



# REPORT TO PLANNING COMMISSION City of Sacramento

# 9

915 I Street, Sacramento, CA 95814-2671

PUBLIC HEARING  
April 14, 2011

To: Members of the Planning Commission

**Subject: The Gateway on Fremont Park**

Review and Comment on a Capitol Area Development Authority (CADA) project, for the construction of a five (5) story, 30 residential unit building with approximately 5,847 square feet of ground floor retail on .44± acres in the General Commercial (C-2) zone.

**Location/Council District:**

1601 16<sup>th</sup> Street, Sacramento, CA

Assessor's Parcel Number 006-0293-001, -002, -026

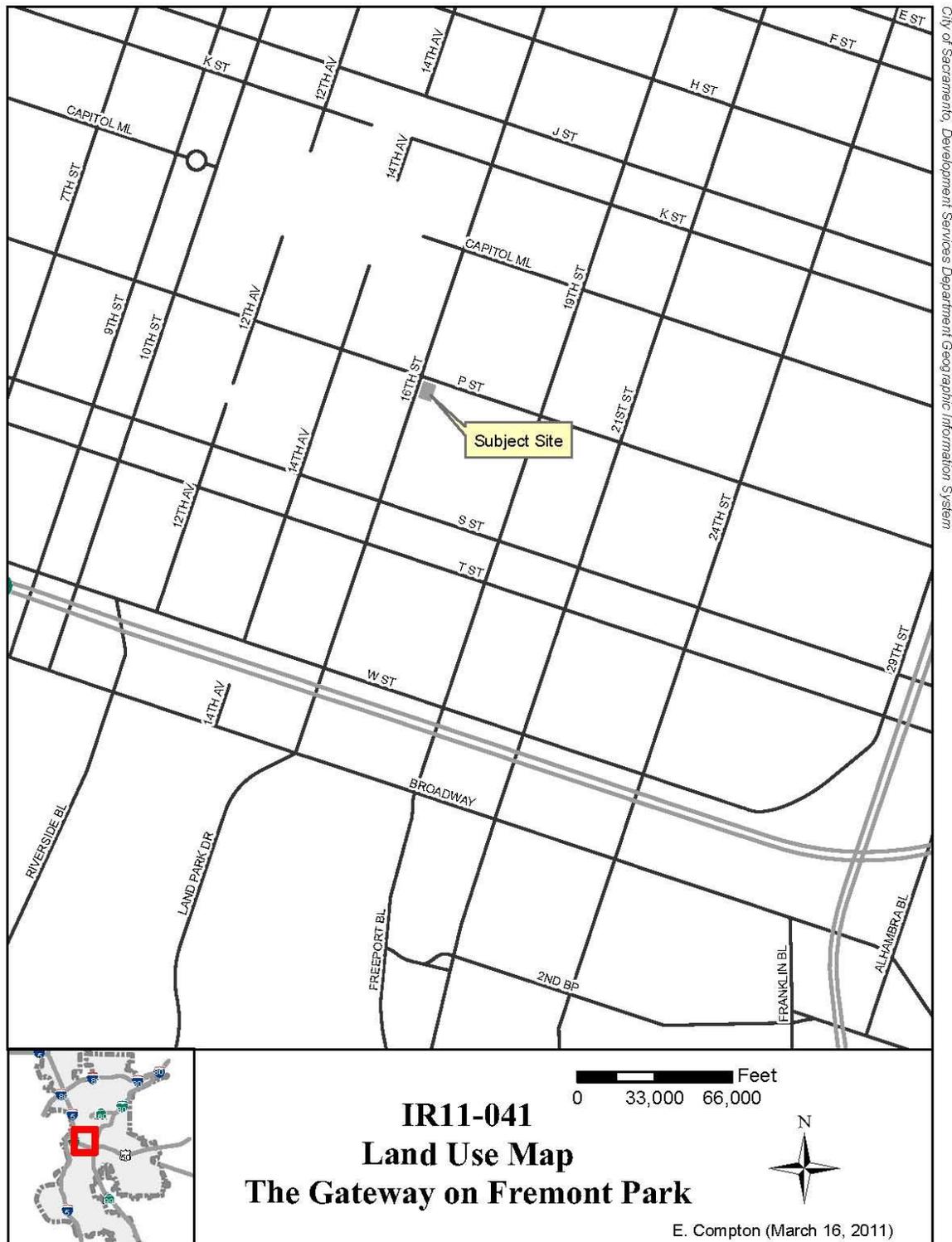
Council District 3

**Recommendation:** Staff recommends the Planning Commission review and forward advisory conditions of approval to the Capital Area Development Authority (CADA).

**Contact:** Evan Compton, Associate Planner, (916) 808-5260; Stacia Cosgrove, Senior Planner, (916) 808-7110

**Applicant:** John Leonard, Sukna Global Holdings, Inc., (916) 443-8300, 2210 K Street, Suite 101, Sacramento, CA 95816.

**Owner:** Tom Kigar, Capital Area Development Authority (CADA), (916) 322-2114, 1522 14th Street, Sacramento, CA 95814.



**Summary:** The development consists of a five story mixed-use building at the southeast corner of 16<sup>th</sup> and P Streets. This project includes demolishing an existing motel. The proposed building will accommodate 5,847 square feet of commercial space and 30 residential units. The proposed 30 units would include 11 one bedroom units and 19 two bedroom units ranging in size from 925 square feet to 1,535 square feet.

CADA is exempt from Planning and Design Review entitlements from the City, but is requesting a review and comment in the form of Advisory Conditions of Approval.

<b>Table 1: Project Information</b>
<b>General Plan designation:</b> Urban Corridor High
<b>Existing zoning of site:</b> C-2 (General Commercial)
<b>Existing use of site:</b> Motel
<b>Property dimensions/area:</b> 120 feet by 160 feet or 19,200 square feet (.44± acres)
<b>Building square footage:</b> 56,272 ± square feet
<b>Building height:</b> 60'6" to top of parapet, 68' to top of photovoltaic panel system.
<b>Exterior building materials:</b> Cement plaster and brick veneer, precast concrete veneer, aluminum storefront systems, steel railings, and steel awnings.

**Background Information:** Prior to consideration by the CADA Board of Directors, CADA is required to obtain advisory input from the City Planning Commission and the City Design Commission.

Pursuant to the Joint Powers Agreement (City and State) CADA projects are not required to obtain planning entitlements from the City. Were this other than a CADA project, the following entitlements would be necessary:

1. **Tentative Map** to subdivide the parcel for condominium purposes.
2. **Special Permit** to construct a building over 40,000 square feet in the General Commercial (C-2) zone.
3. **Special Permit** to construct 30 alternative ownership housing units.
4. **Special Permit** to waive a total of 70 parking spaces including 68 parking spaces for ground floor restaurants accommodating 203 seats and 2 parking spaces for residential guest spaces.
5. **Special Permit** to allow for an increase in the height limit from 35 feet to 60.5 feet for a property within 100 feet of a residential zone.
6. **Special Permit** to exceed 36 dwelling units per net acre in the General Commercial (C-2) zone with a proposal of 68 dwelling units per net acre.

**Public/Neighborhood Outreach and Comments:** Staff notified the adjacent property owners within 500 feet of the subject site and neighborhood associations for the April 14, 2011 Planning Commission hearing and the March 16, 2011 Design Commission hearing. The following organizations were noticed: Capitol Area R Street Association,

Downtown Partnership, Richmond Grove Neighborhood Association, Midtown Neighborhood Association, Midtown Business Association, and the Newton Booth Neighborhood Association.

The applicant has contacted property owners and neighborhood associations during the initial planning and design phase.

