>Density</b>
Carriage Lane 3	Residential (Townhouses)	13.9	10.9	188	38	17.2 units per net acre

<b>Project Designation</b>	<b>Proposed Land Use Designation</b>	<b>Gross Acres</b>	<b>Net Acres</b>	<b>Units</b>	<b>Buildings</b>	<b>Density</b>
Provence (P09-006)	Residential (Condos)	13.9	10.8	233	134	22 units per net acre
Provence (P06-194)	Residential (Townhouses)		0.2	5	5	

The development of a multi-family residential in the employment center zone is considered a non-primary use ancillary to the primary use intended for the greater area of the employment center zoned parcels within a Planned Unit Development. Staff finds that the PUD Schematic Plan Amendment conforms to policies of the General Plan and North Natomas Community Plan to provide adequate housing sites and opportunities for all households and to promote efficient development within a new growth area. Furthermore, the PUD Schematic Plan Amendment will not be injurious to the public welfare, nor to other properties in the vicinity of the development in that the project is compatible with adjacent developments and the site will be developed according to the requirements of the PUD Guidelines.

### ***Tentative Map design***

Map Design: The tentative map proposes to subdivide 182 residential lots and 38 common lots (per approval of file P06-194) into 28 residential condominium lots, 5 private street lots, 16 private drive lots and one landscape lot on 10.8 net acres; the tentative map design is summarized below:

<b>Lot Number:</b>	<b>Total Net Acreage:</b>	<b>Use:</b>
1 - 28	7.7	Condominium Units
A – P	1.0	Private Drives
Q - U	1.8	Private Streets
V	0.3	Landscape Lot

The project creates condominium parcels which consist of an undivided interest in common in a portion of real property coupled with a separate interest in space called a unit, the boundaries of which are described on a recorded final map, parcel map, or condominium plan in sufficient detail to locate all boundaries thereof. [Civil Code Section 1350 (f)].

Vehicular Circulation and Parking: Access to the project site is provided at Benefit Way to the north and Advantage Court to the west. The main vehicular access is the northern driveway and secondary vehicular access is provided to the west. The units along Advantage Court provide direct pedestrian access to the sidewalks along this street. Pedestrian connections are also provided from the interior of the site to the sidewalks along Benefit Way and East Commerce Way.

Pedestrian Circulation: Existing sidewalk and rolled curb are found at the frontage on East Commerce Way, Benefit Way and Advantage Court. The project does not impact or change existing circulation in and around the site.

Walls and Fencing: Existing fencing at the pool area will remain. New fencing is to be installed all along the southern perimeter of the site. Fencing abutting the vacant adjacent parcel to the west shall be provided when the site is developed. As required by the Fire Department, a gate will be installed at the emergency vehicle access entrance at East Commerce Way.

On December 15, 2010, the Subdivision Review Committee, with all ayes, voted to recommend approval of the proposed Tentative Map, subject to the conditions of approval as found in Attachment 1.

In evaluating tentative maps, the Commission is required to make the following findings:

- A. None of the conditions described in Government Code Section 66474, subsection (a) through (g), inclusive, exist with respect to the proposed subdivision;
- B. The proposed subdivision, together with the provisions for its design and improvement, is consistent with the City General Plan and Title 16 Subdivisions of the City Code, which is a specific plan of the City (Gov. Code §66473.5);
- C. The discharge of waste from the proposed subdivision into the existing community sewer system will not result in a violation of the applicable waste discharge requirements prescribed by the California Regional Water Quality Board, Central Valley Region, in that existing treatment plants have a design capacity adequate to service the proposed subdivision (Gov. code §66474.6);
- D. The design of the proposed subdivision provides, to the extent feasible, for future passive or natural heating and cooling opportunities (Gov. Code §66473.1);
- E. The Planning Commission has considered the effect of the approval of this tentative subdivision map on the housing needs of the region and has balanced these needs against the public service needs of its residents and available fiscal and environmental resources (Gov. Code §66412.3).

Staff finds that the Tentative Map is consistent with the policies of the General Plan and Title 16 of the City Code. The site is physically suitable for the type of development proposed and suited for the proposed density; the design of the subdivision and the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife their habitat, and the design of the subdivision and the type of improvements will not conflict with easements, acquired by the public at large, for access through or use, of, property within the proposed subdivision. The project will not overly burden the sewer system, nor will it preclude future passive or natural heating and cooling opportunities.

The Carriage Lane III development project, previously approved for this site, has a Private Recreational Facilities Agreement (City Agreement 2006-0298). The Agreement allowed 5% parkland dedication credit in exchange for the development of a private pool for the use of all residents in the 187 unit Carriage Lane III subdivision. The Agreement was specific to the Carriage Lane III project and is not transferable to this project. The Applicant for the Provence project has requested a new Private Recreational Facilities Agreement. The new Provence Private Recreational Facilities Agreement will be for the development of a private pool for the use of all residents in the 233 unit development, plus the five existing model homes from the Carriage Lane III project. Pursuant to Section 16.64.120 of the Sacramento City Code and at the time of the hearing on the tentative subdivision map, the Planning Commission shall make a recommendation concerning the request for a Private Recreational Facilities Agreement to the City Council. Staff supports a new Private Recreational Facilities Agreement, with the remainder of the parkland dedication obligation to be fulfilled through the payment of in lieu fees.

### ***Special Permit***

The applicant proposes to develop 223 condominium units on approximately 10.8 net acres in the Employment Center Planned Unit Development (EC-50-PUD) zone. Section 17.192 of the Zoning Code permits new condominium developments with the issuance of a special permit. In evaluating special permit proposals of this type, the Planning Commission is required to make the following findings:

1. A special permit shall be granted upon sound principles of land use.  
Staff finds that the proposed development is an appropriate land use that will have positive contribution to the surrounding area, in that the project site is in close proximity to future commercial and open space uses and that the site will be well served by auto, bicycle, and pedestrian linkages.
2. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.  
Staff finds that the proposed condominium development site and building design are consistent with the Natomas Crossing PUD Guidelines and the Multi-Family Residential Design Principles and will not be detrimental to public health, safety or welfare.
3. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.  
The proposed project is consistent with the goals and policies of the employment center designation in the General Plan and the North Natomas Community Plan.

Staff believes that the current proposal is a well designed project in that it provides various housing types and different architectural variations, and it provides usable private and public outdoor spaces for the residents. The proposed project should have a positive contribution to the surrounding area.

### Access, Circulation and Parking

Vehicular access to the project site is provided through three driveways: one off of Benefit Way and two off of Advantage Court. Additional pedestrian connections are provided from the units to the sidewalks along Advantage Court, Benefit Way, and East Commerce Way.

<b>Use</b>	<b>Required Parking</b>	<b>Proposed Parking</b>	<b>Difference</b>
Condominiums	233 (one per unit)	410	+177

Two types of parking will be available at this complex: garage spaces within the individual units and open spaces for residents and guests. The parking areas comply with the Multi-Family Residential Design Guidelines in that: the majority of the surfaced parking areas are located away from the adjacent public roadways; parking areas are generally located away from street corners; and landscaping and walkways are provided between buildings and paved parking areas. According to the Sacramento City Code (Section 17.192), a minimum of 1 parking space per unit is required. Therefore, the proposal is required to provide a minimum of 233 parking spaces. The applicant is proposing 410 parking spaces on site, consisting of 371 garage spaces and 39 open spaces. It is staff's opinion that the 371 garage spaces and the 48 extra parking spaces are not excessive, in that: each unit has either a one-car or a two-car garage; minimal on-street parking is allowed on the surrounding streets; and the open parking is provided throughout the site.

The proposal is also consistent with the Natomas Crossing PUD guidelines, which state that where reasonable, locate parking lots away from the primary adjacent roadways, behind buildings, or within the buildings as structured parking. (p. 57)

### Height, Bulk and Setbacks

<b>Standard</b>	<b>Allowance</b>	<b>Proposed</b>	<b>Deviation?</b>
Height	Maximum four stories	Two to three stories	No
East Commerce Way	Minimum of 12'-6" Maximum of 15'-0"	12'-6" to 15'-0"	No
Benefit Way	Minimum of 17'-6" Maximum of 23'-0"	17'-6"	No

Advantage Court	15'-0"	15'-0"	No
Southern Property Line	15'-0"	15'-0"	No

As indicated above, the project meets or exceeds all applicable height and area requirements.

### **Building design, signage and landscaping**

The condominium buildings are three stories in height which is within the four-story maximum building height allowed in the Employment Center 50 zone. The exterior building materials will consist of integral color stucco, stone veneer, plaster corbels, plaster window trims, metal railing details, and decorative shutters. Roofing is proposed to be composite tiles. Vinyl windows, fiberglass entry doors and metal sectional garage doors are also used. The applicant is proposing two housing types, the Urban Villas and the Courts.

The Urban Villas are located at the northernmost and southernmost portions of the site. All three unit types are three stories tall and have ground floor living area adjacent to two-car garages; in some instances, unit types 1 and 2 are attached by a deck in between the buildings. All unit entries face onto a pedestrian paseo or a public street; all garages are accessed at the private drives. All building side elevations facing a street shall be enhanced with window openings, trims and decorative elements.

The Courts are located at the central portion of the site and at the East Commerce Way frontage. All building in the Courts are three stories in height and other than Plan 2/3 and 5/7, all buildings have partial living area on the ground floor. Some of the Courts buildings contain two-story elements to alleviate the massing. All units have an entry that faces onto a public street, a private street or a pedestrian paseo; all garages are accessed at the private drives. Plans 1, 2, 4, 6, 8, 9, 10 and 11 are shown with one-car garages while Plans 3, 5 and 7 are shown with two-car tandem garages. All building side elevations facing a street shall be enhanced with window openings, trims and decorative elements.

The following is a general summary of the building types:

<b>Building Type</b>	<b>Building Count</b>	<b>Unit Count</b>	<b>Maximum Height</b>	<b>Number of Floors</b>
Urban Villa 1	7	7	32'-0"	3
Urban Villa 1 & 2	17	34	32'-0"	3
Urban Villa 3	21	21	32'-0"	3
Court – Plan 1 / 4	16	32	33'-0"	3
Court – Plan 2 / 3	14	28	33'-0"	3
Court – Plan 5 / 7	31	62	35'-0"	3
Court – Plan 6 / 9	11	22	34'-0"	3

Court – Plan 8	7	7	33'-0"	3
Court – Plan 10 / 11	10	20	32'-0"	3
Total	134	233	n/a	n/a

The proposal is required to be consistent with the following Natomas Crossing PUD Guidelines:

- Front-on buildings are encouraged. Avoid soundwalls, replace with mounds and other sound absorption features. (p. 22)
- Architectural facades should provide visual interest and scale to the adjacent streets. Avoid overly monotonous facades that do not have relief, shadow, or textural changes at the pedestrian level. (p. 57)
- Provide windows that look out to the adjacent streetscapes and parking lot areas. (p. 57)
- Orient building entrances toward the adjacent streetscape and celebrate the connection between public and private uses. (p. 57)
- Buildings should be located close to the public utility easement (PUE). (p. 78)
- Residential buildings should have pedestrian access and visual orientation to the adjacent roadways and/or open space features. (p. 85)
- Residential buildings shall be oriented on the site to create interesting and safe common open space areas that promote neighborly interaction. (p. 85)

The residential buildings are consistent with the Multi-Family Residential Design Guidelines in that the buildings are arranged to provide functional public and private outdoor spaces, and pedestrian orientation is encouraged in the allocation of space, building size and placement, and open space design. The buildings provide windows and active spaces to enhance security and visual interest; a variety in architecture is provided, and the buildings vary roof form, mass, shape and material changes to create variations in plans. The applicant will be required to provide exterior lighting of residential quality and design for this proposal, consistent with the pedestrian and light poles for this proposal. All signage will be required to conform to the Natomas Crossing – Area #3 PUD Guidelines and the City's Sign Ordinance, where applicable.

A preliminary landscaping plan for the project is provided; the applicant has also provided playground shade structure and paseo concept plans. Trees have been planted between a separated sidewalk and the street along the public streets. The proposed landscaping on site will consist of shade trees and flowering ornamental trees, as well as shrubs and ground cover. The proposed landscaping is required to be consistent with the North Natomas Development Guidelines – Plant Species and the Natomas Crossing PUD Guidelines – Roadway master Plan Matrix (Table 4). The parking areas will be required to comply with the City's Tree Shading Ordinance, requiring 50 percent tree shading within 15 years.

The proposed landscaping is consistent with the Multi-Family Residential Design Guidelines, in that: the exterior site design and landscaping provide functional recreational spaces and community site amenities; the exterior spaces are designed to

enhance the overall appearance and compatibility of this development by providing privacy, buffering and daylight, and to provide a pleasant transition to the street.

### ***Special Permit Major Modification***

The previous approval on the site is for 187 townhouse units (P06-194). The site is currently developed with five of the 187 units, along with interior streets and the pool and pool house. A Special Permit Major Modification is required to amend the previous approval to allow the construction of a total of 233 condominiums on the remaining portion of the site. In evaluating special permit proposals of this type, the Commission is required to make the following findings:

1. A special permit shall be granted upon sound principles of land use.  
Staff finds that the proposed condominium development is a better designed project than the previous approval on the site; the project provides more architectural variations and both private and public outdoor spaces for the occupants.
2. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.  
The project will not be detrimental to the public health, safety or welfare since the buildings will be constructed to meet code standards. Residential developments of this type have been approved in many areas of the city. Staff finds that the proposed development is consistent with the Natomas Crossing PUD Guidelines and the Multi-Family Residential Design Principles.
3. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.  
The proposed project is consistent with the employment center and residential policies of the General Plan and the North Natomas Community Plan.

Staff believes that the current proposal is a better project in that it provides various housing types and different architectural variations, and it provides usable private and public outdoor spaces for the residents. The proposed project is also consistent with the goals and policies of the General Plan.

**Conclusion:** Staff recommends the Commission forward to City Council a recommendation of approval for the project. Staff finds: 1) the proposal's consistency with the policies of the General Plan, North Natomas Community Plan, and the Natomas Crossing – Area #3 Planned Unit Development (PUD) Guidelines and Schematic Plan; 2) the proposed use is consistent with the Employment Center zoning designation; and 3) the project contributes positively to the surrounding area.

Respectfully submitted by: David Hung  
DAVID HUNG  
Associate Planner

Approved by: Lindsey Alagozian  
LINDSEY ALAGOZIAN  
Senior Planner

Recommendation Approved:

Gregory Bitter  
GREGORY BITTER, AICP  
Principal Planner

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**Attachment 1**  
**Proposed Findings of Fact and Conditions of Approval**  
**Provence (P09-006)**  
**Area southwest of East Commerce Way and Benefit Way**

**Findings of Fact**

- A&B.** The Planning Commission has reviewed and considered the information contained in the Mitigated Negative Declaration and Mitigation Monitoring Plan in making the recommendations set forth below.
  
- C.** The Planning Commission recommends approval and forwards to the City Council the **PUD Schematic Plan Amendment** for the Project as set forth in Attachment 3.
  
- D.** The Planning Commission recommends approval and forwards to the City Council the **Project Approval** for the Project as set forth in Attachment 4.

**Attachment 2: Mitigated Negative Declaration – Findings – Draft Resolution**

**RESOLUTION NO.**

Adopted by the Sacramento City Council

**ADOPTING THE MITIGATED NEGATIVE DECLARATION AND THE MITIGATION MONITORING PROGRAM FOR THE PROVENCE PROJECT (P09-006)**

**BACKGROUND**

A. On March 10, 2011, the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve with conditions the Provence Project.

B. On April 19, 2011, the City Council conducted a public hearing, for which notice was given pursuant Sacramento City Code Section 17.200.010 (C)(2)(a, b, and c) and received and considered evidence concerning the Provence Project.

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:**

Section 1. The City Council finds as follows:

A. The Project initial study was prepared to analyze whether the Project was described in the Master EIR and whether the Project would cause any significant additional environmental effects (project-specific effects) that were not analyzed in the Master EIR for the 2030 General Plan.

B. The Initial Study concluded that the Project was described in the Master EIR, and identified mitigation in the Master EIR that would apply to the Project. The Initial Study identified mitigation measures that were incorporated to revise the project before the environmental document was released for public review pursuant to CEQA Guidelines Section 15073 in order to avoid or mitigate the identified effects to a level of insignificance. (CEQA Guidelines Section 15178(b)). As part of the Master EIR process, the City incorporated all feasible mitigation measures or feasible alternatives appropriate to the project as set forth in the Master EIR (CEQA Guidelines Section 15177(d)).

C. The above review concluded that there is no substantial evidence that the Project as revised and conditioned would have a significant effect on the environment. A Mitigated Negative Declaration (MND) for the Project was then completed, noticed and circulated in accordance with the requirements of the California Environmental Quality Act (CEQA), the State CEQA Guidelines and the Sacramento Local Environmental Procedures as follows:

1. On May 8, 2009 a Notice of Intent to Adopt the MND (NOI) dated May 8, 2009 was circulated for public comments for 20 days. The NOI was sent to those public agencies that have jurisdiction by law with respect to the proposed project and to other interested parties and agencies, including property owners within 500 feet of the boundaries of the proposed project. The comments of such persons and agencies were sought.

2. On May 8, 2009, the NOI was published in the Daily Recorder, a newspaper of general circulation, and the NOI was posted in the office of the Sacramento County Clerk.

Section 2. The City Council has reviewed and considered the information contained in the MND, including the Initial Study, the revisions and conditions incorporated into the Project, and the comments received during the public review process and the hearing on the Project. The City Council has determined that the MND constitutes an adequate, accurate, objective and complete review of the environmental effects of the proposed project.

Section 3. Based on its review of the MND and on the basis of the whole record, the City Council finds that the MND reflects the City Council's independent judgment and analysis and that there is no substantial evidence that the Project will have a significant effect on the environment.

Section 4. The City Council adopts the MND for the Project.

Section 5. Pursuant to CEQA section 21081.6 and CEQA Guidelines section 15074, and in support of its approval of the Project, the City Council adopts a Mitigation Monitoring Program to require all reasonably feasible mitigation measures be implemented by means of Project conditions, agreements, or other measures, as set forth in the Mitigation Monitoring Program.

Section 6. Upon approval of the Project, the City's Environmental Planning Services shall file or cause to be filed a Notice of Determination with the Sacramento County Clerk and, if the project requires a discretionary approval from any state agency, with the State Office of Planning and Research, pursuant to section 21152(a) of the Public Resources Code and section 15075 of the State EIR Guidelines adopted pursuant thereto.

Section 7. Pursuant to Guidelines section 15091(e), the documents and other materials that constitute the record of proceedings upon which the City Council has based its decision are located in and may be obtained from, the Office of the City Clerk at 915 I Street, Sacramento, California. The City Clerk is the custodian of records for all matters before the City Council.

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Exhibit 2A: Mitigation Monitoring Program – 6 pages

**Exhibit 2A: Mitigation Monitoring Plan**

**MITIGATION MONITORING PLAN**

FOR

PROVENCE (P09-006)

**TYPE OF ENVIRONMENTAL DOCUMENT:**  
INITIAL STUDY/ NEGATIVE DECLARATION

**PREPARED FOR:**

CITY OF SACRAMENTO, COMMUNITY DEVELOPMENT DEPARTMENT

**DATE:**

DATE

**ADOPTED BY:**  
CITY OF SACRAMENTO  
PLANNING COMMISSION

**DATE:**

\_\_\_\_\_

**ATTEST:**

\_\_\_\_\_

**Provence (P09-006)  
MITIGATION MONITORING PLAN**

This Mitigation Monitoring Plan (MMP) has been required by and prepared for the City of Sacramento Community Development Department, Environmental Planning Services, 300 Richards Boulevard, Sacramento, CA 95811, pursuant to CEQA Guidelines Section 21081.6.

**SECTION 1: PROJECT IDENTIFICATION**

**Project Name / File Number:** Provence (P09-006)

**Owner/Developer- Name:** Kevin Smith  
RC Natomas LLC

**Address:** 4060 Campus Drive, Suite 100  
Newport Beach, CA 92660  
916-838-6651

**Project Location / Legal Description of Property (if recorded):**

The proposed project site is located southwest of the intersection of East Commerce Way and Benefit Way. The proposed project is located within the North Natomas Community Plan area of the City of Sacramento. The project site includes Assessor's Parcel Number 225-2330-002 thru -076, 225-2680-002 thru -083 and 225-2690-002 thru -069.

**Project Description:**

The proposed project consists of development entitlements to construct 237 condominium units on the project site. The proposed project seeks a modification to the previous approval for 187 units of townhouse development (P06-194) of which five homes have been constructed. The proposed project requires a PUD Schematic Plan Amendment to re-designate the use and allowable density of the site, a Tentative Map, a Special Permit for alternative ownership housing and a Special Permit Modification to amend the previous approval on the site (P06-194).

**SECTION 2: GENERAL INFORMATION**

The Plan includes mitigation for Aesthetics, Cultural Resources, Hydrology and Noise. The intent of the Plan is to prescribe and enforce a means for properly and successfully implementing the mitigation measures as identified within the Initial Study for this project. Unless otherwise noted, the cost of implementing the mitigation measures as prescribed by this Plan shall be funded by the owner/developer identified above. This Mitigation Monitoring Plan (MMP) is designed to aid the City of Sacramento in its implementation and monitoring of mitigation measures adopted for the proposed project.

The mitigation measures have been taken from the Initial Study and are assigned the same number they have in the document. The MMP describes the actions that must take place to implement each mitigation measure, the timing of those actions, and the entities responsible for implementing and monitoring the actions. The developer will be responsible for fully understanding and effectively implementing the mitigation measures contained within the MMP. The City of Sacramento will be responsible for ensuring compliance.

**Provence (P09-006)  
MITIGATION MONITORING PLAN**

**MITIGATION AGREEMENT**

PROJECT NAME / FILE NUMBER: Provence (P09-006)  
OWNER/DEVELOPER/APPLICANT: Kevin Smith  
RC Natomas LLC  
Address: 4060 Campus Drive, Suite 100  
Newport Beach, CA 92660  
916-838-6651

I, <sup>RC</sup> ~~NATHAN S. BURCHALL, NATOMAS LLC~~ owner/developer/applicant), agree to amend the project application P09-006 to incorporate the attached mitigation measures as identified in the Initial Study for the project. I understand that by agreeing to these mitigation measures, all identified potentially significant environmental impacts should be reduced to below a level of significance, thereby enabling the Environmental Coordinator to prepare an Addendum to the previously approved Mitigated Negative Declaration of environmental impact for the above referenced project.

I also understand that the City of Sacramento will adopt a Mitigation Monitoring Plan (Plan) for this project. This Plan will be prepared by the Community Development Department, pursuant to the California Environmental Quality Act Guidelines Section 21081.6 and pursuant to Article III of the City's Local Administrative Procedures for the Preparation of Environmental Documents.

I acknowledge that this project, P09-006, would be subject to this Plan at the time the Plan is adopted. This Plan will establish responsibilities for the monitoring of my project by various City Departments and by other public agencies under the terms of the agreed upon mitigation measures. I understand that the mitigation measures adopted for my project may require the expenditure of owner/developer funds where necessary to comply with the provisions of said mitigation measures.

  
\_\_\_\_\_  
Signature (Owner/Developer/Applicant)

CFO  
\_\_\_\_\_  
Title

12/8/10  
\_\_\_\_\_  
Date

**Provence (P09-006)  
MITIGATION MONITORING PLAN**

Environmental Resource	Mitigation Measure	Responsible Entity	Compliance Milestone / Confirm Complete
Aesthetics	<b>Aesthetics 1:</b> Project outdoor lighting shall be oriented away from adjacent properties and shall not produce a glare or reflection on neighboring properties or adjacent streets or property.	Community Development Department	Prior to issuance of any grading or building permit, measures identified on plans shall be verified for compliance. The Community Development Department shall assure that measures are identified on construction plans and specifications and confirm compliance prior to issuance of any grading or building permit.
Cultural Resources	<b>CR-1:</b> In the event that any prehistoric subsurface archeological features or deposits, including locally darkened soil ("midden"), that could conceal cultural deposits, animal bone, obsidian and/or mortars are discovered during construction-related earth-moving activities, all work within 50 meters of the resources shall be halted, and the City shall consult with a qualified archeologist to assess the significance of the find. Archeological test excavations shall be conducted by a qualified archeologist to aid in determining the nature and integrity of the find. If the find is determined to be significant by the qualified archeologist, representatives of the City and the qualified archeologist shall coordinate to determine the appropriate course of action. All significant cultural materials recovered shall be subject to scientific analysis and professional museum curation. In addition, a report shall be prepared by the qualified archeologist according to current professional standards.	Community Development Department,  Native American Heritage Commission	Prior to issuance of any grading or building permit, measures identified on plans shall be verified for compliance. The Community Development Department shall assure that measures are identified on construction plans and specifications and confirm compliance prior to

**Provence (P09-006)  
MITIGATION MONITORING PLAN**

	<p><b>CR-2:</b> If a Native American site is discovered, the evaluation process shall include consultation with the appropriate Native American representatives.</p> <p>a. If Native American archeological, ethnographic, or spiritual resources are involved, all identification and treatment shall be conducted by qualified archeologists, who are certified by the Society of Professional Archeologists (SOPA) and/or meet the federal standards as stated in the Code of Federal Regulations (36 CFR 61), and Native American representatives, who are approved by the local Native American community as scholars of the cultural traditions.</p> <p>b. In the event that no such Native American is available, persons who represent tribal governments and/or organizations in the locale in which resources could be affected shall be consulted. If historic archeological sites are involved, all identified treatment is to be carried out by qualified historical archeologists, who shall meet either Register of Professional Archeologists (RPA), or 36 CFR 61 requirements.</p> <p><b>CR-3:</b> If a human bone or bone of unknown origin is found during construction, all work shall stop in the vicinity of the find, and the County Coroner shall be contacted immediately. If the remains are determined to be Native American, the coroner shall notify the Native American Heritage Commission, who shall notify the person most likely believed to be a descendant. The most likely descendant shall work with the contractor to develop a program for re-internment of the human remains and any associated artifacts. No additional work is to take place within the immediate vicinity of the find until the identified appropriate actions have taken place.</p>		<p>issuance of any grading or building permit.</p>
<p>Hydrology and Water Quality</p>	<p><b>Hydro 1</b> Construction and operation of the project shall not commence prior to recertification of the Natomas levees by the USACE and FEMA, and the subsequent removal of Natomas Basin from the 100-year floodplain and associated flood zone redesignation; or until FEMA redesignates the Natomas Basin with a flood zone designation that would permit development of the proposed project. The above measures shall terminate upon the first recertification of the levees by the U.S. Army Corps of Engineers.</p>	<p>Community Development Department,  United States Army Corp of Engineers</p>	<p>Prior to issuance of any grading or building permit, measures identified on plans shall be verified for compliance. The Community Development Department</p>

**Provence (P09-006)  
MITIGATION MONITORING PLAN**

	<p><b>Hydro 2:</b> The project applicant shall participate in a funding mechanism such as an assessment district established by SAFCA and/or the City for the purpose of implementing measures that would provide no less than 100-year flood protection including the North Natomas Area, or for that portion of the Natomas Basin requiring re-certification for 100-year flood protection including the Project site provided that such funding mechanism is (i) based on a nexus study; (ii) is regional in nature; (iii) is proportionate; (iv) complies with all applicable laws and ordinances; and (3) the requirements of the applicable FEMA zone and corresponding requirements under the City of Sacramento's Floodplain Ordinance shall be satisfied prior to the issuance of building permits for the project. Any future homeowners within the floodzone shall maintain federal flood insurance, as required under the applicable FEMA and City of Sacramento Floodplain Management Ordinance regulations. The above measures shall terminate upon the first recertification of the levees by the U.S. Army Corps of Engineers.</p>		<p>shall assure that measures are identified on construction plans and specifications and confirm compliance prior to issuance of any grading or building permit.</p>
<p>Noise</p>	<p><b>Noise 1:</b> STC 32 rated window assemblies shall be installed in all second and third floor windows with a direct view of East Commerce Way.</p> <p><b>Noise 2:</b> Each residential unit shall include a mechanical ventilation system to allow occupants to keep windows and doors closed to achieve isolation from exterior noise sources.</p>	<p>Community Development Department</p>	<p>Prior to issuance of any grading or building permit, measures identified on plans shall be verified for compliance. The Community Development Department shall assure that measures are identified on construction plans and specifications and confirm compliance prior to issuance of any grading or building permit.</p>

**Attachment 3: PUD Schematic Plan Amendment – Draft Resolution**

**RESOLUTION NO.**

Adopted by the Sacramento City Council

**APPROVING A PUD SCHEMATIC PLAN AMENDMENT FOR THE NATOMAS CROSSING PUD (AREA 3) TO DESIGNATE MULTI-FAMILY RESIDENTIAL USE FOR THE PROVENCE PROJECT (P09-006)(APN: 225-2330-002-0000 through 225-2330-076-0000, 225-2680-002-0000 through 225-2680-083-0000, and 225-2690-002-0000 through 225-2690-069-0000)**

**BACKGROUND**

- A. On March 10, 2011, the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve the PUD Schematic Plan Amendment for the Provence project.
- B. On April 19, 2011, the City Council conducted a public hearing, for which notice was given pursuant Sacramento City Code Section 17.200.010(C)(2)(a) and (c) (publication and mail 500'), and received and considered evidence concerning the Provence project.
- C. The proposed PUD Schematic Plan Amendment conforms to policies of the General Plan to provide adequate housing sites and opportunities for all households and to promote efficient development within a new growth area.
- D. The PUD Schematic Plan Amendment will not be injurious to the public welfare, nor to other properties in the vicinity of the development in that the project is compatible with adjacent developments and the site will be developed according to the requirements of the PUD Guidelines.

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:**

- Section 1. The PUD Schematic Plan Amendment for the Provence Project (as shown on the attached Exhibit) is approved.

**Table of Contents:**

Exhibit 3A: PUD Schematic Plan Amendment – 1 page

Exhibit 3A: PUD Schematic Plan Amendment



**Attachment 4: Project Approval – Draft Resolution**

**RESOLUTION NO.**

Adopted by the Sacramento City Council

**ADOPTING FINDINGS OF FACT AND APPROVING THE PROVENCE  
PROJECT (P08-013)**

**BACKGROUND**

A. On March 10, 2011, the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve the Provence project.

B. On April 19, 2011, the City Council conducted a public hearing, for which notice was given pursuant Sacramento City Code Section 17.200.010(C)(2)(a), (b), and (c) (publication, posting, and mail 500'), and received and considered evidence concerning the Provence project.

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL  
RESOLVES AS FOLLOWS:**

Section 1. Based on the verbal and documentary evidence received at the hearing on the Provence project, the City Council approves the project based on the findings of fact and subject to the conditions of approval as set forth below.

Section 2. The City Council approves the Project entitlements based on the following findings of fact:

**A. Tentative Map:** The Tentative Map to create 28 residential condominium lots, 5 private street lots, 16 private drive lots and one landscape lot on 10.8 net acres is **approved** based on the following findings of fact:

1. None of the conditions described in Government Code Section 66474, subsection (a) through (g), inclusive, exist with respect to the proposed subdivision as follows:
  - a. The proposed subdivision, together with the provisions for its design and improvement, is consistent with the City's General Plan, all applicable community and specific plans, and Title 16 of the City Code, which is a specific plan of the City;

- b. The site is physically suitable for the type of development proposed and suited for the proposed density;
  - c. The design of the subdivision and the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife their habitat;
  - d. The design of the subdivision and the type of improvements are not likely to cause serious public health problems;
  - e. The design of the subdivision and the type of improvements will not conflict with easements, acquired by the public at large, for access through or use, of, property within the proposed subdivision.
2. The proposed subdivision, together with the provisions for its design and improvement, is consistent with the City General Plan and Title 16 Subdivisions of the City Code, which is a specific plan of the City (Gov. Code §66473.5);
  3. The discharge of waste from the proposed subdivision into the existing community sewer system will not result in a violation of the applicable waste discharge requirements prescribed by the California Regional Water Quality Board, Central Valley Region, in that existing treatment plants have a design capacity adequate to service the proposed subdivision (Gov. code §66474.6);
  4. The design of the proposed subdivision provides, to the extent feasible, for future passive or natural heating and cooling opportunities (Gov. Code §66473.1);
  5. The Planning Commission has considered the effect of the approval of this tentative subdivision map on the housing needs of the region and has balanced these needs against the public service needs of its residents and available fiscal and environmental resources (Gov. Code §66412.3).

**B. Special Permit:** The Special Permit to develop 233 condominium units in the Employment Center 50 Planned Unit Development (EC-50-PUD) is **approved** based on the following findings of fact:

1. A special permit shall be granted upon sound principles of land use.  
Staff finds that the proposed development is an appropriate land use that will have positive contribution to the surrounding area, in that the project site is in close proximity to future commercial and open space uses and that the site will be well served by auto, bicycle, and pedestrian linkages.
2. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.  
Staff finds that the proposed condominium development site and building design are consistent with the Natomas Crossing PUD Guidelines and the Multi-Family

Residential Design Principles and will not be detrimental to public health, safety or welfare.

3. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.

The proposed project is consistent with the goals and policies of the employment center designation in the General Plan and the North Natomas Community Plan.

**C. Special Permit Major Modification:** The Special Permit to amend previous approval for 187 townhouse units per P06-194 is **approved** based on the following findings of fact:

1. A special permit shall be granted upon sound principles of land use.

Staff finds that the proposed condominium development is a better designed project than the previous approval on the site; the project provides more architectural variations and both private and public outdoor spaces for the occupants.

2. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.

The project will not be detrimental to the public health, safety or welfare since the buildings will be constructed to meet code standards. Residential developments of this type have been approved in many areas of the city. Staff finds that the proposed development is consistent with the Natomas Crossing PUD Guidelines and the Multi-Family Residential Design Principles.

3. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.

The proposed project is consistent with the employment center and residential policies of the General Plan and the North Natomas Community Plan.

Section 3. The City Council approves the Project entitlements subject to the following conditions of approval:

### **Conditions of Approval**

- A. The **Tentative Map** to create 28 residential condominium lots, 5 private street lots, 16 private drive lots and one landscape lot on 10.8 net acres is hereby approved subject to the following conditions:

**NOTE: These conditions shall supersede any contradictory information shown on the Tentative Map or any contradictory provisions in the PUD guidelines approved for this project (P01-028). The design of any improvement not**

**covered by these conditions or the PUD Guidelines shall be to City standard.**

**GENERAL:** All Projects

- A1. Pay off existing assessments, or file the necessary segregation requests and fees to segregate existing assessments.
- A2. Pursuant to City Code Section 16.40.190, indicate easements on the Final Map to allow for the placement of centralized mail delivery units. The specific locations for such easements shall be subject to review and approval of the Department of Transportation after consultation with the U.S. Postal Service.
- A3. The applicant shall participate in the North Natomas Financing Plan, adopted by Resolution No. 94-495 on August 9, 1994, and updated by Resolution No 2005-584 on August 2, 2005, and shall execute any and all agreements, which may be required in order to implement this condition.
- A4. Comply with the North Natomas Development Guidelines and the PUD guidelines approved for this project (P01-028) to the satisfaction of the Planning Director and the Department of Transportation.
- A5. Private reciprocal ingress, egress, maneuvering and parking easements are required for future development of the area covered by this Tentative Map. The applicant shall enter into and record an Agreement For Conveyance of Easements with the City stating that a private reciprocal ingress/egress, maneuvering, and parking easement shall be conveyed to and reserved from each common access lots (A, B, C, D, E, F, H, I, J, K, L, M, N, O, P) to each individual Lot along the common Lot, and from all private street lots (G, Q, R, S, T and U) at no cost, at the time of sale or other conveyance of either parcel.
- A6. Comply with requirements included in the Mitigation Monitoring Plan developed by, and kept on file in, the Planning Division Office (P01-028).
- A7. Meet all conditions of the existing PUD (P01-028) unless the condition is superseded by a Tentative Map condition.
- A8. Meet all conditions of the development agreement.
- A9. Show all continuing and proposed/required easements on the Final Map.
- A10. Multiple Final Maps may be recorded. Prior to recordation of any Final Map all infrastructure/improvements necessary for the respective Final Map must be in place to the satisfaction of the Departments of Utilities, and Department of Transportation.
- A11. Prior to submittal of improvement plans for this project, the developer's design

consultant(s) shall participate in a pre-design conference with City staff. The purpose of this conference is to allow City staff and the design consultants to exchange information on project design requirements and to coordinate the improvement plan review process. Contact the Department of Transportation, Plan Check Engineer at 808-7915 to schedule the conference. It is strongly recommended that the conference be held as early in the design process as possible.

**Department of Transportation: Streets (Anis Ghobril, DOT, 808-5367)**

- A12. The applicant shall be responsible for the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk fronting the property along East Commerce Way, Benefit Way and Advantage Court per City standards and to the satisfaction of the Department of Transportation. This shall include any needed street lighting.
- A13. The design and placement of walls, fences, signs and Landscaping near public intersections and private streets shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height. The area of exclusion shall be determined by the Department of Transportation.
- A14. This project shall require street lighting. There is an existing street lighting system around this project area. Improvements of right-of-way may require modification to the existing system. Electrical equipment shall be protected and remain functional during construction. The applicant shall provide acorn lighting as required along East Commerce Way to the satisfaction of the Department of Transportation.
- A15. The applicant shall make provisions for bus stops, shelters, etc. to the satisfaction of Regional Transit.
- A16. The applicant shall dedicate (if necessary) and construct bus turn-outs for all bus stops adjacent to the subject site to the satisfaction of the Department of Transportation.

**Department of Transportation: Private Streets (DOT)**

- A17. The applicant shall repair/reconstruct any deteriorated curb, gutter and sidewalks along the private streets to the satisfaction of the Department of Transportation.

**PUBLIC/PRIVATE UTILITIES (Yujean Kim, SMUD, (916) 732-5027)  
(Salam Khan, SASD, (916) 876-6094)**

- A18. Dedicate a standard 12.5 foot public utility easement (PUE) for underground and overhead facilities and appurtenances adjacent to all street right of ways.
- A19. Dedicate all private drives and 5-feet adjacent thereto, as public utility easement for underground and overhead facilities and appurtenances.
- A20. The owner or developer must disclose to future or potential owners the existing 69KV electrical facilities.
- A21. Sewer lines are already constructed and accepted by SASD. Any addition or modification to the SASD's sewer system shall be required to the satisfaction of SASD. SASD Design Standards apply to sewer construction or modification. (SASD)
- A22. While reconfiguring the lots, it must be ensured that each parcel shall have a separate connection to SASD public sewer system. If there is more than one building in any single parcel and the parcel is not proposed for split, then each building on that parcel shall have a separate connection to a private on-site sewer line or SASD public sewer line. (SASD)
- A23. Sewer easements have already been recorded for SASD sewer line installed within the project area. However, any modification to these sewer easements, if required, shall to the satisfaction of SASD. (SASD)
- A24. The subject project owner(s) and successors in interest thereof, shall be responsible for repair and/or replacement of all non-asphalt and/or enhanced surface treatments of streets and drives (such as stamped/colored/decorative concrete, concrete pavers, etc.) within these easements damaged by District maintenance and repair operations, including landscaping, channelization's, lighting, fountain area, sidewalk, and any other appurtenances conflicting therein. This requirement shall be set forth in easement grant documents and be a covenant running with the land, be responsibility of successors in interest in future land transfers and divisions and by language approved by the District. The District will only replace asphalt and standard concrete roadway/driveway disturbed due to maintenance/repair of its sewer line. If the repair is of decorative or stamped concrete, the District will only replace with standard concrete. (SASD)
- A25. Additional SASD/SRCSD Sewer impact fee (one time connection fee) may be required before issuance of building permits. Applicant should contact the Fee Quote Desk at (916) 876-6100 for sewer impact fee information. (SASD)

**CITY UTILITIES** (Jesus Reyes, Dept. of Utilities, 808-1721)

- A26. Applicant/Association shall execute an Agreement with the Department of Utilities (DOU) to assume ownership of the existing 8" public water line currently located in the existing private drives. The Agreement shall be to the satisfaction

of the DOU, Fire Department and the City Attorney.

- A27. Install 8" water meters and 8" RP back flow prevention assemblies at the two points of service (one at Benefit Way and the other one at Advantage Court) to the satisfaction of Department of Utilities. Meters shall be special meters suitable for use in combination Fire and Domestic water systems.
- A28. Applicant shall pay appropriate tap, meter and development fees associated with the installation of the special meters.
- A29. Water services for the existing townhouses (APN's: 225-2330-019, 020, 021, 022 & 023) shall be disconnected from the existing 8" water main and re-connected to the existing 12" public water main in East Commerce Way or Benefit Way. City will abandon the existing water services and install new water service taps to the point of service for fee. (Note: The existing water services may remain connected to the existing water main if the townhouses/lots become part of the Provence Master Association).
- A30. Abandon existing "Public Water Easements" (PWE) relating to the existing on-site public water main/appurtenances.
- A31. Execute and record a Utility Service Agreement (USA) approved by the City Attorney's Office and Department of Utilities.
- A32. If required, construct public water, sewer, drainage and service connections to the satisfaction of DOU.
- A33. All existing easements that are to remain and all existing right-of-ways shall be shown on the Final Map.
- A34. All onsite streets, drives, common areas, storm drain & water facilities shall be private facilities. Prior to or concurrent with the recording of the final map a homeowners association (HOA) shall be formed and C.C. & R.s shall be approved by the City and recorded assuring maintenance of the private streets, private drives, storm drainage facilities, water facilities and common areas. Private easements shall be dedicated for these facilities. The private street and drive maintenance shall include all pavement, curb, gutter and v-gutter.
- A35. Concurrent with the recordation of the final map, the applicant shall enter into and record an Agreement for Conveyance of Easements with the City, in a form acceptable to the City Attorney, requiring that private easements be granted, as needed, for drainage, water and sanitary sewer at no cost at the time of sale or other conveyance of any lot. A note stating the following shall be placed on the Final Map: "The lots created by this map shall be developed in accordance with recorded agreement for conveyance of easements # (Book\_\_\_\_, Page\_\_\_\_)."

**FIRE** (King Tunson, Fire Department, 808-1358)

- A36. Maintenance agreements shall be provided for the interior roadways of the proposed project and for the fire protection systems. The agreement shall be record with the Public Records Office having jurisdiction and shall provide for the following:
- Provisions for the necessary repair and maintenance of the roadway surface.
  - Removal of vegetation overgrowing the roadway and infringing on the roadway clear vertical height of thirteen feet six inches (13'6") and/or width of twenty feet (20').
  - Provisions for the maintenance, repair, and/or replacement of NO PARKING-FIRE LANE signage or striping.
  - Unrestricted use of and access to the roadways covered by the agreements.
  - Provisions for the control of vehicle parking in prohibited areas and a mechanism for the removal of vehicles illegally parked.
  - Maintenance and timely repair of all fire protection systems, including but not limited to hydrants, fire alarm systems and fire sprinklers.
- A37. Provide the required fire hydrants in accordance with CFC 508 and Appendix C, Section C105. Hydrant spacing shall be decreased where T courts are used. Hydrants shall be provided halfway between each T court, on one side of the street, and to the satisfaction of the Fire Department. *Due to the project increasing the number of units from 187 to 237, additional hydrants may be required.*
- PPDS:** Parks (Raymond Costantino, Parks Department, 808-8826)
- A38. **Payment of In-lieu Park Fee:** Pursuant to Sacramento City Code Chapter 16.64 (Parkland Dedication) the applicant shall pay to City an in-lieu park fee in the amount determined under SCC §§16.64.040 and 16.64.050 equal to the value of land prescribed for dedication under 16.64.030 and not satisfied by dedication. (See Advisory Note).
- A39. **Maintenance District:** The applicant shall initiate and complete the formation of a parks maintenance district (assessment or Mello-Roos special tax district), or annex the project into an existing parks maintenance district. The applicant shall pay all city fees for formation of or annexation to a parks maintenance district. (Contact Public Improvement Financing, Special Districts Project Manager. In assessment districts, the cost of neighborhood park maintenance is equitably spread on the basis of special benefit. In special tax districts, the cost of neighborhood park maintenance is spread based upon the hearing report, which specifies the tax rate and method of apportionment.).
- A40. **Multi-Use Trail:** A multi-use trail and adjacent landscaping shall be dedicated and constructed as specified below and in compliance with the Park Planning & Development Services (PPDS) "Multi-Use Trail Design Guidelines" available by contacting PPDS.

- 1 The City 2010 Bikeway Master Plan identifies an off-street multi-use trail on the southern portion of the project site, north of Snowy Egret Boulevard. The applicant shall comply with the Bikeway Master Plan and construct a multi-use trail in compliance with PPDS guidelines.
  - 2 The applicant shall submit and obtain PPDS approval of the alignment and design of the multi-use trail prior to submitting improvement plans for the trail.
  - 3 The proposed multi-use trail shall comply with Class I bike trail standards, including regulatory signage, as defined in Chapter 1000 of State Department of Transportation Highway Design Manual. The trail shall be a minimum 8' of asphalt concrete paving, with clear, graded shoulders that are a minimum of 2' in width. Shoulders should be decomposed granite or an alternate material approved by PPDS. Pavement sections shall be 3" minimum asphaltic concrete over 6" min of aggregate base, with a centerline stripe (refer to PPDS Trail detail and specification).
  - 4 Vehicular access controls shall be placed at the entrance to all access points to the trail (refer to PPDS details and specifications for approved designs). Access to the trail via Lots CC, DD and EE, along Colmars, Bastille and Garonne Walks is encouraged.
  - 5 Wherever possible and as approved by PPDS and the Department of Utilities, multi-use trails shall be designed as joint-use with utility service roads utilizing the service roads aggregate base as the trail's aggregate base course. Applicant shall design the pavement to meet all required design loads.
  - 6 Where a multi-use trail is located adjacent to any embankment with a greater than 4:1 slope, the Applicant shall, at his expense, install a post-and-cable fence along the top of the embankment, between the embankment and the multi-use trail.
  - 7 Residential lots adjoining the trail should be fenced w/ open tubular steel fencing.
  - 8 The Applicant shall disclose the location of the planned multi-use trail to all future/potential owners of parcels within the subdivision.
- A41. **New Private Facility Credits:** The Carriage Lane III (P05-079) development project, previously approved for this site, has a Private Facilities Agreement (City Agreement No. 2006-0298) which allowed 5% parkland dedication credit in exchange for the development of a private pool for the use of all residents in the then 187 unit subdivision. That Agreement is not transferable to this project. Prior to recording a Final Map, Applicant may opt to enter into a new private facilities agreement for the 238 units (including the 5 existing units), in which

case the following shall apply.

City Code Chapter 16.64, Sections 16.64.100, 110 and 120 address granting of private recreation facility credits. The city may grant credits for privately owned and maintained open space or local recreation facilities, or both, in planned developments as defined in Section 11003 of the Business and Professions Code, condominiums as defined in Section 783 of the Civil Code, and other common interest developments. Such credit, if granted in acres, or comparable in lieu fees, shall not exceed twenty-five (25) percent of the dedication or fees, or both, otherwise required under this chapter and no more than five percent per category of open space or recreational facilities described in this Chapter under 16.64.100.

Should the applicant elect to request City consideration of private facilities credit, contact PPDS at least three (3) months prior to recordation of Final Map. The Private Facilities Agreement will require City Council approval and must be in place prior to recordation of the Final Map.

## **MISCELLANEOUS**

- A42. Title to any property required to be dedicated to the City in fee shall be conveyed free and clear of all rights, restrictions, easements, impediments, encumbrances, liens, taxes, assessments or other security interests of any kind (hereafter collectively referred to as "Encumbrances"), except as provided herein. The applicant shall take all actions necessary to remove any and all Encumbrances prior to approval of the Final Map and acceptance of the dedication by City, except that the applicant shall not be required to remove Encumbrances of record, including but not limited to easements or rights-of-way for public roads or public utilities, which, in the sole and exclusive judgment of the City, cannot be removed and/or would not interfere with the City's future use of the property. The applicant shall provide title insurance with the City as the named beneficiary assuring the conveyance of such title to City.
- A43. Form a Homeowner's Association. CC&R's shall be approved by the City and recorded assuring maintenance of private roadway(s). The Homeowner's Association shall maintain all private streets, common lights, common landscaping and common areas.

## **ADVISORY NOTES:**

The following advisory notes are informational in nature and are not a requirement of this Tentative Map:

- A44. Prior to the issuance of any building permits, provide the City with a copy of the certificate of payment of any school fees for the applicable school district(s). (DOT)
- A45. If unusual amounts of bone, stone, or artifacts are uncovered, work within 50

meters of the area will cease immediately and a qualified archaeologist shall be consulted to develop, if necessary, further mitigation measures to reduce any archaeological impact to a less than significant effect before construction resumes. A note shall be placed on the final improvement plans referencing this condition. (DOT)

- A46. Dedicate slope easements and right-of-way necessary, for the Snowy Egret Boulevard overcrossing to the satisfaction of the Department of Transportation. The appropriate slope easements will be determined during the plan check phase of the improvement plans for this map. The Snowy Egret overcrossing shall line up with the street on the west side of I-5, to the satisfaction of the Department of Transportation. If a retaining wall is used in the construction of the overcrossing, then an engineering design shall be submitted to the satisfaction of the Department of Transportation. (DOT)
- A47. SASD policy prohibits gates that prevent access within sewer easements unless SASD standards for accessibility through gates are met. (SASD)
- A48. Any use of SASD sewer easements, which is not compatible or interferes with the construction, reconstruction, operation, maintenance, or repair of the SASD's sanitary sewer(s), shall not be allowed. Each proposed use shall be reviewed and approved in writing by the District Engineer prior to the use of the easement by the Grantor. This includes landscaping. (SASD)
- A49. A sewer study entitled Carriage Lane III was approved and the sewer infrastructure for this project has been constructed and accepted by SASD. (SASD)
- A50. As per City Code, the applicant will be responsible to meet his/her obligations regarding: (Parks)
- 1 Title 16, 16.64 Park Dedication / In Lieu (Quimby) Fees, due prior to approval of the final map. The Quimby in-lieu fee due for this project is estimated at \$401,610, if there is no new private facilities agreement, or \$382,866 if a new private facilities agreement is approved by City Council before the final map. Option 1 is based on 46 new multi-family units (difference between 233 and 187) and an average land value of \$687,500 per acre for the North Natomas Planning Area, plus an additional 20% for off-site park infrastructure improvements (which totals \$333,960), plus reimbursable to City of the 5% Quimby credit received on P05-079 for 187 units (\$67,650). Any change in these factors will change the amount of the Quimby fee due. The final fee is calculated using factors at the time of payment.
  - 2 Option 2 is based on 233 multi-family units and an average land value of \$687,500 per acre for the North Natomas Planning Area, plus an additional 20% for off-site park infrastructure improvements (which totals 1,691,580), less 5% Credit for new Private Facilities Agreement on 238 units for P06-006

(\$86,394), less Allegheny Credits applied to Quimby Obligation for P05-079 (\$1,289,970), plus reimbursable to City of the 5% Quimby credit received on P05-079 for 187 units (\$67,650). Any change in these factors will change the amount of the Quimby fee due. The final fee is calculated using factors at the time of payment.

- 3 Title 18, 18.44 Park Development Impact Fee, due at the time of issuance of building permit. The Park Development Impact Fee due for this project is estimated at \$712,514. This is based on 233 multi-family units at the rate of \$3,058 per unit. Any change in these factors will change the amount of the PIF due. The fee is calculated using factors at the time that the project is submitted for building permit.
- 4 Community Facilities District 2002-02, Neighborhood Park Maintenance CFD Annexation.

B. The **Special Permit** to develop 233 condominium units in the Employment Center 50 Planned Unit Development (EC-50-PUD) is hereby approved subject to the following conditions:

### Planning

- B1. This approval is for the construction of 233 condominium units per attached exhibits. Any change in the design, materials, or colors from this approval shall be submitted to the Planning Division for review and determination for further actions.
- B2. Final landscape and circulation plans shall be submitted to Planning Division for review and comment prior to submittal for building permits.
- B3. The applicant shall obtain all necessary building permits prior to commencement of construction.
- B4. Building plans and elevations shall be plotted that no two same elevations and no two color schemes are adjacent to one another. All building side elevations facing a street shall be enhanced with window openings, trims and decorative elements.
- B5. Provide an ownership association responsible for the care and maintenance of all common areas and common improvements and any other interest common to the condominium owners. Complete and true copies of all covenants, conditions and restrictions, articles of incorporation and by-laws shall be subject to review and approval by the city prior to occupancy as a condominium unit.

- B6. The homeowner's association shall conduct periodic inspections, not less than monthly, of the exterior of all buildings, trash enclosures and recreation facilities.
- B7. The homeowner's association shall establish and conduct a regular program of routine maintenance for the property. Such a program shall include common areas and scheduled repainting, replanting and other similar activities that typically require attention at periodic intervals but not necessarily continuous. Owner/Operator shall repaint or retreat all painted or treated areas at least once every 8 years; provided that the Planning Director may approve less frequent painting or re-treatment upon a determination that less frequent repainting or re-treatment is appropriate, given the nature of the materials used or other factors. The program shall be subject to review and approval by the Planning Director.
- B8. The homeowner's association shall maintain landscaping and irrigation in a healthy and serviceable condition.
- B9. The homeowner's association shall indicate and maintain all locations of parking stalls for handicapped/disabled access and strictly enforce rules related thereto.
- B10. Each condominium unit shall comply with the state of California's Noise Insulation Standards (California Amended Code Section 1092).
- B11. Each condominium unit shall provide the following:
  - a. A separate sewer service hookup; provided, that the planning commission may permit the use of common sewer lines that are oversized by one size or more, or which are hydraulically designed with the concurrence of the city engineer, finds the common sewer lines can adequately service the condominiums and that separate service hookups would not be feasible. For this provision, the Planning Commission is delegating the approval to the Planning Director subject to concurrence with the Public Works Director and the Utilities Department.
  - b. A separate water service hookup or shutoff; provided, that the planning commission may permit a single water system to service more than one condominium unit where shutoffs are provided wherever practicable and where the planning commission, with the concurrence of the city engineer, finds that the single water system can adequately service the condominiums and separate service hookups or shutoffs are not feasible. For this provision, the Planning Commission is delegating the approval to the Planning Director subject to concurrence with the Public Works Director and the Utilities Department.
  - c. A separate gas service where gas in a necessary utility.
  - d. A separate electrical service, with separate meters and disconnects and ground fault interrupters where and as required by Building Code.

- B12. Each unit of a condominium project, and all commonly owned portions of a Condominium building shall comply with all applicable building code standards. Nothing herein shall be construed to prevent or prohibit the applicant or the city from providing or requiring building standards greater than those set forth in the Building Code where the greater standards are found to be necessary to carry out the purposes and objectives of this chapter. (Ord. 99-015 § 6-3-D)
- B13. Landscaping & Walls and Fencing:
- a. The project shall comply with the City's Tree Shading Ordinance which requires 50 percent shading of the parking area within 15 years.
  - b. Install two 15-gallon trees per lot frontage, with the average spacing of 30' on center, as measured along the entire length of the street.
  - c. All landscaping and planting shall conform to City standards for sight line requirements at intersections and driveways.
  - d. All mechanical equipment shall be located within enclosed cabinets or screened by landscaping and/or screening/fencing.
  - e. In order to provide adequate surveillance opportunities, all *plants and shrubs* are to be maintained at maximum height of thirty inches (30"); the lowest branch height shall be at least six feet (6'). Decorative planting shall be maintained so as not to obstruct or diminish lighting level throughout the project.
  - f. Walls and fences shall conform to City standards for sight line requirements at intersections and driveways.
- B14. The applicant shall comply with the City's Recycling Ordinance (Section 17.72).
- B15. Signage:
- a. All detached signs shall be monument-type, constructed of masonry with finish materials and colors, which are consistent with building architecture. One detached monument sign is permitted and may be located within a landscape setback area but no closer than 10 feet from public right-of-way.
  - b. Attached signage shall consist of address numbers only.
  - c. All signage shall comply with the Natomas Crossing PUD signage criteria; a sign permit shall be obtained prior to construction of any sign.
- B16. Lighting:

- a. The type and location of the outdoor lighting (building, parking lot, walkway, etc.) must be approved by the Planning Director prior to issuance of a building permit. Lighting shall be provided in garage areas and each building address number shall be illuminated.
  - b. Project lighting shall be provided as follows: one foot-candle of minimum maintained illumination per square foot of parking space and exterior walkways/sidewalks during hours of darkness and 0.25 foot-candle of minimum maintained illumination per square foot of surface on any interior walkway, alcove, passageway, etc., from one-half hour before dusk to one-half hour after dawn. All light fixtures are to be vandal-resistant.
  - c. Per Section 17.68.030(B), exterior lighting, if provided, shall reflect away from residential areas and public streets.
  - d. Fixtures shall be unobtrusive and complementary to the architectural design of the building. Lighting shall be designed so as not to produce hazardous and annoying glare to motorists and building occupants, adjacent residents, or the general public.
- B17. The applicant shall comply with the approved Mitigation Monitoring Plan (P09-006) on file at the Planning Division.
- B18. The final playground shade structure and sculptures specifications shall be submitted for review and approval by the Planning Director prior to the issuance of building permit.
- B19. The final building color palette shall be submitted for review and approval by the Planning Director prior to the issuance of building permit.
- B20. The applicant shall paint electrical meters/cabinets, telephone connection boxes and other utility appurtenances to match the building to which they are attached.
- B21. *If security becomes a concern and if deemed necessary by the Police Department*, the applicant shall contract with a reputable security firm to provide on-site security, night and day.
- B22. Prior to the issuance of any building permits, the applicant shall provide the City with a copy of the certificate of payment of school fees for the applicable school district(s).
- B23. This approval does not include any vehicular gates. Any proposal for vehicular gates will require an additional Special Permit subject to approval by the Planning Commission.
- B24. Condominiums addressing shall be to City standards.

- B25. The pool and tot lot shall be used by tenants and their guests only.
- B26. Auto repairs are not permitted at any time, except in emergencies, such as flat tire or dead battery.
- B27. Visitor parking shall be strictly enforced.

**Department of Transportation**

- B28. The applicant shall be responsible for the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk fronting the property along East Commerce Way, Benefit Way and Advantage Court per City standards and to the satisfaction of the Department of Transportation. This shall include any needed street lighting.
- B29. This project shall require street lighting. There is an existing street lighting system around this project area. Improvements of right-of-way may require modification to the existing system. Electrical equipment shall be protected and remain functional during construction. The applicant shall provide acorn lighting as required along East Commerce Way to the satisfaction of the Department of Transportation.
- B30. The design and placement of walls, fences, signs and Landscaping near public intersections and private Streets shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height. The area of exclusion shall be determined by the Department of Transportation.
- B31. The applicant shall repair/reconstruct any deteriorated curb, gutter and sidewalks along the private streets to the satisfaction of the Department of Transportation.
- B32. Form a Homeowner's Association. CC&R's shall be approved by the City and recorded assuring maintenance of private roadway(s). The Homeowner's Association shall maintain all private streets, common lights, common landscaping and common areas.

**Fire**

- B33. Install gate at emergency vehicle access at East Commerce Way.

**Utilities**

- B34. Applicant/Association shall execute an Agreement with the Department of Utilities (DOU) to assume ownership of the existing 8" public water line currently

located in the existing private drives. The Agreement shall be to the satisfaction of the DOU, Fire Department and the City Attorney.

- B35. Install 8" water meters and 8" RP back flow prevention assemblies at the two points of service (one at Benefit Way and the other one at Advantage Court) to the satisfaction of Department of Utilities. Meters shall be special meters suitable for use in combination Fire and Domestic water systems.
- B36. Applicant shall pay appropriate tap, meter and development fees associated with the installation of the special meters.
- B37. Water services for the existing townhouses (APN's: 225-2330-019, 020, 021, 022 & 023) shall be disconnected from the existing 8" water main and re-connected to the existing 12" public water main in East Commerce Way or Benefit Way. City will abandon the existing water services and install new water service taps to the point of service for fee. (Note: The existing water services may remain connected to the existing water main if the townhouses/lots become part of the Provence Master Association)
- B38. Abandon existing "Public Water Easements" (PWE) relating to the existing on-site public water main/appurtenances.
- B39. All water connections shall comply with the City of Sacramento's Cross Connection Control Policy.
- B40. Per Sacramento City Code, water meters shall be located at the point of service which is located at the back of curb for separated sidewalks or the back of walk for connected sidewalks or at a location acceptable to DOU.
- B41. All onsite streets, drives, common areas, storm drain & water facilities shall be private facilities. A homeowners association (HOA) shall be formed and C.C. & R.s shall be approved by the City and recorded assuring maintenance of the private streets, private drives, storm drainage facilities, water facilities and common areas. Private easements shall be dedicated for these facilities. The private street and drive maintenance shall include all pavement, curb, gutter and v-gutter. If required by the Department of Utilities (DOU), the responsible maintenance agency shall enter into and record an agreement with the City regarding the maintenance of these facilities. The agreement shall be to the satisfaction of the DOU and the City Attorney.
- B42. Prior to issuance of a building permit, the owners and ownership association shall enter into a utility service agreement with the City to receive such utility services at points of service designated by the DOU. Such agreement shall provide, among other requirements, for payment of all charges for the storm drainage and water services, shall authorize discontinuance of utility services at the City's point(s) of service in the event that all or any portion of such charges are not paid when and as required, shall require compliance with all relevant

utility billing and maintenance requirements of the City, and shall be in a form approved by the City Attorney.

- B43. Per City Code, the Subdivider may not develop the project in any way that obstructs, impedes, or interferes with the natural flow of existing off-site drainage that crosses the property. The project shall construct the required public and/or private infrastructure to handle off-site runoff to the satisfaction of the DOU. If private infrastructure is constructed to handle off-site runoff, the applicant shall dedicate the required private easements and/or, at the discretion of the DOU, the applicant shall enter into and record an Agreement for Maintenance of Drainage with the City, in a form acceptable to the City Attorney.
- B44. The proposed development is located within Sacramento Area Sewer District (SASD) Satisfy all SASD requirements.
- B45. A grading plan showing existing and proposed elevations is required. All lots/parcels shall be graded so that drainage does not cross property lines or private drainage easements shall be dedicated. Adjacent off-site topography shall also be shown to the extent necessary to determine impacts to existing surface drainage paths. At a minimum, one-foot off-site contours within 100 feet of the project boundary are required (per Plate 2, page 3-7 of the City Design and Procedures Manual). No grading shall occur until the grading plan has been reviewed and approved by the DOU.
- B46. This project is greater than 1 acre in size; therefore, the project is required to comply with the State "NPDES General Permit for Stormwater Discharges Associated with Construction Activity" (State Permit). To comply with the State Permit, the applicant will need to file a Notice of Intent (NOI) with the State Water Resources Control Board (SWRCB) and prepare a Stormwater Pollution Prevention Plan (SWPPP) prior to construction. A copy of the State Permit and NOI may be obtained from [www.swrcb.ca.gov/stormstr/construction.html](http://www.swrcb.ca.gov/stormstr/construction.html). The SWPPP will be reviewed by the DOU prior to issuing a grading permit. The following items shall be included in the SWPPP: (1) vicinity map, (2) site map, (3) list of potential pollutant sources, (4) type and location of erosion and sediment BMP's, (5) name and phone number of person responsible for SWPPP and (6) certification by property owner or authorized representative.
- B47. The applicant must comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinance. This ordinance requires the applicant to show erosion and sediment control methods on the subdivision improvement plans. These plans shall also show the methods to control urban runoff pollution from the project site during construction.
- B48. Post construction (permanent), stormwater quality control measures shall be incorporated into the development to minimize the increase of urban runoff pollution caused by development of the area. Since the project is in an area served by a regional water quality control facility, only source control measures

are required. Storm drain message and signage is required at all drain inlets. Improvement plans must include the source controls measures selected for the site. Refer to the latest edition of the "Guidance Manual for On-Site Stormwater Quality Control Measures" for appropriate source control measures.

Advisory notes for the **Special Permit**:

1. Since December 8, 2008, the proposed project has been in a 100-year flood plain, designated as an AE zone by the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Maps (FIRMs). The base flood elevation ("BFE") for the Natomas Basin is 33' feet above sea level. The DOU expects the area to be taken out of the 100-year floodplain in 2012. Contact Nancy Dorfer (916-808-3539) for more information and updates on this issue. City Code Chapter 15.104 Floodplain Management Regulations require that any new construction of and/or substantial improvement to any structure located in Zone AE must have the lowest floor, including the basement, elevated a minimum of one (1) foot above the BFE. Non-residential structures have the option of flood proofing to one (1) foot above the BFE in lieu of the elevation requirement.
2. Unless otherwise approved by the Department of Utilities (DOU), all sanitary sewer, storm drain, water, and flood control improvements shall be in place and fully functioning prior to issuance of any building permits within the subject area.
3. **Many projects within the City of Sacramento require on-site booster pumps for fire suppression and domestic water systems.** Prior to design of the subject project, the DOU suggests that the applicant request a water supply test to determine what pressure and flows the surrounding public water distribution system can provide to the site. This information can then be used to assist the engineers in the design of the on-site fire suppression system.

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Exhibit 4B – Existing Condition

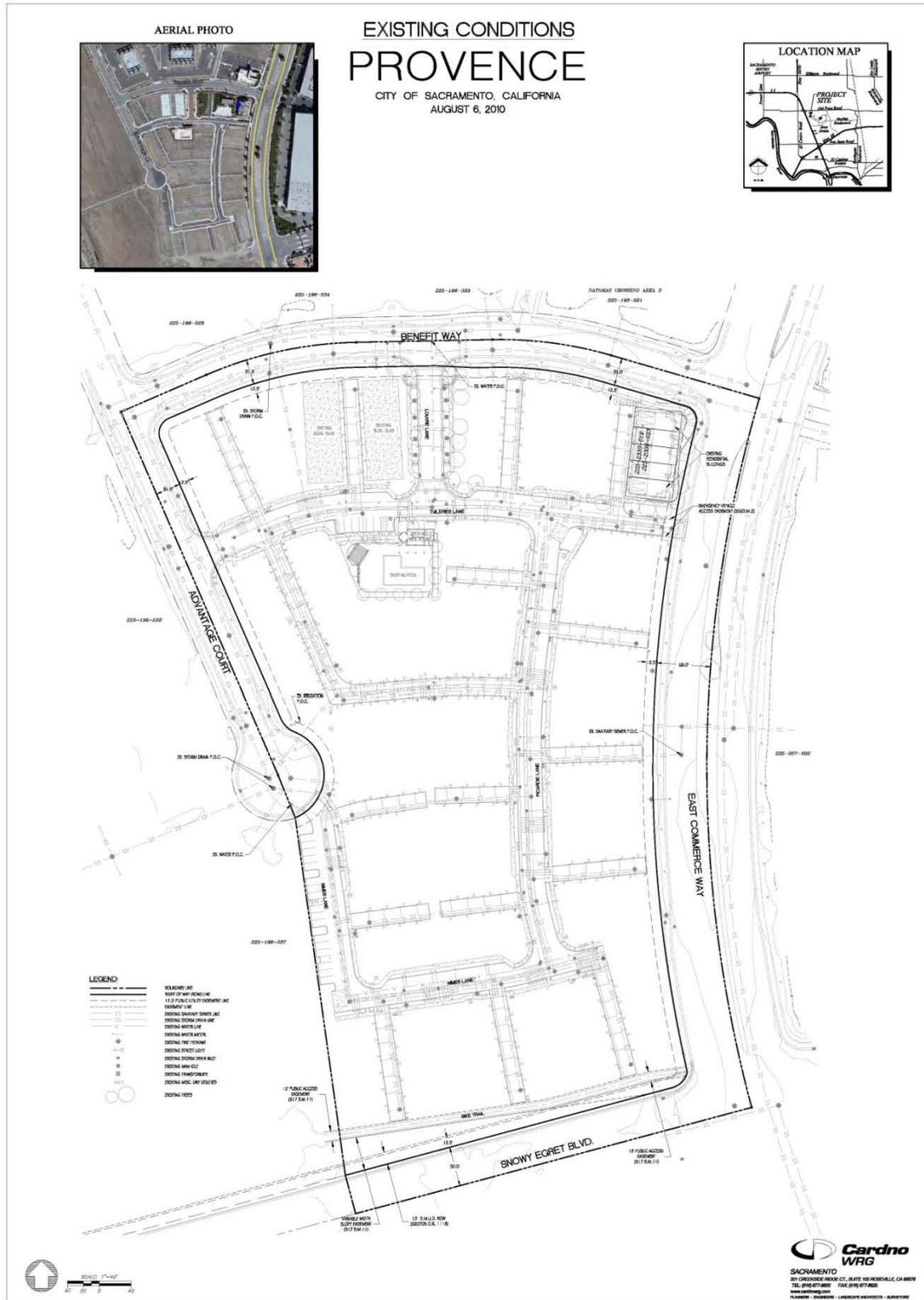






Exhibit 4E – Circulation Plan



Exhibit 4F – East Commerce Views Looking Southwest & West – With Trees



Note: Different monitors and printers may skew colors. Please reference the color boards provided for “true color and contrast.”

Building: 53  
 Type: 8  
 Elevation: A  
 Color Scheme: 3

Building: 52  
 Type: 8  
 Elevation: C  
 Color Scheme: 4

Building: 51  
 Type: 6-9  
 Elevation: A  
 Color Scheme: 1

Building: 46  
 Type: 6-9  
 Elevation: B  
 Color Scheme: 2

Building: 45  
 Type: 1-4  
 Elevation: C  
 Color Scheme: 3

**PROVENCE**

2/24/2011



Courts - East Commerce Way looking Southwest -With Trees



Note: Different monitors and printers may skew colors. Please reference the color boards provided for "true color and contrast."

Building: 53  
Type: 8  
Elevation: A  
Color Scheme: 3

Building: 52  
Type: 8  
Elevation: C  
Color Scheme: 4

Building: 51  
Type: 6-9  
Elevation: A  
Color Scheme: 1

Building: 46  
Type: 6-9  
Elevation: B  
Color Scheme: 2

Building: 45  
Type: 1-4  
Elevation: C  
Color Scheme: 3

**PROVENCE**

2/24/2011



Courts - East Commerce Way looking West -With Trees

Exhibit 4G – East Commerce Views Looking Southwest & West – Without Trees



Note: Different monitors and printers may skew colors. Please reference the color boards provided for “true color and contrast.”

Building: 53
Type: 8
Elevation: A
Color Scheme: 3

Building: 52
Type: 8
Elevation: C
Color Scheme: 4

Building: 51
Type: 6-9
Elevation: A
Color Scheme: 1

Building: 46
Type: 6-9
Elevation: B
Color Scheme: 2

Building: 45
Type: 1-4
Elevation: C
Color Scheme: 3

**PROVENCE**

2/24/2011



Courts - East Commerce Way looking Southwest -Without Trees



Note: Different monitors and printers may skew colors. Please reference the color boards provided for "true color and contrast."

Building: 53
Type: 8
Elevation: A
Color Scheme: 3

Building: 52
Type: 8
Elevation: C
Color Scheme: 4

Building: 51
Type: 6-9
Elevation: A
Color Scheme: 1

Building: 46
Type: 6-9
Elevation: B
Color Scheme: 2

Building: 45
Type: 1-4
Elevation: C
Color Scheme: 3

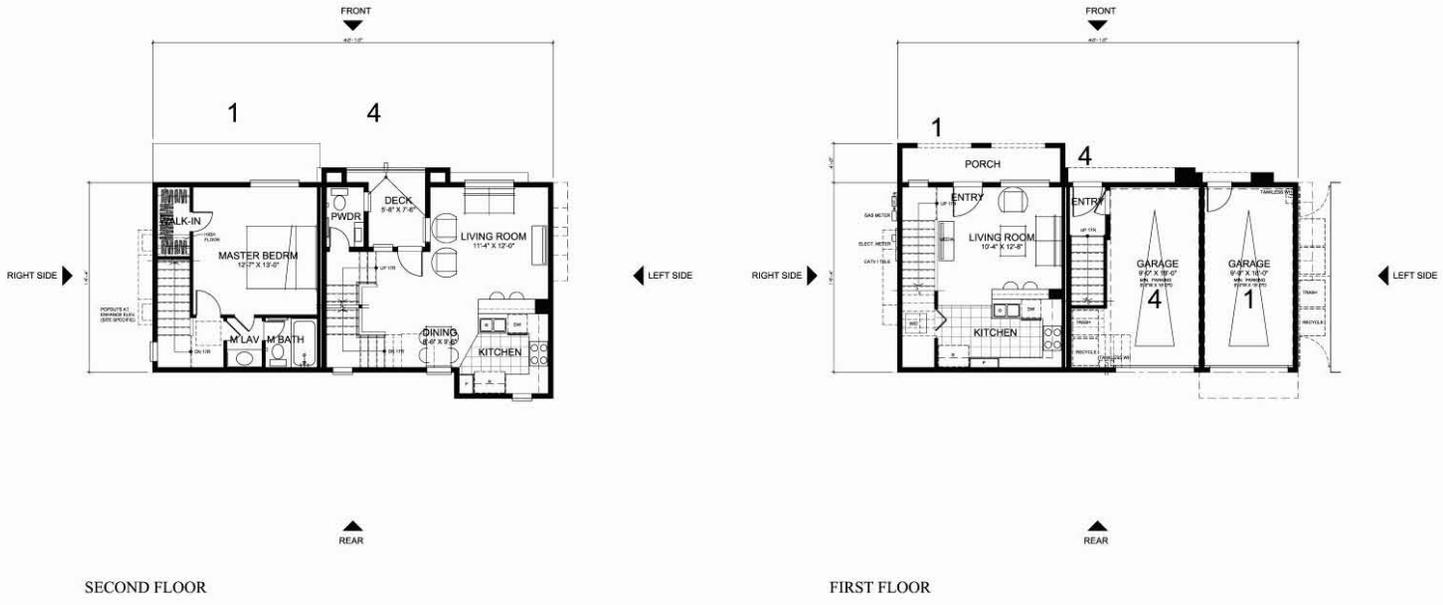
**PROVENCE**

2/24/2011



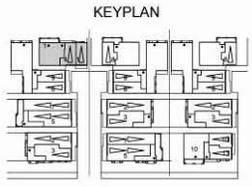
Courts - East Commerce Way looking West -Without Trees

Exhibit 4H – Courts – House Plans and Elevations



SECOND FLOOR

FIRST FLOOR



NOTES: REFER TO ALL OTHER COURT FLOOR PLANS FOR THE SAME AREA. SEE THE NAME SIDE LIGHT AND/OR THE NAME STYLE, THE GARAGE AND CONCRETE OR MISC.

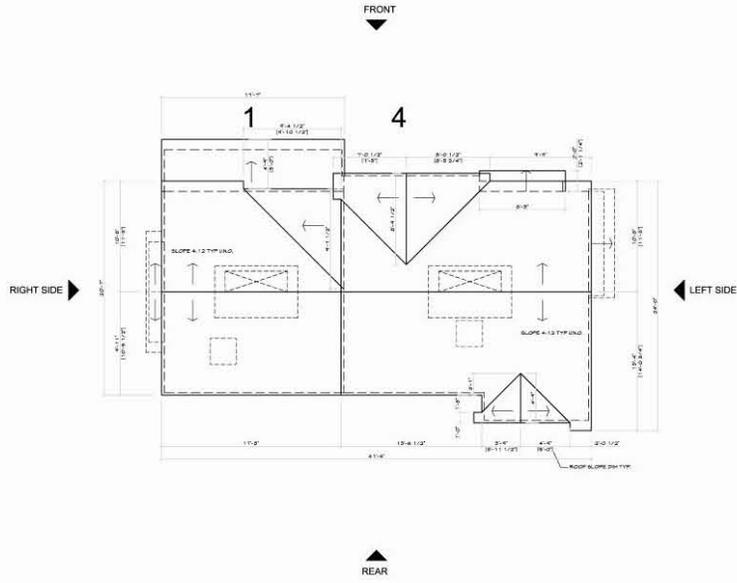
UNIT 1 & 4  
FIRST / SECOND FLOOR PLANS  
ELEVATION STYLE - 'A'

1/4"=1'-0"

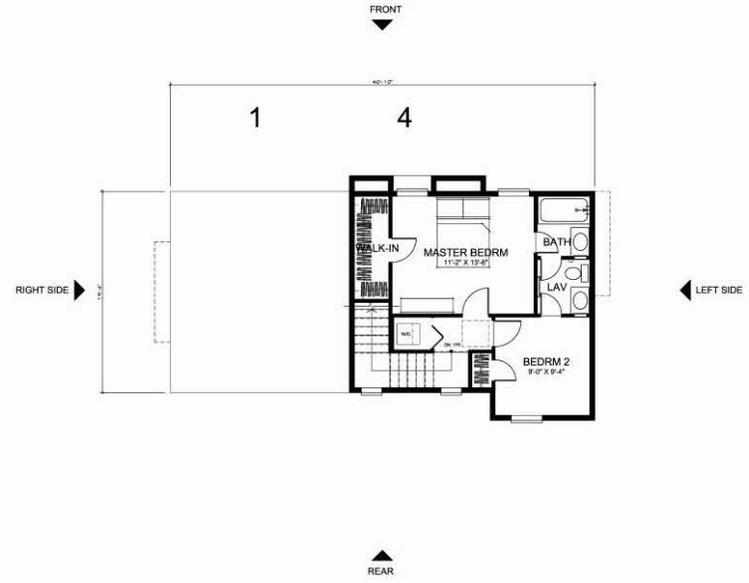
PROVENCE - COURTS  
RC NATOMAS, LLC



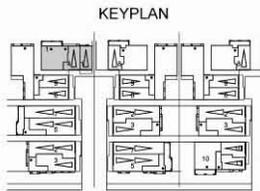
AREA ANALYSIS	
<b>UNIT 1</b>	
FIRST FLOOR	324 SF 8.3' F
SECOND FLOOR	324 SF 8.3' F
<b>TOTAL LIVING AREA</b>	<b>648 SF 8.3' F</b>
GARAGE	180 SF 5.3' F
PORCH	88 SF 2.3' F
<b>Total Area</b>	<b>916 SF 5.3' F</b>
<b>UNIT 4</b>	
FIRST FLOOR	56 SF 1.3' F
SECOND FLOOR	402 SF 8.3' F
THIRD FLOOR	484 SF 8.3' F
<b>TOTAL LIVING AREA</b>	<b>842 SF 8.3' F</b>
GARAGE	277 SF 6.3' F
DECK	80 SF 2.3' F
<b>Total Area</b>	<b>1199 SF 6.3' F</b>



ROOF PLAN



THIRD FLOOR



NOTES: WHERE THE FLOORING VARIATION IS NOT SHOWN, THE SAME SHALL BE USED THROUGHOUT THE ENTIRE FLOOR. UNLESS OTHERWISE NOTED, ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.  
 1 - THIS IS THE FINISHED FLOOR LEVEL.  
 2 - THIS IS THE FINISHED FLOOR LEVEL.  
 3 - THIS IS THE FINISHED FLOOR LEVEL.  
 4 - THIS IS THE FINISHED FLOOR LEVEL.

