



REPORT TO PLANNING COMMISSION City of Sacramento

7

915 I Street, Sacramento, CA 95814-2671

PUBLIC HEARING
June 9, 2011

To: Members of the Planning Commission

Subject: East Sac Self Storage (P11-023)

A request to reuse an existing 74,588 square-foot vacant industrial building for a mini-storage facility consisting of 400 to 500 individual storage spaces and office/retail space on a 1.9-acre parcel in the General Commercial Special Planning District (C-2-SPD) zone in the Alhambra Corridor Special Planning District.

- A. Environmental Determination: Exempt per CEQA Guidelines Section 15332 (Infill Development);
- B. Special Permit to locate a 74,558 square-foot mini storage facility in an existing vacant building in the General Commercial Special Planning District (C-2-SPD) zone;

Location/Council District:

3301 S Street, Sacramento, CA 95816

Assessor's Parcel Number 010-0063-011

Council District 4

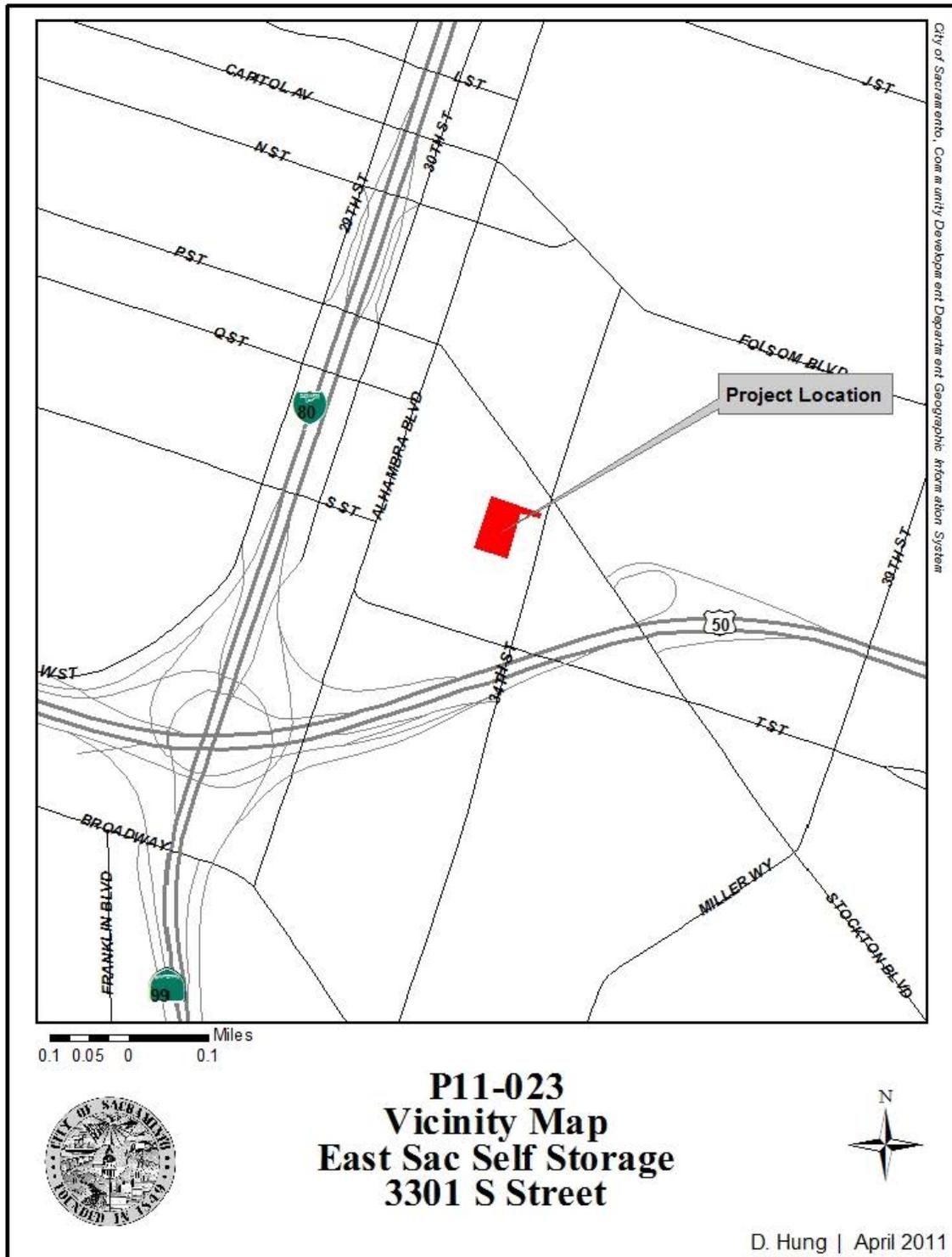
Recommendation: Staff recommends the Commission approve the request based on the findings and subject to the conditions listed in Attachment 1. **The project is opposed by Regional Transit at the time of writing this report.** The Commission has final approval authority over items A-B above, and its decision is appealable to City Council.

Contact: David Hung, Associate Planner, (916) 808-5530; Greg Bitter, Principal Planner, (916) 808-7816

Applicant: Potter-Taylor & Co. c/o Tim Taylor, (916) 923-0200, 1792 Tribute Rd, Suite 270, Sacramento, CA 95815

Owner: Watkins Co. Inc. II, Attn: Justin Dobrinski, (916) 504-4904, 2725 Riverside Blvd., Suite 400, Sacramento, CA 95818

Vicinity Map



Summary: The applicant is proposing to reuse an existing 74,588 square-foot vacant industrial building for a climate-controlled mini-storage facility consisting of 400 to 500 individual storage spaces and office/retail space on a 1.9-acre parcel. The project requires a Special Permit to establish a mini storage in the C-2 zone. Staff has sent early notices to surrounding property owners and various neighborhood groups in the area. The project is opposed by Regional Transit for the concern that the use is not transit supportive.

Table 1: Project Information
General Plan designation: Urban Corridor Low
Existing zoning of site: C-2-SPD (General Commercial Special Planning District)
Special Planning District: Alhambra Corridor
Design Review District: Alhambra Corridor SPD
Existing use of site: warehouse/office
Property area: 1.9 acres

Background Information: On August 25, 1981, the City Council approved a Special Permit to operate an outpatient psychiatric clinic at the subject location that carried a term limit of 18 months and allowed for reevaluation of the permit at the end of that time (P9467). In December of 1982, a Special Permit application was submitted to the City to continue the use on the site with no term limit (P82-288), which was approved by the Planning Commission on January 13, 1983. On July 9, 1992, the Planning Commission approved a Lot Line Adjustment and a Variance to reduce required parking by 26 spaces (P92-052). The subject building was occupied until July 2009 by the State of California as a data center and some light storage; it has remained vacant since then.

Public/Neighborhood Outreach and Comments: The project was routed to various advisory groups including the Newton Booth Neighborhood Association, the East Sacramento Improvement Association, the East Sacramento Preservation Inc., the McKinley East Sacramento Neighborhood Association, the East Sacramento Chamber of Commerce, the Capitol Area R Street Association and WalkSacramento. An Early Notice was also sent to property owners within 500 feet radius of the project site on May 10, 2011. Staff also received an inquiry concerning why the project requires a Special Permit and fencing adjacent to the site. The McKinley East Sacramento Neighborhood Association takes no exception to the project. The East Sacramento Chamber of Commerce wrote that the self storage “would be a great reuse for the current property and will benefit many residents and small businesses in the East Sacramento and Midtown communities needing just such a place to store items in a protected climate controlled facility”. The East Sacramento Preservation Inc. recommended that “the illuminated signage that occurs on S Street be on a timer so that the light goes off at a reasonable hour in the evening (10-11 p.m.) so as to not inconvenience residential properties directly across the street”. A condition is added to the project to limit sign illumination on S Street to no later than 10 p.m. The applicant presented the project to the Newton Booth Neighborhood Association on April 21, 2011. Staff attended a

meeting on May 18, 2011 where adjacent neighbors were invited by the applicant to review the proposal; only one neighbor attended the meeting looking for information on the operation.