A neighbor contacted staff to indicate they supported the project and had two comments: 1) Residential units should have at least one shower to improve the marketability of the units for buyers who need units with ADA access and some of the units are showing two bathtubs; and 2) The entire alley, and possibly the alley to the north, should be paved with the approval of this project. Staff has forwarded the floor plan suggestion to the applicant for consideration. Staff has also confirmed with the Department of Transportation that alley improvements will be required for the project (see condition 14 of Attachment 1) however, it is limited to the length of the project site.

**Environmental Considerations:** CADA prepared and certified an environmental impact report that included analysis of development on the project site. CADA has indicated that additional CEQA analysis may be required. The City does not have discretionary authority for the project, but would review and submit any appropriate comments if additional environmental review is completed by CADA.

**Policy Considerations:** The 2030 General Plan Update was adopted by City Council on March 3, 2009. The 2030 General Plan's goals, policies, and implementation programs define a roadmap to achieving Sacramento's vision to be the most livable city in America. The 2030 General Plan Update designation of the subject site is Urban Corridor High. As stated in the General Plan, this designation is in urbanized areas and includes multistory structures and highly developed transit service. New development along the corridor contributes to a more compact and consistent pattern that relocates parking primarily to structures and to the rear of buildings. Street level frontages are lined with retail and other pedestrian-oriented uses. The streetscape is appointed with pedestrian amenities that support and enhance pedestrian activity.

The proposal is consistent with the following General Plan policies:

*Redeveloping Automobile-Oriented Corridors. The City shall promote redevelopment of existing automobile corridors and the upgrading of existing commercial development to create vibrant, mixed use boulevards that balance efficient movement of motor vehicles with the creation of attractive pedestrian-friendly districts that serve the adjoining neighborhoods as well as passing motorists. (Policy LU 6.1.3)* Staff finds that the existing motel has surface parking at the corner and does not activate the street whereas the project proposal will provide strong corner elements and ground level commercial to enhance pedestrian activity and provide neighborhood serving retail/restaurant uses.

*Neighborhood Enhancement. The City shall promote infill development, redevelopment, rehabilitation, and reuse efforts that contribute positively (e.g., architectural design) to*

*existing neighborhoods and surrounding areas. (LU 2.1.6)* Staff finds that the proposal provides an attractive building with appropriate massing, a variety of building materials, inset balconies, multiple building planes, a tenant courtyard, and substantial fenestration which enhance the overall neighborhood.

*Reduce Minimum Parking Standards. The City shall reduce minimum parking standards over time to promote walkable neighborhoods and districts and to increase the use of transit and bicycles. (M 6.1.2)* Staff finds that the project provides parking for the residential units but will not provide onsite parking for residential guests or commercial customers however, the site is within 1/8 mile of three bus routes and within 1/4 mile of the 16<sup>th</sup> Street light rail station.

*Transitions in Scale. The City shall require that the scale and massing of new development in higher-density centers and corridors provide appropriate transitions in building height and bulk that are sensitive to the physical and visual character of adjoining neighborhoods that have lower development intensities and building heights. (Policy LU 2.7.3)* Staff finds that the project steps down in height along the alley with a pavilion (south property line) and references an existing datum line at 1612 P Street with ground floor brick veneer on the new structure (east property line) and the massing of the new structure overall is respectful of neighboring properties.

*Buildings that Engage the Street. The City shall require buildings to be oriented to and actively engage and complete the public realm through such features as building orientation, build-to and setback lines, façade articulation, ground-floor transparency, and location of parking. (Policy LU 2.7.7)* Staff finds that the building engages the street by providing active commercial uses on the ground floor, a strong street wall, large storefront windows, a lobby area for the residential entrance, and outdoor seating.

*Screening of Off-street Parking. The City shall reduce the visual prominence of parking within the public realm by requiring most off-street parking to be located behind or within structures or otherwise fully or partially screened from public view. (Policy LU 2.7.8)* Staff finds that the parking area is located behind the commercial storefronts and is screened from public view.

*Housing Element. The City shall continue to work with Capital Area Development Authority (CADA) and the State of California, a major employer in Sacramento, to further the production of affordable and workforce housing, especially in and around downtown. (Policy H 1.2.9)* Staff finds that the project will further the goal of locating additional housing units in the downtown area.

The proposal is also consistent with the following Central City Community Plan Policy:

*Mixed-Use Buildings. The City shall provide the opportunity for mixture of housing with other uses in the same building or on the same site at selected locations to capitalize on the advantages of close-in living. (CC.H 1.1)* Staff finds that the proposal is a mixed use building with retail/restaurant uses on the ground floor and residential units on the upper floors.

**Project Design:**

The Design Commission completed a Review and Comment on March 16, 2011. The hearing body was very supportive of the project and the advisory conditions of approval have been included in this report as Attachment 5.

**Demolition of the Existing Motel:** The existing structure was determined through environmental review to not be eligible as a historic resource per the California Environmental Quality Act. (IR06-184)

**Alternative Modes**

Bus route 6 provides 60 minute weekday service to the site with connectivity to the downtown area, light rail, and the Pocket Transit Center. Bus route 38 provides 60 minute service seven days per week with connectivity to the downtown area and the University/65<sup>th</sup> Street light rail station. Bus route 109 is a peak-only route connecting Fair Oaks and Orangevale to the downtown. All of these bus routes are within 1/8 mile of the project site. Additionally, the project site is within 2 blocks of the 16<sup>th</sup> Street light rail station.

**Access, Circulation and Parking**

The proposed project has parking access located on the alley. The main entrances to the commercial spaces are along 16<sup>th</sup> Street. The residential lobby entrance is located on P Street. The site provides the required amount of parking for the residential units however, there is no onsite parking for residential guests or commercial customers. Staff supports the parking reduction request because a) the proposal will provide adequate parking for the residential units and the waiver applies only to the guest and commercial parking which is typically high turnover; b) the reduction in parking supports the bus routes and light rail station near the project site, and; c) the commercial and residential guest parking needs will be generally satisfied with onstreet parking in the area.

<b>Use</b>	<b>Required Parking</b>	<b>Proposed Parking</b>	<b>Difference</b>
Residential Units	30 spaces (1 per unit)	30 spaces	No
Residential Guests	2 spaces (1 per 15 units)	0 spaces	-2 spaces
Restaurant with 203 seats	68 spaces (1 per 3 seats)	0 spaces	-68 spaces

<b>Table 2a: Bicycle Parking</b>			
<b>Total parking required</b>	<b>Required bicycle parking</b>	<b>Provided bicycle parking</b>	<b>Difference</b>
100	5	5+	No

### Height, Bulk and Setbacks

Under the Special Permit for Alternative Ownership Housing, the Planning Commission has the authority to vary the setbacks. The proposal requests reduced setbacks and stepbacks to allow the buildings to be constructed with a strong street wall and establish an urban identity. Staff supports the reduced setbacks because the project provides a consistent, defined street wall and a strong corner element activated with pedestrian oriented uses as encouraged in the Urban Corridor High General Plan designation.

<b>Table 3: Height and area standards</b>			
<b>Standard</b>	<b>Required</b>	<b>Proposed</b>	<b>Deviation?</b>
Height	35 feet	60.5 feet	-25.5 feet
Front setback (P Street)	4 feet*	0 feet	-4 feet
Interior Side Setback (East property line)	7.5 foot setback for first 26 feet in height	6 feet	-1.5 feet
	15 foot stepback for remainder of building above 26 feet in height	6 feet	-9 feet
Street side setback (16 <sup>th</sup> Street)	0 foot at ground level	0 foot	No
	10 foot stepback above 3 <sup>rd</sup> Floor**	0 feet	-10 feet
Rear setback (South property line)	15 foot setback for first 26 feet in height***	25.5 feet	No
	25 foot stepback for remainder of building above 26 feet*	25.5 feet	No
Courtyard	10 feet	15 feet	No

General Plan Floor Area Ratio Requirement	.3 to 6.0.	2.9	No
Density	36 d.u./n.a.	68 d.u./n.a.	Yes

\*Setback is calculated as the average of the two adjacent properties

\*\*10 foot stepback above the third story to accommodate street trees as required on streets designated as "storefront streets" in the Central City.