UNIT 1 & 4  
 THIRD FLOOR / ROOF PLAN  
 ELEVATION STYLE - 'A'

1/4"=1'-0"

PROVENCE - COURTS  
 RC NATOMAS, LLC

200904012



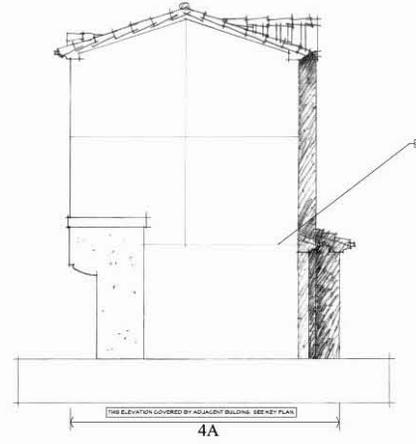
1.5.11

AREA ANALYSIS	
<b>UNIT 1</b>	
FIRST FLOOR	324.37 S.F.
SECOND FLOOR	324.18 S.F.
<b>TOTAL LIVING AREA</b>	<b>648.55 S.F.</b>
GARAGE	789.71 S.F.
POSBY	69.33 S.F.
<b>Total area</b>	<b>1507.67 S.F.</b>
<b>UNIT 4</b>	
FIRST FLOOR	36.71 S.F.
SECOND FLOOR	492.58 S.F.
THIRD FLOOR	484.81 S.F.
<b>TOTAL LIVING AREA</b>	<b>974.10 S.F.</b>
GARAGE	277.89 S.F.
DECK	50.83 S.F.
<b>Total area</b>	<b>1702.82 S.F.</b>





REAR ELEVATION



LEFT SIDE ELEVATION

# A

UNIT 1 & 4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

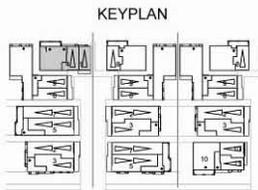
1/4" = 1'-0"

## PROVENCE - COURTS RC NATOMAS, LLC

200806020



1.5.11

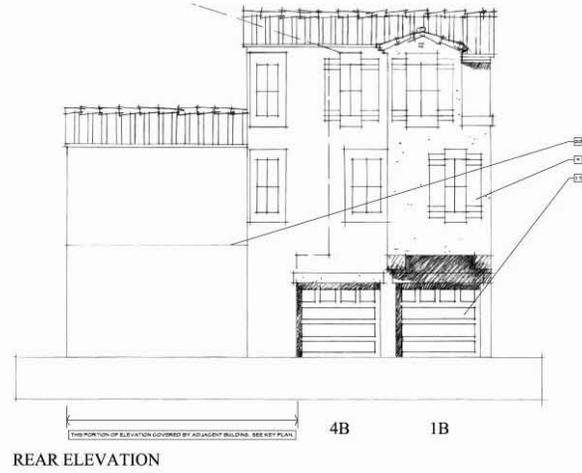
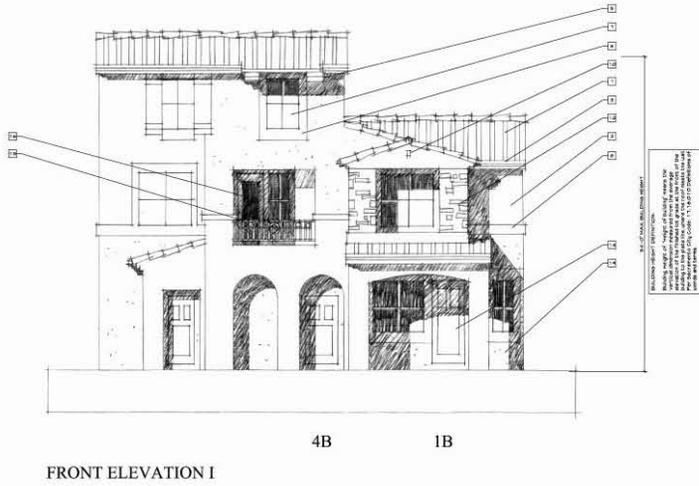


KEYPLAN

NOTE: WHERE ELEVATION LABELS BRACKET TO ONE SIDE ONLY OF THE NAME (BUILDING) INDICATE THE NAME STYLE, THE OTHER SIDE INDICATES A REFERENCE.  
1 - THREE DIMENSION ELEMENTS IN ALL ELEVATIONS OF BUILDING.  
2 - THREE DIMENSION ELEMENTS IN THREE VIEWS OF BUILDING.  
3 - FIBERGLASS ENTRY DOOR WITH FIBERGLASS WINDOW (1/2" MIN.)

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR 3/8" TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAFTER TAIL
<input type="checkbox"/>	STONE PATTERN VENER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED PILL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

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# B

SEE 'A' ELEVATIONS FOR SIDES

UNIT 1 & 4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'

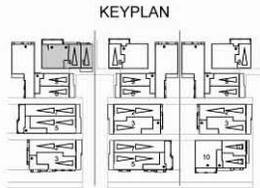
1/4"=1'-0"

PROVENCE - COURTS  
RC NATOMAS, LLC

200806020



1.5.11

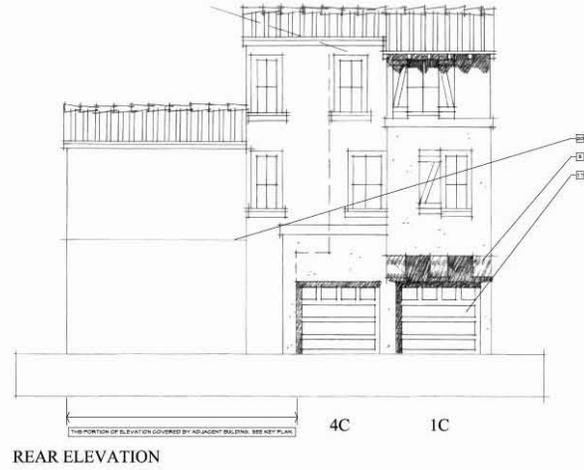
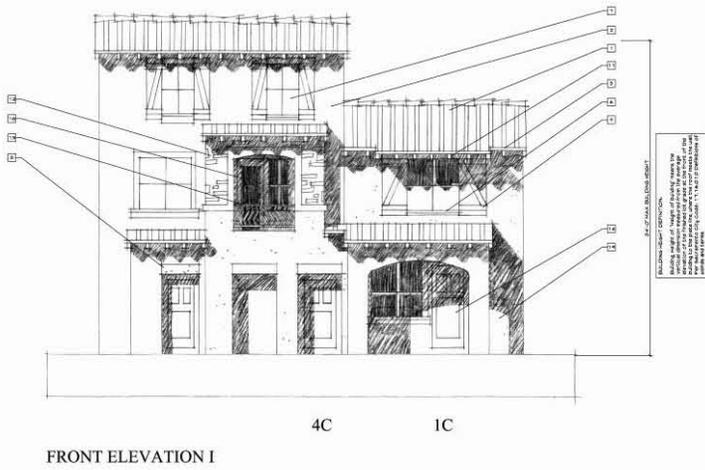


NOTE: REFER TO EXTERIOR ELEVATIONS FOR THE SAME USE OF THE NAME (BUILDING) AS IN THE SAME STYLE. THE FOLLOWING ARE ASSIGNED A NUMBER:  
1 - THREE BEDROOM ELEVATION (ALL ELEVATIONS TO MATCH)  
2 - THREE BEDROOM ELEVATION (ALL ELEVATIONS TO MATCH)  
3 - FIBERGLASS ENTRY DOOR WITH WINDOW  
4 - FIBERGLASS ENTRY DOOR WITH WINDOW (TOP HALF)

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR 3/4" TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAFTER TAIL
<input type="checkbox"/>	STONE PATTERN VENER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED PAINT
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

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March 24, 2011



# C

SEE 'A' ELEVATIONS FOR SIDES

UNIT 1 & 4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

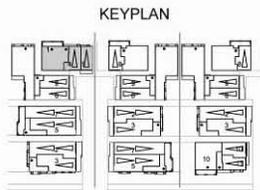
1/4"=1'-0"

## PROVENCE - COURTS RC NATOMAS, LLC

200906020



1.5.11



NOTE: WHERE ELEMENTS ARE SHOWN IN THE SAME VIEW OF THE SAME BUILDING, INDICATE THE SAME STYLE, THE DIFFERENT AND ANNOTATE A CHANGE.

1 - THREE DIMENSION ELEMENTS IN ALL ELEVATIONS OF BUILDING  
 2 - THREE DIMENSION ELEMENTS IN THREE VIEWS OF BUILDING  
 3 - FLOOR FINISHES AND WALL FINISHES (TOP VIEW)

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR 3/4" TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAFTER TAIL
<input type="checkbox"/>	STONE PATTERN VENER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED PAINT
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

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March 24, 2011



4A 1A  
FRONT ELEVATION II



4A 1A  
FRONT ELEVATION III

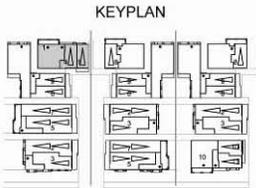
# A

UNIT 1 & 4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC



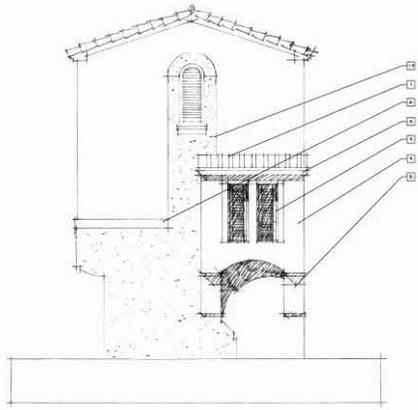
1.5.11



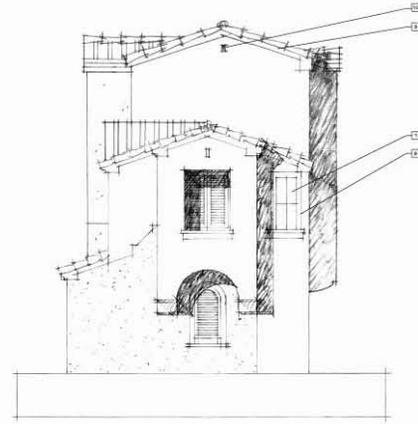
NOTE: NUMBER REFERENCE TO ELEMENTS IN THIS KEY PLAN ARE OF THE SAME BUILDING AND IN THE SAME STYLE. THE SHADING INDICATES AN APPROXIMATE REFERENCE.  
1 - THREE BEDROOM ELEVATION (ALL 4 ELEVATIONS OF THIS UNIT)  
2 - THREE BEDROOM ELEVATION (ALL 4 ELEVATIONS OF THIS UNIT)  
3 - THREE BEDROOM ELEVATION (ALL 4 ELEVATIONS OF THIS UNIT)  
4 - THREE BEDROOM ELEVATION (ALL 4 ELEVATIONS OF THIS UNIT)

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR 3/4" TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	SHAPED TAIL
<input type="checkbox"/>	STONE PATTERN VENER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED PILL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

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1A  
ENHANCED LEFT SIDE ELEVATION II



1A  
ENHANCED RIGHT SIDE ELEVATION II

# A

UNIT 1 & 4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

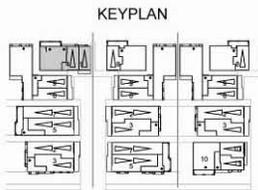
1/4" = 1'-0"

## PROVENCE - COURTS RC NATOMAS, LLC

200806020



1.5.11

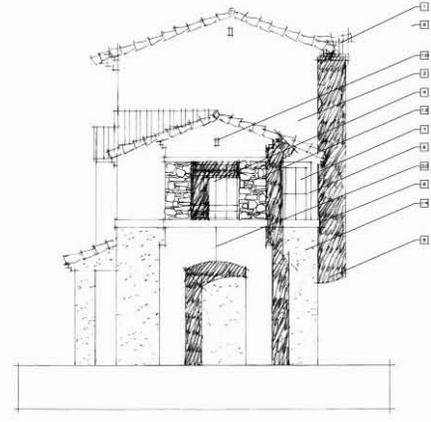


NOTE: NUMBER REFERENCE TABLES INDICATE THE SAME TYPE OF THE NAME BUILDING  
INDICATE THE SAME STYLE, THE TABLES AND ARE NOT A REFERENCE.

1 - OTHER FINISH ELEMENTS IN ALL ELEMENTS OF THIS  
2 - OTHER FINISH ELEMENTS IN THIS ELEMENTS OF THIS  
3 - OTHER FINISH ELEMENTS IN THIS ELEMENTS OF THIS

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR 3/8" TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAFTER TAIL
<input type="checkbox"/>	STONE PATTERN VENER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED PILL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

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1B

ENHANCED RIGHT SIDE ELEVATION I

# B

SEE 'A' ELEVATIONS FOR STANDARD SIDES

UNIT 1 & 4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'

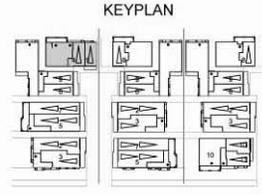
1/4" = 1'-0"

## PROVENCE - COURTS RC NATOMAS, LLC

200906020

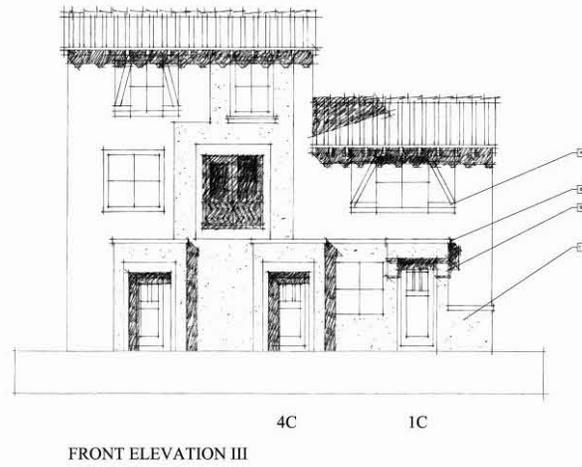
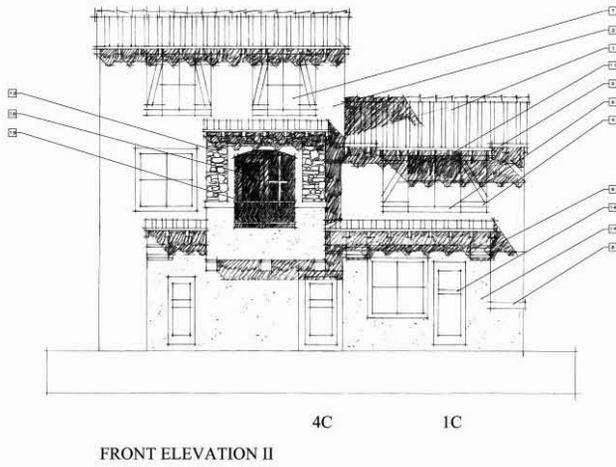


1.5.11



NOTE: NUMBER REFERENCE TABLES INDICATE THE NUMBER OF THE NAME BEHIND  
 AND IN THE SAME STYLE. THE TABLES ARE ASSIGNED A NUMBER.  
 1 - THREE DIMENSION ELEMENTS IN ALL ELEVATIONS AND  
 2 - THREE DIMENSION ELEMENTS IN THE ELEVATIONS AND  
 3 - FLOOR FINISHES AND WALL FINISHES (IF ANY)  
 4 - FLOOR FINISHES AND WALL FINISHES (IF ANY)

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR 3/4" TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	SHAPED TAIL
<input type="checkbox"/>	STONE PATTERN VENER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED PILL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



C

SEE 'A' ELEVATIONS FOR STANDARD SIDES

UNIT 1 & 4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

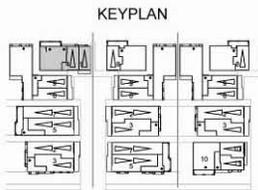
1/4"=1'-0"

PROVENCE - COURTS  
RC NATOMAS, LLC

200906020



1.5.11



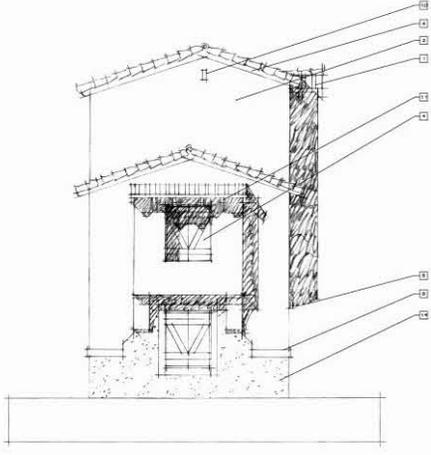
NOTE: NUMBER 1 IS THE UNIT NUMBER AND IS NOT THE SAME AS THE UNIT NAME (BUILDING)  
UNLESS THE NAME SPECIF. THE BUILDING AND UNIT AND/OR A NUMBER.

1 - THREE DIMENSION ELEMENTS IN ALL ELEVATIONS OF MASS  
2 - THREE DIMENSION ELEMENTS IN ZONA, REFLECTIVE MASS  
3 - FIBERGLASS INSULATION WALL THICKNESS (1 1/2" MAX)

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR 3/8" TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAFTER TAIL
<input type="checkbox"/>	STONE PATTERN VENER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED PALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

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March 24, 2011



1A

ENHANCED RIGHT SIDE ELEVATION II

# C

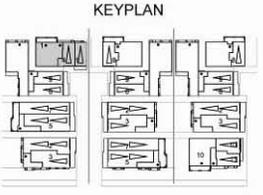
SEE 'A' ELEVATIONS FOR STANDARD SIDES

UNIT 1 & 4  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

## PROVENCE - COURTS RC NATOMAS, LLC

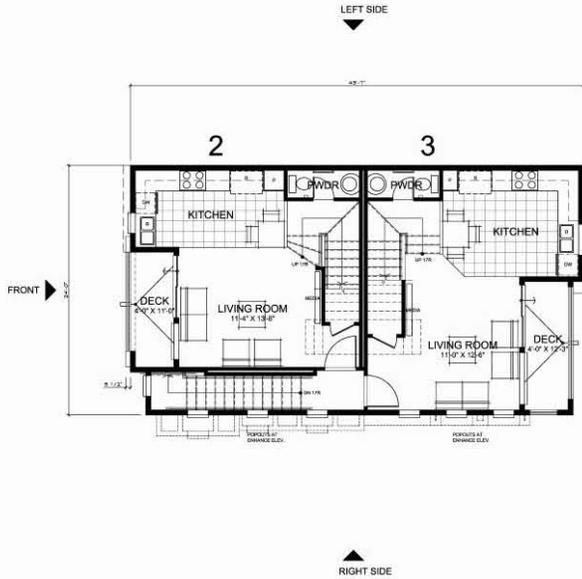


1.5.11

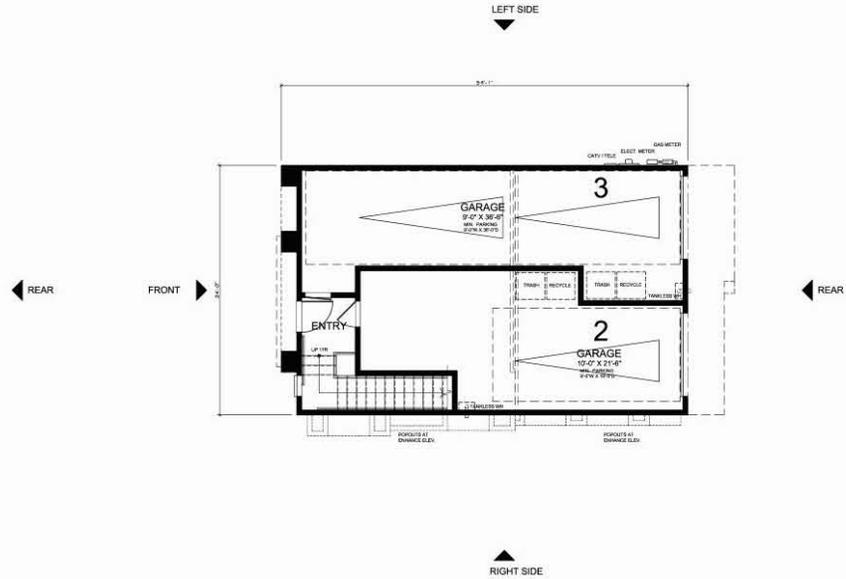


NOTE: NUMBER THE ELEVATION LABELS CORRESPONDING TO THE NUMBER OF THE NAME BEHIND THEM TO THE SAME STYLE. THE WALLS AND JOINTS ARE SHOWN AS A REFERENCE.  
1 - THREE DIMENSION ELEMENTS IN ALL ELEVATIONS OF MASS  
2 - THREE DIMENSION ELEMENTS IN THREE ELEVATIONS OF MASS  
3 - FIBERGLASS ENTRY DOOR WITH PANEL  
4 - FIBERGLASS ENTRY DOOR WITH PANEL

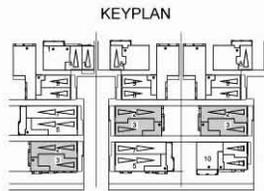
TYPICAL BUILDING MATERIALS	
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<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
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<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAFTER TAIL
<input type="checkbox"/>	STONE PATTERN VENER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED PILL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



SECOND FLOOR



FIRST FLOOR



NOTE: THESE FLOOR PLANS SHOW THE GENERAL LAYOUT OF THE UNIT. THE EXACT DIMENSIONS AND FINISHES ARE SUBJECT TO THE ARCHITECT'S FINAL DESIGN. THE ARCHITECT'S OFFICE SHALL BE RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION PROVIDED HEREON.

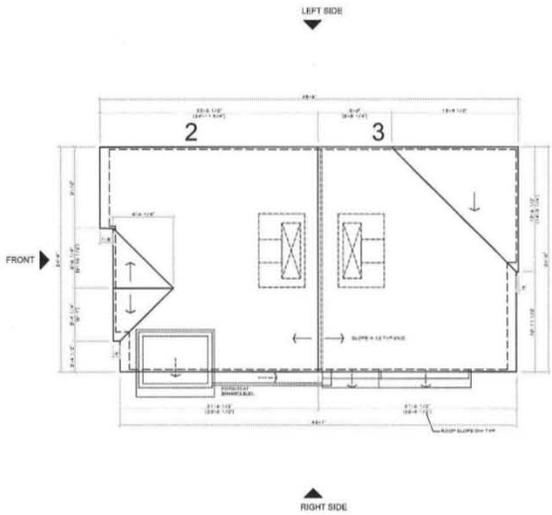
UNIT 2 & 3  
FIRST / SECOND FLOOR PLANS  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC

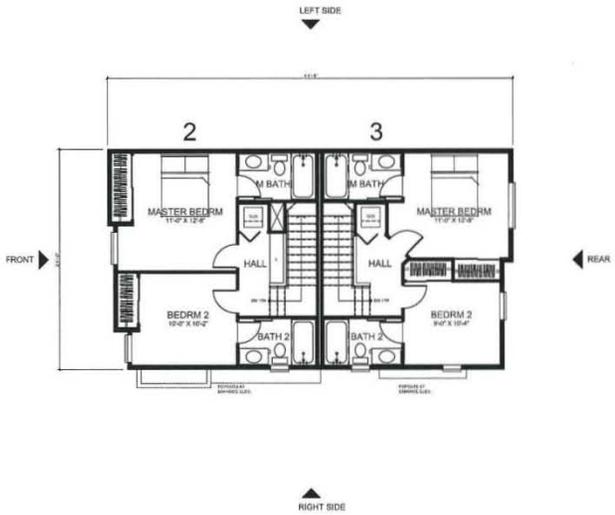


1.5.11

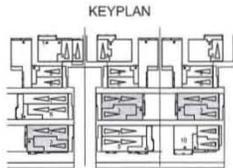
AREA ANALYSIS	
<b>UNIT 2</b>	
FIRST FLOOR ENTRY	124.63 S.F.
SECOND FLOOR	377.90 S.F.
THIRD FLOOR	325.50 S.F.
<b>TOTAL LIVING AREA</b>	<b>828.03 S.F.</b>
GARAGE	380.38 S.F.
DECK	84.55 S.F.
<b>Useful area</b>	<b>1293.59 S.F.</b>
<b>UNIT 3</b>	
FIRST FLOOR ENTRY	124.63 S.F.
SECOND FLOOR	419.58 S.F.
THIRD FLOOR	483.55 S.F.
<b>TOTAL LIVING AREA</b>	<b>1027.76 S.F.</b>
GARAGE	452.83 S.F.
DECK	59.91 S.F.
<b>Useful area</b>	<b>1540.50 S.F.</b>



ROOF PLAN



THIRD FLOOR



NOTE: ROOMS IDENTIFIED IN THIS KEYPLAN ARE FOR THE MAIN SIDE OF THE COMMON BUILDING.  
 ABOVE THE MAIN LEVEL, THE 3-BEDROOM AND 2-BATHROOM.  
 1 - OTHER ROOMS IDENTIFIED IN THIS KEYPLAN.  
 2 - THIS IS A REPRESENTATIVE OF THE 3-BEDROOM AND 2-BATHROOM.  
 3 - OTHER ROOMS IDENTIFIED IN THIS KEYPLAN.

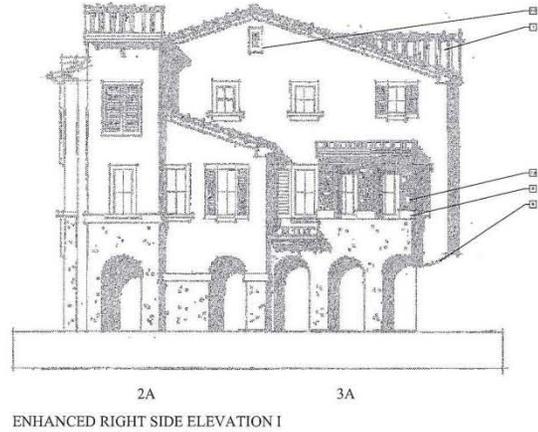
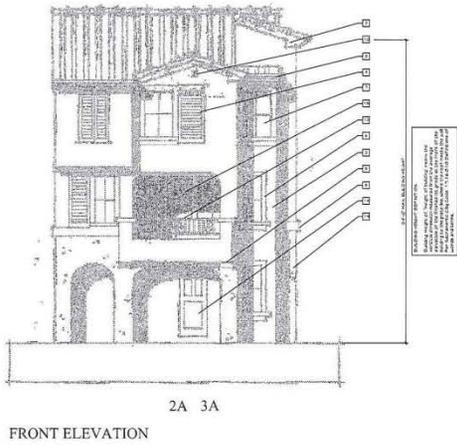
UNIT 2 & 3  
 THIRD FLOOR / ROOF PLAN  
 ELEVATION STYLE - 'A'

PROVENCE - COURTS  
 RC NATOMAS, LLC



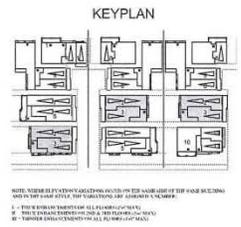
1.5.11

AREA ANALYSIS	
UNIT 2	
FIRST FLOOR FLOOR PLAN	154.83 SQ. FT.
SECOND FLOOR	277.78 SQ. FT.
THIRD FLOOR	538.38 SQ. FT.
TOTAL LIVING AREA	1071.00 SQ. FT.
STAIRS	4.00 SQ. FT.
ROOF	1200.00 SQ. FT.
UNIT 3	
FIRST FLOOR FLOOR PLAN	154.83 SQ. FT.
SECOND FLOOR	277.78 SQ. FT.
THIRD FLOOR	451.11 SQ. FT.
TOTAL LIVING AREA	883.72 SQ. FT.
STAIRS	4.00 SQ. FT.
ROOF	1200.00 SQ. FT.
TOTAL	1954.72 SQ. FT.



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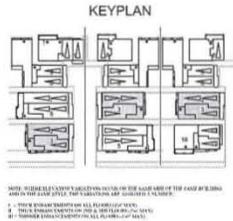
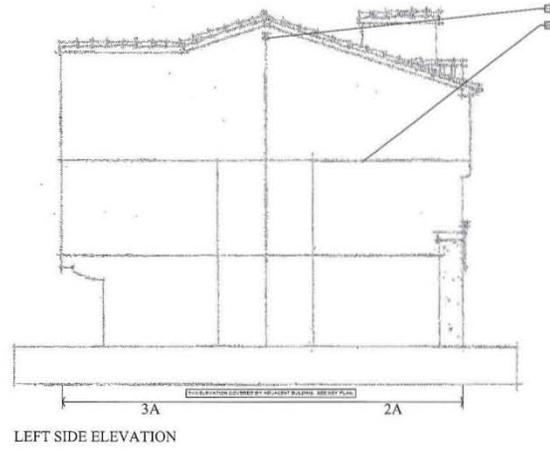
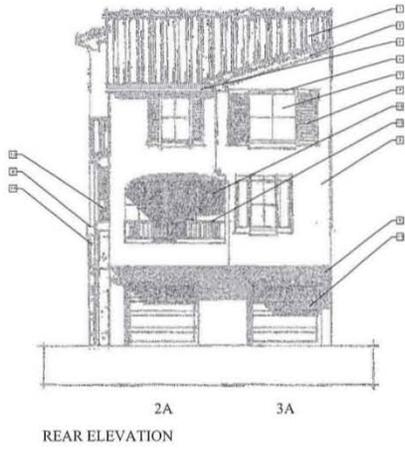
**A**  
 SEE 'B' FOR STANDARD RIGHT  
 UNIT 2 & 3  
 EXTERIOR ELEVATIONS  
 ELEVATION STYLE - 'A'



PROVENCE - COURTS  
 RC NATOMAS, LLC



TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR 1/2" TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	KAFFER TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL MAIL / SWARDMAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED WALL
* ALL METAL AND HOOD ELEMENTS TO BE PAINTED	



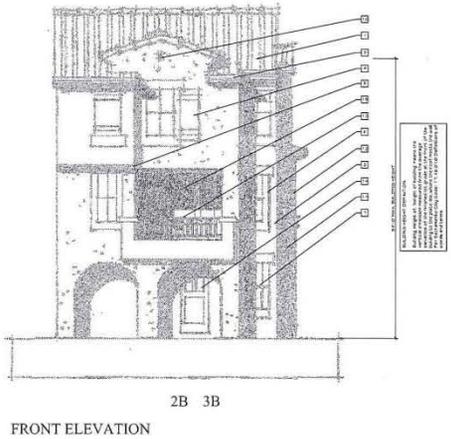
# A

UNIT 2 & 3  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC



TYPICAL BUILDING MATERIALS	
[ ]	INTEGRAL COLOR 1/2" TILE ROOF
[ ]	INTEGRAL COLOR PLASTER
[ ]	FASCIA
[ ]	RAKE BOARD
[ ]	PLASTER CORNICE
[ ]	PLASTER WINDOW TRIM
[ ]	VINYL WINDOW
[ ]	PLASTER TRIM
[ ]	DECORATIVE SHUTTER
[ ]	ORIGINAL PLASTER PROJECTION
[ ]	KAFFER TAIL
[ ]	STONE PATTERN VENEER
[ ]	METAL RAIL / GUARDRAIL
[ ]	TEMPERED GLASS ENTRY DOOR
[ ]	BUILDING GLASS DOOR
[ ]	FRENCH DOOR
[ ]	METAL SECTIONAL GARAGE DOOR WITH WINDOW
[ ]	SLOPPING PLASTER BALL
[ ]	HEAVY TEXTURE STUCCO
[ ]	EXPANSION JOINT
[ ]	PLASTER BATTERED WALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

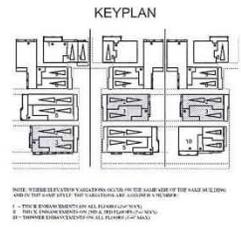


ALL MATERIALS TO BE FINISHED WITH A LIGHT SAND FINISH UNLESS OTHERWISE NOTED.

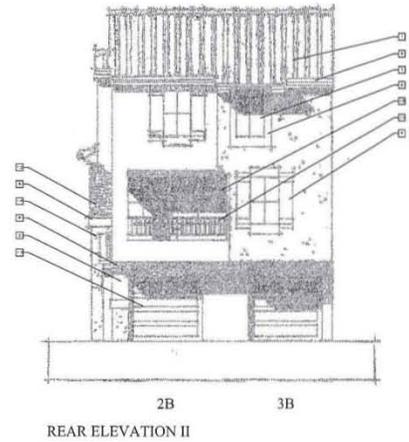
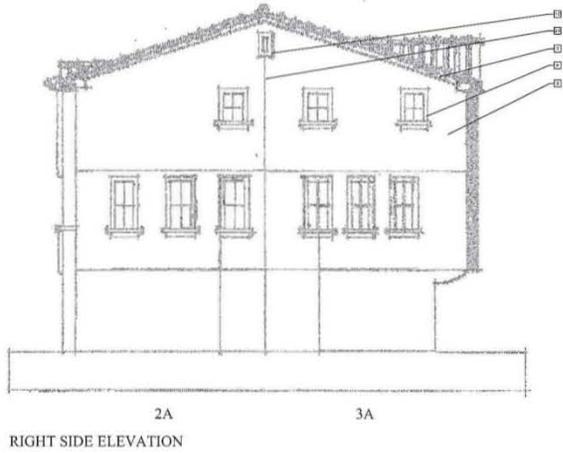
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UNIT 2 & 3  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'

PROVENCE - COURTS  
RC NATOMAS, LLC



TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR 3/8" TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
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<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	KAFFER TAL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL RAIL / SHARDORAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLIPING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED PAUL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



# A, B

SEE 'A' ELEVATIONS FOR LEFT SIDE

UNIT 2 & 3  
EXTERIOR ELEVATIONS  
ELEVATION STYLE 'A' & 'B'

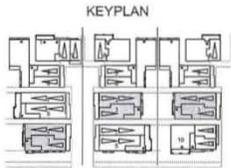
1/2" = 1'-0"

PROVENCE - COURTS  
RC NATOMAS, LLC

2009/01/02



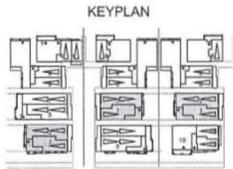
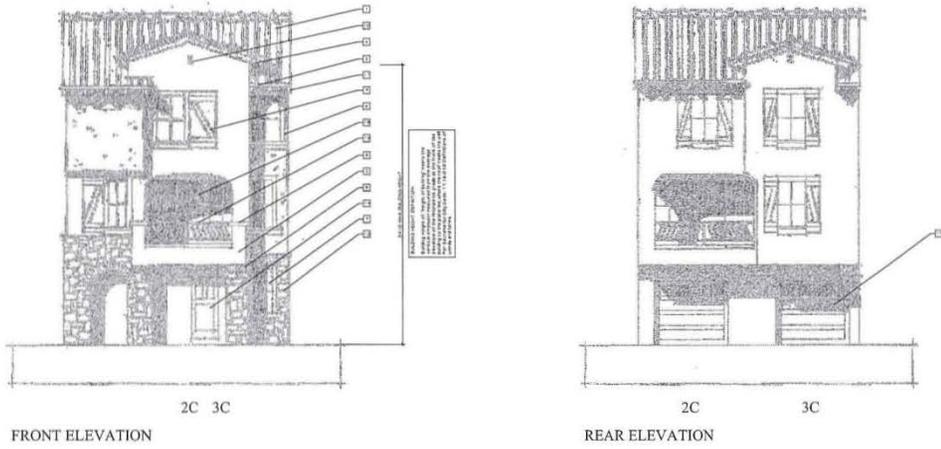
1.5.11



NOTE: THESE ELEVATIONS ARE PROVIDED FOR THE GENERAL INFORMATION OF THE ARCHITECT AND ARE NOT TO BE USED FOR CONSTRUCTION. THE ARCHITECT SHALL VERIFY THE ACCURACY OF THE INFORMATION PROVIDED HEREIN AND SHALL BE RESPONSIBLE FOR ANY ERRORS OR OMISSIONS. THE ARCHITECT SHALL BE RESPONSIBLE FOR ANY CHANGES TO THE INFORMATION PROVIDED HEREIN.

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERIOR COLOR 1/2" TILE ROOF
<input type="checkbox"/>	INTERIOR COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	KAFFER TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL MAIL / GUARDRAIL
<input type="checkbox"/>	REINFORCED ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLIDING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED WALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

17 OF 62



UNITS SHOWN IN ELEVATION CORRESPOND TO THE GRID ASSUMED BY THE GRID NUMBERING AND IN THE GRID STYLE. THE GRID NUMBERING ASSUMED IS AS FOLLOWS:

A - 10'0\"

**C**  
SEE 'A' ELEVATIONS FOR STANDARD SIDES

UNIT 2 & 3  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

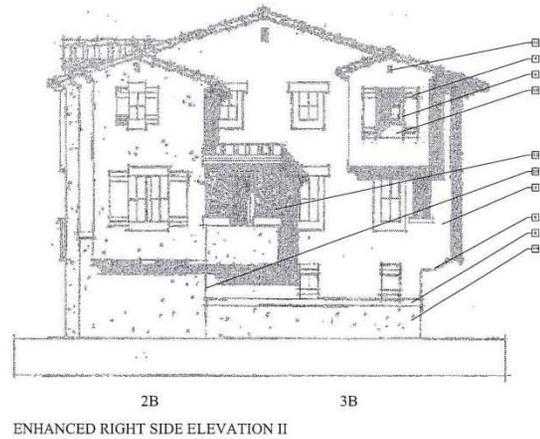
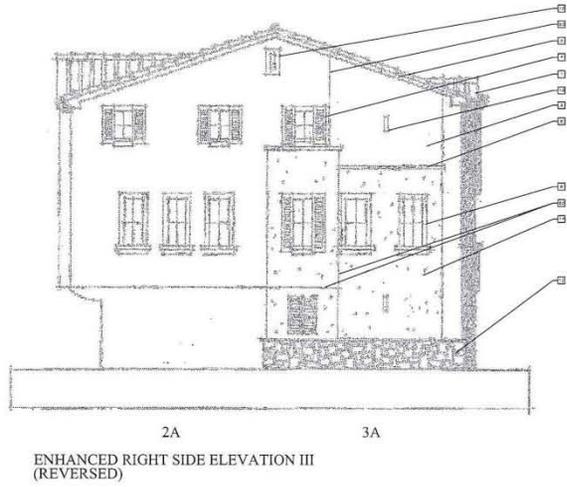
PROVENCE - COURTS  
RC NATOMAS, LLC



13.11

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHITTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAPPORT TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL RAIL / BARRIER
<input type="checkbox"/>	FRIGID-GLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL RESTROOM GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER RAIL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED WALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

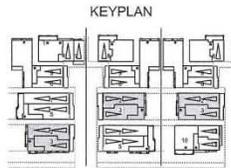
18 OF 62



# A, B

SEE 'A' ELEVATIONS FOR STANDARD SIDES

UNIT 2 & 3  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A' & 'B'



NOTE: THESE ELEVATION VIEWS ARE INTENDED TO BE USED FOR THE SIDE OF THE UNIT IN WHICH THE UNIT IS TO BE INSTALLED. SEE ARCHITECT'S NOTES FOR UNIT ORIENTATION.  
1 - THIS ELEVATION SHOWS ALL EXTERIOR FINISHES.  
2 - THIS ELEVATION SHOWS THE UNIT'S EXTERIOR FINISHES.  
3 - THIS ELEVATION SHOWS THE UNIT'S EXTERIOR FINISHES.

## PROVENCE - COURTS RC NATOMAS, LLC



TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR 1/2" TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER GORBEL
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	KAPITELE
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED WALL
* ALL METAL AND HOOD ELEMENTS TO BE PAINTED	



ENHANCED RIGHT SIDE ELEVATION III

ENHANCED LEFT SIDE ELEVATION III

# B

SEE 'A' ELEVATIONS FOR STANDARD SIDES

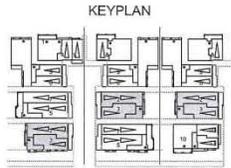
UNIT 2 & 3  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'

PROVENCE - COURTS  
RC NATOMAS, LLC

2010-04-20

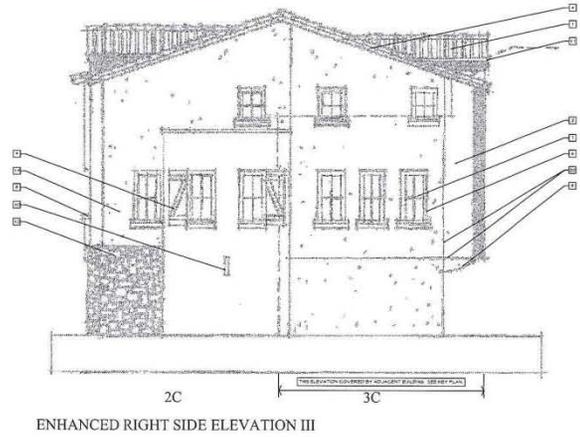


1.E11



NOTE: THESE ELEVATIONS AND MATERIALS ARE FOR THE HOME SIDE OF THE HOME. THE OTHER SIDE OF THE HOME IS TO BE DETERMINED BY THE ARCHITECT. SEE ELEVATION 'A' FOR THE OTHER SIDE OF THE HOME.  
1 - THIS IS A STANDARD ELEVATION FOR ALL UNITS OF THIS TYPE.  
2 - THIS IS A STANDARD ELEVATION FOR ALL UNITS OF THIS TYPE.  
3 - THIS IS A STANDARD ELEVATION FOR ALL UNITS OF THIS TYPE.

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR 3/8" TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAFTER TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLIPING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED PAINT
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



# C

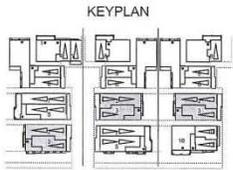
SEE 'A' ELEVATIONS FOR STANDARD SIDES

UNIT 2 & 3  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

PROVENCE - COURTS  
RC NATOMAS, LLC

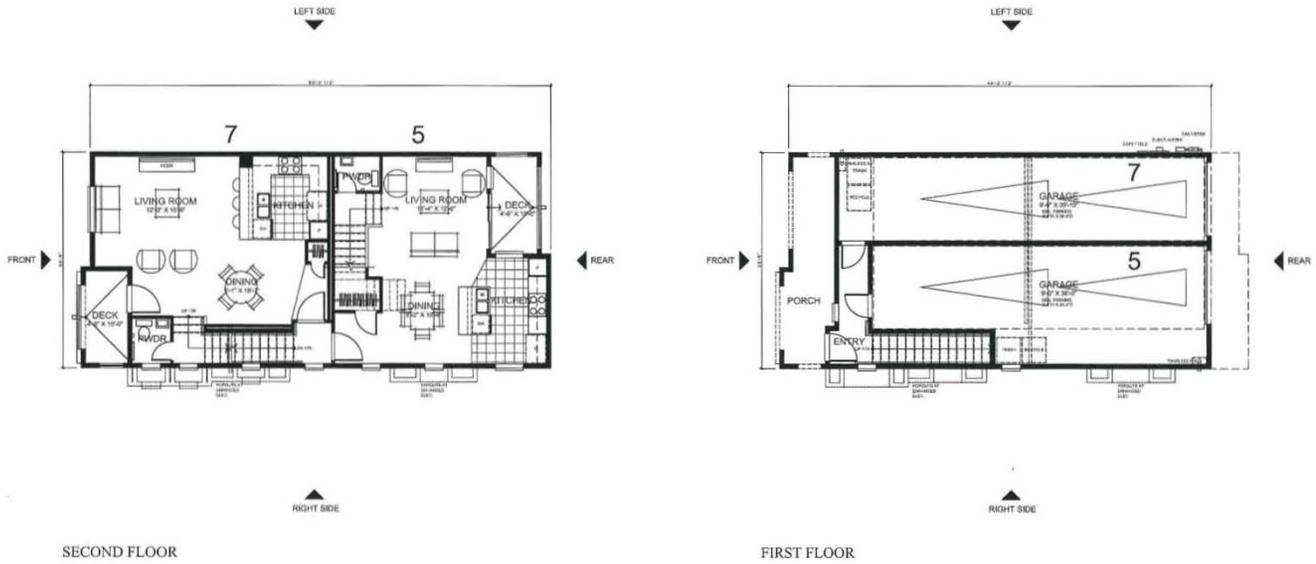


LS11



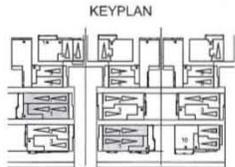
NOTE: THESE ELEVATIONS AND MATERIALS LIST ARE FOR INFORMATION ONLY. THE ARCHITECT IS NOT RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION OR THE MATERIALS LISTED. THE ARCHITECT IS NOT RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION OR THE MATERIALS LISTED.

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR W/ TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	KAFFER TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL K-VAL / QUADRANT
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLIDING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED HALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



SECOND FLOOR

FIRST FLOOR



NOTE: WHERE EXISTING VARIATIONS OCCUR ON THE SAME SHEET OF THE SAME BUILDING, SHOW THE FORM STYLE AND NUMBER OF THE EXISTING CONDITION.  
 1 - FLOOR FINISHES AND MATERIALS TO BE USED.  
 2 - FLOOR FINISHES AND MATERIALS TO BE USED.  
 3 - FINISH FINISHES AND MATERIALS TO BE USED.

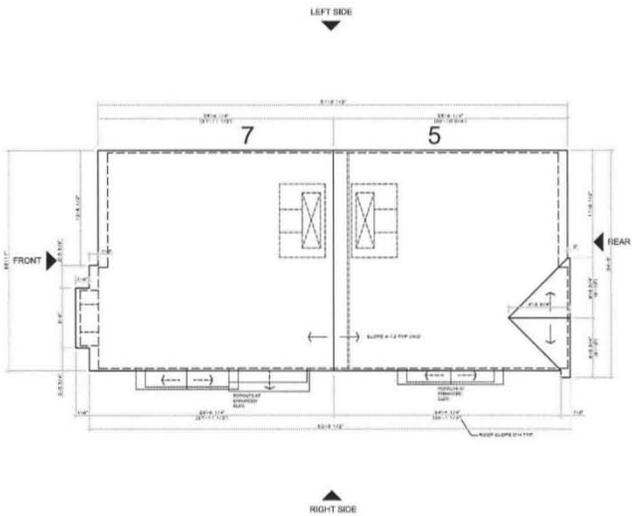
UNIT 5 & 7  
 FIRST / SECOND FLOOR PLANS  
 ELEVATION STYLE - 'A'

PROVENCE - COURTS  
 RC NATOMAS, LLC



1311

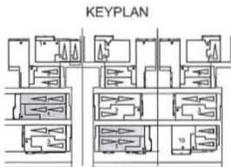
AREA ANALYSIS	
UNIT 5	
FIRST FLOOR AREA	1,062.00 S.F.
SECOND FLOOR	1,062.00 S.F.
TOTAL FLOOR	2,124.00 S.F.
TOTAL LAND AREA	1,062.00 S.F.
GARAGE	463.72 S.F.
DECK	463.72 S.F.
TOTAL AREA	1,525.80 S.F.
UNIT 7	
FIRST FLOOR AREA	1,062.00 S.F.
SECOND FLOOR	1,062.00 S.F.
TOTAL FLOOR	2,124.00 S.F.
TOTAL LAND AREA	1,062.00 S.F.
GARAGE	463.72 S.F.
DECK	463.72 S.F.
TOTAL AREA	1,525.80 S.F.



ROOF PLAN



THIRD FLOOR



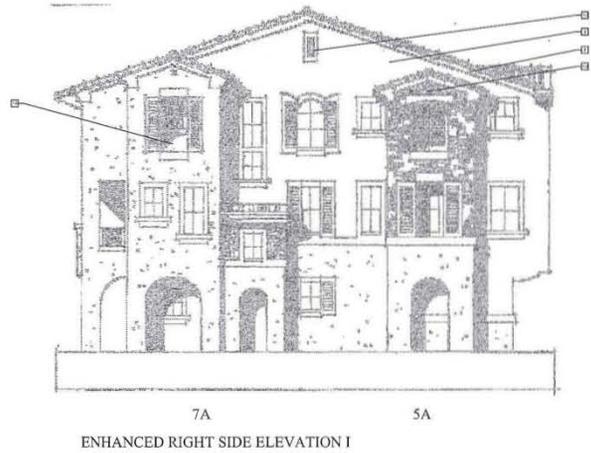
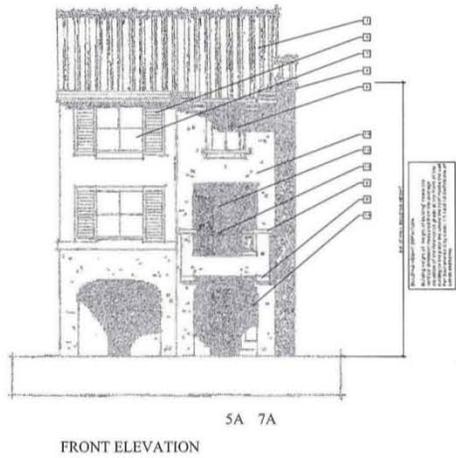
NOTE: REFER TO EXISTING'S CREATION/LOCATION ON THE SAME SIDE OF THE EAST BULLETS  
 ABOUT THE LINE STYLE AND SHADING FOR DIMENSIONED FEATURES.  
 1 - THIS IS A PRELIMINARY PLAN. SEE DRAWING 101-101 FOR THE FINAL PLAN.  
 2 - THIS IS A PRELIMINARY PLAN. SEE DRAWING 101-101 FOR THE FINAL PLAN.  
 3 - THIS IS A PRELIMINARY PLAN. SEE DRAWING 101-101 FOR THE FINAL PLAN.

UNIT 5 & 7  
 THIRD FLOOR / ROOF PLAN  
 ELEVATION STYLE - 'A'  
 1/8"=1'-0"  
**PROVENCE - COURTS**  
 RC NATOMAS, LLC



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AREA ANALYSIS	
UNIT 5	
FIRST FLOOR ENTRY	138.31 S.F.
SECOND FLOOR	481.16 S.F.
THIRD FLOOR	135.87 S.F.
TOTAL LIVING AREA	755.34 S.F.
CORRIDOR	453.12 S.F.
STAIR	48.00 S.F.
TOTAL UNIT	1256.52 S.F.
UNIT 7	
FIRST FLOOR ENTRY	138.31 S.F.
SECOND FLOOR	481.16 S.F.
THIRD FLOOR	844.03 S.F.
TOTAL LIVING AREA	1463.50 S.F.
CORRIDOR	287.08 S.F.
STAIR	89.08 S.F.
TOTAL UNIT	1840.11 S.F.



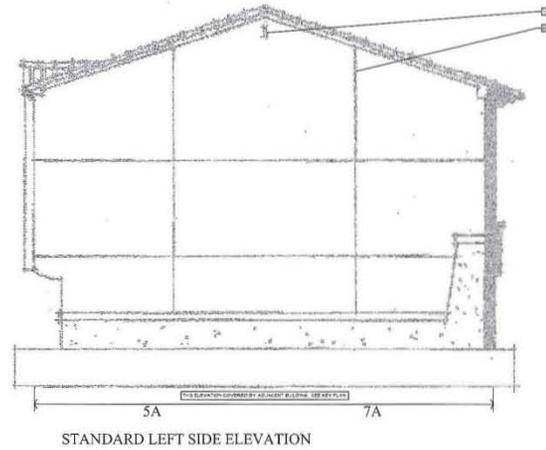
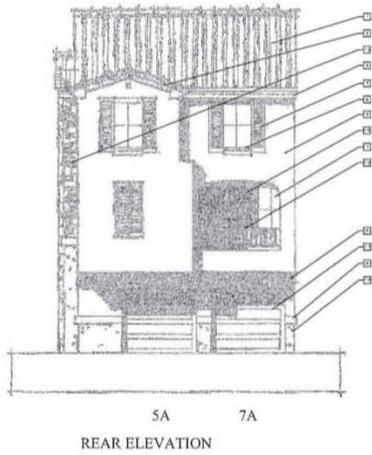
**A**

UNIT 5 & 7  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

157424P  
**PROVENCE - COURTS**  
RC NATOMAS, LLC  
200904020

1511

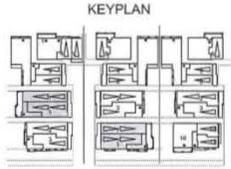
TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAFTER TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL RAIL / QUADRANT
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL RESTONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLIPING PLASTER BALL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED MALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



# A

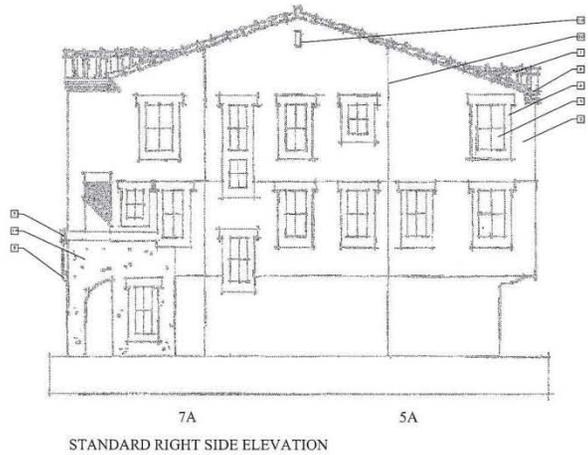
UNIT 5 & 7  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC



NOTES: REAR ELEVATION IS TO BE USED FOR THE SIDE WALL OF THE HOME INCLUDING SIDE WALLS AND PORCHES. THIS ELEVATION IS TO BE USED FOR THE SIDE WALLS AND PORCHES.  
1. THIS ELEVATION IS TO BE USED FOR THE SIDE WALLS AND PORCHES.  
2. THIS ELEVATION IS TO BE USED FOR THE SIDE WALLS AND PORCHES.  
3. THIS ELEVATION IS TO BE USED FOR THE SIDE WALLS AND PORCHES.

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERIOR COLOR TILE ROOF
<input type="checkbox"/>	INTERIOR COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAFTER TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL FLAG / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER BALL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED WALL
* ALL METAL AND MOOD ELEMENTS TO BE PAINTED	



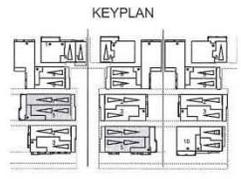
# A

UNIT 5 & 7  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC

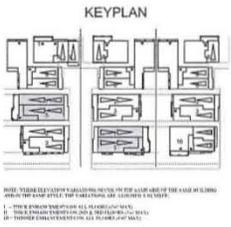
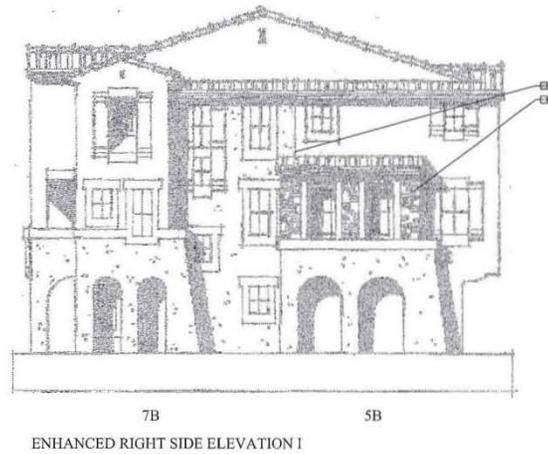
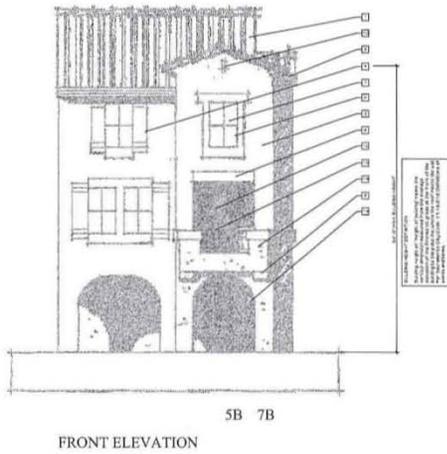


(S11)



NOTE: THESE ELEVATIONS ASSUMED TO BE ON THE SOUTH SIDE OF THE UNIT BEING SHOWN UNLESS OTHERWISE NOTED. THE ARCHITECT HAS CONDUCTED VISUAL VERIFICATION OF THE EXTERIOR ELEVATIONS.  
1 - FINISH ELEVATION SAMPLES AND ALL MATERIALS TO BE USED.  
2 - FINISH ELEVATION SAMPLES AND ALL MATERIALS TO BE USED.  
3 - FINISH ELEVATION SAMPLES AND ALL MATERIALS TO BE USED.

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR W/ TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	KAPFER TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL NAIL / SHARDRAL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLIDING PLASTER RAIL
<input type="checkbox"/>	HEAVY TEXTURE STICCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED WALL
*ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

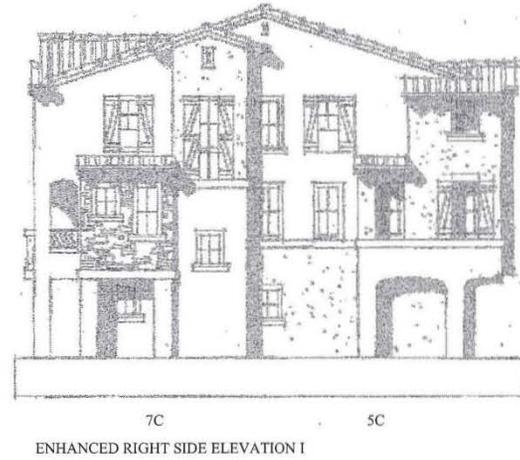
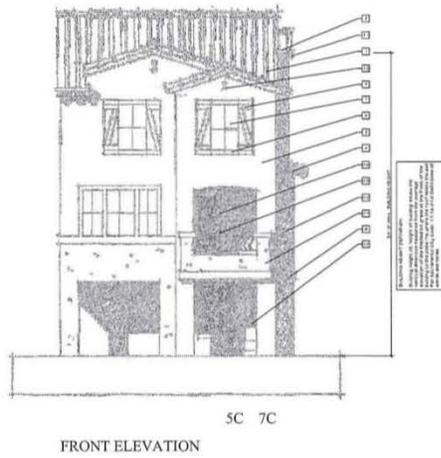


**B**  
 SEE 'A' ELEVATIONS FOR SIDES  
 UNIT 5 & 7  
 EXTERIOR ELEVATIONS  
 ELEVATION STYLE - 'B'  
 RC NATOMAS, LLC



TYPICAL BUILDING MATERIALS	
☐	INTERNAL COLOR 1" TILE ROOF
☐	INTERNAL COLOR PLASTER
☐	FASCIA
☐	RAKE BOARD
☐	PLASTER CORNICE
☐	PLASTER WINDOW TRIM
☐	VINYL WINDOW
☐	PLASTER TRIM
☐	DECORATIVE SHUTTER
☐	DECORATIVE PLASTER PROJECTION
☐	RAFTER TAIL
☐	STONE PATTERN VENEER
☐	METAL RAIL / SHADROWAL
☐	THERMAL-BREAK DOOR
☐	SLIDING GLASS DOOR
☐	FRENCH DOOR
☐	METAL REGIONAL GARAGE DOOR WITH WINDOW
☐	BUFFING PLASTER SILL
☐	HEAVY TEXTURE STUCCO
☐	EXPANSION JOINT
☐	PLASTER BATTERED RAIL
*ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

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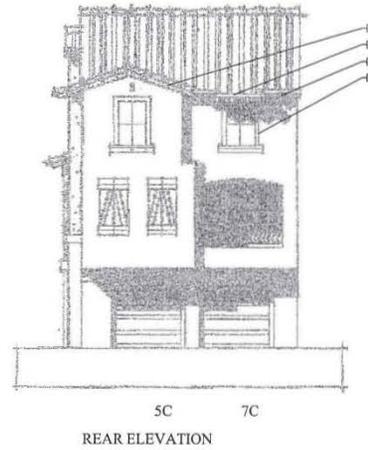
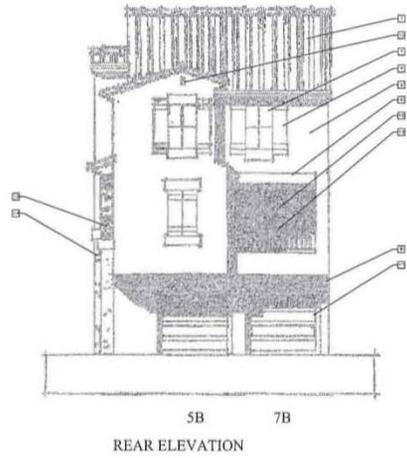


**C**  
 SEE 'A' ELEVATIONS FOR SIDES  
 UNIT 5 & 7  
 EXTERIOR ELEVATIONS  
 ELEVATION STYLE - 'C'  
 10/14/11  
**PROVENCE - COURTS**  
 RC NATOMAS, LLC  
 200904-030



13.11

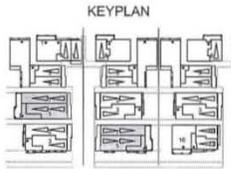
TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR 9" TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAPPEL TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL BAL. GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOW
<input type="checkbox"/>	SLEEPING PLASTER HILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED HALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



# B,C

UNIT 5 & 7  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B' & 'C'

PROVENCE - COURTS  
RC NATOMAS, LLC



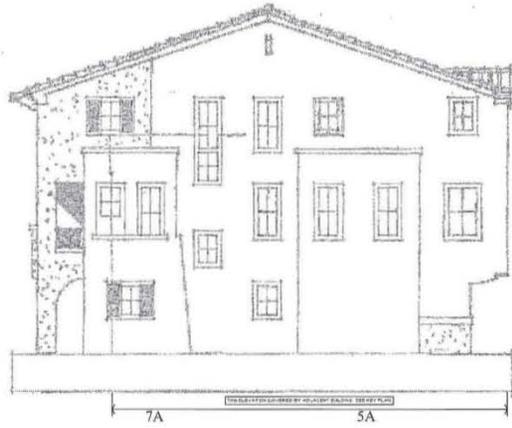
NOTE: THESE ELEVATIONS ARE DRAWINGS ONLY AND ARE NOT TO BE USED FOR THE ASSESSMENT OF THE ASSESSMENT. ANY OTHER INFORMATION NOT SHOWN OR INDICATED IS TO BE OBTAINED FROM THE ARCHITECT'S RECORD DRAWINGS.

1 - THIS DRAWING IS THE PROPERTY OF THE ARCHITECT AND IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM.

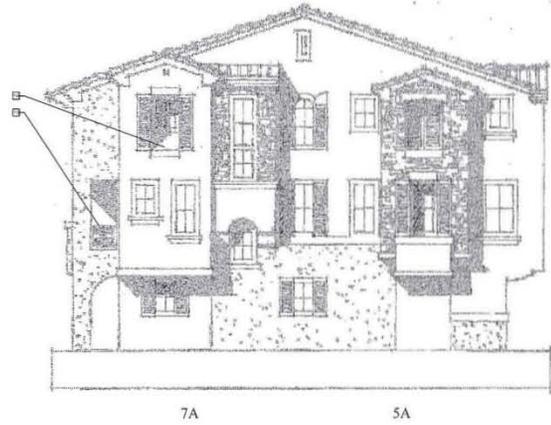
2 - THIS DRAWING IS THE PROPERTY OF THE ARCHITECT AND IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM.

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL, COLOR 'B' TILE ROOF
<input type="checkbox"/>	INTERNAL, COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAPPER TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS BUTY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLIPPER PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED PAUL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

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RIGHT SIDE ELEVATION III



ENHANCED RIGHT SIDE ELEVATION II

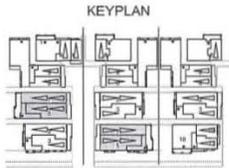
# A

UNIT 5 & 7  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC

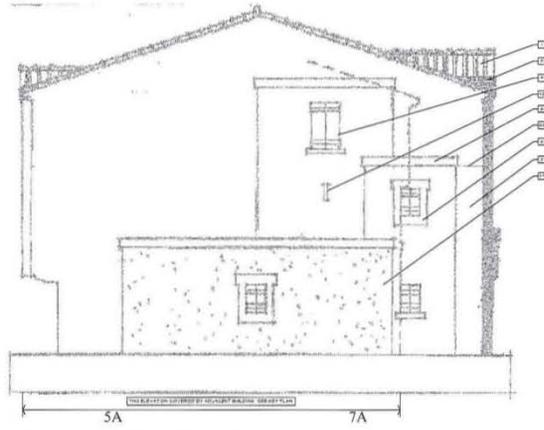


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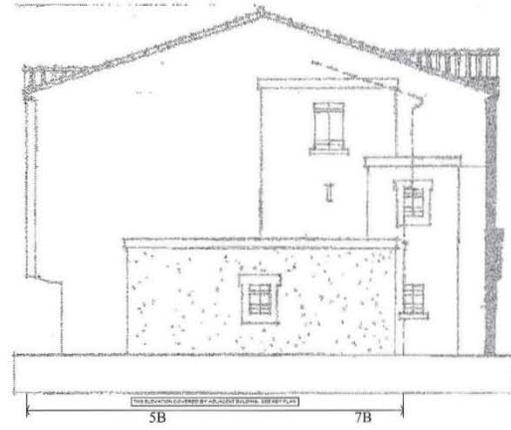


NOTES: THESE ELEVATIONS ARE SUBMITTED FOR THE USE OF THE ARCHITECT AND THE CONTRACTOR. THE CONTRACTOR SHALL VERIFY THE ACCURACY OF THE INFORMATION AND DIMENSIONS SHOWN. ALL DIMENSIONS SHALL BE IN FEET AND INCHES. ALL DIMENSIONS SHALL BE TO FACE UNLESS OTHERWISE NOTED.

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR 1/2" TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNER
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAFTER TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL RAIL / SUBSTRATE
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLIDING PLASTER WALL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED WALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



LEFT SIDE ELEVATION III - 'A'



LEFT SIDE ELEVATION III - 'B'

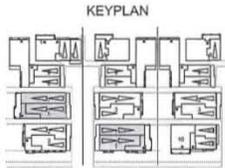
# A,B

UNIT 5 & 7  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A' & 'B'

PROVENCE - COURTS  
RC NATOMAS, LLC



1.5.11



NOTE: THESE ELEVATIONS ARE SUBMITTED FOR THE USE OF THE CASE IN ORDER TO OBTAIN THE NECESSARY PERMITS FROM THE CITY OF HOUSTON, TEXAS.

1 - THIS IS A STYLED ELEVATION AND SHOULD NOT BE USED FOR CONSTRUCTION.

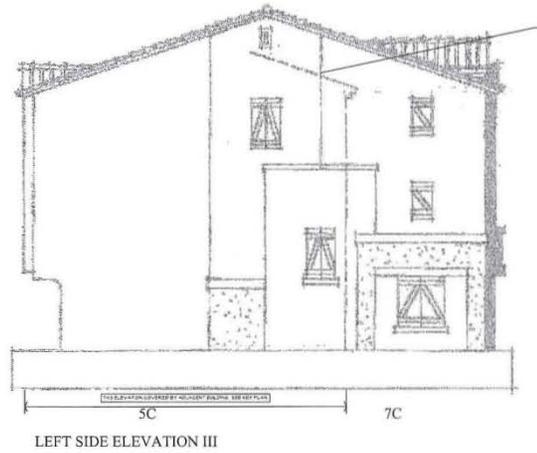
2 - THIS IS A STYLED ELEVATION AND SHOULD NOT BE USED FOR CONSTRUCTION.

3 - THIS IS A STYLED ELEVATION AND SHOULD NOT BE USED FOR CONSTRUCTION.

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR V. TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER PENDUL TRIM
<input type="checkbox"/>	VINYL PENDUL
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAFTER TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL SEAL / BRACKET
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH PENDULS
<input type="checkbox"/>	SLIPPER PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED RAIL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

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# C

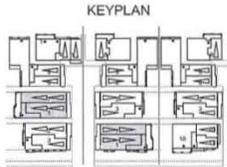
SEE 'A' ELEVATIONS FOR STANDARD SIDES

UNIT 5 & 7  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

PROVENCE - COURTS  
RC NATOMAS, LLC



LS11



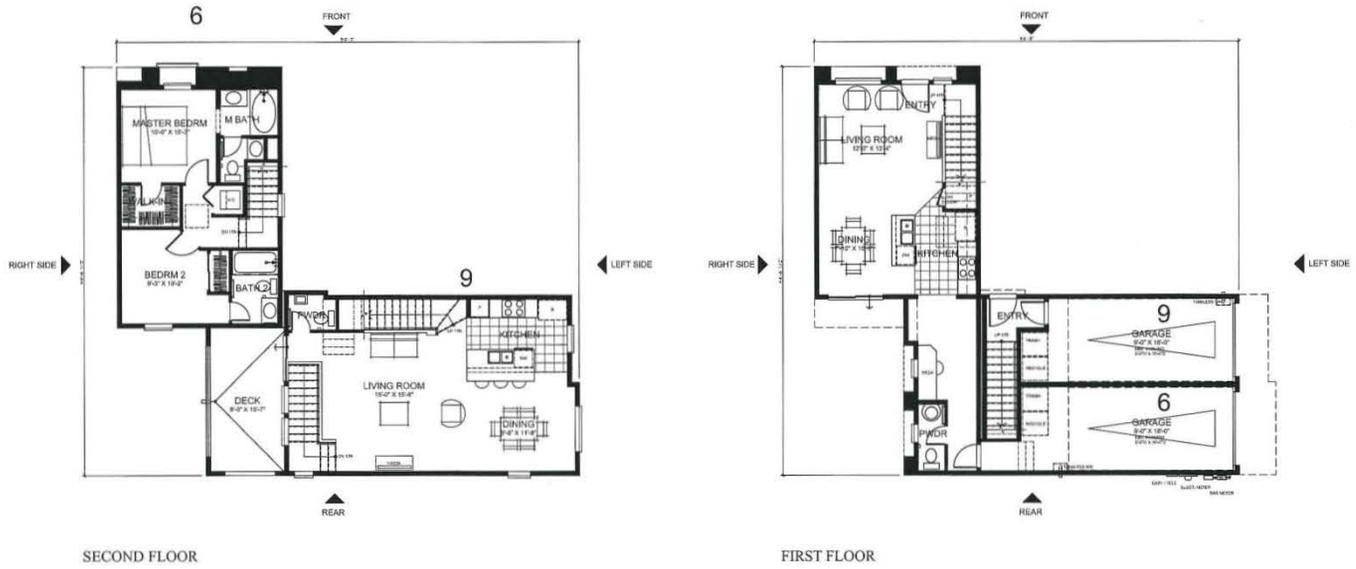
NOTE: THESE ELEVATIONS ARE FOR INFORMATION ONLY. THE EXACT APPEARANCE OF THE HOME INCLUDING MATERIALS AND FINISHES WILL BE DETERMINED BY THE ARCHITECT AND CONTRACTOR.

1 - THIS IS A GENERAL REPRESENTATION OF THE HOME INCLUDING MATERIALS AND FINISHES. THE EXACT APPEARANCE OF THE HOME WILL BE DETERMINED BY THE ARCHITECT AND CONTRACTOR.

2 - THESE ELEVATIONS ARE FOR INFORMATION ONLY. THE EXACT APPEARANCE OF THE HOME WILL BE DETERMINED BY THE ARCHITECT AND CONTRACTOR.

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR V-TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	PANEA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORBEL
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE BUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAFTER TAIL
<input type="checkbox"/>	STONE PATTERN VENER
<input type="checkbox"/>	METAL MAIL / SHARDMAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER BELT
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED WALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

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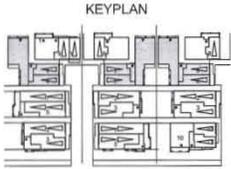


SECOND FLOOR

FIRST FLOOR

UNIT 6 & 9  
FIRST / SECOND FLOOR PLANS  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC

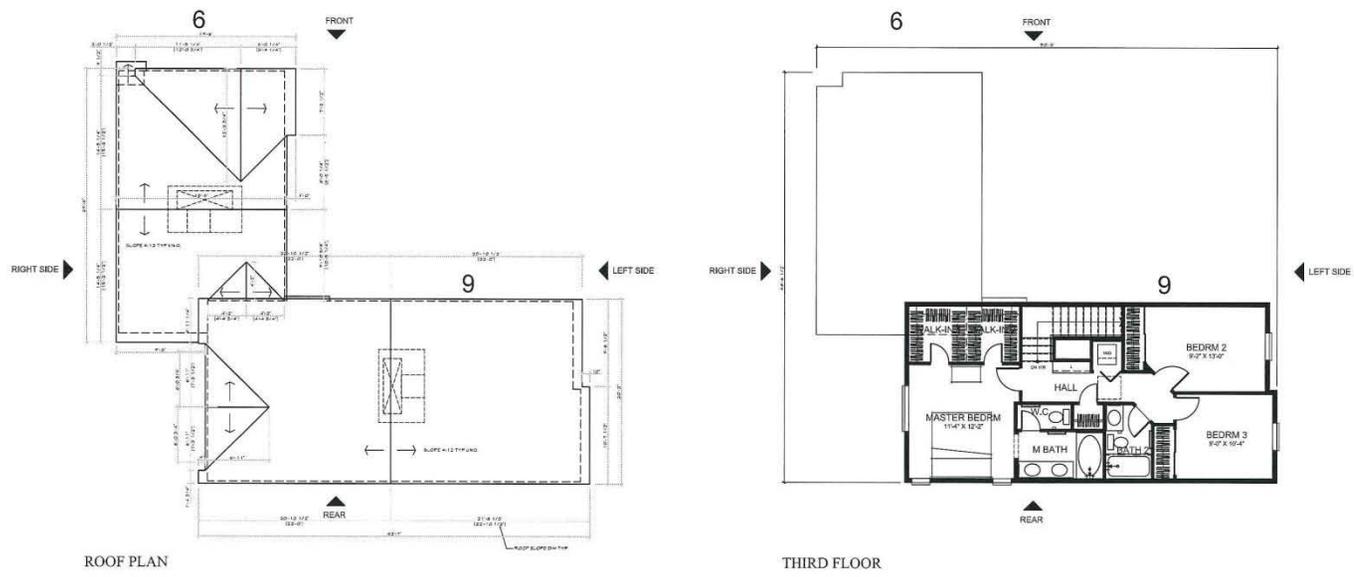


NOTE: WHERE ELEVATION IS INDICATED ON THE GROUND OF THE FLOOR PLANS, REFER TO THE ELEVATION SHEET FOR THE CORRECT ELEVATION.  
 1. - THESE PLANS ARE FOR INFORMATION ONLY.  
 2. - THESE PLANS ARE NOT TO BE USED FOR CONSTRUCTION.  
 3. - THESE PLANS ARE NOT TO BE USED FOR ANY OTHER PURPOSE.



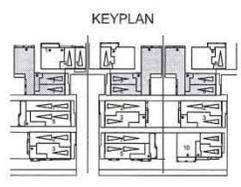
1.1.1

AREA ANALYSIS	
<b>UNIT 6</b>	
FIRST FLOOR	807 SF ±
SECOND FLOOR	492 SF ±
TOTAL LIVING AREA	1299 SF ±
GARAGE	336 SF ±
TOTAL AREA	1635 SF ±
<b>UNIT 9</b>	
FIRST FLOOR	700 SF ±
SECOND FLOOR	440 SF ±
TOTAL LIVING AREA	1140 SF ±
GARAGE	324 SF ±
TOTAL AREA	1464 SF ±
TOTAL AREA	3099 SF ±



ROOF PLAN

THIRD FLOOR



NOTE: WHERE ELEVATION IS INDICATED ON THE SAME SIDE OF THE SAME WALL, THE TOP ELEVATION IS FOR THE EXTERIOR AND THE BOTTOM ELEVATION IS FOR THE INTERIOR.  
 1 - SINGLE FINISH PARTITION AS SHOWN.  
 2 - DOUBLE FINISH PARTITION AS SHOWN.  
 3 - FINISH PARTITION ON ONE SIDE ONLY.  
 4 - FINISH PARTITION ON BOTH SIDES.

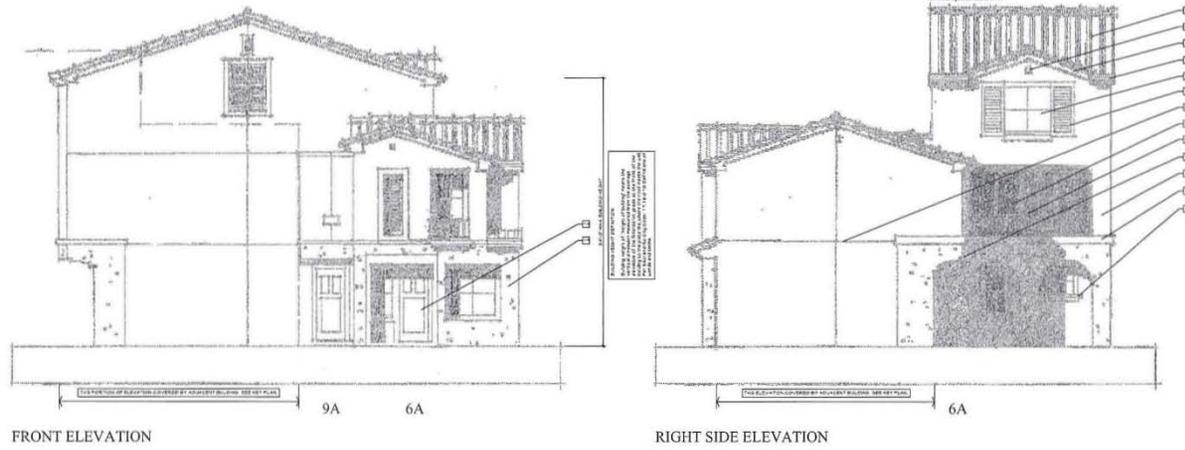
UNIT 6 & 9  
 THIRD FLOOR / ROOF PLAN  
 ELEVATION STYLE - 'A'

PROVENCE - COURTS  
 RC NATOMAS, LLC



LS11

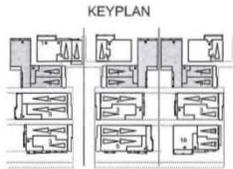
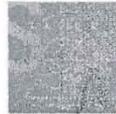
AREA ANALYSIS	
<b>UNIT 6</b>	
THIRD FLOOR	867.45 S.F.
ROOF PLAN	412.21 S.F.
<b>TOTAL LIVING AREA</b>	<b>1279.66 S.F.</b>
STAIRS	100.00 S.F.
<b>NET AREA</b>	<b>1379.66 S.F.</b>
<b>UNIT 9</b>	
THIRD FLOOR	70.35 S.F.
ROOF PLAN	341.55 S.F.
<b>TOTAL LIVING AREA</b>	<b>412.90 S.F.</b>
<b>TOTAL LIVING AREA</b>	<b>1692.56 S.F.</b>
STAIRS	100.00 S.F.
<b>NET AREA</b>	<b>1792.56 S.F.</b>



A

UNIT 6 & 9  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

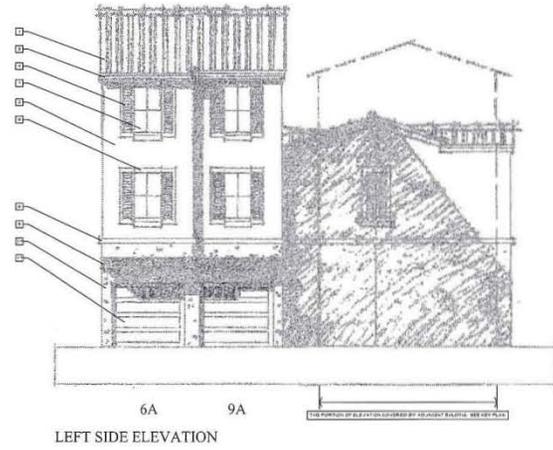
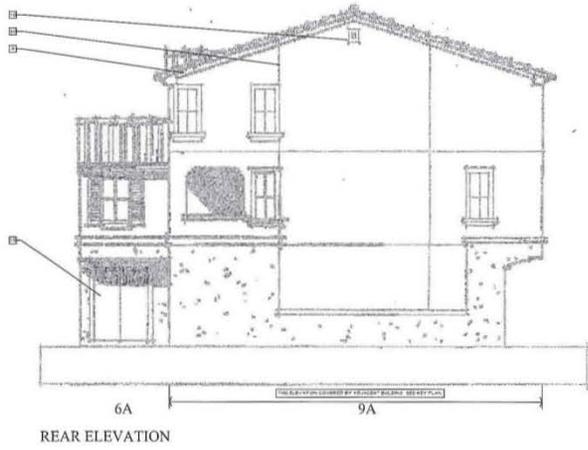
PROVENCE - COURTS  
RC NATOMAS, LLC



NOTE: REFER TO SECTION 05100 FOR THE FINISHES OF THE HARD WEAR SURFACES IN THIS ELEVATION. THE FINISHES ARE SUBJECT TO CHANGE.  
1. THE FINISHES SHOWN ON THIS ELEVATION ARE SUBJECT TO CHANGE.  
2. THE FINISHES SHOWN ON THIS ELEVATION ARE SUBJECT TO CHANGE.  
3. THE FINISHES SHOWN ON THIS ELEVATION ARE SUBJECT TO CHANGE.