Staff received a letter from Regional Transit in opposition to the project (Attachment 3). Regional Transit is not supportive of the project since the site is within a quarter mile radius of a light rail station and the proposed use is not transit supportive. The letter stated that a mini-storage use is not permitted according to the Retail and Personal Service list within the C-2 zone in the Alhambra Corridor Special Planning Area (chapter 17.104 of the Zoning Code). This comment is not accurate, since the Retail and Personal Service uses within the C-2 zone correspond only to Retail Node properties located between J Street and Folsom Boulevard on the east side of Alhambra; west of Alhambra Boulevard between T and U Streets; fronting 29th Street between S and V Streets and between N and P Streets. The project site is located outside of this area. Furthermore, the Retail and Personal Service use apply only to the retail component in office buildings and requires that a minimum of ten (10) percent of the total occupiable square footage must be devoted for retail commercial use. While the project site itself is within a quarter mile of a light rail station, the probable pedestrian route is outside of this quarter mile radius (see Attachment 4). City Staff believes the approval of a special permit is appropriate and is based on sound land use principles. This proposal is a viable reuse of an existing vacant building that was previously not opened to the public. Finally, as evidenced by the letters of support from the surrounding community, the use serves residents and businesses in the surrounding community and is compatible to the adjacent residential and commercial uses.

Environmental Considerations: The Community Development Department, Environmental Planning Services Division has reviewed this project and determined that this is exempt from the provisions of the California Environmental Quality Act (CEQA) pursuant to Section 15332 of the CEQA Guidelines, In-fill Development Projects. The project is consistent with the applicable general plan designation and all applicable general plan policies as well as with applicable zoning designation and regulations, occurs within City limits on a project site of no more than five acres substantially surrounded by urban uses, the project site has no value as habitat for endangered, rare or threatened species, approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality, and the site can be adequately served by all required utilities and public services.

Policy Considerations:

2030 General Plan. The 2030 General Plan Update was adopted by City Council on March 3, 2009. The 2030 General Plan's goals, policies, and implementation programs define a roadmap to achieving Sacramento's vision to be the most livable city in America. The 2030 General Plan Update designation of the subject site is Urban Corridor Low that includes street corridors that have multistory structures and more intense uses at major intersections, lower-intensity uses adjacent to neighborhoods, and access to transit service throughout. The 2030 General Plan has identified goals

and policies under the Land Use and Urban Design Element and the Economic Development Element that are supported by the project:

1. *Land Use and Urban Design Element (Policy LU 2.1.6) Neighborhood Enhancement.* The City shall promote infill development, redevelopment, rehabilitation, and reuse efforts that contribute positively (e.g., architectural design) to existing neighborhoods and surrounding areas.
2. *Land Use and Urban Design Element (Policy LU 2.8.1) Equitable Distribution of Uses and Amenities.* The City shall strive to ensure that desirable uses and neighborhood amenities are distributed equitably throughout the city.
3. *Land Use and Urban Design Element (Policy LU 6.1.1) Mixed-Use Corridors.* The City shall create or improve mixed-use corridors by requiring compact development patterns that are oriented to and frame the street, establish a safe and comfortable environment for walking, and avoid encroachment upon adjacent residential areas.

Staff believes that the proposed mini storage is a viable reuse of an existing warehouse structure whereas the previous use was not opened to the public and the proposed use will serve both residents and businesses in the surrounding community. The mini storage will be a compatible use with the adjacent residential and commercial uses and helps improve the mixed-use corridor. Furthermore, the reuse of the building does not preclude possible future redevelopment of the site.

Alhambra Corridor Special Planning District. The project site is within the Alhambra Corridor Special Planning District (SPD); the goals of the SPD are as follows:

- A. Maintain and improve the character, quality and vitality of individual neighborhoods;
- B. Maintain the diverse character and housing opportunities provided in these urban neighborhoods;
- C. Provide the opportunity for a balanced mixture of uses in neighborhoods adjacent to transit facilities and transportation corridors;
- D. Maintain the neighborhood character of existing commercial neighborhoods while allowing for limited office to serve the medical complex in this area;
- E. Provide the opportunity for reuse and rehabilitation of heavy commercial and industrial neighborhoods to take advantage of close-in living while reducing the number of obsolete and underutilized buildings and sites.

The proposal is a reuse and rehabilitation of an existing structure within a mixed-use neighborhood and should improve the quality and vitality of the neighborhood. Per Section 17.104.020(J)(1) of the Zoning Code for the Alhambra Corridor Special Planning District on Restricted General Commercial Use, the subject property on the north side of S Street is required to provide a 50-foot building setback and a 25-foot landscape setback. However, since the structure was built prior to the establishment of the SPD and there are no proposed expansion or setback changes to the building, the

existing nonconforming building and landscape setback are allowed to remain. The subject property is not subject to section 17.104.020(J)(2) for Retail Nodes since it's not located in the areas cited; therefore, the site is also not subject to Table 2 of section 17.104.020 which lists permitted retail and personal services uses associated with retail commercial requirement for office buildings. Finally, section 17.104.020(J)(3) is not applicable to the project since it's outside of the area cited.

Project Design:

Land Use

The applicant proposes to locate a 74,558 square-foot climate-controlled mini storage facility in an existing vacant building in the General Commercial Special Planning District (C-2-SPD) zone. The facility is to consist of 400 to 500 individual storage spaces divided between two floors and also an office and small retail space. Additionally, the facility offers long term parking within the gated area for its patrons; the long term parking is an incidental use to the mini storage facility. Section 17.24.050(5) of the zoning code requires the issuance of a special permit in order to locate the proposed use in the General Commercial zone. In evaluating the special permit for a mini storage in the C-2 zone, the Commission is required to make the following findings:

A. A special permit shall be granted upon sound principles of land use.

The proposed mini storage is a viable reuse of an existing warehouse structure whereas the previous use was not opened to the public and the proposed use will serve both residents and businesses in the surrounding community.

B. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.

The mini storage will be a compatible use with the adjacent residential and commercial uses and helps improve the mixed-use corridor; therefore, it will not be detrimental to public health, safety or welfare nor will it result in the creation of a nuisance.

C. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.

The proposed project supports policies for the Urban Corridor Low land use designation of the General Plan and the goals of the Alhambra Corridor Special Planning District.

The proposal is a reuse and rehabilitation of an existing structure within a mixed-use neighborhood and should improve the quality and vitality of the neighborhood. The mini storage will be a compatible use with the adjacent residential and commercial uses and helps improve the mixed-use corridor. Lastly, the reuse of the building does not preclude possible future redevelopment of the site.

Access, Circulation and Parking

The subject property is bounded by S street to the south, Caltrans parking lot and 34th Street to the east, light rail tracks to the north and a residential mixed-use development to the west. The S Street driveway allows for both ingress and egress; the 34th Street frontage allows for right turn egress only as conditioned by the Department of Transportation. A developed customer parking lot is accessed from S Street; an existing trash enclosure also exists within the parking lot. A drive aisle and long term parking spaces exist on the west and north perimeters of the site which is gated and accessible only to patrons.

Table 2: Parking			
Use	Required Parking	Proposed Parking	Difference
Mini Storage (500 individual storage space)	6 (one space per 100 storage units and one space for the manager)	9	+3
Long Term Parking	N/A	44	0
Total		53	+3

Table 2a: Bicycle Parking			
Total parking provided	Required bicycle parking	Provided bicycle parking	Difference
6	1	1	0

As indicated above, the project meets or exceeds parking requirements. One Class I bicycle parking facility is conditioned for the project.