\*\*\*May use 20 foot wide alley to satisfy the setback and stepback requirement.

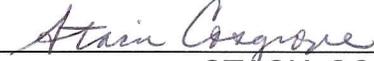
The proposal also exceeds the height and density requirements in the General Commercial (C-2) zone. The Zoning code allows the Planning Commission to grant a Special Permit to exceed the height and density requirements. For additional density, the Planning Commission is required to make a finding that there will be adequate public infrastructure in place to support the increase in density at the time of occupancy of the development. Staff supports the request for additional height and density because the proposal is: a) consistent with the General Plan designation which allows up to 150 dwelling units per net acre; b) consistent with the Urban Form Guidelines in the General Plan which encourage building heights from three to eight stories; c) providing residential amenities onsite including a common courtyard area and balconies; d) increasing the amount of residential units in the downtown area as encouraged in the Housing Element; and e) located along the 16<sup>th</sup> Street corridor which has fully developed public infrastructure to support the additional residential units and commercial areas.

### **Alley Café Seating**

The proposal for alley seating is not a part of this project and will be reviewed separately. The site plan shows seating on the alley for the ground floor restaurant. The applicant is seeking a closure of the alley from 10 am to 10 pm and would like to utilize decorative bollards. An alley activation committee is working on how this could be accomplished given issues with necessary vehicular access, trash pickup, emergency access, maintenance, turnaround space, and other potential concerns with the operation of a partial alley closure.

**Recommendation:** Staff recommends the Commission provide the attached advisory conditions of approval for consideration by the CADA Board and the developer because the project: a) increases the amount of housing stock in the downtown area; b) fosters a walkable neighborhood with a mixed use building; c) provides residential amenities onsite with a common courtyard area on the podium level and balconies; and d) encourages further redevelopment of the existing automobile corridor into a mixed use boulevard; and e) supports transit use since the site is located near a light rail station and several bus routes.

Respectfully submitted by:   
EVAN COMPTON  
Associate Planner

Approved by:   
STACIA COSGROVE  
Senior Planner

Recommendation Approved:

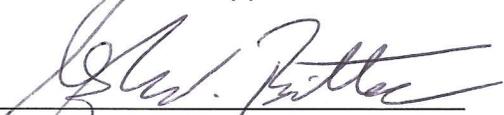
  
GREGORY BITTER, AICP  
Principal Planner

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**Attachment 1**  
**Proposed Advisory Conditions of Approval**  
**The Gateway on Fremont Park (IR11-041)**  
**1601 16<sup>th</sup> Street, Sacramento, CA**

At the regular meeting of April 14, 2011 the City Planning Commission heard and considered evidence in the above entitled matter. Based on verbal and documentary evidence at said hearing, the Planning Commission took the following action for the location listed above:

*Provided Review and Comment on a Capitol Area Development Authority (CADA) project, for the construction of a five (5) story, 30 residential unit building with approximately 5,847 square feet of ground floor retail on .44± acres in the General Commercial (C-2) zone and forwarded to the CADA Board the following Advisory Conditions of Approval:*

**Planning**

1. Prior to the issuance of a building permit, the project will require a parcel merger or map to consolidate the parcels.
2. Secure building permits prior to construction.

**Utilities**

3. The building pad elevation shall be approved by the DOU and shall be a minimum of 1.5 feet above the local controlling overland release elevation or a minimum of 1.2 feet above the highest adjoining back of sidewalk elevation, whichever is higher, unless otherwise approved by the Department of Utilities.
4. The applicant must comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinance. This ordinance requires the applicant to show erosion and sediment control methods on the subdivision improvement plans. These plans shall also show the methods to control urban runoff pollution from the project site during construction.
5. This project is served by the Combined Sewer System (CSS). Therefore, the developer/property owner will be required to pay the Combined System Development Fee prior to recording the final map. The impact to the CSS is estimated to be 24 ESD's. The Combined Sewer System fee is estimated to be \$2,718.48 plus any increases to the fee due to inflation.
6. Per City code section 13.04.230, no permanent structure (including without limitation concrete slabs, fences, decorative pavement, tool shed and similar structures) shall be constructed on top of water, sewer or drainage pipelines or anywhere within the associated utility easements, unless approved by the director upon execution of a hold harmless agreement approved by the city attorney.

7. Prior to or concurrent with the submittal of improvement plans, a combined sanitary sewer study is required and shall be approved by the DOU. The study shall provide an analysis of the pre and post development condition of both the sewer and drainage flow that is contributing to the combined system. If it is shown that the post development conditions cause an increase in flow to the combined system, either onsite storage and/or improvements to existing combined sanitary system will be required to the satisfaction of the DOU.
8. The proposed project is located in the Flood zone designated as **Shaded X** zone on the Federal Emergency Management Agency (FEMA) Federal Insurance Rate Maps (FIRMs) that have been revised by a Letter of Map Revision effective February 18, 2005. Within the Shaded X zone, there are no requirements to elevate or flood proof.
9. Prior to design of the subject project, the Department of Utilities suggests that the applicant request a water supply test to determine what pressure and flows the surrounding public water distribution system can provide to the site. This information can then be used to assist the engineers in the design of the fire suppression systems.
10. Both the 8" City water main and 8" City combined sewer main in P/Q Alley are over 100 years old. It is recommended that the sections of main fronting the project be removed and replaced as approved by the DOU prior to paving the alley.
11. The DOU is in the process of installing meters on any existing unmetered water services within the City. Prior to laying any pavement in the alley, coordinate with the DOU regarding the installation of water meters on existing service (including services for neighboring properties) located in any portion of the alley that is to be paved.
12. For any café seating or other related use of the public alley a hold harmless agreement is required regarding the potential temporary loss of business that may occur if any Utility related work is done within the alley.

### ***Development Engineering***

13. Construct standard improvements as noted in these conditions pursuant to chapter 18 of the City Code. Improvements shall be designed and constructed to City standards in place at the time that the Building Permit is issued. All improvements shall be designed and constructed to the satisfaction of the Department of Transportation. Any public improvement not specifically noted in these conditions shall be designed and constructed to City Standards. This shall include street lighting and the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk fronting the property along P Street and 16<sup>th</sup> Street per City standards and to the satisfaction of the Department of Transportation.

14. The applicant shall repair any deteriorated portions of the existing alley per City standards (in Concrete) and to the satisfaction of the Department of Transportation. The limit of work shall be from 16<sup>th</sup> Street to the project's boundary along the alley.
15. Prior to obtaining any building permits, the applicant shall identify if there are any encroachments onto the right-of-way from any balconies, awnings, building overhang, etc. Any encroachment into the right of way would need the review and approval of the Department of Transportation and would require a revocable encroachment permit.
16. The applicant shall pay the fair share for the future signal at the intersection of 15<sup>th</sup> and O Street. Prior to issuance of any Building permits, the applicant/CADA shall enter into an agreement with the City of Sacramento that states that a fair share amount for the construction of a future signal at the intersection of 15<sup>th</sup> Street and O Street is required.
17. The site plan shall conform to A.D.A. requirements in all respects. This shall include the replacement of any curb ramp that does not meet current A.D.A. standards at the south-east corner of the intersection of 16<sup>th</sup> Street and P Street per City standards and to the satisfaction of the Department of Transportation.
18. Prior to obtaining any building permits, a lot merger is required to accommodate the proposed project.
19. The applicant shall provide for all appropriate signage and markings at the entrance to the project from the alley. The driveway entrance shall be equipped with a mirror to alert the drivers to any potential pedestrians along the alley to the satisfaction of the Department of Transportation.
20. The site plan shall conform to the parking requirements set forth in chapter 17 of City Code (Zoning Ordinance).
21. The design of walls fences and signage near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height at maturity. The area of exclusion shall be determined by the Department of Transportation.