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR W/ TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAFTER TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL RAIL / SWANSEAS
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLUING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER HILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED WALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

36 OF 62



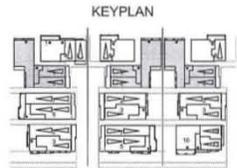
# A

UNIT 6 & 9  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC



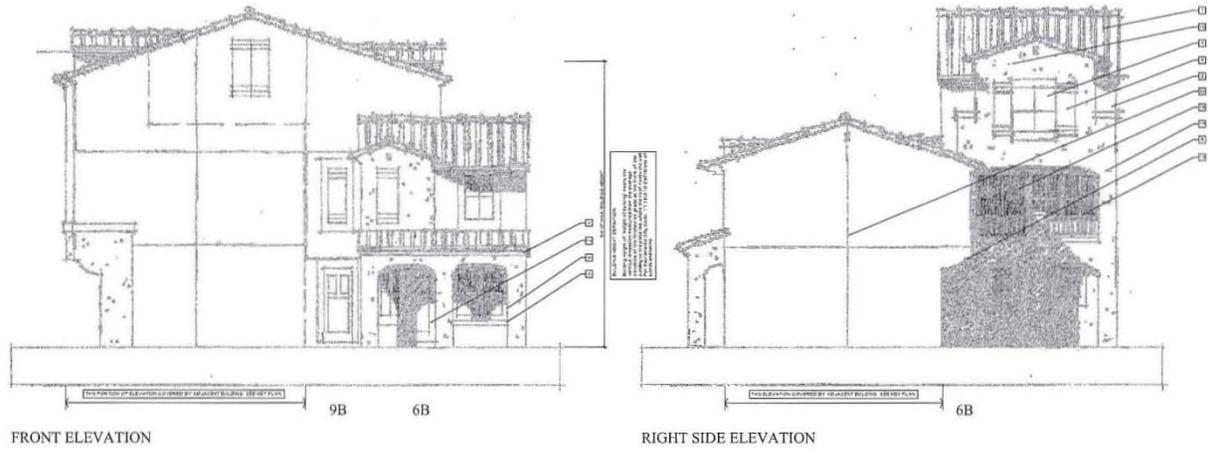
1.5.11



NOTE: THESE ELEVATION VIEWS ARE INTENDED TO SHOW THE APPEARANCE OF THE BUILT BUILDING AND DO NOT REPRESENT THE EXACT APPEARANCE OF THE BUILT BUILDING.  
1. THIS ELEVATION REPRESENTS THE EXTERIOR OF THE BUILDING.  
2. THIS ELEVATION REPRESENTS THE EXTERIOR OF THE BUILDING.  
3. THIS ELEVATION REPRESENTS THE EXTERIOR OF THE BUILDING.  
4. THIS ELEVATION REPRESENTS THE EXTERIOR OF THE BUILDING.

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	NEUTRAL COLOR 1" TILE ROOF
<input type="checkbox"/>	NEUTRAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PRODUCTION
<input type="checkbox"/>	RAFTER TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL RAIL / SHARDING
<input type="checkbox"/>	FIBER-GLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLIPING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED WALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

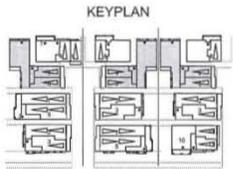
37 OF 62



# B

UNIT 6 & 9  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'

PROVENCE - COURTS  
RC NATOMAS, LLC

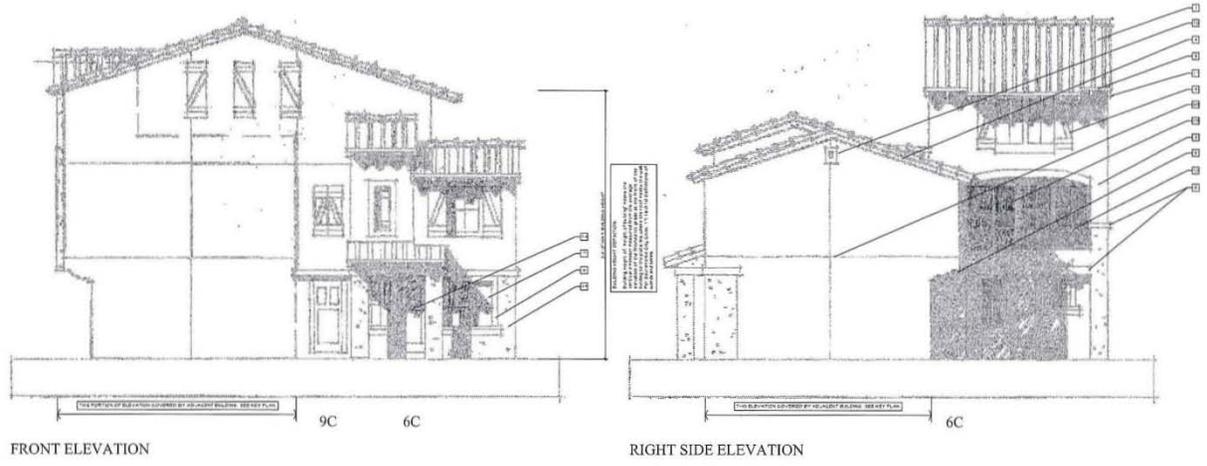


NOTE: THESE ELEVATIONS ARE INTENDED FOR INFORMATION ONLY AND ARE NOT TO BE USED FOR CONSTRUCTION. ALL DIMENSIONS ARE IN FEET AND INCHES. UNLESS OTHERWISE NOTED, ALL DIMENSIONS ARE TO FACE. ALL MATERIALS AND FINISHES ARE TO BE AS SHOWN. ALL MATERIALS AND FINISHES ARE TO BE AS SHOWN. ALL MATERIALS AND FINISHES ARE TO BE AS SHOWN.

1/2"=1'-0"



TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR W/ TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER FRENCH TRIM
<input type="checkbox"/>	VIVIL FRENCH
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	KAFFER TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL RAIL / HANDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER BALL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED WALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



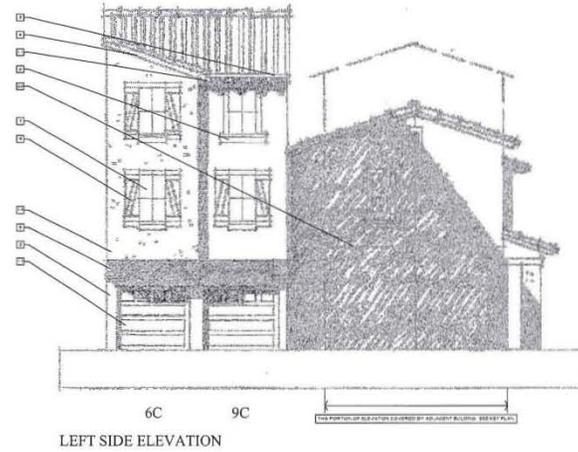
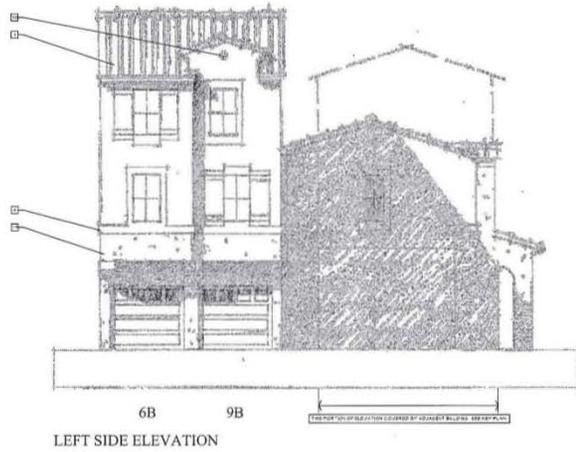
C

UNIT 6 & 9  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - C

PROVENCE - COURTS  
RC NATOMAS, LLC



TYPICAL BUILDING MATERIALS	
[Symbol]	INTERNAL COLOR 3/4" TILE ROOF
[Symbol]	INTERNAL COLOR PLASTER
[Symbol]	FASCIA
[Symbol]	RAKE BOARD
[Symbol]	PLASTER CORBEL
[Symbol]	PLASTER FINISH TRIM
[Symbol]	VINYL FINISH TRIM
[Symbol]	PLASTER TRIM
[Symbol]	DECORATIVE SHUTTER
[Symbol]	DECORATIVE PLASTER PROJECTION
[Symbol]	KAFTER TAIL
[Symbol]	STONE PATTERN VENEER
[Symbol]	METAL RAIL / GUARDRAIL
[Symbol]	FIBERGLASS ENTRY DOOR
[Symbol]	SLIDING GLASS DOOR
[Symbol]	FRENCH DOOR
[Symbol]	METAL SECTIONAL GARAGE DOOR WITH PENDORS
[Symbol]	SLIDING PLASTER BALL
[Symbol]	HEAVY TEXTURE STUCCO
[Symbol]	EXPANSION JOINT
[Symbol]	PLASTER BATTERED WALL
* ALL METAL AND FLOOR ELEMENTS TO BE PAINTED	



# B,C

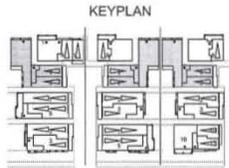
SEE 'A' ELEVATIONS FOR REARS

UNIT 6 & 9  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B' & 'C'

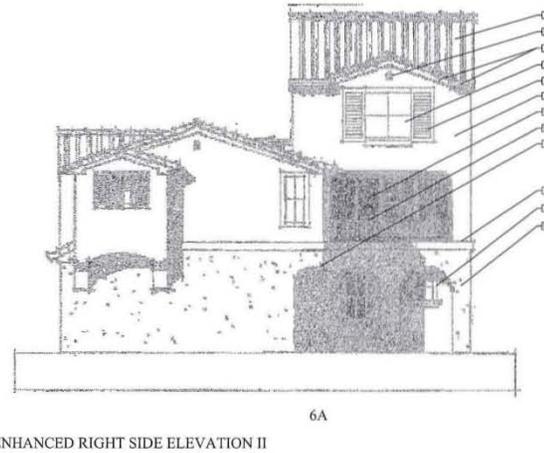
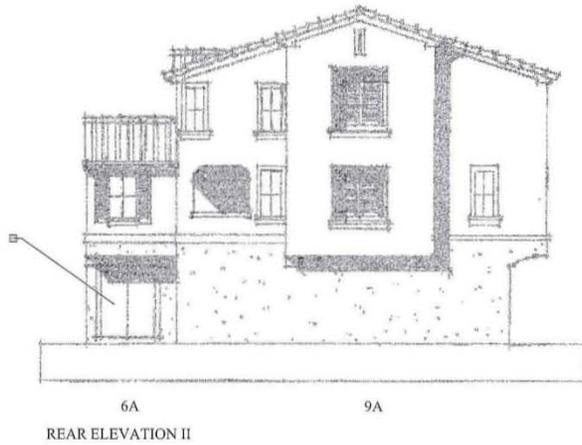
PROVENCE - COURTS  
RC NATOMAS, LLC



LS11



NOTE: THESE ELEVATIONS ARE FOR INFORMATION ONLY. THE EXACT APPEARANCE OF THE HOME WILL BE DETERMINED BY THE ARCHITECT AND CONSTRUCTION TEAM.  
1 - THIS DRAWING REPRESENTS ALL UNITS (2A-2F, 3A-3F, 4A-4F, 5A-5F, 6A-6F, 7A-7F, 8A-8F, 9A-9F, 10A-10F, 11A-11F, 12A-12F, 13A-13F, 14A-14F, 15A-15F, 16A-16F, 17A-17F, 18A-18F, 19A-19F, 20A-20F, 21A-21F, 22A-22F, 23A-23F, 24A-24F, 25A-25F, 26A-26F, 27A-27F, 28A-28F, 29A-29F, 30A-30F, 31A-31F, 32A-32F, 33A-33F, 34A-34F, 35A-35F, 36A-36F, 37A-37F, 38A-38F, 39A-39F, 40A-40F, 41A-41F, 42A-42F, 43A-43F, 44A-44F, 45A-45F, 46A-46F, 47A-47F, 48A-48F, 49A-49F, 50A-50F, 51A-51F, 52A-52F, 53A-53F, 54A-54F, 55A-55F, 56A-56F, 57A-57F, 58A-58F, 59A-59F, 60A-60F, 61A-61F, 62A-62F, 63A-63F, 64A-64F, 65A-65F, 66A-66F, 67A-67F, 68A-68F, 69A-69F, 70A-70F, 71A-71F, 72A-72F, 73A-73F, 74A-74F, 75A-75F, 76A-76F, 77A-77F, 78A-78F, 79A-79F, 80A-80F, 81A-81F, 82A-82F, 83A-83F, 84A-84F, 85A-85F, 86A-86F, 87A-87F, 88A-88F, 89A-89F, 90A-90F, 91A-91F, 92A-92F, 93A-93F, 94A-94F, 95A-95F, 96A-96F, 97A-97F, 98A-98F, 99A-99F, 100A-100F, 101A-101F, 102A-102F, 103A-103F, 104A-104F, 105A-105F, 106A-106F, 107A-107F, 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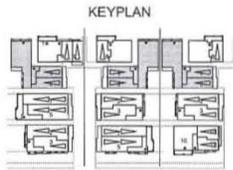
REAR ELEVATION II

ENHANCED RIGHT SIDE ELEVATION II

# A

UNIT 6 & 9  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC



NOTE: THESE ELEVATION DRAWINGS SHALL BE FOR INFORMATION ONLY AND NOT TO BE USED FOR CONSTRUCTION. ALL DIMENSIONS SHALL BE AS SHOWN UNLESS OTHERWISE NOTED.  
1 - THIS ELEVATION SHALL BE FOR INFORMATION ONLY.  
2 - THIS ELEVATION SHALL BE FOR INFORMATION ONLY.  
3 - TURNER SHALL VERIFY ALL DIMENSIONS.

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOUR 9" TILE ROOF
<input type="checkbox"/>	INTERNAL COLOUR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER GIMBREL
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAPPER TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBREGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL RESTONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLEEPING PLASTER BALL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED MALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

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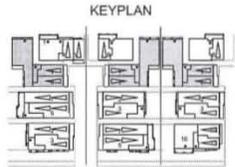


# B

SEE 'A' ELEVATIONS FOR STANDARD SIDES

UNIT 6 & 9  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'

PROVENCE - COURTS  
RC NATOMAS, LLC



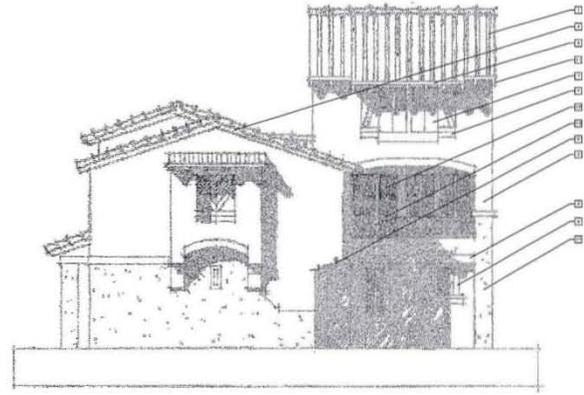
NOTE: REAR ELEVATION PARALLEL TO THE SIDE WALL OF THE BASE BUILDING  
UNITS 6 & 9 ARE STYLED FOR CONSTRUCTION OF CONCRETE & STONE.

1. THIS ELEVATION REPRESENTS ALL PROPOSED DETAILS.  
2. THIS ELEVATION REPRESENTS ONE OF THE PROPOSED DETAILS.  
3. THIS ELEVATION REPRESENTS ONE OF THE PROPOSED DETAILS.

TYPICAL BUILDING MATERIALS	
☐	INTERIAL COLOR W/ TILE ROOF
☐	INTERIAL COLOR PLASTER
☐	FASCIA
☐	RAKE BOARD
☐	PLASTER CORNICE
☐	PLASTER WINDOW TRIM
☐	VINYL WINDOW
☐	PLASTER TRIM
☐	DECORATIVE SHUTTER
☐	DECORATIVE PLASTER PROJECTION
☐	KAPFER TAIL
☐	STONE PATTERN VENEER
☐	METAL HALL / SHARDING
☐	FRIGGLASS ENTRY DOOR
☐	SLIDING GLASS DOOR
☐	FRENCH DOOR
☐	METAL SECTIONAL GARAGE DOOR WITH WINDOW
☐	SLIDING PLASTER RILL
☐	HEAVY TEXTURE STUCCO
☐	EXPANSION JOINT
☐	PLASTER BATTERED WALL
* ALL METAL AND MOOD ELEMENTS TO BE PAINTED	



FRONT ELEVATION II



ENHANCED RIGHT SIDE ELEVATION II

C

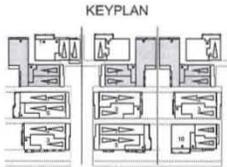
SEE 'A' ELEVATIONS FOR STANDARD SIDES

UNIT 6 & 9  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

PROVENCE - COURTS  
RC NATOMAS, LLC



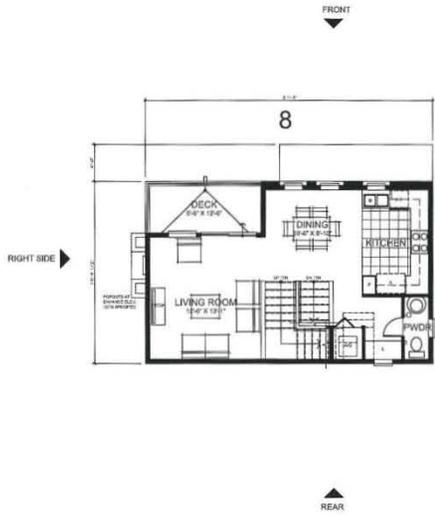
L111



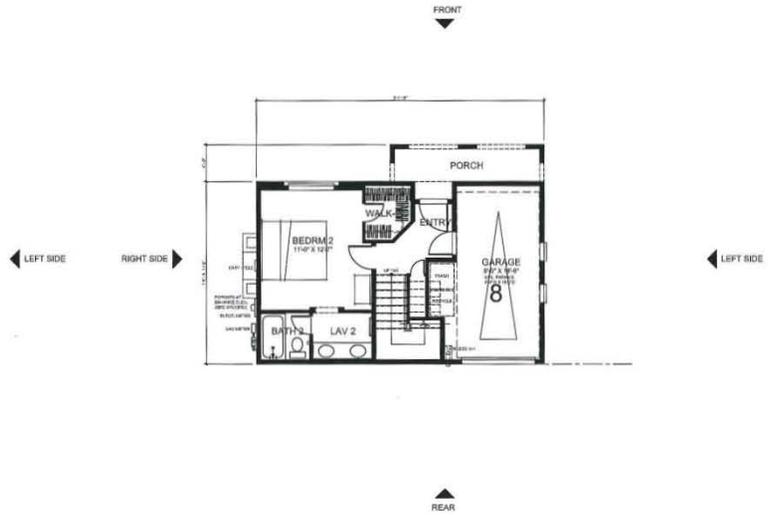
NOTE: THESE ELEVATIONS ARE INTENDED TO BE USED AS A GUIDE ONLY. THE ARCHITECT ASSUMES NO RESPONSIBILITY FOR THE ACCURACY OF THE INFORMATION PROVIDED HEREIN. THE ARCHITECT ASSUMES NO RESPONSIBILITY FOR THE ACCURACY OF THE INFORMATION PROVIDED HEREIN.

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR 9" TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RANS BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER FINISH TRIM
<input type="checkbox"/>	VINYL FINISH
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAFTER TAIL
<input type="checkbox"/>	STONE PATTERN VENER
<input type="checkbox"/>	METAL RAIL / BOARDING
<input type="checkbox"/>	FRIBGLASS ENTRY DOOR
<input type="checkbox"/>	SLING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOW
<input type="checkbox"/>	SLIPING PLASTER BALL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED WALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

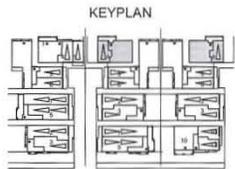
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SECOND FLOOR



FIRST FLOOR



NOTE: THESE PLANS ARE FOR INFORMATION ONLY. THE OWNER SHALL BE RESPONSIBLE FOR VERIFYING THE ACCURACY OF ALL DIMENSIONS AND CONDITIONS. THE ARCHITECT SHALL NOT BE RESPONSIBLE FOR ANY ERRORS OR OMISSIONS. THE ARCHITECT SHALL NOT BE RESPONSIBLE FOR ANY CONSTRUCTION DEFECTS OR DELAYS. THE ARCHITECT SHALL NOT BE RESPONSIBLE FOR ANY OTHER MATTERS NOT SPECIFICALLY MENTIONED IN THESE PLANS.

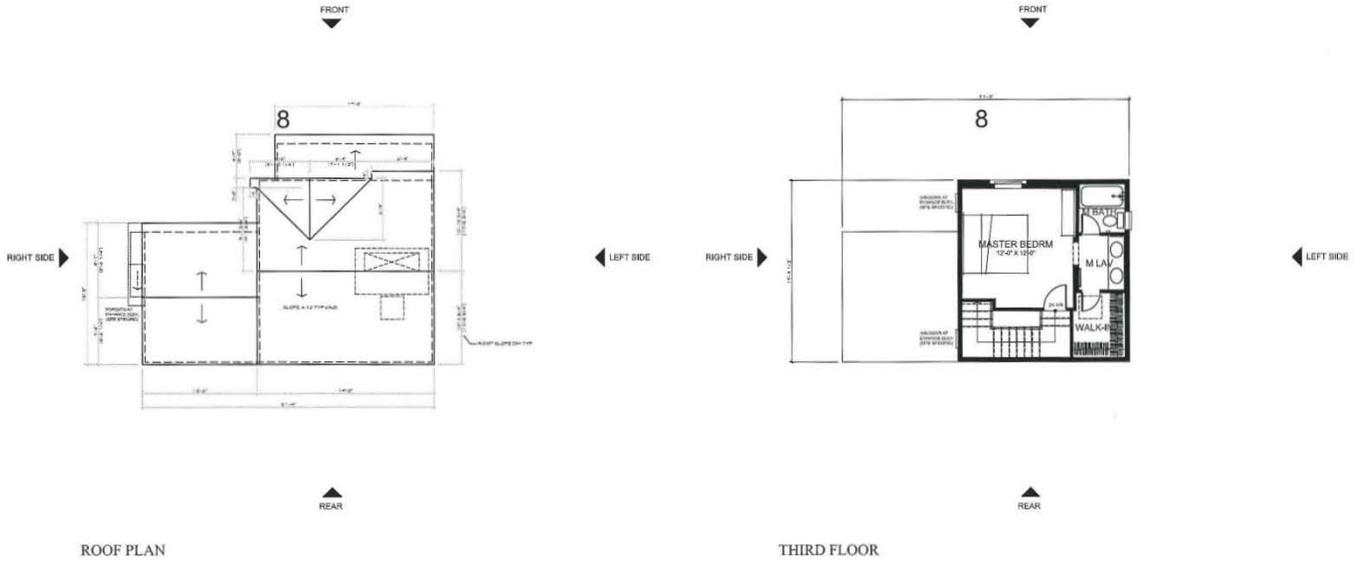
UNIT 8  
FIRST / SECOND FLOOR PLANS  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC



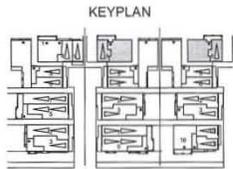
1.1.11

AREA ANALYSIS	
UNIT 8	367.74 S.F.
FIRST FLOOR	241.92 S.F.
SECOND FLOOR	322.89 S.F.
TOTAL LIVING AREA	426.88 S.F.
DECK	96.00 S.F.
PORCH	75.00 S.F.
TOTAL AREA	597.97 S.F.



ROOF PLAN

THIRD FLOOR



NOTE: DIMENSIONS OF UNITS AND COMMON AREAS ARE THE SAME AS SET OUT IN THE UNIT AND COMMON AREAS SCHEDULES OF THE CONDOMINIUM PLAN. DIMENSIONS OF COMMON AREAS ARE THE SAME AS SET OUT IN THE COMMON AREAS SCHEDULE OF THE CONDOMINIUM PLAN.

1 - DIMENSIONS SHOWN ON ALL PLANS. DO NOT SCALE.

2 - DIMENSIONS SHOWN ON ALL PLANS. DO NOT SCALE.

3 - DIMENSIONS SHOWN ON ALL PLANS. DO NOT SCALE.

UNIT 8  
 THIRD FLOOR / ROOF PLAN  
 ELEVATION STYLE - 'A'

1/4" = 1'-0"

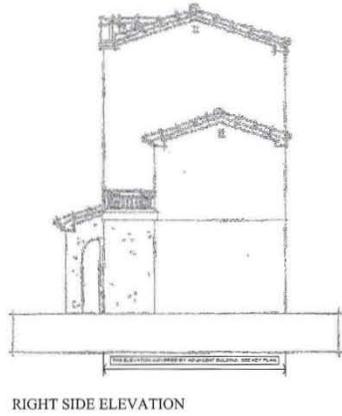
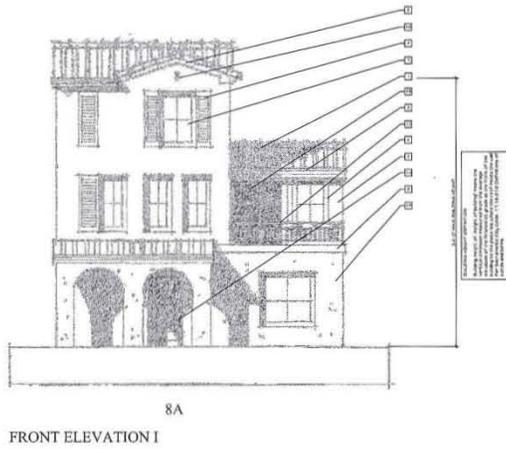
**PROVENCE - COURTS**  
 RC NATOMAS, LLC

3000HQ10



1.1.1

AREA ANALYSIS	
UNIT 8	357.78 S.F.
MASTER BEDRM	144.00 S.F.
MASTER BATH	32.00 S.F.
MASTER LAV	13.78 S.F.
WALK-IN	64.00 S.F.
TOTAL UNIT AREA	357.78 S.F.
COMMON	76.80 S.F.
TOTAL AREA	434.58 S.F.



# A

UNIT 8  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC



1:1

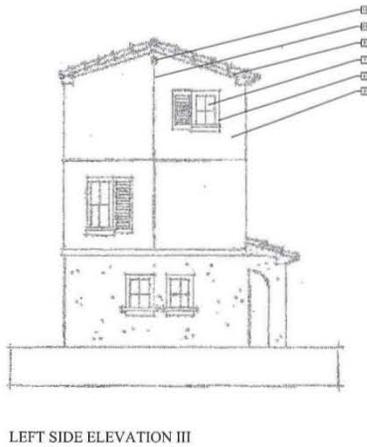
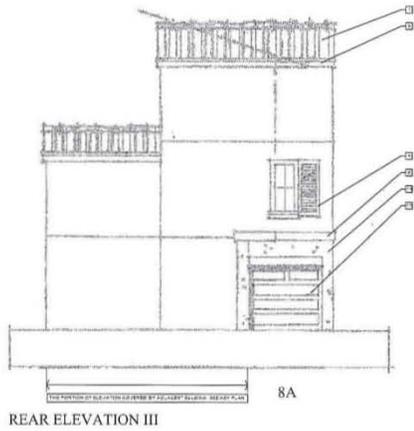
**KEYPLAN**

NOTE: THESE REPRESENTATIONS ARE FOR INFORMATION ONLY AND ARE NOT TO BE USED FOR CONSTRUCTION OF THE BUILDING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL DIMENSIONS ON THE GROUND.

1 - SEE & DOUBLE CHECK ALL DIMENSIONS ON THIS DRAWING.  
2 - THIS DRAWING IS TO BE USED FOR CONSTRUCTION OF THE BUILDING.  
3 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL DIMENSIONS ON THE GROUND.

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	FABRIC
<input type="checkbox"/>	RAISE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER ARCH/CH/TECH
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	KAPTEK TAIL
<input type="checkbox"/>	STONE PATTERN VENER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOW
<input type="checkbox"/>	SLIPING PLASTER ALL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED PILL
*ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

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**A**

UNIT 8  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC

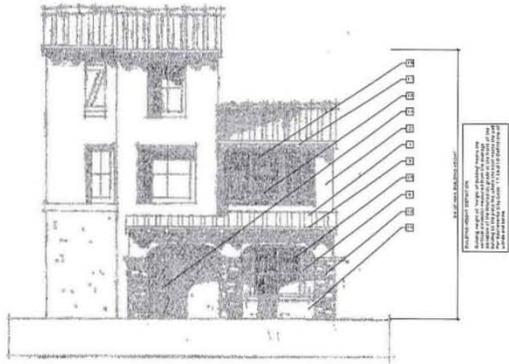


**KEYPLAN**

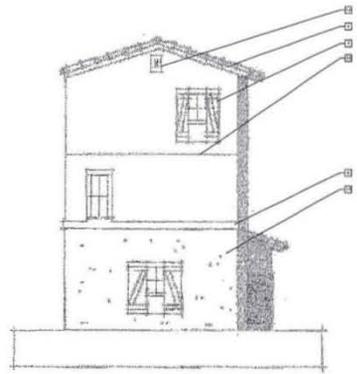
NOTE: THESE EXTERIOR ELEVATIONS APPLY TO THE EXTERIOR OF THE UNIT AS SHOWN WITH THE EXTERIOR FINISHES AND MATERIALS TO BE USED.

- 1 - THIS KEY PLAN SHOWS THE LOCATION OF ALL EXTERIOR WALLS.
- 2 - THIS KEY PLAN SHOWS THE LOCATION OF ALL EXTERIOR DOORS.
- 3 - THIS KEY PLAN SHOWS THE LOCATION OF ALL EXTERIOR WINDOWS.

TYPICAL BUILDING MATERIALS	
	INTERNAL COLUMN TILE ROOF
	INTERNAL COLUMN PLASTER
	FASCIA
	RAKE BOARD
	PLASTER CORNER
	PLASTER WINDOW TRIM
	VINYL WINDOW
	PLASTER TRIM
	DECORATIVE SHUTTER
	DECORATIVE PLASTER PROJECTION
	RAFTER TAIL
	STONE PATTERN VENEER
	METAL SEAL / GASKETING
	FIBERGLASS ENTRY DOOR
	SLIDING GLASS DOOR
	FRENCH DOOR
	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
	SLOPING PLASTER SILL
	HEAVY TEXTURE STUCCO
	EXPANSION JOINT
	PLASTER BATTENED WALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



8C  
FRONT ELEVATION I



LEFT SIDE ELEVATION III

C

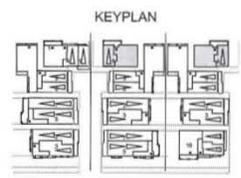
SEE 'A' ELEVATIONS FOR RIGHT SIDE & REAR

UNIT 8  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'  
(8B DOES NOT OCCUR)

PROVENCE - COURTS  
RC NATOMAS, LLC

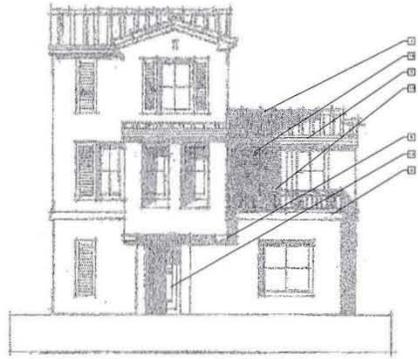


1.1.1

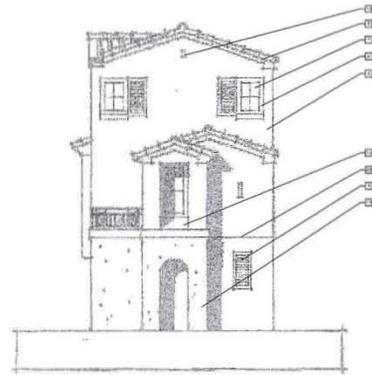


NOTE: THESE ELEVATIONS ARE FOR INFORMATION ONLY AND ARE NOT TO BE USED FOR CONSTRUCTION. ALL DIMENSIONS ARE IN FEET AND INCHES.  
1 - THIS IS A SHOWN DIMENSION FOR ALL DIMENSIONS.  
2 - THIS IS A SHOWN DIMENSION FOR ALL DIMENSIONS.  
3 - THIS IS A SHOWN DIMENSION FOR ALL DIMENSIONS.

TYPICAL BUILDING MATERIALS	
[Symbol]	INTERNAL COLOR TILE ROOF
[Symbol]	INTERNAL COLOR PLASTER
[Symbol]	FABRIC
[Symbol]	RANK BOARD
[Symbol]	PLASTER CORNER
[Symbol]	PLASTER WINDOW TRIM
[Symbol]	VINYL WINDOW
[Symbol]	PLASTER TRIM
[Symbol]	DECORATIVE RAFTER
[Symbol]	DECORATIVE PLASTER PROJECTION
[Symbol]	RAFTER TAIL
[Symbol]	STONE PATTERN VENEER
[Symbol]	METAL SEAL / ANCHOR
[Symbol]	FIBERGLASS ENTRY DOOR
[Symbol]	SLIDING GLASS DOOR
[Symbol]	FRENCH DOOR
[Symbol]	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
[Symbol]	SLOPING PLASTER SILL
[Symbol]	HEAVY TEXTURE STUCCO
[Symbol]	EXPANSION JOINT
[Symbol]	PLASTER BATTERED MALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



8A  
FRONT ELEVATION II



ENHANCED RIGHT SIDE ELEVATION I

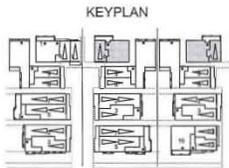
# A

UNIT 8  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC



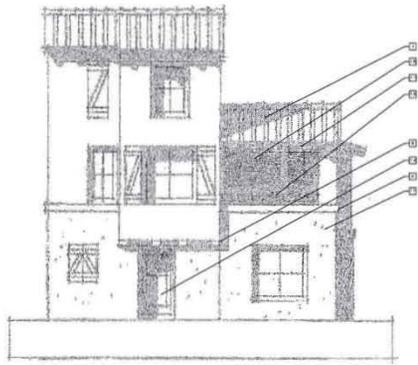
1.5.11



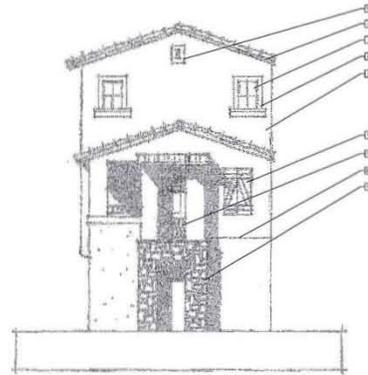
1. - UNIT 8 BOUNDARY AND ALL PLUMBING/WATER  
2. - THIS ELEVATION IS FOR THE EXTERIOR OF THE UNIT  
3. - REFER TO ELEVATION II FOR ALL PLUMBING/WATER

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	FABRIC
<input type="checkbox"/>	NAKED BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHAFTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	KAPTEL TAL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL RAIL / HANDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER BALL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED PILL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

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8C  
FRONT ELEVATION II



ENHANCED RIGHT SIDE ELEVATION I

C

SEE 'A' ELEVATIONS FOR STANDARD RIGHT SIDE & REAR

UNIT 8  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'  
(RB DOES NOT OCCUR)

1/2"=1'-0"

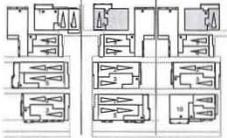
PROVENCE - COURTS  
RC NATOMAS, LLC

3/10/10/2010



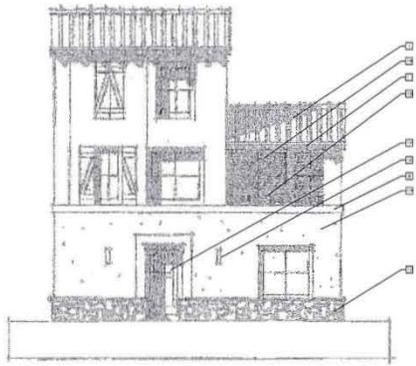
1:11

KEYPLAN

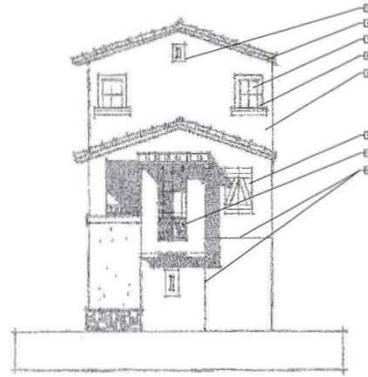


SEE: TYPICAL DEPT. OF CIVIL ENGINEERING FOR THE STATE OF NEW YORK RECORDS FOR THE PROJECT FOR THE PROVISIONS AND CONDITIONS OF THE CONTRACT.  
1. FOR A BROWN SHEDS ON ALL ROOFS AND SLOPES.  
2. FOR A BROWN SHEDS ON THE ROOFS AND SLOPES.  
3. FOR A BROWN SHEDS ON THE ROOFS AND SLOPES.

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	NATURAL COLOR 1" TILE ROOF
<input type="checkbox"/>	NATURAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	KAPTER TAIL
<input type="checkbox"/>	STONE PATTERN VENER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER BALL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED RAIL
* ALL METAL AND PLUMB ELEMENTS TO BE PAINTED	



8C  
FRONT ELEVATION III



ENHANCED RIGHT SIDE ELEVATION II

# C

SEE 'A' ELEVATIONS FOR STANDARD RIGHT SIDE & REAR

UNIT 8  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'  
(8B DOES NOT OCCUR)

1/4"=1'-0"

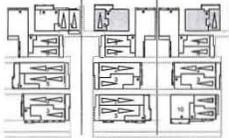
## PROVENCE - COURTS RC NATOMAS, LLC

20090209



1.5.11

### KEYPLAN



NOTE: THESE ELEVATIONS ARE FOR INFORMATION ONLY. THE EXACT APPEARANCE OF THE BUILDING SHALL BE DETERMINED BY THE ARCHITECT'S FINAL DESIGN.  
1 - FOR 8' ELEVATION HEIGHTS ONLY. ALL DIMENSIONS IN FEET.  
2 - THIS ELEVATION IS FOR THE EXTERIOR OF THE BUILDING.  
3 - DIMENSIONS ARE IN FEET AND INCHES (1/4"=1'-0").

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERAL COLOR TILE ROOF
<input type="checkbox"/>	INTERAL COLOR PLASTER
<input type="checkbox"/>	FABRIC
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER GUTTER
<input type="checkbox"/>	PLASTER FINISH TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE BAUTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	KAFTER TAL
<input type="checkbox"/>	STONE PATTERN VENER
<input type="checkbox"/>	METAL SILL / HANDRAIL
<input type="checkbox"/>	TERRAZZO ENTRY DOOR
<input type="checkbox"/>	SLOPING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED PILL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

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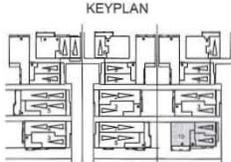


SECOND FLOOR

FIRST FLOOR

UNIT 10 & 11  
FIRST / SECOND FLOOR PLANS  
ELEVATION STYLE - 'A'

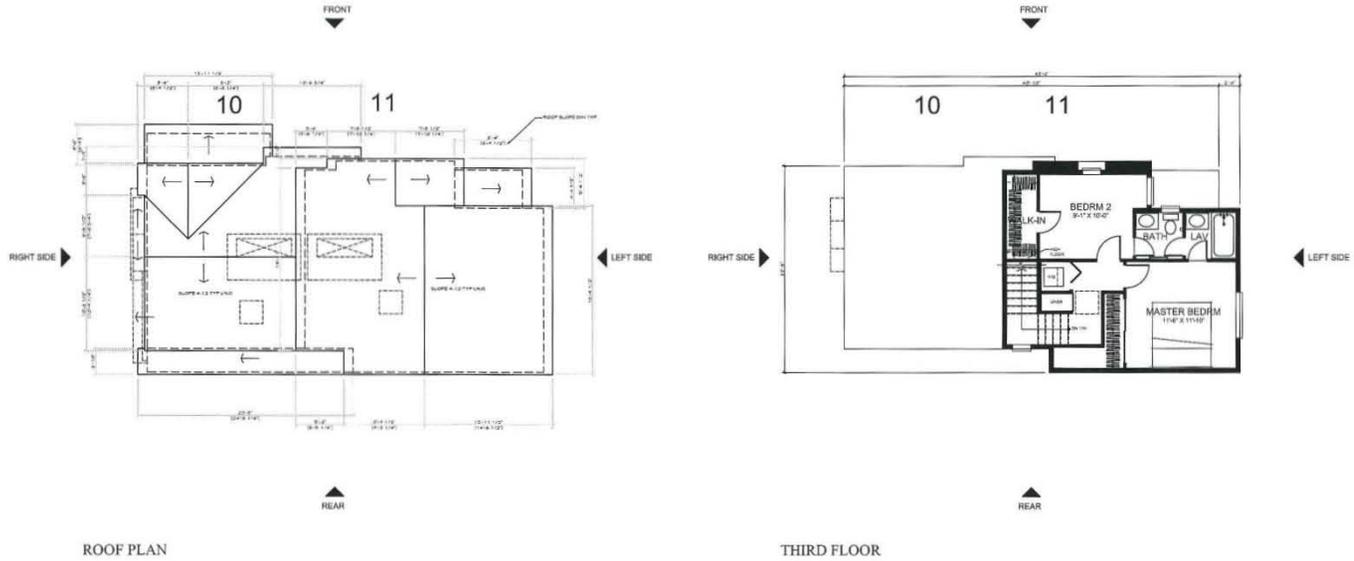
PROVENCE - COURTS  
RC NATOMAS, LLC



NOTES: THESE PLANS SHOW CONSTRUCTION ON THE SIDE SIDE OF THE CARP HOLDING AREA FOR THE EAST SIDE OF THE TOWER. SEE ARCHITECT'S WORK FOR A NUMBER.  
1 - SEE A REVISIONS LIST ON ALL PLANS. (2-11-11)  
2 - SEE A REVISIONS LIST ON ALL PLANS. (2-11-11)  
3 - SEE A REVISIONS LIST ON ALL PLANS. (2-11-11)

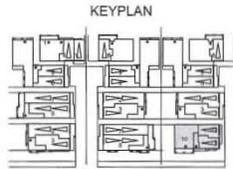


AREA ANALYSIS	
UNIT 10	297.87 S.F.
FIRST FLOOR	297.87 S.F.
SECOND FLOOR	297.87 S.F.
TOTAL LIVING AREA	595.74 S.F.
GARAGE	228.00 S.F.
POUCH	88.33 S.F.
TOTAL AREA	811.87 S.F.
UNIT 11	110.31 S.F.
FIRST FLOOR	110.31 S.F.
SECOND FLOOR	110.31 S.F.
TOTAL LIVING AREA	220.62 S.F.
GARAGE	228.00 S.F.
POUCH	88.33 S.F.
TOTAL AREA	536.95 S.F.
DECK	77.80 S.F.
TOTAL AREA	614.75 S.F.



ROOF PLAN

THIRD FLOOR



NOTE: REFER TO PLAN'S DIMENSIONS FOR ALL DIMENSIONS OF THE UNIT INCLUDING  
 OVER THE GARAGE AND THE TERRACE AND UNDER THE PORCH.  
 1 - DIMENSIONS SHOWN ON ALL PLANS.  
 2 - DIMENSIONS SHOWN ON THIS PLAN.  
 3 - DIMENSIONS SHOWN ON ALL PLANS.

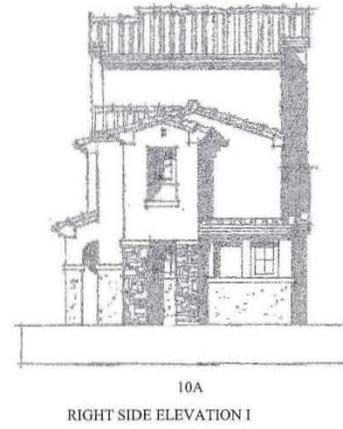
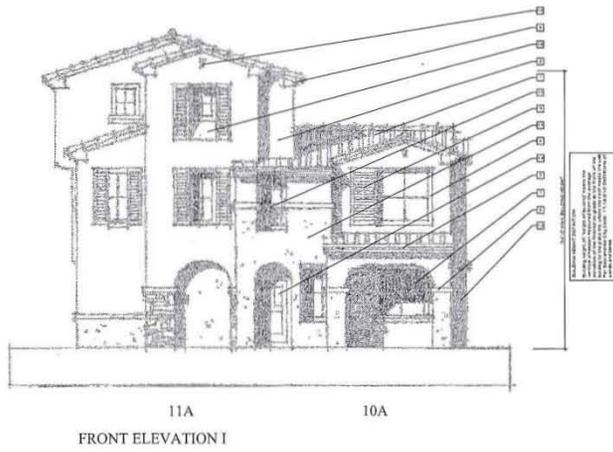
UNIT 10 & 11  
 THIRD FLOOR / ROOF PLAN  
 ELEVATION STYLE - 'A'

PROVENCE - COURTS  
 RC NATOMAS, LLC



(A1)

AREA ANALYSIS	
UNIT 10	
FIRST FLOOR	417.37 S.F.
SECOND FLOOR	354.36 S.F.
TOTAL LIVING AREA	771.73 S.F.
GARAGE	27.87 S.F.
POUCH	88.27 S.F.
TOT. AREA	887.87 S.F.
UNIT 11	
FIRST FLOOR	110.31 S.F.
SECOND FLOOR	420.38 S.F.
THIRD FLOOR	626.33 S.F.
TOTAL LIVING AREA	1157.02 S.F.
GARAGE	139.24 S.F.
TOT. AREA	1296.26 S.F.
TOT. AREA	2184.13 S.F.



# A

UNIT 10 & 11  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC

200904/03

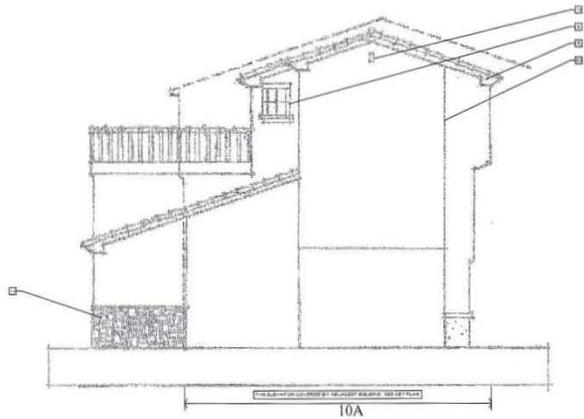


**KEYPLAN**

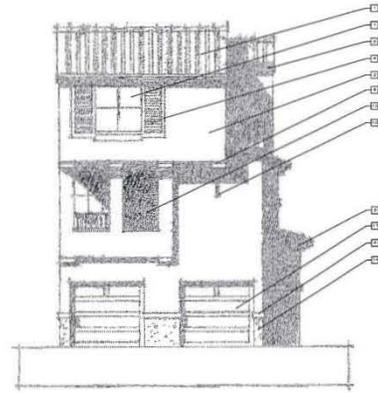
NOTE: THESE ELEVATIONS CORRESPOND TO THE MAIN LEVEL OF THE SAID BUILDING.  
OTHER THAN INDICATED, THE MATERIALS ARE STANDARD COLORS.

1 - SEE 8 FOR AN INDICATION OF ALL FINISHES TO BE USED.  
2 - THIS IS A GUIDE DRAWING FOR THE ARCHITECT'S USE ONLY.  
3 - FINISHES TO BE USED IN ALL PLACES OF THE SAID BUILDING.

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERIOR COLOR TILE ROOF
<input type="checkbox"/>	INTERIOR COLOR PLASTER
<input type="checkbox"/>	FABRIC
<input type="checkbox"/>	FAHSE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	KAUFER TAL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOW
<input type="checkbox"/>	SLOPPAN PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED WALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



REAR ELEVATION III



LEFT SIDE ELEVATION

# A

UNIT 10 & 11  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC

30000020



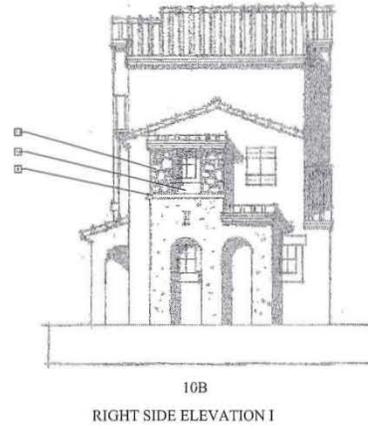
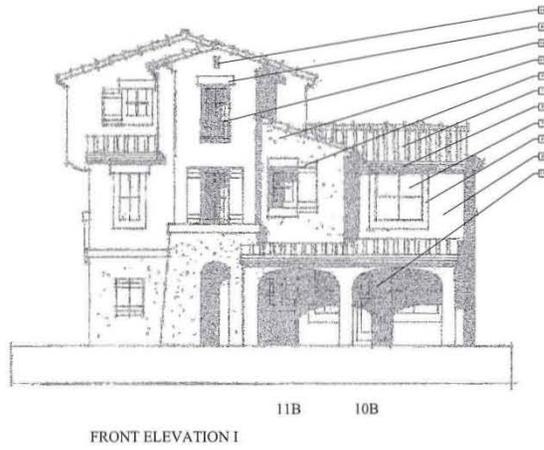
**KEYPLAN**

NOTE: THESE ELEVATIONS ARE INTENDED TO BE USED AS A GUIDE TO THE DESIGN OF THE BUILDING. THE ARCHITECT IS NOT RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION PROVIDED HEREIN.

1 - FINISH AND MATERIALS TO BE USED FOR ALL FINISHES AND MATERIALS.  
2 - FINISH AND MATERIALS TO BE USED FOR ALL FINISHES AND MATERIALS.  
3 - FINISH AND MATERIALS TO BE USED FOR ALL FINISHES AND MATERIALS.

TYPICAL BUILDING MATERIALS	
[Pattern]	INTERIOR COLOR TILE ROOF
[Pattern]	INTERIOR COLOR PLASTER
[Pattern]	FASCIA
[Pattern]	RIDGE BOARD
[Pattern]	PLASTER CORNICE
[Pattern]	PLASTER WINDOW TRIM
[Pattern]	VINYL WINDOW
[Pattern]	PLASTER TRIM
[Pattern]	DECORATIVE SHUTTER
[Pattern]	DECORATIVE PLASTER PROJECTION
[Pattern]	RAPPORT TAIL
[Pattern]	STONE PATTERN VENEER
[Pattern]	METAL RAIL / SHARDRAL
[Pattern]	FIBERGLASS ENTRY DOOR
[Pattern]	BLOWN GLASS DOOR
[Pattern]	FRENCH DOOR
[Pattern]	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
[Pattern]	BLOTTING PLASTER SILL
[Pattern]	HEAVY TEXTURE STUCCO
[Pattern]	EXPANSION JOINT
[Pattern]	PLASTER BATTERED PILL
* ALL METAL AND HARD ELEMENTS TO BE PAINTED	

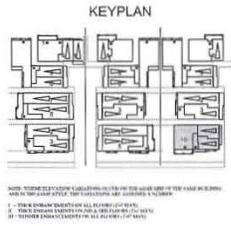
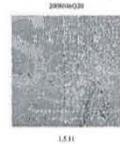
55 OF 62



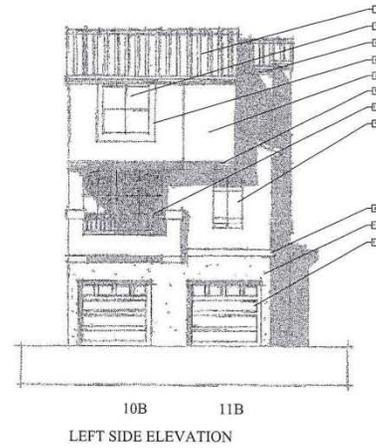
# B

UNIT 10 & 11  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'

PROVENCE - COURTS  
RC NATOMAS, LLC



TYPICAL BUILDING MATERIALS	
[Symbol]	INTERNAL COLOR TILE ROOF
[Symbol]	INTERNAL COLOR PLASTER
[Symbol]	FABRIQ
[Symbol]	RAND BOARD
[Symbol]	PLASTER CORNICE
[Symbol]	PLASTER WINDOW TRIM
[Symbol]	VINYL WINDOW
[Symbol]	PLASTER TRIM
[Symbol]	DECORATIVE BUTTER
[Symbol]	DECORATIVE PLASTER PROJECTION
[Symbol]	RAFTER TAIL
[Symbol]	STONE PATTERN VENEER
[Symbol]	METAL RAIL / GUARDRAIL
[Symbol]	FIBERGLASS ENTRY DOOR
[Symbol]	SLIDING GLASS DOOR
[Symbol]	FRENCH DOOR
[Symbol]	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
[Symbol]	SLOPING PLASTER SILL
[Symbol]	HEAVY TEXTURE STUCCO
[Symbol]	EXPANSION JOINT
[Symbol]	PLASTER BATTERED PILL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



# B

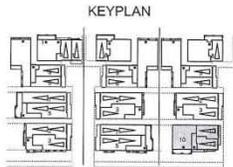
SEE 'A' ELEVATION FOR REAR

UNIT 10 & 11  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'

PROVENCE - COURTS  
RC NATOMAS, LLC



1.5.11

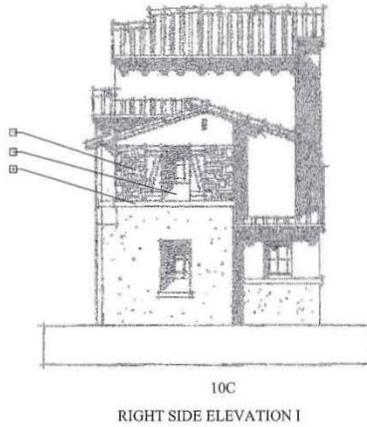


NOTES: THESE ELEVATIONS SHALL BE USED FOR THE EXTERIOR OF THE HOME IN ALL AREAS UNLESS OTHERWISE NOTED. THE CONTRACTOR SHALL VERIFY THE FINISHES AND MATERIALS WITH THE ARCHITECT AND THE CLIENT.

- 1. THE CONTRACTOR SHALL VERIFY ALL FINISHES AND MATERIALS WITH THE ARCHITECT AND THE CLIENT.
- 2. THE CONTRACTOR SHALL VERIFY THE FINISHES AND MATERIALS WITH THE ARCHITECT AND THE CLIENT.
- 3. THE CONTRACTOR SHALL VERIFY THE FINISHES AND MATERIALS WITH THE ARCHITECT AND THE CLIENT.

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR 2' TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	PARGA
<input type="checkbox"/>	RAKE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	KAPTEE TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL TAIL / QUADRANT
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	BLOPPING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED HALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

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C

UNIT 10 & 11  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

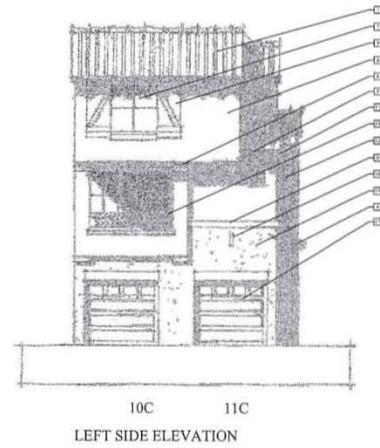
PROVENCE - COURTS  
RC NATOMAS, LLC



NOTES: REFER TO THE GENERAL NOTES FOR THE FULL LIST OF THE STANDARD FINISHES AND MATERIALS.  
1 - THIS IS EXTERIOR ELEVATION AND ALL FINISHES ARE TO BE APPLIED TO THE EXTERIOR SURFACE.  
2 - THIS IS EXTERIOR ELEVATION AND ALL FINISHES ARE TO BE APPLIED TO THE EXTERIOR SURFACE.  
3 - REFER TO THE GENERAL NOTES FOR THE FULL LIST OF FINISHES.



TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	FABRIKA
<input type="checkbox"/>	BASE BOARD
<input type="checkbox"/>	PLASTER CORBEL
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE BUTTRESS
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAPPEL TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRANCE DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED WALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



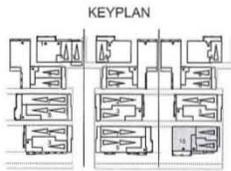
# C

SEE 'A' ELEVATION FOR REAR  
 UNIT 10 & 11  
 EXTERIOR ELEVATIONS  
 ELEVATION STYLE - 'C'

## PROVENCE - COURTS RC NATOMAS, LLC



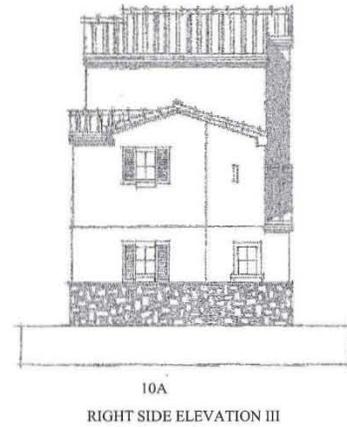
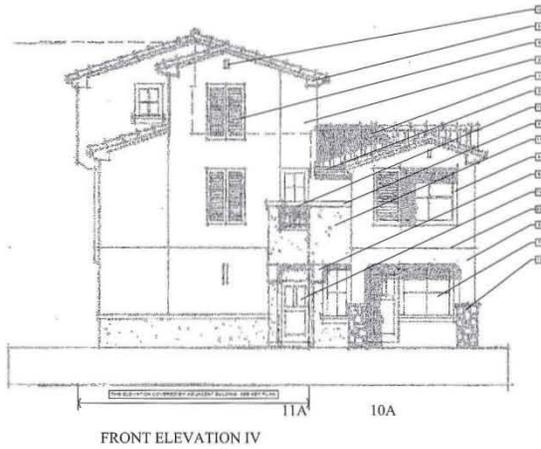
EX-11



NOTE: THESE ELEVATIONS REPRESENT THE PROPOSED APPEARANCE OF THE CASE BUILDING  
 WITH THE UNDERSTANDING THAT THE ARCHITECTURE AND MATERIALS MAY VARY.

1 - THIS IS AN EXTERIOR ELEVATION. ALL DIMENSIONS ARE IN FEET.  
 2 - THIS DRAWING IS FOR INFORMATION ONLY. IT IS NOT TO BE USED FOR CONSTRUCTION.  
 3 - DIMENSIONS SHOWN ARE APPROXIMATE. ALL DIMENSIONS SHALL BE AS SHOWN ON THE DRAWING.

TYPICAL BUILDING MATERIALS	
	INTEGRAL COLOR 18" TILE ROOF
	INTEGRAL COLOR PLASTER
	PARGE
	RANK BOARD
	PLASTER CORBEL
	PLASTER WINDOW TRIM
	VINYL WINDOW
	PLASTER TRIM
	DECORATIVE BRITTS
	DECORATIVE PLASTER PROJECTION
	RAPTER TAIL
	STONE PATTERN VENER
	METAL SAIL / GUARDRAIL
	FIBERGLASS ENTRY DOOR
	SLIDING GLASS DOOR
	FRENCH DOOR
	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
	SLOPHS PLASTER SILL
	HEAVY TEXTURE STUCCO
	EXPANSION JOINT
	PLASTER BATTERED WALL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



# A

UNIT 10 & 11  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'A'

PROVENCE - COURTS  
RC NATOMAS, LLC

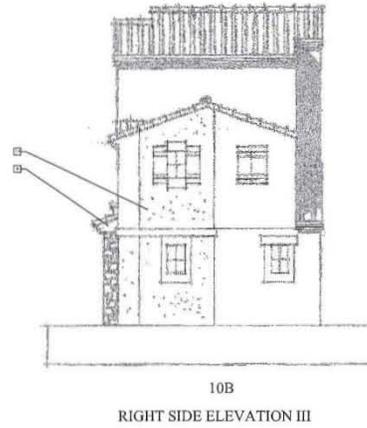
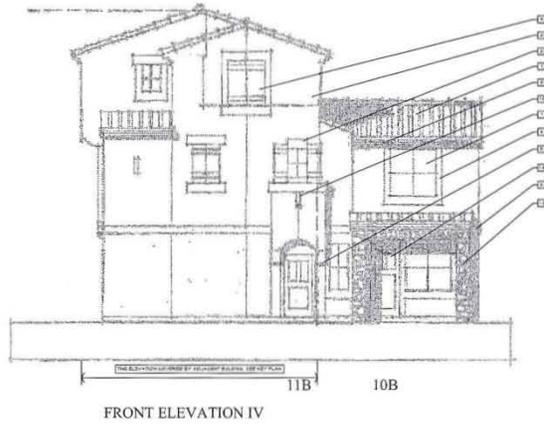


**KEYPLAN**

NOTE: THESE EXTERIOR ELEVATIONS ARE FOR ALL UNITS OF THE HOUSES LOCATED IN THE AREAS OF THE PROVISIONS AND CHANGES TO THE PLAN.  
1 - THIS IS THE EXTERIOR ELEVATION OF ALL UNITS OF THE HOUSES.  
2 - THIS IS THE EXTERIOR ELEVATION OF THE HOUSES.  
3 - THIS IS THE EXTERIOR ELEVATION OF THE HOUSES.

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR W/ TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	FABRIC
<input type="checkbox"/>	RAVE BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAFTER TAIL
<input type="checkbox"/>	STONE PATTERN VENER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED PILL
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

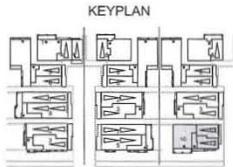
60 OF 62



# B

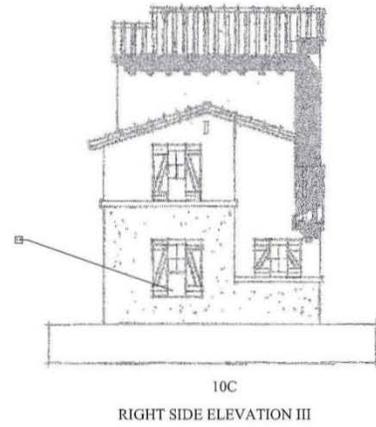
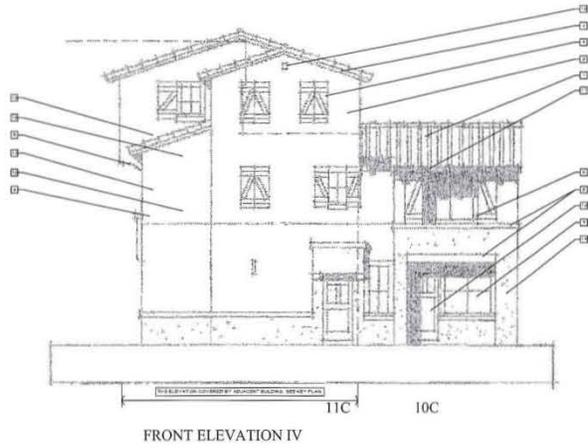
UNIT 10 & 11  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'B'

PROVENCE - COURTS  
RC NATOMAS, LLC



NOTES: 1. REFER TO ALL OTHER ELEVATIONS AND SECTION DRAWINGS OF THIS UNIT FOR DETAILS. 2. REFER TO THE GENERAL NOTES FOR FINISHES AND MATERIALS. 3. REFER TO THE GENERAL NOTES FOR FINISHES AND MATERIALS. 4. REFER TO THE GENERAL NOTES FOR FINISHES AND MATERIALS.

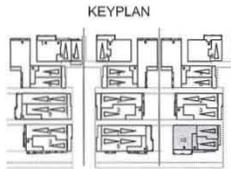
TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERIAL COLOR 1" TILE ROOF
<input type="checkbox"/>	INTERIAL COLOR PLASTER
<input type="checkbox"/>	PAVING
<input type="checkbox"/>	RANGE HOOD
<input type="checkbox"/>	PLASTER CORBEL
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE BUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	KAFFER TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL RAIL / SIVERRAL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL RECTANGULAR GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLOPING PLASTER BELL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED PILL
* ALL METAL AND FINISH ELEMENTS TO BE PAINTED	



C

UNIT 10 & 11  
EXTERIOR ELEVATIONS  
ELEVATION STYLE - 'C'

PROVENCE - COURTS  
RC NATOMAS, LLC



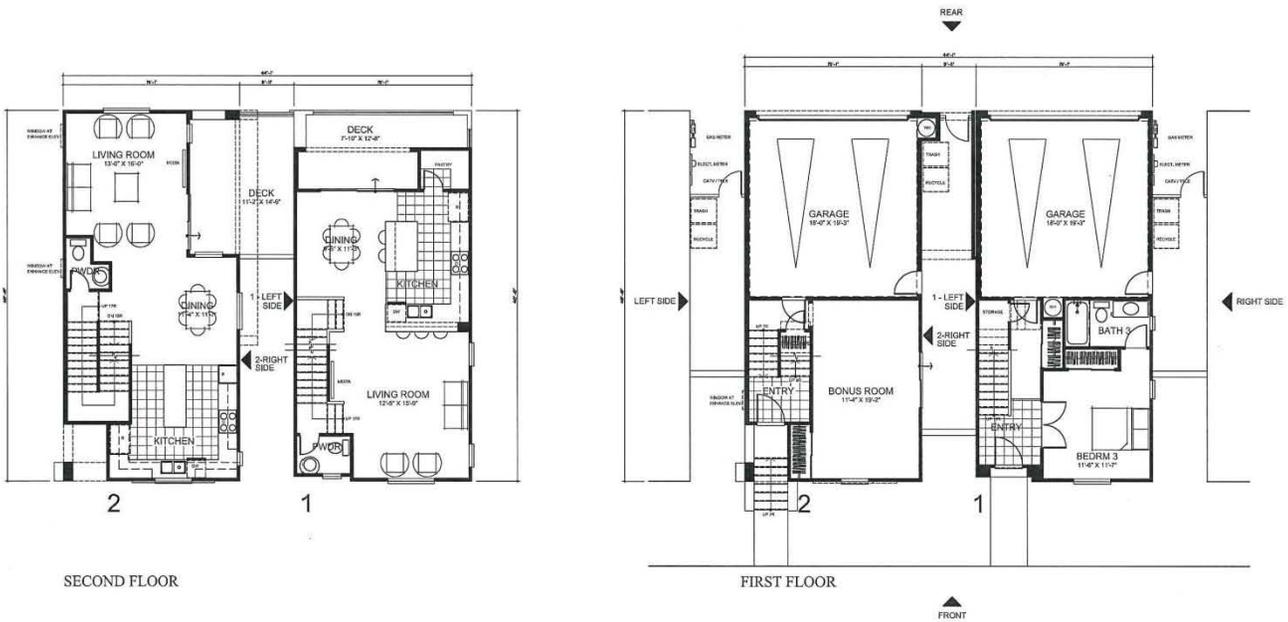
1 - THE 8' SECTION MARKS ON ALL PLANS OF THIS UNIT  
2 - THE 8' SECTION MARKS ON THIS SECTION ONLY  
3 - TURNER ENGINEERING, INC. (P. 10/11)



TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERAL COLOR 'I' TILE ROOF
<input type="checkbox"/>	INTERAL COLOR PLASTER
<input type="checkbox"/>	FASCIA
<input type="checkbox"/>	RANK BOARD
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	RAPPEL TAIL
<input type="checkbox"/>	STONE PATTERN VENEER
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	FRENCH DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR WITH WINDOWS
<input type="checkbox"/>	SLIPON PLASTER SILL
<input type="checkbox"/>	HEAVY TEXTURE STUCCO
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	PLASTER BATTERED PILL
* ALL METAL AND MOOD ELEMENTS TO BE PAINTED	

62 OF 62

Exhibit 41 – Urban Villas – House Plans and Elevations



UNIT 1 & 2  
FIRST / SECOND FLOOR PLANS  
ELEVATION STYLE - 'A'

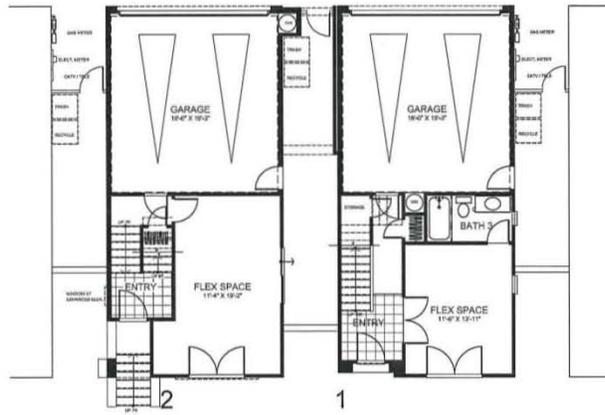
PROVENCE - URBAN VILLAS  
RC NATOMAS, LLC



010531

AREA ANALYSIS	
UNIT 1	
FIRST FLOOR	964.95 S.F.
SECOND FLOOR	111.85 S.F.
THIRD FLOOR	555.98 S.F.
TOTAL LIVING AREA	1632.78 S.F.
Garage	379.92 S.F.
Deck	758.52 S.F.
Other Area	293.34 S.F.
UNIT 2	
FIRST FLOOR	540.38 S.F.
SECOND FLOOR	156.82 S.F.
THIRD FLOOR	123.32 S.F.
TOTAL LIVING AREA	820.52 S.F.
Garage	300.00 S.F.
Deck	278.88 S.F.
Other Area	259.32 S.F.
UNIT 3	
FIRST FLOOR	491.41 S.F.
SECOND FLOOR	188.85 S.F.
THIRD FLOOR	151.70 S.F.
TOTAL LIVING AREA	831.96 S.F.
Garage	428.40 S.F.
Deck	324.27 S.F.
Other Area	254.09 S.F.

1 OF 14



FIRST FLOOR



FRONT ELEVATION

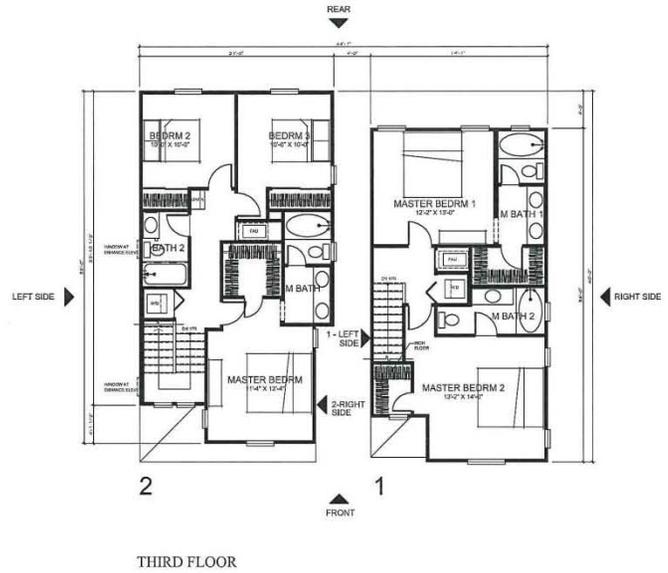
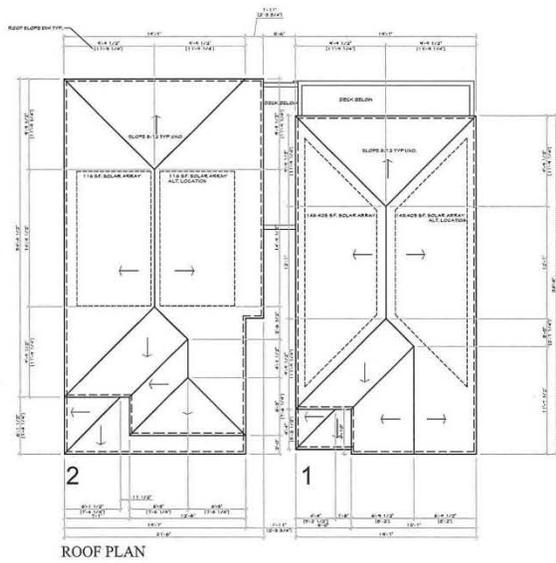
UNIT 1 & 2  
 FLEX SPACE ALTERNATE  
 ELEVATION STYLE - 'A'

PROVENCE - URBAN VILLAS  
 RC NATOMAS, LLC



BL0111

AREA ANALYSIS	
<b>UNIT 1</b>	
FIRST FLOOR	866.40 S.F.
SECOND FLOOR	615.40 S.F.
THIRD FLOOR	635.88 S.F.
TOTAL LIVING AREA	1,117.68 S.F.
SCREENING	379.87 S.F.
POOR	136.33 S.F.
total area	2,133.93 S.F.
<b>UNIT 2</b>	
FIRST FLOOR	846.38 S.F.
SECOND FLOOR	666.40 S.F.
THIRD FLOOR	722.22 S.F.
TOTAL LIVING AREA	1,235.00 S.F.
SCREENING	388.87 S.F.
POOR	176.88 S.F.
total area	2,200.75 S.F.
<b>UNIT 3</b>	
FIRST FLOOR	481.41 S.F.
SECOND FLOOR	654.97 S.F.
THIRD FLOOR	361.76 S.F.
TOTAL LIVING AREA	1,498.14 S.F.
SCREENING	436.82 S.F.
POOR	166.57 S.F.
total area	2,101.53 S.F.

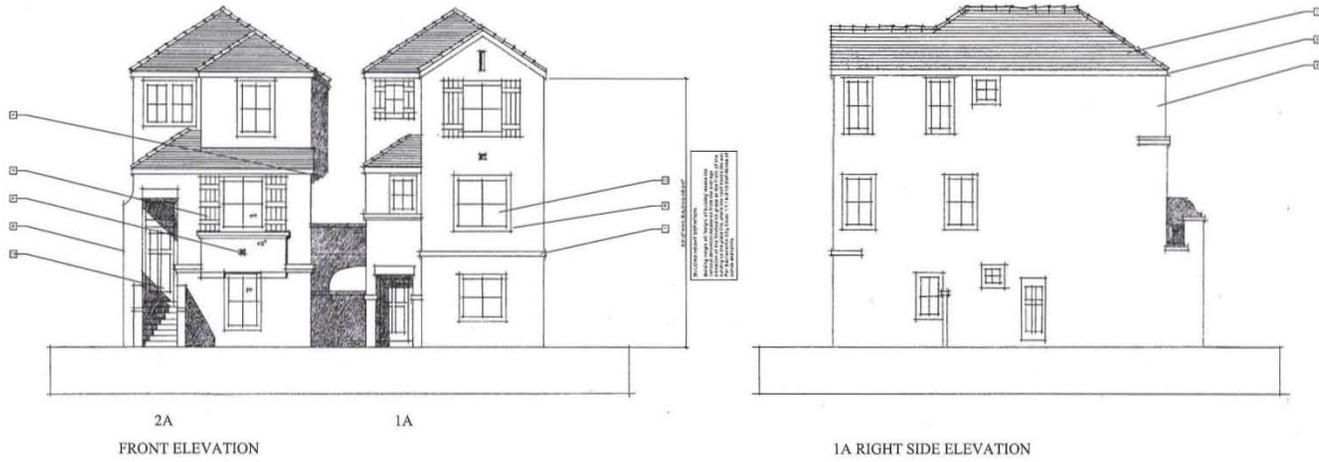


UNIT 1 & 2  
THIRD FLOOR / ROOF PLAN  
ELEVATION STYLE - 'A'

PROVENCE - URBAN VILLAS  
RC NATOMAS, LLC



AREA ANALYSIS	
<b>UNIT 1</b>	
FIRST FLOOR	364.35 S.F.
SECOND FLOOR	615.45 S.F.
THIRD FLOOR	653.88 S.F.
TOTAL LIVING AREA	1633.68 S.F.
COVERAGES	378.91 S.F.
DECK	178.33 S.F.
TOTAL AREA	2752.22 S.F.
<b>UNIT 2</b>	
FIRST FLOOR	346.38 S.F.
SECOND FLOOR	655.42 S.F.
THIRD FLOOR	723.22 S.F.
TOTAL LIVING AREA	1725.02 S.F.
COVERAGES	382.81 S.F.
DECK	176.86 S.F.
TOTAL AREA	2284.79 S.F.
<b>UNIT 3</b>	
FIRST FLOOR	481.41 S.F.
SECOND FLOOR	655.32 S.F.
THIRD FLOOR	351.70 S.F.
TOTAL LIVING AREA	1488.43 S.F.
COVERAGES	426.93 S.F.
DECK	104.21 S.F.
TOTAL AREA	2020.57 S.F.