Height, Bulk and Setbacks

No expansion is proposed to the existing structure; therefore, the height, setback and lot coverage remains the same. Furthermore, there is no proposed floor increase to the existing building. A summary of current height and area standards and any deviation to these standards are shown below:

Table 3: Height and area standards			
Standard	Required	Proposed	Deviation?
Height	35'	No change to existing height (31'-9" to top plate)	no

Front setback (on S Street) per Alhambra Corridor SPD	50' (building setback) and 25' (landscape setback)	No change to existing setback and building was built prior to SPD establishment	no
Side setback	0'	No change to existing setback (min. 18")	no
Street side setback (on 34 th Street)	5'	No change to existing setback	no
Rear setback (adjacent to light rail tracks)	0'	No change to existing setback	no
Lot coverage	Special Permit for expansion above 40,000 square feet	No expansion to existing floor space	no
Floor Area Ratio (FAR)	0.3 to 3.0	0.89	no

As indicated above, the project meets or exceeds all applicable height and area requirements.

Building design, signage and landscaping

Building design: The subject building is within the Alhambra Corridor SPD Design Review District and is subject to staff level approval due to the exterior rehabilitation to the structure. The exterior rehabilitation is limited to the west elevation of the structure and includes the following:

1. New aluminum overhead truck door with glazing to replace existing truck door adjacent to the front loading dock area.
2. New storefront door to replace existing front door adjacent to the front loading dock area.
3. Three new overhead roll-up doors to replace three existing louvered grilles at the northwest side of the building.
4. New overhead roll-up door to replace existing door and louver at the northwest side of the building.
5. New 42" high removable guardrails at the front loading dock.

The Conditions of Approval for the staff level Design Review on the exterior rehabilitation is attached (Attachment 2). The first floor plan shows a reception and waiting area adjacent to the front entry. A small retail area, office room and break room are also shown in the vicinity. The retail use will be for the selling of miscellaneous moving supplies such as moving boxes, file boxes, locks, tapes, mailing supplies bubble

wrap and packaging materials. Personal Wine Storage and Pharmaceutical spaces are also shown on the first floor. The storage of such items will be in individual lockers ranging in size from 3'x3' to 8'x8'; all lockers will be kept in certain temperature range. The bulk of the first floor contains storage units of various sizes, ranging from 3'x3' stacked units to full height storage units. The units are made with pre-manufactured steel partitions and all full height units receive a 4x8 or 8x8 overhead door that is keypad controlled. The second floor contains all storage units of various sizes. Except for units on the northwest side of the building, all storage units are climate controlled. An elevator and two staircases connect the first and second floors. Restrooms are located on the first floor. Surveillance cameras will be located at multiple spots both inside and outside the building; all recording is backed up on a server at the site as well as at a remote site.

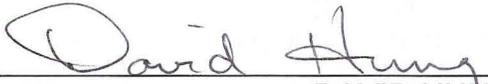
The hours of operation of the facility is proposed to be 7 a.m. to 7 p.m. daily. Any change to the hours of operation shall be reviewed by the Planning Division.

Landscaping: All existing landscaping elements are to remain.

Walls and Fencing: Existing chain link fencing occurs at the western, northern and eastern property lines. The adjacent mixed-use development to the west of the site constructed a wrought iron fence that runs parallel to the western chain link fencing. Since the subject site was developed prior to the mixed-use development, the existing fencing may remain. New automatic gates and fixed fencing will be installed at the vehicular entrance adjacent to S Street and exit at 34th Street; the new gates and fencing replace the existing gates and fencing.

Signage: The project proposes new attached signage at the south elevation and east elevation. Signage shall comply with sign requirements for the C-2 zone. Since the project is in the Alhambra Corridor SPD Design Review District, no cabinet signs are allowed. Any new signage shall be subject to Design Review approval and the issuance of sign permit. The project is conditioned that any new signage facing S Street shall not be illuminated beyond 10 p.m. daily.

Recommendation: Staff recommends the Commission approve the requested entitlements based on the findings and subject to the conditions listed in Attachment 1.

Respectfully submitted by: 
DAVID HUNG
Associate Planner

Recommendation Approved:

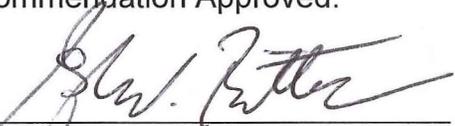

GREGORY BITTER, AICP
Principal Planner

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**Attachment 1
Proposed Findings of Fact and Conditions of Approval
East Sac Self Storage (P11-023)
3301 S Street**

Findings of Fact

A. Environmental Determination: Exemption

Based on the determination and recommendation of the City's Environmental Planning Services Manager and the oral and documentary evidence received at the hearing on the Project, the Planning Commission finds that the Project is exempt from review under Section 15332 (Infill Development Projects) of the California Environmental Quality Act Guidelines as follows:

The project is consistent with the applicable General Plan designation and all applicable general plan policies as well as with applicable zoning designation and regulations, occurs within City limits on a project site of no more than five acres substantially surrounded by urban uses, the project site has no value as habitat for endangered, rare or threatened species, approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality, and the site can be adequately served by all required utilities and public services.

B. The **Special Permit to locate a 74,558 square-foot mini storage facility in an existing vacant building in the General Commercial Special Planning District (C-2-SPD) zone is approved subject to the following Findings of Fact and Conditions of Approval:**

1. A special permit shall be granted upon sound principles of land use. The proposed mini storage is a viable reuse of an existing warehouse structure whereas the previous use was not opened to the public and the proposed use will serve both residents and businesses in the surrounding community.
2. A special permit shall not be granted if it will be detrimental to the public health, safety or welfare, or if it results in the creation of a nuisance.

The mini storage will be a compatible use with the adjacent residential and commercial uses and helps improve the mixed-use corridor; therefore, it will not be detrimental to public health, safety or welfare nor will it result in the creation of a nuisance.
3. A special permit use must comply with the objectives of the general or specific plan for the area in which it is to be located.

The proposed project supports policies for the Urban Corridor Low land use designation of the General Plan and the goals of the Alhambra Corridor Special Planning District.

Conditions of Approval

- B. The Special Permit to locate a 74,558 square-foot mini storage facility in an existing vacant building in the General Commercial Special Planning District (C-2-SPD) zone is hereby approved subject to the following conditions:

Planning

- B1. Development of this site shall be in compliance with the attached exhibits, except as conditioned. Any modification to the project shall be subject to review by Planning staff prior to the issuance of building permits. Modifications to the project may require subsequent entitlements.
- B2. The applicant shall obtain all necessary building permits prior to commencing construction.
- B3. Project shall comply with Design Review conditions of approval for proposed exterior rehabilitation (see Attachment 2).
- B4. Lighting:
- a. Lighting shall be designed so as not to produce hazardous and annoying glare to motorists, adjacent properties, or the general public. All fixtures should be placed in a manner that avoids glare when observed from the street or other public areas.
 - b. The premises, while closed for business after dark, must be sufficiently lighted by use of interior night-lights.
 - c. Open parking lot shall be provided with a minimum maintained one foot-candle of light as measured at the parking surface, from one half-hour before sunset until one half-hour after sunrise. All lighting devices shall be equipped with weather and vandal resistant covers.
 - d. Aisles, passageways and recesses related to and within the building complex shall be illuminated with an intensity of at least twenty-five one-hundredths minimum maintained foot-candle of light as measured at ground level during the hours of darkness.
 - e. Applicant shall submit a lighting plan to Planning Division for review prior to the issuance of building permits.
- B5. The trash enclosure shall meet all requirements of the Sacramento City Code, Chapter 17.72 (Recycling and Solid Waste Disposal Regulations), including, but not limited to, perimeter landscaping, masonry walls, solid metal gate, concrete apron, overhead clearance, signs and setbacks.