**Fire**

22. Provide the required fire hydrants in accordance with CFC 507 and Appendix C, Section C105.
23. Timing and Installation. When fire protection, including fire apparatus access roads and water supplies for fire protection, is required to be installed, such

protection shall be installed and made serviceable prior to and during the time of construction. CFC 501.4

24. Provide a water flow test. (Make arrangements at the Permit Center walk-in counter: 300 Richards Blvd, Sacramento, CA 95814). CFC 508.4
25. Provide appropriate Knox access for site. CFC Section 506
26. An automatic fire sprinkler system shall be installed in any portion of a building when the floor area of the building exceeds 3,599 square feet.
27. Locate and identify Fire Department Connections (FDCs) on address side of building no further than 50 feet and no closer than 15 feet from a fire hydrant.
28. An approved fire control room shall be provided for all buildings protected by an automatic fire extinguishing system. Fire control rooms shall be located within the building at a location approved by the Chief, and shall be provided with a means to access the room directly from the exterior. Durable signage shall be provided on the exterior side of the access door to identify the fire control room. CFC 903.8
29. Comply with the Office of the State Fire Marshal's Solar Photovoltaic Installation Guideline.
30. Project has indicated a portion of the alley will be used for café seating. The city provides a process for utilization of the alleys. Fire will evaluate each proposal and place conditions of approval that will maintain public safety. *Sacramento Alley Assessment Policy*.

### ***Sacramento Area Sewer District***

31. Developing the property will require the payment of Sacramento Regional County Sanitation District (SRCSD) sewer impact fees. Impact fees shall be paid prior to issuance of building permits. The applicant should contact the Fee Quote Desk at 876-6100 for sewer impact fee information.

### ***Parks***

32. As per City Code, the applicant will be responsible to meet his/her obligations regarding: Title 18, 18.44 Park Development Impact Fee, due at the time of issuance of building permit. The Park Development Impact Fee due for this project is estimated at \$43,744. This is based on 30 multifamily units at the Specified Infill Rate of \$1,425 per unit and 5,847 square feet of Retail/Commercial Services at the Specified Infill Rate of \$0.17 per square foot. Any change in these factors will change the amount of the PIF due. The fee is calculated using factors at the time that the project is submitted for building permit.

***Regional Transit***

33. Project construction shall not disrupt transit service or pedestrian access to transit stops.
34. The property owner and business owners shall join the Sacramento Transportation Management Association.
35. Transit information shall be displayed in a prominent location in the residential sales/rental office, through a homeowner's association, or with real estate transactions and within businesses for both patrons and employees. Please use the attached Request Form in Attachment 6 (also available on [www.sacrt.com](http://www.sacrt.com)) to order transit information materials.
36. Project shall provide clear and easy accessibility and connectivity for all transit users, including those with disabilities.
37. Consider developing a program to offer transit passes at a 50% or greater discount to new home owners for a period of six months or more. Program should be reviewed and approved by RT prior to approval of any special permit for the project.
38. Employers should offer employees subsidized transit passes at 50% or greater discount.
39. To provide more incentive to use alternate forms of transportation, RT suggests parking spaces to be priced separately and optional to building occupant, both retail and homeowners.

Attachment 2: Land Use and Vicinity Map

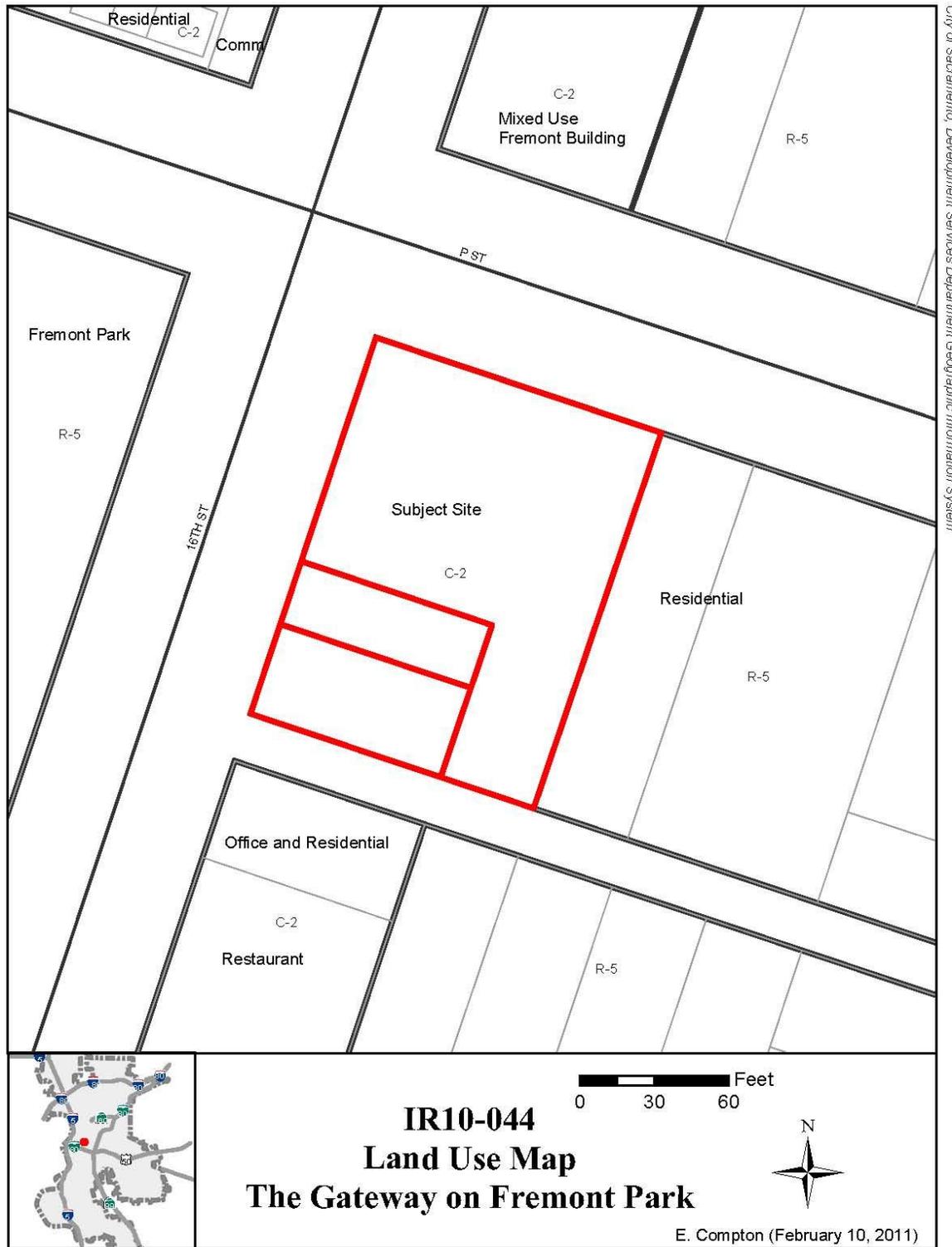




Exhibit B: Design Narrative

The Gateway on Fremont Park / Design Narrative

With expansive frontage along the east side of Fremont Park, our design team has endeavored to create a functional, environmentally responsive, and architecturally inspiring mixed-use project for CADA's Gateway Site 4. Envisioned as a catalyst urban infill development along the historic 16th Street Corridor, the sustainable project consists of a five-story mixed-use building with street level restaurant and retail space. The design responds to its context with a blend of traditional and modern materials, including a detailed brick facade emphasizing structure, a glazing system highlighting the significance of its corners, and stepped back building massing providing views from the majority of its residential units. The project will endeavor to be a model of sustainability, employing an array of technologies which will provide its residents with zero energy use.