# A

EXTERIOR ELEVATIONS  
ELEVATION STYLE - '1A & 2A'

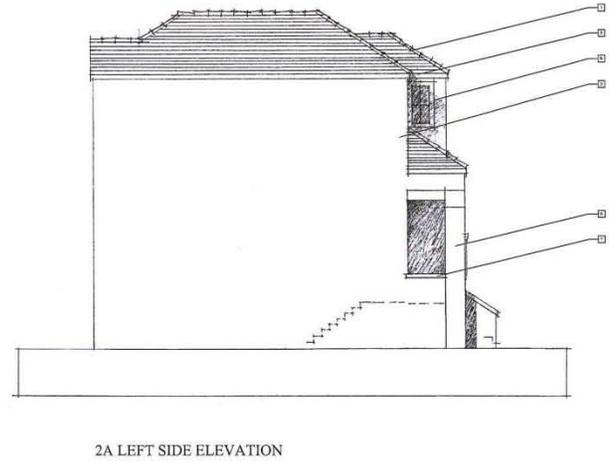
PROVENCE - URBAN VILLAS  
RC NATOMAS, LLC



BL511

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERIOR COLOR FLAT TILE ROOF
<input type="checkbox"/>	INTERIOR COLOR PLASTER
<input type="checkbox"/>	2X FASCIA
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER FINISH TRIM
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	PLASTER BATTERED WALL
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	VINYL FINISH
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIP-RESISTANT DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

4 OF 14



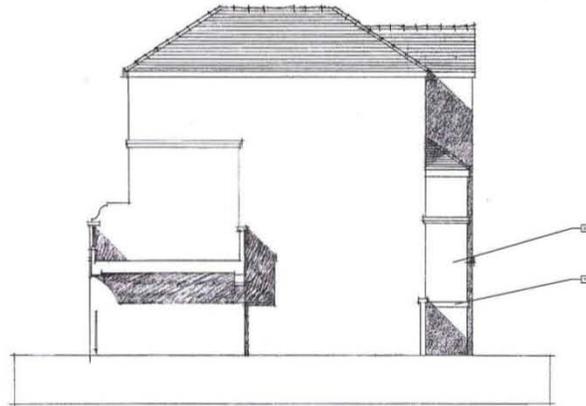
# A

EXTERIOR ELEVATIONS  
ELEVATION STYLE - '1A & 2A'

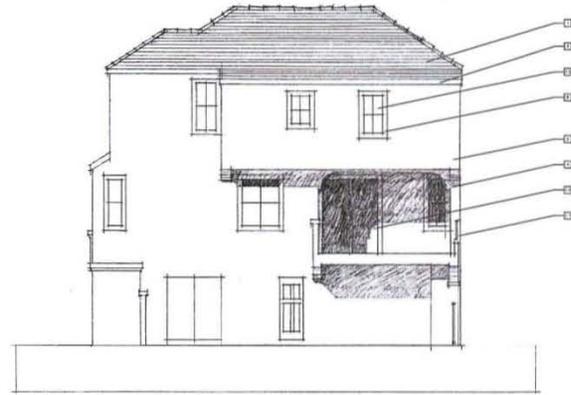
PROVENCE - URBAN VILLAS  
RC NATOMAS, LLC



TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR PLAT TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	3X FASGA
<input type="checkbox"/>	PLASTER CORBEL
<input type="checkbox"/>	PLASTER FINISH TRIM
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	PLASTER BATTERED FINISH
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



1A LEFT SIDE ELEVATION



2A RIGHT SIDE ELEVATION

# A

EXTERIOR ELEVATIONS  
ELEVATION STYLE - '1A & 2A'

PROVENCE - URBAN VILLAS  
RC NATOMAS, LLC



01.02.11

**TYPICAL BUILDING MATERIALS**

- INTERNAL COLOR FLAT TILE ROOF
  - INTERNAL COLOR PLASTER
  - 2X FASGA
  - PLASTER GORBEL
  - PLASTER WINDOW TRIM
  - DECORATIVE PLASTER PROJECTION
  - PLASTER TRIM
  - PLASTER BATTERED WALL
  - DECORATIVE SHUTTER
  - EXPANSION JOINT
  - METAL RAIL / GUARDRAIL
  - VINYL WINDOW
  - FIBERGLASS ENTRY DOOR
  - SLIDING GLASS DOOR
  - METAL SECTIONAL GARAGE DOOR
- \* ALL METAL AND WOOD ELEMENTS TO BE PAINTED

6 OF 14



2A  
2A ENHANCED LEFT SIDE ELEVATION



1A ENHANCED RIGHT SIDE ELEVATION

# A

EXTERIOR ELEVATIONS  
ELEVATION STYLE - "1A & 2A"

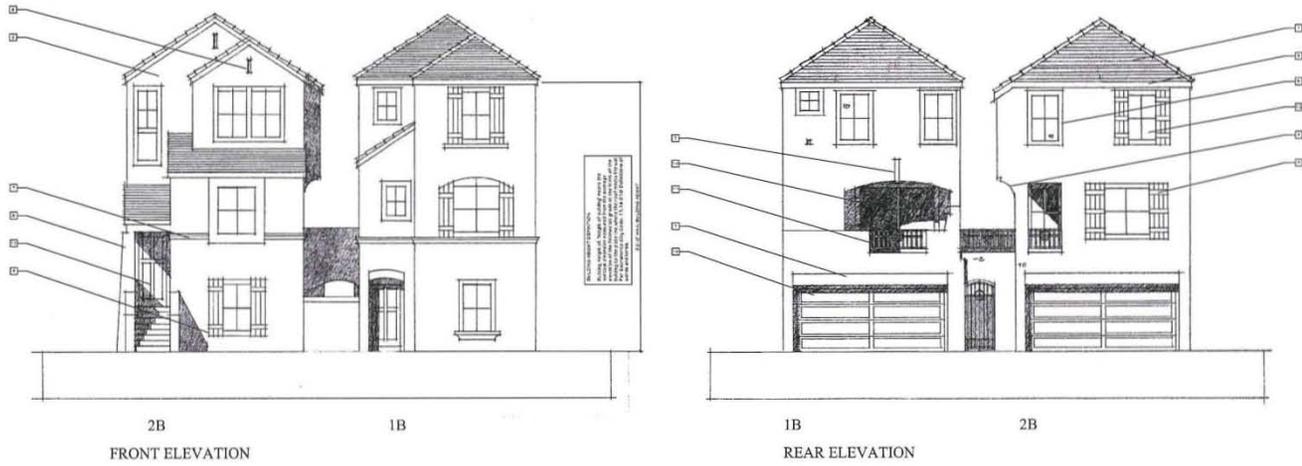
PROVENCE - URBAN VILLAS  
RC NATOMAS, LLC



RL03.11

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR FLAT TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	3X FASCIA
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	PLASTER BATTERED WALL
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	VINYL HANDS
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	

7 OF 14



2B  
FRONT ELEVATION

1B

1B  
REAR ELEVATION

2B

# B

SEE 'A' ELEVATIONS FOR SIDES

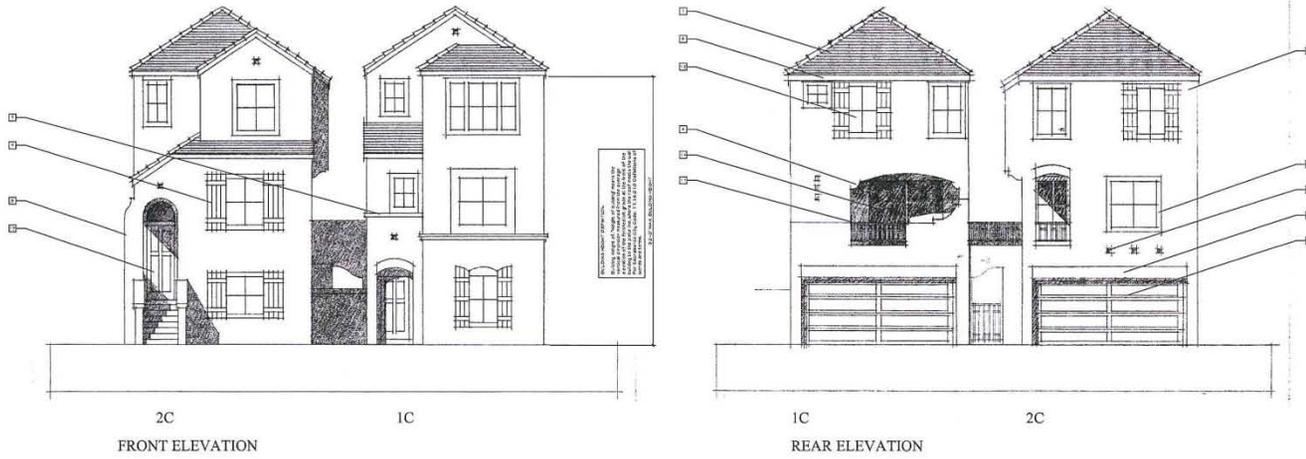
EXTERIOR ELEVATIONS  
ELEVATION STYLE - '1B & 2B'

## PROVENCE - URBAN VILLAS RC NATOMAS, LLC



06.08.11

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR FLAT TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	2X FASCIA
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	PLASTER BATTERED PAINT
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR
* ALL METAL AND HOOD ELEMENTS TO BE PAINTED	

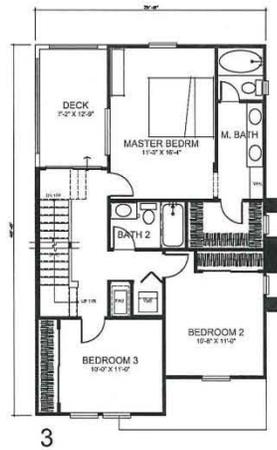


**C**  
 SEE 'A' ELEVATIONS FOR SIDES  
 EXTERIOR ELEVATIONS  
 ELEVATION STYLE - '1C & 2C'

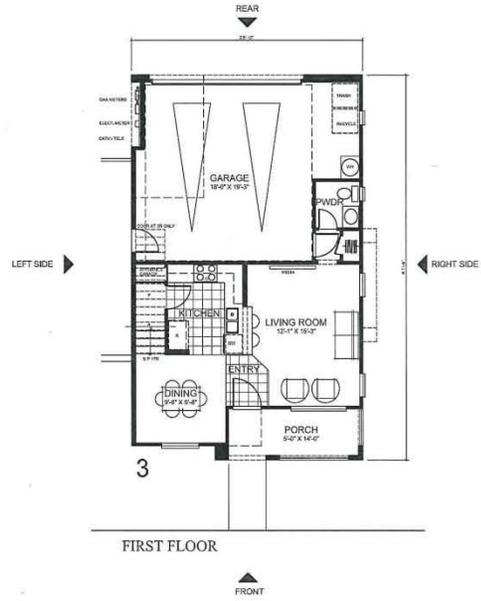
PROVENCE - URBAN VILLAS  
 RC NATOMAS, LLC



TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR FLAT TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	2X FASCIA
<input type="checkbox"/>	PLASTER CORNICE
<input type="checkbox"/>	PLASTER HINDOJA TRIM
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	PLASTER BATTERED MALL
<input type="checkbox"/>	DECORATIVE SHIMMER
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	VINYL HINDOJA
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



3  
 SECOND FLOOR



3  
 FIRST FLOOR

UNIT 3  
 FIRST / SECOND FLOOR PLANS  
 ELEVATION STYLE - 'A'

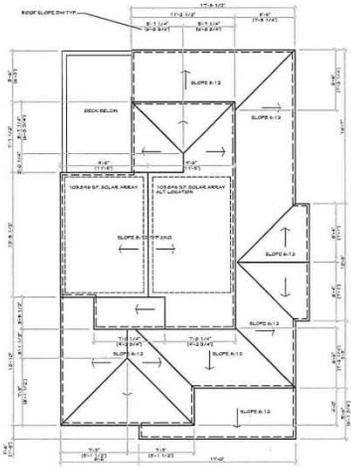
PROVENCE - URBAN VILLAS  
 RC NATOMAS, LLC



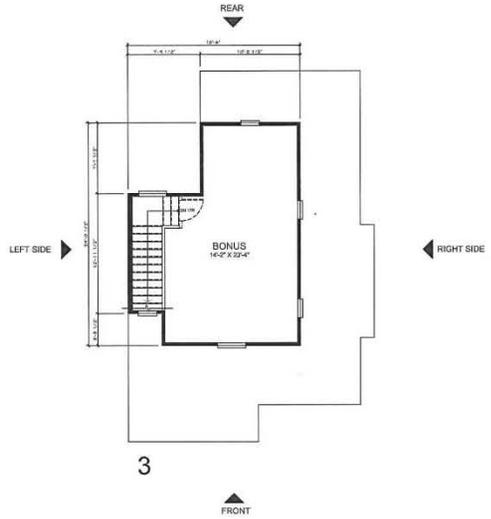
01.05.11

AREA ANALYSIS	
<b>UNIT 1</b>	
FIRST FLOOR	364.83 S.F.
SECOND FLOOR	814.45 S.F.
THIRD FLOOR	655.88 S.F.
TOTAL LIVING AREA	1835.16 S.F.
COVERAGE	319.97 S.F.
CCDF	136.37 S.F.
Total area	2762.33 S.F.
<b>UNIT 2</b>	
FIRST FLOOR	349.33 S.F.
SECOND FLOOR	686.42 S.F.
THIRD FLOOR	723.22 S.F.
TOTAL LIVING AREA	1861.02 S.F.
COVERAGE	366.87 S.F.
CCDF	178.98 S.F.
Total area	2247.79 S.F.
<b>UNIT 3</b>	
FIRST FLOOR	491.41 S.F.
SECOND FLOOR	896.82 S.F.
THIRD FLOOR	351.76 S.F.
TOTAL LIVING AREA	1760.04 S.F.
COVERAGE	436.97 S.F.
CCDF	194.27 S.F.
Total area	2401.07 S.F.

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ROOF PLAN



THIRD FLOOR

UNIT 3  
THIRD FLOOR / ROOF PLAN  
ELEVATION STYLE - 'A'

PROVENCE - URBAN VILLAS  
RC NATOMAS, LLC



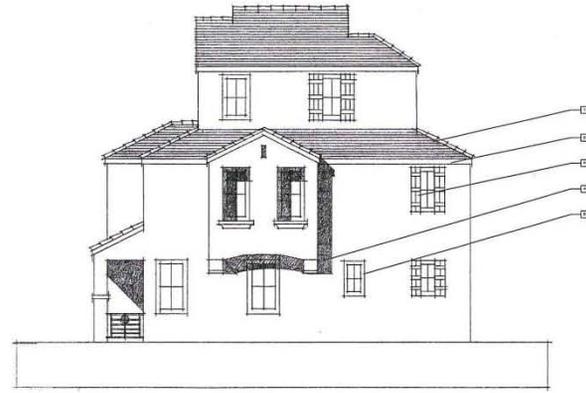
AREA ANALYSIS	
UNIT 1	
FIRST FLOOR	364.46 S.F.
SECOND FLOOR	814.48 S.F.
THIRD FLOOR	685.88 S.F.
TOTAL LIVING AREA	1864.82 S.F.
GARAGE	379.91 S.F.
LOBBY	186.33 S.F.
TOTAL area	2431.06 S.F.
UNIT 2	
FIRST FLOOR	340.38 S.F.
SECOND FLOOR	858.42 S.F.
THIRD FLOOR	727.22 S.F.
TOTAL LIVING AREA	1926.02 S.F.
GARAGE	360.87 S.F.
LOBBY	716.96 S.F.
TOTAL area	2993.79 S.F.
UNIT 3	
FIRST FLOOR	461.41 S.F.
SECOND FLOOR	858.82 S.F.
THIRD FLOOR	361.70 S.F.
TOTAL LIVING AREA	1681.93 S.F.
GARAGE	436.87 S.F.
LOBBY	194.27 S.F.
TOTAL area	2313.07 S.F.

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FRONT ELEVATION

SEE SECTION 308849230 FOR  
 FINISHES AND MATERIALS  
 LISTING. SEE SECTION 308849230 FOR  
 FINISHES AND MATERIALS LISTING.  
 SEE SECTION 308849230 FOR  
 FINISHES AND MATERIALS LISTING.



ENHANCED RIGHT SIDE ELEVATION

# A

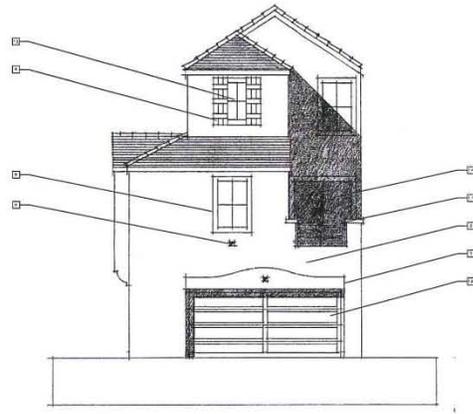
EXTERIOR ELEVATIONS  
 ELEVATION STYLE - '3A'

PROVENCE - URBAN VILLAS  
 RC NATOMAS, LLC

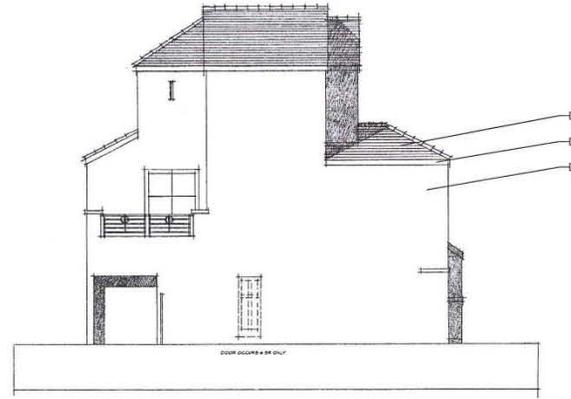


01.05.11

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR FLAT TILE ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	3X FASGA
<input type="checkbox"/>	PLASTER GORBEL
<input type="checkbox"/>	PLASTER FINISH TRIM
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	PLASTER BATTERED WALL
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	THERMALLY-BREAKING ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR
* ALL METAL AND WOOD ELEMENTS TO BE PAINTED	



REAR ELEVATION



LEFT SIDE ELEVATION

A

EXTERIOR ELEVATIONS  
ELEVATION STYLE - '3A'

PROVENCE - URBAN VILLAS  
RC NATOMAS, LLC



01.05.11

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTERNAL COLOR FLAT T&B ROOF
<input type="checkbox"/>	INTERNAL COLOR PLASTER
<input type="checkbox"/>	2X FASGA
<input type="checkbox"/>	PLASTER CORBEL
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	PLASTER BATTERED MULL
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	METAL RAIL / GUARDRAIL
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLIDING GLASS DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR
* ALL METAL AND HOOD ELEMENTS TO BE PAINTED	

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FRONT ELEVATION

ENHANCED RIGHT SIDE ELEVATION

REAR ELEVATION

# B

EXTERIOR ELEVATIONS  
ELEVATION STYLE - '3B'

PROVENCE - URBAN VILLAS  
RC NATOMAS, LLC



06.03.11

TYPICAL BUILDING MATERIALS	
<input type="checkbox"/>	INTEGRAL COLOR FLAT TILE ROOF
<input type="checkbox"/>	INTEGRAL COLOR PLASTER
<input type="checkbox"/>	2X FASCIA
<input type="checkbox"/>	PLASTER GORBEL
<input type="checkbox"/>	PLASTER WINDOW TRIM
<input type="checkbox"/>	DECORATIVE PLASTER PROJECTION
<input type="checkbox"/>	PLASTER TRIM
<input type="checkbox"/>	PLASTER BATTERED WALL
<input type="checkbox"/>	DECORATIVE SHUTTER
<input type="checkbox"/>	EXPANSION JOINT
<input type="checkbox"/>	METAL KAIL / SHARDIRAL
<input type="checkbox"/>	VINYL WINDOW
<input type="checkbox"/>	FIBERGLASS ENTRY DOOR
<input type="checkbox"/>	SLEIGH GLASS DOOR
<input type="checkbox"/>	METAL SECTIONAL GARAGE DOOR
* ALL METAL AND FLOOD ELEMENTS TO BE PAINTED	

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## Exhibit 4J – Building Data Charts

PROVENCE - 5088676.00

**Structure Square Footage****Existing Structure Square Footage**

PRODUCT	TOTAL LIVING AREA	GARAGE	TOTAL AREA
Unit 1B	1279 SF	429 SF	1708 SF
Unit 2A	1351 SF	429 SF	1780 SF
Unit 3A	1265 SF	429 SF	1694 SF
Unit 2RB	1351 SF	429 SF	1780 SF
Unit 1RC	1279 SF	429 SF	1708 SF

Total Existing Structure Square Footage: **4,730 SF** NOT A PART (P06-194)**Proposed Structure Square Footage**

PRODUCT	TOTAL LIVING AREA	GARAGE	DECK/ PORCH	GROSS AREA PER BLDG.	BLDGS. PER PLAN	TOTAL BLDG. AREA
<b>Courts</b>						
Plan 1/4	1623 SF	402 SF	119 SF	2143 SF	16	34290 SF
Plan 2/3	1806 SF	803 SF	114 SF	2724 SF	14	38133 SF
Plan 5/7	2154 SF	861 SF	118 SF	3133 SF	31	97121 SF
Plan 6/9	2477 SF	473 SF	137 SF	3087 SF	11	33957 SF
Plan 8	1242 SF	215 SF	145 SF	1602 SF	7	11217 SF
Plan 10/11	1793 SF	425 SF	140 SF	2359 SF	10	23585 SF
<b>Urban Villas</b>						
Unit 1	1634 SF	380 SF	136 SF	2150 SF	7	15050 SF
Unit 1&2	3315 SF	761 SF	313 SF	4389 SF	17	74613 SF
Unit 3	1702 SF	437 SF	174 SF	2313 SF	21	48573 SF

Total Proposed Structure Square Footage: **376538 SF****Lot Coverage**

PRODUCT	COVERAGE PER BUILDING	BLDGS. PER PLAN	TOTAL BLDG. COVERAGE
<b>Courts</b>			
Plan 1/4	893 SF	16	14288 SF
Plan 2/3	1024 SF	14	14336 SF
Plan 5/7	1178 SF	31	36518 SF
Plan 6/9	1205 SF	11	13255 SF
Plan 8	743 SF	7	5201 SF
Plan 10/11	1057 SF	10	10570 SF
<b>Urban Villas</b>			
Unit 1	760 SF	7	5320 SF
Unit (1&2)	1540 SF	17	26180 SF
Unit 3	1009 SF	21	21190 SF

Total Building Coverage Area: **146858 SF**Total Residential/Landscape Lot Area: **348,480 SF (8.0AC)**Total Lot Coverage Percentage: **42%**

ATTACHMENT A

PROVENCE - 5088676.00

## Building Heights & Materials

### Proposed Building Heights

PRODUCT	MAX. BUILDING HEIGHT	NUMBER OF FLOORS
Courts - Plan 1/4	33' - 0" max	3
Courts - Plan 2/3	33' - 0" max	3
Courts - Plan 5/7	35' - 0" max	3
Courts - Plan 6/9	34' - 0" max	3
Courts - Plan 8	33' - 0" max	3
Courts - Plan 10/11	34' - 0" max	3
Urban Villas	32' - 0" max	3

### Existing Building Heights

PRODUCT	MAX. BUILDING HEIGHT	NUMBER OF FLOORS
Unit 1B	19' - 3.5"	2
Unit 2A	19' - 3.5"	2
Unit 3A	19' - 3.5"	2
Unit 2RB	19' - 3.5"	2
Unit 1RC	19' - 3.5"	2

### Proposed Building Materials

Exterior Walls & Soffits, Trim: Integral color stucco, 2x fascia, plaster corbel, & plaster window trim  
rake board, rafter tail, stone pattern veneer,

Roofing: Composit MAXItile

Misc: metal rail/ guardrail, vinyl windows, decorative shutter,  
sliding glass door, fiberglass entry door, metal sectional garage door,  
metal sectional garage door with windows, and french door

### Existing Building Materials

Exterior Walls & Soffits, Trim: Stucco, Stucco over foam trim

Roofing: Class A Fire Retardent flat concrete tiles

Shutters: decorative Foam

Misc: metal exterior balcony guardrail  
plywood mounted decorative clay pipe  
decorative 3/4" round wrought iron

ATTACHMENT B

Elev Enhancement Plotting 02-10-2011.xls

Exhibit 4K – Elevation Variation Plotting and Color Schemes

Provence-Courts														
2008046														
1/3/2011														
1/24/2011 City revisions, additional plan info														
Elevation Variation Plotting						Color Plotting								
Notes: Not all sides of all buildings are visible and enhanced						Notes: Please refer to the Color Lists for scheme specifications (Schemes 1-4)								
Areas are approximate, see design drawing for accurate plan areas														
Variations											Symbol			
Thick enhancements on all floors (2'-6" max)											I			
Thick enhancements on 2nd & 3rd floors (2'-6" max)											II			
Thinner enhancements (1'-0" max)											III			
Thick enhancements all floors, not full building width(2'-6" max)											IV			
Bldg No	Bldg type	Areas		Br/Ba		Garage		Height	Elev Style	Color Scheme	Front	Right	Rear	Left
31	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	B	2		I	
32	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	C	4			
33	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	A	3			
34	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	C	1		II	
35	2-3	2: 903 sf	3: 903 sf	2: 2Br/2.5 Ba	3: 2Br/2.5 Ba	2: 1 car	3: 2 car tandem	2: 34'	3: 34'	A	2		I	
36	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	B	3		III	
37	6-9	6: 1040 sf	9: 1437 sf	6: 2 Br/2.5 Ba	9: 3 Br/2.5 Ba	6: 1 car	9: 1 car	6: 24'	9: 34'	C	1	II	II	
38	1-4	1: 649 sf	4: 974 sf	1: 1 Br/1 Ba	4: 2 Br/1.5 Ba	1: 1 car	4: 1 car	1: 24'	4: 34'	A	4	II		II
41	10-11	10: 727 sf	11: 1066 sf	10: 1 Br/1 Ba	11: 2 Br/1.5 Ba	10: 1 car	11: 1 car	10: 24'	11: 34'	A	4	I	I	
42	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	B	2			
43	6-9	6: 1040 sf	9: 1437 sf	6: 2 Br/2.5 Ba	9: 3 Br/2.5 Ba	6: 1 car	9: 1 car	6: 24'	9: 34'	C	3		II	
44	1-4	1: 649 sf	4: 974 sf	1: 1 Br/1 Ba	4: 2 Br/1.5 Ba	1: 1 car	4: 1 car	1: 24'	4: 34'	A	4			
45	1-4	1: 649 sf	4: 974 sf	1: 1 Br/1 Ba	4: 2 Br/1.5 Ba	1: 1 car	4: 1 car	1: 24'	4: 34'	C	1			
46	6-9	6: 1040 sf	9: 1437 sf	6: 2 Br/2.5 Ba	9: 3 Br/2.5 Ba	6: 1 car	9: 1 car	6: 24'	9: 34'	B	2			
47	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	A	3		III	
48	10-11	10: 727 sf	11: 1066 sf	10: 1 Br/1 Ba	11: 2 Br/1.5 Ba	10: 1 car	11: 1 car	10: 24'	11: 34'	C	1	I	I	
49	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	B	2		I	
50	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	C	4			
51	6-9	6: 1040 sf	9: 1437 sf	6: 2 Br/2.5 Ba	9: 3 Br/2.5 Ba	6: 1 car	9: 1 car	6: 24'	9: 34'	A	1			
52	8	8: 1242 sf		8: 2 Br/2.5 Ba		8: 1 car		8: 24'	8: 34'	C	3	I		
53	8	8: 1242 sf		8: 2 Br/2.5 Ba		8: 1 car		8: 24'	8: 34'	A	4	I		
54	6-9	6: 1040 sf	9: 1437 sf	6: 2 Br/2.5 Ba	9: 3 Br/2.5 Ba	6: 1 car	9: 1 car	6: 24'	9: 34'	B	2			
55	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	A	1			
56	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	C	3		I	
57	10-11	10: 727 sf	11: 1066 sf	10: 1 Br/1 Ba	11: 2 Br/1.5 Ba	10: 1 car	11: 1 car	10: 24'	11: 34'	A	4	I	I	
58	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	B	3		III	
59	6-9	6: 1040 sf	9: 1437 sf	6: 2 Br/2.5 Ba	9: 3 Br/2.5 Ba	6: 1 car	9: 1 car	6: 24'	9: 34'	C	1			
60	1-4	1: 649 sf	4: 974 sf	1: 1 Br/1 Ba	4: 2 Br/1.5 Ba	1: 1 car	4: 1 car	1: 24'	4: 34'	A	2	I		
61	1-4	1: 649 sf	4: 974 sf	1: 1 Br/1 Ba	4: 2 Br/1.5 Ba	1: 1 car	4: 1 car	1: 24'	4: 34'	B	3	I	I	
62	2-3	2: 903 sf	3: 903 sf	2: 2Br/2.5 Ba	3: 2Br/2.5 Ba	2: 1 car	3: 2 car tandem	2: 34'	3: 34'	C	4		III	
63	2-3	2: 903 sf	3: 903 sf	2: 2Br/2.5 Ba	3: 2Br/2.5 Ba	2: 1 car	3: 2 car tandem	2: 34'	3: 34'	A	1			

Elev Enhancement Plotting 02-10-2011.xls

Bldg No	Bldg type	Areas		Br/Ba		Garage		Height		Elev Style	Color Scheme	Front	Right	Rear	Left
64	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	B	2		I		III
65	2-3	2: 903 sf	3: 903 sf	2: 2Br/2.5 Ba	3: 2Br/2.5 Ba	2: 1 car	3: 2 car tandem			B	2		I		III
66	1-4	1: 649 sf	4: 974 sf	1: 1 Br/1 Ba	4: 2 Br/1.5 Ba	1: 1 car	4: 1 car	1: 24'	4: 34'	C	1	I	II		
67	1-4	1: 649 sf	4: 974 sf	1: 1 Br/1 Ba	4: 2 Br/1.5 Ba	1: 1 car	4: 1 car	1: 24'	4: 34'	A	3	I			
68	6-9	6: 1040 sf	9: 1437 sf	6: 2 Br/2.5 Ba	9: 3 Br/2.5 Ba	6: 1 car	9: 1 car	6: 24'	9: 34'	B	4			II	
69	2-3	2: 903 sf	3: 903 sf	2: 2Br/2.5 Ba	3: 2Br/2.5 Ba	2: 1 car	3: 2 car tandem	2: 34'	3: 34'	C	1				
70	2-3	2: 903 sf	3: 903 sf	2: 2Br/2.5 Ba	3: 2Br/2.5 Ba	2: 1 car	3: 2 car tandem	2: 34'	3: 34'	A	2		III		
89	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	A	3		I		
90	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	B	2				
91	10-11	10: 727 sf	11: 1066 sf	10: 1 Br/1 Ba	11: 2 Br/1.5 Ba	10: 1 car	11: 1 car	10: 24'	11: 34'	C	1	IV	I		
92	1-4	1: 649 sf	4: 974 sf	1: 1 Br/1 Ba	4: 2 Br/1.5 Ba	1: 1 car	4: 1 car	1: 24'	4: 34'	A	4	II	II		
93	1-4	1: 649 sf	4: 974 sf	1: 1 Br/1 Ba	4: 2 Br/1.5 Ba	1: 1 car	4: 1 car	1: 24'	4: 34'	C	3	II	II		
94	2-3	2: 903 sf	3: 903 sf	2: 2Br/2.5 Ba	3: 2Br/2.5 Ba	2: 1 car	3: 2 car tandem	2: 34'	3: 34'	B	1		III		III
95	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	A	4		I		
96	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	C	3		II		
97	10-11	10: 727 sf	11: 1066 sf	10: 1 Br/1 Ba	11: 2 Br/1.5 Ba	10: 1 car	11: 1 car	10: 24'	11: 34'	B	4	IV	III		
98	8	8: 1242 sf		8: 2 Br/2.5 Ba		8: 1 car		8: 24'	8: 34'	A	2	II	I		
99	8	8: 1242 sf		8: 2 Br/2.5 Ba		8: 1 car		8: 24'	8: 34'	C	3	II	I		
100	10-11	10: 727 sf	11: 1066 sf	10: 1 Br/1 Ba	11: 2 Br/1.5 Ba	10: 1 car	11: 1 car	10: 24'	11: 34'	A	2	IV	III		
101	2-3	2: 903 sf	3: 903 sf	2: 2Br/2.5 Ba	3: 2Br/2.5 Ba	2: 1 car	3: 2 car tandem	2: 34'	3: 34'	A	1				
102	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	C	4		II		
103	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	B	2		I		
104	2-3	2: 903 sf	3: 903 sf	2: 2Br/2.5 Ba	3: 2Br/2.5 Ba	2: 1 car	3: 2 car tandem	2: 34'	3: 34'	A	3				
105	10-11	10: 727 sf	11: 1066 sf	10: 1 Br/1 Ba	11: 2 Br/1.5 Ba	10: 1 car	11: 1 car	10: 24'	11: 34'	C	1	IV	III		
106	8	8: 1242 sf		8: 2 Br/2.5 Ba		8: 1 car		8: 24'	8: 34'	A	4	II	I		
107	8	8: 1242 sf		8: 2 Br/2.5 Ba		8: 1 car		8: 24'	8: 34'	C	1	II	I		
108	10-11	10: 727 sf	11: 1066 sf	10: 1 Br/1 Ba	11: 2 Br/1.5 Ba	10: 1 car	11: 1 car	10: 24'	11: 34'	A	3	IV	III		
109	10-11	10: 727 sf	11: 1066 sf	10: 1 Br/1 Ba	11: 2 Br/1.5 Ba	10: 1 car	11: 1 car	10: 24'	11: 34'	B	2	I	I		
110	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	A	1		I		
111	2-3	2: 903 sf	3: 903 sf	2: 2Br/2.5 Ba	3: 2Br/2.5 Ba	2: 1 car	3: 2 car tandem	2: 34'	3: 34'	B	4				III
112	1-4	1: 649 sf	4: 974 sf	1: 1 Br/1 Ba	4: 2 Br/1.5 Ba	1: 1 car	4: 1 car	1: 24'	4: 34'	A	2	I	II		
113	1-4	1: 649 sf	4: 974 sf	1: 1 Br/1 Ba	4: 2 Br/1.5 Ba	1: 1 car	4: 1 car	1: 24'	4: 34'	B	3	I	I		
114	10-11	10: 727 sf	11: 1066 sf	10: 1 Br/1 Ba	11: 2 Br/1.5 Ba	10: 1 car	11: 1 car	10: 24'	11: 34'	A	2	IV	I		
115	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	C	1				
116	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	A	4		I		
117	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	C	1		I		
118	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	B	2				
119	6-9	6: 1040 sf	9: 1437 sf	6: 2 Br/2.5 Ba	9: 3 Br/2.5 Ba	6: 1 car	9: 1 car	6: 24'	9: 34'	A	4		II		
120	8	8: 1242 sf		8: 2 Br/2.5 Ba		8: 1 car		8: 24'	8: 34'	C	1	III	II		
121	2-3	2: 903 sf	3: 903 sf	2: 2Br/2.5 Ba	3: 2Br/2.5 Ba	2: 1 car	3: 2 car tandem	2: 34'	3: 34'	B	3		II		
122	2-3	2: 903 sf	3: 903 sf	2: 2Br/2.5 Ba	3: 2Br/2.5 Ba	2: 1 car	3: 2 car tandem	2: 34'	3: 34'	A	2				
123	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	C	4		I		
124	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	B	3		I		
125	6-9	6: 1040 sf	9: 1437 sf	6: 2 Br/2.5 Ba	9: 3 Br/2.5 Ba	6: 1 car	9: 1 car	6: 24'	9: 34'	C	1		II		
126	1-4	1: 649 sf	4: 974 sf	1: 1 Br/1 Ba	4: 2 Br/1.5 Ba	1: 1 car	4: 1 car	1: 24'	4: 34'	A	4	III	II		
127	1-4	1: 649 sf	4: 974 sf	1: 1 Br/1 Ba	4: 2 Br/1.5 Ba	1: 1 car	4: 1 car	1: 24'	4: 34'	C	2	III	II		

Elev Enhancement Plotting 02-10-2011.xls

Bldg No	Bldg type	Areas		Br/Ba		Garage		Height		Elev Style	Color Scheme	Front	Right	Rear	Left
128	6-9	6: 1040 sf	9: 1437 sf	6: 2 Br/2.5 Ba	9: 3 Br/2.5 Ba	6: 1 car	9: 1 car	6: 24'	9: 34'	B	3		II		
129	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	A	4		II		III
130	6-9	6: 1040 sf	9: 1437 sf	6: 2 Br/2.5 Ba	9: 3 Br/2.5 Ba	6: 1 car	9: 1 car	6: 24'	9: 34'	A	1		II	II	
131	1-4	1: 649 sf	4: 974 sf	1: 1 Br/1 Ba	4: 2 Br/1.5 Ba	1: 1 car	4: 1 car	1: 24'	4: 34'	B	3	I			
132	1-4	1: 649 sf	4: 974 sf	1: 1 Br/1 Ba	4: 2 Br/1.5 Ba	1: 1 car	4: 1 car	1: 24'	4: 34'	C	2	II	II		
133	2-3	2: 903 sf	3: 903 sf	2: 2Br/2.5 Ba	3: 2Br/2.5 Ba	2: 1 car	3: 2 car tandem	2: 34'	3: 34'	B	4		I		
134	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	C	3		I		
135	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	A	1		I		
136	1-4	1: 649 sf	4: 974 sf	1: 1 Br/1 Ba	4: 2 Br/1.5 Ba	1: 1 car	4: 1 car	1: 24'	4: 34'	B	2	I			
137	2-3	2: 903 sf	3: 903 sf	2: 2Br/2.5 Ba	3: 2Br/2.5 Ba	2: 1 car	3: 2 car tandem	2: 34'	3: 34'	A	4		I		
138	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	C	3		I		
139	5-7	5: 1021 sf	7: 1133 sf	5: 2Br/2.5 Ba	7: 2Br/2.5 Ba	5: 2 car tandem	7: 2 car tandem	5: 34'	7: 34'	B	2		I		
<b>Provence-Urban Villas</b>															
2008046															
1/10/2011															
<b>Elevation Variation Plotting</b>															
<b>Color Plotting</b>															
Notes:		Areas are approximate, see design drawing for accurate plan areas								Notes:		Please refer to the Color Lists for scheme specifications (Schemes 4-6)			
Bldg No	Bldg type	Areas		Br/Ba		Garage		Height		Elev Style	Color Scheme	Front	Right	Rear	Left
1	3	3: 1702 sf		3: 3 Br/2.5 Ba		3: 2 car tandem		3: 32'		B	5				
2	1-2	1: 1634 sf	2: 1681 sf	1: 3 Br/3.5 Ba	2: 3 Br/2.5 Ba	1: 2 car tandem	2: 2 car tandem	1: 32'	2: 32'	C	bldg1:6/bldg 2:4				
3	1	1: 1634 sf		1: 3 Br/3.5 Ba		1: 2 car tandem		1: 32'		B	5				
4	3	3: 1702 sf		3: 3 Br/2.5 Ba		3: 2 car tandem		3: 32'		AR	6				
5	3	3: 1702 sf		3: 3 Br/2.5 Ba		3: 2 car tandem		3: 32'		B	5				
6	1-2	1: 1634 sf	2: 1681 sf	1: 3 Br/3.5 Ba	2: 3 Br/2.5 Ba	1: 2 car tandem	2: 2 car tandem	1: 32'	2: 32'	A	bldg1:4/bldg 2:6				
7	1	1: 1634 sf		1: 3 Br/3.5 Ba		1: 2 car tandem		1: 32'		B	5				
8	3	3: 1702 sf		3: 3 Br/2.5 Ba		3: 2 car tandem		3: 32'		AR	4				
9	3	3: 1702 sf		3: 3 Br/2.5 Ba		3: 2 car tandem		3: 32'		B	6				
10	1-2	1: 1634 sf	2: 1681 sf	1: 3 Br/3.5 Ba	2: 3 Br/2.5 Ba	1: 2 car tandem	2: 2 car tandem	1: 32'	2: 32'	C	bldg1:4/bldg 2:5				
11	1-2	1: 1634 sf	2: 1681 sf	1: 3 Br/3.5 Ba	2: 3 Br/2.5 Ba	1: 2 car tandem	2: 2 car tandem	1: 32'	2: 32'	A	bldg1:6/bldg 2:4				
12	1-2	1: 1634 sf	2: 1681 sf	1: 3 Br/3.5 Ba	2: 3 Br/2.5 Ba	1: 2 car tandem	2: 2 car tandem	1: 32'	2: 32'	C	bldg1:6/bldg 2:5				
13	1-2	1: 1634 sf	2: 1681 sf	1: 3 Br/3.5 Ba	2: 3 Br/2.5 Ba	1: 2 car tandem	2: 2 car tandem	1: 32'	2: 32'	B	bldg1:4/bldg 2:6				
14	3	3: 1702 sf		3: 3 Br/2.5 Ba		3: 2 car tandem		3: 32'		AR	5				
15	3	3: 1702 sf		3: 3 Br/2.5 Ba		3: 2 car tandem		3: 32'		B	4				
16	1-2	1: 1634 sf	2: 1681 sf	1: 3 Br/3.5 Ba	2: 3 Br/2.5 Ba	1: 2 car tandem	2: 2 car tandem	1: 32'	2: 32'	A	bldg1:5/bldg 2:6				
17	1-2	1: 1634 sf	2: 1681 sf	1: 3 Br/3.5 Ba	2: 3 Br/2.5 Ba	1: 2 car tandem	2: 2 car tandem	1: 32'	2: 32'	B	bldg1:4/bldg 2:5				
18	3	3: 1702 sf		3: 3 Br/2.5 Ba		3: 2 car tandem		3: 32'		A	6				
19	1-2	1: 1634 sf	2: 1681 sf	1: 3 Br/3.5 Ba	2: 3 Br/2.5 Ba	1: 2 car tandem	2: 2 car tandem	1: 32'	2: 32'	C	bldg1:5/bldg 2:4				
20	1	1: 1634 sf		1: 3 Br/3.5 Ba		1: 2 car tandem		1: 32'		B	6				

Elev Enhancement Plotting 02-10-2011.xls

Bldg No	Bldg type	Areas	Br/Ba	Garage	Height	Elev Style	Color Scheme	Front	Right	Rear	Left
21	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	AR	5				
22	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	B	6				
23	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	A	4				
24	1	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32'	C	5				
25	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	BR	4				
39	1-2	1: 1634 sf 2: 1681 sf	1: 3 Br/3.5 Ba 2: 3 Br/2.5 Ba	1: 2 car tandem 2: 2 car tandem	1: 32' 2: 32'	C	bldg1:6/bldg 2:5				
40	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	AR	4				
71	1-2	1: 1634 sf 2: 1681 sf	1: 3 Br/3.5 Ba 2: 3 Br/2.5 Ba	1: 2 car tandem 2: 2 car tandem	1: 32' 2: 32'	B	bldg1:6/bldg 2:5				
72	1-2	1: 1634 sf 2: 1681 sf	1: 3 Br/3.5 Ba 2: 3 Br/2.5 Ba	1: 2 car tandem 2: 2 car tandem	1: 32' 2: 32'	C	bldg1:4/bldg 2:6				
73	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	AR	5				
74	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	B	4				
75	1-2	1: 1634 sf 2: 1681 sf	1: 3 Br/3.5 Ba 2: 3 Br/2.5 Ba	1: 2 car tandem 2: 2 car tandem	1: 32' 2: 32'	A	bldg1:5/bldg 2:6				
76	1	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32'	C	4				
77	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	AR	5				
78	1-2	1: 1634 sf 2: 1681 sf	1: 3 Br/3.5 Ba 2: 3 Br/2.5 Ba	1: 2 car tandem 2: 2 car tandem	1: 32' 2: 32'	C	bldg1:4/bldg 2:6				
79	1-2	1: 1634 sf 2: 1681 sf	1: 3 Br/3.5 Ba 2: 3 Br/2.5 Ba	1: 2 car tandem 2: 2 car tandem	1: 32' 2: 32'	B	bldg1:5/bldg 2:4				
80	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	AR	6				
81	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	B	5				
82	1-2	1: 1634 sf 2: 1681 sf	1: 3 Br/3.5 Ba 2: 3 Br/2.5 Ba	1: 2 car tandem 2: 2 car tandem	1: 32' 2: 32'	C	bldg1:6/bldg 2:4				
83	1	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32'	B	5				
84	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	AR	6				
85	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	B	5				
86	1-2	1: 1634 sf 2: 1681 sf	1: 3 Br/3.5 Ba 2: 3 Br/2.5 Ba	1: 2 car tandem 2: 2 car tandem	1: 32' 2: 32'	A	bldg1:4/bldg 2:6				
87	1	1: 1634 sf	1: 3 Br/3.5 Ba	1: 2 car tandem	1: 32'	C	5				
88	3	3: 1702 sf	3: 3 Br/2.5 Ba	3: 2 car tandem	3: 32'	AR	4				

**PROVENCE – COURTS / RC NATOMAS, LLC  
 COLOR SCHEMES / SAAP NO. 2008046  
 January 03, 2011**

COLOR SCHEMES	• PLASTER 1 (LOWER)	• PLASTER 2 (UPPER)	• PLASTER TRIM • PLASTER WINDOW TRIM • DECORATIVE PLASTER PROJECTION	• FASCIA • RAKE BOARD • ENTRY DOOR • GARAGE DOOR • FRENCH DOOR	• SHUTTER	• METAL RAIL / GUARDRAIL	• STONE PATTERN VENEER	• COMPOSITE'S' ROOF TILE
1	CL 2923	CL 2911	CL 3163	CL 3165	CL 3105	CL 3226	ADANTE	CLAY
2	CLC 1250	CLC 1249	CL 2934	CL 2955	CL 2996	CL 3226	UMBRIA	RED FLASHED
3	CL 2814	CL 2813	CL 2506	CL 2507	CL 2496	CL 3226	MESETA	CLAY FLASHED
4	CL 2924	CL 2913	CL 2914	CL 3014	CL 3016	CL 3226	PADOVA	CLAY FLASHED

ALL COLOR TO BREAK AT INSIDE CORNERS. RAIN GUTTERS AND DOWNSPOUTS TO BE PAINTED TO MATCH ADJACENT SURFACE – I.E., GUTTERS TO MATCH FASCIA, DOWNSPOUTS TO MATCH STUCCO.

- PLASTER BY EXPO STUCCO - 16/20 SAND FINISH AND HEAVY TEXTURE PLASTER (HEAVY DASH).
- PAINT IS BY FRAZEE (CLW/CLC/CLV/CL 0000).
- STONE PATTERN VENEER: COLOR / STONE TYPE / TEXTURE SIMILAR TO ELDERADO FIELDLEDGE.
- ROOF TILE COMPOSITE 'S' TILE MaxiLITE P10 BY MAXITILE.
- Fourth Color Scheme added 12/07/10
- Changes highlighted 01/03/11, per Jim Murar's request.

**PROVENCE – URBAN VILLAS / RC NATOMAS, LLC  
 COLOR SCHEMES / SAAP NO. 2008046  
 June 17, 2010**

COLOR SCHEMES	• PLASTER 1 (LOWER)	• PLASTER 2 (UPPER)	• PLASTER TRIM • WINDOW TRIM • DECORATIVE PLASTER PROJECTION	• FASCIA • ENTRY DOOR • GARAGE DOOR	• METAL RAIL / GUARDRAIL • SHUTTER	• FLAT COMPOSITE ROOF TILE
4	CL 2802	CL 2792	CL 2846	CL 2925	CL 2676	FLASHED BROWN
5	CL 1873	CL 2762	CL 2785	CL 2866	CL 2887	OXFORD GRAY
6	CL 2834	CL 2923	CL 2835	CL 2936	CL 2937	FLASHED OXFORD

ALL COLOR TO BREAK AT INSIDE CORNERS. RAIN GUTTERS AND DOWNSPOUTS TO BE PAINTED TO MATCH ADJACENT SURFACE – I.E., GUTTERS TO MATCH FASCIA, DOWNSPOUTS TO MATCH STUCCO.

- PLASTER BY EXPO STUCCO - 16/20 SAND FINISH.
- PAINT IS BY FRAZEE (CLW/CLC/CLV/CL 0000).
- ROOF TILE COMPOSITE FLAT TILE MaxiSHAKE / MaxiSLATE BY MAXITILE.



PROVENCE - COURTS  
UNIT 1/4  
FRONT ELEVATION  
ELEVATION STYLE 'B'





PROVENCE - COURTS  
UNIT 2/3  
ENHANCED RIGHT ELEVATION  
ELEVATION STYLE 'B'





PROVENCE - COURTS  
UNIT 5/7  
ENHANCED RIGHT ELEVATION  
ELEVATION STYLE 'A'





PROVENCE - COURTS  
UNIT 6/9  
FRONT ELEVATION  
ELEVATION STYLE 'A'





PROVENCE - COURTS  
UNIT 8  
FRONT ELEVATION  
ELEVATION STYLE 'C'



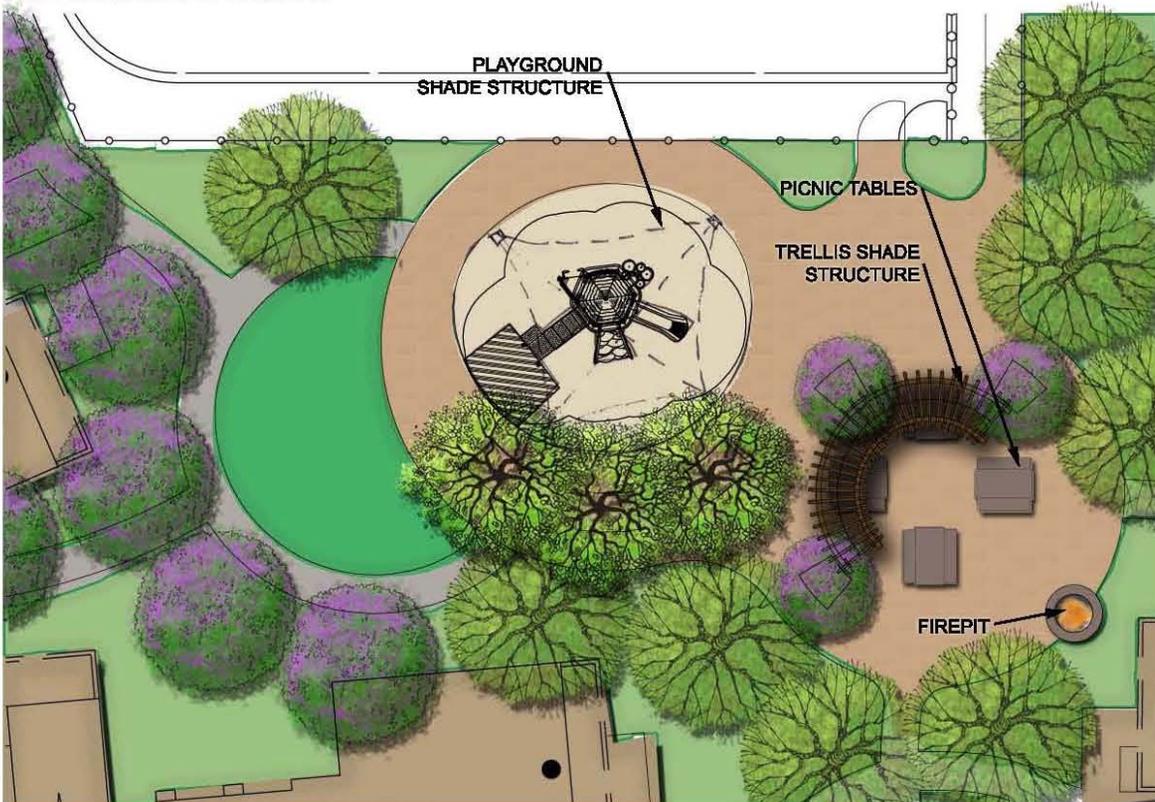


PROVENCE - COURTS  
UNIT 10/11  
FRONT ELEVATION  
ELEVATION STYLE 'C'



Exhibit 4L – Playground Shade Structure and Paseo Concept Plans

NOTE: CONSIDERING OPTIONS FOR SHADE STRUCTURE.  
FINAL DESIGN TO BE DETERMINED.



# PROVENCE

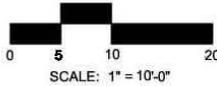
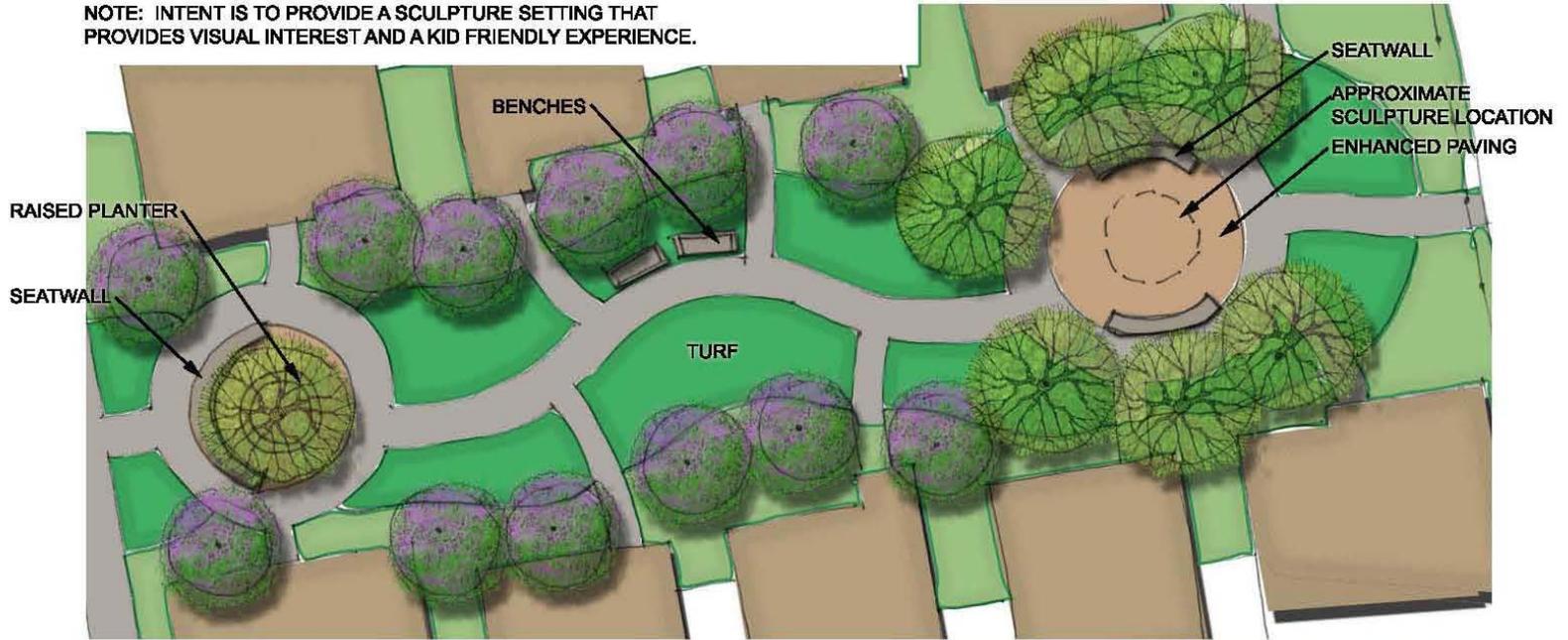
RC NATOMAS, LLC

SACRAMENTO, CA



PLAYGROUND SHADE EXHIBIT

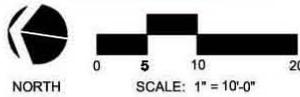
NOTE: INTENT IS TO PROVIDE A SCULPTURE SETTING THAT PROVIDES VISUAL INTEREST AND A KID FRIENDLY EXPERIENCE.



**PROVENCE**  
RC NATOMAS, LLC

02/22/2011  
SACRAMENTO, CA   
SOUTH PASEO CONCEPT PLAN

NOTE: INTENT IS TO PROVIDE A SCULPTURE SETTING THAT PROVIDES VISUAL INTEREST AND A KID FRIENDLY EXPERIENCE.



**PROVENCE**  
 RC NATOMAS, LLC

SACRAMENTO, CA 02/22/2011  
  
 NORTH PASEO CONCEPT PLAN

**Attachment 5: Private Recreational Facilities Agreement – Draft Resolution**

**RESOLUTION NO.**

Adopted by the Sacramento City Council

**APPROVING AN AGREEMENT: PROVENCE (P09-006)**

**BACKGROUND**

- A. On March 28, 2006, the City entered into City Agreement No. 2006-0298 allowing DR Horton Inc. to construct and maintain private recreational facilities (a swimming pool) to serve its 187 unit Carriage Lane III development project (P05-079). In exchange, DR Horton Inc. received 5% parkland dedication credit, valued at \$67,650.
- B. The final map for Carriage Lane III recorded on September 13, 2007, after which five model homes were constructed.
- C. In March 2009, RC Natomas LLC submitted an application to resubdivide the Carriage Lane III project site (with the exception of the five model homes) to develop 233 multi-family units. RC Natomas LLC has requested a new Agreement to Construct and Maintain Private Recreational Facilities to serve the 233 multi-family units, in addition to the five model units, for the Provence development project (P09-006).
- D. City Agreement No. 2006-0298 is specific to the Carriage Lane III project and is not transferable to the Provence project due to the re-subdivision and change in the number of residential units.
- E. Pursuant to Section 16.64.100 of City Code, the City may enter into an agreement to grant parkland dedication credit for eligible private recreational facilities such as pools, recreational buildings and court areas, each of which is eligible for up to 5% of the total parkland dedication requirement.
- F. The agreement that is the subject of this Resolution for the Provence development project (P09-006) stipulates that the facilities shall be adequately maintained by the RC Natomas LLC and shall not be removed from use without consideration by City Council.

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL RESOLVES AS FOLLOWS:**

- Section 1. The City Manager, or City Manager's designee, is authorized to execute an agreement for private recreational facilities and parkland fee credit with RC Natomas LLC under Sacramento City Code Chapter 16.64 for the 233 unit Provence development project (P09-006). The agreement also includes the five residential units from the Carriage Lane III project (P05-079), and satisfies 5% of the parkland dedication requirement for the Provence project. The agreement is valued at \$86,657.
- Section 2. The agreement identified in Section 1 is attached as Exhibit A and made a part of this Resolution.

**Table of Contents:**

Exhibit 5A – Agreement to Construct and Maintain Private Recreational Facilities – 4 pages

Exhibit 5A – Agreement to Construct and Maintain Private Recreational Facilities

DRAFT

Record for the benefit of )  
The City of Sacramento )

Fee Exempt Pursuant to )  
Government Code )  
Section 6103 )

When Recorded, Mail to: )

Office of the City Clerk )  
Historic City Hall )  
915 "I" Street, 1<sup>st</sup> Floor )  
Sacramento CA 95814 )

(Space above this line for recorder's use)

AGREEMENT TO CONSTRUCT AND MAINTAIN  
PRIVATE RECREATIONAL FACILITIES  
(Sacramento City Code Chapter 16.64)  
Provence P09-006

This Agreement is made as of \_\_\_\_\_, 2011 by and between RC Natomas LLC ("Owner") and the City of Sacramento ("City").

RECITALS

1. Owner owns that real property in the City of Sacramento, County of Sacramento, shown and described in Exhibit A - Legal Description and Exhibit A-1 - Plat to Accompany Description, attached and incorporated herein by reference ("the Property").

2. Previous Owner (DR Horton Inc.) obtained approval from City for a tentative subdivision map and special permit for the Property known as Carriage Lane III P05-079 and entered into an Agreement to construct and maintain private recreational facilities to serve the Project, for which it received 0.082 acres in park dedication credits valued at \$67,650 (\$687,750 / acres x 0.082 acres x 1.2). A final map was recorded to create 187 single family residential lots.

3. Owner has obtained approval from City for a tentative subdivision map to resubdivide the Property and a special permit for the Property known as Provence P09-006 ("Project"), subject to certain conditions relating to the dedication of parkland and/or the payment of in-lieu fees for park and recreational facilities to serve the Project pursuant to Sacramento City Code Chapter 16.64 ("Chapter 16.64").

4. Owner has proposed to construct and maintain privately owned open space and/or recreational facilities in the Project for which credits may be granted under sections 16.64.100 and 16.64.110 of Chapter 16.64, and City has agreed to grant the credit as follows:

The owner shall construct a private recreational swim area (swimming pool) and receive 5% credit towards the required dedication for park purposes for the Project (the "Private Recreation Facilities").

5. Granting of the credit is conditioned on Owner entering into an agreement with City providing that the private recreational facilities shall be constructed and adequately maintained in perpetuity, and that use of the facilities shall be limited to park and local recreation purposes and shall not be changed to another use without the express written consent of the City Council.

5. Owner and City enter into this Agreement to provide for the construction, maintenance, and use of the private open space and/or recreational facilities and the granting of credits pursuant to Chapter 16.64.

#### AGREEMENT

NOW, THEREFORE, the parties agree as follows:

6. Owner shall construct the Private Recreational Facilities as described above in section 4 to the satisfaction of the City's Director of Parks and Recreation. Construction of the Private Recreational Facilities shall be completed no later than issuance of occupancy permits for 50% of the residential units (119 units out of 238 total units) constructed within the project.

7. City shall grant to Owner a credit of 0.105 acres against the required parkland dedication and/or payment of in-lieu fees described above in section 3.

8. Owner shall, in perpetuity, maintain the Private Recreational Facilities in good repair and condition and in compliance with all applicable City Code requirements.

9. Owner shall, in perpetuity, allow use of the Private Recreational Facilities for the residents of the Project and shall not put the Private Recreational Facilities to another use without the express written consent of the City Council.

10. The provisions of this Agreement are in furtherance of and satisfy the conditions of approval of the Project. A breach of this Agreement shall be an abrogation of the contractual agreement that arises from the conditions of approval of the Subdivision and shall render Owner and its successors, heirs, and assigns, liable to City under the Subdivision Map Act (Government Code section 66410 et seq.), City Code Chapter 16.64, and any other applicable state and local ordinances and statutes. The provisions of this Agreement are covenants which will run with the land and will be binding in perpetuity on all parties having or acquiring any right, title, or interest in the Property, including a homeowners association or other legal entity that assumes any right, title, or interest in the Private Recreational Facilities constructed by Owner, and will inure to the benefit of CITY in furtherance of the public welfare.

11. Should Owner, its successors, heirs, and assigns, breach this Agreement, City may seek injunctive relief to enforce this Agreement, or may, at City's option, recover from Owner, its successors, heirs, and assigns, the sum of \$86,394 (\$687,500 per acre x 0.105 acres x 1.2) with interest thereon from the date of this Agreement until the date of payment. Interest shall be calculated at the average rate earned during that period by City's Pool A or such other average rate earned by City on its general investments during that period.

12. In any legal action brought by either party to enforce the terms of this agreement, the prevailing party is entitled to all costs incurred in connection with such an action, including reasonable attorney fees.

13. If any term or provision of this Agreement is held invalid or unenforceable, the remainder of this Agreement shall not be affected; provided that the intent of the Agreement may then be reasonably fulfilled.

14. No waiver by the City of any breach of or default by Owner under this Agreement shall be deemed to be a waiver of any other or subsequent breach or default.

15. This Agreement shall be recorded in the office of the Sacramento County Recorder.

This Agreement has been entered as of the date first above written.

"Owner"

By \_\_\_\_\_

Printed Name: \_\_\_\_\_

Title: \_\_\_\_\_

City of Sacramento

By \_\_\_\_\_  
Cassandra H.B. Jennings, Assistant City Manager  
For: Gus Vina, Interim City Manager

APPROVED AS TO FORM

\_\_\_\_\_  
DEPUTY CITY ATTORNEY

ATTEST:

\_\_\_\_\_  
CITY CLERK

NOTARIZATION REQUIRED

Attach Exhibit A and A-1 – Legal Description, Plat Map and Project Map

AGREEMENT TO CONSTRUCT AND MAINTAIN  
PRIVATE RECREATIONAL FACILITIES  
Provence P09-006  
Page 4 of 4

**Attachment 6: WALKSacramento Comment Letter**



March 15, 2011

David Hung  
Associate Planner  
Community Development Department  
300 Richards Boulevard, 3rd Floor  
Sacramento, CA 95811

**RE: Provence (P09-006)**

Dear Mr. Hung:

We appreciate the effort the applicant has made to improve the pedestrian circulation and provide the greatest opportunities for walking. The project site is adjacent to shopping, restaurants, and office – as such, it should be in demand by people that want a walkable residence north of the American River.

WALKSacramento would like to identify several pedestrian-friendly features of the proposed Provence residential project.

Tree shade on hardscape is important for providing pleasant pedestrian environment, and in the Sacramento region climate tree shade is crucial to creating a hospitable outdoor environment.

**The Provence project will provide an exceptionally tree-shaded environment for pedestrians.**

- Many of the private (alley) streets in this project include small trees such as Crape Myrtle and Crab Apple. The private streets that are not alleys have many mid-size trees within setbacks and street planters
- Shade trees currently stand within the street planters between the public streets and sidewalks on three sides of the project.
- The landscaped areas of the project have many small- and mid-size trees that will shade the walkways on the site.

**Pedestrians and cyclists will have six convenient points of access to the multi-use trail to be constructed along the southern edge of the site.**

- This number of access points on a trail segment of less than 500' is outstanding.
- Direct path-of-travel to the trail is provided for all residents and should be a great amenity for the future residents.

Related to these two beneficial components of the project mentioned above, there are two concerns that should be addressed.

1. The preliminary landscape plan seems to show a few trees planted on walkways. We would like to see all of the trees planted and the 50% shading requirement to actually be met. **We recommend staff review of the final landscape plan before occupancy.**
2. Special consideration should be given to the multi-use trail and the public sidewalk at the intersection of East Commerce Way and future Snowy Egret Way. The interface between bicyclists, pedestrians, and vehicles is critical for everyone's safety and comfort. **We recommend that the trail design be routed to the Alternate Modes Coordinator for review to insure the trailhead is designed correctly.**

*WALKSacramento* encourages people to walk and bicycle in their communities. The benefits include improved physical fitness, less motor vehicle traffic congestion, better air quality, and a stronger sense of cohesion and safety in local neighborhoods. *WALKSacramento* is working to support increased physical activity such as walking and bicycling in local neighborhoods as well as helping to create community environments that support walking and bicycling.

Thank you for your consideration of these comments and recommendations. If you have questions or need additional information, please contact me at (916) 446-9255 or [cholm@walksacramento.org](mailto:cholm@walksacramento.org).

Sincerely,



Chris Holm  
Project Analyst

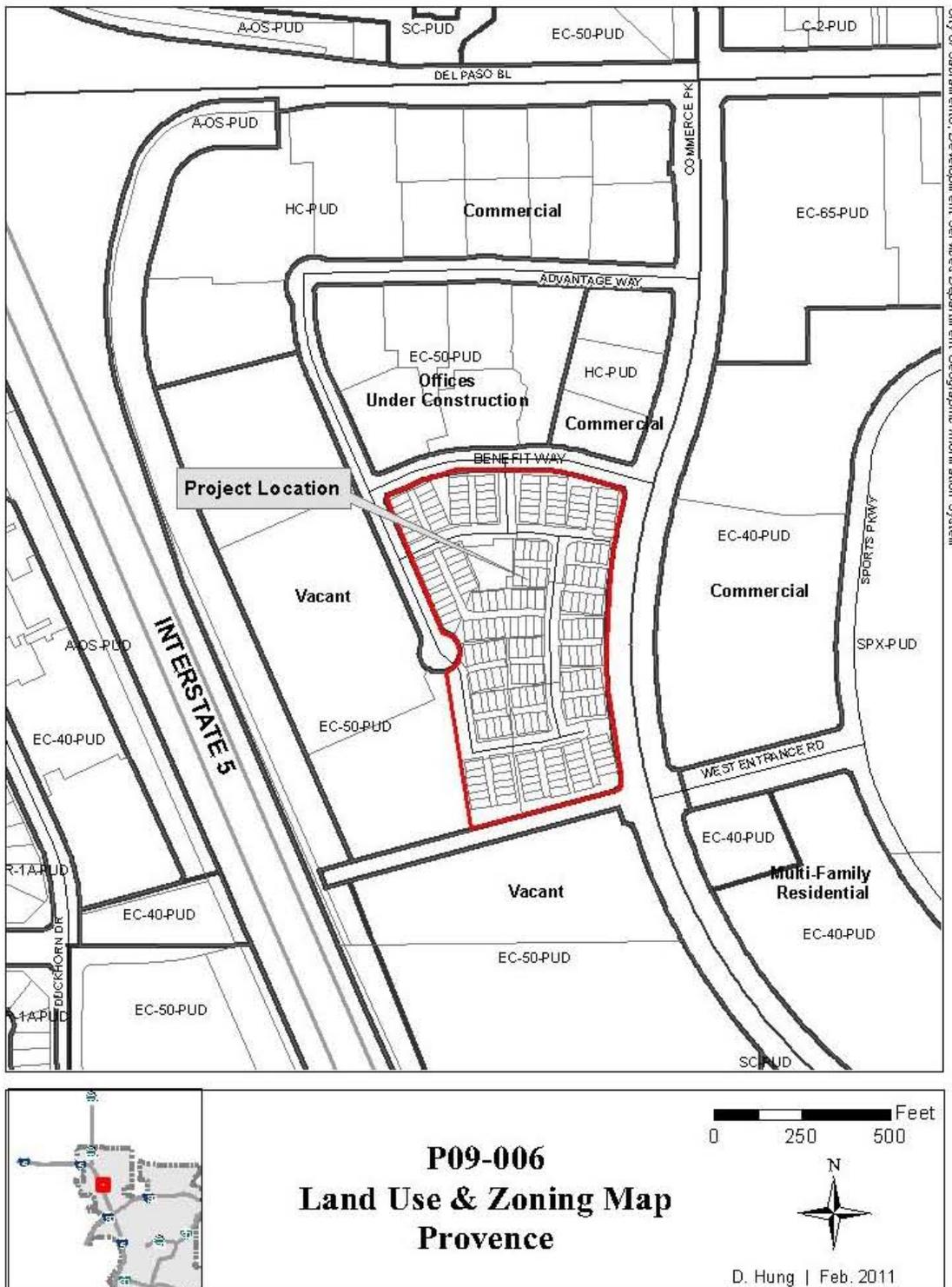
**Attachment 7: Mitigated Negative Declaration**

A Copy of the Mitigated Negative Declaration  
is available for review at:

City of Sacramento  
Community Development Department  
300 Richards Boulevard, 3<sup>rd</sup> Floor  
Sacramento, CA 95811  
9 a.m. to 4 p.m.  
Monday through Friday

It is also currently posted on the Community Development Department's web site at:  
<http://www.cityofsacramento.org/dsd/planning/environmental-review/eirs/>

**Attachment 8: Land Use & Zoning Map**



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# REPORT TO PLANNING COMMISSION City of Sacramento

# 4

915 I Street, Sacramento, CA 95814-2671

**PUBLIC HEARING**  
**March 24, 2011**

To: Members of the Planning Commission

**Subject: Courtyard Condominiums (P10-089)**  
**Addendum to March 10, 2011 Staff Report**

## BACKGROUND:

At the March 10<sup>th</sup> Planning Commission meeting, after the close of public testimonies, the commission deliberated and voted to continue the Courtyard Condominiums project ('Project') to the March 24<sup>th</sup> Planning Commission meeting to allow time for the applicant and neighbors to engage in further discussions on the issues surrounding the project. It was noted that no additional noticing will be required since the item was being continued. Since there is no new information to report at the time of the filing of this report, the March 10, 2011 staff report is enclosed as Attachment 1. Also, the supplemental materials presented at the March 10<sup>th</sup> meeting are enclosed under Attachment 2.

## RECOMMENDATION:

Staff recommends the Planning Commission take the following actions:

- A.** Finds that the Project is exempt from review under Section 15332 (Infill) of the California Environmental Quality Act Guidelines;
- B.** Approve the **Tentative Map** to create one condominium lot on approximately 0.19 acres in the Multi-Family (R-3A) zone;
- C.** Approve the **Special Permit** to develop six condominium units in the Multi-Family (R-3A) zone;
- D.** Approve the **Special Permit** to reduce one required parking space for a multi-family development;
- E.** Approve the **Special Permit** to reduce the required street side setback from five feet to two feet for an accessory structure; and

- F. Approve the **Variance** to waive the required trash enclosure for a multi-family development.

Respectfully submitted by: David Hung  
DAVID HUNG  
Associate Planner

Approved by: Sandra Yope  
SANDRA YOPE  
Senior Planner

Recommendation Approved:

Gregory Bitter  
GREGORY BITTER, AICP  
Principal Planner

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Page iv	Attachment 2 – Supplemental Materials

## **Attachment 1**

### **March 10, 2011 Staff Report (P10-089)**



# REPORT TO PLANNING COMMISSION City of Sacramento

915 I Street, Sacramento, CA 95814-2671

**PUBLIC HEARING**  
**March 10, 2011**

To: Members of the Planning Commission

**Subject: Courtyard Condominiums (P10-089)**

A request to develop six condominium units on approximately 0.19 acres in the Multi-Family (R-3A) zone.

- A. Environmental Determination: Exempt (CEQA Guidelines Section 15332 for Infill Development);
- B. Tentative Map to create one condominium lot on approximately 0.19 acres in the Multi-Family (R-3A) zone;
- C. Special Permit to develop six condominium units in the Multi-Family (R-3A) zone;
- D. Special Permit to reduce one required parking space for a multi-family development;
- E. Special Permit to reduce the required street side setback from five feet to two feet for an accessory structure;
- F. Variance to waive the required trash enclosure for a multi-family development.

**Location/Council District:**

Northwest Corner of T Street and 24<sup>th</sup> Street, Sacramento, CA

Assessor's Parcel Number 010-0036-011-0000 and 010-0036-012-0000

Council District 4

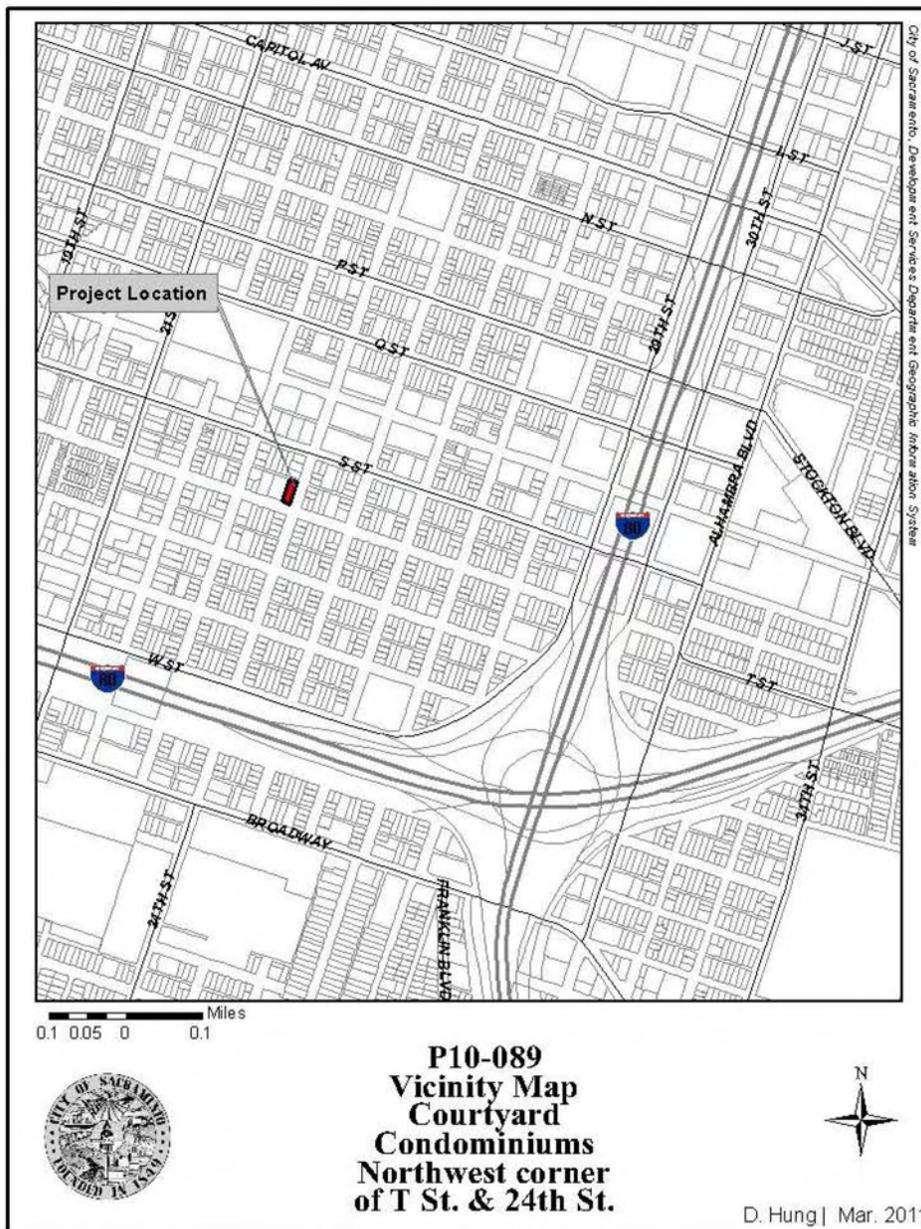
**Recommendation:** Staff recommends the Commission approve the request based on the findings and subject to the conditions listed in Attachment 1. **The project is controversial as surrounding neighbors are opposed to the density and design of the project.** The Commission has final approval authority over items A-F above, and its decision is appealable to City Council.

**Contact:** David Hung, Associate Planner, (916) 808-5530; Sandra Yope, Senior Planner, (916) 808-7158

**Applicant:** Rosen Development LLC, Attn: Andrea Rosen, (916) 508-6721, 2226 Portola Way, Sacramento, CA 95818

**Owner:** Rosen Development LLC, Attn: Andrea Rosen, (916) 508-6721, 2226 Portola Way, Sacramento, CA 95818; Benjamin Rosen, (916) 761-1912

Vicinity Map



**Summary:** The current application request is for the necessary entitlements to develop six condominium units on approximately 0.19 acres. The project is controversial as surrounding neighbors are opposed to the density and design of the project. Staff notified all property owners within 500 feet of the site for this public hearing.

<b>Table 1: Project Information</b>
<b>General Plan designation:</b> Traditional Neighborhood Medium Density (8 to 21 units)
<b>Existing zoning of site:</b> R-3A
<b>Existing use of site:</b> Vacant (previously residential)
<b>Property area:</b> 0.193 acres (8,428 square foot)

**Background Information:** The site was previously developed with one single-family home which was approved for demolition by the Preservation Director on June 21, 2010 (file IR10-196). The building was demolished in September of 2010 and the site is now vacant. The applicant has previously submitted a Zoning Administrator application for the development of six apartment units (Z10-142) on September 9, 2010. Due to intense opposition by neighbors at a community meeting on November 18, 2010, staff decided to elevate the project from the Zoning Administrator level to the Planning Commission level and from Design Director level to the Design Commission level. On December 17, 2010, the applicant submitted a new Planning Commission application to develop six condominium units in place of the withdrawn Zoning Administrator file; the application will also be subject to approval by the Design Commission.

**Public/Neighborhood Outreach and Comments:** The project was routed to various advisory groups including the Newton Booth Neighborhood Association, the Southside Neighborhood Association and the Richmond Grove Neighborhood Association. An Early Notice was also sent to property owners within 500 feet radius of the project site on February 11, 2011. During the processing of the project, staff received support letters from WALKS Sacramento, Sacramento Area Bicycle Advocates (SABA), Sacramento Area Council of Governments (SACOG), Sacramento Metropolitan Air Quality Management District (SMAQMD), Midtown Business Association, Regional Transit, Friends of Light Rail & Transit, Policy in Motion, Design Sacramento 4 Health and Smart Growth Leadership Recognition Program as well as various community members. Some of the reasons for the support include: 1) The project will help reduce vehicle trips due to its proximity to light rail; 2) The project promotes walking and bicycling due to its proximity to nearby shops and offices; 3) The project is consistent with the General Plan goals for density goals and diversity in housing; 4) The project is a quality infill development with buildings that help activate the street frontages. Support letters are attached to the staff report.

Staff also received a number of letters of opposition to the project which are attached to the staff report. It was due to the overwhelming opposition that staff has elevated the project to the Planning Commission and Design Commission. Neighbors who opposed the project expressed concerns of the addition of six new units in a densely populated neighborhood, that the project will overburden the on-street parking in the

neighborhood, and that the proposed architectural design does not blend with the surrounding buildings. Some neighbors were concerned that they weren't notified of the demolition of the previous home on the lot; however, the demolition was not subject to notification to neighbors. The applicant has also met with many of the neighbors to discuss the project; in response to concerns over the original proposal for apartment units, the applicant is now requesting for condominium units.

Staff received two separate petitions to oppose the project. The first petition (attached), submitted on October 18, 2010, by a group of 21 neighbors, stated the following concerns:

1. There are already multiple apartment complexes in the neighborhood.
2. Parking in the neighborhood is already a problem.
3. The architectural design is out of character with the neighborhood.
4. The project will disrupt the existing condition of the neighborhood.

A second petition (attached) came from a group of 40 neighbors and described the following concerns:

1. The overall historic and aesthetic profile of a neighborhood that is already challenged by inappropriate and haphazard design approvals from prior decades.
2. The already overwhelming density of on-street parking.
3. The delicate and tenuous balance that presently exists between single family residences and large multi-unit complexes.

The Concerned Neighbors of Newton Booth and Poverty Ridge has suggested the following stipulations to the project:

1. A reasonable increase of density from that of the previous single-dweller, one-story home on .19325 acres, to three housing units.
2. An architectural style of good faith integrity that is appropriate to the immediate surrounding neighborhood within a 300 square foot radius--an area that includes, albeit not exclusively, Tudor cottages, Craftsman bungalows, and Depression-era brick duplexes.
3. Pitched roofs with a height not markedly greater than that of the Mirabella Apartments immediately adjacent to the site on T Street.
4. Setbacks that adhere to city requirements.
5. Porches that evoke an "eyes on the neighborhood" affect while still adhering to the aforementioned setback requirements.
6. Off-street parking accommodations provided for all units.

In light of these comments, the applicant expressed that a feasible project will require up to six units. The design concept of the buildings is to echo the Prairie style mansions and some of the Art Deco apartment buildings in the neighborhood and by adding interest to the existing buildings with Tudor and Craftsman style architecture. The two residential structures on the site comply with all setback requirements; only the accessory structure encroaches into the street side setback. Most units have raised stoop entries that faces the street. The project requires the reduction of just one parking space.

**Environmental Considerations:** The Community Development Department, Environmental Planning Services Division has reviewed this project and determined that this is exempt from the provisions of the California Environmental Quality Act (CEQA) Section 15332, In-Fill Development Projects. The project is consistent with the applicable general plan designations and all applicable general plan policies as well as with applicable zoning designations and regulations, occurs within city limits on a project site that is no more than 5 acres substantially surrounded by urban uses, site has no habitat value for endangered, rare or threatened species, site can be adequately served by all required utilities and public services, and would not result in any significant effects relating to traffic, air quality, noise or water quality.

**Policy Considerations:**

Following is how the proposed project adhere to policies within the 2030 General Plan, the Central City Community Plan, Zoning, Smart Growth principles and Multi-Family Design Principles.