- B6. Building Design:
- a. All mechanical equipment shall be screened. All rooftop mechanical and communications equipment shall be completely screened from view from public streets by the building parapet, screen wall, and architectural projections which are integral to the building design.
 - b. Anti-graffiti coating shall be applied to street facing elevations that will be susceptible to vandalism.
- B7. Fencing and gates at vehicular entrance and exit shall be decorative metal tubing.
- B8. The proposal is required to meet the Sacramento City Code regulations, regarding bicycle parking (Section 17.64.050). Bicycle parking shall be located in a secure area located in close proximity public view. One bicycle parking is required for the project.
- B9. The project is required to provide six (6) vehicular parking spaces based on the maximum of 500 storage units.
- B10. Any new signage shall require Design Review approval prior to issuance of sign permit.
- B11. Any new signage facing S Street shall not be illuminated beyond 10 p.m. daily.
- B12. The hours of operation of the facility is proposed to be 7 a.m. to 7 p.m. daily. Any change to the hours of operation shall be reviewed by the Planning Division.

Department of Transportation

- B13. Construct standard improvements as noted in these conditions pursuant to chapter 18 of the City Code. Improvements shall be designed and constructed to City standards in place at the time that the Building Permit is issued. Any public improvement not specifically noted in these conditions shall be designed and constructed to City Standards. This shall include street lighting and the repair replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk fronting the property along "S" Street per City standards and to the satisfaction of the Department of Transportation.
- B14. All new and existing driveways shall be designed and constructed to City Standards and must be ADA-compliant to the satisfaction of the Department of Transportation. The applicant shall reconstruct any existing deteriorated driveways and any driveways that do not meet current ADA-standards along "S" street.
- B15. The existing driveway along 34th street shall be restricted to **right-out only** movements. The applicant shall construct a physical barrier along the center line

of 34th street close to the railroad tracks to prevent any left-out movements. The barrier can either be a raised concrete curb, or the use of traffic channelizers to the satisfaction of the Department of Transportation. The applicant shall also provide for a signage and markings package to restrict the driveway movements to the satisfaction of the Department of Transportation.

- B16. The site plan shall conform to the parking requirements set forth in Title 17 of City Code (Zoning Ordinance).
- B17. The design of walls fences and signage near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height at maturity. The area of exclusion shall be determined by the Department of Transportation.

Department of Transportation – Engineering Services – Electrical Section

- B18. This project shall require street lighting. There is an existing street lighting system around this project area. Improvements of right-of-way may require modification to the existing system. Electrical equipment shall be protected and remain functional during construction.

Fire Department

- B19. All turning radii for fire access shall be designed as 35' inside and 55' outside. CFC 503.2.4 *West side of the building shall meet this requirement.*
- B20. Roads used for Fire Department access shall have an unobstructed width of not less than 20' and unobstructed vertical clearance of 13'6" or more. CFC 503.2.1
- B21. Provide the required fire hydrants in accordance with CFC 507 and Appendix C, Section C105.
- B22. Timing and Installation. When fire protection, including fire apparatus access roads and water supplies for fire protection, is required to be installed, such protection shall be installed and made serviceable prior to and during the time of construction. CFC 501.4
- B23. Provide a water flow test. (Make arrangements at the Permit Center walk-in counter: 300 Richards Blvd, Sacramento, CA 95814). CFC 508.4
- B24. Provide appropriate Knox access for site. CFC Section 506
- B25. Roads used for Fire Department access that are less than 28 feet in width shall be marked "No Parking Fire Lane" on both sides; roads less than 36 feet in width shall be marked on one side.

- B26. An automatic fire sprinkler system shall be installed in any portion of a building when the floor area of the building exceeds 3,599 square feet. *This shall be required due to change of occupancy.*
- B27. Locate and identify Fire Department Connections (FDCs) on address side of building no further than 50 feet and no closer than 15 feet from a fire hydrant.
- B28. An approved fire control room shall be provided for all buildings protected by an automatic fire extinguishing system. Fire control rooms shall be located within the building at a location approved by the Chief, and shall be provided with a means to access the room directly from the exterior. Durable signage shall be provided on the exterior side of the access door to identify the fire control room. CFC 903.8
- B29. Vehicle gates shall be AC powered and provide 20 feet clear access. Split gates shall provide 16 feet clear access minimum for each gate. Gates shall be provided with Key override switch (Knox) and Radio operated controller (Click2Enter).

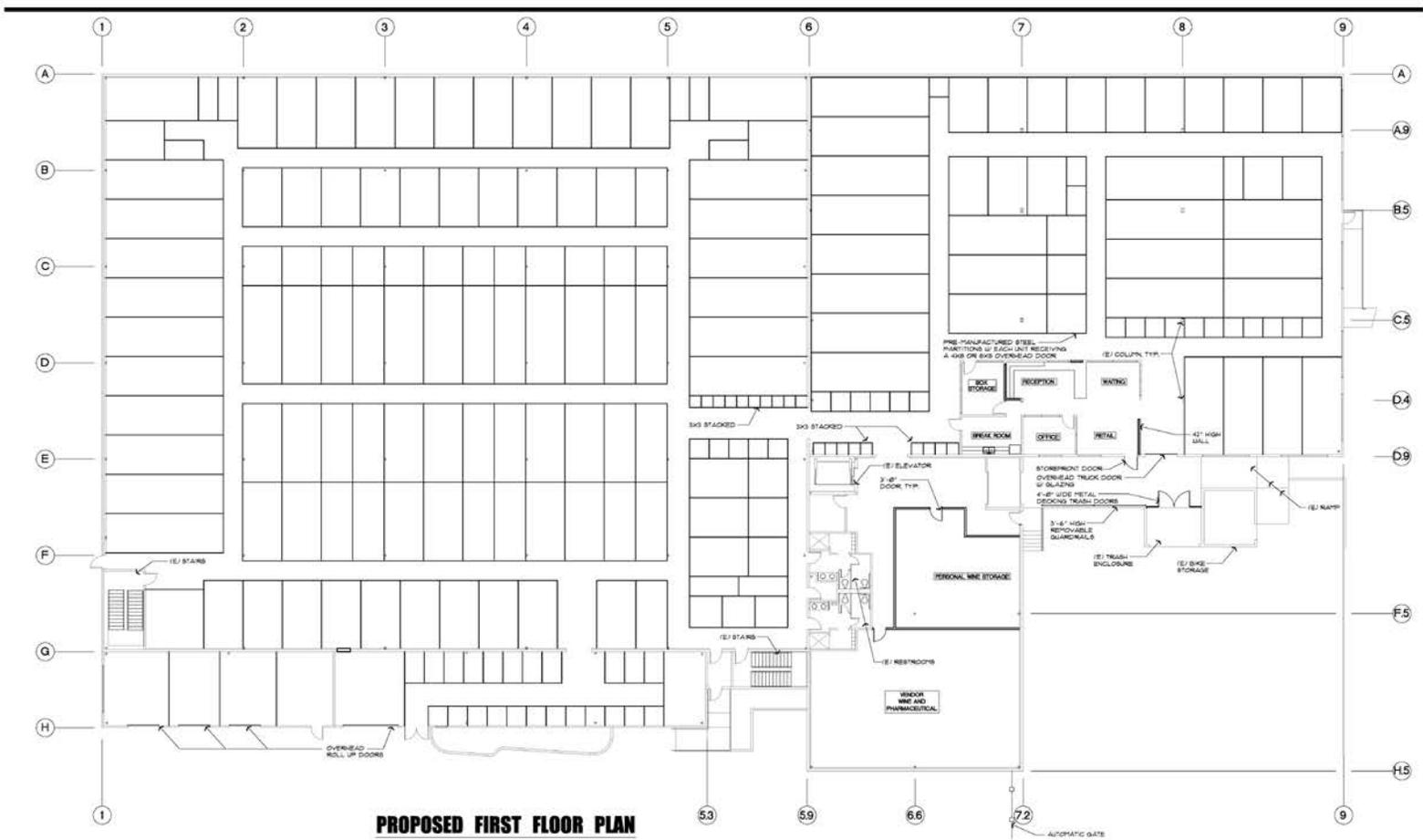
Police Department

- B30. A video camera system that records to a DVD shall be installed to monitor the main entrance(s).
- B31. A small sign shall be installed at the entrance that says the property is under video surveillance.