The project also presents an opportunity to advance many of the goals of the Capital Area Development Authority, and the evolving 16th Street Corridor. Guided by a program that emphasizes sustainability and excellence in design, The Gateway development has the potential to become a project that fosters a new future of growth in the Sacramento region.

Our design concept seeks to implement the following objectives:

Gateway Presence

Each day thousands pass Site 4, often traveling north on 16th Street into the city. Our design recognizes that for those passing in cars, the alley corner of the site becomes the "primary corner". We have designed the building to have significant features at the corner of the alley as well as the corner of P Street, to both welcome the motorists and to create a positive impression of the development and community.

Park Views

With flat topography and a no ocean frontage, Sacramento is a city largely absent views. Parks provide an exception, and our project takes advantage of its long frontage facing Fremont Park by providing park views in 22 of the 30 units, each with private balconies on the park side. Our project is "single-loaded", meaning all units are oriented to streetsides, and will be entered from a common courtyard space on the interior side of the site.

Solar Orientation

While the park views require windows to the west, our design solution wraps many of the windows around the corner to the south also, enhancing views while significantly improving the ability to utilize passive solar shading to allow solar gain in the winter, and full shade in the summer months.

Alley Activation

With its metal edge and primary focal point abutting the alley, the site presents an excellent opportunity to build on the alley activation commitment that has seen enjoying success in Midtown neighborhoods. We propose to improve infrastructure and introduce pedestrian and bike friendly details into the alley, with cafe seating and landscaping spilling to its edge.

Courtyard Common Area

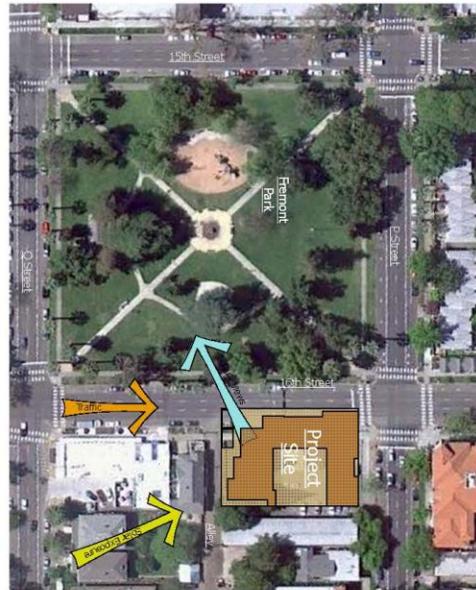
At the center of the project design is an expansive open courtyard on the podium level, providing common green space for residents, and an all-terrain fitness space for primary circulation to individual units. Each unit will enjoy cross-ventilation with windows into the courtyard for natural cooling, and each second level unit will have a semi-private entrance patio on the courtyard.

Reduced Tree Balustrade

The existing reduced fence on the site will be milled and utilized as a fence wall retained in interior courtyard spaces as the lobby, for decks and outdoor common in the courtyard, and in a key location as proposed decorative columns.

Passive Solar

A significant commitment to solar energy capture is evidenced at the roof level design, which has accommodated 11,000 sq ft of photovoltaic panels on a steel frame elevated over the parapet, producing a substantial percentage of building energy needs.



Site Plan

Multimodal Transportation

The project design seeks a balance between the myriad of transportation options. While providing one parking space per residential unit (30 total), the project is designed to engage the pedestrian experience with its active edges and proximity to transit, shops, and jobs. Class A bicycle facilities will also be provided.

Sustainability

Empowering both density and quality living spaces, the project design will reduce vehicle miles traveled by providing for alternative means to an auto drive commute. This includes car sharing, carpooling, and unit design has been focused on reducing energy use. Additionally, the project will include energy efficient systems which will be employed throughout the development, creating a project that will be a showcase of green technology.

The Gateway on Fremont Park will be a catalyst for a more vibrant, urban, pedestrian friendly, transit-oriented 16th Street Corridor. With an emphasis on urban design principles, we will endeavor to create a mixed-use, net-zero project that will be inspiring to both the surrounding community and the passing commuter alike.



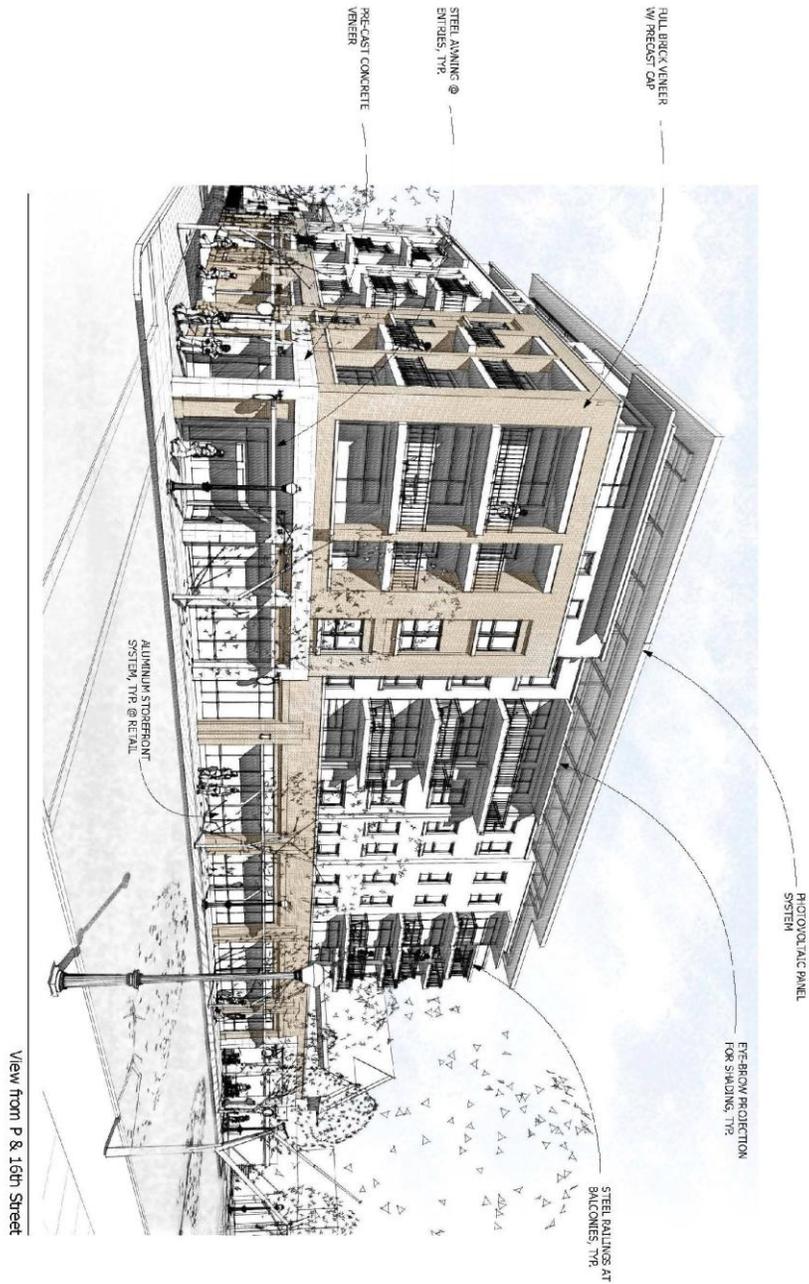
Exhibit D: Corner Entry at 16<sup>th</sup> and P Street

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Exhibit E: View from P and 16<sup>th</sup> Street

PHOTO ILLUSTRATION BY: [unreadable]



View from P & 16th Street







Exhibit I: Third and Fourth Floor Plan

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Exhibit L: Typical Units

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WEST UNIT 05, TYPICAL  
SCALE 3/16" = 1'-0"  
1145 SF