***General Plan/Zoning***

The 2030 General Plan Update was adopted by City Council on March 3, 2009. The 2030 General Plan's goals, policies, and implementation programs define a roadmap to achieving Sacramento's vision to be the most livable city in America. The 2030 General Plan Update designation of the subject site is Traditional Neighborhood Medium Density which provides for provides for higher intensity medium-density housing and neighborhood-support uses and allows a density from 8 units per acre to 21 units per acre. The 2030 General Plan has identified goals and policies under the Land Use and Urban Design Element and the Housing Element. Some of the goals and policies supported by this project are:

1. *Land Use and Urban Design Element (Goal LU 4.1) Neighborhoods.* Promote the development and preservation of neighborhoods that provide a variety of housing types, densities, and designs and a mix of uses and services that address the diverse needs of Sacramento residents of all ages, socio-economic groups, and abilities.
2. *Replacement of Non-Conforming Densities in Traditional Neighborhoods. (Policy LU 4.3.2)* The City shall preserve the existing diversity of housing types and densities on each block of Traditional Neighborhoods. Where proposed residential development on a parcel within a Traditional Neighborhood block would exceed the maximum allowed density, the City may allow the development if it would not cause the overall density for the block to be exceeded. Where the density of existing development on a Traditional Neighborhood block falls outside the applicable density range of its land use designation, the City shall allow replacement development on the parcel that maintains the same density.
3. *Housing Element (Policy H-2.2.1)* The City shall promote quality residential infill development through the creation/adoption of flexible development standards and with funding resources.

4. *Central City Community Plan. Land Use and Urban Design (CC.LU 1.3)*  
Interrelated Land Uses. The City shall provide for organized development of the Central City whereby the many interrelated land use components of the area support and reinforce each other and the vitality of the community.

Based on the General Plan land use designation (Traditional Neighborhood Medium Density), the site may be developed to a density from 8 units per acre to 21 units per acre. The size of the subject parcel is 0.193 acres (8,428 square feet), which would allow the site to be developed with two (2) to four (4) units.

The General Plan does provide policy direction for the replacement of non-conforming densities in Traditional neighborhoods. Within a Traditional Neighborhood, Land Use policy 4.3.2 (see above), allows the density on a particular parcel to exceed the maximum General Plan density if it would not cause the overall density for the Traditional Neighborhood block to be exceeded. The proposed project is situated on a block approximately 2.4 acres in size. Based on the maximum density of 21 units per acre, a total of 50 units could be allowed on this block. Staff has tabulated that approximately 37 units currently exist on the block containing the subject site, and with the addition of six units, the block will be under 50 units. Therefore, in considering the entire block, the development does not cause the overall density for the block to be exceeded.

Development on the subject site is restricted to six (6) units based on the parcel's zoning designation of Multi-Family (R-3A). The R-3A zone allows a maximum density of 36 units and a minimum lot size of 1,200 square feet per residential unit. The proposed project is located on 8,428 square feet and can therefore, accommodate a maximum of six (6) units.

Overall, the proposed project meets the 2030 General Plan goals and policies related to Citywide Land Use and Urban Design for development within the Traditional Neighborhood Medium Density designation.

***Smart Growth Planning Principles:***

“Smart Growth” is a term coined by the United States Environmental Protection Agency (USEPA) as an umbrella term for the many initiatives intended to address some of the negative consequences of urban sprawl. Smart Growth generally occurs when development patterns are sustainable and balanced in terms of economic objective, social goals, and use of environmental/natural resources. The following Smart Growth principles apply to the proposed project:

- Higher-density, cluster development.
- Multi-modal transportation and land use patterns that support walking, cycling and public transit.
- Streets designed to accommodate a variety of activities.
- Planned and coordinated projects between jurisdictions and stakeholders.

The proposed project has been designed to incorporate many of the Smart Growth Principles listed above.

**Multi-Family Residential Design Guidelines:**

The Multi-Family Residential Design Guidelines were approved by the City Council on August 5, 2000 (Resolution CC2000-487). This document articulates design principles for multi-family residences to assist the Planning Commission, City Council, City staff and project planners and designers by identifying the City’s design criteria for multi-family development. The intent is to achieve well-designed projects to enhance the community’s overall value and appearance. The project is generally consistent with the Multi-Family Residential Design Guidelines as identified in the building design section of this staff report.

**Project Design:**

The following discusses project in relation to Tentative Map, Special Permits and Variance.

**Land Use**

**Tentative Map**

Map Design: The tentative map proposes to merge two parcels and create one condominium parcel on the resulting parcel in the R-3A zone; the tentative map design is summarized below:

<b>Table 2: Map Design Summary</b>			
<b>Parcel No.:</b>	<b>Lot Size:</b>	<b>Lot Description:</b>	<b>Use:</b>
One Condominium Parcel	0.19 acres	Corner Lot	Condominiums

Vehicular Circulation and Parking: The subject site is located on the northwest corner of T Street and 24<sup>th</sup> Street. Both T Street and 24<sup>th</sup> Street are two-way public streets. Off-street parking for the proposed project is provided from the alley to the north of the site; the previous driveway on 24<sup>th</sup> Street will be closed. On-street parking is available on both street frontages.

Pedestrian Circulation: Existing sidewalk and curb are found at the frontage on T Street and 24<sup>th</sup> Street. The project does not impact or change existing circulation in and around the site.

Walls and Fencing: The courtyard area will be walled and gated for private use by the tenants. A new six-foot wood fence will be constructed on the west property line adjacent to the existing residential development.

On January 19, 2011, the Subdivision Review Committee, with all ayes, voted to recommend approval of the proposed Tentative Map, subject to the conditions of approval as found in Attachment 1.

In evaluating tentative maps, the Commission is required to make the following findings:

- A. None of the conditions described in Government Code Section 66474, subsection (a) through (g), inclusive, exist with respect to the proposed subdivision;
- B. The proposed subdivision, together with the provisions for its design and improvement, is consistent with the City General Plan and Title 16 Subdivisions of the City Code, which is a specific plan of the City (Gov. Code §66473.5);
- C. The discharge of waste from the proposed subdivision into the existing community sewer system will not result in a violation of the applicable waste discharge requirements prescribed by the California Regional Water Quality Board, Central Valley Region, in that existing treatment plants have a design capacity adequate to service the proposed subdivision (Gov. code §66474.6);
- D. The design of the proposed subdivision provides, to the extent feasible, for future passive or natural heating and cooling opportunities (Gov. Code §66473.1);
- E. The Planning Commission has considered the effect of the approval of this tentative subdivision map on the housing needs of the region and has balanced these needs against the public service needs of its residents and available fiscal and environmental resources (Gov. Code §66412.3).

Staff recommends approval of the Tentative Map with conditions as it is consistent with the policies of the General Plan and Title 16 of the City Code. The site is physically suitable for the type of development proposed and suited for the proposed density; the design of the subdivision and the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife their habitat; the design of the subdivision and the type of improvements are not likely to cause serious public health problems, and the design of the subdivision and the type of improvements will not conflict with easements, acquired by the public at large, for access through or use, of, property within the proposed subdivision. The project will not overly burden the sewer system, nor will it preclude future passive or natural heating and cooling opportunities.

### ***Special Permit for Condominium Development***

The applicant proposes to develop six condominium units on approximately 0.19 acres in the R-3A zone. Section 17.192 of the zoning code permits condominium development with the issuance of a special permit by the Planning Commission. In evaluating special permit proposals of this type, the Commission is required to make the following findings:

- A. A special permit shall be granted upon sound principles of land use.

The project, as conditioned, is based upon sound principles of land use in that:

- a. The proposed multi-family use is located within close proximity to transit, commercial developments and parks; and
- b. The proposed multi-family use is well serviced by auto, transit, bicycle, and pedestrian linkages.

B. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.

The project, as conditioned, will not be detrimental to the public health, safety, and welfare, and will not result in the creation of a public nuisance, in that the site design and the building design are consistent with the Multi-Family Residential Design Principles and all building code requirements.

C. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.

The project is consistent with policies of the General Plan and Central City Community Plan regarding density and housing.

Staff supports the Special Permit for **up to** six condominium units; as discussed in the policy section above, in considering the entire block, the development does not cause the overall density for the block to be exceeded. The project site is close to transit and commercial developments and promotes walking and bicycling opportunities.

**Access, Circulation and Parking**

Vehicular access to the project site is provided at the alley to the north of the site. Following is a summary of the vehicular parking.

<b>Table 3: Parking</b>			
<b>Type of Parking</b>	<b>Required Parking</b>	<b>Proposed Parking</b>	<b>Difference</b>
Vehicular	6 (one per unit)	5	1 deficient

According to the Sacramento City Code (Section 17.192), a minimum of 1 parking space per unit is required. Therefore, the proposal is required to provide a minimum of six parking spaces. The applicant is proposing five spaces in the garage accessory structure facing the alley; the applicant is requesting the approval of the reduction of one parking space for the sixth unit. The Special Permit to reduce one parking space is discussed later in the report. The project will result in the closing of curb cut on 24<sup>th</sup> Street which in turn will add additional room on the street towards parking.

**Height, Bulk and Setbacks**

Following is a summary of the height and area standards:

<b>Table 4: Height and area standards</b>			
<b>Standard</b>	<b>Allowed</b>	<b>Proposed</b>	<b>Deviation?</b>
Height	35'-0" maximum	23'-0" to top of parapet	No
Front Setback	Average of adjacent	9'-0" (same setback as adjacent building)	No
Rear/Accessory Structure Setback	6'-0"	6'-0"	No
Street Side Setback (condo buildings)	5'-0"	5'-0"	No
Interior Side Setback (condo buildings)	5'-0"	5'-0"	No
Street Side Setback (accessory structure)	5'-0"	2'-0"	Yes
Interior Side Setback (accessory structure)	No requirement	0'-6"	No

As indicated above, the project meets or exceeds all applicable height and area requirements except street side setback for the accessory garage structure. Applicant is requesting the Special Permit to reduce that setback and the entitlement is discussed later in the report.

**Building design, signage and landscaping: The design component of the project is subject to approval by the Design Commission.**

The site is divided into three structures as detailed below:

<b>Table 5: Building Summary</b>			
<b>Building Name:</b>	<b>Description:</b>	<b>Height:</b>	<b>Area (square feet):</b>
South Building	3 (Units 1 to 3)	Two-story (23'0" to top of parapet)	2,843 (not including patio)
North Building	3 (Units 4 to 6)	Two-story (23'0" to top of parapet)	3,280 (not including patio)
Garages	5 parking spaces	One-story (12'-0" to top of building)	1,028

The south building has street frontages on T Street and 24<sup>th</sup> Street. The north building has frontage on 24<sup>th</sup> Street. The garages fronts on the alley to the north. A residential courtyard is shown between the south building and the north building; the courtyard contains a common garden area and common storage for the residents; the courtyard is enclosed by a solid wall facing 24<sup>th</sup> Street to provide privacy. The building exterior is finished with cement plaster and brick veneer at unit entries. Windows are aluminum clad, and a number of windows have eyebrow overhang. Roofing is single-ply membrane roofing. Raised brick planters are shown at the entries. Lighting fixtures shall be unobtrusive and complementary to the architectural design of the building.

Following is a breakdown of the six condominium units:

<b>Unit Number:</b>	<b>Number of Levels:</b>	<b>Living Area (square feet):</b>	<b>Patio Area (square feet):</b>	<b>Number of Bedroom/Baths:</b>
1	Two Levels	1,062	88	2 / 2-1/2
2	One Level	900	90	2 / 1
3	One Level	881	97	2 / 1
4	Two Levels	1,076	100	2 / 2-1/2
5	Two Levels	1,102	110	2 / 2-1/2
6	Two Levels	1,102	110	2 / 2-1/2

The residential buildings are consistent with the Multi-Family Residential Design Guidelines, in that: the buildings are arranged to provide functional public and private outdoor spaces; pedestrian orientation is encouraged in the allocation of space, building size and placement, and open space design; the building provides windows and active spaces to provide for additional security and visual interest; the buildings vary roof form, mass, shape and material changes to create variations in plans; and material textures and colors are used to help articulate the building designs.

No signage has been proposed at this project. The applicant has submitted a revised landscape plan to address comments by Urban Forest Services and is attached to this report.

***Special Permit for Parking Reduction***

The applicant is requesting to provide a total of five parking spaces for six condominium units, which is one less than the required parking ratio of one space per dwelling unit per Chapter 17.192 of the Zoning Code. The approval of a special permit is required to reduce parking for a multi-family residential development per Chapter 17.64 of the Zoning Code. In evaluating special permit proposals of this type, the Commission is required to make the following findings:

- A. A special permit shall be granted upon sound principles of land use.  
The parking reduction is supported since the site is conducive to alternative modes such as public transit, bicycling and walking.

- B. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.

The proposed off-street parking satisfies the requirement for five out of the six units and ample on-street parking is provided along the building frontages. Also, the elimination of an existing curb cut provides additional room for parking; therefore, the reduction of one required parking will not be detrimental to public health, safety or welfare and will not result in the creation of a nuisance.

- C. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.

The proposed project is consistent with the residential land use policies and density requirements of the General Plan and the Central City Community Plan.

Staff supports the parking reduction since the site is easily accessible to public transportation and is within walking distance to nearby commercial uses. The parking reduction is supported by Regional Transit, noting that reduced parking and the maximum number of units allowed per the zoning code is supportive of the nearby light rail facility. Also, the elimination of an existing curb cut on 24<sup>th</sup> Street provides additional room for parking. In conclusion, staff supports the requested parking reduction and finds the proposed parking to provide adequate parking for both residents and guests.

### ***Special Permit for Reduced Setback at Accessory Structure***

The applicant is proposing two-foot street side setback at the garage structure instead of the required five feet for an accessory structure. Therefore, the applicant is requesting a special permit to reduce required street side setback at the accessory garage structure. In evaluating special permit proposals of this type, the Commission is required to make the following findings:

- A. A special permit shall be granted upon sound principles of land use.

The proposed setback reduction is necessary to provide up to five covered off-street parking spaces.

- B. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.

The setback reduction is not detrimental to the public health, safety or welfare, nor does it result in the creation of a nuisance since the accessory structure does not encroach into public right-of-way. The project provides on-site parking at a single-story structure that helps activate the alley.

- C. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.

The proposed setback reduction at the accessory structure does not contradict the residential land use policies and density requirements of the General Plan and Central City Community Plan.

Staff supports the setback reduction at the accessory structure since it will provide an addition covered parking for the site and it will not be a nuisance to the public.

### **Variance**

Per Chapter 17.72 of the Zoning Code, a trash enclosure is required for multi-family developments with five or more units. The applicant is requesting the Variance to waive the requirement for a trash enclosure for the proposed six-unit condominiums. The applicant cited that there is no space for a trash enclosure adjacent to the alley or street because the five garages on the alley are needed for parking and because the existing curb cut on 24th is being eliminated to increase parking on 24th Street and to make the project more pedestrian friendly. In lieu of the trash enclosure, the occupants will be provided recycling and trash cans for their use; the site plan shows an area adjacent to both the North and South buildings for storing the cans. The City's Solid Waste Division has reviewed the proposal and has no objections to the use of recycling and trash cans subject to conditions. The following findings must be made in order to grant a variance:

1. The project will not be detrimental to public health, safety, or welfare nor result in a nuisance.

Granting the variance will not be injurious to public welfare, nor to property in the vicinity of the applicant in that alternate trash and recycling containment will be employed for the project and the individual cans have a specified location on site for storage.

2. The project will provide adequate capacity, number, and distribution of recycling and trash enclosures and receptacles to serve the new or existing development.

The project is conditioned to provide adequate capacity, number, and distribution as required by the recycling and solid waste disposal regulations (Chapter 17.72 of the Zoning Code).

Staff supports the variance to waive the trash enclosure as described above since the occupants will be using individual cans to collect trash and recycling and there are locations on the site to store the cans. Solid Waste Division has reviewed the proposal and has no objections to the proposal as long as attached conditions are met. Due to the above findings, staff has no objections to the variance.

**Conclusion:** Staff recommends the Commission approved the requested entitlement with conditions of approval shown on Attachment 1. Staff finds that the proposed plans comply with all applicable General Plan and Zoning Ordinance requirements, and that they meet the intent of the Multi-Family Residential Design Principles.

Respectfully submitted by: David Hung  
DAVID HUNG  
Associate Planner

Approved by: Sandra L. Yope  
SANDRA YOPE  
Senior Planner

Recommendation Approved:

Gregory Bitter  
GREGORY BITTER  
Principal Planner

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**Attachment 1**  
**Proposed Findings of Fact and Conditions of Approval**  
**Courtyard Condominiums (P10-089)**  
**Northwest Corner of T Street and 24<sup>th</sup> Street**

**Findings of Fact**

**A. Environmental Determination: Exemption-Infill**

Based on the determination and recommendation of the City's Environmental Planning Services Manager and the oral and documentary evidence received at the hearing on the Project, the Planning Commission finds that the Project is exempt from review under Section 15332 (Infill) of the California Environmental Quality Act Guidelines as follows:

- a. The project complies with all applicable policies of the General Plan, as well as with the applicable zoning regulations;
- b. The proposed development occurs within City limits on a project site of no more than five (5) acres substantially surrounded by urban uses;
- c. The project site has no value as habitat for endangered, rare or threatened species;
- d. Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality; and
- e. The site can be adequately served by all required utilities and public services.

**B. The Tentative Map** to create one condominium lot on approximately 0.19 acres in the Multi-Family (R-3A) zone is approved subject to the following Findings of Fact and Conditions of Approval:

1. None of the conditions described in Government Code Section 66474, subsection (a) through (g), inclusive, exist with respect to the proposed subdivision as follows:
  - a. The proposed subdivision, together with the provisions for its design and improvement, is consistent with the City's General Plan, all applicable community and specific plans, and Title 16 of the City Code, which is a specific plan of the City;
  - b. The site is physically suitable for the type of development proposed and suited for the proposed density;

- c. The design of the subdivision and the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife their habitat;
  - d. The design of the subdivision and the type of improvements are not likely to cause serious public health problems;
  - e. The design of the subdivision and the type of improvements will not conflict with easements, acquired by the public at large, for access through or use, of, property within the proposed subdivision.
2. The proposed subdivision, together with the provisions for its design and improvement, is consistent with the City General Plan and Title 16 Subdivisions of the City Code, which is a specific plan of the City (Gov. Code §66473.5);
  3. The discharge of waste from the proposed subdivision into the existing community sewer system will not result in a violation of the applicable waste discharge requirements prescribed by the California Regional Water Quality Board, Central Valley Region, in that existing treatment plants have a design capacity adequate to service the proposed subdivision (Gov. code §66474.6);
  4. The design of the proposed subdivision provides, to the extent feasible, for future passive or natural heating and cooling opportunities (Gov. Code §66473.1);
  5. The Planning Commission has considered the effect of the approval of this tentative subdivision map on the housing needs of the region and has balanced these needs against the public service needs of its residents and available fiscal and environmental resources (Gov. Code §66412.3).
- C. The Special Permit** to develop six condominium units in the Multi-Family (R-3A) zone is approved subject to the following Findings of Fact and Conditions of Approval:
1. A special permit shall be granted upon sound principles of land use.  
The project, as conditioned, is based upon sound principles of land use in that:
    - a. The proposed multi-family use is located within close proximity to transit, commercial developments and parks; and
    - b. The proposed multi-family use is well serviced by auto, transit, bicycle, and pedestrian linkages.
  2. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.

The project, as conditioned, will not be detrimental to the public health, safety, and welfare, and will not result in the creation of a public nuisance, in that the site design and the building design are consistent with the Multi-Family Residential Design Principles and all building code requirements.

3. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.

The project is consistent with policies of the General Plan and Central City Community Plan regarding density and housing.

**D. The Special Permit to reduce one required parking for a multi-family development is approved subject to the following Findings of Fact and Conditions of Approval:**

1. A special permit shall be granted upon sound principles of land use.  
The parking reduction is supported since the site is conducive to alternative modes such as public transit, bicycling and walking.
2. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.  
The proposed off-street parking satisfies the requirement for five out of the six units and ample on-street parking is provided along the building frontages. Also, the elimination of an existing curb cut provides additional room for parking; therefore, the reduction of one required parking will not be detrimental to public health, safety or welfare and will not result in the creation of a nuisance.
3. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.  
The proposed project is consistent with the residential land use policies and density requirements of the General Plan and the Central City Community Plan.

**E. The Special Permit to reduce required street side setback from five feet to two feet for an accessory structure is approved subject to the following Findings of Fact and Conditions of Approval:**

1. A special permit shall be granted upon sound principles of land use.  
The proposed setback reduction is necessary to provide up to five covered off-street parking.
2. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.  
The setback reduction is not detrimental to the public health, safety or welfare, nor does it result in the creation of a nuisance since the

accessory structure does not encroach into public right-of-way. The project provides on-site parking at a single-story structure that helps activate the alley.

3. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.

The proposed setback reduction at the accessory structure does not contradict the residential land use policies and density requirements of the General Plan and Central City Community Plan.

- F.** The Variance to waive a trash enclosure for a multi-family development is approved subject to the following Findings of Fact and Conditions of Approval:

1. The project will not be detrimental to public health, safety, or welfare nor result in a nuisance.

Granting the variance will not be injurious to public welfare, nor to property in the vicinity of the applicant in that alternate trash and recycling containment will be employed for the project and the individual cans have a specified location on site for storage.

2. The project will provide adequate capacity, number, and distribution of recycling and trash enclosures and receptacles to serve the new or existing development.

The project is conditioned to provide adequate capacity, number, and distribution as required by the recycling and solid waste disposal regulations (Chapter 17.72 of the Zoning Code).

### **Conditions of Approval**

- B.** The **Tentative Map** to create one condominium lot on approximately 0.19 acres is hereby approved subject to the following conditions:

**NOTE: These conditions shall supersede any contradictory information shown on the Tentative Map approved for this project (P10-089). The design of any improvement not covered by these conditions shall be to City standard.**

### **GENERAL: All Projects**

- B1.** Pay off existing assessments, or file the necessary segregation requests and fees to segregate existing assessments.

B2. Show all continuing and proposed/required easements on the Parcel Map.

**Department of Transportation: (Anis Ghobril, Department of Transportation, 808-5367)**

- B3. Construct standard subdivision improvements as noted in these conditions pursuant to section 16.48.110 of the City Code. All improvements shall be designed and constructed to the satisfaction of the Department of Transportation. Improvements required shall be determined by the City. The City shall determine improvements required for each phase prior to recordation of each phase. Any public improvement not specifically noted in these conditions or on the Tentative Map shall be designed and constructed to City standards. This shall include street lighting and the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk fronting the property along "24<sup>th</sup>" Street and "T" Street per City standards and to the satisfaction of the Department of Transportation.
- B4. The design and placement of walls, fences, signs and Landscaping near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height. The area of exclusion shall be determined by the Department of Transportation.
- B5. The applicant shall repair/reconstruct any deteriorated portions of the existing alley per City standards (In Concrete) and to the satisfaction of the Department of Transportation. The limit of repair of the alley shall be along the parcel's frontage to 24<sup>th</sup> Street.
- B6. As Part of the Public improvements, the applicant shall remove any existing driveways along the site's frontage and reclaim the existing planter and reconstruct the curb to the satisfaction of the Department of Transportation.
- B7. The applicant shall be responsible for the reconstruction/replacement of any curb ramp that does not meet current A.D.A. standards at the north-west corner of the intersection of T and 24<sup>th</sup> Streets to the satisfaction of the Department of Transportation.

**PUBLIC/PRIVATE UTILITIES (Yujean Kim, SMUD, 732-5027)**

- B8. Dedicate the north 5-feet adjacent to the alley as a public utility easement for overhead and underground facilities and appurtenances. (SMUD)

**CITY UTILITIES (Neal Joyce, Department of Utilities, 808-1912)**

- B9. Any new domestic water services shall be metered. Only one domestic water service is allowed per parcel. Excess services shall be abandoned to the satisfaction of the DOU. All water connections shall comply with the City of Sacramento's Cross Connection Control Policy.
- B10. Building pad elevations shall be approved by the DOU and shall be a minimum of 1.5 feet above the local controlling overland release elevation or a minimum of 1.2 feet above the highest adjoining back of sidewalk elevation, whichever is higher, unless otherwise approved by the Department of Utilities.
- B11. The applicant must comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinance. This ordinance requires the applicant to show erosion and sediment control methods on the subdivision improvement plans. These plans shall also show the methods to control urban runoff pollution from the project site during construction.
- B12. A grading plan showing existing and proposed elevations is required. Adjacent off-site topography shall also be shown to the extent necessary to determine impacts to existing surface drainage paths. No grading shall occur until the grading plan has been reviewed and approved by the DOU.
- B13. This project is served by the Combined Sewer System (CSS). Therefore, the developer/property owner will be required to pay the Combined System Development Fee prior to the issuance of a building permit. The impact to the CSS is estimated to be 6 ESD's. The Combined Sewer System fee is estimated to be \$453.08 plus any increases to the fee due to inflation.
- B14. The onsite water, sewer and storm drain systems shall be private systems maintained by the association. Prior to the initiation of any water, sanitary sewer or storm drainage services to the condominium project, an ownership association shall be formed and C.C. & R.s shall be approved by the City and recorded assuring maintenance of sanitary sewer, water and storm drainage facilities within the condominium project. The C.C.&R.s shall authorize the association to contract on behalf of all owners within the condominium project for sanitary sewer, water and storm drainage services for the condominium units, common area(s) and all other areas within the condominium project.

**FIRE (King Tunson, Fire Department, 808-1358)**

- B15. Maintenance agreements shall be provided for the interior roadways of the proposed complex and for the fire protection systems. The agreement shall be record with the Public Records Office having jurisdiction and shall provide for the following:
- 1 Provisions for the necessary repair and maintenance of vehicle and pedestrian access gates and opening systems.
  - 2 Unrestricted use of and access to the gates and opening systems covered

- by the agreements.
- 3 Maintenance and timely repair of all fire protection systems, including but not limited to fire alarm systems and fire sprinklers.
- B16. Provide the required fire hydrants in accordance with CFC 507 and Appendix C, Section C105.

**PPDS:** Parks (Raymond Costantino, Parks Department, 808-8826)

- B17. **Payment of In-lieu Park Fee:** Pursuant to Sacramento City Code Chapter 16.64 (Parkland Dedication) the applicant shall pay to City an in-lieu park fee in the amount determined under SCC §§16.64.040 and 16.64.050 equal to the value of land prescribed for dedication under 16.64.030 and not satisfied by dedication. (See Advisory Note).
- B18. **Maintenance District:** The applicant shall initiate and complete the formation of a parks maintenance district (assessment or Mello-Roos special tax district), or annex the project into an existing parks maintenance district. The applicant shall pay all city fees for formation of or annexation to a parks maintenance district. (Contact Public Improvement Financing, Special Districts Project Manager. In assessment districts, the cost of neighborhood park maintenance is equitably spread on the basis of special benefit. In special tax districts, the cost of neighborhood park maintenance is spread based upon the hearing report, which specifies the tax rate and method of apportionment.).

## **MISCELLANEOUS**

- B19. Form a Homeowner's Association. CC&R's shall be approved by the City and recorded assuring maintenance of private drives/parking areas. The Homeowner's Association shall maintain all private drives/parking areas, lights, common landscaping and common areas. (DOT)

## **ADVISORY NOTES:**

The following advisory notes are informational in nature and are not a requirement of this Tentative Map:

- B20. If unusual amounts of bone, stone, or artifacts are uncovered, work within 50 meters of the area will cease immediately and a qualified archaeologist shall be consulted to develop, if necessary, further mitigation measures to reduce any archaeological impact to a less than significant effect before construction resumes. A note shall be placed on the final improvement plans referencing this condition. (DOT)
- B21. Prior to the initiation of any water, sanitary sewer or storm drainage services to the condominium project, the owner(s) and ownership association shall enter into a utility service agreement with the City to receive such utility services at points

of service designated by the DOU. Such agreement shall provide, among other requirements, for payment of all charges for the condominium project's water, sanitary sewer and storm drainage services, shall authorize discontinuance of utility services at the City's point(s) of service in the event that all or any portion of such charges are not paid when and as required, shall require compliance with all relevant utility billing and maintenance requirements of the City, shall require sub-metering of water service to the condominium units if requested by the DOU or required by any other government agency, and shall be in a form approved by the City Attorney. (DOU)

- B22. Many projects within the City of Sacramento require on-site booster pumps for the fire suppression and domestic water system. Prior to design of the subject project, the Department of Utilities suggests that the applicant request a water supply test to determine what pressure and flows the surrounding public water distribution system can provide to the site. This information can then be used to assist the engineers in the design of the fire suppression systems. (DOU)
- B23. The proposed project is located in the Flood zone designated as **Shaded X** zone on the Federal Emergency Management Agency (FEMA) Federal Insurance Rate Maps (FIRMs) that have been revised by a Letter of Map Revision effective February 18, 2005. Within the Shaded X zone, there are no requirements to elevate or flood proof. (DOU)
- B24. The only public water main fronting this property is in S/T Alley. All water taps will need to come off the alley unless a public main extension is done in 24<sup>th</sup> St. to the satisfaction of the DOU. Per the City's cross connection control policy, all fire and irrigation service taps will require backflow prevention devices to be installed within five feet of the point of service, which in this case is the edge of alley. These devices are above ground and may cause a conflict with the proposed garage locations. This should be taken into consideration during design. (DOU)
- B25. Developing this property will require the payment of SRCSD sewer impact fees. Impact fees shall be paid prior to issuance of Building Permits. Applicant should contact the Fee Quote Desk at 876-6100 for sewer impact fee information. (SASD)
- B26. As per City Code, the applicant will be responsible to meet his/her obligations regarding: (PARKS)
- 1 Title 16, 16.64 Park Dedication / In Lieu (Quimby) Fees, due prior to recordation of the final map. The Quimby fee due for this project is estimated at \$26,820. This is based on 6 single family units at an average land value of \$250,000 per acre for the Central City Planning Area, plus an additional 20% for off-site park infrastructure improvements, less acres in land dedication. Any change in these factors will change the amount of the Quimby fee due. The final fee is calculated using factors at the time of

payment.

- 2 Title 18, 18.44 Park Development Impact Fee, due at the time of issuance of building permit. The Park Development Impact Fee due for this project is estimated at \$14,478. This is based on 6 single family units at the Specified Infill Rate of \$2,413 per unit. Any change in these factors will change the amount of the PIF due. The fee is calculated using factors at the time that the project is submitted for building permit.
- 3 Community Facilities District 2002-02, Neighborhood Park Maintenance CFD Annexation.

C. The **Special Permit** to develop six condominiums in the Multi-Family (R-3A) zone is hereby approved subject to the following conditions:

#### Planning

- C1. This approval is for the construction of six condominium units per attached exhibits. Any change in the design, materials, or colors from this approval shall be submitted to the Planning Division for review and determination for further actions.
- C2. The project shall require approval by the Design Commission.
- C3. **The condominium tentative map must be finalized and recorded prior to the issuance of building permits for the structures.**
- C4. The applicant shall obtain all necessary building permits prior to commencement of construction; any modification to the project shall be subject to review and approval by Planning staff (and may require additional entitlements) prior to the issuance of building permits.
- C5. Provide an ownership association responsible for the care and maintenance of all common areas and common improvements and any other interest common to the condominium owners. Complete and true copies of all covenants, conditions and restrictions, articles of incorporation and by-laws shall be subject to review and approval by the city prior to occupancy as a condominium unit.
- C6. The homeowner's association shall conduct periodic inspections, not less than monthly, of the exterior of all buildings, trash enclosures and recreation facilities.
- C7. The homeowner's association shall establish and conduct a regular program of routine maintenance for the property. Such a program shall include common areas and scheduled repainting, replanting and other similar activities that typically require attention at periodic intervals but not necessarily continuous. Owner/Operator shall repaint or retreat all painted or treated areas at least once every 8 years; provided that the Planning Director may approve less frequent

- painting or re-treatment upon a determination that less frequent repainting or re-treatment is appropriate, given the nature of the materials used or other factors. The program shall be subject to review and approval by the Planning Director.
- C8. The homeowner's association shall maintain landscaping and irrigation in a healthy and serviceable condition.
- C9. The homeowner's association shall indicate and maintain all locations of parking stalls for handicapped/disabled access and strictly enforce rules related thereto.
- C10. Each condominium unit shall comply with the state of California's Noise Insulation Standards (California Amended Code Section 1092).
- C11. Each condominium unit shall provide the following:
- a. A separate sewer service hookup; provided, that the planning commission may permit the use of common sewer lines that are oversized by one size or more, or which are hydraulically designed with the concurrence of the city engineer, finds the common sewer lines can adequately service the condominiums and that separate service hookups would not be feasible. For this provision, the Planning Commission is delegating the approval to the Planning Director subject to concurrence with the Public Works Director and the Utilities Department.
  - b. A separate water service hookup or shutoff; provided, that the planning commission may permit a single water system to service more than one condominium unit where shutoffs are provided wherever practicable and where the planning commission, with the concurrence of the city engineer, finds that the single water system can adequately service the condominiums and separate service hookups or shutoffs are not feasible. For this provision, the Planning Commission is delegating the approval to the Planning Director subject to concurrence with the Public Works Director and the Utilities Department.
  - c. A separate gas service where gas is a necessary utility.
  - d. A separate electrical service, with separate meters and disconnects and ground fault interrupters where and as required by Building Code.
- C12. Each unit of a condominium project and all commonly owned portions of a Condominium building shall comply with all applicable building code standards. Nothing herein shall be construed to prevent or prohibit the applicant or the city from providing or requiring building standards greater than those set forth in the Building Code where the greater standards are found to be necessary to carry out the purposes and objectives of this chapter. (Ord. 99-015 § 6-3-D)
- C13. Landscaping & Walls and Fencing:

- a. All landscaping and planting shall conform to City standards for sight line requirements at intersections and driveways.
- b. All mechanical equipment shall be located within enclosed cabinets or screened by landscaping and/or screening/fencing.
- c. Walls and fences shall conform to City standards for sight line requirements at intersections and driveways.

C14. Signage:

- a. One detached monument sign is permitted and may be located within the site but no closer than 10 feet from public right-of-way.
- b. Attached signage shall consist of address numbers only.
- c. All signage shall comply with the Sign Ordinance, City Code Section 15.148; a sign permit shall be obtained prior to construction of any sign.

C15. Lighting:

- a. The type and location of the outdoor lighting (building, parking lot, walkway, etc.) must be approved by the Planning Director prior to issuance of a building permit. Lighting shall be provided in garage areas and each building address number shall be illuminated.
- b. Project lighting shall be provided as follows: one foot-candle of minimum maintained illumination per square foot of parking space and exterior walkways/sidewalks during hours of darkness and 0.25 foot-candle of minimum maintained illumination per square foot of surface on any interior walkway, alcove, passageway, etc., from one-half hour before dusk to one-half hour after dawn. All light fixtures are to be vandal-resistant.
- c. Per Section 17.68.030(B), exterior lighting, if provided, shall reflect away from residential areas and public streets.
- d. Fixtures shall be unobtrusive and complementary to the architectural design of the building. Lighting shall be designed so as not to produce hazardous and annoying glare to motorists and building occupants, adjacent residents, or the general public.

- C16. The applicant shall paint electrical meters/cabinets, telephone connection boxes and other utility appurtenances to match the building to which they are attached.

- C17. Prior to the issuance of any building permits, the applicant shall provide the City with a copy of the certificate of payment of school fees for the applicable school district(s).
- C18. The project shall reasonably maintain the buildings and landscaping.
- C19. Condominiums addressing shall be to City standards.

#### Department of Transportation

- C20. Construct standard improvements as noted in these conditions pursuant to chapter 18 of the City Code. Improvements shall be designed and constructed to City standards in place at the time that the Building Permit is issued. All improvements shall be designed and constructed to the satisfaction of the Department of Transportation. Any public improvement not specifically noted in these conditions shall be designed and constructed to City Standards. **This shall include street lighting and the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk fronting the property along "T" Street and 24<sup>th</sup> Street per City standards and to the satisfaction of the Department of Transportation.**
- C21. The applicant shall repair/reconstruct any deteriorated portions of the existing alley per City standards (In Concrete) and to the satisfaction of the Department of Transportation. The limit of repair of the alley shall be along the parcel's frontage to 24<sup>th</sup> Street.
- C22. The applicant shall remove any existing driveways along the site's frontage and reclaim the existing planter and reconstruct the curb to the satisfaction of the Department of Transportation.
- C23. The site plan shall conform to A.D.A. requirements in all respects. This shall include the reconstruction/replacement of any curb ramp that does not meet current A.D.A. standards at the north-west corner of the intersection of T and 24<sup>th</sup> Streets to the satisfaction of the Department of Transportation.
- C24. The site plan shall conform to the parking requirements set forth in chapter 17 of City Code (Zoning Ordinance). All proposed parking off the alley shall be off-set by a minimum of 6-feet.
- C25. The design of walls fences and signage near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height at maturity. The area of exclusion shall be determined by the Department of Transportation.

## Fire Department

- C26. Timing and Installation. When fire protection, including fire apparatus access roads and water supplies for fire protection, is required to be installed, such protection shall be installed and made serviceable prior to and during the time of construction. CFC 501.4
- C27. Provide a water flow test. (Make arrangements at the Permit Center walk-in counter: 300 Richards Blvd, Sacramento, CA 95814). CFC 508.4
- C28. Provide appropriate Knox access for site. CFC Section 506
- C29. An automatic fire sprinkler system shall be installed in any portion of a building when the floor area of the building exceeds 3,599 square feet.
- C30. Locate and identify Fire Department Connections (FDCs) on address side of building no further than 50 feet and no closer than 15 feet from a fire hydrant.
- C31. An approved fire control room shall be provided for all buildings protected by an automatic fire extinguishing system. Fire control rooms shall be located within the building at a location approved by the Chief, and shall be provided with a means to access the room directly from the exterior. Durable signage shall be provided on the exterior side of the access door to identify the fire control room. CFC 903.8

## Building Division

- C32. At all locations with walls separating dwelling units, provide a one hour rated fire partition per CBC 420.2. See CBC Section 709.
- C33. At all locations where a floor / ceiling assembly is separating dwelling units, provide a one hour rated horizontal assembly per CBC 420.3. See CBC Section 712.
- C34. Provide exterior wall protection, as needed, per CBC Table 602.
- C35. Provide exterior wall opening protection, as needed, per CBC Table 705.8.
- C36. Provide fire sprinklers throughout all buildings with a group R fire area per CBC 903.2.8.
- C37. This project shall comply with all applicable requirements of the 2010 California Code of Regulations Title 24 parts 2, 3, 4, 5, 6, 9, 11, and 12.

## Urban Forest Services

- C38. Prior to issuance of building permits, submit final landscape plan to Urban Forest Services to demonstrate the allowance of sufficient planter or canopy space for all new trees that will not cause infrastructure conflict and pedestrian safety problems.
- C39. The applicant will need to obtain an Urban Forest Services permit before removing or pruning street trees.
- C40. Installation of 6' high 7' x 10' chain link fence enclosure around each street tree will be required prior to construction activity.
- C41. Existing street trees must receive regular irrigation during all phases of construction per City code (12.56.050).

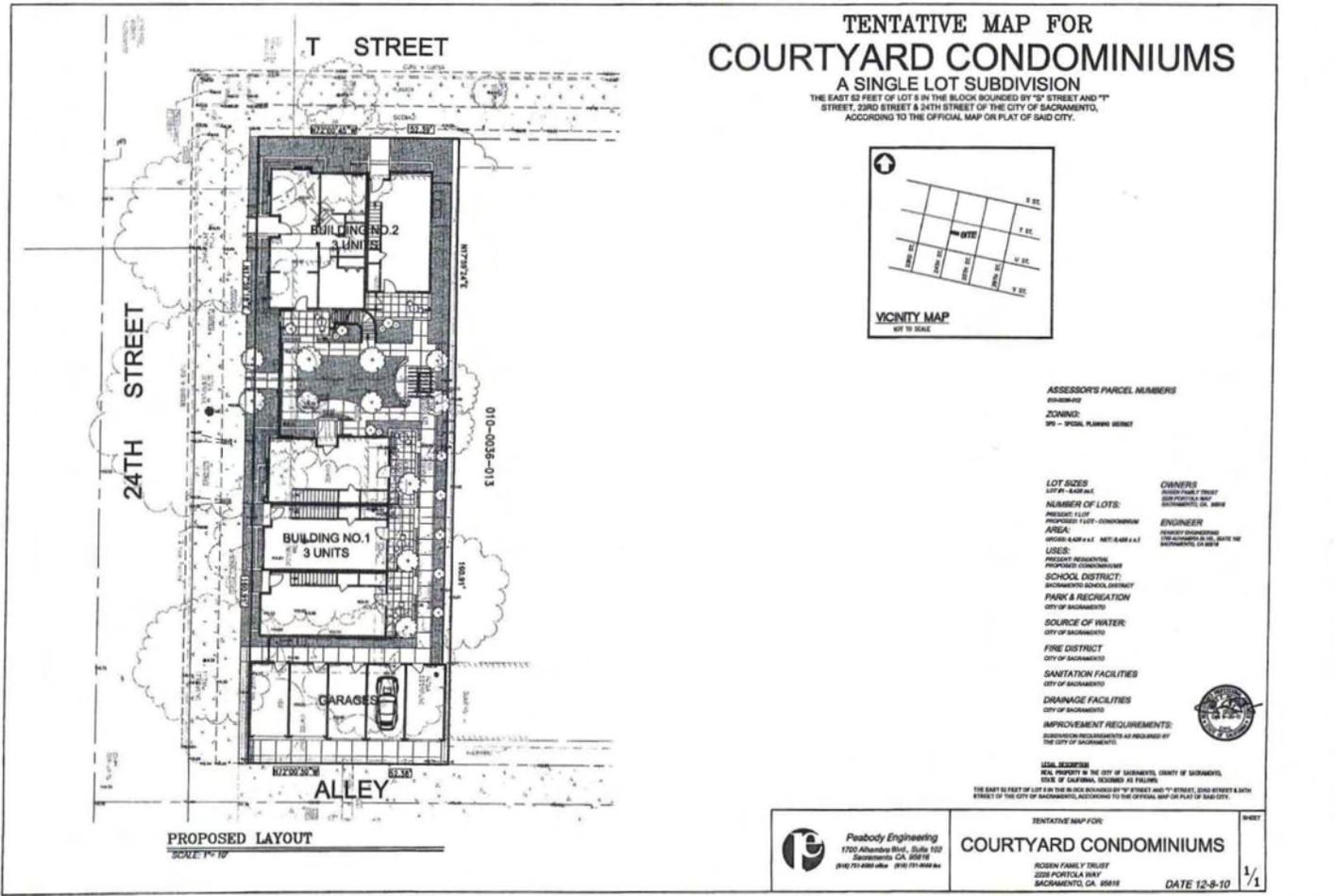
#### Regional Transit

- C42. Transit information shall be displayed in a prominent location in the residential sales/rental office, through a homeowner's association, or with real estate transactions.

#### Advisory Notes

- C43. (Building) Note that walls and floor ceiling assemblies separating dwelling units shall comply with the sound transmission control requirements of 2010 CBC Section 1207.
- D. The **Special Permit** to reduce required parking for a multi-family development is hereby approved subject to the following conditions:
  - D1. The project is required to provide a minimum of five vehicular parking spaces.
- E. The **Special Permit** to reduce required street side setback at an accessory structure is hereby approved subject to the following conditions
  - E1. The garage structure shall provide a minimum of two feet, landscaped, setback adjacent to the right-of-way at 24<sup>th</sup> Street.
- F. The **Variance** to waive a trash enclosure at a multi-family development is hereby approved subject to the following conditions
  - F1. Project must meet the requirements outlined in Sacramento City Code Chapter 17.72.
  - F2. Solid waste trucks must be able to safely move about the properties, with minimum backing, and be able to empty the bins and cans safely.

- F3. Properties must accommodate trucks, as well as cans or bins that are roughly the dimensions outlined in the attached file, "Truck, Bin, Can Dimensions".
- F4. A pair of trash and recycling cans are required for each condominium, and are to be placed as shown on the file "P10-089 trash can placement.pdf", on non-collection days.
- F5. North Building trash and recycling cans are to be set out for collection at the curb on 24th Street. South Building trash and recycling cans are to be set out for collection on T Street.



**P10-089  
DEC 17, 2010**

Exhibit 1B – Cover Sheet



24th & T Courtyard Condominiums

	First Floor (SF)	Second Floor (SF)	Private Patio (SF)	Project Totals (SF)
South Bldg.				
Unit 2	519	543	88	1,050
Unit 2 - ground	900	*	90	900
Unit 3 - Level 2	*	881	97	881
North Bldg.				
Unit 4	553	523	100	1,076
Unit 5	550	552	110	1,102
Unit 6	550	552	110	1,102
Garages (G)	*	*	*	1,028
Totals	3,072	3,051	595	

Total Building:	6,123 sf
Total Patio:	595 sf
Total Garage:	1,028 sf

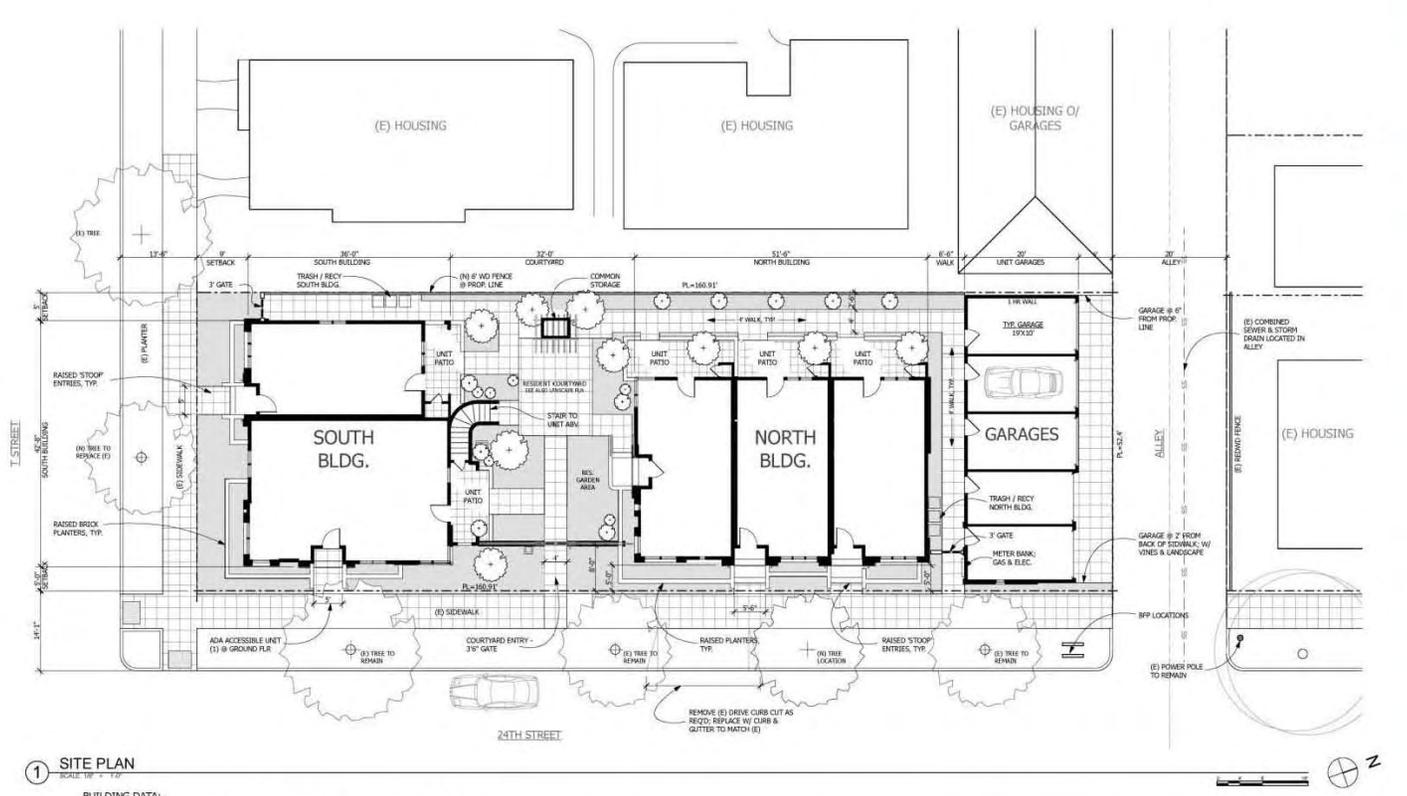
PROJECT DATA



VICINITY MAP

CONSULTING ARCHITECTS  
**WILLIAMS SYRITTA**  
 24th & T - Courtyard Condominiums  
 DATE: 03/10/11  
 SHEET: 02 OF 11

Exhibit 1C – Site Plan



1 SITE PLAN  
SCALE: 1/8" = 1'-0"

BUILDING DATA:

	First Floor (SF)	Second Floor (SF)	Private Patio (SF)	Project Totals (SF)
<b>South Bldg.</b>				
Unit 1	519	543	88	1062
Unit 2 - ground	900	*	90	990
Unit 3 - Level 2	*	881	97	881
<b>North Bldg.</b>				
Unit 4	553	523	100	1076
Unit 5	550	552	110	1102
Unit 6	550	552	110	1102
<b>Garages (5)</b>	*	*	*	1028
<b>Totals</b>	<b>3072</b>	<b>3051</b>	<b>595</b>	

Total Building:	6,123 sf
Total Patio:	595 sf
Total Garage:	1,028 sf

**SITE DATA:**

LOT SIZE:	8430 sf
SOUTH BLDG FOOTPRINT:	1419 sf
NORTH BLDG FOOTPRINT:	1653 sf
GARAGE:	1028 sf

**SETBACKS:**

T ST. FRONT SETBACK:	AVE. OF STREET
24TH STREET SIDEYARD:	5 FT
INTERIOR SIDEYARD:	5 FT
ALLEY SETBACK:	6 FT

**SITE DRAINAGE:**  
ON-SITE: RUNOFF AND BUILDING DRAINAGE TO BE COLLECTED AND ROUTED TO TO (E) UNDERGROUND STORM DRAIN SYSTEM LOCATED IN STREETS & ALLEY.

**LANDSCAPE:** (See attached landscape plan)  
ON SITE: DROUGHT TOLERANT PLANTS & TREES, GRASS IN SMALL LAWN AREAS  
OFF SITE: (E) STREET TREES AND GRASS PLANTER TO REMAIN EXCEPT WHERE NOTED.

PATTERN INDICATES ON SITE & OFF SITE PEDESTRIAN CIRCULATION

24th & T - Courtyard Condominiums  
 2231 T Street, Sacramento  
 DATE: 03/09/10  
 REV: 02/06/11  
 WILAKAS ARCHITECTS

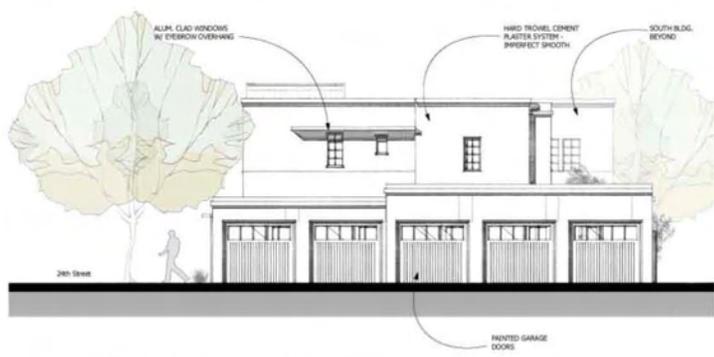
Exhibit 1D – Building Elevations (1)



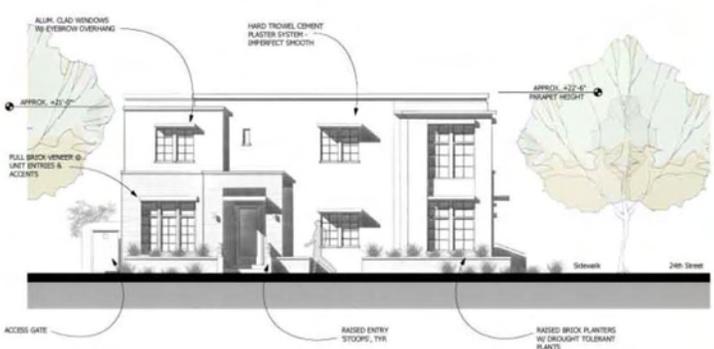
1 East Elevation - 24th Street

PROPOSED FINISH SCHEDULE

FINISH	MATERIAL	WTR / COULR
ROOFING:	SINGLE PLY MEMBRANE ROOFING SYSTEM	COOL ROOF / TPO OR EQ.
SIDING 1:	HARD TROWEL CEMENT PLASTER SYSTEM	WESTERN BLENDED PRODUCTS / 206 STONEWALL
SIDING 2:	FULL BRICK VENEER	PAINTED BRICK TRD
STEEL ACCENTS & TRIM:	RAILINGS, COURTYARD DETAILS, ETC.	CLEAR COAT WEATHERED STEEL
WINDOWS OPTION 1:	ALUMINUM CLAD WOOD WINDOW & DOORS	LINCOLN / COOL WEATHERED COPPER
WINDOWS OPTION 2:	ALUMINUM FRAME WINDOW SYSTEM	MI WINDOWS / ANODIZED FRAMES - BRONZE/ALUM



3 North Elevation - Garages at Alley



2 South Elevation - T Street

WILLIAMS ARCHITECTS  
 24th Street - T & Courtyard Condominiums  
 DATE: 03.10.11  
 REV: 02.00

Exhibit 1E – Building Elevations (2)

**PROPOSED FINISH SCHEDULE**

FINISH	MATERIAL	FIN. / COLOR
ROOFING:	SINGLE PLY MEMBRANE ROOFING	COOL ROOF / TPO OR EQ.
SIDING 1:	HARD TROWEL CEMENT PLASTER SYSTEM	WESTERN BLENDED PRODUCTS / 206 STONEWALL
SIDING 2:	FULL BRICK VENEER	PAINTED BRICK TIBO
STEEL ACCENTS & TRIM:	RAILINGS, COURTYARD DETAILS, ETC.	CLEAR COAT WEATHERED STEEL
WINDOWS OPTION 1:	ALUMINUM CLAD WOOD WINDOW & DOORS	LINCOLN / COOL WEATHERED COPPER
WINDOWS OPTION 2:	ALUMINUM FRAME WINDOW SYSTEM	RE WINDOWS / ANODIZED FRAMES - BRONZE/ALUM

**North Bldg - Courtyard Elevation**  
SCALE: 3/16" = 1'-0"

**South Bldg - Courtyard Elevation**  
SCALE: 3/16" = 1'-0"

**West Elevation**  
SCALE: 3/16" = 1'-0"

**Annotations:**  
 - HARD TROWEL CEMENT PLASTER SYSTEM - IMPERFECT SMOOTH  
 - BALCONY DOOR & METAL RAILING  
 - ALUM. CLAD WINDOWS W/ EYEBROW OVERHANG  
 - RAISED BRICK PLANTERS W/ DROUGHT TOLERANT PLANTS  
 - COMMON COURTYARD W/ LANDSCAPE  
 - PLASTER STAIR W/ METAL RAIL TO UNIT 3 ABOVE  
 - TRASH / RECYC.

WILLIAMS ARCHITECTS  
 24th Street, Suite 100, San Francisco, CA 94114  
 415.774.1111  
 24th & T - Courtyard Condominiums  
 DATE: 03/10/11  
 REV: 03/10/11

Exhibit 1F – Floor Plans (North Building)

24th & T - Courtyard Condominiums  
SOMMERSET, VERMONT 05401  
DATE: 03/08/11  
BY: SMM/ML

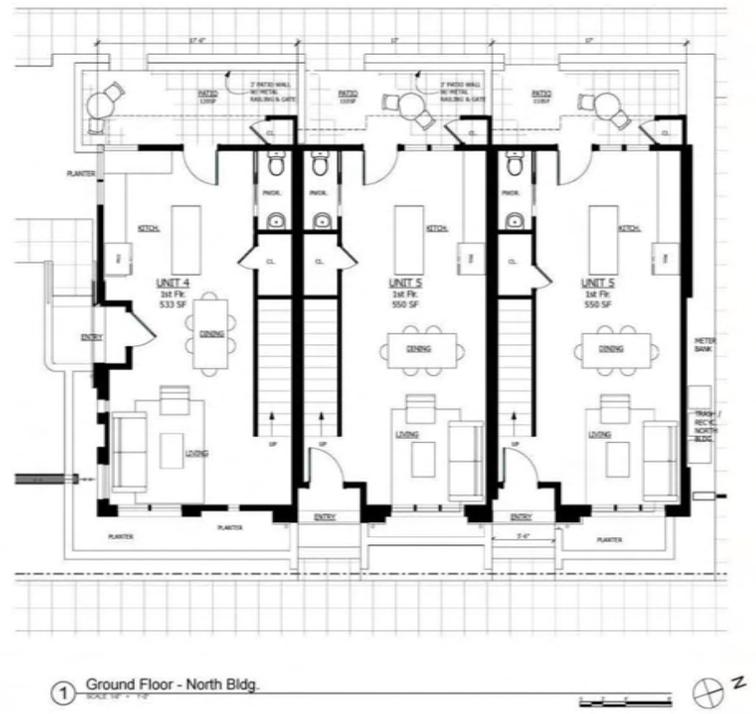
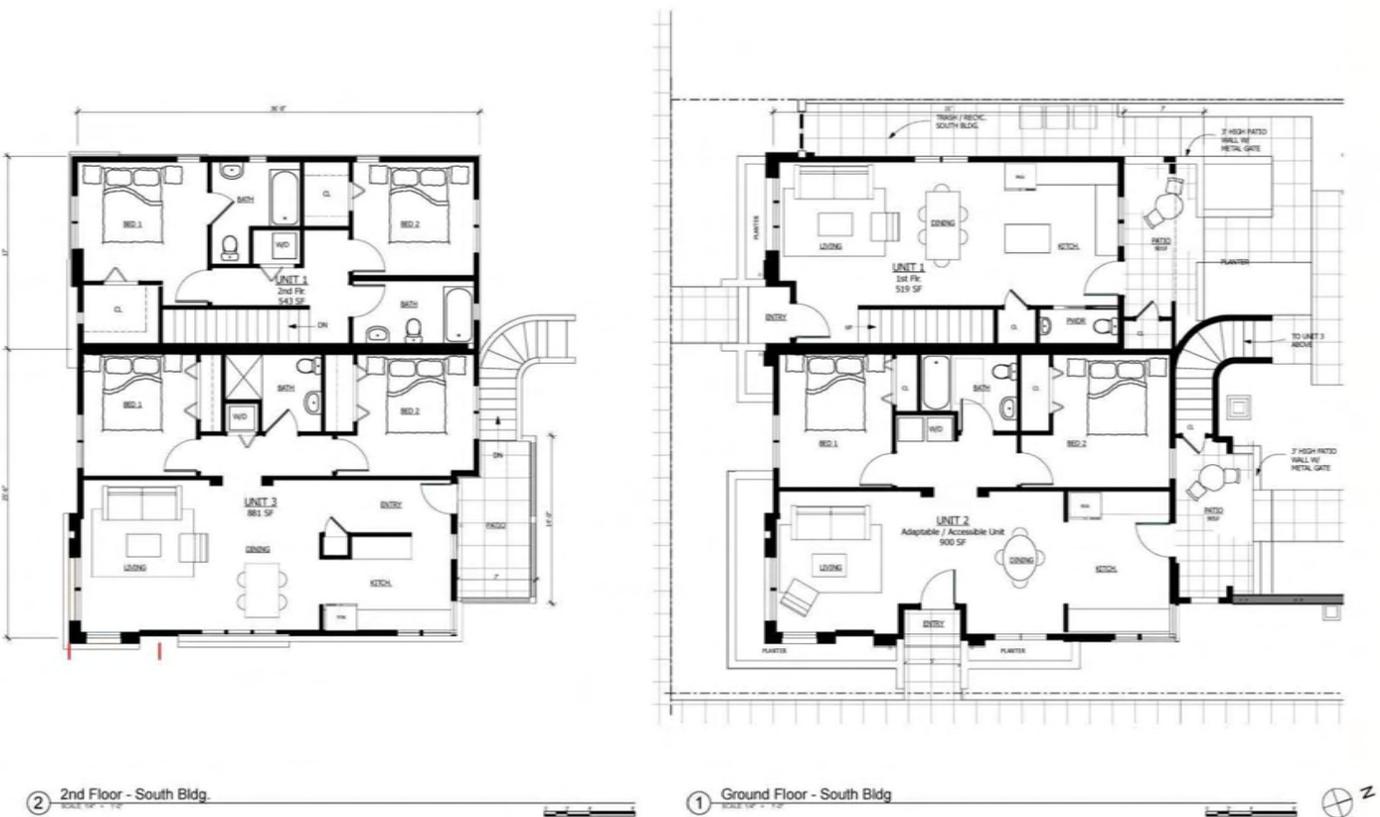


Exhibit 1G – Floor Plans (South Building)



PROJECT: COURTYARD CONDOMINIUMS, 1000 11TH AVENUE, SE, SUITE 100, ATLANTA, GA 30316  
 ARCHITECT: SPARKS  
 COURTYARD CONDOMINIUMS, 1000 11TH AVENUE, SE, SUITE 100, ATLANTA, GA 30316  
 ARCHITECT: SPARKS

Exhibit 1H – Materials Board

24th & I - Courtyard Condominiums  
 24th & I Street, Sacramento, CA 95811  
 DATE: 03/08/10  
 BY: [Signature]  
 03/09/11

Architectural elevation drawing of a building facade. The drawing shows a two-story structure with a mix of materials: brick veneer, plaster, wood, and metal. Callout lines connect specific areas of the drawing to material samples on the right. The samples include:
 

- Full brick veneer - painted, TBD
- Eye-brow awnings - wood face w/ steel support paint - 'Old Redwood'ICI #160
- Plaster - imperfect smooth; Western Blended Products / 'Navajo' (Base B)
- Railing details - iron / dark painted steel
- Windows: aluminum clad; Lincoln 'Cool Weather Copper' or 'Adobe' (shown)
- Raised planters - painted brick

Materials Board

Exhibit 11 – Streetscape Elevations



1 Streetscape Elevation: 24th St  
SCALE: 1/8" = 1'-0"



2 Streetscape Elevation: T St  
SCALE: 1/8" = 1'-0"

COURTYARD CONDOMINIUMS (P10-089) - 24TH & T STS  
 ARCHITECT: SWANSON  
 24th & T - Courtyard Condominiums  
 ARCHITECT: SWANSON  
 DATE: 03/10/11

Exhibit 1J – Photos

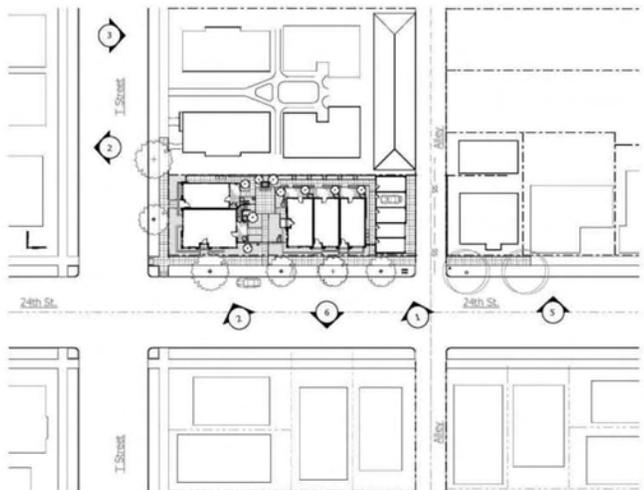
244th & T - Courtyard Condominiums  
 244th & T Street, Sacramento, CA 95811  
 DATE: 03/09/11  
 DRAWN BY: SYMURA  
 PROJECT: 244th & T Street, Sacramento, CA 95811



① Project Site North - View to Alley



② Project Site South from 24th St.



Site Context Plan  
SCALE: 1" = 20'



③ North View of T St. (Adjacent to Site)



④ South View of T St. (Across from Site)



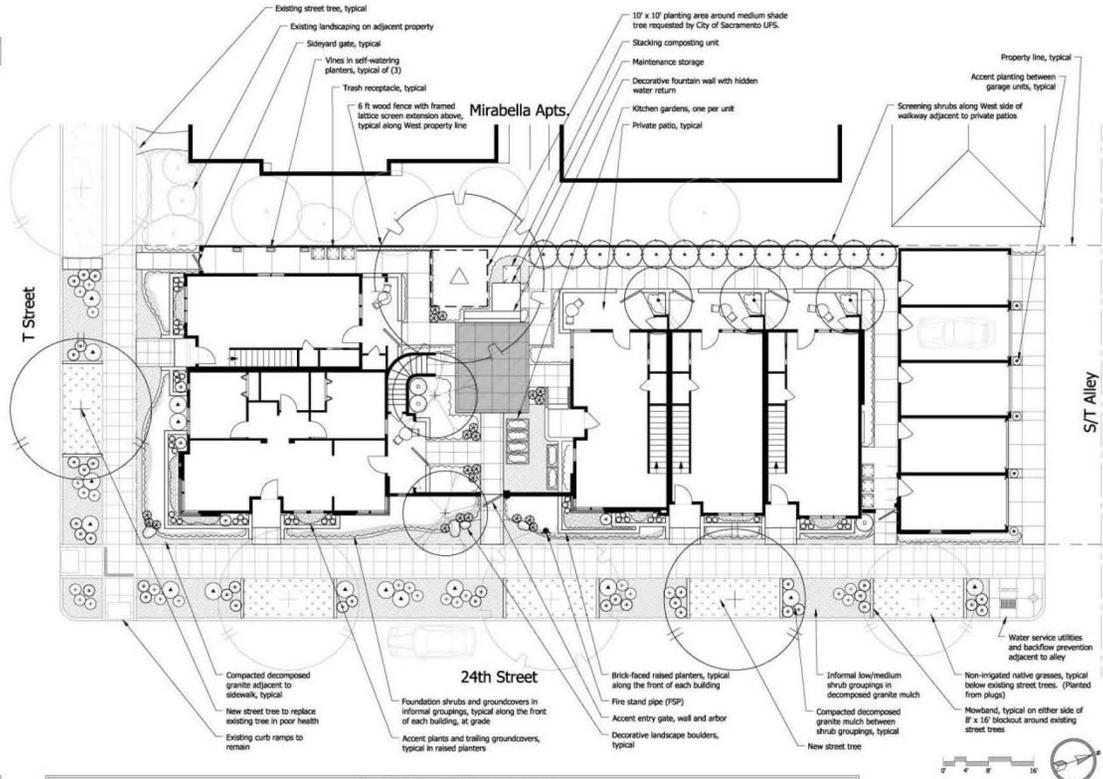
⑤ West View of 24th Street (Adjacent to Site)



⑥ East View of 24th St. (Across from Site)

**Preliminary Landscape Notes**

1. Refer to Architectural Site Plan for building layout, setbacks, and site information.
2. All construction shall comply with the Standard Specifications of the City of Sacramento, unless otherwise noted in the Supplemental Specifications.
3. Existing street trees are to be retained as shown on plan. Refer to Urban Forest Service recommendations for tree protection.
4. **Landscape design concepts:**  
 The landscape within and around the 24/T Courtyard Condominiums has been designed to fit in with and respond to desirable patterns found in the surrounding neighborhood. The design relies on informal groupings of low and medium height shrubs, groundcovers, and decorative landscape features to provide visual interest at the pedestrian scale along the street facing fronts of the buildings.  
 Plant selection will emphasize the use of native and/or climate adapted plants to promote water efficiency and habitat value in addition to creating a pleasing visual effect.  
 Trees and screening shrubs have been carefully considered to provide both aesthetic value and privacy for the homeowners and for residents of the adjacent buildings.  
 The design includes a central courtyard for use by the homeowners, separated from the sidewalk to provide improved privacy without the need for additional screening around the entry gate on 24th Street.
5. **Irrigation design concepts:**  
 Irrigation for planting around each residence and for planting in the parkway strip will be provided by a common system connected to a separate water meter. The irrigation system will have an ET-based automatic irrigation controller, and will primarily utilize drip irrigation to provide water to each plant.  
 The irrigation system will be designed to meet the requirements of the City of Sacramento Water Efficient Landscape Ordinance.
6. **Calculations (on-site):**  
 Planting area: 1550 sqft  
 Kitchen garden area: 125 sqft  
 Decomposed granite mulch: 750 sqft  
 Onsite walks/paving: 1575 sqft  
 Total landscape area: 3500 sqft
7. \* Preliminary plant legend is intended to provide a guideline for plant species proposed. Additional plant selections may be included in the final design, in keeping with the overall design intent.



**Paving Legend**

	Concrete paving, standard gray
	Accent concrete paving at central courtyard
	Existing concrete sidewalk
	Decomposed granite mulch

**Preliminary Planting Legend**

Proposed trees	Size	Water Use	Proposed screening shrubs	Size	Water Use	Proposed shrubs & perennials	Size	Water Use
Quercus shumardii	24" Box	M	Cupressus sempervirens 'Fastigata'	15 gallon	L	Arctostaphylos 'Emerald Carpet'	1 gallon	L
Pistacia chinensis	24" Box	M	Koelerutera paniculata 'Fastigiate'	15 gallon	M	Festuca californica 'San Rafael Blue'	1 gallon	L
Cercis canadensis	15 gallon	M	Prunus caroliniana 'Bright n' Tight'	15 gallon	M	Lavandula spp.	1 gallon	L
Lagerstroemia x.	15 gallon	L	Taxus x media 'Hickall'	15 gallon	L	Muhlenbergia rigens	1 gallon	L
			Proposed vines	Size	Water Use	Hyppocrepis parviflorum 'Prostratum'	1 gallon	L
			Hardenbergia violacea	1 gallon	M	Mynisae arbuscula	1 gallon	L
			Passiflora x alataocerulea	5 gallon	M	Nassella tenuissima	1 gallon	L
			Tricholopemum asiaticum	1 gallon	M	Rosmarinus 'Hill Hardy'	1 gallon	L
						Rosmarinus o. 'Huntington Carpet'	1 gallon	L
						Salvia gregii 'Furthmans Red'	1 gallon	L
						Salvia leucantha	1 gallon	L



Attachment 2 – Support Letters

1. WALKSacramento
2. Sacramento Area Bicycle Advocates (SABA)
3. Sacramento Area Council of Governments (SACOG)
4. Sacramento Metropolitan Air Quality Management District (SMAQMD)
5. Midtown Business Association
6. Regional Transit
7. Friends of Light Rail & Transit
8. Policy in Motion
9. Design Sacramento 4 Health
10. Smart Growth Leadership Recognition Program
11. Karen Jacques
12. Kay Kneprath
13. Michael Monasky
14. William Burg
15. Ruth Ann Bertsch, M.D., Ph.D., F.A.C.P.
16. J. Matthew Gerken, AICP
17. Dr. Jon B. Marshack
18. Dr. Nita Davidson
19. Sarah Underwood



January 18, 2011

David Hung  
City of Sacramento  
City Hall  
Sacramento, CA 95814

RE: 24<sup>th</sup> & T Street Courtyard Condominiums – R-3A Multi-Family Zoning

Dear Mr. Hung:

Our review of the project finds that it is an excellent example of a mini-infill project with features that will add to the walkability of the neighborhood. The location is very walkable with convenient pedestrian access to many nearby destinations as well as light rail. [www.walkscore.com](http://www.walkscore.com) gives this location an 84 or a "very walkable" score. Transit is good as well.

The orientation of the units to the street is very positive as is the elimination of a driveway and the extension of the planter space. Additionally, having housing units facing the street where there has been a vacant lot and vacant house is a positive for the pedestrian environment.

The layout of the buildings provides an internal pedestrian circulation system that supports neighborly interaction along the pathways and in the courtyard. The separation of the garages from the units will add both to the neighbor interaction as well as giving residents pause before they use their cars which could result in less auto usage in this pedestrian and transit friendly environment.

For these reasons, we urge that the City approve the project and allow it to move forward. If you have questions about these comments, please contact me at 916 – 446-9255.

Best regards,

A handwritten signature in cursive script that reads "Anne Geraghty".

Anne Geraghty  
Executive Director  
WALKSacramento  
909 - 12th Street #122  
Sacramento, CA 95814

**David Hung**

---

**From:** Walt Seifert [bikesaba@gmail.com]  
**Sent:** Tuesday, November 02, 2010 2:15 PM  
**To:** David Hung  
**Cc:** andreaosen@sbcglobal.net  
**Subject:** Support for 24th and T Courtyard Housing

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

The Sacramento Area Bicycle Advocates (SABA) supports urban infill projects and compact development. The 24th and T Courtyard Housing project as proposed meets SABA's criteria for the type of project that supports increased transit use and bicycle use in Sacramento.

Projects, such as this one, located near transit and bikeways encourage transit and bicycle use, decrease vehicle traffic, improve traffic safety, provide environmental benefits and improve community quality of life.

SABA is an award-winning nonprofit organization with more than 1400 members. We represent bicyclists. Our aim is more and safer trips by bike. We are working for a future in which bicycling for everyday transportation is common because it is safe, convenient, and desirable. Bicycling is the healthiest, cleanest, cheapest, quietest, most energy efficient, and least congesting form of transportation.

Walt Seifert  
Executive Director  
Sacramento Area Bicycle Advocates (SABA)  
(916) 444-6600  
[saba@sacbike.org](mailto:saba@sacbike.org)  
[www.sacbike.org](http://www.sacbike.org)

"SABA represents bicyclists. Our aim is more and safer trips by bike."



1415 L Street,  
Suite 300  
Sacramento, CA  
95814

tel: 916.321.9000  
fax: 916.321.9551  
tdd: 916.321.9550  
www.sacog.org



December 2, 2010

Mr. David Kwong  
Planning Director  
City of Sacramento Community Development Dept.  
300 Richards Boulevard, 3<sup>rd</sup> Floor  
Sacramento, CA 95811

Mr. Kwong:

This letter is in response to your request for review of the proposed development at 24<sup>th</sup> and T Street in the midtown area of Sacramento. Thank you for the invitation to comment on this project as it relates to the Preferred Blueprint Scenario map and principles.

The proposed project was compared to the Preferred Blueprint Scenario. The Preferred Blueprint Scenario is a conceptual map based on the principles of smart growth. This Preferred Scenario is not intended to direct how a specific parcel should or should not be developed in a particular manner, but rather give some direction on how the region needs to develop generally to reap the benefits of the Preferred Scenario. For this reason, it is not possible to apply them at a parcel level. With that caveat, the proposed site plan is consistent with the Preferred Blueprint Scenario map.

SACOG staff evaluated the proposed project, which is a six-dwelling apartment on 0.19 acres. The land will be the result of two merged parcels at 2331 T Street and 1918 24<sup>th</sup> Street. The project site is located 1.5 blocks from the 24<sup>th</sup> Street light rail station. The project applicant states that this will be built in the tradition of courtyard apartments found throughout midtown Sacramento. The applicant also states that the project will construct two new 2-story buildings each with 3 two-bedroom units. A detached accessory structure including 5 garages will be built with garage doors facing the alley.

**Findings and Evaluation:**

- Infill projects in general offer the opportunity to reduce the amount of automobile traffic and reduce the impacts of greenhouse gas emissions. A commonly used planning measure to determine whether automotive transportation is reduced is vehicle miles traveled (VMT). VMT is the amount of mileage the average residential household and/or employee in an area will travel in one day. SACOG modeling and observed data show that residents of locations like this project – those with housing densities similar to midtown – will generate less than one-half the VMT of residents of typical suburban locations. The residents will also walk, bike, or use transit

Auburn  
Citrus Heights  
Colfax  
Davis  
El Dorado County  
Elk Grove  
Folsom  
Galt  
Isleton  
Lincoln  
Live Oak  
Loomis  
Marysville  
Placer County  
Placerville  
Rancho Cordova  
Rocklin  
Roseville  
Sacramento  
Sacramento County  
Sutter County  
West Sacramento  
Wheatland  
Winters  
Woodland  
Yolo County  
Yuba City  
Yuba County

Mr. David Kwong  
 Page 2  
 December 2, 2010

at two to four times the rate of residents of typical suburban locations. In addition to its location, these travel benefits are in part due to the higher density housing in the proposed plan. A comparison of this project to a more suburban project is shown in the table below:

Area Type	Land Use Characteristics <sup>1</sup>				Travel Characteristics <sup>2</sup>	
	Total Density <sup>3</sup>	Proximity to Transit <sup>4</sup>	Proximity to Frequent Transit <sup>5</sup>	Street Pattern <sup>6</sup>	Non-Auto Mode Share <sup>7</sup>	Weekday VMT per Household <sup>8</sup>
Typical Suburban Residential Area	5	1/4 to 1/2 mi	n/a	20	5 to 10%	40 to 50 miles
Low Density Suburban Residential Area	3	>1.5 mi	n/a	15	<5%	>50 miles
<b>24<sup>th</sup> &amp; T Location</b>	<b>21</b>	<b>&lt;1/4 mi</b>	<b>&lt;1/4 mi</b>	<b>&gt;100</b>	<b>12 to 22%</b>	<b>20 to 25 miles</b>

Source: SACOG, November 2010.  
 Notes:  
 1 Land use characteristics based on SACOG's parcel land use datasets  
 2 Travel characteristics based on household travel survey data  
 3 Total Density = Jobs + Dwellings Per Gross Acre within 1/2 mile of place of residence  
 4 Distance to nearest transit of any frequency  
 5 Distance to transit with peak headway of 15 minutes or less  
 6 Number of "good" intersections (3 or 4 leg) within 1/2 mile of place of residence  
 7 Non-Auto Mode Share = the % of trips made using bike, walk or transit modes by residence at location, on average  
 8 VMT = Vehicle miles traveled

- Infill development is a strategy essential to the success of the Blueprint and our Metropolitan Transportation Plan (MTP). The proposed project clearly supports this principle as it is on a vacant lot located in close proximity to transit in the midtown area. The Blueprint Preferred Scenario, the currently adopted MTP, and the latest scenarios developed for the MTP update achieve transportation, air quality, and other quality of life benefits by relying in part on infill projects, such as this one, to be developed at the densities allowed in local general plans and zoning codes. In some infill areas near major transit, our plans also include targeted increases in allowed densities. The benefits analyzed under these regional plans cannot be achieved if projects in these areas are built to a density that is lower than what the current zoning allows for, such as if the city permitted only a single family home on this site.
- Compact development and a variety of housing options are critical Blueprint planning principles. The proposed project supports both principles by locating housing near existing jobs and services and providing multi-family housing. The midtown/downtown area of Sacramento represents some of the most compact and diversified land uses in the region.

Mr. David Kwong  
Page 3  
December 2, 2010

This project fits the spirit and nature of the existing community while promoting the Blueprint principles.

- Quality of design, another key Blueprint Principle, is a strong determinant of whether or not developments create stronger communities. As with any proposed infill development, it will be essential that the City's design review process carefully examine the design of the proposed structures to ensure the attractiveness of the project and its enhancement of the surrounding area. However, the project's design team, led by Ron Vrlikakis, has much experience in development similar to this in the urban core of Sacramento that have been used as model examples of Blueprint-style development by SACOG.

In conclusion, the proposed development plan, its location in a neighborhood that provides a surrounding mix of uses – retail, residential, office, and its close proximity to transit – make this opportunity an ideal Blueprint implementation project. SACOG has been actively trying to encourage this type of infill development throughout the region. Jurisdictions that can expedite the review process of smaller infill projects that are as clearly consistent as this project with intent of the Blueprint Preferred Alternative and the current zoning will serve to encourage the smart growth that the region is striving for under the Blueprint.

If you have any questions, please feel free to contact myself or SACOG staff members Kacey Lizon (916) 340-6265 or Greg Chew (916) 340-6227.