Sacramento Area Sewer District (SASD)

- B32. Reuse of this property may require the payment of SRCSD sewer impact fees. Impact fees shall be paid prior to issuance of Building Permits. Applicant should contact the Fee Quote Desk at 876-6100 for sewer impact fee information.

Exhibit 1B – Proposed First Floor Plan



PROPOSED FIRST FLOOR PLAN

EAST SAC SELF STORAGE
 SACRAMENTO, CA
 POTTER-TAYLOR & Co.

SD JOB NO.: 3.130
 DATE: 03-15-11

2

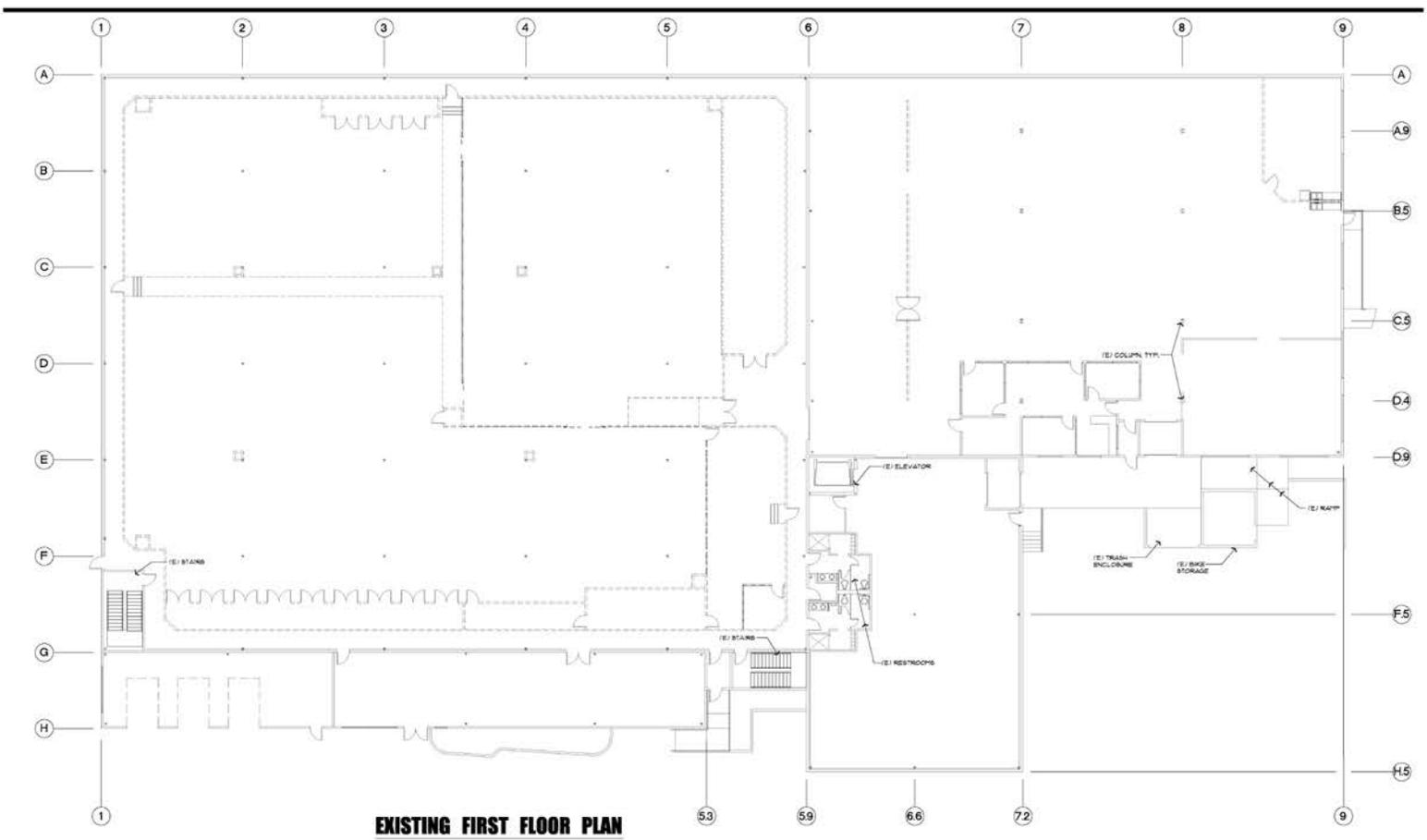
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Stark Designs ARCHITECTURE

2411 Gold Anderson Dr.
 Sacramento, CA 95825
 916-930-2725
 916-930-6329 FAX
 www.starkdesigns.com

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Exhibit 1C – Existing First Floor Plan



EXISTING FIRST FLOOR PLAN

EAST SAC SELF STORAGE

SACRAMENTO, CA
POTTER-TAYLOR & Co.

SD JOB NO.: 3.130
DATE: 03-15-11

2A

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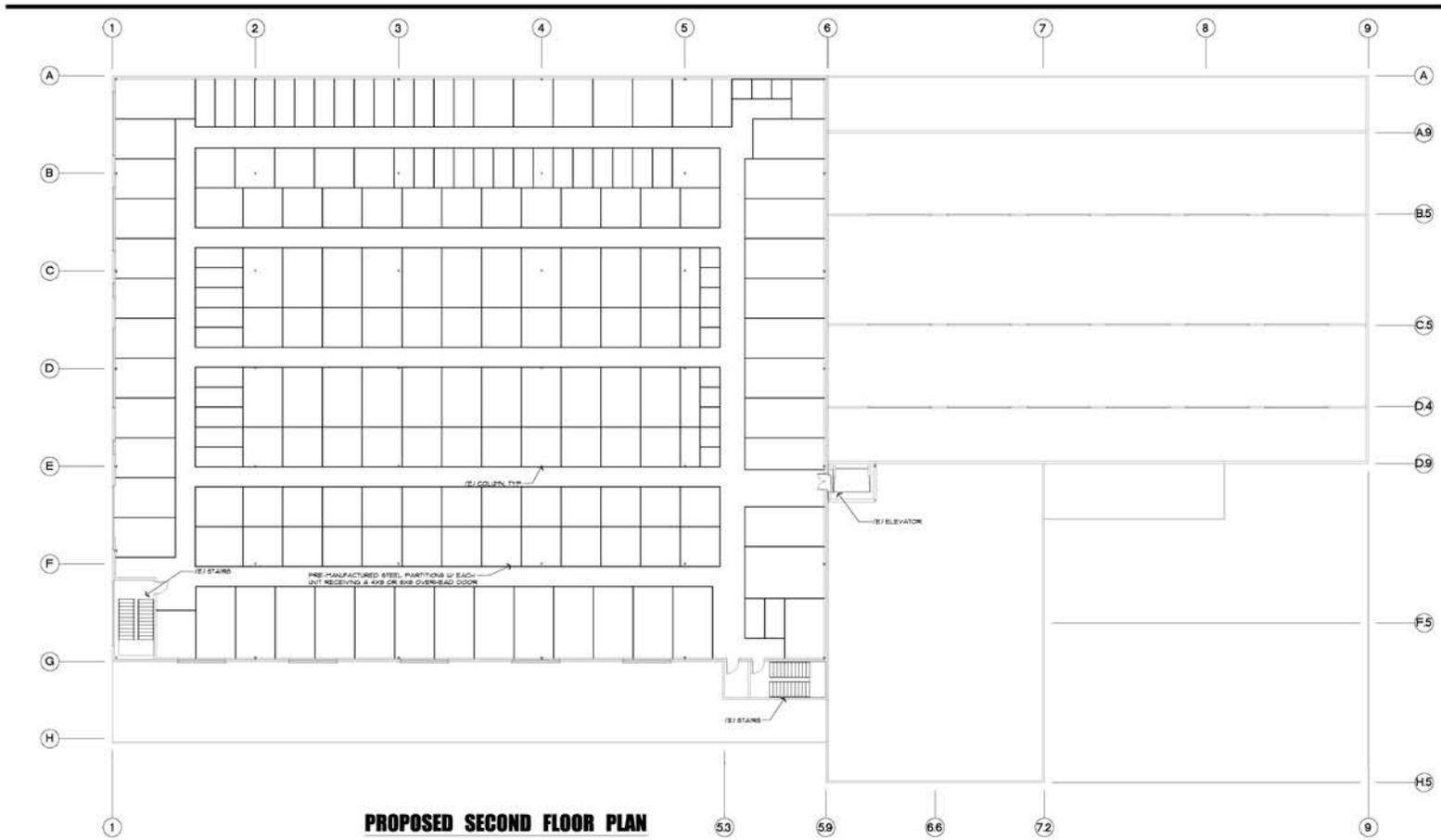
1" = 32' 0" / 1" = 64'