NORTHEAST CORNER UNIT 01, TYPICAL  
SCALE 3/16" = 1'-0"  
1112 SF



SOUTH UNIT 07, TYPICAL  
SCALE 3/16" = 1'-0"  
925 SF



NORTHWEST CORNER UNIT 03, TYPICAL  
SCALE 3/16" = 1'-0"  
1248 SF









Exhibit P: Material Palette

FIELD REP. & FLOORING CONTRACTOR TO STAMP APPROVAL





Exhibit R: Site Photos

\\0121\018\_8\_4\_CAD\011\041\0121\_1041\_SitePhotos.rvt



1 West View Site, Alley & Hot Italian



2 View of Fremont Park, looking west

3 NW Corner of 16th and P Streets



4 Looking North from Site



5 North View of Site

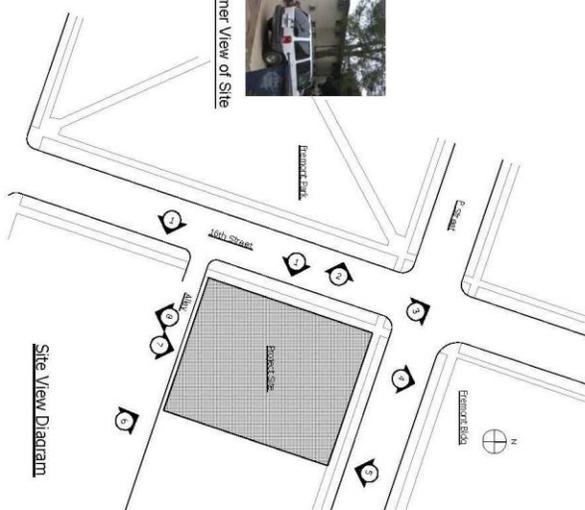


7 South View of Site at Alley

6 Southeast Corner View of Site

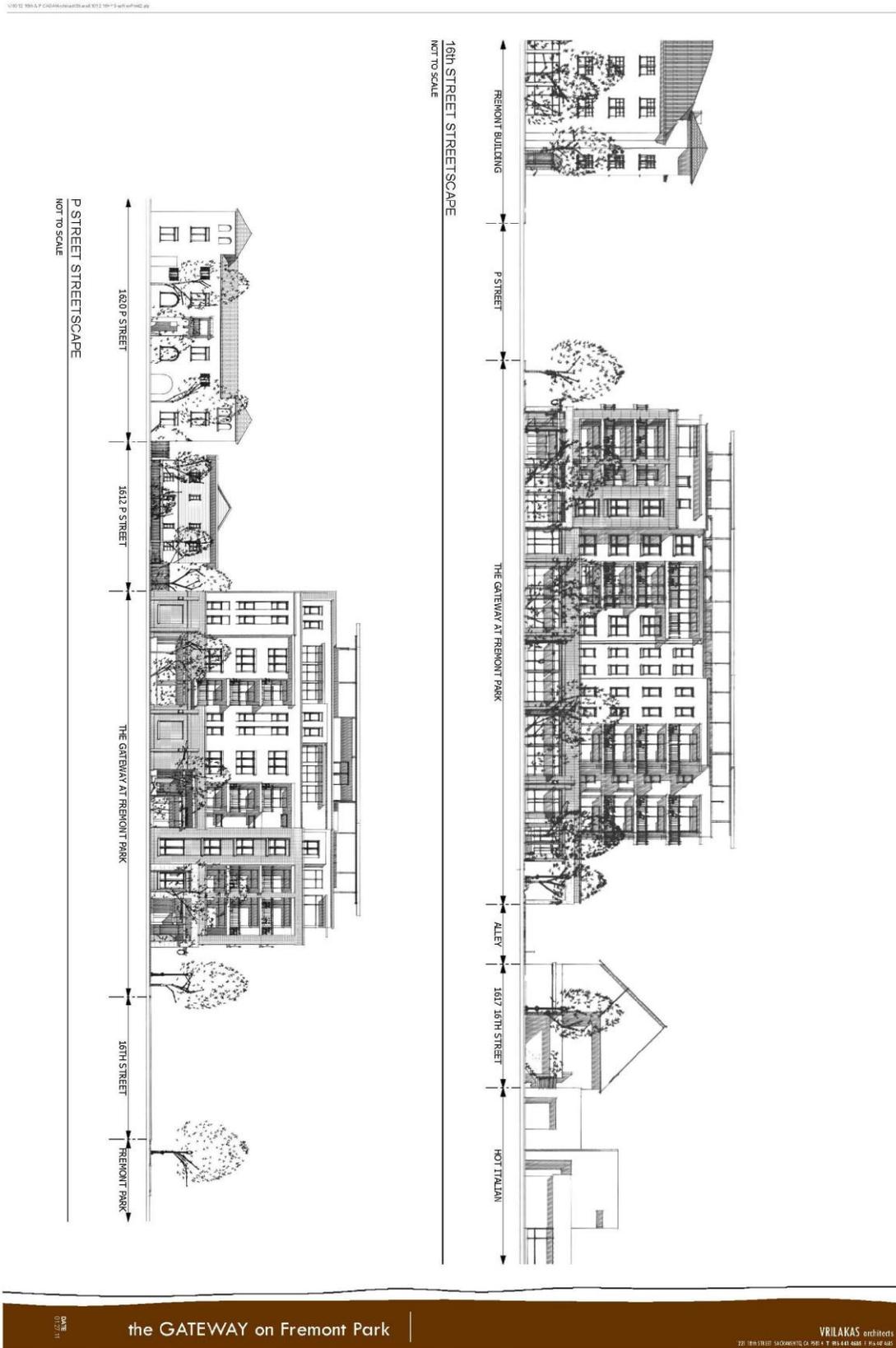


8 Alley View, looking south



Site View Diagram

Exhibit S: Streetscape Drawings



Attachment 4: Design Review Narrative

**THE GATEWAY ON FREMONT PARK**

A MIXED-USE DEVELOPMENT AT 16<sup>TH</sup> & P

**VRILAKAS**  
architects

**Design Narrative**

With expansive frontage along the east side of Fremont Park, our design team has endeavored to create a functional, environmentally responsive, and architecturally inspiring mixed-use project for CADA' s Gateway Site 4. Envisioned as a catalyst urban infill development along the historic 16<sup>th</sup> Street Corridor, this sustainable project consists of a five-story mixed-use building with street level restaurant and retail space. The design responds to its context with a blend of traditional and modern materials, including a detailed brick façade emphasizing structure; a glazing system highlighting the significance of its corners; and stepped back building massing providing views from the majority of its residential units. The project will endeavor to be a model of sustainability, employing an array of technologies which will provide its residents net-zero energy use.

The project also presents an opportunity to advance many of the goals of the Capitol Area Development Authority, and the evolving 16<sup>th</sup> Street Corridor. Guided by a process that emphasizes sustainability and excellence in design, *The Gateway* development has the potential to become a project that foreshadows a new future of growth in the Sacramento region.

Our design concept seeks to implement the following objectives:

**Gateway Presence**

Each day thousands pass Site 4, often traveling north on 16<sup>th</sup> Street into the city. Our design recognizes that for those passing in cars, the alley corner of the site becomes the “ primary corner” . We have designed the building to have significant features at the corner of the alley as well as the corner of P Street, to both welcome the motorists and to create a positive impression of the development and community.

1221 18<sup>TH</sup> STREET SACRAMENTO, CA 95811 tel 916.441.4685 fax 916.447.4685  
www.vrilakasarchitects.com

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**Park Views**

With flat topography and a no ocean frontage, Sacramento is a city largely absent views. Parks provide an exception, and our project takes advantage of its long frontage facing Fremont Park by providing park views in 22 of the 30 units, each with private balconies on the park side. Our project is “single – loaded” , meaning all units are oriented to street sides, and will be entered from a common courtyard space on the interior side of the site.