Sincerely,



Mike McKeever  
Executive Director

S:\SECURED\Mike\24th and T BP Review Ltr. 120110.doc



Larry Greene  
AIR POLLUTION CONTROL OFFICER

January 13, 2011

Councilmember Robert King Fong  
City of Sacramento  
915 I Street, 5th Floor  
Sacramento, CA 95814

Subject: Proposed development of Courtyard Housing at 24<sup>th</sup> and T Street

Dear Councilmember Fong,

The Sacramento Metropolitan Air Quality Management District (District) is writing to express support for the proposed development of 6 courtyard condominiums at 24<sup>th</sup> and T Street. It is the District's position that the project's density, design, and location are consistent with smart growth principals that will reduce the per capita vehicle miles travelled (VMT) and associated emissions of air pollutants. The project is also consistent with goals of the SACOG Regional Blueprint, the City's General Plan designation of Traditional Neighborhood- Medium Density, and the existing zoning for the site. The project presents Sacramento City Council with an opportunity to approve a project that puts into practice essential policies from the General Plan.

Recognizing that 47 percent of our/the region's ozone precursor emissions come from on-road mobile sources<sup>1</sup>, it is essential that new residential units minimize the need for use of personal motor vehicles. The Courtyard Housing at 24<sup>th</sup> and T Street has a compact design, minimum parking, and a transit-supportive density proximate to a light rail station - features that have been linked to a reduction in personal motor vehicle use<sup>2</sup>.

There is a causal relationship between land use decisions and air quality. Consequently, California Health and Safety Code Section 40961 directs the District to "represent the citizens of the Sacramento District in influencing the decisions of other public and private agencies whose actions may have an adverse impact on air quality." The Code also states in Section 41015 that in exercising this duty, the District may not infringe upon the authority of local governments to plan or control land use. The District is always cognizant that it is up to the Sacramento City Council to shape the land use policies for the City, but it is our responsibility to attempt to bring air quality considerations to the forefront. These comments are consistent with the City's goals for Inter-governmental Coordination<sup>3</sup>, as outlined in the Administration and Implementation section of the General Plan.

The District encourages the City Council to consider the project's potential to benefit regional air quality when it is presented for approval.

Sincerely,

A handwritten signature in black ink, appearing to read "Larry Greene".

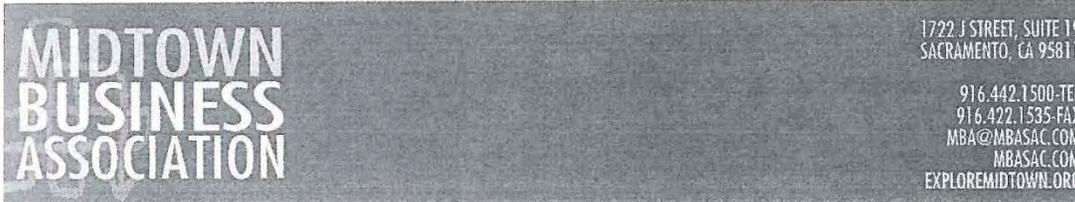
Larry Greene  
Executive Director

<sup>1</sup> Sacramento Region 2005 Ozone-Precursor Emissions Inventory, available online at: <http://www.airquality.org/>

<sup>2</sup> Online TDM encyclopedia, Victoria Transportation Institute; <http://www.vtpi.org/tm/tm81.htm>

<sup>3</sup> City of Sacramento General Plan, Part 4 Administration and Implementation, page 4-8

777 12th Street, 3rd Floor ■ Sacramento, CA 95814-1908  
916/874-4800 ■ 916/874-4899 fax  
[www.airquality.org](http://www.airquality.org)



February 11, 2011

Ms. Andrea Rosen  
Project Proponent  
24<sup>th</sup> & T Courtyard Condominiums  
Delivered via email

Dear Ms. Rosen:

As the Executive Director of the Midtown Business Association, I am writing to offer our support for the project the 24<sup>th</sup> & T Courtyard Condominiums in Midtown.

The MBA Land Use Policy committee reviewed the plans for the project and unanimously agreed that it will make a valuable addition to the Midtown residential community. The committee members were particularly impressed with the design and features of the proposal, including the proposed density. They supported the inclusion of six units in the project.

At the MBA, we believe that the one of the key strengths of Midtown is the neighborhoods. Your project will make a meaningful addition to both the quality and diversity of the housing that Midtown has to offer.

In closing, let me thank you for undertaking this project and offer our support and assistance for its speedy approval and completion.

Sincerely,

Rob Kerth  
Executive Director



**Regional  
Transit**

**Sacramento Regional  
Transit District**  
A Public Transit Agency  
and Equal Opportunity Employer

**Mailing Address:**  
P. O. Box 2110  
Sacramento, CA 95812-2110

**Administrative Office:**  
1400 29th Street  
Sacramento, CA 95816  
(916) 321-2800  
(29th St. Light Rail Station/  
Bus 36,38,50,67,88)

**Light Rail Office:**  
2700 Academy Way  
Sacramento, CA 95815  
(916) 648-8400

Public Transit Since 1973

[www.sacrt.com](http://www.sacrt.com)

Date

Name of project manager  
Title  
Org  
Address

NAME OF DEVELOPMENT: Courtyard Condominiums

CONTROL NUMBER: P10-089

TYPE OF DOCUMENT: Tentative Map, Special Permit

The Courtyard Condominiums project proposes six condominium units on .19 acres in an R-3A zone with reduced parking. The site is located on the northwest corner of T and 24<sup>th</sup> streets in the Central City.

The 23<sup>rd</sup> Street light rail station is within ¼ mile of the project site. Sacramento Regional Transit (RT) supports the overall project as a high-density residential project within the Central Business District that supports existing transit facilities. RT also supports reduced parking for the project, as that will encourage transit use. The project is also providing improved pedestrian connections to transit facilities.

RT staff has reviewed the proposed project and recommends the following:

**Conditions:**

- Transit information shall be displayed in a prominent location in the residential sales/rental office, through a homeowner's association, or with real estate transactions. Please use the attached Request Form (also available on [www.sacrt.com](http://www.sacrt.com)) to order transit information materials.

Thank you for the opportunity to comment. Please send any subsequent documents and hearing notices that pertain to this project as they become available. If you have further questions regarding these recommendations, please contact me at (916) 556-0513 or [tcanfield@sacrt.com](mailto:tcanfield@sacrt.com).

Sincerely,

Traci Canfield  
Planner

c: RoseMary Covington, AGM Planning and Transit System Development, RT  
Andrea Rosen, Rosen Development

Attachment

I



Friends of Light Rail & Transit  
1818 L Street, Suite 615  
Sacramento, CA 95811  
916.447.1960

February 18, 2011

David Hung  
c/o City of Sacramento Planning Department  
300 Richards Blvd  
Sacramento, CA 95814

Dear Mr. Hung:

On behalf of the Board of Directors of Friends of Light Rail & Transit (FLRT) we would like to express our support for the 24<sup>th</sup> and T Streets Courtyard Condominiums project proposed by Rosen Development, LLC.

The FLRT Executive Committee received a project presentation from Andrea Rosen at our February 17, 2011 meeting and unanimously voted to support the project. It was clear to the committee that the major benefits of this infill project were the close proximity to the 24<sup>th</sup> and R Street light rail station (1.5 blocks), the high quality design, and the pedestrian elements.

After much discussion about the site plan, we believe that the addition of a seventh unit (as originally designed) reflects a better utilization of the available land and is not disruptive to the neighborhood (in terms of massing and scale).

FLRT is happy to support smart growth, transit-oriented development projects like the 24<sup>th</sup> and T Condos. Further, we applaud Ms. Rosen's extensive outreach efforts and her careful attention to detail. We look forward to hearing the commission's discussion and hope that you will approve this project.

Sincerely,

A handwritten signature in black ink, appearing to read 'Dain Domich', is written over a light blue horizontal line.

Dain Domich  
President

cc: Andrea Rosen, Rosen Development, LLC  
cc: Robert King Fong, City of Sacramento, Councilmember, Dist. 4  
cc: David Kwong, City of Sacramento, Planning Director

*(Support letter circulated by email)*

**David Hung**

**From:** Lauren Michele [lauren.michele@policyinmotion.com]  
**Sent:** Friday, February 25, 2011 5:37 PM  
**To:** David Hung  
**Subject:** 24th and T Courtyard Condominiums or P10-089  
**Attachments:** Letter of Support for 24th and T Courtyard Condos P10-089.pdf; Growing Beautiful Communities\_Policy in Motion.pdf

Dear Mr. Hung,

Please find attached a letter of support for the 24th and T Street Courtyard Condominiums. As a resident of Newton Booth neighborhood, I am writing in strong support of the City's approval of the six units proposed by this applicant. I also am a certified Small and Underutilized Disadvantaged Business Enterprise owner in this neighborhood (Policy in Motion) and I believe this project *as proposed* would greatly enhance my neighborhood. I have spoken with my neighbors and fellow business owners in Newton Booth and they agree.

My letter outlines three major points:

- 1) The City of Sacramento has a responsibility to implement the community-approved General Plan, and this project is consistent with those goals and standards/codes
- 2) Abandoned lots NOT developed in Newton Booth are causing businesses and residents nuance and safety issues – with three businesses and myself as a resident often calling City Police
- 3) The City should support this project because it will increase economic development and competitiveness locally and puts the region in a better position to secure federal housing/transit funding
- 4) I work on sustainable community implementation and policy with the federal government, and they are using Sacramento as a nation model in promoting residential infill development near transit consistent with "Transit Priority Areas" in Senate Bill 375 (Steinberg, 2008)

In addition to being a proud Newton Booth resident and business owner, I am also have the following credentials:

- Board Member of WALK Sacramento Since 2007
- Car-free Sacramento Resident and Tenant Since 2006
- Transportation Planner/Engineer with Fehr & Peers during City of Sac General Plan Update
- B.S. from UC Davis in Environmental Policy Analysis and Planning
- M.S. from UC Davis in Transportation Policy under the Urban Land Use and Transportation Center
- Policy in Motion Work on Federal and State Legislation on Implementing Sustainable Developments in Communities

Please feel free to contact me with any questions,

~Lauren Michele~



POLICY  
in Motion

Woman Business Enterprise | 530.848.4342 | [lauren.michele@policyinmotion.com](mailto:lauren.michele@policyinmotion.com)

HOW CAN TRANSPORTATION POLICY GROW BEAUTIFUL COMMUNITIES?  
[www.policyinmotion.com](http://www.policyinmotion.com)

Lauren Michele / 530.848.4342 / lauren.michele@policyinmotion.com / www.policyinmotion.com



Lauren Michele  
2715 V Street, Unit B  
Sacramento, CA

February 25, 2011

RE: 24<sup>th</sup> and T Courtyard Condominiums P10-089

As a resident of Newton Booth neighborhood, I am writing in strong support of the City's approval of the six units proposed by this applicant. I also am a certified Small and Underutilized Disadvantaged Business Enterprise owner in this neighborhood (Policy in Motion) and I believe this project *as proposed* would greatly enhance my neighborhood. I have spoken with my neighbors and fellow business owners in Newton Booth and they agree. Three other neighborhood supportive business owners include:

- Dr. Richard Detrano, Dentistry at 2714 V Street
- Dr. Lance Cassazza, Cassazza Chiropractic at 2716 V Street
- Gina Geneshla, Revolution Wines at S and 28<sup>th</sup>

Dr. Cassazza and Detrano both share similar concerns of mine regarding the problems associated with abandoned lots that are NOT developed – having encountered numerous problems with homelessness and nuisances on a lot at 27<sup>th</sup> and V Street which is similar in size to the lot at 24<sup>th</sup> and T Street. We all frequently call Sacramento City Police over the problems associated with this abandoned lot. Further, there are plans for a future school on V Street between 26<sup>th</sup> and 27<sup>th</sup> and it will not be safe for our children to have ANY abandoned lots in our neighborhood, including that at 24<sup>th</sup> and T Street. The City needs to recognize the SAFETY and NUISANCE problems associated with abandoned lots.

Residents and business owners benefit from neighborhood-scale, high-end residential development such as the proposed project because it would:

- 1) Remove the safety and nuance problems associated with abandoned lots;
- 2) Benefit the economic development of the surrounding property home values;
- 3) Bring additional economic development to local businesses

I am also proud to be a car-free resident of Newton Booth and this project, being so close to the 24<sup>th</sup> Street light rail station, will make it much easier for residents who live there to pursue car-free or less auto-dependent lifestyles. There are so many benefits from living close to transit and in a walk-friendly, bike-friendly location such as 24<sup>th</sup> and T, I welcome these new residents to our neighborhood who I would image to be very similar in socio-demographic characteristics to myself as a young professional. We are very fortunate that this applicant is willing to spend the considerable sums it will take to build such a well-designed project that is proposed.

It is commendable that this applicant has crafted a project for this parcel that perfectly matches the requirements of the current multi-family zoning and the Sacramento General Plan. This is exactly the type of project that the City planned for this location. Objections to a use that is consistent with existing zoning and the General Plan should be summarily rejected. Lower density suggestions are

Lauren Michele / 530.848.4342 / lauren.michele@policyinmotion.com / www.policyinmotion.com



not consistent with the General Plan's infill goals, transportation goals, air pollution and greenhouse gas reduction goals. Why miss an opportunity like this?

Midtown Sacramento must use one of its most precious resources- land near light rail- for the highest and best use. Land use decisions such as this one have very long-term impacts. This project is medium-density, modest-scale and traditionally laid out. Putting the garages on the alley follows the tradition of the apartments on this half-block and makes the 24<sup>th</sup> Street side much more pedestrian-friendly. The applicant requests minor non-substantive variances which should be granted. The applicant should be applauded for putting a parking space plus back onto 24<sup>th</sup> Street by eliminating the existing curb cut which also makes 24<sup>th</sup> Street more pedestrian-friendly.

As a professional transportation planner and policy analyst on implementing sustainable communities, I urge the City of Sacramento to expedite approval of this project. If Sacramento wants to be a sustainable community as it claims, it should be sending the right message to developers of Smart Growth projects. I work on federal policy which frequently highlights the efforts in the Sacramento region in supporting infill projects, and it would be an embarrassment to the City of Sacramento to report to the Federal Partnership for Sustainability that efforts locally in supporting economically, environmentally and socially responsibly development is getting emotionally hijacked by irrational concerns from other neighbors. The Sacramento Region recently received \$1.5 million from this Partnership to implement residential housing within half a mile of transit stations, in alignment with California's Senate Bill 375 (2008) stimulations to incentivize "Transit Priority Areas." NOT approving this project will decrease the region's economic competitiveness for future funding.

I have attached to this letter an illustration of how the City of Sacramento can choose to be a model in supporting the building of beautiful communities. Please feel free to contact me should you have any questions, and I look forward to seeing you at the March 10<sup>th</sup> Planning Commission and any upcoming City Council meetings where I will be happy to point out the economic impacts of the City's choices.

Sincerely,

~Lauren Michele~



Woman Business Enterprise | 530.848.4342 | lauren.michele@policyinmotion.com

HOW CAN TRANSPORTATION POLICY GROW BEAUTIFUL COMMUNITIES?  
[www.policyinmotion.com](http://www.policyinmotion.com)



David Hung, Planner  
City of Sacramento  
Sent by Email

January 13, 2011

Dear David Hung:

Design Sacramento 4 Health is a group of physicians and other health professionals who are committed to improving public health through changes in our built environment. Design Sacramento 4 Health supports community development that supports and promotes human health, such as urban infill, compact development, and mixed-use development that encourages walking, bicycling, and use of public transit. Our goals are:

- High-quality, compact development where people can live, work, shop and play without necessarily needing a car
- Communities that are safe for pedestrians and bicyclists
- Reduced generation of air pollution through replacement of vehicle trips with walking and bicycling

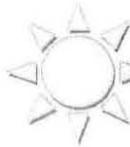
We have reviewed the information on the infill project proposed by Rosen Development LLC in midtown Sacramento at 24<sup>th</sup> and T Streets. Many features of the project will promote public health and physical activity, including that it's a short walk to the closest light rail station, its proximity to many retail outlets and services, the interior bicycle storage, and its pedestrian and bike-friendly orientation. The car is accommodated by the alley-loaded garage but it is not emphasized. We support the five proposed off-street parking spaces and see no need for any additional parking as this project will attract residents who do not rely exclusively on cars for transportation. The project would improve infrastructure for walking by eliminating a driveway, which results in one less conflict point with cars. These features motivate residents to walk, bicycle, or use public transit to arrive at nearby jobs, services, and shopping, providing them the benefits of increased physical activity and giving them a direct, personal opportunity to reduce air pollution by driving less. Also, as more residents walk, bicycle, and take public transit to their destinations, increased social interaction and "eyes on the street" form a safety net that promotes community connectedness, reduces incidence of depression and other mental health issues, and can reduce the incidence of crime and violence.

Design Sacramento 4 Health supports the proposed project at 24<sup>th</sup> and T Streets as currently proposed for its potential to contribute to improving public health in the city of Sacramento. Further, we support your compliance with the current Sacramento General Plan by building at least six dwelling units on this large parcel near light rail giving more central city residents a unique opportunity to live in newly- built high quality housing near light rail. If you have any questions, please do not hesitate to contact me at 916-874-5257, or [TDuarte@airquality.org](mailto:TDuarte@airquality.org).

Sincerely,

Teri H. Duarte, MPH, Chair  
Design Sacramento 4 Health

Cc: Councilman Robert King Fong  
Mayor Kevin Johnson  
David Kwong, City of Sacramento  
Andrea Rosen, Rosen Development LLC



**SMART GROWTH LEADERSHIP RECOGNITION PROGRAM**

October 28, 2010

Rosen Development, LLC.  
2226 Portola Way  
Sacramento, CA 95818  
Andrea Rosen: (916) 457-6721, cell:(916) 508-6721  
[andrearosen@sbcglobal.net](mailto:andrearosen@sbcglobal.net)  
Ben Rosen: cell:(916) 761-1912 [bcrosen@gmail.com](mailto:bcrosen@gmail.com)

**Re: Letter of Endorsement for Courtyard Housing at 24<sup>th</sup> and T Project**

Dear Andrea Rosen:

On behalf of the Smart Growth Leadership Recognition Program, we are pleased to inform you that your project **Courtyard Housing at 24<sup>th</sup> and T**, on the basis of the application and exhibits you submitted and your presentation to the program review committee on October 7<sup>th</sup> 2010, the committee and Boards of Directors of the American Institute of Architects Central Valley (AIACV), and the Environmental Council of Sacramento (ECOS), find that the project meets the guidelines of the Smart Growth Leadership Recognition Program and have awarded this Endorsement.

A copy of this endorsement letter will be posted on the websites of ECOS and the AIACV in a locked PDF format. Copies of endorsement letters will only be provided to third parties with your written consent.

We appreciate the value and sustainability that the proposed Courtyard Housing at 24<sup>th</sup> and T project will provide the community. We hope this Endorsement will help you advance your project. This endorsement must be re-evaluated if the project undergoes significant changes and this Endorsement letter is then no longer valid for use as a vehicle of support.

Endorsements are based on the overall quality of a project and made in an effort to be proactive without waiting for completion of legal and environmental review. Endorsement does not reflect an opinion as to project consistency with any requirements that may apply to governmental agency consideration or approval.

Any recognition regarding this Endorsement should be made in the name of the Smart Growth Leadership Recognition Program, a joint program of AIACV and ECOS.

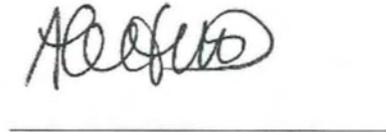
We are looking forward to seeing the Courtyard Housing at 24<sup>th</sup> and T project completed and wish you success.

Sincerely,



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President  
AIA Central Valley



---

President  
ECOS

**David Hung**

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**From:** Karen Jacques [threegables@macnexus.org]  
**Sent:** Sunday, September 26, 2010 9:04 PM  
**To:** David Hung  
**Cc:** Rosen Andrea  
**Subject:** Z10-142

Dear Mr. Hung,

I am a long term Central City activist and also own property within the Newton Booth neighborhood. (My four properties are within approximately three blocks of the project.)

I have had the opportunity to review this project in detail and am writing in support of it. The house and garage that were formerly on the site (the only properties on the site) were both in extremely poor condition and it did not appear to be economically feasible to repair them. This project, which consists of six apartments and five garages, is both attractively designed and makes good use of the entire site. It is a modern take on two older courtyard apartments that are located in the same block. While modern in design, the project is attractive and is compatible with those earlier apartments in terms of height and massing. It will eliminate the blight of a building that has been vacant and deteriorating for the past several years and will be a pleasant addition to the neighborhood. I also appreciate that the project design will result in the elimination of a driveway that is currently on 24th Street and thus create an extra on street parking space on the block. In my view, that extra space provides mitigation for the fact that the project has six units and only five garages.

The project owner, Andrea Rosen, has indicated that she is considering building the apartments to condominium standards so that they could be converted to individual ownership at some future date. I hope she will do this as the units are attractive enough that owner occupants would be interested in them and the neighborhood, like all Central City neighborhoods, could benefit from more owner occupants.

I appreciate Ms. Rosen taking the time to meet with me about this project and I also appreciate the opportunity to comment on it.

Sincerely,

Karen Jacques  
[threegables@macnexus.org](mailto:threegables@macnexus.org)

cc: Andrea Rosen

**David Hung**

---

**From:** ANDREA ROSEN [andrearosen@sbcglobal.net]  
**Sent:** Monday, October 11, 2010 9:46 AM  
**To:** David Hung  
**Subject:** Fw: Re: 24th and T Courtyard Housing

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

**Andrea Rosen**

(916) 457-6721  
[andrearosen@sbcglobal.net](mailto:andrearosen@sbcglobal.net)

--- On Sun, 10/10/10, Gene and Kay Knepprath <[kaygenek@saclink.csus.edu](mailto:kaygenek@saclink.csus.edu)> wrote:

From: Gene and Kay Knepprath <[kaygenek@saclink.csus.edu](mailto:kaygenek@saclink.csus.edu)>  
Subject: Re: 24th and T Courtyard Housing  
To: "ANDREA ROSEN" <[andrearosen@sbcglobal.net](mailto:andrearosen@sbcglobal.net)>  
Date: Sunday, October 10, 2010, 10:07 PM

I sent it in Word X which is not the latest version.

KAY KNEPPRATH

2620 P STREET

Sacramento, CA, 95816

David Hung  
Planning Department  
City of Sacramento

RE: Z10-142

Dear Mr. Hung:

I am writing to support the proposed development of the 24<sup>th</sup> & T Street Courtyard Housing by Rosen Development, LLC. I have reviewed the plans for this site and believe that the proposed project will be an improvement to the area.

A known and reliable mother and son although a new corporation, form Rosen Development, LLC. Andrea Rosen is active in the community and in her neighborhood. Her son, Ben, works for a non-profit housing developer. Both are experienced in developing housing. They have contracted with Ron Vrilakas, one of

Sacramento's architects known for his sensitivity to neighborhood values. This team will build a project of which the Poverty Ridge neighborhood will be proud.

This proposal is innovative in its land use, merging two lots, one of which has lain fallow and the other that has had a vacant house on it for some time. Six units of superior rental housing will be added to the neighborhood, and a blighted corner will be transformed into an eye-catching building.

I urge that the special permit to waive one parking space be granted. On street parking is historically acceptable in the neighborhood. Few residences had parking when they were built in the early 1900's. Additionally, one curb cut will be filled, making on-street parking accessible. I also urge that the special permit to reduce the street side setback for the garage building be granted and that the Plan Review-New Site Plan be approved.

If you would like to contact me for more information, I'm at 916-457-3793.

Sincerely,

Kay Knepprath

On Oct 10, 2010, at 9:48 PM, ANDREA ROSEN wrote:

Thanks Kay. but I can't open it. what do you suggest?

Congrats to Paul on his BIG upcoming birthday. I remember mine!

**Andrea Rosen**  
(916) 457-6721  
[andrearosen@sbcglobal.net](mailto:andrearosen@sbcglobal.net)

-- On Sun, 10/10/10, Gene and Kay Knepprath <[kaygenek@saclink.csus.edu](mailto:kaygenek@saclink.csus.edu)> wrote:

From: Gene and Kay Knepprath <[kaygenek@saclink.csus.edu](mailto:kaygenek@saclink.csus.edu)>  
Subject: Re: 24th and T Courtyard Housing  
To: "ANDREA ROSEN" <[andrearosen@sbcglobal.net](mailto:andrearosen@sbcglobal.net)>  
Date: Sunday, October 10, 2010, 4:15 PM

Andrea: Here's my letter. Glad to do it. Hope it helps.  
Kay

Michael Monasky  
9035 Plaza Park Drive  
Elk Grove, CA 95624  
916-832-5750  
Thursday, January 13, 2011

David Hung, Planner  
City of Sacramento

***RE: ROSEN PROJECT AT 24<sup>TH</sup> & T STREETS***

Dear Mr. Hung:

At the January 6, 2011 Design Sacramento For Health (DS4H) meeting, I listened to the project proposed by developer Andrea Rosen and I am in support of the concepts she communicated.

First, the project includes five garages for six units. This will require a variance from the city, but it will put cars in the alley instead of on the street or in the front yards.

The project includes six large units with individual patios.

There is a large, outdoor commons area in the center of the project, which includes bike racks for visitors.

The project will accommodate a flat or pitched roof, whichever the neighbors prefer.

The project is endorsed by: Michael McKeever and the Sacramento Area Congress of Governments which fits the Regional Blueprint as smart growth, and; the Sacramento Area Bicycle Advocates, as the project allows for reduced automobile use and increased bicycle and pedestrian use.

The project is less than two blocks from light rail.

The project features upscale, moderately priced rentals that serve the midtown, government office, and hospital service sectors.

Ms. Rosen's project deserves special consideration by the Planning Department, the Planning Commission, and the Sacramento City Council. It reflects the best conversion of property to smart growth development. The city is fortunate to have such a thoughtfully designed project before it for consideration. I heartily endorse the concept as it builds community with its shared commons spaces, and is a pedestrian-friendly project in the interest of smart infill growth that will encourage healthy, aerobic activities such as walking, bicycling, and use of public transit.

Sincerely,

Michael Monasky

**David Hung**

---

**From:** b.burg@comcast.net  
**Sent:** Monday, February 14, 2011 2:24 PM  
**To:** David Hung; Robert King Fong; ANDREA ROSEN; Lisa Nava  
**Subject:** Letter of Support for Z10-142, Courtyard Condominiums at 24th and T

To whom it may concern:

I am a resident of the Winn Park neighborhood, four blocks from the project site. As a student of Sacramento's history and the history of urban planning, transport and development, I wish to address how the 24th & T project reflects and meets the historic patterns of the neighborhood where it is planned.

This letter is intended to express my support for Project Z10-142, the 24th & T Courtyard Condominiums project. The project is located between the Poverty Ridge and Newton Booth neighborhoods, whose development was driven by the presence of a streetcar line along T Street that allowed residents easy access to downtown Sacramento. Unlike postwar suburban neighborhoods, streetcar suburbs like this neighborhood included both single-family and multi-family housing on small lots, with the highest property values and highest densities adjacent to the transit line. The built environment of the neighborhood reflects this diversity of density, as a mixture of single-family homes and 1-3 story apartment buildings, apparent even after 70 years without a streetcar on T Street. The end of streetcar service, changes in development policy and other factors caused a shift in later neighborhoods to promote exclusive single-family neighborhoods over the mixed, diverse and walkable neighborhoods of previous eras, and dependence upon the automobile encouraged low-density neighborhoods that limited walkability and excluded public transit. Even in that era, this neighborhood maintained its walkability, reflected by the creation of high-density apartments on single lots.

As American cities like Sacramento rediscover the urban planning lessons learned a century ago, using terms like "transit-oriented development" and "walkable neighborhoods", development projects like this one represent a return to traditional neighborhood densities. The project site still has close proximity to transit and a street network well-suited to walking and cycling. The proposed density is effectively identical to the neighborhood's historic density in the era of its construction. The proposed units are lower in density than many of the mid-century apartment buildings in the neighborhood, with unit sizes more reflective of the neighborhood's historic homes. This project represents an excellent balance between unit size, affordability and density, within a walkable historic neighborhood.

While the project area is not within a currently listed city historic district, the neighborhood's architectural character is very similar to many of Sacramento's currently listed historic neighborhoods. The proposed buildings have a height and form that does not overpower or overshadow adjacent buildings, with roof height equal to or lower than neighboring buildings. Their architecture does not seek to mimic a particular historic architectural style, but is visually similar to earlier flat-roofed Art Deco and Prairie style buildings found within the neighborhood, constructed during the streetcar era. Since their function replicates the neighborhood's historic use as a mixed-density residential area, their architectural form expresses a return to that historic function.

The current project will add at least one on-street parking space by filling in a former curb cut, but I encourage the applicant and the city to include the condition that parking along 24th Street be converted to angled parking. This will provide additional parking spaces to address neighborhood

concerns about increased parking demand due to this project. I also encourage the city and the applicant to use a lighting plan that provides safe and well-lit routes from these residences along the sidewalks to the street, an amenity that will benefit the safety of both the project residents and those already in the neighborhood.

I strongly encourage the City of Sacramento to support this project.

William Burg

**David Hung**

---

**From:** Ruth Ann Bertsch [rbertsch@stanfordalumni.org]  
**Sent:** Saturday, February 26, 2011 6:20 AM  
**To:** David Hung  
**Cc:** 'ANDREA ROSEN'  
**Subject:** Sacramento needs Andrea Rosen to build the courtyard houses at 24th and T  
**Attachments:** Andrea Rosen email for the Planning Commission b.doc

4237 J Street  
 Sacramento, CA 95819-3743  
 (916) 454-4021  
 Pager (916) 499-0239  
[rbertsch@stanfordalumni.org](mailto:rbertsch@stanfordalumni.org)

February 26, 2011

Dear Planning Commissioners:

Andrea Rosen's proposed six-unit housing development at 24<sup>th</sup> and T is a wonderful opportunity for Sacramento and should be approved. Speaking as a physician, her method of elegantly housing several families in this medium density plan will reduce pollution in Sacramento and improve its health. From a taxpayer's and amateur urban planner's perspective, locating up-scale housing so close to existing light rail supports the local economy and increases tax revenue to our city.

The residents of the 24<sup>th</sup> and T Courtyard will be more likely to use public transit than most Sacramentans. T and 24<sup>th</sup> is only 1.5 blocks away from light rail. It is within the area of the city that is easily navigable by bicycle. The Courtyard project offers a single car garage, promoting families that prefer to rely heavily on alternative modes of transportation. The more middle and upper class denizens use public transportation, the more everyone uses it.

This translates into less pollution (and more revenue for public transit). If the handful of households of the 24<sup>th</sup> and T Courtyard weren't living in the Courtyard, they would be living in the suburbs, driving cars more and polluting the air Sacramento breathes. It may seem hard to believe that a handful of homes can make a difference in public health. However, when Atlanta increased its bus services during the Olympic games, the number of pediatric visits to emergency rooms for asthma attacks dropped (*JAMA* 2001 285:897-905). Speaking as a hospital physician, Sacramento suffers from a disproportionately high level of respiratory disease that is worsened by pollution, including emphysema, COPD, and asthma.

Infill housing encourages more people to use active modes of transportation more often. This helps reduce obesity, depression, arthritis, diabetes, and hypertension. Obesity and its companion complications are the biggest growing public health problem of this century. Approximately half of Americans are overweight now. One third of the kids born in the last decade will develop diabetes. Diabetes shortens people's life spans approximately ten years, and the last year of life is frequently not fun – often full of painful neuropathies, amputations, and dialysis. Although *individuals* can often commit to losing weight and exercising more, a public health problem needs a public health solution. Encouraging people to walk or bike more and making it more difficult to drive is very effective. Just a few blocks of walking to a light rail station can reduce a population's weight by approximately five pounds over 1-1.5 years (*Am J Prev Med* 2010;39(2):105-112.)

From the perspective of a fellow resident of Sacramento, having more people outdoors walking and biking in the streets fosters strong communities, for which some studies show American consumers are willing to pay

more per house. Denser housing also increases spending within Sacramento, as opposed to more far-flung locales which are more accessible by car. People who use active modes of transit tend to spend within a very small radius around their home.

Sincerely,

Ruth Ann Bertsch, M.D., Ph.D., F.A.C.P.

\*\*\*\*\*  
Ruth Ann Bertsch, M.D., Ph.D., F.A.C.P.  
Assistant Clinical Professor, U.C. Davis School of Medicine  
pager (916) 499-0239  
email: [rbertsch@stanfordalumni.org](mailto:rbertsch@stanfordalumni.org)  
home: (916) 454-4021  
\*\*\*\*\*

**David Hung**

---

**From:** Matthew Gerken [jmatthewgerken@gmail.com]  
**Sent:** Saturday, February 26, 2011 7:48 AM  
**To:** David Hung  
**Subject:** Support for 24th and T Street Courtyard Condominiums

Dear Mr. Hung,

This email indicates my support for approval of the 24th and T Street Courtyard Condominiums project.

This project would greatly enhance the neighborhood in which it is proposed, as well as provide a range of benefits for the City as a whole. Additional "rooftops" in this area will support existing businesses and could, along with other similar residential activity in the area, provide the criteria needed by future businesses to open in the area. Improvements to property and structures will increase the property tax base for the City. Activating this area with buildings that are oriented to the street and sidewalk, with additional "eyes on the street" will enhance safety and security. Establishing more compact housing choices near the 23rd Street light rail station will help support Regional Transit ridership and provide the opportunity for more workers to live near job centers downtown and in midtown and to access those jobs via light rail.

At times, private and localized concerns can derail thoughtful development plans and projects with broader benefits for the entire community. In those instances, it is the responsibility of urban planners to help our appointed and elected leaders understand the full range of issues and benefits. It is important to frame the discussion for projects such as this at the neighborhood and communitywide level, and to underscore the consistency between the project at hand and the recently adopted General Plan. I am confident that any objective analysis along these lines would lead staff and the Planning Commission to support the proposed project.

Thank you for considering my comments and for your public service.

Sincerely,  
J. Matthew Gerken, AICP  
Sacramento, CA 95819

**David Hung**

---

**From:** Jon Marshack [jon.marshack@att.net]  
**Sent:** Saturday, February 26, 2011 3:20 PM  
**To:** David Hung  
**Cc:** Andrea Rosen; Steve Cohn; Robert King Fong; Ron Vrilakas  
**Subject:** 24th and T - Courtyard Condominiums

Mr. Hung,

I am writing to express my support for the Courtyard Condominiums project proposed for the northwest corner of 24th and T Streets in Midtown. I have reviewed the plans and elevations for the project, have walked the neighborhood where this project is proposed to be built, and have met with the project proponent, Andrea Rosen. I believe that this project is a good fit for the neighborhood. Its scale, massing, style, and density of dwelling units is quite compatible with the context in which it is proposed to be built. It fits the 2030 General Plan goal of adding additional density to the Central City while also meeting the Central City Neighborhood Design Guidelines that govern this project. Drawing on art deco and art moderne stylistic elements of the 1920s and '30s, the Courtyard Condominiums project will complement its surrounding, somewhat eclectic neighborhood of high water bungalows, tudor courtyard apartments, and two story apartment buildings. (Note: I am ignoring the shingle-fronted four-plex immediately east of the project across 24th Street as an aberration from a time before neighborhood design guidelines were created.)

This project situation is nothing like that of the proposed 2207 C Street project that was recently called up to City Council by Councilman Cohn. That proposed single family project had both scale and design that were out of place in the immediate neighborhood of small, single family pre-WW II residential structures. While in my opinion not completely supportable by the Central City Neighborhood Design Guidelines, the City Council's decision to either accept the proposed project's scale or its design, but not both, was an appropriate compromise. No comparison should be made between 2207 C Street project and the proposed Courtyard Condominiums project.

Dr. Jon B. Marshack  
2308 H Street  
(916) 202-8331

**David Hung**

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**From:** Nita Davidson [nadavidson@att.net]  
**Sent:** Sunday, February 27, 2011 11:19 PM  
**To:** David Hung  
**Cc:** Robert King Fong; Steve Cohn  
**Subject:** Courtyard Condominiums, T & 24th Streets

Dear Mr. Hung,

I'm writing in support of the infill project, the Courtyard Condominiums, proposed for 24th and T streets.

As both an environmentalist and preservationist—and resident of Midtown since 1988—I support sensible infill. I've seen the plans for the Courtyard Condominiums and am impressed by the well-designed Art Deco look of the project and the number of units for this double parcel. The size of each unit is perfect for people who want manageable, affordable space in a prime urban spot. The project's location—close to light rail, jobs, and Midtown attractions—characterizes it as smart, sustainable infill.

I opposed the project at 2207 C Street because it didn't represent good infill. Unlike the Courtyard Condominiums, the proposed C Street project was a three-story, single-family home that towered over the quaint, one-story C Street houses like a suburban eyesore. The neighbors who opposed that project did so because the house was architecturally discordant with their houses. Also, the house's location would have compromised the problematic sewer system. The C Street neighbors do not embrace NIMBYism. In 2004 they actively rallied behind a major infill project, the development of Courtyard School, despite the traffic and noise the school has generated in the area.

The Courtyard Condominiums project is ideally situated for its location, fits in well with the Sacramento General Plan, and exemplifies smart growth for Midtown.

Sincerely,

Dr. Nita Davidson  
714 21st Street  
Sacramento 95811

**David Hung**

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**From:** sarah underwood [skunderwood@gmail.com]  
**Sent:** Saturday, February 26, 2011 10:49 AM  
**To:** David Hung  
**Subject:** YES on 24th and T Street Courtyard Condos (P10-089)

I encourage you to support the T Street Courtyard Condos. I'm a mid-town resident, and in favor of this development.

Sincerely,  
Sarah Underwood

Attachment 3 – Opposition Letters

1. Petition #1
2. Petition #2
3. Bill Robertson
4. Heather C. Scott
5. Mabel Lee Robbins
6. Christina Jewett and Floyd Marvin
7. Susan Woodward (“Kelley”)
8. John Hagar
9. Alex Zabelin (President of Newton Booth Neighborhood Association)
10. Pat Melarkey
11. Pamela J. Wade
12. Bridget Whitted
13. Steve Whitted
14. Marlene Rice
15. Linda A. McNamara
16. Timothy Gussner
17. Alan LoFaso
18. Claire Pomeroy, MD, MBA
19. Letter sent to Claire Pomeroy
20. Michael Trostel

PETITION #1

October 18, 2010

City Councilman Rob Fong  
City of Sacramento  
916 I Street  
Sacramento, CA 95814

**Subject: 24<sup>th</sup> and T Streets Courtyard Apartments Project**

Dear Councilman Fong:

We are strongly opposed to the proposed Courtyard Apartments Project for the following reasons:

1. Currently, there are multiple apartment complexes located in the neighborhood where this project is being proposed, please see the attachment.
2. Parking in the neighborhood is already a problem. Currently, there are two four-unit apartment complexes directly across the street from the proposed site which do not provide parking and require on-street parking only.
3. We strongly oppose the design of the project. It doesn't fit in with the architectural design of the current neighborhood. Most of the current homes were built in the 1920's and 1930's. The flat top roofing, aluminum windows, and vertical wood siding and stained cedar siding, we believe would not only look out of character for our neighborhood but be an eye-sore.
4. We value the quiet neighborhood, single family homes from a specific era and feel there is no need to add additional multi-unit housing in these areas. We strongly oppose this multi-unit project.

We and most of our other neighbors are appalled that the home at this location was destroyed without any advance notice – at least within a block of the site. We are angry that we were not given an opportunity to express our concerns to preserve this home and maintain the character of our neighborhood.

Respectfully submitted,

Neighbors

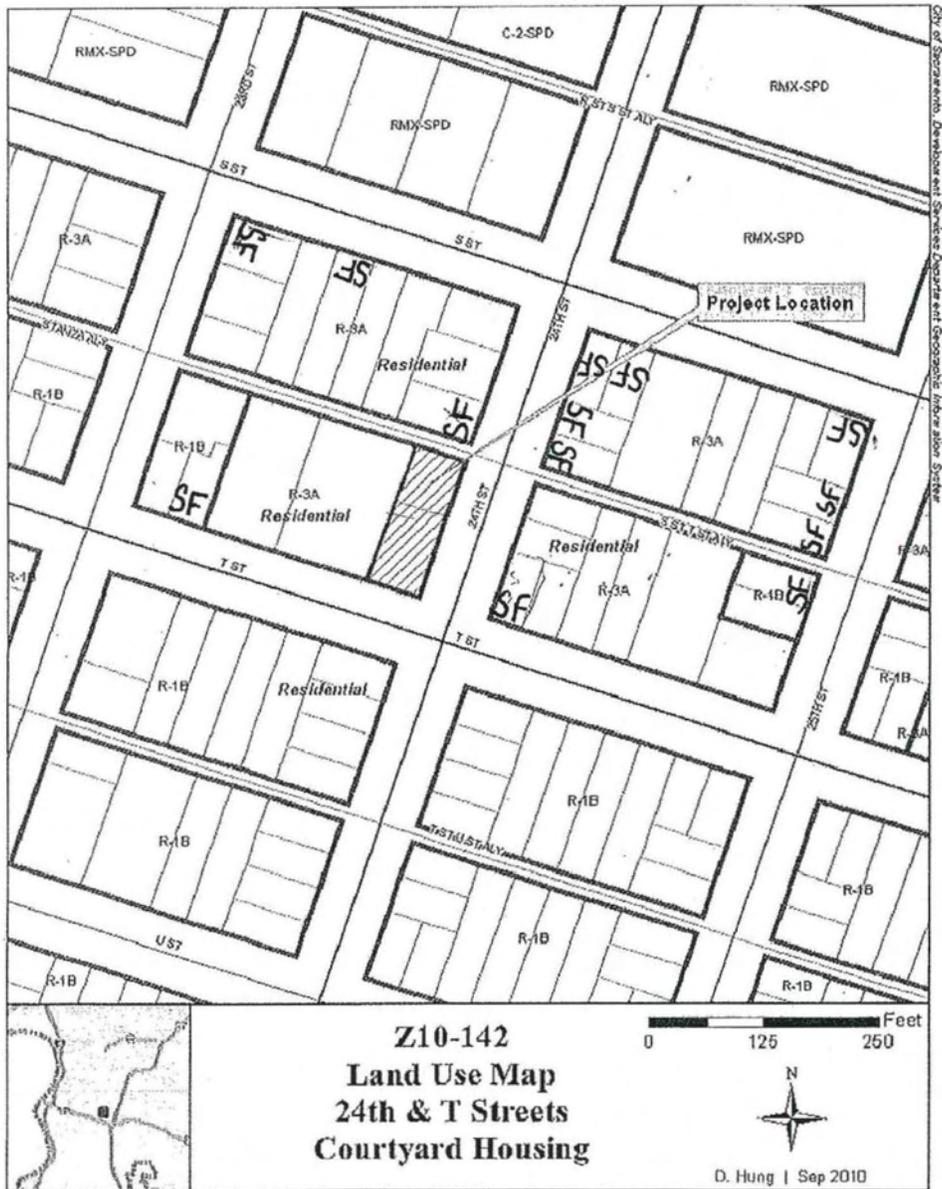
24<sup>th</sup> and T Streets Courtyard Apartment Project – OPPOSITION

Name	Address	Phone or Email
<u>Jenny Gomez</u>	<u>1914 24<sup>th</sup> St.</u>	<u>margomez1@srscglobal.net</u>
<u>Mary Gomez</u>	<u>1914 24<sup>th</sup> St.</u>	<u>margomez1@srscglobal.net</u>
<u>Linda M. Amara</u>	<u>1911 24<sup>th</sup> St.</u>	<u>amarood2@srscglobal.net</u>
<u>Anna Miller</u>	<u>2400 S St.</u>	<u>tanitrosteL@comcast.net</u>
<u>Juditha Celorio</u>	<u>2320 T Street.</u>	<u>Kristine.Celorio@yahoo.com</u>
<u>Jose H. Prado</u>	<u>2305 T St</u>	<u>JHASKO@ATT.NET</u>
<u>Mabel Lee Robbins</u>	<u>2010 24<sup>th</sup> Street</u>	<u>mrobbin@pacbell.net</u>
<u>Reed Richerson</u>	<u>1915 24<sup>th</sup> St.</u>	<u>reed.richerson@gmail.com</u>
<u>Tim Gussner</u>	<u>1911 24<sup>th</sup> St</u>	
<u>Terry Guilford</u>	<u>1921 24<sup>th</sup> St.</u>	<u>tguil75_@hotmail.com</u>
<u>Aracilio Reyes</u>	<u>2112 24<sup>th</sup> St</u>	<u>451-41691</u>
<u>Susan Woodward</u>	<u>2006 24<sup>th</sup> St. So</u>	<u>2006SEW@comcast.net</u>
<u>Susan Maastucht</u>	<u>2325 T. Street</u>	<u>Apt E suzmaasto@yahoo.com</u>
<u>Nikki Corbett</u>	<u>1914 24<sup>th</sup> St</u>	<u>nikkicorbett@gmail.com</u>
<u>ALBERT MONCADA</u>	<u>2009 24<sup>th</sup> St</u>	<u>916-416-1786</u>
<u>Eulalia Moncada</u>	<u>" "</u>	<u>000-000-0000</u>
<u>Ma. A. Moncada</u>	<u>2009-24 St.</u>	
<u>Charles Hippert</u>	<u>2020 24<sup>th</sup> St</u>	<u>(916) 320-4484</u>
<u>Leonard M. Chy</u>	<u>2623 S St</u>	<u>(916) 617 6661</u>
<u>Nichole Pettit</u>	<u>1915 24<sup>th</sup> St</u>	<u>chole531@gmail.com</u>
<u>Atencio Romeo</u>	<u>1908 24<sup>th</sup> St</u>	<u>(916) 739-0576</u>

**Attachment**

**See attached Land Use Map of 24<sup>th</sup> & T Streets:**

A survey was done of the areas between from 23<sup>rd</sup> and S to 25<sup>th</sup> and S Streets and from 23<sup>rd</sup> and T to 25<sup>th</sup> and T Streets and there were 156 multi-family units and 13 single family homes found, which has been highlighted on the attached map:



SF - Single family

Z10-142

Page 2 of 2

CALL 916 264-5011 我們講中文 • Hablamos español • Мы говорим по-русски • ພວກເຮົາເວົ້າພາສາລາວ • Peb hais lus Hmoob • Chúng tôi nói tiếng Việt

PETITION #2

PETITION AGAINST THE APPROVAL OF PROPOSAL Z10-142  
(6-dwelling unit courthouse apartment building at 24th & T Street)

We the undersigned homeowners and residents of the Poverty Ridge and Newton Booth neighborhoods who live immediately around and near the intersection of T Street and 24th Street wish to express our opposition to the requested 6-dwelling unit apartment structure with the file number Z10-142 as it is currently proposed.

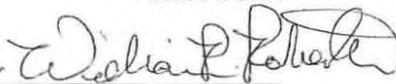
While we acknowledge that the developer and the architect of the proposed structure have contended a sensitivity to the concerns of the surrounding neighborhood, and while the proposal, for better or worse, may fall within the City of Sacramento's current zoning and development guidelines, we contend that the impact of the structure's scope and scale, as well as the waivers requested, will adversely affect the quality of life in the neighborhood with regard to:

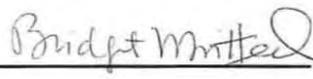
- The overall historic and aesthetic profile of a neighborhood that is already challenged by inappropriate and haphazard design approvals from prior decades.
- The already overwhelming density of on-street parking.
- The delicate and tenuous balance that presently exists between single family residences and large multi-unit complexes.

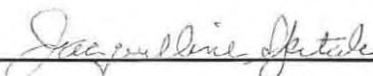
Further, while acknowledging the courtesy and assistance of City planners when contacted, as a point of interest, we wish to express our displeasure with existing City guidelines:

- That allow for the demolition of a home and its vegetation in a residential neighborhood without fair notification to surrounding homeowners before approval of a replacement structure has been granted.
- That in so doing leverages debate in such a way that contesting neighbors must accept either a dirt lot with a chain-link fence or undesired construction.

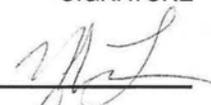
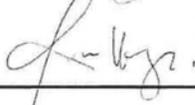
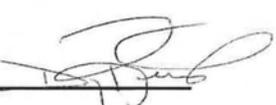
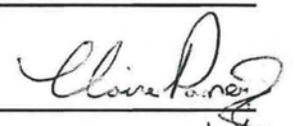
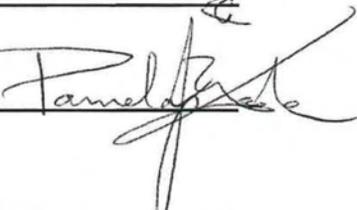
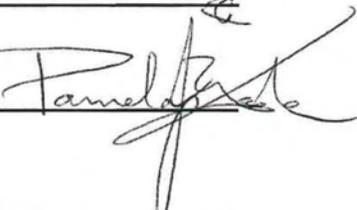
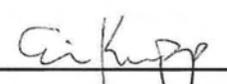
NAME	ADDRESS	SIGNATURE
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William P. Robertson	2009 23rd St.	
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Bridget Whitted	2314 T Street, 95816	
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<del>John</del> Jacqueline Spitalo		
	2050 24th St	

PETITION AGAINST THE APPROVAL OF PROPOSAL Z10-142  
 (6-dwelling unit courthouse apartment building at 24th & T Street)

NAME	ADDRESS	SIGNATURE
Nick Timothy	2110 24th St	
JOHN HAGAR John Hagar	2417 U Street	
Biane Crawford	2413 U St.	
Bill Rader	2423 U ST	
Steve Whitted	2314 T Street	
DANNY RENTSCHLER	2021 24TH STREET	
Eddie Bulesteros	2325 T St. Apt. A	
Claire Pomeroy	2009 23rd St.	
Pamela J. Wade	2020 24th St	
" Landlord	2408 T Street	
Eric Knapp	2016 24th St.	

PETITION AGAINST THE APPROVAL OF PROPOSAL Z10-142  
 (6-dwelling unit courthouse apartment building at 24th & T Street)

NAME	ADDRESS	SIGNATURE
<u>Dana Ngo</u>	<u>2016 24<sup>th</sup> St Sacramento, 95818</u>	<u>Dana Ngo</u>
<u>Alan LoFaso</u>	<u>2001 24<sup>th</sup> Street Sac 95818</u>	<u>Alan LoFaso</u>
<u>Christina Jewett</u>	<u>2424 T St. Sacramento 95816</u>	<u>Christina Jewett</u>
<u>Floyd C. Marvick</u>	<u>2424 T St. Sacramento, CA 95816</u>	<u>Floyd C. Marvick</u>
<u>Susan Woodward</u>	<u>2006 24<sup>th</sup> St Sacramento, 95818</u>	<u>Susan E. Woodward</u>
<u>Cole Miller</u>	<u>2000 24<sup>th</sup> St. Sacramento, 95818</u>	<u>Cole Miller</u>
<u>Huy Ngo</u>	<u>2016 24<sup>th</sup> St Sac, 95818</u>	<u>Huy Ngo</u>
<u>Kathleen Strawser</u>	<u>2308 T St #1 SAC CA 95816</u>	<u>Kathleen Strawser</u>
<u>James J. Sims</u>	<u>2308 T St #5 SAC CA 95816</u>	<u>James J. Sims</u>
<u>Rene Strawser-Brusacoram</u>	<u>2308 T St. #1 Sac., CA. 95816</u>	<u>Rene Strawser-Brusacoram</u>
<u>Ellen Shinfuku</u>	<u>2015 23<sup>rd</sup> St SAC CA 95818</u>	<u>Ellen Shinfuku</u>

PETITION AGAINST THE APPROVAL OF PROPOSAL Z10-142  
(6-dwelling unit courthouse apartment building at 24th & T Street)

NAME	ADDRESS	SIGNATURE
YUJI P. SHINFUKU	2015 23RD ST	Y P Shinfuku
Robert McCartney	2014 25th St.	Robert McCartney
Karen A. Nelson	2014 25th St.	Karen A. Nelson
Frick Whisenhunt	2408 T St	Frick Whisenhunt
Melissa Shinfuku	2015 23rd St.	Melissa Shinfuku
Richard Haroun	2217 U St.	Richard Haroun
Carmie Camarena	2109 25th St	Carmie Camarena
Doug Morrow	2319 T APT D	Doug Morrow
Susan Maastricht	2325 T. Street	Susan Maastricht
Amber Rankin	2325 T St. Apt P	Amber Rankin
Zenaida Gutierrez	2325 T St. Apt. B.	Zenaida Gutierrez

PETITION AGAINST THE APPROVAL OF PROPOSAL Z10-142  
(6-dwelling unit courthouse apartment building at 24th & T Street)

NAME ADDRESS SIGNATURE

David Larson 2319 T St. #A 

Jennifer Law 2319 T St #A 

(per email) owner: R Street Mkt. Lofts X  
Paul Petrevich Whisky Hill Lofts

Cynthia Dale 2420 T St. Suite (916) 838-1503  
95816

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**WILLIAM PRESTON ROBERTSON  
2009 23RD STREET  
SACRAMENTO, CA 95818**

Email: [w.preston.robertson@gmail.com](mailto:w.preston.robertson@gmail.com) | Phone: (916) 607-2405

David Hung, Associate Planner  
Community Development Department  
300 Richards Boulevard, 3rd Floor  
Sacramento, CA 95811

RE: Opposition to P10-089

David:

Thank you for allowing me to condense several months worth of detailed correspondences into one final letter regarding the proposed 6-unit development at 24th & T Street (P10-089).

As a member of the core group of neighborhood representatives that formed in the wake of an emotional community meeting facilitated by Councilman Fong's office in November of 2010, I have been a key participant in communications between the developer, Andrea Rosen, and the concerned residents and property owners of Poverty Ridge and Newton Booth who are affected by this project.

Due to fundamentals of the project that by consensus the neighborhood feels are inappropriate to this specific site, and due to the developer's refusal to negotiate or even discuss those fundamentals in meetings, I must strongly oppose the project at this time.

The developer proposes a two-story, flat-roofed courtyard structure comprised of six two bedroom units with five garage spaces on a centrally located corner lot that previously held a 1940's one-story, single family home with garage until it was demolished this past fall to make way for the project.

The consensus of the neighborhood is that the scale, style and ultimately, the ambition of the project as proposed, if constructed, will represent an inappropriate encroachment into the heart of the neighborhood by the sort of high-density infill development more appropriate to the industrial R Street Corridor a block away.

Rather than fulfilling a nuanced transition from lower density historic residences to higher density urban structures, this project promises an abrupt and unsubtle shift in style and density that effectively redraws the R Street corridor to T Street and in so doing, erodes the integrity of a neighborhood that is bordered by rail lines and freeways and that, despite bearing the scars of decades-past missteps in city planning, is nevertheless still a definable neighborhood worthy of respect.

With nuanced transition in mind, the neighborhood proposed an increase of density to three units with garages rather than six, and in a pitched roof style that more appropriately referenced the neighborhood. This was understood to be our starting point for a negotiation that we hoped would work toward a compromise between the developer's goals and the neighborhood's. Sadly, our negotiations never advanced beyond this stage. The developer stated emphatically that the number of units was "not up for discussion," and the expected war of words followed.

2

The architect of this project has a reputation in Sacramento for noteworthy urban infill projects. Similarly, the developer has her own reputation within the culture of local urban planning buffs--and drawing on this, she invokes well-honed principles of Smart Growth to promote her project.

The principles of Smart Growth are laudable and embraced by our neighborhood's residents every day. We walk, we bike, we use light rail--property owner and renter alike. We welcome diversity. And we enjoy watching our portion of the city molt its tired industrial skin and yield to a more vibrant retail/residential city as evidenced by the R Street Market development, the Whisky Hill Lofts, the Tuli Bistro condo development and Tapestri Square.

This is not now nor has it ever been a debate on Smart Growth principles. This is a debate over how best to apply Smart Growth principles in a specific location.

No one wants a vacant lot. But nor does one want an inappropriately oversized structure in a high profile neighborhood location.

I, along with other neighborhood representatives, remain open to discussions with the developer over how best to apply Smart Growth in our neighborhood. Until then, I must oppose this project's approval.

Sincerely,

William P. Robertson

1. Initial communication from W.P. Robertson to A. Rosen regarding concerns.
2. A. Rosen responds to W.P. Robertson's initial email.
3. Neighborhood petition of opposition submitted to Rob Fong and David Hung.
4. Account of informal presentation on 12-11-10 by A. Rosen to invited neighbors.
5. A. Rosen informed of 12-14-10 neighborhood meeting: creation of consensus and formation of "core group" of representatives.
6. Account to R. Fong & D. Hung of 12-14-10 neighborhood meeting and initial "core group" meeting; official consensus and names of "core group" submitted; scheduled meeting with A. Rosen for 1-9-11 mentioned.
- 6-A. Neighborhood consensus document as presented to A. Rosen on 1-9-11.
7. Account to R. Fong & D. Hung of 1-9-11 meeting between "core group" and A. Rosen: presentation to her of official neighborhood consensus and discussion.
8. A. Rosen response to neighborhood consensus.
- 8-A. Account to R. Fong, L. Nava, & D. Hung re. "core group" follow-up request by H. Scott to A. Rosen for clarification of her consensus response & A. Rosen's reply; desire by "core group" for negotiation affirmed; request by "core group" to R. Fong for design & density workshop.
9. Request by A. LoFaso to R. Fong for design & density workshop.
10. Account to R. Fong & L. Nava re. A. Rosen email finding fault with "core group" for terminating communications; explanation to Fong & Nava of "core group" position; request for mediation by R. Fong.

**1. Initial communication from W.P. Robertson to A. Rosen regarding concerns.**

Date: Wed, 6 Oct 2010 17:05:05 -0700  
Subject: 24th & T Courtyard Housing proposal Z10-142  
From: Bill Robertson <[w.preston.robertson@gmail.com](mailto:w.preston.robertson@gmail.com)>  
To: Andrea Rosen <[andrearosen@sbcglobal.net](mailto:andrearosen@sbcglobal.net)>  
Cc: David Hung <[dhung@cityofsacramento.org](mailto:dhung@cityofsacramento.org)>

Hi, Andrea--

I'm Bill Robertson, the Poverty Ridge resident you called last Friday (but were unable to reach) to discuss your proposed courtyard apartment building at the corner of 24th & T Street. First off, thanks for your swift response after hearing from Associate Planner David Hung. I apologize for not returning the courtesy and getting back to you over the weekend. My wife Claire Pomeroy is vice chancellor and dean at the UC Davis Health System and a string of back-to-back events throughout the weekend that required my participation prevented me from contacting you telephonically. Hopefully this email address, which I pulled from the building proposal will reach you.

To set context, my wife and I live in the big Prairie Style house located at the corner of 23rd and T street, across the street and down the block from your proposed building. About 3 years ago, Claire and I undertook a major renovation of our side yard to replace the off-putting 6-foot wood fence, and rotting wood stairs and decks we inherited when we bought the home. Our new design incorporated a 4-foot stucco wall that matched the house, with 2 feet of wire lattice for vines accented by downward-shining lights on the wall's capped posts. In the yard's interior we built two stucco-and-iron railed porches with stairs, as well as a stamped concrete patio and a large architectural stucco fountain.

Our design intent was two-fold. We wanted to create something that looked architecturally consistent with our 1912 house, and also "communicate" somewhat with the neighborhood. We placed two illuminated corral maples in a recessed part of the exterior wall and we allowed some degree of street interaction with the yard through the vines and iron gates.

Our effort was pretty successful, I think. Neighbors walking home from work have told us that they've changed their route to and from the light rail so that they can pass by our yard. Mothers frequently bring their children and lift them up to peer at our fountain.

So that's where I'm coming from. I like having a nice home that makes my neighbor's feel good, too. My wife and I are big proponents of integrated gentrification.

With this in mind, let me say that I appreciate the design effort you've shown in creating a courtyard apartment. As you know, there are a number of rental structures from the 70's in the neighborhood that gravely lack architectural respect for the neighborhood's

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historical flavor. One of the worse offenders abuts our property on T Street. The people living there are very nice folk. But the apartment building they live in is an eyesore. So thank you for respecting the neighborhood.

Having read your proposal, there are a couple of concerns I would like to raise and they are somewhat linked.

My first concern is with the parking waiver request.

Living on 23rd, I enjoy full, but reasonably uncompetitive parking. This is not the case along T Street or 24th Street. In the evening, those streets are pretty full--particularly when patrons of the Round Corner Bar at S and 24th are taken into account. A busy night there tips the scale. At its current parking capacity, I would say that the neighborhood is manageably saturated at night.

The addition of a 6-unit apartment risks further tipping the scale. The five units of the proposal that have garage parking provided may still yield extras cars on the street, depending on the occupants. Having an entire dwelling's worth of cars definitively added on top of that seems untenable.

My second concern, as well as my wife's, is one of street profile--or the overall roof line of the buildings of the T Street block--and I think it plays into the occupancy issue.

In the current proposal, one of the tallest and chunkiest sections of the structure stands dominantly right at the corner of T and 24th, fairly close to the sidewalk. The artist's depiction in the proposal suggests the height of the structure will be not much higher than the pitched roof of the 1-story Tudor apartment next door. I think the reality will be much different. The "weight" of the building at that corner, will, I fear, impact the overall architectural profile of T Street between 24th and 23rd.

It seems to me that the wish to house 6 dwellings total on the site is a strong influence in this design. I can't help but wonder that if the structure housed 5 dwellings as opposed to 6, a less dominant appearance might be accomplished, with no need for parking waivers.

So that's my spiel. Again, I applaud and thank you for your interest in communicating with the neighborhood, and in the goal to create something attractive. However, I do have concerns about having 6 units on the site both because of parking availability and because of the heavy design it dictates at the corner.

Thanks,

Bill Robertson  
2009 23rd Street  
Sacramento, CA 95818  
916-607-2405

**2. A. Rosen responds to W.P. Robertson's initial email.**

Re: 24th & T Courtyard Housing proposal Z10-142

ANDREA ROSEN <[andrearosen@sbcglobal.net](mailto:andrearosen@sbcglobal.net)>

Wed, Oct 6, 2010 at 8:50 PM

To: Bill Robertson <[w.preston.robertson@gmail.com](mailto:w.preston.robertson@gmail.com)>

Cc: ron vrilakas <[Ron@vrilakasarchitects.com](mailto:Ron@vrilakasarchitects.com)>, Ben Rosen <[bcrosen@gmail.com](mailto:bcrosen@gmail.com)>

Hi Bill

thanks for your email. I'm glad to hear from you. Claire Pomeroy is a local celeb and I know her name if not her likeness.

You'll be tickled to hear that my son ( my development partner) and I LOVE your house and its partner, and largely selected our design approach based on your house. We are also inspired by your side yard which graces T Street.

I think the best next step is to meet in person so we can go over the design and I'd like to include my architect so he can explain better the massing and scale.

I'm off to Ashland tomorrow for my annual Shakespeare and other theatre fix and will return late Sunday night.

Can I set something up for us early next week? if so, please give me some available times. Would you like to meet in the neighborhood, say at Temple Coffee or at the architect's office near Zocalo?

I'll bring some larger drawings and we can go over whatever you wish.

Many thanks,

best way to reach me is by cell phone 916 5086721.

Andrea Rosen  
(916) 457-6721  
[andrearosen@sbcglobal.net](mailto:andrearosen@sbcglobal.net)

**3. Neighborhood petition of opposition submitted to Rob Fong and David Hung.**

Date: Thu, Nov 18, 2010 at 12:28 AM  
Subject: Petition against Z10-142  
From: Bill Robertson <[w.preston.robertson@gmail.com](mailto:w.preston.robertson@gmail.com)>  
To: "Robert K. Fong" <[rkfong@cityofsacramento.org](mailto:rkfong@cityofsacramento.org)>, Lisa Nava <[LNava@cityofsacramento.org](mailto:LNava@cityofsacramento.org)>  
Cc: David Hung <[dhung@cityofsacramento.org](mailto:dhung@cityofsacramento.org)>

Rob and Lisa:

Attached please find a petition (in .pdf format) opposing the 6-dwelling unit apartment building at T & 24th Street as it is currently proposed. The project is identified by the Zoning Administration with the file number of Z10-142.

The petition is signed by 39 homeowners, renters and property owners in the neighborhoods of Poverty Ridge and west Newton Booth. This petition is independent of the previously submitted petition, which was submitted by a different group of concerned neighbors. This petition has different wording and different signatures, with, I believe, only one repetition.

I will be hand-delivering the original copy to the Zoning Administration representatives at the community meeting on the 18th.

Thank you,

William P. Robertson  
2009 23rd Street  
Sacramento, CA

916-607-2405

Z10-142 petition.pdf  
1758K

**4. Account of informal presentation on 12-11-10 by A. Rosen to invited neighbors.**

Sent: Tuesday, December 21, 2010 1:02 PM  
Subject: Update on Dec. 11 informal meeting w/Andrea Rosen  
From: Bill Robertson [mailto:w.preston.robertson@gmail.com]  
To: Robert King Fong; Lisa Nava  
Cc: David Hung

Rob and Lisa:

I want to catch you up to speed on two recent meetings pertaining to Andrea Rosen's proposed 6-unit apartment development at 24th & T Streets, and where things currently stand in my estimation. This email will concern itself with an informal meeting with the developer on Dec. 11. In a follow-up email, I'll report on the outcome of a Dec. 14 private neighborhood meeting that was held at my house.

Ms. Rosen organized a meeting at the Vizcaya mansion on Saturday, Dec. 11 with a small group of neighbors of her choosing. The neighbors were: me, Kelley Woodward, Heather Scott, Alan LoFaso, Steve Whitted, Stephanie Fiore, Alex Zabelin and Morris Lum. Alex Zabelin and Morris Lum are both board members of the Newton Booth Neighborhood Association, but their attendance was not in that capacity, I don't believe, rather as concerned neighbors. Also, Kelley Woodward and Steve Whitted were present at my urging, since both live near the proposed development site.

It was unclear why Ms. Rosen had called the meeting with this specific group of people, and there was concern among the larger neighborhood (as represented in an email list compiled from the Nov. 18 community meeting), as well as by the attendees themselves, that this group would be considered somehow officially representational of the larger neighborhood when that was not the case. A meeting to create just such a small representational group was scheduled for the following Tuesday, Dec. 14, at my home. Any meetings with the developer before that seemed premature, but the selected neighbors felt that demonstrating a willingness to communicate was also important. The neighbors affirmed their non-representational status throughout the meeting with the Dec. 11 meeting with her.

For context's sake, let me say that there's a certain collective paranoia in the neighborhood that was generated by the unannounced demolition of the house on the proposed project's location as well as by the speed with which the proposal seemed to be advancing in the beginning. This collective paranoia has caused a lot of second-guessing on the part of neighbors as they enter a dialogue with the developer. However, we're all aware of the phenomenon and are doing our best.

The December meeting with Andrea and her selected group was extremely amicable. I think the neighbors present were pleasantly surprised at her congeniality as compared to her more aggressive stance before the project was elevated to its current status with the City. Ms. Rosen presented a new drawing of the proposed 6-unit building's exterior that addressed, to her understanding, some of the concerns expressed by the

neighbors at the larger community meeting on Nov. 18. Also presented was a helpful list that summarized changes to her proposal she was willing to consider.

For clarity's sake, I am attaching to this email three .pdf files of what was in part presented by the developer to the attendees with the understanding that she may already have shown them to you.

The overall impression of the small group of neighbors present at the Dec. 11 meeting was that the drawing and listed changes represented an improvement over the drawing in the Z10-142 proposal currently being considered by the City. The building as represented in the new (and unofficial) drawing was far less visually incongruous in its modernness to the surrounding historic architecture, which neighbors have generally characterized as "modest bungalows." Rather than the sort of exterior one sees in buildings located in industrial or retail-heavy urban environments, the new drawing suggested a more "residential" and "traditional" look, with small decorative porches, smaller windows, a lowered courtyard wall and a modestly pitched roofline among other touches.

The developer noted that she was no longer requesting a waiver on setback and was putting in bike racks, among other touches. She also said that her intention now was to sell the units as condos at the outset rather than renting them for ten years and then selling them as condos as she had previously said, and that an application for this change was being filed.

At the end of the meeting, I stated to Ms. Rosen that while all present seemed to find the artist's drawing an improvement, the larger neighborhood, as I had expressed previously, had yet to meet and establish a consensus. After that had occurred, I explained, whatever representative body was formed might present a different set of concerns and represented opinions, and that she should expect to re-explain what she had shown that day.

After this socially congenial, informal and noncommittal meeting with Ms. Rosen, neighbors who attended the meeting spoke with each other in person and in subsequent communications, and a number of points were noted about the developer's presentation:

The developer stated categorically that she was not going to spend money on further redesigns until a specific redesign was agreed to by neighbors and developer. At least one neighbor at the meeting understood this to mean that a letter of neighborhood support for the project had to be issued to Councilman Fong's office before money was spent on a redesign. The neighbors informally agreed that this required an inequitable leap of faith on the part of the neighbors and a level of trust that had not yet been achieved.

While the artist's drawing was an improvement over the drawing presented in the existing proposal, it was also noted that scale was not significantly diminished. It was

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also noted that photographic examples of similar apartment structures presented by the developer were, like the examples presented by architect Vrillakas at the community meeting of Nov. 18, not from the affected neighborhood specifically and, indeed, did not represent any structure near the site.

Finally, it was noted that while the developer emphasized her application to make the structures condominiums for sale sooner rather than later, she did say that if the economy did not improve the structures would still be rented. It was agreed that the application to sell as condos sooner rather than later was an unimportant factor, and that its relevance to the larger neighborhood was questionable in the first place since many of the concerned neighbors are themselves long-term renters and not homeowners.

In the end it was agreed by the neighbors in attendance that what was accomplished at the meeting with Andrea Rosen on Dec. 11 lay more in the realm of interpersonal dynamics among select individuals rather than anything substantive with regard to the proposed development. All agreed that the private neighborhood meeting on Dec. 14 at my house would mark the true beginning of any negotiation process with Ms. Rosen, and that this had been expressed to her.

In a follow-up email, I'll provide an account of the Dec. 14 meeting at my house.

Thanks,

Bill

--

William P. Robertson  
2009 23rd Street  
Sacramento, CA 95818  
916-607-2405

**5. A. Rosen informed of 12-14-10 neighborhood meeting: creation of consensus and formation of "core group" of representatives.**

Date: Fri, 17 Dec 2010  
Subject: neighborhood update  
From: Bill Robertson <w.preston.robertson@gmail.com>  
To: Andrea Rosen <andrearosen@sbcglobal.net>  
Cc: "Robert K. Fong" <rkfong@cityofsacramento.org>, Lisa Nava <LNava@cityofsacramento.org>, David Hung <dhung@cityofsacramento.org>

Andrea--

As promised during the informal meeting at the Vizcaya on Saturday, Dec. 11, this is to let you know that a private neighborhood meeting was held at my house on Tuesday, Dec. 14 during which over 30 members of the neighborhood met to establish an unambiguous consensus regarding your proposed development at 24th & T Street, as well as to create a means by which this consensus could be communicated to you and through which future discussions with you might be held. It was an extremely productive and organized meeting, and included unofficial attendance by members of the NBNA board.

The purpose of this email is not to present any details about the neighborhood's collective stance, but rather to let you know the organizational structure that was created. A core group of approximately 8 neighbors was established to meet with you and represent the larger neighborhood. It will not have bylaws, and it will act wholly independently of the NBNA and not as a sub-committee of the neighborhood association. There are some members of the NBNA board represented in the core group, but they do so in their capacity as private citizens.

I know that your preference is for an ad hoc committee under the guidance of NBNA, but it was decided by all present that greater clarity could be accomplished this way and that an added layer of representation did little to convey the neighborhood's position. You can certainly continue to communicate with the NBNA if you so wish, but please do so with the understanding that with regard to your development, they do not represent an official voice of the neighborhood, nor, as I understand it, do they pretend to with any formal intent.

The informal secretary of our core group suffered a family loss and so we have been delayed pulling together our notes and contact information in a more timely manner, and for this I apologize. I would ask that you give us a few days to mobilize at which point we can set up a meeting with you. At that time, you can present the redesign you presented on Dec. 11, and you can hear from us the consensus that we have been charged with presenting to you. I'm fairly certain that I will be your contact person with this core group, if only due to precedent. But I expect the reigns of communication to be a bit tighter than previously, with less off-the-cuff opining on my part so as not to confuse the neighborhood's position.

Talk to you soon.

Bill Robertson

**6. Account to R. Fong & D. Hung of 12-14-10 neighborhood meeting and initial “core group” meeting; official consensus and names of “core group” submitted; scheduled meeting with A. Rosen for 1-9-11 mentioned.**

Date: Fri, 7 Jan 2011 18:52:30 -0800  
 Subject: 24th & T Street development (Z10-142) - neighborhood update  
 From: Bill Robertson <[w.preston.robertson@gmail.com](mailto:w.preston.robertson@gmail.com)>  
 To: "Robert K. Fong" <[rkfong@cityofsacramento.org](mailto:rkfong@cityofsacramento.org)>, David Hung <[dhung@cityofsacramento.org](mailto:dhung@cityofsacramento.org)>, Lisa Nava <[L.Nava@cityofsacramento.org](mailto:L.Nava@cityofsacramento.org)>  
 Cc: Alan LoFaso <[alofaso@sbcglobal.net](mailto:alofaso@sbcglobal.net)>, Christina Jewett <[christina.jewett@gmail.com](mailto:christina.jewett@gmail.com)>, Doug Morrow <[douglas.morrow@asm.ca.gov](mailto:douglas.morrow@asm.ca.gov)>, Ed Randolph <[efr3@yahoo.com](mailto:efr3@yahoo.com)>, Heather Scott <[Heather.Scott@sen.ca.gov](mailto:Heather.Scott@sen.ca.gov)>, Kelley Woodward <[2006sew@comcast.net](mailto:2006sew@comcast.net)>

Rob and David:

I trust you both had pleasant holidays. I want to bring you up to date on where things stand regarding Andrea Rosen's 24th & T Street 6-unit building (Z10-142) and the position of concerned neighbors in the Newton Booth and Poverty Ridge areas who have organized over the issue.

As you may recall, after the fairly passionate community meeting arranged by Rob in November, Ms. Rosen arranged an informal private meeting on Dec. 11 with select members of the neighborhood chosen by her, during which she presented a revised drawing of the building. It was a less volatile meeting than the larger community event, and in fact I, along with everyone present, I believe, felt it was even cordial in tone. At the same time, however, the neighbors present asserted to Ms. Rosen that they didn't consider themselves representative of the neighborhood and given that, she should not misconstrue the importance of the Dec. 11 meeting insofar as outreach to the neighborhood was concerned. She was told there would be an organizational meeting of concerned neighbors at my home on Dec. 14, during which a clearly defined consensus regarding her project was hoped to be established along with a more formalized process by which the neighborhood's consensus could be conveyed to Ms. Rosen. After that Dec. 14 meeting, she was told, we would consider good faith communications between the neighborhood at developer to have begun.

The meeting at my house on Dec. 14 was well attended, with a group of more than 30 people present. I was surprised and pleased at how orderly the meeting ran, given the passions expressed at the November event--and I was further impressed, if not amazed, at the consistency of opinions expressed with regard to the preferences for the development. Consensus was pretty instantaneous.

A core group of 8 representatives, comprised of both renters and homeowners, was created to communicate the neighborhood's preferences to Ms. Rosen and pursue good faith communication with her thereafter. The decision was made not to act under the aegis of the Newton Booth Neighborhood Association in our dealings with Ms. Rosen--not for any contrarian reason, but rather for the sake of clarity and decisiveness.

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There was some minor confusion between Ms. Rosen and myself in the days following the Dec. 14 meeting with regard to how soon a meeting with her could be arranged. Understandably, she was eager to move forward as soon as possible. And understandably the neighbors, despite their organizational accomplishment, were distracted with the usual activities of the holiday season. I believe you both received the exchange of emails I had with Ms. Rosen during this time, and a quick reading shows that the confusion was of no great consequence.

Now that we are into the New Year, I am pleased to relay that the core group of neighborhood representatives has been able to meet among themselves and that a meeting between that group and Ms. Rosen has been arranged for 2:00 pm this Sunday afternoon, Jan. 9, at the home of Heather Scott, a member of the group.

The core group of representatives was 8, but is now 7, as one of our members had to step down due to work conflicts. Here are the names:

Heather Scott <Heather.Scott@sen.ca.gov>  
Christina Jewett <christina.jewett@gmail.com>  
Alan LoFaso <alofaso@sbcglobal.net>  
Doug Morrow <douglas.morrow@asm.ca.gov>  
Kelley Woodward <2006sew@comcast.net>  
Ed Randolph <efr3@yahoo.com>  
Bill Robertson <w.preston.robertson@gmail.com>

The consensus of the neighborhood that we will be conveying to Ms. Rosen is as follows.

"We, the Concerned Neighbors of Newton Booth and Poverty Ridge, embrace and welcome the growth of a combined neighborhood that represents the very best of urban life--a neighborhood rich in diversity with regard to the culture and economic status of its residents, renter and homeowner alike, and with regard to the architectural history abundantly evidenced in its homes, whether grand or modest.

In the spirit of this, we would suggest that the development proposed for the joined parcels at 2331 T Street and 1918 24th Street at the northwest corner of 24th and T (known as Proposal Z10-142) would best serve the urban planning goals of both the City of Sacramento at large and the specifically affected neighborhoods of Newton Booth and Poverty Ridge, by adhering to the following stipulations:

1. A reasonable increase of density from that of the previous single-dweller, one-story home on .19325 acres, to three housing units.
2. An architectural style of good faith integrity that is appropriate to the immediate surrounding neighborhood within a 300 square foot radius--an area that includes, albeit not exclusively, Tudor cottages, Craftsman bungalows, and Depression-era brick duplexes.

3. Pitched roofs with a height not markedly greater than that of the Mirabella Apartments immediately adjacent to the site on T Street.
4. Setbacks that adhere to city requirements.
5. Porches that evoke an "eyes on the neighborhood" affect while still adhering to the aforementioned setback requirements.
6. Off-street parking accommodations provided for all units."

Rob and David--I realize that there is a striking disparity between Ms. Rosen's proposal and what is entailed above. But at the Dec. 14 meeting at my house, we calmly went around the room of 30-plus neighborhood residents in attendance and asked ourselves the question, "Ideally, what would you like to see?" The answers were consistent for renters and homeowners alike. Our simple goal in this first official meeting with Ms. Rosen is merely to present her with the neighborhood's ideal. It is not our intention to be unreasonable combative, only to represent.

At her informal gathering on Dec. 11, Ms. Rosen voluntarily stated to those of us invited that she was not willing to negotiate on the number of units. Since that gathering was not deemed by us to be a representational negotiation, we don't regard her remark as a fixed declaration of a consideration "not on the table," to use her vernacular. Still, she did say it and we are all aware that she said it.

I am hoping for a productive and reasonable exchange. I will let you know the outcome.

Sincerely,

Bill Robertson

--

William P. Robertson  
2009 23rd Street  
Sacramento, CA 95818  
916-607-2405

**6-a. Neighborhood consensus document as presented to A. Rosen on 1-9-11.**

We, the Concerned Neighbors of Newton Booth and Poverty Ridge, embrace and welcome the growth of a combined neighborhood that represents the very best of urban life--a neighborhood rich in diversity with regard to the culture and economic status of its residents, renter and homeowner alike, and with regard to the architectural history abundantly evidenced in its homes, whether grand or modest.

In the spirit of this, we would suggest that the development proposed for the joined parcels at 2331 T Street and 1918 24th Street at the northwest corner of 24th and T (known as Proposal Z10-142) would best serve the urban planning goals of both the City of Sacramento at large and the specifically affected neighborhoods of Newton Booth and Poverty Ridge, by adhering to the following stipulations:

1. A reasonable increase of density from that of the previous *single-dweller, one-story home* on .19325 acres, to *three housing units*.
2. An architectural style of good faith integrity that is appropriate to the immediate surrounding neighborhood within a 300 square foot radius--an area that includes, albeit not exclusively, Tudor cottages, Craftsman bungalows, and Depression-era brick duplexes.
3. Pitched roofs with a height not markedly greater than that of the Mirabella Apartments immediately adjacent to the site on T Street.
4. Setbacks that adhere to city requirements.
5. Porches that evoke an "eyes on the neighborhood" affect while still adhering to the aforementioned setback requirements.
6. Off-street parking accommodations provided for all units.

Delivered to Andrea Rosen, developer, on January 9, 2011, per neighborhood consensus taken December 14, 2010.