Stark Designs ARCHITECTURE

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916-624-6000 FAX
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Exhibit 1D – Proposed Second Floor Plan



PROPOSED SECOND FLOOR PLAN

EAST SAC SELF STORAGE

SACRAMENTO, CA
POTTER • TAYLOR & Co.

SD JOB NO.: 3.130
DATE: 03-15-11

3

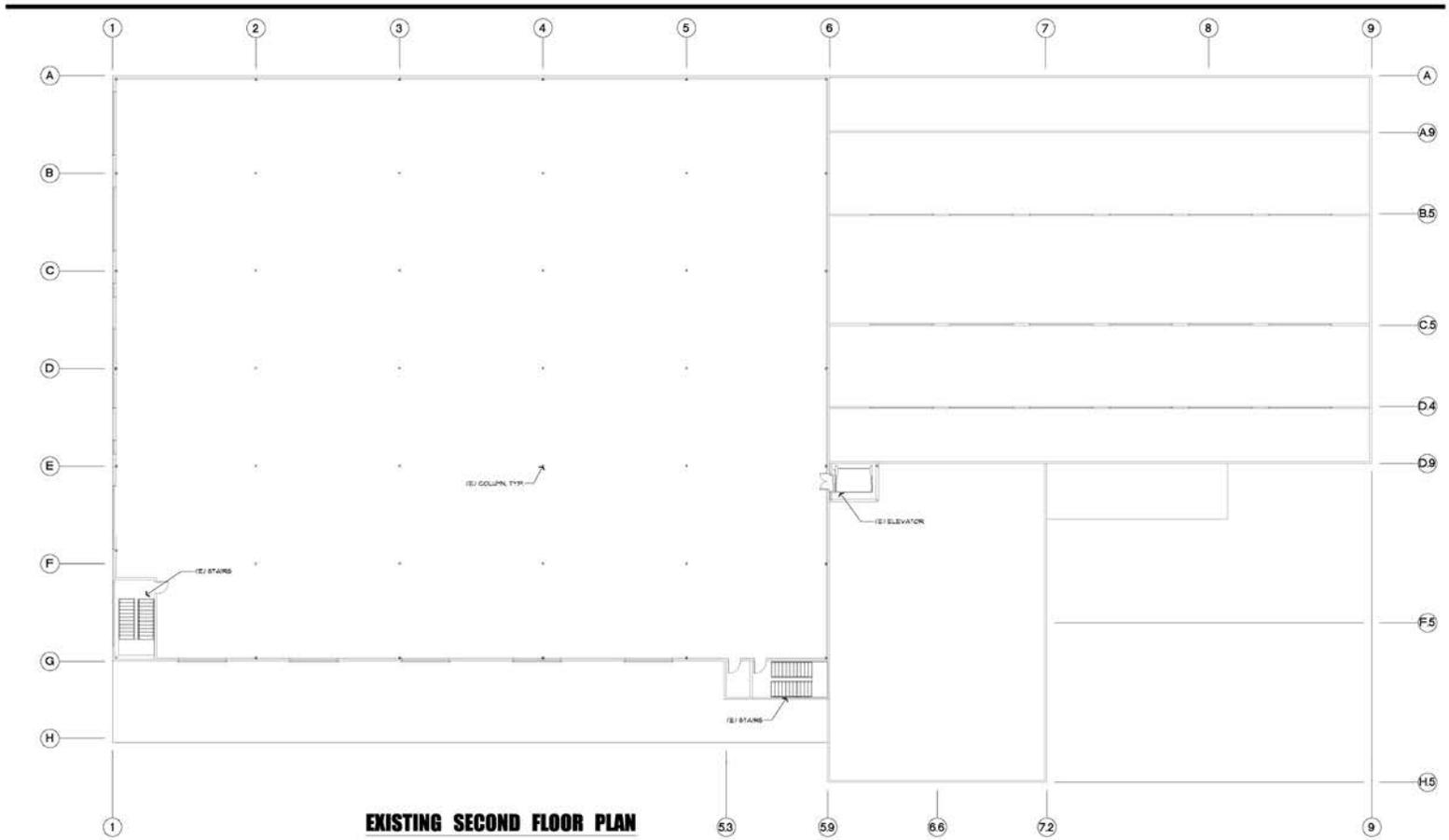
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Stark Designs ARCHITECTURE

2011 Sutter Anderson Dr.
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Exhibit 1E – Existing Second Floor Plan

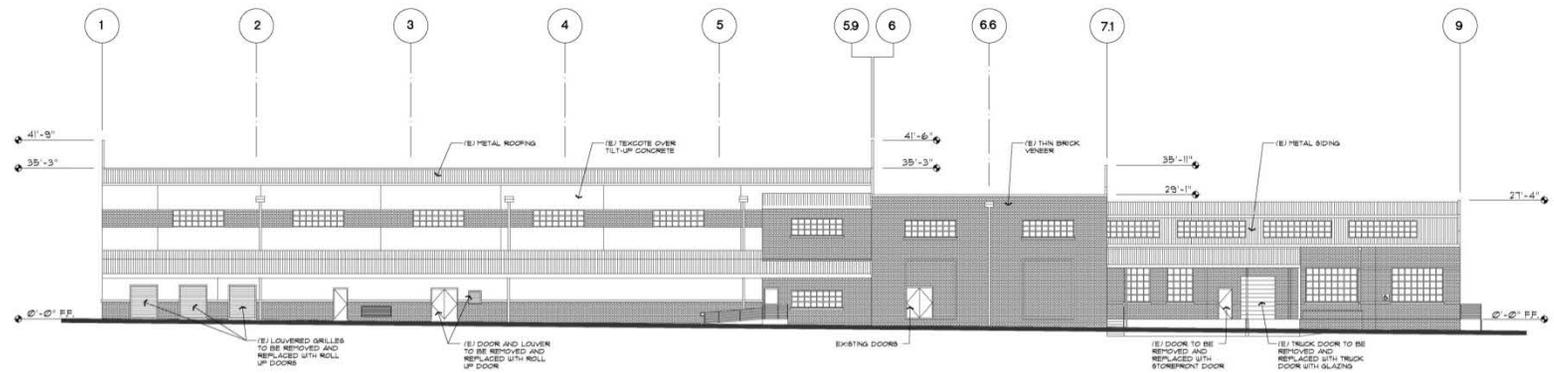


EAST SAC SELF STORAGE
 SACRAMENTO, CA
 POTTER-TAYLOR & Co.