**Solar Orientation**

While the park views require windows to the west, our design solution wraps many of the windows around the corner to the south also, enhancing views while significantly improving the ability to utilize passive solar shading to allow solar gain in the winter, and full shade in the summer months.

**Alley Activation**

With its retail edge and primary focal point abutting the alley, the site presents an excellent opportunity to build on the alley activation movement that has been enjoying success in Midtown neighborhoods. We propose to improve infrastructure and introduce pedestrian and bike friendly details into the alley, with café seating and landscaping spilling to its edge.

**Courtyard Common Area**

At the center of the project design is an expansive open courtyard on the podium level, providing common green space for residents, and an atrium feeling space for primary circulation to individual units. Each unit will enjoy cross-ventilation with windows into the courtyard for natural cooling, and each second level unit will have a semi-private entrance patio on the courtyard.

**Redwood Tree Re-Use**

The existing redwood trees on the site will be milled and utilized as a finish wall wainscot in interior common spaces such as the lobby, for details and outdoor benches in the courtyard, and in a few locations as exposed decorative columns.

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**Rooftop Solar**

A significant commitment to solar energy capture is evidenced at the roof level design, which has accommodated 11,000 sf of photovoltaic panel on a steel frame elevated over the parapet, producing a substantial percentage of building energy needs.

**Multi- Modal Transportation**

The project design seeks a balance between the myriad of transportation options. While providing one parking space per residential unit (30 total), the project is designed to engage the pedestrian experience with its active edges and proximity to transit, shops, and jobs. Class A bicycle facilities will also be provided.

**Sustainability**

Emphasizing both density and quality living spaces, the project design will reduce vehicle miles traveled by providing for and attracting many to an urban environment. The building massing, orientation, and unit design has been executed utilizing passive solar considerations. And lastly, state of the art sustainable systems will be employed throughout the development, creating a project that will be a showcase of green technology.

*The Gateway* project will be a catalyst for a more vibrant, urban, pedestrian friendly, transit-oriented 16<sup>th</sup> Street Corridor. With an emphasis on urban design principles, we will endeavor to create a mixed-use, net-zero project that will be inspiring to both the surrounding community and the passing commuter alike.

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**THE GATEWAY ON FREMONT PARK**

A MIXED-USE DEVELOPMENT AT 16<sup>TH</sup> & P

**VRILAKAS**  
architects

**Parking Narrative**

The Gateway Mixed-Use project is located in the heart of the central city, two blocks from light-rail, along numerous bus lines, and within a walk of major employment centers. Parking for the residential units within the project are provided, consistent with City of Sacramento parking standards (however guest space is not provided).

Bicycle parking will exceed City of Sacramento requirements.

In order to add to the vitality and quality of life of the neighborhood, restaurant space is provided along the sidewalk edge of the ground floor. The storefronts will also serve to screen the onsite parking from the public way, and restaurant patrons will enjoy a view of Fremont Park (and the park will enjoy enhanced security with the addition of the active use).

We are not proposing to provide parking for the restaurant uses, due to the impractical nature of providing on-site parking, not uncommon for an urban project. Restaurant operators are expected to provide valet service, as necessary, typical for restaurants in the vicinity. While this site is within the Central City, it is a few blocks south of the designated Central Business SPD- the significance being that within the Central Business SPD, restaurants are not required to provide parking. The Gateway project area is, however, within a mixed-use, multi-modal community which exhibits all of the characteristics found in the Central Business SPD, two blocks to its north.

Due to its proximity and qualities, we are proposing no on-site parking for the restaurant uses.

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## **The Gateway on Fremont Park**

### **Alley Partial-closure and Alternative Use Plan**

The "Gateway on Fremont Park" mixed-use residential condominium project proposed for the CADA East End Gateway 4 site at the SEC of 16<sup>th</sup> and P Streets contemplates an upscale "farm to table" dining experience sited directly across the street from Fremont Park. In an effort to maximize the outside dining experience, the developer, Sukna Global Holdings, and architect Ron Vrilakas propose a partial alley closure (see attached graphics and rendering of proposed partial closure area, as well as images of other currently operated partial alley closures) from 10:00 am to 10:00 pm daily, with immediate decorative bollard removal capability by safety crews at any time.

This plan, while acknowledging the ongoing use and need of the alley, and the critical access needs for fire and life safety vehicles, proposes a creative and expanded alternate use for the alleyway: an outdoor patio dining area for the proposed restaurant use. This area, protected from traffic by temporary and removable bollards, would enable the maximum use of this area, and provide direct views of the neighborhood's key visual amenity, Fremont Park.

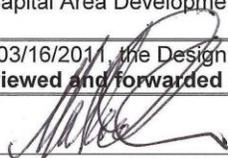
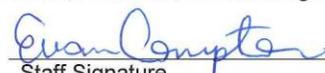
The proposed improvements of the alley, currently unfinished gravel, could include paving stones, or colored and stamped decorative concrete, properly graded and drained per city standard, as well as decorative and removable bollards at the edge of the sidewalk. These improvements will help address the projected increase in foot traffic generated by the project's new pedestrian oriented uses.

This is a new urbanism concept currently embraced by numerous other cities such as San Francisco, Portland, and Seattle, to name a few. It is a concept already in place in several other alleys throughout midtown Sacramento. It will increase the vibrancy, visibility and capacity of the restaurant, while adding to the new fabric of the neighborhood. This is the future of alternate, and part time uses for our urban alleyways, while still maintaining the critical ingress/egress and access for safety vehicles, and off-business hour access for commercial service vehicles such as waste removal and materials delivery.



**CITY OF SACRAMENTO DESIGN COMMISSION  
RECORD OF DECISION**

New City Hall, 915 I Street, 3<sup>rd</sup> Floor, Sacramento, CA 95814

Project Name:	The Gateway on Fremont Park	
Project Number:	IR11-041	
Project Location:	1601 16 <sup>th</sup> Street, 006-0293-001, -002, and -026	
Action Status:	Provided Advisory Conditions of Approval	Action Date: 03/16/2011
<b>REQUESTED ENTITLEMENT(S):</b>	<b>Recommendation:</b> Reviewed and forwarded advisory conditions of approval to Capital Area Development Authority (CADA).	
<b>ACTIONS TAKEN:</b>	On 03/16/2011, the Design Commission took the following actions: <b>Reviewed and forwarded advisory conditions of approval.</b>	
<b>Action certified by:</b>	 William Crouch, AIA, FRAIA, NCARB, LEED AP, Urban Design Manager	
	By: 	Staff Signature

**Findings Of Fact**

- A.** The **Design Review** request to develop a five (5) story, 30 residential unit project with ground floor retail is approved, subject to the following Findings of Fact:
1. The project, as conditioned, enhances the surrounding neighborhood.
  2. The project, as conditioned, will complement structures in the vicinity, and conforms to the Design Commission's design criteria.
  3. The proposed use will be consistent with the objectives of the City of Sacramento General Plan.
  4. The project is based upon sound principles of land use in that the proposed use is allowed in the General Commercial (C-2) and includes conditions addressing building design, site design and signage.