Representing the neighborhood:

Christina Jewett  
Alan LoFaso  
Doug Morrow  
Ed Randolph  
Bill Robertson  
Heather Scott  
Kelley Woodward

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**7. Account to R. Fong & D. Hung of 1-9-11 meeting between "core group" and A. Rosen: presentation to her of official neighborhood consensus and discussion.**

Date: Tue, 11 Jan 2011 16:39:55  
 Subject: Update on neighborhood meeting with 24th & T developer Jan. 9  
 From: Bill Robertson <[w.preston.robertson@gmail.com](mailto:w.preston.robertson@gmail.com)>  
 To: "Robert K. Fong" <[rkfong@cityofsacramento.org](mailto:rkfong@cityofsacramento.org)>, Lisa Nava <[LNava@cityofsacramento.org](mailto:LNava@cityofsacramento.org)>, David Hung <[dhung@cityofsacramento.org](mailto:dhung@cityofsacramento.org)>  
 Cc: Alan LoFaso <[alofaso@sbcglobal.net](mailto:alofaso@sbcglobal.net)>, Christina Jewett <[christina.jewett@gmail.com](mailto:christina.jewett@gmail.com)>, Doug Morrow <[douglas.morrow@asm.ca.gov](mailto:douglas.morrow@asm.ca.gov)>, Ed Randolph <[efr3@yahoo.com](mailto:efr3@yahoo.com)>, Heather Scott <[Heather.Scott@sen.ca.gov](mailto:Heather.Scott@sen.ca.gov)>, Kelley Woodward <[2006sew@comcast.net](mailto:2006sew@comcast.net)>

Rob and David:

I wanted to check in with a report on the first meeting between the core group of neighborhood representatives from the Newton Booth-Poverty Ridge areas, whose assemblage I reported to you in an earlier email, and Andrea Rosen, the developer of the proposed 6-unit apartment building at the corner of 24th & T Streets. It goes without saying that this is our particular take on what transpired and was communicated, and that Ms. Rosen will have her own interpretation. It would be wonderful if there were at least some similarities between our two accounts, given that common ground is so crucial to the commencement of good faith negotiation.

The meeting was held at the home of Heather Scott on Sunday, Jan. 9. The core group representatives present were:

Heather Scott <[Heather.Scott@sen.ca.gov](mailto:Heather.Scott@sen.ca.gov)>  
 Christina Jewett <[christina.jewett@gmail.com](mailto:christina.jewett@gmail.com)>  
 Alan LoFaso <[alofaso@sbcglobal.net](mailto:alofaso@sbcglobal.net)>  
 Doug Morrow <[douglas.morrow@asm.ca.gov](mailto:douglas.morrow@asm.ca.gov)>  
 Kelley Woodward <[2006sew@comcast.net](mailto:2006sew@comcast.net)>  
 Bill Robertson <[w.preston.robertson@gmail.com](mailto:w.preston.robertson@gmail.com)>

At the meeting, Ms. Rosen was presented with the document of neighborhood consensus I emailed to you both on Friday. That document remains unchanged, but for the sake of convenience, I am attaching it to this email as a .pdf file. Upon reading the document, Ms. Rosen said that she was "shocked" by the disparity between her position and ours. While this characterization of her emotional state was duly noted, we did not necessarily take the proclamation at face value given the neighborhood passions that were exhibited at the community meeting arranged by Rob in November, and given whatever psychological motivation one might intuit, rightly or wrongly, from her PRA request to the City.

That having been said, and to be fair, she did seem if not flummoxed, at least unprepared or simply unready to offer much in the way of a counter response. She focused on certain details of the document that she felt were "inaccurate" to a degree that required correction. One such detail was that the Zoning Administration ID of her proposal was not Z10-142, and that it had an entirely different ID in light of her

application for condominium status. Another detail she termed "inaccurate" was the document's assessment of previous lot density, given that the lot was comprised of two separate parcels. Neither of these points became subjects of debate, as the core group of representatives had discussed in advance a desire not to become mired in arguing minutiae, but rather to view the meeting as a starting point of basic positions and to try to stay focused on that objective.

One concern expressed by Ms. Rosen that seems reasonable is what the neighborhood meant by the term "three units." At first blush, we felt that "three units" should carry with it at least some general meaning from which a discussion might be launched, but given that issues like parking variances and garages are concerns for any developer, we conceded that clarity on this point was helpful to all. Of particular concern to Ms. Rosen was whether "three units" meant three free-standing structures or three units in a single building, and whether garages were understood inclusions with each unit or not, in which case, apparently, the neighborhood was expressing a preference for one house with two garages or two houses with one garage. We told Ms. Rosen that we would try to get a clearer definition of "three units" from the larger neighborhood and would report back to her.

At this point, Ms. Rosen said that she was not sure how to react to our proposal and would have to think about it. She made an off-the-cuff suggestion that Rob Fong should be brought in to mediate a negotiation between herself and the core group of neighborhood representatives, but the subject was not pursued. Ms. Rosen also referred, as she has in the past, to various Zoning Administration guidelines and urban planning principles that supported her desires for property that she rightfully noted she owned. She also referenced letters of support she had from urban planning organizations. And, as she had stated previously at the informal and non-representational get-together she held among a select group of neighbors chosen by her on Dec. 11, the financial reckoning of her project did not calculate to her benefit with "anything less" than what she was now proposing.

At this point, we attempted to clarify matters by giving our understanding of the process before us. We explained in various voices and with various articulations that we were not her business partners, nor were we zoning guideline experts or urban planning philosophers. The latter two subjects were of interest to us, of course, as they should be to any engaged citizen, but that our primary mission that day was to convey the consensus of the neighborhood as defined by the 30-plus people who had met privately on Dec. 14. (I used the phrase "we don't care" during my own remarks concerning her finances and the zoning legitimacy of her project. It was a histrionic and intentionally colloquial use of language, however, swiftly and rightfully refined by another member of the group.)

We expressed to Ms. Rosen our understanding of the type of situation we all found ourselves in and what we saw our roles to be. The "upside" of being a developer, we noted, was that she stood to make money from her project. However, it was noted, there were challenges to being a developer as well, and one of those was that while she

may own a property, and while city guidelines might allow development of that property, she must also work in good faith with an intangible known as "the neighborhood." And we were the neighborhood. The neighborhood we said, was comprised of residents who live in an area, who open their doors, and who then simply react to what they see. The developer must deal with the neighborhood's reaction or not at her peril. And the same, we noted, goes for the neighborhood, with respect to their dealings with the developer. This, we said, was called "negotiation." And that was what we hoped for.

At this point, the core group of representatives suggested a walk around the neighborhood to look at the architecture and the general neighborhood profile surrounding the proposed site of Ms. Rosen's development. We had discussed this in advance among ourselves because it seemed like a socially amiable, non-confrontational approach by which we might soft-sell our position to her with a firm reality. I can't say with any conviction that that our objective was successful and perhaps understandably so. We wanted to walk past single-family homes and low profile structures; she wanted to count utility meters and deduce density of multi-unit structures. We wanted to walk in residential areas; she preferred the more industrial landscape of S Street. None of this was contentious. It was mostly an issue of subtle steering of the group and visual and conversational focus. All-in-all, it seemed to produce nothing on which we could build future discussions.

Concluding our meeting, we emphasized to Ms. Rosen that the neighborhood's consensus had been presented that day in the spirit of negotiation, and that we hoped to hear a counter proposal from her. She asked whether such a counter might include "information," and by this we understood her to mean more information about zoning guidelines and urban planning principles. We told her that we didn't find that as useful to forward progress as we did more substantive changes in her proposal. We told her that we were not interested in being unreasonable, that we wanted to negotiate, and we expressed a general desire to "get to yes." We also again said we would try to extract from the neighbors what they meant by "three units."

All in all, I would characterize our meeting with Ms. Rosen as tense at moments, but overall not unfriendly. I should note, however, that no concession to us was given by her--even in the form of a noncommittal consent to negotiate. We did not schedule a second meeting with her--nor did either side suggest one. The only concrete step suggested by Ms. Rosen during our afternoon with her was idea that Rob might somehow act as a mediator between our two parties. I know from email exchanges with Lisa Nava that Ms. Rosen had made such a request previously--and even before our Jan. 9 meeting of Sunday. My recollection was that she was told she must first try to work things out with us herself. I don't think that has happened yet.

It is our hope that she will.

Cordially,

Bill Robertson

**8. A. Rosen response to neighborhood consensus.****Response to January 9, 2011 Stipulations Requested by the Concerned Neighbors of Newton Booth and Poverty Ridge from Andrea Rosen and Ben Rosen Regarding P10-089 24<sup>th</sup> and T Courtyard Condominiums**

In the spirit of good faith negotiation, I offer the following for your consideration and for further discussion. I appreciate the neighbors' recognition of the importance of diversity in urban neighborhoods, such as this one, in both culture and economic status of its residents, multi-family mixed in with single family and in architectural designs.

**STIPULATIONS WHICH HAVE BEEN MET OR DISCUSSED AS PART OF ALTERNATIVE DESIGNS****2. Architectural style of good faith integrity that is appropriate to the surrounding neighborhood.**

The current filed design is a contemporary take on many, very nice buildings in the neighborhood including ones not far from our parcel. It repeats the key theme of the largest developments on this half-block- the courtyard- and expands and celebrates this feature. Your stipulation recognizes the architectural diversity of the neighborhood which includes art deco, Moderne and prairie-style designs in addition to Tudor and Craftsman. The materials proposed in the filed design are stucco and brick which are common in this neighborhood as are many of the design features such as stoops. This project will enrich the existing architectural diversity of this neighborhood, however it's worth noting that design is very personal and subjective.

**3. Pitched roofs of height not markedly greater than that of the Mirabella.**

The current filed design does not feature a pitched roof and is a two story building 23' in height. There are many two-story houses and apartment buildings in the vicinity of this parcel in all directions. Two- storey was selected over the allowed 3 stories in order to minimize massing. See the Sutter Brownstones at 26<sup>th</sup> and N. The neighbors there worked with the architect and agreed that the best way to reduce massing was to go with a flat roof. Two storey was chosen order to allow open green space in the form of a courtyard for residents and large patios. I can't tell if this stipulation is requesting single story; but if so, it's not an option here. The courtyard will be behind a 4 foot wall which will allow passersby to enjoy it and the fountain. A pitched roof alternative design was presented to neighbors on December 11, 2011. On January 9<sup>th</sup>, I was told that the alternative designs were discussed at the Dec. 14, 2011 neighborhood meeting.

**4. Setbacks that adhere to city requirements.**

City required front and side setbacks have been met. The only other setback is for the accessory structure (garage) and a 4' side setback variance is requested in order to allow for 10' wide garages which will accommodate both a vehicle and a few bicycles. This is a limited variance that will run only for the length of the garage -17' out of the 160' length of 24<sup>th</sup> St side of the parcel.

**5. Porches that evoke "eyes on the neighborhood".**

Multi-family developments in this neighborhood typically have patios or private backyard spaces like the Mirabella has. They don't typically have porches since porches in the neighborhood are typically raised several feet and set back and up from the sidewalk giving the porch-sitter some privacy and separation from the sidewalk. Adding porches to this project was proposed as integral to the alternative design (not yet discussed). Porches at grade will need to carefully balance and realistically consider the user's feeling of security in order to be worthwhile in practice. Townhome owners report that more real "eyes on the street" results from windows from living spaces on the corners of buildings looking out on the street.

**Development Options for this Parcel to Discuss:**

Based on the current zoning ordinance which defines the rules for R-3A zoning and Sacramento General Plan designation of Traditional Neighborhood- Medium Density and based on what I can afford to build on this site, here are options to discuss:

A. Single building, 3 stories( 35 ft at point where roof starts), seven 2- bedroom units, all City setbacks met, seven parking spaces onsite accessed via the alley(5) and via double driveway off 24<sup>th</sup> St (2) . Pitched roof, stucco with brick accents. No courtyard. Common patio and walkways. Vintage Traditional design.

B. Two triplex buildings. 7 units total. 3 2-story buildings. 3 2-bedroom units in each two buildings with one 1-bedroom apartment over garage. Accessory structure with 5 garages; plus two on slab at grade parking spaces accessed via double driveway off 24<sup>th</sup> St. No courtyard. Limited open space. All city setbacks met. All parking onsite. 24<sup>th</sup> St parking spaces might be under buildings and those units might be one bedrooms as a result.

C. Two triplex buildings with 3 2-bedroom units. Six at grade on slab parking spaces (no garage) accessed via alley. All City setbacks met. Current filed design. Retains Courtyard.

D. Two triplex buildings, six units total. Five garage parking spaces; One parking space accessed off 24<sup>th</sup> St under north building; May result in 2 2-bedrooms and 1 1-bedroom in north building. All City Setbacks met; Smaller courtyard due to onsite 24<sup>th</sup> St. parking space. Curb cut on 24<sup>th</sup>.

E. Two triplex buildings; six units; 5 spaces in garage off alley; one space under North Bldg. Two 2- bedroom units in North building and one apartment over garage. Courtyard smaller (due to onsite parking off 24<sup>th</sup> St).

I offer these development options as ones that we could afford to build, that would meet within the City's existing zoning ordinance and General Plan designation and may meet the requested Stipulations regarding pitched roofs, onsite parking and City setbacks.

One idea that I would propose for discussion is that we consider petitioning the City for angled parking on this stretch of 24<sup>th</sup> Street. A Newton Booth property owner suggested this idea and noted that it has been tried in other parts of Newton Booth and midtown with success. It was

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suggested that the City traffic engineer be consulted as to which side of 24<sup>th</sup> St be converted as only one side is eligible. Angled parking increases the number of spaces possible and slows traffic. I am supportive of exploring this option.

I look forward to meeting soon to discuss these ideas and your concerns.

Signed,

Andrea Rosen and Ben Rosen

January 17, 2011

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**8-A. Account to R. Fong, L. Nava, & D. Hung re. "core group" follow-up request by H. Scott to A. Rosen for clarification of her consensus response & A. Rosen's reply; desire by "core group" for negotiation affirmed; request by "core group" to R. Fong for design & density workshop.**

Date: Tue, 25 Jan 2011 20:02:57 -0800  
 Subject: Revised: Update on 24th & T Street development -- Jan. 25  
 From: Bill Robertson <[w.preston.robertson@gmail.com](mailto:w.preston.robertson@gmail.com)>  
 To: David Hung <[dhung@cityofsacramento.org](mailto:dhung@cityofsacramento.org)>, Lisa Nava <[LNava@cityofsacramento.org](mailto:LNava@cityofsacramento.org)>, "Robert K. Fong" <[rkfong@cityofsacramento.org](mailto:rkfong@cityofsacramento.org)>  
 Cc: Alan LoFaso <[alofaso@sbcglobal.net](mailto:alofaso@sbcglobal.net)>, Christina Jewett <[christina.jewett@gmail.com](mailto:christina.jewett@gmail.com)>, Doug Morrow <[douglas.morrow@asm.ca.gov](mailto:douglas.morrow@asm.ca.gov)>, Heather Scott <[Heather.Scott@sen.ca.gov](mailto:Heather.Scott@sen.ca.gov)>, Kelley Woodward <[2006sew@comcast.net](mailto:2006sew@comcast.net)>

Rob, Lisa, and David:

I believe this email exchange was CC'd to Lisa last night, but to be safe, I thought I would cut-and-paste the exchange and send it to David and Rob as well in the hope of creating an ongoing account of events.

As you may remember, a neighborhood letter of neighborhood consensus was presented to developer Rosen in the hope of engaging in good faith negotiations over her proposed 6-unit building. After meeting with her, she sent a response to our letter and I forwarded it on to you without commentary because our core group of neighborhood representatives had not yet met to discuss it and form an official position. We have now met.

The core group of neighborhood representatives was concerned that Ms. Rosen's official point-by-point numbered response to us began with **number 2** and did not pointedly address the neighborhood's number one issue, both numerically and literally, which was: "a reasonable increase of density from that of the previous single-dweller, one-story home on .19325 acres, to three housing units." While trying to juggle schedules to meet with Ms. Rosen, we sent an email via core group member Heather Scott, requesting her to directly address the issue in writing.

Below is that exchange. I will continue with my commentary following it.

>>>>Hello Andrea-

Our group is happy to meet with you again to discuss issues related to the 24th and T proposed project, however, with respect to the process, we feel that you need to address one of our primary concerns that this document does not address before we can proceed.

You seem to have omitted a response to issue number one: a reasonable increase of density from that of the previous single-dweller, one-story home on .19325 acres, to three housing units.

Please amend your attached original document then we can reschedule a time to talk.

Thank you,  
Heather Scott<<

>>Heather

I thought that we were trying to have a dialogue- talking together in a group- and that's what I am committed to do. I offered a written response this time because I got a sense that the group at our first meeting that the group expected a written response. I believe the group, especially Doug, acknowledged early in the meeting that the number of units for the parcel is the toughest nut to crack. And I was expecting we would try to address this question head on

I apologize if my more lengthy section on Development Options for this Parcel to discuss: copied below was not sufficiently clear. This section was in response to Requested Stipulation #1; please see now bolded sentence from our response below.

As Bill mentioned either at the most recent meeting or earlier, I did explain at the December 11, 2010 meeting that my proposal is for six units as I cannot afford to build fewer than that number of units and build a high quality project which is my commitment to the neighborhood and myself and the community at large. It's very important to me to build buildings that I can be proud of and that make a positive contribution to the neighborhood and the City. For these and other reasons, our proposal remains at six units.

Six units on 8500 s.f. of land matches the Mirabella ( expanded most recently in the 1950's), the Tudor apartments and the other land use of parcels on this half- block. In other words, our proposed use of this double parcel is entirely consistently with the historic land use on this half-block. I have no explanation as to why there was a single house on one of the two parcels but the City has never adopted single family residential for that parcel in spite of the fact that someone chose to build a single family house on that parcel in 1940. The Mirabella apartments has already been built by 1940 at the time that the former house on this parcel was built.

Lastly, as I suggested earlier, I urge you to touch base with David Hung regarding the last possible date he has given my project to submit changes to the City. My hearing date is now set for March 10, 2011 and last Friday David informed me that he must have everything finalized by February 10 for my project. You don't have to take my word for it; contact him.

If these negotiations are going to produce anything in the way of changes to the project, we don't have much time left. I am committed to meeting to try to work something out, but we've got to keep moving forward.

From our written response to the group:

**Development Options for this Parcel to Discuss:**

Based on the current zoning ordinance which defines the rules for R-3A zoning and Sacramento General Plan designation of Traditional Neighborhood- Medium Density and **based on what I can afford to build on this site**, here are options to discuss:

A. Single building, 3 stories( 35 ft at point where roof starts), seven 2- bedroom units, all City setbacks met, seven parking spaces onsite accessed via the alley(5) and via double driveway off 24th St (2) . Pitched roof, stucco with brick accents. No courtyard. Common patio and walkways. Vintage Traditional design.

B. Two triplex buildings. 7 units total. 3 2-story buildings. 3 2-bedroom units in each two buildings with one 1-bedroom apartment over garage. Accessory structure with 5 garages; plus two on slab at grade parking spaces accessed via double driveway off 24th St. No courtyard. Limited open space. All city setbacks met. All parking onsite. 24th St parking spaces might be under buildings and those units might be one bedrooms as a result.

C. Two triplex buildings with 3 2-bedroom units. Six at grade on slab parking spaces (no garage) accessed via alley. All City setbacks met. Current filed design. Retains Courtyard.

D. Two triplex buildings, six units total. Five garage parking spaces; One parking space accessed off 24th St under north building; May result in 2 2-bedrooms and 1 1-bedroom in north building.

All City Setbacks met; Smaller courtyard due to onsite 24th St. parking space. Curb cut on 24th.

E. Two triplex buildings; six units; 5 spaces in garage off alley; one space under North Bldg. Two 2- bedroom units in North building and one apartment over garage. Courtyard smaller (due to onsite parking off 24th St).

**I offer these development options as ones that we could afford to build, that would meet within the City's existing zoning ordinance and General Plan designation and may meet the requested Stipulations regarding pitched roofs, onsite parking and City setbacks.**

I look forward to hearing from you soon.

**Andrea Rosen**  
 (916) 457-6721  
[andrearosen@sbcglobal.net](mailto:andrearosen@sbcglobal.net)<<<<<<

Rob, Lisa and David, we seem to be at an unfortunate impasse here in our negotiations with the developer of this project--if indeed what has transpired thus far could be considered "negotiation." My own understanding, and that of my fellow neighborhood representatives, is that negotiations are supposed to be an exchange of proposals and counter proposals that build to a common compromise. This was always our intention. "Three units" was our ideal, just as "six units" was developer Rosen's ideal. We accepted and embraced the idea that common ground had to be found--we still do.

But we have been unable to get developer Rosen to respect and consider our ideal of "three units," so that we can all proceed toward a just and reasonable resolution. Instead, what we have been subjected to is a continued assertion that she is right and we are wrong. When parties counter each other with the exact same position previously stated, offering nothing but variations and "new information" to support their points, this is not, to our understanding, "negotiation." It is simple debate. Debate is a competition

eventually decided on by panel of judges. Debate is not a process in which "both parties win," which is the true objective of negotiation as we understand it. We want both parties to win. We want to negotiate.

It is correct that developer Rosen, in an obvious proactive maneuver, told the group of hand-picked neighbors with whom she met with on Dec.11 at the Vizcaya House that "anything less than 6 units was not up for discussion," but as that meeting was not representational, but rather an informal gathering of neighborhood individuals chosen by her, we did not consider that statement the beginning of any sort of "negotiation." We told her to allow us to establish a process of communication and negotiation, and for her to respect that process. I think it is fair to say that she has not respected the process. Indeed, she has even taunted us with the absurd suggestion that we consider "7 units."

As I said, our meetings with developer Rosen have been structurally no more forward-moving than a debate. In our case, the aforementioned "panel of judges" who will determine the winner of this debate is in Ms. Rosen's mind apparently the City Planning Board--and if the debate continues beyond that, the City Council. I can't help but feel that in these very challenging times for government, the City has larger issues to manage than a combative situation between a development naif and the neighborhood on whose metaphorical foyer rug she has tracked something unwanted. So let me reiterate:

We want to negotiate.

As a neighborhood, the NewtonBooth/Poverty Ridge area has not had a lot of experience dealing with this sort of controversy. But there is one thing on which we all agree, particularly in light of the passions present at Rob's community meeting last November: We know that we do not want to be a shrill, unreasonable group who are stridently resistant of any and all change in their neighborhood. The "not in my backyard" psychopathology that is so frequently demonstrated in other communities is nothing we wish to emulate. We know that Newton Booth/Poverty Ridge is a city neighborhood and not a suburban one. We are aware that a city must grow and be ever-changing if it is to thrive. We understand and we welcome that reality. It's why we live here.

Common ground is a beautiful thing, I think. Both in a city and in negotiations.

Please consider this the first of what will no doubt be other requests, including one with a lot of signatures from the neighborhood, for a design and density workshop to handle this impasse with the developer of the 24th & T Street proposal.

Cordially,

Bill Robertson

**9. Request by A. LoFaso to R. Fong for design & density workshop.**

Date: Wed, 2 Feb 2011 14:05:15 -0800 (PST)  
Subject: 24th & T Street Project - Thank you - and request  
From: Alan LoFaso <alofaso@sbcglobal.net>  
To: rkfong@cityofsacramento.org  
Cc: L.Nava@cityofsacramento.org, dhung@cityofsacramento.org,  
Bill Robertson <w.preston.robertson@gmail.com>,  
Heather Scott <Heather.Scott@sen.ca.gov>,  
Doug Morrow <douglas.s.morrow@gmail.com>,  
Kelly Woodward <2006sew@camcast.net>,  
Christina Jewett <christina.jewett@gmail.com>

Council Member Fong,

I would like to belatedly thank you for hosting the November 18th community meeting regarding the proposed 6-unit project at the north-western corner of the intersection of 24th and T Streets (No. P10-089). As I am sure you appreciated, there are strong feelings in our neighborhood regarding the historic integrity and architectural authenticity of the Newton Booth/Poverty Ridge area. Many residents have misgivings regarding poorly conceived developments from decades past, and many place great weight on the value of preserving period homes to the greatest extent possible. I associate myself with those views.

At the outset, I believe the community meeting allowed residents to voice their frustration with the unnoticed demolition of the historic home previously located on the corner lot. Although not entirely satisfying, the clear explanation by city staff, in my view, allowed neighbors to move on from that issue to what faces us now-- development of the now vacant lot. Moreover, I believe the meeting helped give focus to neighbors' anxiety regarding poorly communicated intentions of the developer by making the city planning process and resources more accessible to those not familiar with the workings of City Hall.

As I know you're also aware, my neighbors and I are not opposed to development on the now vacant lot. In fact, the neighbors have offered to discuss with the developer a proposal focusing on a 3-unit development with a mass and scale more consistent with the immediately surrounding architecture. While there are many differences in details and emphasis, there is close to consensus among the neighbors regarding the appropriate size of the development.

Efforts to work positively with the developer have not been successful, as the developer has shown no willingness to discuss the 6-unit mass/scale or any inclination to revise the project along those lines. Most neighbors understand that a fair negotiation between neighbors and developer is likely to result in a project that will not conform to our ideal 3-unit suggestion. However, if there is no dialogue, we cannot arrive at a fair and reasonable result for all.

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My neighbors and I have recently become aware of the idea of a design and density workshop to enable neighbors to express their concerns and offer constructive suggestions to inform the city planning process of alternatives supported by the neighborhood. Given the lack of constructive engagement by the developer, I join my neighbors in requesting that the city conduct such a workshop for this particular project. I believe it would be a productive use of this process to give positive, focused input into the planning process regarding this project.

Again, thank you for your actions in support of the Newton Booth/Poverty Ridge neighborhood and, in advance, for your consideration of my request.

Regards,

Alan LoFaso  
2001 24th Street

**10. Account to R. Fong & L. Nava re. A. Rosen email finding fault with "core group" for terminating communications; explanation to Fong & Nava of "core group" position; request for mediation by R. Fong.**

Date: Wed, 9 Feb 2011 13:24:03 -0800  
Subject: 24th & T Street development - request for mediation  
From: Bill Robertson <w.preston.robertson@gmail.com>  
To: "Robert K. Fong" <rkfong@cityofsacramento.org>, Lisa Nava <LNava@cityofsacramento.org>  
Cc: David Hung <dhung@cityofsacramento.org>, Luis Sanchez <lsanchez@cityofsacramento.org>, "Alan LoFaso" <alofaso@sbcglobal.net>, "Christina Jewett" <christina.jewett@gmail.com>, "Doug Morrow" <douglas.morrow@asm.ca.gov>, "Heather Scot" <Heather.Scott@sen.ca.gov>, "Kelley Woodward" <2006sew@comcast.net>

Rob and Lisa--

We have received the following email from Andrea Rosen. After it I will make comments.

>>Heather -

As the designated liaison for the neighborhood group, through this communication to you, I am asking the group if they would like to meet in the next week or two to discuss the design of the 24th and T Courtyard Condominiums. I have made some changes and am offering to meet with neighbors to present them and get feedback from you.

It is my impression that discussions were prematurely and unilaterally terminated by the group as I have not received any emails or phone calls since you told me that you were canceling our last scheduled meeting which had been set for 7:15pm on January 26, 2011.

Please let me know asap as I know how challenging it is to accommodate folks' schedules.

I believe it is important to keep the lines of communications open and have been endeavoring to do that.

Thanks.

Andrea Rosen  
(916) 457-6721  
andrearosen@sbcglobal.net<<

Our neighborhood core group representative Heather Scott will respond and accept her offer to meet. We will do so despite our understanding that her application deadline is February 10 and no practical input is being sought from us.

It should be noted that Ms. Rosen's remark about our group "prematurely and unilaterally" terminating discussions is a fairly obvious and artless attempt on her part to cast our neighborhood as uncooperative. I should note that she uses the word "discussions" and not "negotiations". "Discussions" and "communications" are, of course noble things. "Negotiations", however are even nobler.

What Ms. Rosen calls "discussion" and "communication" in actuality has been little more than a repetition by her to us that a.) she is in the right and our neighborhood is in the wrong and b.) she will not discuss with us or communicate about those issues that are of the greatest concern to the neighborhood. A lack of interest in subjecting oneself to repeated condescension and insult can perhaps accurately be termed "unilateral" termination in a court of facts, but it is puerile on a human level. We are fully aware that developer Rosen wanted to continue her condescension and insult, we just felt it was...let me find a word here...unilateral in its benefit.

As previously stated, our neighborhood wanted to discuss and negotiate the number of units, which we felt had a direct relationship on the scale and design. She refused our overtures to even discuss the issue and indeed countered with suggestions in writing that she make the project larger and taller.

I spoke with the always very helpful David Hung this morning at some length and was told that the Zoning Administration has declined our neighborhood request for a Design and Density Workshop to facilitate between neighborhood and developer what we had hoped simple maturity and adult respect would have generated but did not. We are, it seems, left to lobby in advance and then present our positions to the Planning board and, beyond that, the City Council. We are more than willing to do this, but it seems needlessly contentious and a colossal waste of time on everyone's part. Mr. Hung suggested that we might request that Rob serve the function of mediator to a negotiation much in the way that Councilman Steve Cohn mediated a recent controversy in his neighborhood.

I am happy to make that request. Indeed developer Rosen mused about the prospect of such a process herself at one point.

That having been said, I wish to express my great disappointment that increasingly the City Council is being called upon to do a job that other areas of government have been budgeted to handle, but wish not to.

Part of this is, I must confess, is very personal to me, and here I am speaking solely for myself and not the other members of our core group.

As you know, my wife is Claire Pomeroy, Vice Chancellor of Human Health Science and Dean of the School of Medicine at UC Davis. She is following this development with great concern and is not happy about Ms. Rosen's behavior. She is more than willing to become involved as a resident of the neighborhood, but she is also very busy with enormous responsibilities barely imaginable to Ms. Rosen. I confess I resent having to

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further waste my wife's precious time to deal with an issue that we pay taxes to have the City handle with greater competence than I have seen to this point.

Rob himself made a comment addressing this problem during the last City Council meeting--and I couldn't agree more. This should not be the role of the City Council, nor should unreasonable developers with small neighborhood-oriented projects along with the Zoning Administration conduct themselves as though it were. If the City of Sacramento is ever to have a quality of greatness to it, we should stop treating the City Council as though it were a neighborhood association.

Until such greatness arrives, however, I must humbly and regretfully request of Rob mediation in this matter.

Sincerely,

Bill Robertson

--

William P. Robertson  
2009 23rd Street  
Sacramento, CA 95818  
916-607-2405

October 4, 2010

Mr. David Hung  
Associate Planner  
Community Development Department  
300 Richards Boulevard, 3rd Floor  
Sacramento, CA 95811

Dear Mr. Hung:

As a resident of the Newton Booth neighborhood in Midtown Sacramento, I am writing to express my concerns with the Courtyard Housing project being considered for the corner of 24<sup>th</sup> Street at T Street.

My first concern is with increasing the number of apartment units in proportion to single family homes and the corresponding number of designated parking spaces for the apartments. When I learned of the plan to build more apartments in this neighborhood, I surveyed the number of units currently in existence in the immediate vicinity of the property under discussion. From what I could determine, the results are as follows:

- The 2300 block of T Street has 30 apartment units and 18 dedicated parking spaces or garages.
- The 2400 block of T Street has 48 apartment units and 23 dedicated parking spaces or garages.
- 24<sup>th</sup> Street between T Street and S Street has 13 apartment units and 6 dedicated parking spaces or garages.
- Thus, the total number of apartment units on just these three blocks is 91, with only 47 designated parking spaces and garages.
- On these same three blocks, there are currently 17 single family homes.

In looking at the plans for the 24<sup>th</sup> & T Courtyard Housing project, I see that a total of six apartment units are proposed with five corresponding detached garages. Given the square footage of the intended units, there is occupancy for easily ten to twelve residents with a potentially equal number of cars. As the plan only includes five garages, this development could put another five to seven cars on the street.

As a result of the already large number of apartments and roughly half as much parking, the streets are often full of the parked cars of residents and can not accommodate visitors. As I live on a corner lot, I can honestly say that most nights I have two cars parked in front of my house and three cars parked alongside of my house. Particularly on weekend nights, the cars come and go quite frequently, which is noisy and a nuisance to the peace of the neighborhood. Further, there are always vacancies in these apartments, as indicated by the constant presence of the "For Rent" signs. Parking would be even more challenging if every apartment were continuously occupied.

My next concern with the Courtyard Housing project is the design and architecture of the proposed units. Newton Booth and Poverty Ridge are neighborhoods dominated by early 20<sup>th</sup> century cottage style homes and bungalows; many with wide porches, basements, and peaked roofs. The style of the apartments is purported to be, "flat-roofed buildings...that echo the Art Deco era buildings of the neighborhood..." Yet, the drawings included with the Development Project Routing Form indicate that the buildings will more closely resemble the modern architecture reflected in the Whiskey Flats condos located at 22<sup>nd</sup> and S Street.

I am also troubled by the speed at which this project seems to be progressing. I walk by the property everyday on my way home from work, and I was surprised that over the course of three days the existing home was demolished, the basement and foundation dug out, the debris removed, and the chain link fence erected. While I do agree that the single family home that was demolished was neglected and the property unkempt, I do think it could have been rehabbed and resold. Many homes in the area have undergone such transformations, as evidenced by the recently remodeled property at 1915 24<sup>th</sup> Street that sold a short time after it was put up for sale.

Finally, I am concerned with tipping the balance of this neighborhood more in the direction of renters and away from property owner/residents. I am proud to know so many of my neighbors and call them friends. I can honestly say that the homeowners of this area are all familiar with each other and frequently stop to with each other. Not surprisingly, this project has been a considerable topic amongst many of us. The spirit of this community seems like it would be jeopardized by a greater influx of renters that are often temporary residents that have little or no stake in the long term health and development of Newton Booth. I believe the community would be better served by building one or two single family homes on this site, providing another anchoring property to this region and contributing to the neighborhoods' stability.

Thank you for your consideration of my concerns regarding this project. I am happy to further discuss my concerns and suggestions with the City of Sacramento or the Planning Commission if they so desire. I would also ask that you include me as part of your distribution list for any changes to or development of this plan in the future.

Sincerely,

Heather C. Scott  
2430 T Street  
Sacramento, CA 95816

**David Hung**

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**From:** Scott, Heather [Heather.Scott@sen.ca.gov]  
**Sent:** Friday, February 25, 2011 12:22 PM  
**To:** David Hung  
**Cc:** Lisa Nava; Robert King Fong  
**Subject:** Concerns with project P10-089

Dear Mr. Hung-

As a resident of the Newton Booth neighborhood in Midtown Sacramento, I am writing to express my concerns with the Courtyard Condominium project being considered for the corner of 24<sup>th</sup> Street at T Street (P10-089).

My first and primary concern with this project is the number of units. The merged lots that this project may be built upon initially had one single family home before the developer had it demolished. The plans for the 24<sup>th</sup> & T project propose six condominium units with five corresponding detached garages. Given the square footage of the intended units, there is occupancy for easily ten to twelve residents with a potentially equal number of cars. As the plan only includes five garages, this development could easily put another five to seven cars on the street.

To be clear, I do support building more housing in this space; but I would argue that a reasonable increase in density, say to that of three single family homes with a corresponding number of garages, keeps with the vibrant and mixed nature of the neighborhood.

My second concern is that proposed design of the condos is in too far contrast to the immediate surrounding structures and is not complementary. Newton Booth and Poverty Ridge are known for their high water bungalows, cottages, and Tudor style homes. These architectural styles also have components that make the neighborhood safer and more welcoming than the proposed structure. Porches and front yard space give neighbors more opportunity to interact with each other and create a more secure environment. The development instead closes off the residents from the rest of the neighborhood with its walled in courtyard and lack of porches.

My final concern is with the lack of meaningful discourse between the developer and the neighborhood. As a neighbor that has been involved with this project since September 2010, I feel that the many discussions have yielded little in the way of compromise from the developer. This is especially troubling given that the developer does not live, nor intend to live, in this neighborhood and that it is investment property instead. As residents and owners we do have to live with what the city decides is appropriate for this parcel and the consequences of those decisions.

As one of the core group of neighbors that has been working with Ms. Rosen, I feel that we have tried to be clear, rational, and flexible with our requests. I feel that we have been open and tried to work within the process that the city prescribes. I do not feel that we received reciprocal consideration from the developer about our concerns.

Thank you for your consideration of these comments and for your work and helpfulness.

Sincerely,

Heather Scott  
2430 T Street

Sacramento, Ca 95816

**David Hung**

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**From:** Mabel Lee Robbins [mlrobbin@pacbell.net]  
**Sent:** Thursday, October 07, 2010 2:17 PM  
**To:** David Hung  
**Subject:** Property: 2231 T Street & 1918 24th Street  
**Attachments:** mlrobbin.vcf

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Mr. Hung,

How will the parking to be handled for this new apartment complex? I live at 2010 24th Street, and I am concern about the parking situation in this neighborhood already. It seems that with the inclusion of six additional apartment units, there will be a need for at least six additional parking spaces, and more than likely, a total of twelve spaces (2 per unit) to this neighborhood. It's getting very crowded, and I am very concern about the parking situation in this area.

How many spaces will be built into the apartment unit or will this be street parking only? We already have multiple apartment units in the immediate area, and with the addition of six more units, I feel that the apartment complex needs to include enough parking spaces for its tenants before approving the the building permit.

Thank you for your assistance.

Respectfully,  
Mabel Lee Robbins  
916.453.0680

**David Hung**

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**From:** Mabel Lee Robbins [mlrobbin@pacbell.net]  
**Sent:** Monday, February 28, 2011 11:58 AM  
**To:** David Hung  
**Cc:** Robert King Fong; Lisa Nava; 2006sew@comcast.net  
**Subject:** RE: PROPOSED PROPERTY, "24TH & T STREETS COURTYARD HOUSING PROJECT" (P10-089)  
**Attachments:** mlrobbin.vcf

Hi Mr. Hung,

My apology for the delay in submitting my email ... if possible, can this email be included.

I am Mabel Lee Robbins, located at 2010 24th Street (24th between T & U Streets), and I have some comments in regards to the staff report to the Planning Commission proposed "24th and T Streets Courtyard Housing Project" (P10-089). The project proposes a **six** two-story condominiums with **five** garages with a International/Moderne style of architecture in a predominantly single family home neighborhood.

I am concern that the new development will change the dynamics of the neighborhood. I have been living in this neighborhood for over 27 years and it has remained relatively the same, except for increased traffic. With the inclusion of an additional six unit condos, plus 12+ additional people with cars, it will heavily impact the parking situation; especially on our block that has a very narrow width for cars and bicyclists. Aside from the parking situation, six condo units crammed in to a small space will not be esthetically pleasing, much less reflective of the neighborhood.

Please reconsider the development of this proposed Courtyard Housing Project. I agree with the Neighborhood Core Group that the property will provide for a more appropriate placement of:

Three residential housing units with adequate size garages, pitched roofs, porches (insert # of sq ft divided by 3 times 80% density formula). There are a number of appropriate examples in the immediate neighborhood (1, 2, and 3 unit). There are no flat-roof examples that would be appropriate for this proposed project.

Architectural design (California Craftsman style bungalow; brick style; or Tudor style) There are a number of single family homes, duplex, and tri-plex structures in the immediate neighborhood. I can provide current photos of the structures in the immediate neighborhood if needed for design examples.

Height of structures: No more than 2 stories. There are no 3-story housing structures in the immediate neighborhood.

Please reconsider the proposed "24th & T Streets Courtyard Housing Project" (P10-089). Your assistance is greatly appreciated.

Have a good day.  
 Mabel Lee Robbins  
 2010 24th Street  
 Sacramento, CA 95818  
 916.453.0680

David Hung  
Associate Planner  
Community Development Department  
300 Richards Boulevard, 3rd Floor  
Sacramento, CA 95811

Oct. 15, 2010

Dear David –

Greetings. My fiancé and I are new homeowners on the 2400 block of T Street. We made an offer on our home, a high-water bungalow, the first day it was offered for sale this spring. We loved the big, open mahogany porch that's perfect for a swing and rocking chairs. We were not excited about the amount of apartment units on the block because we knew from firsthand experience that renters have no incentive to protect and preserve the virtues of a neighborhood. However, we overlooked that concern and are proud owners of our home.

We are displeased to learn about the plans for courtyard housing at 24<sup>th</sup> and T Streets. For one, the configuration of the apartments is oriented away from the sidewalk and the street and toward the courtyard, as the name implies. The development is planned to be gated. Both of these attributes reduce the vitality, energy and vigilance on the street, thus reducing the crime deterrent effect that eyes on the street can have. The gates, like bars on a window, imply that there is a security threat in the neighborhood. As any student of the "broken windows" theory knows, such an implication can be a self-fulfilling prophecy and erode the safety and security of a neighborhood.

My concerns about crime are not without backing. There was a drug-motivated home-invasion robbery at 26<sup>th</sup> and T Street just weeks ago. A quick look at crime statistics shows that auto burglaries and vandalism are common. Plans for a gated, insular fortress will do little to enhance the security of the surrounding area.

My concern is also that the area directly surrounding the proposed project is at a tipping point where the number of apartment-filled lots is on the verge of eclipsing the family atmosphere of the area. I bought this home hoping to raise children here. I believed that the "story" of Midtown's historic areas was that too many apartments were built in the 70s. The ideal scenario is to preserve the historic charm of Arts and Crafts and Victorian homes that were picked up from trains on R Street by horse and buggy. I regret to see a project that packs too many units in too small of a space, further upsetting the delicate balance that separates a stable, historic neighborhood from a transient and forgettable one.

Despite these concerns, I want to be clear that I am in favor of multi-family development and housing in close proximity to light rail. I am keenly aware and pleased that the R Street corridor is zoned for multi-family units. I believe that there is no better place in the neighborhood for apartments and condos and stand behind plans for such development and investment along that corridor.

However, my position remains that T Street is the place for single-family homes or at minimum, development in the spirit of Metro Square that conforms and blends with a historic neighborhood like Newton Booth or Poverty Ridge. I think a development that keeps eyes on the street and preserves the character of the bungalow-style, Tudor and Victorian homes are best for the neighborhood.

I respectfully ask that no more than four units are approved for the site at 24<sup>th</sup> and T Streets. I think it would be in the best interest of the developer and the neighbors if the units are built to look like two single-family homes that are oriented to the street with a lawn or fountain or porch facing the neighbors. Orientation toward a private, gated courtyard turns a blind eye to the assets of the area and more resembles the ugly 'courtyard' building on the north side of T Street between 24<sup>th</sup> and 25<sup>th</sup> Streets.

Such a compromise would also maintain the appearance of a neighborhood with an equitable balance of single-family homes and apartments while still allowing the developer to reap the financial rewards of upscale multi-family units.

Sincerely,

Christina Jewett and Floyd Marvin

DRAFT – Started October 24, 2010

**“GOOD NEIGHBORS” vs. “EARLY NOTICE OF PLANNING APPLICATION (z10-142)”**

**CONCERNS:**

**1. PRESERVATION OF THE NEIGHBORHOOD**

- a. *“Good Neighbors” maintain their properties to reduce deterioration of good structures and landscaping.*
- b. *“Good Neighbors” watch out for other neighbors.*
- c. *“Good Neighbors” keep their properties clean and safe with no sidewalk/gutter hazards.*
- d. *“Good Neighbors” follow the City of Sacramento’s regulations regarding parking, noise, loitering, litterbugging, etc.*
- e. *“Good Neighbors” know the history of this historic neighborhood and fight to preserve it.*
- f. *“Good Neighbors” reduce trash/glass and other hazards that have been deposited on the sidewalks and front yards by “commuters” in order to facilitate reasonable accommodation for the elderly and disabled in the neighborhood*
- g. *“Good Neighbors” do not Graffiti or do other minor vandalations on neighbors’ properties.*
- h. *“Good Neighbors” try to minimize the amounts of pet excrements deposited on other neighbors’ lawns.*

**2. GENERAL SAFETY OF THE NEIGHBORHOOD**

- a. *During the weekdays*
- b. *During the weekends*
- c. *At night*

**3. PEOPLE CONGESTION:**

- a. *There are 156 multi-family units currently in the immediate neighborhood bounded by S/T STREETS AND 23<sup>RD</sup>/25<sup>TH</sup> STREETS*
  - i. **THERE ARE APPROXIMATELY 50 “COMMUTERS” THAT ACTUALLY ROAM THE STREETS AND LIVE IN THE ALLEYS OR WHEREEVER THEY CAN FIND SHELTER.**
  - ii. **FOR APPROXIMATELY 400 NEIGHBORS IN THE 156 MULTI-FAMILY UNITS**
  - iii. **THERE ARE APPROXIMATELY 100 ON-FOOT “COMMUTERS” THAT PASS THROUGH THE NEIGHBORHOOD DAILY**
  - iv. **THERE ARE APPROXIMATELY 150 VEHICLES THAT PASS THROUGH THE NEIGHBORHOOD DAILY**

**4. PARKING and TRAFFIC DISRUPTIONS**

- a. *There are approximately 400 neighbors that live in the multi-family units.*

- b. *There are approximately 80 on-site parking spaces for the 156 multi-family units.*
- c. *The lightrail station at 24<sup>th</sup> & R Streets does not provide adequate parking beside the tracks for commuters; there are no parking spaces designated for the disabled.*
- d. *The City of Sacramento's garbage collectors and street sweepers/cleaners and garbage collectors use the alley and find it difficult to do a good job because of vehicle and "commuter" obstructions.*
- e. *The City of Sacramento's garbage collectors and street sweepers find it difficult to do a complete job on the streets when vehicles are parked too close to and on top of trash piles.*

**5. PROPOSED ARCHITECTURE**

- a. *The proposed architectural style is not compatible with this neighborhood of Arts & Crafts Style, Art Deco/Moderne style, brick cottage style, and other styles for the period 1900 – 1930. The "Application of Intent" does not agree with the plans (proposed architecture).*
- b. *The proposed structures do not appear to be the most energy efficient (e.g. Flat roofs are not as energy efficient as sloped roofs.)*
- c. *The proposed garages are not big enough for a standard sized vehicle. The proposed garages do not have storage considerations.*

**6. SIDEWALK/GUTTER/STREET HAZARDS CURRENTLY TAKEN CARE OF BY "GOOD NEIGHBORS"**

- a. *Uncollected trash*
- b. *Glass/trash deposits.*
- c. *Graffiti*
- d. *Other minor vandalations not reported or corrected by the City of Sacramento*

**7. REASONABLE ACCOMMODATION CONSIDERATIONS FOR ELDERLY AND DISABLED NEIGHBORS**

- a. *Wheelchair hazards*
- b. *Walking hazards*
- c. *Transportation to hearings, etc.*
- d. *Notifications regarding re-zoning, demolitions, etc.*

**8. CITY OF SACRAMENTO'S RESPONSIBILITIES**

- a. **SAFETY**
- b. **LIGHTRAIL (transportation and parking)**
- c. **STREETS & ALLEYS; STREETLIGHTS**
- d. **SEWAGE; GARBAGE; RECYCLING; YARD TRASH**
- e. **STREET CLEANING; STREET TRASH**

**9. DISTRICT 4'S RESPONSIBILITIES TO "GOOD NEIGHBORS"**

- a. **NEIGHBORHOOD PRESERVATION**
- b. **NEIGHBORHOOD SAFETY**

- c. **NEIGHBORHOOD ISSUES LIKE PARKING AND TRAFFIC**
- d. **NEIGHBORHOOD MAINTENANCE & UPKEEP**
- e. **NEIGHBORHOOD REPAIRS**

**ALTERNATIVES:**

1. **Two single family units with angled roofs:**
  - a. **1918 24<sup>th</sup> Street – A two bedroom/two bath brick cottage-style home with a full garage (including storage space) on alley.**
  - b. **2331 24<sup>th</sup> Street – A three bedroom/two bath Art Deco/Moderne style home with a two-car garage and driveway on 24<sup>th</sup> Street.**
2. **Four multi-family units with angled roofs:**
  - a. **Each unit with two bedrooms/two baths**
  - b. **Four full garages with storage space on alley**
  - c. **Arts & Crafts style; Art Deco/Moderne Style; or brick cottage style.**
3. **Sell the properties to “good neighbors” at no more than what was paid for it in June (?). “Good Neighbors” will do the right thing for the neighborhood.**

**ABOUT 2331 T STREET/1918 TWENTY FOURTH STREET (APN010-0036-011-0000 and 010-0036-012-0000)**

The unique Art Deco/Moderne home was built in the 1940s on a double lot (2331 T Street/1918 Twenty Fourth Street). The home had a detached garage and a very large back yard on 24<sup>th</sup> Street to the alley. A fence ran from the garage to and up the alley.

The first home owner (1944/45) was Chester Dong (meat cutter). The second (and final) homeowner was the Warren A. O'Brien family who moved into the home in 1956/57. Mr. O'Brien worked for Western Electric Co. and retired in 1980. Daughters, Rozanne and Carol, babysat many of the children in the neighborhood in the 1960s. Rozanne (O'Brien) McPhee lived in the home with her parents for many years after her parents had become disabled. A wheelchair ramp was installed in the 1990s to facilitate entry into the home for both Rozanne and her parents. Rozanne moved out of the home about 2001 when her parents had passed. Rozanne and Carol continued doing yard work as they could. The neighbors painted graffiti off the fence for them and swept up glass/trash that frequently ended up on their sidewalk, lawn, and in the gutters. Neighbors expressed interest in purchasing the home when the daughters (O'Brien Family Trust) were ready to sell. Unbeknownst to the neighbors, the home was sold about June 2010. Neighbors continued keeping an eye on the home for the glass/trash hazards that needed to be picked up immediately. The City of Sacramento

continued picking up trash, recycles, and yard refuse. However, the City of Sacramento has never removed yard trash that is under vehicles that have parked right on top of it. The home was demolished sometime between September 17 and September 30, 2010 (Demolition approved September 13, 2010 - # RES-1009306) with no advance notice to the neighbors. Neighbors never received notification regarding re-zoning and merging the double lot. In early October 2010, neighbors received the "Early Notice of Planning Application (Z10-142)" to develop six apartment dwelling units ("multi-family residential" on a "1.9 acre" parcel at 2331 T Street/1918 Twenty Fourth Street.

I'm finding out that no laws were broken when they demolished the home and took out the trees without reasonable notification to the neighbors in advance. Had it been a two-story structure, then the property owner would have been required to give us a two-week notice. Also, the review that the police and fire departments signed off on was just that the demolition could be done safely without impacting the physical safety of the immediate neighbors and their structures. A home that is habitable can just be demolished, unless it is protected in a historic heritage district (like my Poverty Ridge Historic District, right across the street). There does not appear to be a city code that protects and regulates reasonable accommodation and any adverse impacts on the elderly and disabled neighbors in the "hood". The City of Sacramento should have provided enough parking for Lightrail commuters by the lightrail station at 24<sup>th</sup> and R Streets. This causes parking overflow right onto the parking spaces along 24th Street as far as my bungalow.

Availability for comments and questions:

Susan Woodward ("Kelley")  
2006 Twenty Fourth Street  
Sacramento, CA 95818

(916) 837-8991

[2006sew@comcast.net](mailto:2006sew@comcast.net)

**David Hung**

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**From:** 2006sew@comcast.net  
**Sent:** Monday, February 21, 2011 4:19 PM  
**To:** David Hung  
**Cc:** Robert King Fong; Lisa Nava; Kelley Woodward  
**Subject:** Proposed "24th & T Street Courtyard Housing Project" (P10-089)

21 February 2011

Hello, David Hung -

I am Susan Woodward ("Kelley"), and I have comments for the staff report to the Planning Commission regarding the proposed "24th & T Streets Courtyard Housing Project" (P10-089). This project proposes to have six two-story condominiums with five garages in an International/Moderne style of architecture in the heart of what is predominantly a single family home neighborhood with bungalow, brick, and Tudor styles of architecture appropriate to the time period 1900 - 1940's.

I am a concerned neighbor in the Newton Booth neighborhoods. I live at 2006 24th Street, across the street from the proposed project. I am submitting these comments on behalf of myself and other concerned 24th Street neighbors. I am a member of the Core Group of concerned neighbors that have had many communications with you, Council Member Fong, and Lisa Nava these past five months. I am a member of the Newton Booth Neighborhoods Association.

We believe in Smart Growth and understand infill, urban development, and other health and safety issue concerns of the City. We also believe in what is reasonable and appropriate, within the City's Guidelines, for our small, but fine old historic neighborhood.

For us, reasonable and appropriate is:

Three residential housing units with adequate size garages, pitched roofs, porches (insert # of sq ft divided by 3 times 80% density formula). There are a number of appropriate examples in the immediate neighborhood (1, 2, and 3 unit). There are no flat-roof examples that would be appropriate for this proposed project.

Architectural design (California Craftsman style bungalow; brick style; or Tudor style) There are a number of single family homes, duplex, and tri-plex structures in the immediate neighborhood. I can provide current photos of the structures in the immediate neighborhood if needed for design examples.

Height of structures: No more than 2 stories. There are no 3-story housing structures in the immediate neighborhood.

I understand that it is not necessary to resubmit the two petitions that we did in November, or to send copies of previous correspondence, September 2010 - January 2011 to your office.

I can be reached at this e-mail [2006sew@comcast.net](mailto:2006sew@comcast.net) or phone (916) 837-8991 if you have questions.

Thank you for providing this opportunity to address our concerns about the proposed "24th & T Streets Courtyard Housing Project" (P10-089).

Susan Woodward ("Kelley")

2006 24th Street  
 Sacramento, CA 95818

----- Original Message -----

From: "David Hung" <DHung@cityofsacramento.org>  
 To: "lauren@scottadamson.net" <lauren@scottadamson.net>, "morris411@gmail.com" <morris411@gmail.com>, "pjwade@fcusd.org" <pjwade@fcusd.org>, "kiddv@csus.edu" <kiddv@csus.edu>, "mpavisich@juno.com" <mpavisich@juno.com>, "Alex Zabelin" <alexegon@gmail.com>, "Heather 'Scott' <Heather.Scott@sen.ca.gov>, "bgwhitted@att.net" <bgwhitted@att.net>, "Stephen Whitted" <sbwhitted@att.net>, "marlenerice@sbcglobal.net" <marlenerice@sbcglobal.net>, "helmed@cwo.com" <helmed@cwo.com>, "alexives12@yahoo.com" <alexives12@yahoo.com>, "H2ngo@hotmail.com" <H2ngo@hotmail.com>, "j.konopka@sbcglobal.net" <j.konopka@sbcglobal.net>, "carrie camarena" <cdcamarena@yahoo.com>, "Alan LoFaso" <alofaso@sbcglobal.net>, "nomar98@yahoo.com" <nomar98@yahoo.com>, "ed@loftgardens.com" <ed@loftgardens.com>, "whitenightc@live.com" <whitenightc@live.com>, "Christina Jewett" <christina.jewett@gmail.com>, "2006sew@comcast.net" <2006sew@comcast.net>, "Bill Robertson" <w.preston.robertson@gmail.com>, "baxmag@sbcglobal.net" <baxmag@sbcglobal.net>, "amaroo2@sbcglobal.net" <amaroo2@sbcglobal.net>, "Tim and Lynne Gussner" <TimGussner@sbcglobal.net>, "fiores@saccounty.net" <fiores@saccounty.net>, "mlrobbin@pacbell.net" <mlrobbin@pacbell.net>, "marygomez1@sbcglobal.net" <marygomez1@sbcglobal.net>, "tamitrostel@comcast.net" <tamitrostel@comcast.net>, "kristinecelorio@yahoo.com" <kristinecelorio@yahoo.com>, "jhasko@att.net" <jhasko@att.net>, "reed.richerson@gmail.com" <reed.richerson@gmail.com>, "tguil75\_@hotmail.com" <tguil75\_@hotmail.com>, "suzmaast@yahoo.com" <suzmaast@yahoo.com>, "nikkicorbett@gmail.com" <nikkicorbett@gmail.com>, "chole531@gmail.com" <chole531@gmail.com>, "kari@sonic.net" <kari@sonic.net>, "ghostpony916@yahoo.com" <ghostpony916@yahoo.com>  
 Cc: "ron vrilakas" <Ron@VrilakasArchitects.com>, "Mark Groen" <mark@vrilakasarchitects.com>, "Ben Rosen" <bcroser@gmail.com>, "ANDREA ROSEN" <andrearosen@sbcglobal.net>, "Lisa Nava" <LNava@cityofsacramento.org>, "Robert King Fong" <RKFong@cityofsacramento.org>  
 Sent: Wednesday, February 16, 2011 4:00:53 PM  
 Subject: Comments on Courtyard Condominiums (P10-089)

Dear Everyone,

For those who want to comment on any aspects of this project up to now, including those of you who have provided comments to me in the past, please submit your written comments by February 25<sup>th</sup> if you need me to include them in the staff report to the Planning Commission. You may still submit comments after that, up to the hearing date, but those will be forwarded as supplemental materials. Thanks for your attention.

\*\*\*\*\*  
 David Hung  
 Associate Planner  
 Community Development Department  
 300 Richards Boulevard, 3rd Floor  
 Sacramento, CA 95811  
 Phone: (916)808-5530  
 E-mail: dhung@cityofsacramento.org

**David Hung**

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**From:** John Hagar [hagar.john@gmail.com]  
**Sent:** Wednesday, February 23, 2011 3:33 PM  
**To:** David Hung  
**Cc:** Robert King Fong  
**Subject:** Objections to Proposed Development P10-089

Thank you for allowing comments concerning the proposal to construct condominiums at 24<sup>th</sup> and T Streets in mid-town Sacramento ("24<sup>th</sup> & T Courtyard Development" (P10-089)). We live on Q Street between 24<sup>th</sup> and 25<sup>th</sup> Streets in a brick "Tudor" style single-family dwelling. We see several fatal problems with the proposal, including the following:

1. We and our neighbors chose to live in the Newton Booth neighborhood because it is primarily comprised of single family homes, with a few scattered and small multi-unit apartments. At present, the City is working with our neighborhood concerning a number of traffic calming measure. Our neighborhood does not need the increase in density that will result from an additional six-unit condominium/apartment complex (assuming the complex is occupied), especially along narrow and already busy 24<sup>th</sup> Street.
2. The proposed design of this multi-unit building is not in any manner reflective of our neighborhood, and will clash with existing structures.
3. The proposal requests that the City approve far too many variances to critical construction standards, important regulations that should be rigorously enforced. For example, we understand that the developer seeks: (a) a variance that will allow her to build a multi-unit complex in a neighborhood of single family dwellings; (b) a variance to reduce the parking spaces normally required for a proposal of this nature (a variance which, if approved, will add to the already congested street parking on 24<sup>th</sup> Street); (c) a variance that will reduce the required setback for an accessory structure (in essence, a variance to further overcrowd the tiny .19 acre parcel on which the condominiums/apartments are to be placed); and (d) a variance waiving the trash enclosures required for multi-family development (which will create health and safety issues).
4. Finally, we question the timing and appropriateness of this project in terms of the City of Sacramento's vision for the future. The Newton Booth neighborhood is at present bordered by two other condominium projects. The project at 21<sup>st</sup> Street at T Street is only partially completed, leaving almost one entire block barren except for some foundation work. Additional units are not being constructed *because no one wants to buy them*. Another project, lofts along S Street between 21<sup>st</sup> and 22<sup>nd</sup> Streets, have also failed to sell and are now either short term rentals or sitting empty. Naked foundations and empty units are a blight, no matter how recently they were built. While this developer may want to build condominiums, there is no reason to believe that her condominiums will sell. Indeed, if the prior projects in the area are considered evidence, the 24<sup>th</sup> and T Courtyard condos will remain empty, or will be rented to short-term renters for a number of years into the future.

In terms of the realities of today's market, this project is the very opposite of what our City needs to move forward. Let's keep Sacramento a place where you can work, raise a family, and retire in a decent neighborhood. There is no reason to approve four major "Special Permits" to allow this developer to construct another empty multi-unit building in a quiet, established, and very livable neighborhood.

For all of these reasons we request that this project not be approved.

If I can provide other information, please do not hesitate to call at (415) 215-2400.

John Hagar

QuickTime™ and a  
TIFF (Uncompressed) decompressor  
are needed to see this picture.

David Hung  
Associate Planner  
Community Development Department  
300 Richards Boulevard, 3rd Floor  
Sacramento, CA 95811

Dear David,

Developer Andrea Rosen presented her initial plan to the Newton Booth Neighborhood Association at our October meeting. The board liked the design, but declined to write a letter to the City endorsing the project as the plans were not final and the board had not visited the site. The board was also astonished when the original structure was demolished within days. This was not mentioned at the meeting and seemed to undermine confidence in the developer's transparency.

The neighborhood has voiced unanimous disapproval to the scope and size and design of the project. It does not complement the neighboring structures and is of a higher density than existing parcels. The looming corner is an affront and the plan shows the ADA unit being the furthest from the garages (inadequate parking for the number of bedrooms planned). The many variances and permits speak to the inadequacy of the design to meld and dissonance this project is creating.

This design is well suited for an L Street or R Street loft environment.

I oppose this project in its current iteration. T/23rd Street has a bungalow home with three units that perfectly integrates itself into the neighborhood.

Thank you,

Alex Zabelin  
NBNA, President  
2023 23rd Street  
Sacramento, CA 95818

Newton Booth Neighborhoods Association

24 Feb 2011

To: David Hung  
City of Sacramento

I oppose this plan  
as being not appropriate  
for our neighborhood -

Sincerely,  
Pat Melanby  
2431 Yow St  
Sacramento

768-7286

**David Hung**

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**From:** Pamela Wade [peejwade1@att.net]  
**Sent:** Friday, February 25, 2011 2:20 PM  
**To:** David Hung  
**Cc:** peejwade1@att.net  
**Subject:** 24th & T Courtyard Development

Dear Mr. Hung:

I have lived on 24th and T for twenty-one years. I have watched this neighborhood transform from aging homes to beautiful restorations. Our neighborhood is proud of the 1920's architecture, and we have embraced the historical aspect as we restored our homes. I personally, with a business partner, have purchased 3 extremely run-down homes in the neighborhood and restored them to their original beauty. One such property is on a double lot close to Ms. Rosen's property.

I am outraged by Ms. Rosen's lack of consideration towards the homeowners in our neighborhood. She fails to acknowledge our desire to have her complex reflective of the craftsman bungalows and tudor-style homes that surround her. She likes ART-DECO and how dare us not WELCOME her taste in a neighborhood that has worked so hard to embrace the historical architecture of our homes.

I am concerned about the extreme density of her proposal, and the request for parking exemptions in a neighborhood that already struggles for adequate parking. I am concerned about the "cold" design of her courtyard, and how it contrasts with the open porches and welcoming feel of our homes. I feel that for Ms. Rosen, this is simply a "money-making" venture for her, and that our concerns are of NO CONSEQUENCE to her. Simply put, when she has made her money and moved on, our neighborhood will be forever saddled with a design that none of us like, and will have to live with until we move.

It doesn't seem fair in the least. Ms. Rosen is not concerned about any type of "good faith negotiation". I feel as if a "bully" has moved into the neighborhood, and I for one, am frustrated, angry and tired of feeling "pushed around" by her. Please support us in our desire to limit the density of this courtyard, and the blending of her design into the Craftsman, Bungalow and Tudor architecture of the neighborhood.

Our neighborhood is in the midst of an historical "revival", and I feel strongly that this construction would be an INSULT to our efforts.

Sincerely,

Pamela J. Wade

February 24, 2011

David Hung  
Community Development Department  
300 Richards Blvd, 3<sup>rd</sup> Floor  
Sacramento, CA 95811

Re: P10-089

To Mr. Hung,

I am writing to express my opposition to the proposed Courtyard Development located on the northwest corner of 24<sup>th</sup> and T Streets. I live across T Street from the site with my husband and two children and we have invested too much in this neighborhood to see it undermined by Andrea Rosen's development. It absolutely inappropriate for the Newton Booth Neighborhood for its density and design and Ms. Rosen has demonstrated unwillingness to negotiate in good faith with the neighbors who have volunteered to represent the interests of the larger neighborhood.

The proposed density for this development includes six apartments of approximately 1200 – 1800 square feet. This many apartments of this size can easily house at least 12 people and as many as 24 people, somewhat comfortably. What existed at the site fewer than six months ago was a single-family home. The impact from the increase of a single-family home to six apartment units is simply too much for this neighborhood to sustain, as evidenced by the variances that Ms. Rosen has requested: a waiver from providing the number of parking units required for a multi-family dwelling and a waiver from providing the number of trash enclosures required for a multi-family dwelling. This development and these waivers will dramatically reduce available parking on T and 24<sup>th</sup> Streets. Additionally, the waiver for trash enclosures will result in clogging the 24<sup>th</sup> Street bike lane with 12 garbage and recycling cans once a week that may not be removed from the street in a timely fashion.

The design for this development is inconsistent with the existing design of the homes in the neighborhood. The Newton Booth Neighborhood is rife with examples of California Craftsman bungalows, brick style homes and Tudor-style homes. The lack of porches combined with casement windows and flat roofs are not at all reflected in the homes in several surrounding blocks. Our neighborhood already suffers too much from unsightly, inappropriately-designed apartment complexes built in the 1960's and 70's. Similarly, I do not believe that the current design will stand the test of time.

Though the neighbors in the Newton Booth and Poverty Ridge neighborhoods were never notified of the existing home's demolition nor consulted in the developing design of the complex, we organized a group of representatives to reach some sort of compromise on these issues with Ms. Rosen; no compromise has been reached as a result of Ms. Rosen's obstinacy.

My family and I have lived in our home, a Craftsman bungalow, for seven years. We have invested much time, money and energy in restoring the beauty of our home consistent with its original design. We greatly improved its curb appeal with new paint and landscaping. We have watched as seven homes within 300 feet of the proposed development do the same. The trend in our neighborhood is to buy an existing home and restore and beautify it – not demolition it and build the largest complex that can be squeezed into the lot size. Ms. Rosen's complex as proposed does not belong in our neighborhood.

The beloved single-family home that existed across the street from my house, within full view of my living room window will never return and we have accepted that. We cannot accept the current proposed development as a suitable dwelling in our neighborhood.

Sincerely,

Bridget Whitted  
2314 T Street  
Sacramento, CA 95816

October 10, 2010

Mr. David Hung  
Associate Planner  
Community Development Department  
300 Richards Blvd., Third Floor  
Sacramento, CA 95811

Dear Mr. Hung:

I'm writing to express my concern regarding proposed development on the northwest corner of the intersection of T street and 24<sup>th</sup> street. My wife and I bought our house across T street from the property in 2004 because we were attracted to the vibrancy and diversity of the neighborhood. We were drawn to the historic architecture, so we bought and remodeled our 1928 bungalow. Since then we have grown fond of many of our neighbors and have had two children whom we plan to raise in the neighborhood. We're invested in the neighborhood both financially and emotionally, so I was surprised and troubled to come home from work a couple of weeks ago to find that the house across the street had been reduced to rubble. My anxiety mounted as I watched a chain-link fence go up around what had become a dirt lot and I learned of a proposed six-unit apartment complex to be erected on the site. My frustration is two-fold: I am concerned about the impact such a project will have on my neighborhood and I object to the process by which my neighborhood has been drastically altered with no notification or community input.

I see the proposal as detrimental to the neighborhood for a number of reasons. Given that our neighborhood is already saturated with multi-unit apartment complexes, adding more will further tip the balance of renters to home owners, increasing the number of people who may not be invested in the neighborhood for the long term. The six proposed units would likely house 10 to 12 adults and their cars, putting more stress on an already tight parking environment. The architecture of the proposed building is modern and block-like, clashing with the surrounding cottages and bungalows both in size and style. All of these effects detract from the livability, charm and long-term stability of the neighborhood.

I am also disturbed that the site became a vacant lot surrounded by a chain-link fence with no notification of nearby residents and without an approved plan for development. I am not familiar with the guidelines for public notification of this kind of project, but my experience in the neighborhood is that such notification is necessary for making even small changes, so I'm surprised that I didn't receive any information nor did I see any posting at the property. Also, while the house and grounds that previously occupied the lot were neglected, they were certainly salvageable and preferable to the current dirt lot. By allowing demolition of those structures before any redevelopment plan was approved it seems that the city has tacitly approved the project before review. This undermines the review process and puts pressure on neighbors to accept any plan that will address the current blight.

Thank you for taking my concerns into consideration and I will be happy to discuss these issues with you at any point in the process. I would also like to request that I be notified of any changes or action taken with regard to this project.

Sincerely,

Steve Whitted  
2314 T Street  
Sacramento, CA 95816  
(916)743-0889  
sbwhitted@att.net

Steve Whitted  
2314 T Street  
Sacramento CA, 95816  
(916) 743-0889  
sbwhitted@att.net

February 24, 2011

Mr. David Hung  
Associate Planner  
Community Development Department  
300 Richards Blvd., Third Floor  
Sacramento, CA 95811

Dear Mr. Hung:

I am a resident across the street from the proposed development at the corner of 24<sup>th</sup> and T Streets. When the project was initially brought to my attention, I sent a letter expressing my apprehension regarding the proposal, and I'm sorry to report that despite efforts to work with the developer to address neighborhood objections, the project as it was finally submitted to the planning commission ignores concerns I share with many of my neighbors: the size of the buildings and the resulting population density.

Based on communication among neighbors, I understand that the developer, Andrea Rosen, insists that the project is in keeping with "smart growth" principles and that including six units in her proposal is fundamental to these principles. I applaud the idea of development that encourages alternative forms of transportation. Indeed, I choose to live in my neighborhood exactly because it is possible for me and my family to walk, ride our bikes and take light rail easily and safely. I also understand that increased population density is an important component of this kind of urban planning in that more people can live in comfortable proximity to work, entertainment and transportation. However development of this kind has to take established neighborhoods into account. I don't think anyone would advocate for a high rise apartment complex in an existing midtown neighborhood. So where do we strike the balance?

I agree with many of my neighbors that the proposed increase in density at 24<sup>th</sup> and T from one unit (pre demolition) to six is excessive. Three or four single story units would be much more in keeping with our neighborhood than the imposing buildings Ms. Rosen hopes to construct. The proposed buildings would be more at home on the R Street corridor where they would blend with existing light industry and office buildings than in our neighborhood of cottages and bungalows. I also take issue with the idea that fewer than six units would be irresponsible from a smart growth perspective. Certainly the lot in question can and should accommodate more than the one house that existed prior to demolition, but in a neighborhood of single family homes and small apartments, an increase from one to three seems more reasonable.

So I find Ms. Rosen's motive for insisting on six units dubious, and I believe that the only other possible motivation for her unwillingness to negotiate the number of units is financial gain. While I don't begrudge anyone's right to line their pockets, I do object to someone doing it to the

detriment of the neighborhood in which I hope to raise my family. I therefore urge the Planning Committee to reject the proposal in the hope that Ms. Rosen or someone else will find a way to develop the property in a way that balances "smart" growth with the concerns of our neighborhood. Thank you for taking the time to consider my input.

Sincerely,

Steve Whitted

**David Hung**

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**From:** MARLENE RICE [marlenerice@sbcglobal.net]  
**Sent:** Friday, February 25, 2011 10:10 AM  
**To:** David Hung  
**Cc:** Robert King Fong  
**Subject:** RE: Proposed 24th and T Development (P10-089)

Dear Mr. Hung,

I would like to add my concerns regarding this development project. I was alarmed to discover that a single residence could still possibly be replaced by a 6 unit project. The increased density in an area of our neighborhood that already has many multiple unit residences creates an unfair increase in parking issues in an already impacted area.

I am also concerned about the height of the project overshadowing the closest properties. The project is attractive, but has no place at the current site. When a new project discounts the size and architecture of existing properties, it changes the quality of life and property values of existing residents.

I would like to see 3 units maximum with adequate onsite parking to accomodate the new residents and a height and architecture that blends with surrounding properties.

I hope that city representatives now recognize that we are strong and unified neighborhood, not just a development opportunity for an individual who does not have to live with the result of her intrusion.

Thank you for your consideration of my concerns.

Sincerely,

Marlene Rice  
2225 T Street  
(member of the Poverty Ridge Traffic Calming Committee)

**David Hung**

---

**From:** Linda McNamara [amaroo2@sbcglobal.net]  
**Sent:** Friday, February 25, 2011 3:28 PM  
**To:** David Hung  
**Subject:** Proposed 24th & T Development (P10-089)

Dear Mr. Hung,

I would like to express my disappointment with Ms. Rosen's plan for the 24th & T project. I believe she has not listen to the neighborhood or tried to be reasonable with us. We do not oppose any development on the property but we would like a project that would compliment the neighborhood by matching our period stuctures.

My block which is the same block for the proposed structure. We have already two apartment buildings. I feel that my block has already fulfilled the city's desire for "in fill" quota. My desire would be for Ms. Rosen change her design to 3 to 4 units, single story and reflect the styles of the neighborhood.

Linda A. McNamara  
1911 24th Street  
Sacramento, CA 95816  
916/42-3616

**David Hung**

---

**From:** Tim Gussner [timgussner@sbcglobal.net]  
**Sent:** Friday, February 25, 2011 3:40 PM  
**To:** David Hung  
**Subject:** Proposed 24th & T Development (P1-089)

Dear Mr. Hung,

I am disappointed with Ms. Rosen's plan for the 24th & T project. She has not negotiated in good faith with our neighborhood or tried to be reasonable with us. We do not oppose any development on the property but we would like a project that would compliment the neighborhood by matching our period structures.

I live on the same block for the proposed structure. We have already two apartment buildings. I feel that my block has already fulfilled the city's desire for "in fill" quota. Ms. Rosen could change her design to 3 to 4 units, single story and reflect the styles of the neighborhood. This would not require any special clearances.

Any consideration in this matter will be deeply appreciated.

Timothy Gussner  
1911 24th Street  
Sacramento, CA 95816  
916/42-3616

ALAN LoFASO  
2001 24<sup>th</sup> Street  
Sacramento, CA 95818  
(916) 457-4322  
*ALoFaso@sbcglobal.net*

February 25, 2011

David Hung  
Associate Planner  
Community Development Department  
City of Sacramento  
300 Richards Boulevard, 3rd Floor  
Sacramento, CA 95811

**RE: P10-089**

Dear Mr. Hung:

I write to express serious concerns regarding the proposed 6-unit condominium development at the corner of T and 24<sup>th</sup> Streets. This proposed development anticipates two buildings of substantial mass employing an International/Moderne style of architecture that is out of character in a traditional neighborhood of architectural styles largely consisting of Craftsman, Bungalow, and Tudor style homes. The lot is now vacant as a result of the destruction of a historic home last September.

Consistent with many of my neighbors, I request that the City approve a project to be developed on the site comprising of three single-family homes, contained in three separate structures of no more than two stories; all units should provide off-street parking; and no additional height or setback variances should be allowed. Moreover, the architectural style of the development should be consistent with that of structures within the immediately surrounding area, which are generally although not exclusively Tudor cottages, Craftsman bungalows, and Depression-era brick duplexes. In keeping with these styles, and the general character of our neighborhood, the structures should not be too massive for the surrounding area, and they should employ porches and generally adhere to the principal of "eyes on the neighborhood."

This request is consistent with several planning principals enunciated in the City's general plan. For example, Goal LU 4.3.1 of the City's general plan requires that the "City shall protect the ... character of traditional neighborhoods, including ... architectural styles..." Poverty Ridge/Newton Booth is one of 13 traditional neighborhoods specifically mentioned in the general plan. Moreover, the general plan provides that the "City shall preserve the existing ... densities on each block of Traditional Neighborhoods." (See LU 4.3.2.) The general plan also provides that "[w]ithin the Traditional Neighborhood Medium Density designation, [a] development shall be allowed to reach 36 units per acre" only under limited conditions, including that the "development [must] maintain the character of Traditional Neighborhood Medium Density by presenting a façade of single family homes or duplexes..." (See LU 4.3.3.)

David Hung – February 25, 2011 – Page Two

The neighbors' request regarding this project is also consistent with developments in the surrounding area. For example, a nearby project recently approved by the City Planning Commission at 20<sup>th</sup> and S Streets (P10-069) would provide for nine single family homes, each as a separate structure, and built according to a Craftsman architectural style on a site approximately three times the size of the one at issue in this application. Three single family structures, in an appropriate architectural style, would be equivalent to nine units on a site three times as large. Moreover, this proposal would increase the number of units on the site threefold—well beyond the requirement to “preserve existing densities on each block.” Finally, the density of the proposed project at 20<sup>th</sup> and S Streets is approximately 24 DU/AC, within the mid-range of the general plan's density provisions for Traditional Neighborhood Medium Density.

Poverty Ridge/Newton Booth is a unique, historic Sacramento neighborhood with a character that varies even from block to block. It is important to preserve the character of this neighborhood. The surrounding area was the victim of many poor development choices during the 1960s and 1970s. These examples should not be the justification of additional developments out of character with the immediate surrounding area. Moreover, even an architectural style of greater quality that might be appropriate in a part of downtown or midtown Sacramento comprising of more recent infill developments characterized by a more modern style is not appropriate for this particular traditional neighborhood.

City planners have a variety of choices to meet the City's general plan requirements, including policies supporting appropriate infill development and smart growth. My neighbors and I support these principles. However, this application as currently proposed, is not an appropriate approach to meeting these policy goals. There are other alternatives, and the neighborhood is poised to work constructively within these policy goals in a manner that will preserve the traditional character of the Poverty Ridge/Newton Booth neighborhood.

Thank you for your consideration of my concerns.

Sincerely,

/s/

Alan LoFaso

Cc: Honorable Robert King Fong, Council Member, Fourth District  
Lisa Nava, District Director, Office of Council Member Fong

Claire Pomeroy, MD, MBA  
2009 23rd Street  
Sacramento, CA 95818

February 23, 2010

RE: Proposed 24<sup>th</sup> and T project

Dear Planning Commission members,

I write to ask that you help our diverse and thriving neighborhood achieve our dream of rejuvenation with urban renewal and infill projects designed to create a strong community. Specifically, I ask that you do this by requiring that the 24<sup>th</sup> and T project be limited to 3 units and constructed in a style consistent with the character of our neighborhood.

We are a re-emerging city neighborhood that strives to be an important part of the future of Midtown Sacramento. We celebrate our neighbors - people from all walks of life, the elderly and the young, renters and owners, families and singles - who have discovered the joy of living in this special part of the city. We celebrate the combination of stately homes, historic bungalows, garden cottages, and affordable apartments. As we walk through our neighborhood, we see the pride of our neighbors reflected in well-kept gardens; we meet each other to chat as we walk our dogs; and we embrace new families when they move in to build their lives here.

Our neighborhood has a personality - one that has emerged over the past several years. We want to ensure that as we grow and improve, we maintain that personality, so that we become a vibrant part of the future of our city.

That is why I write to you today to ask that you honor and support our neighborhood's wish that the housing project proposed for 24<sup>th</sup> and T not radically diverge from the previous density of housing at that address. I ask that you respect the neighborhood's suggestion to limit the number of units to 3 (an increase in density from the previous single family home that is acceptable to the neighbors) and that the style, height, size and landscaping be consistent with the neighborhood.

I want to emphasize that we believe in urban neighborhoods (that's why we live here!) and we believe in in-fill projects. What we are asking is that the in-fill projects respect the character of our community. We believe that the community's acceptance of an increase from 1 unit to 3 units is a thoughtful and reasonable position on the part of those of us who will welcome the inhabitants as new neighbors. We believe that this is a responsible use of the land that would represent a compromise on both sides. It is unfortunate that the current developer's request to put 6 units on the property appears to be driven by a desire to maximize business profits, not to help create a dynamic city neighborhood. By building an oversized structure with 6 units, we fear the project will skew the neighborhood away from a balance of single family homes and

apartment complexes and risk a “tipping point” that could destroy the neighborhood feel of our community.

A group of neighbors has diligently tried to discuss reasonable compromises with the developer, Andrea Rosen. I am disappointed that she has maintained that the number of units in the project is not an issue that she is willing to discuss. I do not see good faith or evidence of caring about our neighborhood in this negotiation stance. She appears unwilling to think beyond “cash flow” to consider the “impact on our neighborhood”.

Our Newton Booth Neighborhood Association has also expressed their lack of support for the project as currently proposed.