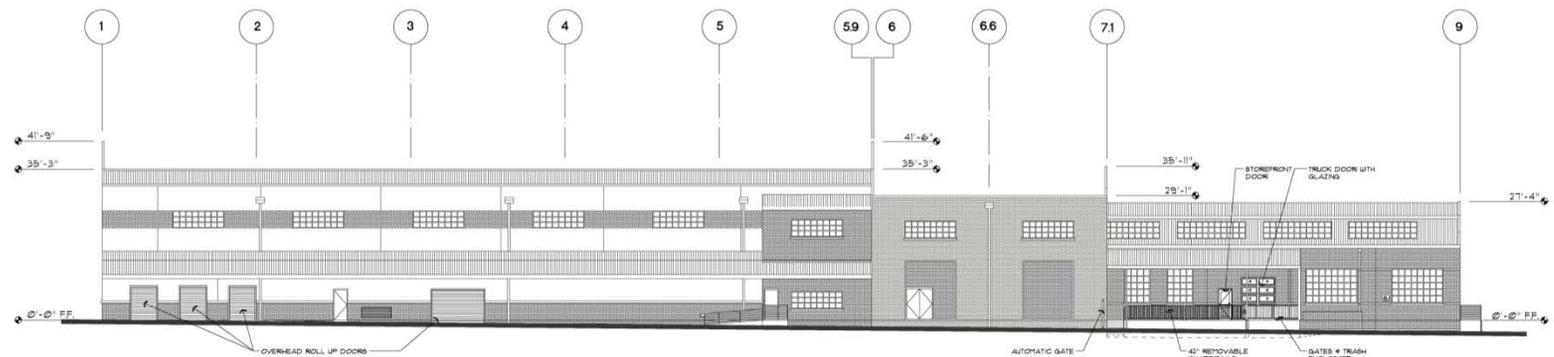
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Exhibit 1F – Existing and Proposed West Elevation



EXISTING WEST ELEVATION



PROPOSED WEST ELEVATION

EAST SAC SELF STORAGE
 SACRAMENTO, CA
 POTTER • TAYLOR & Co.

SD JOB NO.: 3.130
 DATE: 03-15-11

4

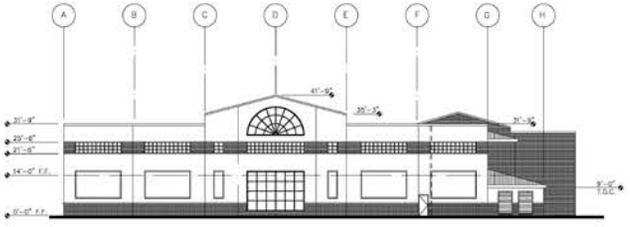
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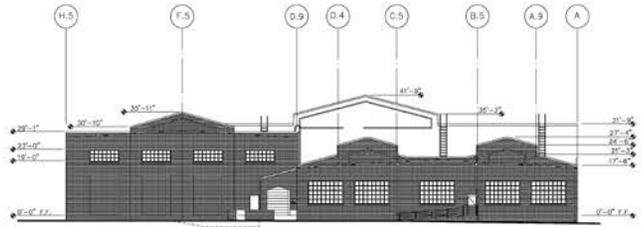
2411 Kahl Anderson Dr.
 Modesto, CA 95230
 530-530-2250
 530-534-0020 FAX
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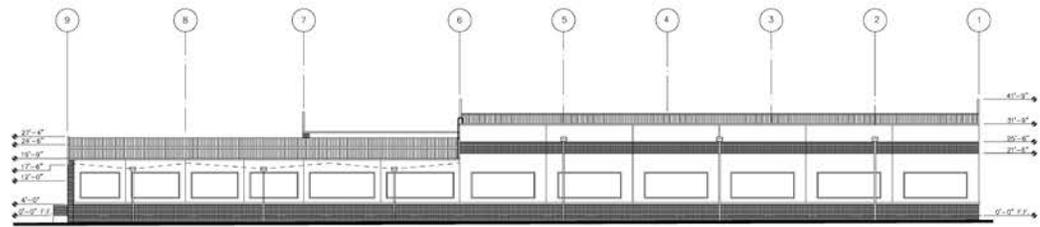
Exhibit 1G – Existing Building Elevations



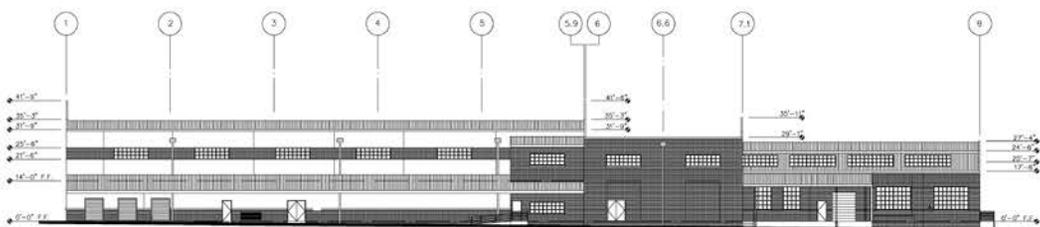
EXISTING NORTH ELEVATION



EXISTING SOUTH ELEVATION



EXISTING EAST ELEVATION



EXISTING WEST ELEVATION

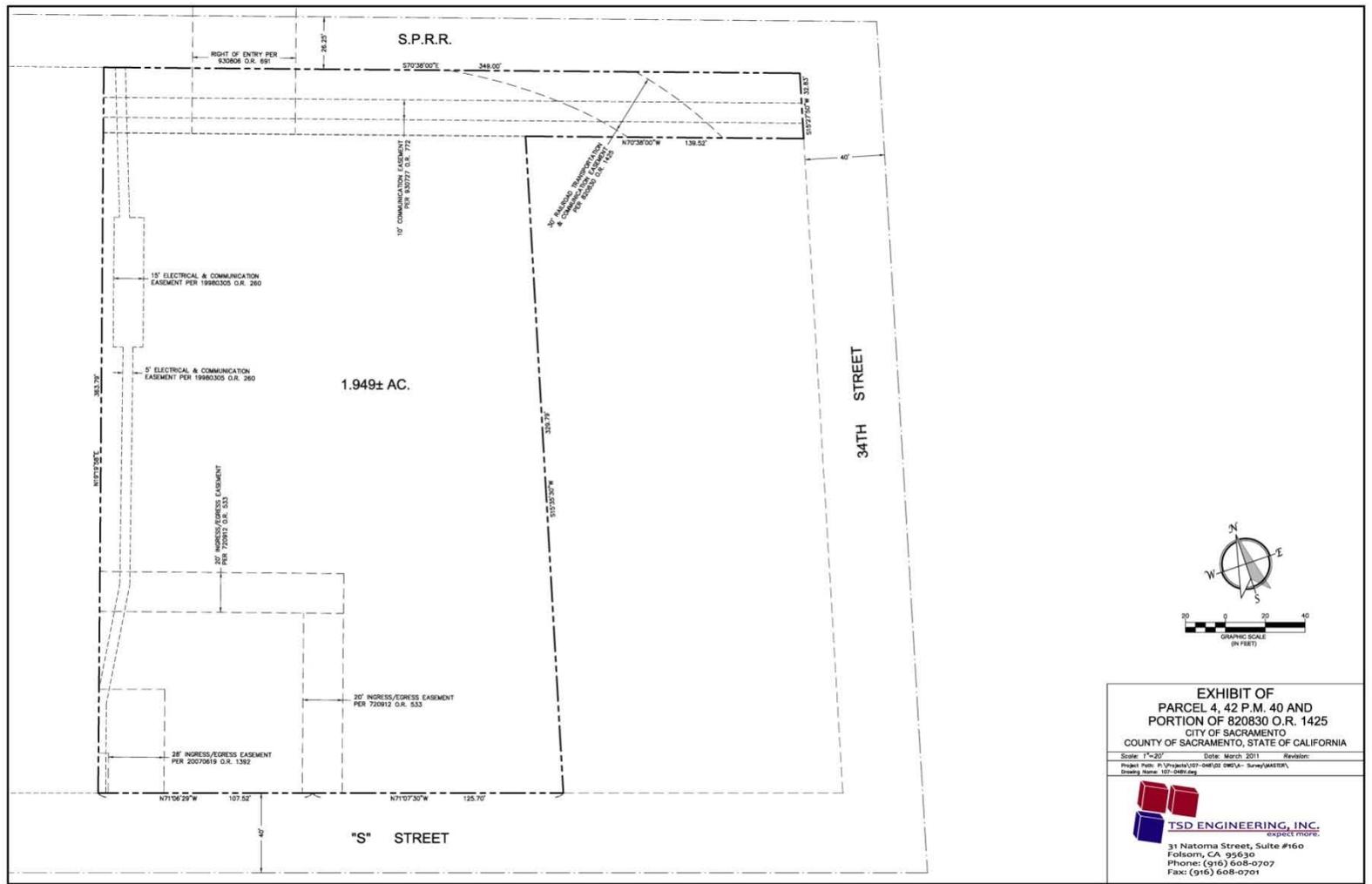
EAST SAC SELF STORAGE

SACRAMENTO, CA
POTTER • TAYLOR & Co.