**Advisory Conditions Of Approval**

***The Design Review request to develop a five (5) story, 30 residential unit project with ground floor retail is hereby approved subject to the following advisory conditions.***

**A. The design of the site (see plans attached) is hereby approved subject to the following advisory conditions. These conditions must be met prior to the issuance of a building permit:**

1. The building shall be sited as indicated in the report and exhibits. The Applicant shall coordinate with the appropriate City agencies regarding alley improvements associated with the overall project
2. Auto access and site layout shall be as indicated in the report and exhibits.
3. The project shall have building setbacks as indicated in the exhibits.
4. The project shall have building entries as indicated in the exhibits. Further development of all project entry elements, including building sign standards and lighting cut sheets, shall be reviewed and approved by Design Review staff prior to Building Permit submittal.
5. The applicant shall work with Design Review Staff on special paving for the outdoor seating area on the alley and the City standard paving in the public right of way. Final paving shall be reviewed and approved by Design Review Staff prior to issuance of Building Permit.
6. The applicant shall work with Development Engineering, Urban Forest, and Design Review staff on the removal and pruning of existing street trees, as well as selection of new trees. All landscaping shall have automatic irrigation.
7. Exterior lighting style and design shall be compatible and consistent with the building design, and the site should be adequately illuminated for safety and security with a minimum 1.0 foot candle throughout. Appropriate lighting should light up wall surfaces or landscape areas. The applicant shall submit all site light fixtures cut sheets and plan locations for review and approval by Design Review staff prior to submitting for Building Permit. Street pole lights shall be energy efficient with cutoff devices included in the acorn style fixtures.
8. Site mechanical equipment and utility vaults shall be incorporated into the project site. Backflow prevention devices, SMUD boxes, etc., shall be placed in vaults or incorporated into building structure where not visible from street views, and screened from any pedestrian view. The applicant shall submit final mechanical locations for review and approval by Design Review staff prior to Building Permit submittal.
9. The integrated trash enclosure details and construction shall be reviewed and approved by Design Review staff prior to Building Permit submittal. All final material, finish, and color, shall match with the project's material and color scheme, and shall be reviewed and approved by Design Review staff prior to Building Permit submittal.
10. The project shall include landscaping elements as indicated in the report and exhibits, and final landscape plans (including hardscape and softscape) shall be reviewed and approved by Design Review staff prior to Building Permit submittal. Final tree species shall be coordinated with Urban Forest staff. Automatic irrigation shall be provided for all planting and landscaping. Drought tolerant vegetation shall be provided at the street tree planters. **Street planters and sidewalk shall be provided per Development Engineering standards and reviewed by Development Engineering, Urban Forest, and Design Review staff prior to Building Permit submittal.**

**B. The design of the new building (see plans attached) is hereby approved subject to the following advisory conditions:**

11. The design of the building shall be as indicated in the report and exhibits with final conditions as approved by the Design Commission.
12. The building elevations shall have a consistency of detail and quality.
13. All the final details for the steel frame structure supporting the rooftop photovoltaic panels shall be reviewed and approved by Design Review staff prior to Building Permit submittal.
14. The exterior materials provided shall be precast concrete veneer, full brick veneer, and cement plaster with an imperfect smooth finish. Design Review Staff to review final colors and materials palette prior to issuance of Building Permit.
15. Single hung windows shall be provided per approved plans. Aluminum storefront system and other window design and placement shall be provided per approved plans.
16. Steel awnings and eye-brow projections shall be provided per approved plans.
17. Ensure green roofs and courtyards have sufficient depth to sustain landscaping. Final plans shall be provided to Design Review Staff for review and approval prior to Building Permit submittal.
18. Exterior lighting style and design shall be compatible and complementary to the building design. **Final building lighting plans and light fixture cut sheets shall be reviewed and approved by Design Review staff prior to Building Permit submittal.**
19. Final mechanical penthouse shall be integrated into the final elevations through the use of materials and design. Final roof plan and mechanical penthouse elevations with mechanical equipment locations shall be reviewed and approved by Design Review staff prior to Building Permit submittal.

**C. The design of the signage is hereby approved subject to the following conditions:**

20. High quality signage with a design and materials that complements the architecture is required and shall meet the sign ordinance and Design Review Guidelines for the Central Core Design Review District. **Signage criteria for the project shall be submitted to Design Review staff and the City Sign Coordinator for review and approval prior to building permit submittal. Signage criteria requirements are locations of signage (elevations) including general size, potential illumination, and materials. Final sign designs shall be reviewed at time of tenant improvement Building Permit submittal.**

**D. General conditions:**

21. All final details affecting the exterior building design that are not determined at the time of the Design Commission's final review shall be reviewed and approved by Design Review staff prior to Building Permit submittal.
22. All other notes and drawings on the final plans as submitted by the applicant are deemed conditions of approval. Any changes to the final set of plans stamped by Design Review staff

shall be subject to review and approval prior to Building Permit submittal. Applicant shall comply with all current building code requirements.

23. Any major revisions to the final approved design are subject to review and approval by the Design Commission.
24. **All required new and revised plans shall be submitted for review and approval by Design Review staff prior to building permit submittal. A set of the appropriate plans (reduced to 11 x 17 set) along with a Letter of Compliance indicating how the project is in compliance with each Condition of Approval with detailed sheet references shall be submitted directly to Design Review Staff 1-2 weeks prior to Building Permit submittal. A final 3D model (SketchUp preferred) shall be provided to Design Review staff prior to building permit submittal. All necessary planning entitlements shall have been approved by the Planning Commission prior to final Design Review sign-off of plans.**
25. The approval shall be deemed automatically revoked unless required permits have been issued and construction begun within three years of the date of the approval. Prior to expiration, an extension of time may be granted by the Design Commission upon written request of the applicant.
26. **The Design Commission decision may be appealed to City Council. Appeals must be filed within 10 calendar days of written notice of the Design Commission action.**
27. Building permits shall not be issued until the expiration of the 10 day appeal period. If an appeal is filed, no permit shall be issued until final approval is received.
28. Final occupancy shall be subject to approval by Design Review Staff and shall involve an on-site inspection by Design Review Staff.
29. **The Record of Decision shall be scanned and inserted into the final set as a general sheet to be submitted for building permit.**



Regional Transit

# Materials Request

Requested By: \_\_\_\_\_ Project Name: \_\_\_\_\_

Control # \_\_\_\_\_

Quantity

Requested Items

- \_\_\_\_\_ **Bus & Light Rail Timetable Book** – 8.5"x11" Complete guide to RT\*
- \_\_\_\_\_ **Color System Map** – 23" x 36" Systemwide map of light rail and bus routes (5 per order)
- \_\_\_\_\_ **Personalized Trip Planner Cards** – 3-1/2" x 8-1/2" Fill out and mail.  
RT will send you a personalized trip plan
- \_\_\_\_\_ **How to Ride Brochure** – 3" x 8" Detailed information about how to ride with your bike
- \_\_\_\_\_ **Sales Outlet Brochure** – 3" x 8" Complete list of locations that sell RT passes
- \_\_\_\_\_ **Accessible Services Brochure** – 3" x 8" Explanation of additional services provided
- \_\_\_\_\_ **Schedule Rack** – Holds pocket timetables in either 10 or 20 slots
- \_\_\_\_\_ **Pocket Timetables (list routes)** – 2-1/4" x 3-3/4" Individual route schedules

Other: \_\_\_\_\_

\*RT will provide one complimentary Timetable Book per organization.

For questions about materials, please contact Kristi Brangle at 916-556-0113.

Date Ordered: \_\_\_\_\_ Estimated Date Materials will be Needed: \_\_\_\_\_  
Month/Year

Will pick up (Please call when ready)     Mail     Deliver (Allow 2-4 weeks)

### Send Materials to:

Name: \_\_\_\_\_ Phone: \_\_\_\_\_

Company: \_\_\_\_\_ Fax: \_\_\_\_\_

Mailing Address: \_\_\_\_\_

City: \_\_\_\_\_ State: \_\_\_\_\_ Zip: \_\_\_\_\_

**Fax completed form to Chris Pair at 916-456-1752  
or e-mail to cpair@sacrt.com  
or mail to Sacramento Regional Transit,  
P.O. Box 2110, Sacramento, CA 95812-2110**

### RT Planning Department use Only:

RT transit information condition met.    Approved By: \_\_\_\_\_ Date: \_\_\_\_\_

Marketing Department Use Only    Order Filled By: \_\_\_\_\_ Date: \_\_\_\_\_

Revised: 11/08 • RJS