Therefore, I ask the Planning Commission to require project plans that limit the density increase on the 24<sup>th</sup> and T project site to 3 units and ensure that the style, height, size, and landscaping are consistent with the overall character of our neighborhood.

Thank you for your consideration,

Claire Pomeroy, MD, MBA  
Vice Chancellor and Dean, UC Davis Health System\*  
Resident - 2009 23<sup>rd</sup> Street, Sacramento, California

\*This title is used for identification purposes only, per UC policy.

cc: Mayor Kevin Johnson

cc: Councilman Rob Fong

**David Hung**

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**From:** Bill Robertson [w.preston.robertson@gmail.com]  
**Sent:** Monday, February 28, 2011 12:10 AM  
**To:** David Hung  
**Subject:** AR letter to CP  
**Attachments:** AR letter to CP.pdf

David--

Attached please find a .pdf of the letter that Ms. Rosen hand-delivered to my wife, Dr. Claire Pomeroy, at the UC Davis Health System, where she is Vice Chancellor and Dean. The letter speaks for itself in its inappropriateness and as a representation of the challenges our neighborhood has faced in its efforts to negotiate, just as, I am sure, Ms. Rosen has had her own perceived challenges with us.

I would note only that Ms. Rosen's claim in the letter of her development's "health benefits" is contradicted by her own admitted haste to demolish the existing 1940's house on the lot in order to avoid impending lead paint abatement guidelines set to take effect.

Please include both the attached .pdf and this email explaining it inclusion.

Thanks,

Bill Robertson

--  
William P. Robertson  
2009 23rd Street  
Sacramento, CA 95818  
916-607-2405

February 9, 2011

Dr. Claire Pomeroy  
Chief Executive Officer, UC Davis Health System  
UC Davis Vice Chancellor for Human Health Sciences  
Dean, School of Medicine  
4610 X Street  
Sacramento, CA 95817

HAND DELIVERED

RECEIVED  
SCHOOL OF MEDICINE  
OFFICE OF THE DEAN  
11 FEB 11 PM 3:22

**RE: Your Position Regarding Two-Triplex Residential Infill Development at 24<sup>th</sup> and T Streets (Courtyard Condominiums)**

Dear Dr. Pomeroy:

I am writing to you in your capacity as a regional health leader in Sacramento. I am asking you to re-evaluate your position on the above-named residential development project in light of the public health benefits of this type of residential development.

This Smart Growth residential infill development brings important short term and long term health benefits to the region and to its residents - current and future. This project contributes to a reduction in Sacramento's air pollution- a continuing public health challenge to the region.

**I am asking you to join other regional health leaders who are supporting this project in large part due its health benefits.** Please see the attached letters from:

- Larry Greene, Sacramento's Air Pollution Control Officer
- Mike McKeever, Sacramento Area Council of Governments Executive Director
- Teri Duarte MPH, Sacramento Design 4 Health Chair\*

These leaders recognize the health benefits of 24<sup>th</sup> and T Courtyard Condominiums and its contribution to regional health improvement by making high quality residential housing available to future Midtown residents who will be able to choose a sustainable lifestyle that includes less or no reliance on the automobile. High quality housing built within close walking distance to light rail increases ridership thereby reducing Vehicle Miles Travelled (VMT), thereby reducing air pollution which improves individual and community public health.

Infill such as 24<sup>th</sup> and T Courtyard Condominiums provides attractive housing options for individuals who choose to prioritize walking and cycling as a lifestyle choice by making these travel options easier and therefore more likely to be used. This project, in particular, is pedestrian-oriented and thus contributes to a healthy lifestyle by its residents.

I encourage you to read the support letters from these important health leaders. I have also attached a bibliography of articles documenting the health effects of air pollution and the health benefits of Transit Oriented Development. 24<sup>th</sup> and T Courtyard Condominiums is a perfect example of a Transit Oriented Development and has been recognized as such.

I know you are very busy, but health and our built environment is an important regional health concern that merits your attention. Community support for Smart Growth residential infill is a priority for Sacramento. (See Sacramento General Plan adopted in March 2009).

I request an opportunity to review this project including its health benefits with you at your earliest opportunity. Please contact me at 916.508-6721 to set up a time to meet.

Dr. Pomeroy. You could use this project as a vehicle to educate the neighborhood and community about the health benefits of Smart Growth residential infill. There are MANY UC experts on this topic who would be happy to work with you to organize such a forum. As a doctor, educator and health leader, I urge you to support the requested residential density at this critically important site.

Respectfully,



Andrea Rosen MPH, JD

Enclosed:  
Support Letters for 24<sup>th</sup> and T Courtyard Housing as described  
Bibliography

\*This group includes doctors from UCD who will be testifying publicly in support of the health benefits of this project.

**David Hung**

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**From:** trostel4@comcast.net  
**Sent:** Sunday, February 27, 2011 9:33 PM  
**To:** David Hung  
**Cc:** Robert King Fong  
**Subject:** Proposed 24th & T st development

Mr Hung,

I apologize for the timing of this email but I have been out of town for approximately one week and just arrived home to find I could voice my family and I concerns with the proposed project on the corner of 24th and T st.

We believe the specifications of the project would bring numerous negatives to the neighborhood.

My wife and I bought our house on the corner of 24th and S st approximately 3 yrs ago. We bought in this particular neighborhood because it encapsulated the quintessential midtown neighborhood with the old victorian architecture and family atmosphere all in one. My wife now runs a successful childcare facility out of our home in which she has multiple children each day.

Although we love our neighborhood it is not without it's problems. Because we have numerous businesses and a bar in the neighborhood we constantly struggle with parking and oversaturation of traffic on our streets. With two young children this makes it difficult for them to play anywhere in the neighborhood outside the boundaries of our fences as well as the difficulty of finding parking for us when we arrive home. There is simply too many cars and not enough parking in the neighborhood. It is my understanding that the developer of the project in question is asking for variances on the parking that is essential for the size of the project. This is of great concern to my family and this will greatly effect the neighborhood in an extremely negative way.

Another concern for my family is the overall size of the project. My wife and I could have moved anywhere but we chose to buy in Sacramento because of it's beautiful victorian architecture as well as it's small city feel. Similar to the brownstones of Brooklyn New York or the row housing of Baltimore, Midtown Sacramento is known for it's early century architecture and that is something that has always attracted people such as myself to this beautiful city. We feel that something in the same theme of the existing neighborhood would be in order with no more than 3 units going into 2 very small lots.

This letter is not a protest against change. My family and I welcome positive changes for the neighborhood, like restaurants shops and even housing. The issues we have with the proposed design is the sheer size and potential problems this project would bring to the neighborhood. We thank you for hearing our concerns and once again I apologize for the timing of this letter.

Thank you,  
Michael Trostel  
2400 S st  
Sacramento Ca 95816  
530-400-1515



## **Attachment 2**

### **Supplemental Materials**

1. Correction to unit information on page 6 of staff report.
2. Comment from Patricia Hedge Emmons
3. Comment from Christina Jewett
4. Piece from Sacramento Bee: Viewpoints: Column insulted Newton Booth neighbors
5. Comment from Mabel Lee Robbins
6. Comment from Paul Petrovich

Correction to the following paragraph on page 6 of the staff report:

Development on the subject site is restricted to ~~six (6)~~ **seven (7)** units based on the parcel's zoning designation of Multi-Family (R-3A). The R-3A zone allows a maximum density of 36 units and a minimum lot size of 1,200 square feet per residential unit. The proposed project is located on 8,428 square feet and can therefore, accommodate a maximum of ~~six (6)~~ **seven (7)** units.

## David Hung

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**From:** Patricia Hedge Emmons [plh170@live.com]  
**Sent:** Thursday, March 10, 2011 6:08 AM  
**To:** David Hung  
**Subject:** P10-089

Dear Mr. Hung,

RE: P10-089

The purpose of this communication is to express my strong objection to the proposed Special Permit for reduced parking requirements for the project located at the Northwest corner of T Street and 24th Street.

At the present time parking can be extremely difficult. People now living in the neighborhood can frequently be forced to park some distance from their residences and, at night particularly, that can be uncomfortable.

If a parking/traffic study has been conducted proving that parking in this area is not the problem I express then I would like to have a copy to review. If no such study has been done then it would be irresponsible for the City of Sacramento to approve such a Special Permit.

Respectively submitted,

Patricia L. Hedge  
Owner: 2313 & 2315 T Street  
Sacramento, CA 95816  
415.730.3691

Mailing address:  
65061 Highland Road  
Bend, OR 97701

## David Hung

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**To:** Christina Jewett  
**Subject:** RE: e-comment

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**From:** Christina Jewett [<mailto:christina.jewett@gmail.com>]  
**Sent:** Thursday, March 10, 2011 11:34 AM  
**To:** David Hung  
**Subject:** e-comment

Hi David -- I just tried to submit an Ecomment on the courtyard project, but was told the e-commenting is closed on this project. I was informed it would be open until noon.

Here is my comment:

This project would be better with: Smarter growth: I am all for Smart Growth. However, this project is decidedly suburban in its orientation to an internal courtyard. It would be better oriented outward, with front porches and eyes on the street. A more gradual profile: Step this project back from the sidewalk, trim the elevations and mass to achieve better integration with surrounding single-family homes. I ask the Planning Commission to call for a mediated negotiation on this project.

# Viewpoints: Column insulted Newton Booth neighbors

By Alan LoFaso and Heather Scott  
Special to The Bee

Published: Thursday, Mar. 10, 2011 - 12:00 am | Page 12A

Last Modified: Thursday, Mar. 10, 2011 - 7:57 am

We were appalled by Mary Lynne Vellinga's column, "For developer Petrovich, it is payback time" (California Forum, March 6). It portrayed us and approximately 30 Poverty Ridge and Newton Booth neighbors as pawns of developer Paul Petrovich in a grudge match over Andrea Rosen's proposed condominium project at 24th and T streets.

Beyond insulting, the article missed an opportunity to discuss real questions of how to implement "smart growth" policies while respecting traditional neighborhoods.

The size and scope necessary for feasible infill development received scant attention, and the voice of neighbors in that process was squelched.

This matter started for most of us the morning of Sept. 22, waking up to the sound of an old house crashing down, learning later that developer Rosen failed to notice an imminent bulldozing at a meeting two days prior.

Confident that a fair process would give us an opportunity to be heard, a core group of neighbors met Rosen with an alternative to her six condominiums, seeking to reduce the mass and scale of the project with a more appropriate architectural style.

She responded as a litigator, questioning technicalities to discredit comments, not discuss them. She claimed she could not afford a smaller project, and discussions reached an impasse.

We then learned of a city planning process using a workshop between neighbors and developer we thought would better engage Rosen's technical and legal orientation. She refused to participate.

Meanwhile, we looked to other local examples, including last month's City Council rejection of an inappropriate modern design elsewhere in midtown. The developer claimed rejection would kill the project, but one of us has since met the happy couple proceeding to build with the same developer after the council-imposed changes supposedly could not work.

Like many developers, Rosen claims she cannot afford to build the project with any major changes to its number of units or square footage.

Moreover, she claims "smart growth" guidelines compel its size and scope. Some observers,

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including columnist Vellinga, apparently agree.

What's at stake is whether neighbors who value walking, cycling, public transit, and access to mixed-use amenities – i.e., smart growth – can have their voices heard, and how infill development respects residents' views of the architectural integrity of their own neighborhoods.

The process, which starts formally tonight at the Planning Commission, has offered no answer yet, but The Bee would do a service to the community to report that story.

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*Alan LoFaso and Heather Scott are neighbors in the Newton Booth section of midtown.*

## David Hung

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**From:** Mabel Lee Robbins [mlrobbin@pacbell.net]  
**Sent:** Thursday, March 10, 2011 1:34 PM  
**To:** David Hung  
**Cc:** Robert King Fong; Lisa Nava; 2006sew@comcast.net  
**Subject:** RE: Proposed Property, "24th & T Streets, Courtyard Housing Project (P10-089())  
**Attachments:** mlrobbin.vcf

Mr. Hung,

I will not be able to attend this evening's meeting regarding the property on 24th & T Streets since I have a prior engagement that I cannot miss.

Please include my continued objection to the development on this property on the following grounds:

With the inclusion of an additional six unit condos, plus 12+ additional people with cars, it will heavily impact the parking situation; especially on our block that has a very narrow width for cars **and** bicyclists. Aside from the parking situation, six condo units crammed in to a small space will not be esthetically pleasing, much less reflective of the neighborhood.

Thank you so much.

Respectfully,  
Mabel Lee Robbins  
2010 24th Street  
Sacramento, CA 95818  
916.453.0680

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**From:** Planning Commissioner Declines [mailto:sacplanning\_declines@me.com]  
**Sent:** Wednesday, March 09, 2011 11:51 PM  
**To:** Commission submit  
**Subject:** Fwd: 24th and T

Sent from my iPhone

Begin forwarded message:

**From:** Paul Petrovich <Paul@petrovichdevelopment.com>  
**Date:** March 9, 2011 9:42:33 PM PST  
**To:** sacplanning\_declines@me.com  
**Subject:** 24th and T

Hi Rommel,

I am writing this letter regarding Andrea Rosen's development proposal instead of attending the hearing tomorrow night. I do not want my presence to detract from the real and more important message being delivered by the neighbors most affected by this project. The Bee mischaracterized this as being personal between developer Rosen's and me. I guess they needed to sensationalize the issue to sell more papers. Yes, activist Rosen led the charge against my project in Curtis Park for seven years, but in fact, this is about the neighbor's and my concerns (as a major stakeholder) in this particular neighborhood. If I was raising concerns over a project she was developing where I had no vested interest, the claims of Rosen and the Bee might have merit. To their discredit, this is not the case. I will explain my stakeholder status later in this letter. My goal is to not let anyone dilute

or cause a distraction to the true issue here; *the concerns of a neighborhood and how the project should be changed to fit the character of its surroundings.*

No one is saying developer Rosen should not have a project of any kind. The issue at hand is how and what is built and how it impacts the people who live adjacent to it. To date, the process has been subverted. Developer Rosen has run rough shod over the neighbors and not once attempted to sit down with them and have a decent and fair conversation. Yes, they came to me to ask for help. They did this based on extreme frustration, knowing I have major investments in their neighborhood and my experience in how projects are supposed to be processed. This only occurred because Rosen has completely subverted the process and made her proposal a "take it or leave it" proposition. The people who live here have never been through an experience like the one Rosen has put them through. They are relying on the system, especially the Planning Commission and Design Review Board to give them a voice and an opportunity to make sure this project becomes additive to their environment, not the opposite. Rosen is a former Planning Commissioner and spent years promoting the "forum based" format to mold projects into a positive development based on heavy input and influences of the stake holder's. She was correct in advocating this process as a Planning Commissioner so long as it is not abused as it is so many times. Now as a developer, she is doing everything in her power to subvert that process to maximize her profits.

From a planning perspective, her proposal is at a density of **71 units** per acre with extremely high mass and scale in what otherwise is a traditional single-family neighborhood. It sets a very bad precedent. If she had cared to engage the neighbors as I have, their concerns are not as much about the number of units, but about the scale and mass of this project. Her concept is completely out of character for this 150-year-old historic enclave of homes. It would be more appropriate as a buffer to existing commercial or industrial uses to transition to lower mass single-family homes. Instead it's being dropped right in the middle of the quiet and calm single-family core of this neighborhood. Yes, there are other multi-plexes in Poverty Ridge, but they are single level and built in the same architectural style and mass as the adjoining homes! Rosen's project elevates vertically two stories just back of the sidewalk with virtually no setbacks to maximize the square footage of the project. The setback waiver is just one of her requests which should be denied. Developer Rosen may claim she is entitled to six or seven units, but not without variances and not at this mass and scale.

Separate from the Bee's characterization, I have legitimate stakeholder status. I have invested \$65 million in the four-block area surrounding Rosen's project with \$18 million invested within two blocks. Despite not living in the Newton Booth neighborhood, my investment and manor and care in which I developed and still maintain these projects entitle me to a voice in this matter. These projects include the R Street Market (the Safeway anchored mixed-use project at 19<sup>th</sup> and S), the re-use of the building across the street with Sweetwater Restaurant and Massage Envy, the neighborhood (half sized) Rite Aid at 21<sup>st</sup> and S (without a drive through) and the Whiskey Hill Lofts at 22<sup>nd</sup> and S. All four of these projects re-used or demolished run down industrial buildings and dilapidated metal sheds that I developed into "Class A" neighborhood serving retail and high quality residential. I still own these projects six years after completion and have great concerns about a bad precedent being created here. The Newton Booth Association reviewed all of these projects and changes were gladly made, particularly to the Whiskey Hill Loft project at their request. The Newton Booth Association endorsed these projects for approval. There was zero vitriol during my interaction with the Newton Booth Association. Today these projects are credited with transforming this neighborhood back to a walking and cohesive livable community since people no longer need to drive to Land Park or Alhambra Boulevard for their daily needs. They also removed blight and created buffers to the remaining industrial base in Poverty Ridge.

Furthermore, the support Rosen has from the density advocates, if read closely, support the concept of density, but do not necessarily carry a ringing endorsement of this specific density in this specific location. If asked, these advocates will clearly state that not all high-density projects are good for neighborhoods. There needs to be a transition and **context to its surroundings**. Everyone believes in the general concept that it makes sense to place high density residential near the transit system. However, the placement of it can't be blind and not relate properly to its surroundings. High-density development at all costs is wrong. It is not a one size fits all concept that applies to each and every development and profit center regardless of its impact on its neighbors.

Moreover, Rosen, by making the decision to tear down the house, (which made up over half the value of the investment) does not allow her the right to argue economic hardship. She is now claiming the economics of this project will not work if she is not granted her massive, out of scale concept. Building from scratch after paying market value for land with a structure usually does not work. It is very risky business. Rosen obviously got a great deal for closing fast. However, most developers know to option property or offer more money for it when developing infill property to gain time in

escrow to obtain the required entitlements and gain acceptance by the surrounding neighbors. If she had experience, she should have rehabbed the house and added another single-family residence and would have made as much profit. I myself would have relocated the house on the property to make room for two or three detached single family residences and made more money while keeping the integrity of the neighborhood. Prior to developing my larger scale retail and residential projects, I made a good living creating this exact type of project by relocating structures successfully.

Instead, developer Rosen is heaping the burden and putting undo pressure on the adjoining residents due to her inexperience and lack of decency to work with the people affected most. Based on her unilateral actions she is now attempting to punish these people for disagreeing with her. I have been developing for 30 years and I can tell you one thing I have been told consistently by planners, planning commissions and elected officials; "economics of a project are not a planning principle". Developer Rosen paid \$300,000 for the house and land. I learned this by asking the title company for the amount of transfer taxes recorded on sale. This is public information. I have run several pro formas based on requests of several concerned individuals and Rosen can still build three single-family residences and make a good profit. In fact, if she had paid \$400,000, she would still make a very good profit. Without getting into the multi page highly detailed pro forma I created using my experience, there is a simple rule of thumb. The age-old formula is that land should cost one-third the cost of the finished home to make a reasonable profit. She can build three \$350,000 homes (which is well within the comparable sales for this neighborhood) and make a great profit. Instead, she is pushing for extreme profits at the expense of this neighborhood.

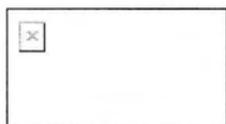
Lastly, the Design Review Board has not reviewed this project. They have not weighed in on the scale and mass of what has been proposed. Other than buying the land, there is virtually no cost of significance invested here by the developer. This, in my opinion, makes it even more reasonable to wait for the Design Review Board to weigh in. I urge you to use Thursday's Planning Commission meeting as a workshop and a means for the neighbors to be heard by you. The developer has refused to engage in a workshop despite the neighbor's pleas and staff's request. David Kwong has first hand knowledge of this if you care to verify this matter.

I urge you to listen to the neighbors and then continue the project until Louis Sanchez and the DRB can weigh in. To render a decision prior to that input as to the number of units, masses and scale is premature. After the DRB has weighed in, the matter can then be brought back to the Planning

Commission for final deliberation. If Sacramento's entitlement process stands for one thing, it is the certainty that all voices will be heard and a process will be followed that allows input from the stakeholders, particularly those that live adjacent to new development. To date, the voices of those concerned have been ignored and someone attempting to game the system has subverted the process. I urge you to level the playing field and have this project go through the process, as it should to develop a project that works for all concerned.

Thank you,

*Paul Petrovich*



*Please note our new address and phone number below effective March 1, 2010*

PETROVICH DEVELOPMENT COMPANY  
825 K STREET  
SACRAMENTO, CA 95814  
PH: (916) 442-4600  
FAX: (916) 442-6313



# REPORT TO PLANNING COMMISSION City of Sacramento

# 5

915 I Street, Sacramento, CA 95814-2671

PUBLIC HEARING  
March 24, 2011

To: Members of the Planning Commission

**Subject: 1831 K Street Offsite Parking (P11-019)**

A request to re-establish an offsite parking lot at 912 20<sup>th</sup> Street for an existing office building at 1831 K Street in the General Commercial (C-2) zone. This request requires a Planning Commission Variance to allow an offsite parking lot under the same ownership as an office building that is located more than 300 feet away.

- A. Environmental Determination: Exempt (CEQA Guidelines Section 15301);
- B. Variance to allow an offsite parking lot for required and nonrequired parking to be located more than 300 feet from an office building under the same ownership.

**Location/Council District:**

912 20<sup>th</sup> Street, Sacramento, CA

Assessor's Parcel Number 007-0014-008

Council District 3

**Recommendation:** Staff recommends the Commission approve the request based on the findings and subject to the conditions listed in Attachment 1. **The project is non-controversial at the time of writing this report.** The Commission has final approval authority over items A-B above, and its decision is appealable to City Council.

**Contact:** Evan Compton, Associate Planner, (916) 808-5260; Stacia Cosgrove, Senior Planner, (916) 808-7110

**Applicant:** Darryl Chinn, DC Architects, (916) 761-6956, 2612 J Street #2, Sacramento, CA 95816

**Owner:** Jayson Javitz, LDR Partners, (916) 371-4960, 840 Delta Lane, West Sacramento, CA 95691



**Summary:** The applicant is requesting to re-establish a developed offsite parking lot for required and nonrequired parking of an existing office building. The offsite parking lot was previously approved for the office building however, the entitlement expired. The proposal requires a Planning Commission Variance to allow an offsite parking lot under the same ownership as the office building to be located more than 300 feet apart.

<b>Table 1: Project Information for Offsite Parking Lot</b>	<b>Office Building at 1831 K Street</b>
<b>General Plan designation:</b> Urban Corridor Low	Urban Corridor High
<b>Existing zoning of site:</b> C-2 (General Commercial)	C-2 UN (General Commercial in the Urban Neighborhood Overlay zone)
<b>Existing use of site:</b> Surface Parking Lot with 56 spaces	26,150 square foot office building
<b>Property area:</b> 80 x 160 feet or .29± acres	90 x 160 or .33± acres

### **Background Information:**

1831 K Street: The site was originally developed with single family homes which were demolished in the 1970s. In June of 1979, a 20,600 square foot office building with eight onsite parking spaces was constructed (P8833). In October of 2001 (P01-075) and November of 2002 (P02-137), the Planning Commission approved a series of entitlements to allow offsite parking for the office building along with a 5,550 square foot expansion. In August 2004, the building permits for the office expansion were finalized.

912 20<sup>th</sup> Street Parking Lot: An automotive service garage was located on the site which was demolished in the 1990s. Remediation for hydrocarbon contamination was performed under the supervision of the Sacramento County Environmental Management Department. All storage tanks and conveyance lines were removed. Contaminated soil was excavated and backfilled. Soil vapor extraction and treatment systems were installed at the site in October of 1997. In February of 2002, design review staff approved a surface parking lot (DR01-175). The parking lot was also reviewed in 2002 by the Planning Commission which also approved tandem parking, an increase in the number of compact spaces, and reduction of maneuvering area for the site. In November 2002, building permits were finalized for the surface parking lot. The parking lot was conditioned under file number P02-137, with a five year term limit which expired on September 30, 2007. After this date, the intention was that the site would be redeveloped to an office or retail use. The site is currently developed with a 56 space surface parking lot.

**Public/Neighborhood Outreach and Comments:** Staff sent early notices to various community groups including Boulevard Park, Marshall School New Era Park, Midtown (formerly Winn Park) Neighborhood Association, and the Midtown Business Association. No comments were received about the project. Public notices were also mailed out to property owners within 500 feet of both the office building and offsite parking lot sites. Staff did not receive any comments at the time of writing this report.

**Environmental Considerations:** The Community Development Department, Environmental Planning Services Division has reviewed this project and determined that

this is exempt from the provisions of the California Environmental Quality Act (CEQA) Section 15301, Existing Facilities. The project consists of the operation of existing facilities and there is no expansion of the existing use.

***Policy Considerations:*** The 2030 General Plan Update was adopted by City Council on March 3, 2009. The 2030 General Plan's goals, policies, and implementation programs define a roadmap to achieving Sacramento's vision to be the most livable city in America. The 2030 General Plan Update designation of the subject site is Urban Corridor Low which provides for retail, service, office and residential uses. The proposed project meets the 2030 General Plan policies and development in the Urban Corridor Low designation.

The proposal is consistent with the following General Plan policies:

*Appropriate Parking. The City shall ensure that appropriate parking is provided, considering access to existing and funded transit, shared parking opportunities for mixed-use development, and implementation of Transportation Demand Management plans. (M6.1.1)* Staff finds the offsite parking lot will provide parking for an existing office building that is consistent with the current minimum and maximum code requirements for office buildings in the General Commercial (C-2) zone.

*Reduction of Parking Areas. The City shall strive to reduce the amount of land devoted to parking through such measures as development of parking structures, the application of shared parking for mixed use developments, and the implementation of Transportation Management plans to reduce parking needs. (M6.1.4)* Staff finds that the proposal will allow for the continuation of a developed parking lot to be utilized for an existing office building which reduces the creation of more parking in the Central City closer to the office building site.

*Development Intensity at Less than the Minimum Floor-Area-Ratio (FAR). The City shall permit development at less than the required minimum FAR if only a ministerial permit is required. Where a discretionary permit is required, a development with a FAR at less than the required minimum may be deemed consistent with the General Plan if the City finds that (1) the use involves no building or by its nature normally conducts a substantial amount of its operations outdoors, or (2) the initial site development is being phased and an overall development plan demonstrates compliance with the FAR standard, or (3) the use is temporary and would not interfere with long-term development of the site consistent with the FAR standard, or (4) the building size or lot coverage is constrained beyond what is otherwise allowed by the zoning designation of the site, due to the existence of an overlay zone or because of environmental features, such as wetlands. (LU 1.1.13)* Staff finds that the parking lot use meets the above criteria to be exempt from the FAR requirement because the parking lot use does not involve a building and the parking lot is existing with no changes proposed.

### **Zoning Code:**

Offices are allowed in the General Commercial (C-2 UN) and located in the Urban Neighborhood Overlay zone. Offsite parking lots are generally allowed with a Zoning

Administrator Special Permit. If the building and the offsite parking lot are owned by the same owner, the distance from the building and the lot may not exceed 300 feet. If the building and the offsite lot are owned by different owners, the distance from the building and the offsite parking lot may not exceed 1,000 feet. The proposed offsite parking lot at 912 20<sup>th</sup> Street is located approximately 574 feet from the existing office building. At the time of obtaining entitlements in 2002, the sites were under separate ownership therefore a Planning Commission Special Permit was obtained. However, since both sites are owned by the same owners at the time of writing this report, a Special Permit cannot be issued since the sites are located more than 300 feet apart. Therefore a Planning Commission Variance is necessary to establish an offsite parking lot that is over the maximum 300 foot distance requirement.

## Parking

The 20,600 square foot office building was originally constructed with eight onsite parking spaces. At the time of the office building construction, the site was zoned Central Business District (C-3) and the Zoning Code in effect did not require minimum onsite parking for office buildings. The site was later rezoned to General Commercial (C-2) which would have required a minimum of 46 parking spaces but since the site was already developed, it was deemed to have a parking credit of 38 spaces.

In 2004, a 5,550 square foot office addition was completed which required a minimum of 12 parking spaces because the site was zoned General Commercial (C-2) and the Zoning Code required a minimum of one parking space per 450 square feet for the new square footage.

The applicant is required to provide a minimum of 20 parking spaces for their use which includes the original 8 onsite parking spaces and the 12 parking spaces for the expansion. However, the applicant has requested to provide the required amount of parking for offices constructed under current standards even though the building has a parking credit of 38 parking spaces.

The applicant obtained entitlements to provide offsite parking for the entire office building (both the square footage with the parking credit and the addition) at 912 20<sup>th</sup> Street. However, there was a term time limit placed on the parking lot which has expired. Therefore, the applicant is required to obtain new entitlements.

**Table 2: Parking for 1831 K Street**

Land Use	Required Parking	Proposed Parking	Difference
Office*	58 to 65 spaces**	64 spaces***	No

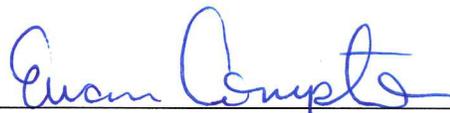
\*Offices within the Central City but outside of the Central Business District are required to provide not less than 1 parking space per 450 square feet but not more than 1 space per 400 square feet.

\*\*When calculating required parking, the 38 space parking credit would allow the applicant to provide only a minimum of 20 required spaces.

\*\*\*This includes eight parking spaces onsite and 56 parking spaces provided offsite at 912 20<sup>th</sup> Street.

The area surrounding 1831 K Street is developed with residential, retail, and office uses. Staff supports the request to provide adequate parking for the 1831 K Street office use because it will discourage employees who need to drive to work from using the onstreet parking which is also needed for high turnover retail customers and/or residents in the parking permit area.

**Recommendation:** Staff recommends the Commission approve the requested offsite parking variance based on the findings and subject to the conditions listed in Attachment 1 because the project: a) allows for a building with substandard parking to be brought into conformance with current parking standards; b) utilizes an existing, developed parking lot to fulfill required and nonrequired parking spaces instead of creating more parking at another site closer to the existing office building at 1831 K Street; c) encourages employees of 1831 K Street to use offstreet parking which will increase onstreet parking supply in the area for nearby retail and residential users; d) is consistent with the prior approval to allow the site to be used as an offsite parking lot for the office building.

Respectfully submitted by:   
EVAN COMPTON  
Associate Planner

Approved by:   
STACIA COSGROVE  
Senior Planner

Recommendation Approved:

  
GREGORY BITTER, AICP  
Principal Planner

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Page 7	Attachment 1 – Proposed Findings of Fact and Conditions of Approval
Page 9	Exhibit A – 1831 K Street Floor Plan for Ground Level
Page 10	Exhibit B – 1831 K Street Floor Plan for Second Level
Page 11	Exhibit C – 912 20 <sup>th</sup> Street Parking Lot (Existing Improvements)

**Attachment 1**  
**Proposed Findings of Fact and Conditions of Approval**  
**1831 K Street Offsite Parking Variance (P11-019)**  
**912 20<sup>th</sup> Street**

**Findings of Fact**

**A. Environmental Determination: Exemption**

Based on the determination and recommendation of the City's Environmental Planning Services Manager and the oral and documentary evidence received at the hearing on the Project, the Planning Commission finds that the Project is exempt from review under Section **15301, Existing Facilities** of the California Environmental Quality Act Guidelines as follows: The project consists of the operation of existing facilities and there is no expansion of the existing use.

**B. The Variance** to allow an offsite parking lot at 912 20<sup>th</sup> Street for required and nonrequired parking for an office building at 1831 K Street which is more than 300 feet away and under the same ownership is **approved** subject to the following Findings of Fact:

1. Granting the variance does not result in a special privilege to one individual property owner in that the number of parking spaces provided on the offsite parking lot does not exceed the maximum amount of parking allowed for the existing office building at 1831 K Street per the Zoning Code;
2. Granting the Variance request does not constitute a use variance in that a surface parking lot is an allowed use in the General Commercial (C-2) zone;
3. Granting the requested variance will not materially and adversely affect the health and safety of persons residing or working in the neighborhood, and will not be materially detrimental to the public welfare or injurious to property and improvements in the neighborhood since the offsite parking lot has been historically used for employees of the office building at 1831 K Street and allowing the continuation of the offsite parking will discourage development of more surface parking near the office building and assist to preserve the amount of onstreet parking supply for retail and residential users near the office building at 1831 K Street; and
4. The variance is consistent with the General Plan policies of the Urban Corridor Low because the existing parking lot is developed with landscaping and lighting which was approved by design review staff and there is only one curb cut on I Street and access from the alley which

enhances the pedestrian friendliness of both 20<sup>th</sup> and I Streets.

**Conditions of Approval**

- B.** The **Variance** to allow an offsite parking lot at 912 20<sup>th</sup> Street for required and nonrequired parking for an office building at 1831 K Street which is more than 300 feet away and under the same ownership is **approved** subject to the following Conditions of Approval:
- B1. Twelve (12) parking spaces shall be provided in perpetuity at the 912 20<sup>th</sup> Street site for the 1831 K Street office building.
  - B2. The project shall comply with all previous conditions of approval from DR02-177.
  - B3. A signed copy of the Affidavit of Zoning Code Development Standards and each of the pages of this Record of Decision shall be scanned and inserted as a general sheet(s) in the plan set for any building permit submittal associated with this project.

**Advisories**

Planning

- 1. Staff would encourage the applicant to consider allowing compatible uses the ability to utilize the 44 nonrequired parking spaces in the parking lot during the evenings and weekends when the office building is closed.

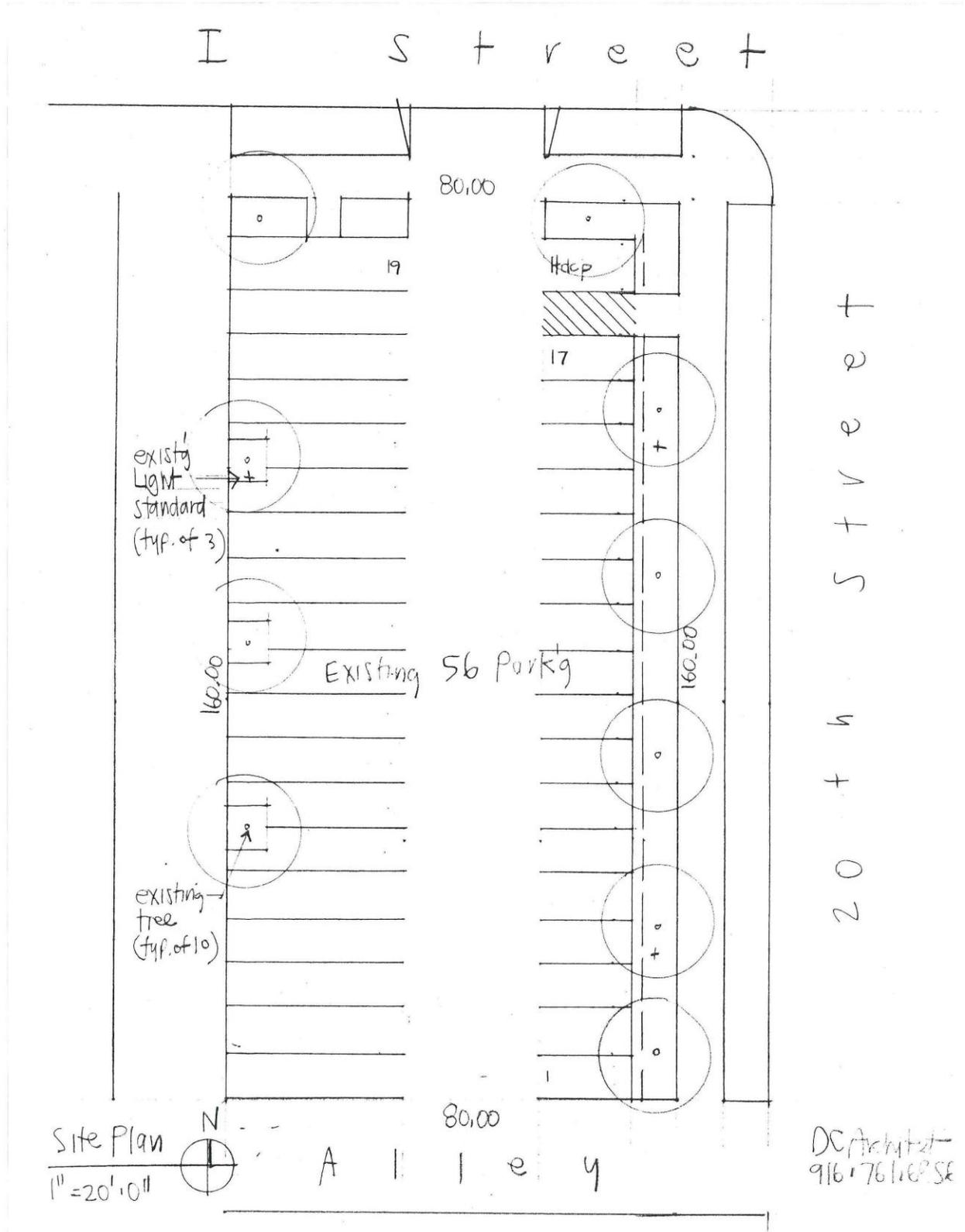
Building

- 2. There are 56 existing parking stalls. Provide 3 accessible parking stalls including one 8 foot wide loading and unloading space per CBC Table 11B-6 and CBC section 1129B.3.
- 3. Show the path of travel from offsite parking to the office building at 1831 K Street on a site plan. Also verify that the path of travel from the offsite parking stalls, and along the accessible route of travel to the main entrance of office building at 1831 K Street, is accessible.





Exhibit C – 912 20<sup>th</sup> Street Parking Lot (Existing Improvements)



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# REPORT TO PLANNING COMMISSION City of Sacramento

# 6

915 I Street, Sacramento, CA 95814-2671

PUBLIC HEARING  
March 24, 2011

To: Members of the Planning Commission

**Subject: Truxel 3 PUD Development Agreement Amendment (P11-021)**

A request to amend City Agreement No. 96-051, which is the development agreement for the Truxel 3 PUD, located in the Highway Commercial Planned Unit Development (HC-PUD) zone.

- A. Environmental Determination: Previously Adopted Mitigated Negative Declaration;
- B. Previously Adopted Mitigation Monitoring Plan; and
- C. Amend City Agreement No. 96-051 (the development agreement for the Truxel 3 PUD) to extend the initial term.

**Location/Council District**

3500 Truxel RD (Northeast corner of Truxel Road and Gateway Park Boulevard)  
Assessor's Parcel Number 225-2110-048-000  
Council District 1

**Recommendation**

Staff recommends the Commission approve the request based on the findings and subject to the conditions listed in Attachment 1. **At the writing of this report, the project is non-controversial.** Staff recommends the Commission forward to City Council a recommendation of approval for items A to C.

**Contact** Lindsey Alagozian, Senior Planner, (916) 808-2659;

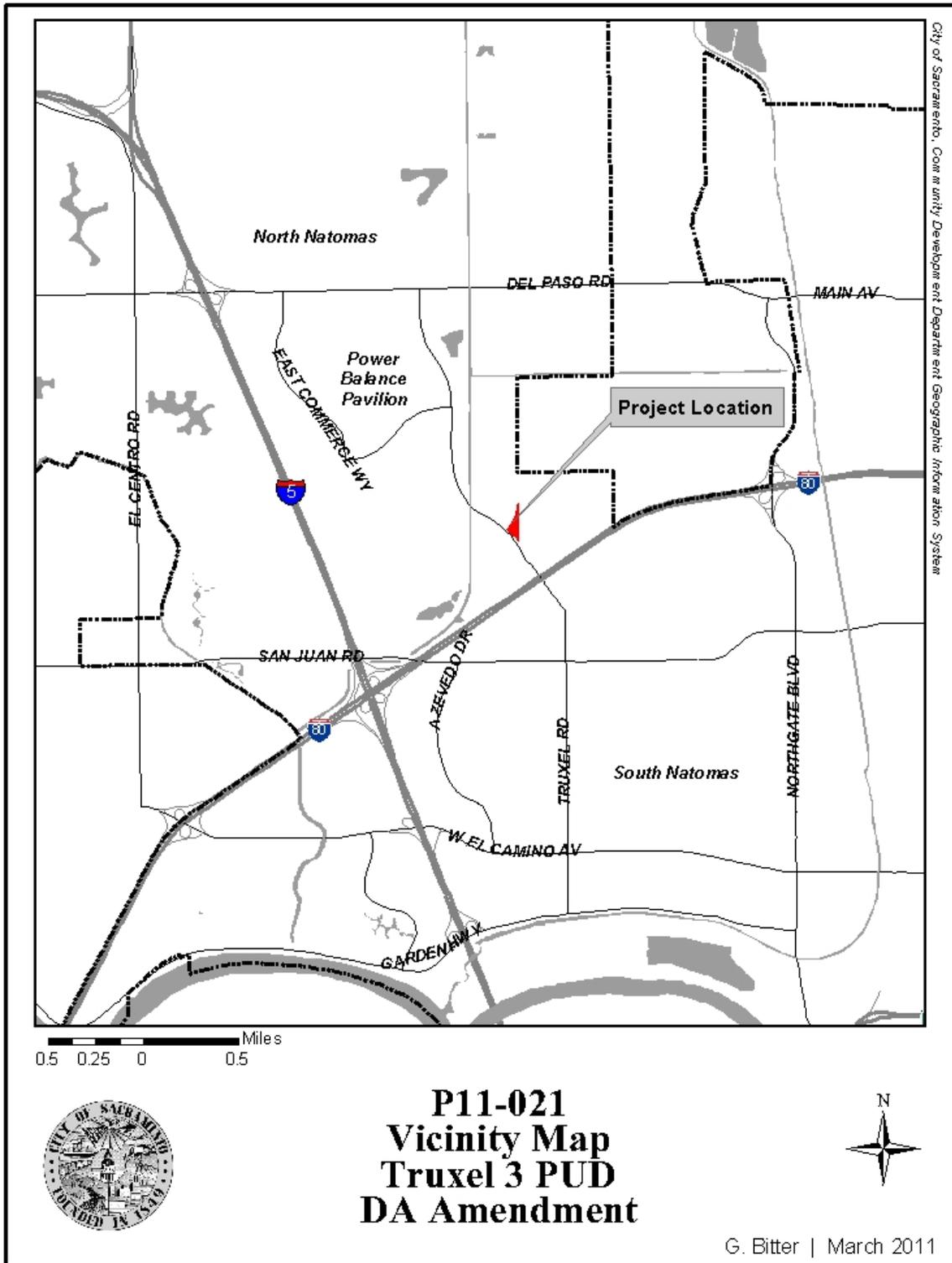
Greg Bitter, AICP, Principal Planner (916) 808-2659

**Applicant** KKP Kim Properties Lincoln, LLC c/o Sang Kim, (916) 780-6670

3300 Douglas Blvd., Suite 385, Roseville, CA 95661

**Owner** AJ Ventures Incorporated, c/o Jeff Owen, (707)-524-3020  
545 Fourth Street, Santa Rosa, CA, 95401

Vicinity Map



## Summary

The subject property is currently entitled to allow the development of a sit-down restaurant and a fast-food restaurant with an associated drive-through (P05-022). The subject property is also subject to a development agreement (DA) that was approved on April 15, 1996, and became effective on May 15, 1996 (City Agreement 96-051). Although the initial term of this DA expires on May 15, 2011, the DA allows for three extensions of five years each, with a provision that a letter requesting an extension be submitted to the City 180 days prior to the expiration of the DA. Because the property was the subject of bankruptcy proceedings, the ownership of the property was in flux and the right to extend the initial term expired on November 15, 2010, approximately two months after A.J. Ventures, Inc., acquired title to the property. The applicant is now requesting to extend the term of the DA by five years. Staff notified all property owners within 500 feet of the site for this public hearing and received no opposition at the writing of this report.

<b>Table 1: Project Information</b>
<b>General Plan designation:</b> Regional Commercial
<b>Existing zoning of site:</b> HC-PUD
<b>Existing use of site:</b> Partially developed (non-completed structures)
<b>Property area:</b> 3.2± gross acre (2.93± net acre)

## Background Information

On April 16, 1996, the City Council approved various entitlements for the Natomas Marketplace project (P95-074). One of these entitlements was a standard North Natomas Development Agreement (DA), approved by Ordinance 96-014, that covered not only the property developed as the Natomas Marketplace but also the property currently known as the Truxel 3 Planned Unit Development.

On September 23, 2003, the City Council adopted an ordinance amending the districts established by the Comprehensive Zoning Ordinance (title 17 of the City Code) from 5.0± gross acres of Manufacturing Research and Development-20 Planned Unit Development (MRD-20 PUD) to 5.0± gross acres of Highway Commercial Planned Unit Development (HC-PUD) and a resolution to designate the 5.0± gross acre (2.8± net acre) site as the Truxel 3 Planned Unit Development with a Planned Unit Development Schematic Plan and Guidelines for the site (P00-123). The approved PUD Guidelines state the uses allowed on this site include a fast food restaurant with drive-through facility.

On October 13, 2005, the City Planning Commission (1) approved a tentative map to subdivide one parcel into two in the Truxel 3 Planned Unit Development, (2) approved special permits to develop a 7,308± square-foot sit-down restaurant and a fast food restaurant, and (3) denied a special permit for a drive-through service facility on 3.2± gross acres in the Highway Commercial Planned Unit Development (HC-PUD) zone. The denial of the special permit for the drive-through service facility was subsequently appealed, and the City Council approved the permit on December 13, 2005.

Construction of the approved project commenced with site preparation in 2007. But the project was slowed by economic conditions, and the initial construction of the buildings did not commence until late 2008. Ultimately, construction was put on hold and the current conditions of the site include a completed parking area and two partially constructed structures. These structures are now scheduled for demolition due to an active dangerous-buildings case.

Beginning in 2009, the land known as the Truxel 3 Planned Unit Development was the subject of bankruptcy proceedings and was ultimately placed in the ownership of A.J. Ventures, Inc. During this time, the landowner's rights to extend the term of the DA expired (on November 15, 2010). A. J. Ventures, Inc. is now requesting an amendment to the DA that would extend the initial term five years and would grant it the right to two additional five-year extensions. The total term of the DA will remain as it is now, 30 years.

### **Public/Neighborhood Outreach and Comments**

The proposed amendment to the DA does not impact the current development entitlements for the site. This proposal was not subject to an early routing to neighborhood groups, however the public notice for the Planning Commission's meeting was routed to all property owners within a 500 foot radius of the project site. Any modifications to the existing entitlements will be subject to early review.

### **Environmental Considerations**

The Environmental Services Manager has reviewed the project for compliance with the requirements of the California Environmental Quality Act (CEQA). The project falls within the scope of the mitigated negative declaration for Truxel 3 PUD (P00-123) which the City Council approved on September 23, 2003, and amended (P05-022) on December 13, 2005. The proposed amendment will not result in new significant environmental effects or a substantial increase in the severity of previously identified significant effects considered in the approved mitigated negative declaration. Pursuant to CEQA Guideline 15162, a subsequent mitigated negative declaration is not required.

The adopted Mitigated Negative Declaration for the Truxel 3 PUD project is available at the Community Development Department's webpage located at the following link:

<http://www.cityofsacramento.org/dsd/planning/environmental-review/eirs/>

### **Policy Considerations**

#### General Plan

The 2030 General Plan designation of the subject site is Regional Commercial. The proposal to extend the term of the DA will allow development of a project previously found to be consistent with the City's General Plan and currently consistent with the policies of the 2030 General Plan for Regional Commercial uses.

North Natomas Community Plan

The policies contained in the North Natomas Community Plan, found within Part 3 of the 2030 General Plan, are organized to mirror the structure of the Citywide General Plan elements and are intended to supplement, but not repeat, Citywide policies. The proposal to extend the term of the DA will allow the development of a project previously found to be consistent with the North Natomas Community Plan.

**Development Agreement Amendment**

The Council-adopted North Natomas Processing Protocols require all development in the North Natomas Community Plan area to enter into a standard DA with the City. The City Council approved the standard DA format on August 9, 1994 (Resolution No. 94-494). On April 16, 1996, the City Council approved various entitlements for the Natomas Marketplace project (P95-074). One of these entitlements was a DA (Ordinance 96-014, City Agreement 96-051) that included not only the property developed as the Natomas Marketplace but also the property currently known as the Truxel 3 Planned Unit Development.

The standard DA allows termination upon the conclusion of development. Before a DA can be terminated, the City must find that a parcel has been fully developed and all of the landowner's obligations (e.g., land dedication, payment of fees) have been satisfied. In the case of the Truxel 3 PUD, development has not been completed, and all of the landowner's obligations have not been satisfied.

The DA allows for an initial term of 15 years and three extensions of five years each, with a provision that a letter requesting an extension be submitted to the City 180 days prior to the expiration of the DA. As described above, the landowner's right to extend the initial term of the DA expired on November 15, 2010. The applicant is now requesting to extend the initial term of the DA by five years and limit subsequent extensions to two terms of five years each. The maximum term of the DA will remain at 30 years.

Extending the term of the DA will provide the City with a continuing contractual obligation that development of the Truxel 3 PUD will fulfill the original obligations imposed on this property. This amendment will also give the landowner, A. J. Ventures, Inc., certainty as to continuing obligations that must be satisfied to complete development of the property. The applicant has coordinated with the City Attorney's Office to complete this amendment to the DA. Planning staff recommends the Planning Commission recommend approval of the this amendment to the DA.

**Conclusion**

Staff recommends the Commission forward to City Council a recommendation of approval for the proposed amendment to the DA. Staff finds that the proposed amendment is consistent with (1) the policies of the General Plan and the North Natomas Community Plan; and (2) the North Natomas Processing Protocols.

Respectfully submitted by:   
 \_\_\_\_\_  
 LINDSEY ALAGOZIAN  
 Senior Planner

Approved by:   
 \_\_\_\_\_  
 LINDSEY ALAGOZIAN  
 Senior Planner

Recommendation Approved:  
  
 \_\_\_\_\_  
 GREGORY BITTER, AICP  
 Principal Planner

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Exhibit 3A	First Amendment to City Agreement No. 96-051	Page 20

**Attachment 1  
Proposed Findings of Fact and Conditions of Approval  
Amendment to Truxel 3 Development Agreement (P11-021)  
3500 Truxel Road**

**Findings of Fact**

- A&B.** The Planning Commission has reviewed and considered the information contained in the Mitigated Negative Declaration and Mitigation Monitoring Plan in making the recommendations set forth below.
- C.** The Planning Commission recommends approval and forwards to the City Council the First Amendment to City Agreement No. 96-051 set forth in Attachment 3.

**Attachment 2 Mitigated Negative Declaration – Findings – Draft Resolution**

**RESOLUTION NO. 2011-**

Adopted by the Sacramento City Council

**RE-ADOPTING THE MITIGATED NEGATIVE DECLARATION AND ADDENDUM AND  
RE-ADOPTING THE MITIGATION MONITORING PLAN IN CONNECTION WITH AN  
AMENDMENT TO THE DEVELOPMENT AGREEMENT FOR TRUXEL 3 PUD  
PROJECT (P11-021)**

**BACKGROUND**

- A. On March 24, 2011 the City Planning Commission conducted a public hearing on, and forwarded to the City Council a recommendation to approve with conditions the proposed amendment to the development agreement for the Truxel 3 PUD (City Agreement No. 96-051)(the “**Project**”).
- B. On April 12, 2011, the City Council conducted a public hearing, for which notice was given pursuant Sacramento City Code section 17.200.010(C)(1) (a), (b), and (c) (publication, posting, and mail [500 feet]), and received and considered evidence and testimony concerning the Project.

**BASED ON THE FACTS SET FORTH IN THE BACKGROUND, THE CITY COUNCIL  
RESOLVES AS FOLLOWS:**

Section 1. The City Council finds as follows:

- A. On September 23, 2003, pursuant to the California Environmental Quality Act (Public Resources Code §21000 et seq. (“CEQA”), the CEQA Guidelines (14 California Code of Regulations §15000 et seq.), and the City of Sacramento environmental guidelines, the City Council adopted a mitigated negative declaration (MND) and a mitigation-monitoring program and approved the Truxel 3 Planned Unit Development (P00-123)(Resolution 2003-666).
- B. On December 13, 2005, pursuant to the California Environmental Quality Act (Public Resources Code §21000 et seq. (“CEQA”), the CEQA Guidelines (14 California Code of Regulations §15000 et seq.), and the City of Sacramento environmental guidelines, the City Council considered the adopted mitigated negative declaration (MND) as amended with an addendum for the Truxel 3 Planned Unit Development (P05-022)(Resolution 2005-914).
- C. The Project does not require the preparation of a subsequent environmental impact report or negative declaration.

- Section 2. In reviewing the Project, the City Council has reviewed and considered the information contained in the previously adopted MND, the addendum for the Truxel 3 Planned Unit Development, and all oral and documentary evidence received during the hearing on the Project. The City Council had determined that the previously adopted MND as amended constitutes an adequate, accurate, objective, and complete review of the proposed Project and finds that no additional environmental review is required based on the reasons set forth below:
- A. The Project involves no substantial changes that will require major revisions of the previously adopted MND because of new significant environmental effects or a substantial increase in the severity of previously identified significant effects.
  - B. No substantial changes have occurred with respect to the circumstances under which the Project will be undertaken which will require major revisions to the previously adopted MND because of new significant environmental effects or a substantial increase in the severity of previously identified significant effects.
  - C. No new information of substantial importance has been found that shows any of the following:
    - 1. The Project will have one or more significant effects not discussed in the previously adopted MND;
    - 2. Significant effects previously examined will be substantially more severe than shown in the previously adopted MND;
    - 3. Mitigation measures previously found to be infeasible would in fact be feasible and would substantially reduce one or more significant effects of the Project; or
    - 4. Mitigation measures which are considerably different from those analyzed in the previously adopted MND would substantially reduce one or more significant effects on the environment.
- Section 3. In connection with its consideration of the Project, and based on its review of the previously adopted MND, the addendum for the Truxel 3 Planned Unit Development, and all oral and documentary evidence received during the hearing on the Project, the City Council finds that the MND and addendum reflect the City Council's independent judgment and analysis and re-adopts the MND as amended.
- Section 4. The mitigation monitoring program is adopted for the Project, and the mitigation measures shall be implemented and monitored as set forth in the program, based on the following findings of fact:

1. The mitigation monitoring program has been adopted and implemented as part of the Project;
2. The addendum to the MND does not include any new mitigation measures, and has not eliminated or modified any of the mitigation measures included in the mitigation monitoring program;
3. The mitigation monitoring plan meets the requirements of CEQA section 21081.6 and CEQA Guideline 15074.

Section 5 Upon approval of the Project, the City's Environmental Planning Services shall file or cause to be filed a Notice of Determination with the Sacramento County Clerk and, if the Project requires a discretionary approval from any state agency, with the State Office of Planning and Research, pursuant to section 21152(a) of the Public Resources Code and the State EIR Guidelines adopted pursuant thereto.

Section 6. Pursuant to Guidelines section 15091(e), the documents and other materials that constitute the record of proceedings upon which the City Council has based its decision are located in and may be obtained from, the Office of the City Clerk at 915 I Street, Sacramento, California. The City Clerk is the custodian of records for all matters before the City Council.

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Exhibit A Mitigation Monitoring Program (Resolution 2003-666)

# RESOLUTION NO. 2003-666

ADOPTED BY THE SACRAMENTO CITY COUNCIL

ON DATE OF SEP 23 2003

**A RESOLUTION RATIFYING THE MITIGATED NEGATIVE DECLARATION AND ADOPTING THE MITIGATION MONITORING PLAN FOR TRUXEL 3 PLANNED UNIT DEVELOPMENT, LOCATED AT THE SOUTHEAST CORNER OF TRUXEL ROAD AND GATEWAY PARK BOULEVARD, SACRAMENTO, CALIFORNIA.**

(APN: 225-0170-043)  
(P00-123)

**WHEREAS**, the Environmental Coordinator has prepared an Initial Study and Mitigated Negative Declaration for the above identified project;

**WHEREAS**, the Initial Study and Mitigated Negative Declaration was prepared and circulated for the above-identified project pursuant to the requirements of CEQA;

**WHEREAS**, the proposed Negative Declaration and comments received during the public review process were considered prior to action being taken on the project;

**WHEREAS**, based upon the Negative Declaration and the comments received during the public review process, there is no substantial evidence that the project will have a significant effect on the environment, provided that mitigation measures are added to the above identified project.

**WHEREAS**, this Mitigated Negative Declaration reflects the lead agency's independent judgment and analysis;

**WHEREAS**, the Environmental Coordinator has prepared a Mitigation Monitoring Plan for ensuring compliance and implementation of the mitigation measures as prescribed in the Initial Study for the above identified project; and

**WHEREAS**, in accordance with Section 21081.6 of the California Public Resources Code, the City of Sacramento requires that a Mitigation Monitoring Plan be developed for implementing mitigation measures as identified in the Initial Study for the project;

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**FOR CITY CLERK USE ONLY**

RESOLUTION NO.: 2003-666

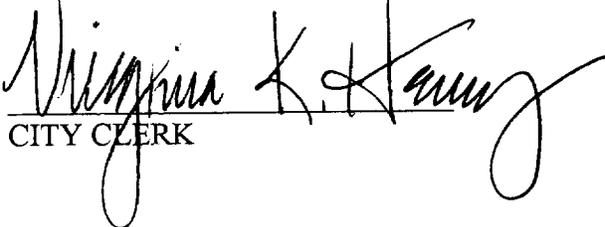
DATE ADOPTED: SEP 23 2003

**NOW, THEREFORE BE IT RESOLVED BY THE COUNCIL OF THE CITY OF SACRAMENTO THAT:**

1. The Negative Declaration for Truxel 3 Planned Unit Development (P00-1213) be ratified.
2. The Mitigation Monitoring Plan is approved for the proposed Truxel 3 Planned Unit Development project based upon the following findings:
  - a. One or more mitigation measures have been added to the above identified project;
  - b. A Mitigation Monitoring Plan, has been prepared to ensure compliance and implementation of the mitigation measures for the above identified project, a copy of which is attached as Exhibit 1.

  
 \_\_\_\_\_  
 MAYOR

ATTEST:

  
 \_\_\_\_\_  
 CITY CLERK

P00-123

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**FOR CITY CLERK USE ONLY**

RESOLUTION NO.: 2003-666  
 DATE ADOPTED: SEP 23 2003

**EXHIBIT 1 – Mitigation Monitoring Plan****TRUXEL 3 PROJECT (P00-123)  
MITIGATION MONITORING PLAN**

This Mitigation Monitoring Plan (MMP) has been required by and prepared for the City of Sacramento Planning and Building Department, Environmental Planning Services, 1231 I Street, Room 300, Sacramento, CA 95814, pursuant to CEQA Guidelines Section 21081.6.

**SECTION 1: PROJECT IDENTIFICATION**

**Project Name / File Number:** Truxel 3 Project (P00-123)  
**Owner/Developer- Name:** Armrod Charitable Foundation, Eleni Tsakopoulos  
**Address:** 7700 College Town Drive, Suite 101  
 Sacramento, CA 95826

**Project Location / Legal Description of Property (if recorded):** The Project is located within the North Natomas Community Plan area. The project site is located at the southeast intersection of Truxel Road and Gateway Park Boulevard. (APN: 225-0170-043).

**Project Description:** The proposed Truxel 3 Project would consist of establishing a Planned Unit Development for developing approximately 5.0± gross acres (2.8 net acres) of vacant land for the purpose of constructing Highway Commercial uses. The Truxel 3 project would provide highway commercial uses for both the North Natomas Community and travelers of I-80. Appropriate off-street parking would be required in accordance with the City's Zoning Ordinance for projects being constructed within the Planned Unit Development.

Specific entitlements being requested for the proposed project include:

- A. Development Agreement
- B. Rezone - to Highway Commercial
- C. Planned Unit Development (PUD) Establishment (PUD Guidelines and PUD Schematic Plan)

**SECTION 2: GENERAL INFORMATION**

The Plan includes mitigation for Seismicity, Soils, and Geology; Air Quality; Biological Resources; and Cultural Resources. The intent of the Plan is to prescribe and enforce a means for properly and successfully implementing the mitigation measures as identified within the Initial Study for this project. Unless otherwise noted, the cost of implementing the mitigation measures as prescribed by this Plan shall be funded by the owner/developer identified above. This Mitigation Monitoring Plan (MMP) is designed to aid the City of Sacramento in its implementation and monitoring of mitigation measures adopted for the proposed project.

**FOR CITY CLERK USE ONLY**

RESOLUTION NO.: 2003-666

DATE ADOPTED: SEP 23 2003

**EXHIBIT 1 – Mitigation Monitoring Plan**

The mitigation measures have been taken verbatim from the Initial Study and are assigned the same number they have in the document. The MMP describes the actions that must take place to implement each mitigation measure, the timing of those actions, and the entities responsible for implementing and monitoring the actions. The developer will be responsible for fully understanding and effectively implementing the mitigation measures contained with the MMP. The City of Sacramento will be responsible for ensuring compliance.

**MITIGATION MONITORING PROGRAM****Introduction**

The California Environmental Quality Act (CEQA) requires review of any plan or project that could have significant adverse effects on the environment. In 1988, CEQA was amended to require reporting on and monitoring of mitigation measures adopted as part of the environmental review process. This MMP is designed to aid the City of Sacramento in its implementation and monitoring of mitigation measures adopted for the Proposed Project.

**MMP Components**

The components of each monitoring form are addressed briefly, below.

**Mitigation Measure:** All mitigation measures that were identified in the Initial Study are presented, and numbered accordingly. The mitigation measures are presented by topic (e.g., Air Quality).

**Implementing Responsibility:** This item identifies the entity that will undertake the required action.

**Monitoring Responsibility:** This item identifies the entity that will monitor the required action.

**Compliance Standards:** This item identifies the specific actions that are required in each mitigation measure.

**Timing:** Each action must take place prior to the time at which a threshold could be exceeded. Implementation of the action must occur prior to or during some part of approval, project design or construction, or on an ongoing basis. The timing for each measure is identified.

**Verification of Compliance:** The individual assigned to assure compliance with identified mitigation measures will initial the form when the measure has been successfully implemented. The individual assigned to assure compliance will date the form when the measure has been successfully implemented.

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**FOR CITY CLERK USE ONLY**RESOLUTION NO.: 2003-666DATE ADOPTED: SEP 23 2003  
14

Item #6

**EXHIBIT 1 - Mitigation Monitoring Plan**

**TRUXEL 3 PROJECT (P00-123)  
MITIGATION MONITORING PLAN**

Mitigation Measure		VERIFICATION OF COMPLIANCE				
		Implementing Responsibility	Monitoring Responsibility	Compliance Standards	Timing	Verification of Compliance (Initials/Date)
<b>3. Seismicity, Soils, and Geology:</b> SSG-1. If groundwater were encountered during excavation activities, pumped water shall be channeled to an infiltration basin, located within an upland area of the construction activities and would eventually percolate into the groundwater. Upon percolation of all pumped water, the infiltration basin shall be backfilled and revegetated or developed per City and Regional Water Quality Control Board requirements.		Applicant	City Planning & Building Department, Utilities, and Department of Public Works.	The listed measure shall be included on all construction plans.	Mitigation measures shall be implemented in the field during construction activities.	
<b>5. Air Quality:</b> AQ-1. Exposed soil shall be watered with adequate frequency to keep soil moist at all times. AQ-2. Loads of haul/dump trucks shall be covered securely. AQ-3. Any exposed piles of dirt, sand, gravel, or other construction debris shall be enclosed, covered, or watered twice daily. AQ-4. All dirt and mud which has been generated from or deposited by construction equipment going to and from the construction site along neighborhood streets shall be removed at a minimum of three times per week. AQ-5. Equipment idling shall be kept to a minimum when equipment is not in use. No piece of equipment shall be left to idle in one place for more than 30 minutes. AQ-6. On-site vehicle speeds shall be limited to 15 miles per hour on unpaved surfaces.		Applicant	City Planning & Building Department, Public Works and SMAQMD	The applicant shall include the listed measures on all grading plans (the City shall not approve any construction plans without them).	Mitigation measures shall be implemented in the field during grading and construction activities	

**FOR CITY CLERK USE ONLY**

RESOLUTION NO.: 2003-666

DATE ADOPTED: SEP 23 2003

**EXHIBIT 1 – Mitigation Monitoring Plan**

**TRUXEL 3 PROJECT (P00-123)  
MITIGATION MONITORING PLAN**

Mitigation Measure		VERIFICATION OF COMPLIANCE			
		Implementing Responsibility	Monitoring Responsibility	Compliance Standards	Timing
AQ-7.	Revegetate disturbed areas immediately after the completion of construction to reduce wind erosion.				
<b>7. Biological:</b>					
BR-1.	Prior to the issuance of a grading permit, the applicant shall either: (i) provide ½ acre of mitigation land that meets the requirements of the Natomas Basin Habitat Conservation Plan (NBHCP) for each acre of land authorized for disturbance; or (ii) pay the required NBHCP fees. No permit can be issued unless one of these has occurred. If the applicant acquires land and transfers it to the Conservancy, the applicant must pay that portion of the NBHCP fees other than the acquisition portion. Applicant land acquisitions must be approved in advance by the Conservancy.	Applicant	City Planning & Building Department; Department of Public Works, The Natomas Basin Conservancy, CA Dept. Fish & Game, U.S. Fish & Wildlife Service.	Mitigation Measures, including construction-timing restrictions shall be included on the Construction Specifications. Pre-construction biological surveys shall be completed as specified and submitted with grading/building plans. The applicant shall comply with all requirements of the NBHCP.	Prior to issuance of any grading or building permit, measures identified on plans shall be verified for compliance. The Building Division and Dept of Public Works shall assure that measures are identified on construction plans and confirm compliance prior to issuance of any grading or building permit. Measures shall also be implemented concurrent with construction activities.
BR-2.	A pre-construction survey shall be completed by a qualified biologist in order to determine the presence and status of special-status species and their habitats within the project area, including Swainson's hawk, western burrowing owl, white-tailed kite, and tricolored blackbird. The results of the pre-construction surveys along with recommended take minimization measures shall be documented in a report and submitted to the USFWS and the CDFG. If necessary, the City shall implement specific take minimization measures as directed by the CDFG and the USFWS.				
BR-3.	The project applicant/developer shall: (1) comply with all requirements of the NBHCP, together with any additional requirements specified in the North Natomas Community Plan EIR; (2) comply with any additional mitigation measures identified in the NBHCP EIR/EIS; and (3) comply with all conditions in the incidental take permits issued by the USFWS and CDFG.				

**FOR CITY CLERK USE ONLY**

RESOLUTION NO.: 2003-666

DATE ADOPTED: SEP 23 2003

**EXHIBIT 1 – Mitigation Monitoring Plan**

**TRUXEL 3 PROJECT (P00-123)  
MITIGATION MONITORING PLAN**

Mitigation Measure	VERIFICATION OF COMPLIANCE		
	Implementing Responsibility	Monitoring Responsibility	Compliance Standards
<p><b>11. Cultural Resources:</b></p> <p>CR-1. If subsurface archaeological, historical, or paleontological remains are discovered during construction, work in the area of the find shall stop immediately. A qualified archaeologist and a representative of the Native American Heritage Commission shall be consulted to develop, if necessary, further mitigation measures to reduce cultural resources impacts to a less-than-significant level before construction continues.</p> <p>CR-2. If human burials are encountered, all work in the area of the find shall stop immediately and the Sacramento County Coroner's office shall be notified. If the remains are determined to be Native American in origin, both the Native American Heritage Commission and any identified descendants would be notified and recommendations for treatment solicited (CEQA Section 15064.5; Health and Safety Code Section 7050.5; Public Resources Code Section 5097.94 and 5097.98).</p>	Applicant	City Planning & Building Department, Department of Public Works	<p>Notes shall be included on the Construction Specifications.</p> <p>Measures shall be implemented in field during grading and construction activities.</p>
			Verification of Compliance (Initials/Date)

**FOR CITY CLERK USE ONLY**

RESOLUTION NO.: 2003-666

DATE ADOPTED: SEP 23 2003

**Attachment 3 Development Agreement Amendment Ordinance**

**ORDINANCE NO.**

Adopted By the Sacramento City Council

April 12, 2011

**AN ORDINANCE RELATING TO THE APPROVAL OF AN AMENDMENT TO A DEVELOPMENT AGREEMENT BETWEEN THE CITY OF SACRAMENTO AND A. J. VENTURES, INCORPORATED, A CALIFORNIA CORPORATION, FOR PROPERTY LOCATED IN NORTH NATOMAS, LOCATED AT THE SOUTHEAST CORNER OF TRUXEL ROAD AND GATEWAY PARK BOULEVARD, SACRAMENTO, CALIFORNIA. (APN: 225-0170-043) (P11-021)**

**BE IT ENACTED BY THE COUNCIL OF THE CITY OF SACRAMENTO:**

**SECTION 1**

- A. This Ordinance incorporates, and by this reference makes part hereof, that certain Development Agreement, by and between the City of Sacramento and A. J. Ventures, Incorporated, a copy of which is attached hereto.
- B. On March 24, 2011, the Planning Commission conducted a noticed public hearing on the application to amend the Development Agreement in accordance with Government Code Section 65867, and received and considered evidence, and forwarded to the City Council a recommendation to approve the amendment to said Development Agreement.
- C. On April 12, 2011, the City Council conducted a noticed public hearing on the application to amend the Development Agreement in accordance with Government Code Section 65867, and received and considered evidence concerning the amendment to said Development Agreement.

**SECTION 2**

The City Council finds:

- A. The amended agreement is consistent with the city general plan and the goals, policies, standards and objectives of any applicable specific or community plan;
- B. The project should be encouraged in order to meet important economic, social, environmental or planning goals of any applicable specific or community plan;
- C. The project would be unlikely to proceed in the manner proposed in the absence of a development agreement;

- D. The landowner will incur substantial costs in order to provide public improvements, facilities or services from which the general public will benefit;
- E. The landowner will participate in all programs established and/or required under the general plan or any applicable specific or community plan and all of its approving resolutions (including any mitigation monitoring plan), and has agreed to financial participation required under any applicable financing plan and its implementation measures, all of which will accrue to the benefit of the public;
- F. The landowner has made commitments to a high standard of quality and has agreed to all applicable land use and development regulations.

### SECTION 3

The Development Agreement Amendment attached hereto is hereby approved, and the Mayor is authorized to execute after the effective date of this Ordinance said Development Agreement Amendment on behalf of the City of Sacramento. This approval and authorization is based upon the re-adoption of a previously adopted Mitigated Negative Declaration and previously adopted Mitigation Monitoring Plan which is the subject of a separate resolution adopted by City Council prior to or concurrent with the adoption of this Ordinance.

#### Table of Contents

Exhibit A      Development Agreement Amendment

Recorded for the benefit of the City of Sacramento and thus exempt from documentary-transfer tax under Revenue and Taxation Code section 11928 and from recording fees under Government Code section 6103.

***When recorded, return to—***

Office of the City Clerk  
 Historic City Hall  
 915 "I" Street, First Floor  
 Sacramento, CA 95814

SPACE ABOVE THIS LINE IS FOR RECORDER'S USE ONLY

**First Amendment to City Agreement No. 96-051  
 North Natomas Development Agreement  
 Truxel 3 Planned Unit Development**

This amendatory agreement, dated April 12, 2011, for purposes of identification, is between the City of Sacramento, a California municipal corporation (the "**City**"); and A. J. Ventures, Inc., a California corporation ("**Landowner**").

**Background**

- A. On June 12, 1996, the City and Gateway Truxel Partners, a California general partnership ("**Gateway**"), entered into a North Natomas Development Agreement that is designated as City Agreement No. 96-051 and is recorded with the Sacramento County Recorder in Book 19960701 at Page 0470 (the "**Original Agreement**"). The Original Agreement covers the real property described in Exhibit A to this amendatory agreement.
- B. Landowner is the successor in interest to Gateway with respect to the real property described in Exhibit A (the "**Landowner's Parcel**"). Landowner acquired title to the Landowner's Parcel on September 17, 2010, by way of a *Trustee's Deed Upon Sale* that is recorded with the Sacramento County Recorder in Book 20100917 at Page 0216.
- C. The initial fifteen-year term of the Development Agreement expires on May 15, 2011. Section 3 in article II of the Original Agreement grants Gateway and its successors in interest the right to extend the initial term by giving the City notice at least 180 days before the initial term expires. But neither Gateway nor Landowner has exercised that right, which expired on November 15, 2010.
- D. Landowner nevertheless desires to extend the initial term as if notice had been given, and the City is willing to agree to that extension by amending section 3 in article II of the Original Agreement as set forth below.

***With these background facts in mind, the City and Landowner agree as follows:***

- 1. **Amendment of Section 3, Article II.** Section 3 in article II of the Original Agreement is amended to read in its entirety as follows, but only with respect to the Landowner's Parcel:

3. **Term.**

a. **Initial Term.** The term of this Agreement shall commence on the Effective Date, which is May 15, 1996, and shall extend for a period of twenty (20) years thereafter, that is, until May 15, 2016, unless it is sooner terminated or modified by the mutual consent of the parties.

b. **Renewal Options.** Subject to the provisions of this subparagraph, LANDOWNER shall have the right to renew this Agreement on its same terms and conditions, taking into account any amendments hereto mutually agreed upon after the Effective Date. The term of this Agreement shall mean and include the initial term, plus any renewal periods. The specific conditions for exercise of the renewal options are as follows:

(1) On the Exercise Date, LANDOWNER shall not be in default in any material respect under this Agreement, including any amendments hereto. For purposes of this subsection, "Exercise Date" shall mean the date that LANDOWNER or LANDOWNER's successor in interest gives written notice of intention to exercise the option to renew this Agreement, in accordance with the provisions of Section 20 hereof.

(2) The option to renew shall be exercisable by giving CITY written notice of LANDOWNER's intention to exercise the option on or before the Exercise Date, which notice shall be given not later than one hundred eighty (180) days prior to expiration of the initial term or any renewal term.

(3) LANDOWNER shall be limited to two (2) renewal periods of five (5) years each; the parties specifically intend that under no circumstances shall the term of this Agreement extend beyond thirty (30) years, unless this Agreement is amended in accordance with the procedures set forth herein for Agreement amendments.

2. **All Other Terms Remain in Force.** Except as amended by sections 1 above, the Original Agreement remain in full force.
3. **Effective Date.** This amendatory agreement takes effect on the effective date of the ordinance that approves it (Government Code, § 65868; Sacramento City Code, §§ 18.16.120 & 18.16.130).
4. **Recording.** Either party may record this amendatory agreement with the Sacramento County Recorder.
5. **Counterparts.** The parties may execute this amendatory agreement in counterparts, each of which will be considered an original, but all of which will constitute the same agreement.

- 6. Entire Agreement and Modification.** This amendatory agreement sets forth the parties' entire understanding regarding the matters set forth above and is intended to be their final, complete, and exclusive expression of those matters. It supersedes all prior or contemporaneous agreements, representations, and negotiations regarding those matters (whether written, oral, express, or implied) and may be modified only by another written agreement signed by both parties. This amendatory agreement will control if any conflict arises between it and the Original Agreement.

*(Signature Page Follows)*

**City of Sacramento**

**A. J. Ventures, Inc.**

By: \_\_\_\_\_  
John Dangberg, Assistant City Manager,  
for Gus Vina, Interim City Manager  
Date: \_\_\_\_\_, 2011

By: \_\_\_\_\_  
Signature  
\_\_\_\_\_  
Name  
\_\_\_\_\_  
Title  
Date: \_\_\_\_\_, 2011

Approved as to Legal Form  
Sacramento City Attorney

Approved as to Legal Form

By: \_\_\_\_\_  
Joseph Cerullo Jr.  
Senior Deputy City Attorney

By: \_\_\_\_\_  
Signature  
\_\_\_\_\_  
Name  
Attorneys for A. J. Ventures, Inc.

First Amendment to City Agreement No. 96-051  
North Natomas Development Agreement  
Truxel 3 PUD

**Exhibit A**  
**Description of Landowner's Parcel**

**EXHIBIT "A"**

THE LAND REFERRED TO IN THIS GUARANTEE IS SITUATED IN THE STATE OF CALIFORNIA, CITY OF SACRAMENTO, COUNTY OF SACRAMENTO AND IS DESCRIBED AS FOLLOWS:

PARCEL 1, OF THAT LOT LINE ADJUSTMENT RECORDED JANUARY 3, 2007, IN BOOK 20070103, PAGE 1161 IN THE OFFICIAL RECORDS OF SACRAMENTO COUNTY, CITY OF SACRAMENTO, STATE OF CALIFORNIA.

TOGETHER WITH A PORTION OF LOT 24 AND LOT 22, AS SHOWN ON THAT MAP ENTITLED "PROMENADE AT NATOMAS" FILED IN BOOK 341 OF MAPS, PAGE 12, SACRAMENTO COUNTY RECORDS; MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF LOT 24, SAID CORNER ALSO BEING THE SOUTHWEST CORNER OF LOT 26 OF AFORESAID MAP; THENCE NORTH 89 DEGREES 21 MINUTES 57 SECONDS EAST 31.00 FEET ALONG A COMMON BOUNDARY LINE BETWEEN LOT 24 AND LOT 26, SAID LINE BEING THE NORTHERLY BOUNDARY LINE OF LOT 24; THENCE LEAVING SAID COMMON BOUNDARY LINE SOUTH 00 DEGREES 22 MINUTES 36 SECONDS EAST 378.83 FEET TO A POINT ON A COMMON BOUNDARY LINE BETWEEN LOT 24 AND LOT 22, SAID LINE BEING A SOUTHERLY BOUNDARY LINE OF LOT 24; THENCE LEAVING SAID BOUNDARY LINE SOUTH 31 DEGREES 29 MINUTES 39 SECONDS EAST 29.88 FEET; THENCE SOUTH 55 DEGREES 49 MINUTES 40 SECONDS WEST 55.89 FEET TO THE SOUTHWEST CORNER OF LOT 22; THENCE NORTH 00 DEGREES 22 MINUTES 36 SECONDS WEST 435.36 FEET ALONG THE WESTERLY BOUNDARY LINES OF LOT 22 AND LOT 24 TO THE POINT OF BEGINNING.

THE BASIS OF BEARINGS FOR THIS DESCRIPTION IS IDENTICAL WITH THAT FINAL MAP FILED IN BOOK 341 OF MAPS, AT PAGE 12, SACRAMENTO COUNTY OFFICIAL RECORDS. ROTATE THE BEARINGS DESCRIBED HEREIN 00 DEGREES 00 MINUTES 09 SECONDS COUNTER-CLOCKWISE TO ACHIEVE THOSE AS SHOWN ON 146 PM 2. BEING PARCEL 1 AS DESCRIBED IN THAT CERTAIN CERTIFICATE OF COMPLIANCE FOR LOT LINE ADJUSTMENT RECORDED JANUARY 3, 2007 IN BOOK 20070103 PAGE 1164, OFFICIAL RECORDS.

EXCEPTING THEREFROM ALL OIL, GAS AND OTHER HYDROCARBON SUBSTANCES, INERT GASES, MINERALS AND METALS, LYING BELOW A DEPTH OF 500 FEET FROM THE SURFACE OF SAID LAND AND REAL PROPERTY, WHETHER NOW KNOWN TO EXIST OR HEREAFTER DISCOVERED, INCLUDING BUT NOT LIMITED TO THE RIGHTS TO EXPLORE FOR, DEVELOP, AND REMOVE SUCH OIL, GAS AND OTHER HYDROCARBON SUBSTANCES, INERT GASES, MINERALS, AND METALS WITHOUT, HOWEVER, ANY RIGHT TO USE THE SURFACE OF SUCH LAND AND REAL PROPERTY OR ANY OTHER PORTION THEREOF ABOVE A DEPTH OF 500 FEET FROM THE SURFACE OF SUCH LAND AND REAL PROPERTY FOR ANY PURPOSE WHATSOEVER, AS RESERVED IN DEED RECORDED FEBRUARY 25, 2004, BOOK 20040225, PAGE 2291, OFFICIAL RECORDS.

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T.S. #10064-EB / KOBRA PROPERTIES