SD JOB NO.: 3.130
DATE: 03-15-11
5
0 1/16 1/8

**Stark Designs
ARCHITECTURE**
2411 Taylor Anderson Dr.
Sacramento, CA 95825
916-988-2728
916-614-8829 FAX
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Exhibit 1H – Site Easements



Attachment 2 – Design Review Conditions of Approval



CITY OF SACRAMENTO
CALIFORNIA

COMMUNITY DEVELOPMENT
DEPARTMENT

PHONE 916-808-5656

300 Richards Blvd, 3rd Floor
SACRAMENTO, CA 95811

STAFF LEVEL PROJECT REVIEW

File Number:	P11-023	Applicant:	Potter-Taylor & Co.
Address:	3301 S Street	Date Filed:	03/29/2011
APN:	010-0063-011	Date Approved:	05/25/2011
Description:	East Sac Self Storage Exterior Rehab	Staff Contact:	David Hung, Assoc. Planner

STAFF ACTION AND CONDITIONS OF APPROVAL:

Staff has reviewed the proposed project, and approves it with the following conditions of approval:

1. Provide one aluminum overhead truck door with glazing and one storefront door adjacent to the front loading dock area per approved plans; doors and trim shall match existing window frame color.
2. Provide four overhead roll-up doors at the northwest side of building per approved plans; doors and trim shall match existing grille color.
3. Provide 42" high removable guardrails at the front loading dock per approved plans.
4. Provide automatic gates and fixed fencing at vehicular entrance and exit per approved plans; gates and fencing shall be decorative tube steel.
5. Final color selections for doors, gates and guardrail shall be subject to Design Review approval.
6. Any new signage shall require Design Review approval prior to issuance of sign permit.
7. Any additional exterior work not approved with this application shall be subject to Design Review approval.
8. The applicant shall obtain all necessary building permits prior to commencement of construction.
9. All other notes and drawings on the final plans as submitted by the applicant are deemed conditions of approval. Any changes to the final set of plans stamped by Design Review staff shall be subject to review and approval prior to any changes. Applicant shall comply with all current building code requirements.
10. A signed copy of the Affidavit of Zoning Code Development Standards, along with the Conditions of Approval, shall be scanned and inserted into the final set as a general sheet to be submitted for building permit.
11. This approval shall expire in three years from the approval date.


 David Hung
 Associate Planner

Attachment 3 – Regional Transit Letter



**Regional
Transit**

**Sacramento Regional
Transit District**
A Public Transit Agency
and Equal Opportunity Employer

Mailing Address:
P.O. Box 2110
Sacramento, CA 95812-2110

Administrative Office:
1400 29th Street
Sacramento, CA 95816
(916) 321-2500
(29th St. Light Rail Station/
Bus 38, 39, 50, 51, 58)

Light Rail Office:
2700 Academy Way
Sacramento, CA 95815
(916) 648-8400

Public Transit Since 1973

www.sacrt.com

May 9, 2011

David Hung
Associate Planner
City of Sacramento, Environmental Planning Services
300 Richards Boulevard, 3rd Floor
Sacramento, CA 95811

NAME OF DEVELOPMENT: East Sac Self Storage

CONTROL NUMBER: P11-023

TYPE OF DOCUMENT: Special Permit

The East Sac Self Storage project proposes to re-use an existing 74,588 sq.ft. vacant light industrial building for a mini-storage facility containing 400 to 500 unit spaces with 1,558 sq.ft. of office space. The site is located in the Alhambra Corridor Special Planning Area (ACSPA) and currently zoned C-2. It contains approximately 53 parking spaces. The site is located on the north side of S Street approximately 300 feet west of 34th Street in the City of Sacramento.

The project site is within 1/8 mile of several bus stops and served by the following routes:

on T Street:

- Bus route 38 provides 60-minute service seven days per week.
- Bus route 67 provides 30-minute weekday service and 60-minute Saturday service.
- Bus route 68 provides 30-minute weekday service and 60-minute weekend service.

on Stockton Boulevard:

- Bus route 109 is an Express route and provides weekday peak service.
- Regional Transit's *2035 TransitAction Plan* identifies Stockton Boulevard to be a Hi-bus corridor in the future. Hi-Bus is a high quality, high capacity and high frequency service, which may be planned as Bus Rapid Transit or Enhanced Bus.

Additionally, the site is just at 1/4 of a mile east of the 29th Street light rail station.

Regional Transit (RT) stresses the importance of appropriate land use and transit compatibility to the vitality of the bus and light rail system. An effective transit system is dependent upon land use patterns within 1/2 mile of bus stops and light rail stations. It is important for this development proposal to be designed to support transit as well as be consistent with the City's ACSPA, which supports transit. Therefore, RT cannot support a mini-storage facility in this location.

David Hung

- 2 -

May 9, 2011

This project is subject to the special rules and regulations of the ACSPA. Goals of these guidelines apply directly to this project site and state the following:

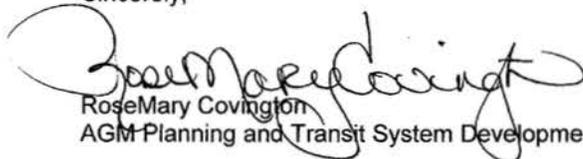
- A. Maintain and improve the character, quality and vitality of individual neighborhoods;
- B. Maintain the diverse character and housing opportunities provide in these urban neighborhoods;
- C. Provide the opportunity for a balanced mixture of uses in neighborhoods adjacent to transit facilities and transportation corridors;
- D. Maintain the neighborhood character of existing commercial neighborhoods while allowing for limited office to serve the medical complex in this area; and
- E. Provide the opportunity for reuse and rehabilitation of heavy commercial and industrial neighborhoods to take advantage of close-in living while reducing the number of obsolete and underutilized buildings and sites.

Permitted uses in a C-2 zone call for Retail and Personal Service uses and do not include a mini-storage in its list of approved uses. The surrounding neighborhood includes a recently completed high-density, mixed use, transit-supportive housing project to the west and older small lot residential homes to the south. The adjacent parcel to the east is an existing parking lot, also zoned C-2 and designated by the ACSPA for "Community Neighborhood Commercial and Office."

Re-development or re-use of this parcel into a mini-storage is clearly not consistent with the above referenced land designation and goals of the ACSPA, which aims to maintain and improve the character, quality and vitality of this neighborhood. RT's Transit-oriented Guidelines recommend land uses within a ½ mile of light rail stations be a mix of employment, residential and retail uses, with a minimum floor area ratio of 1.0. Auto-oriented land uses such as mini-storage facilities should not be within walking distance of light rail stations or future Hi-bus service.

Thank you for the opportunity to comment. Please send any subsequent documents and hearing notices that pertain to this project as they become available. If you have further questions regarding these recommendations, please contact Chris Pair at (916) 556-0514 or cpair@sacrt.com.

Sincerely,


RoseMary Covington
AGM Planning and Transit System Development, RT

c: Traci Canfield, Planner, RT
Chris Pair, Assistant Planner, RT

Attachment 4 – Distances to Light Rail Station



Attachment 5 – Land Use & Zoning Map

